

IOWA DEPARTMENT OF TRANSPORTATION

TO OFFICE: Right of Way **DATE:** March 13th, 2013
ATTENTION: M. J. Sankey **REF. :** Woodbury County
FROM: Adam Juel (Foth) Proj. No. BRF-376-1(6)—38-97
PIN: 10-97-376-020
SUBJECT: Right of Way Submittal (D5)

This project involves the IA 376 northbound bridge (FHWA #53070) replacement over the Burlington Northern Railroad, the Canadian National (Union Pacific) Railroad and Taft Street, approximately 1.2 miles north of County Road D-12.

Traffic will be maintained head to head on the southbound lanes via an existing median crossover south of the bridge and new temporary crossover at the north end of the project.

Access rights will not be acquired on this project.

No printed plan sheets are included with this submittal. The MicroStation and GEOPAK files, chains, and profiles are not included with this submittal, but are described in attached *Project Documentation 97376006.xls* file. The electronic files described in the documentation file will be submitted separately to this project's Office of Design representative.

Construction need lines required for this project are at the catch (intercept) point and have been added to the design file. The Office of ROW is advised to attach the following design models:

File	Model	Level	Need Type
97376006.dsn	Road_Design_Line_Work	dsnGroundlineIntercept	Permanent/Temporary

No culverts are included with the project.

Two potential final borrow sites have been recommended. The borrow sites are located East of the project within the existing Right-Of-Way of US 75 near 28th Street. The borrow locations are shown in Braun Intertec's S2 document dated 2/24/13.

No special mitigation areas required for the project.

The proposed foreslopes and a section of proposed ditch along the East side of IA 376 extend beyond the existing Right-Of-Way from approx. Sta. 196+00 to Sta. 206+00.

Utilities with known facilities on this project are as follows:

Mid American - Overhead Electric
Century Link – Telephone
Verizon - Fiber Optic
Qwest Local Net - Fiber Optic
Fibercomm - Fiber Optic
Longlines (Pioneer Holding) - Fiber Optic
City of Sioux City – Sanitary Sewer, Storm Sewer and Water

The current letting date is 12/16/2014.

You may indicate your acceptance or request additional information by e-mail.

Attach (2) – D5 Plans, Project Documentation 97376006.xls

cc: M. J. Kennerly
D. L. Maifield
K. D. Nicholson
R. L. Stanley
Judy Lensing
E. J. Ranney
D. A. Widick
S. J. Gent
T. Crouch
E. C. Wright
J. N. McCollough
J. Vortherms
M. Hobbs
T. Lazarowicz
S. Tymkowicz
N. L. McDonald
G. A. Novey
D. R. Claman
J. P. Rost
S. C. Marler
L. C. Funnell
T. L. Gettings
M. A. Swenson
J. W. Smith
D. A. Popp
B. Bradley
E. Engle
D. Manley

WOODBURY CO.

BRIDGE REPLACEMENT - PPCB
BRF-376-1(6)--38-97

LETTING DATE
DEC. 16, 2014

Iowa Department of Transportation
Highway Division

PLANS OF PROPOSED IMPROVEMENT ON THE
PRIMARY ROAD SYSTEM
WOODBURY COUNTY
BRIDGE REPLACEMENT - PPCB
IN SIOUX CITY OVER R.R. AND TAFT STREET
0.1 MILES SOUTH OF CO. RD. D-12 (NBL)

SCALES: As Noted

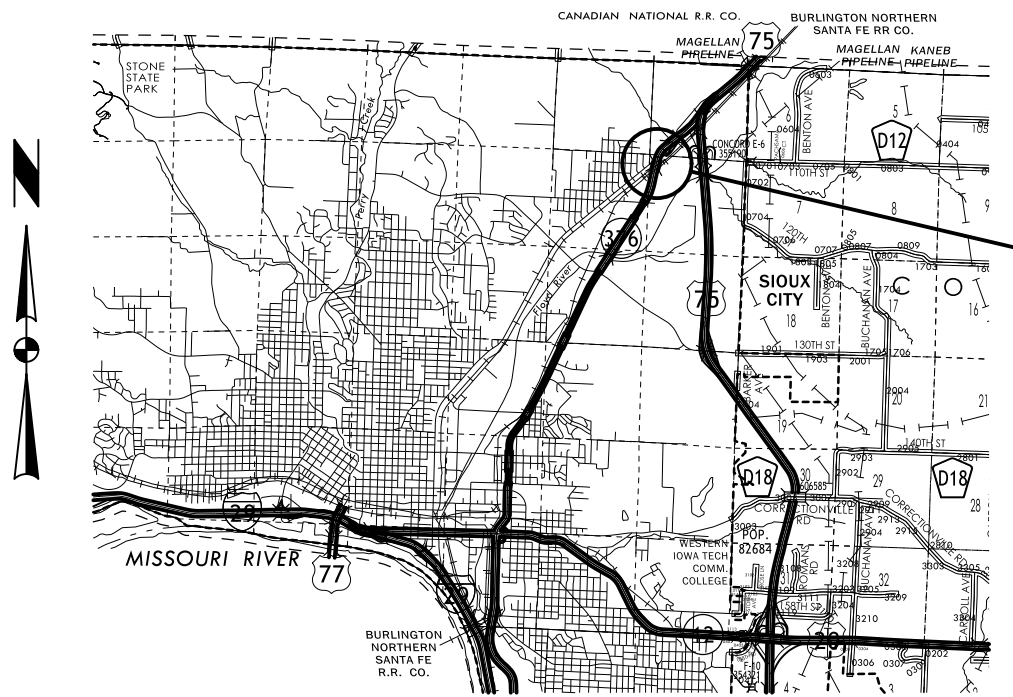
Refer to the Proposal Form for list of applicable specifications.

NO MILEAGE SUMMARY



REVISIONS

TOTAL
PROJECT IDENTIFICATION NUMBER
10-97-376-020
PROJECT NUMBER
BRF-376-1(6)--38-97
R.O.W. PROJECT NUMBER
STPN-376-1(8)--2J-97



PROJECT LOCATION

Borrow = 36,000 CY
(Includes 30% Shrink)

For Project Location Map
Refer to Sheet A.2

PROJECT SCHEDULE	
EVENT	DATE
D3	11/01/12
S2	03/01/13
B1	03/06/13
D5	03/13/13
S4	04/15/13

DESIGN DATA URBAN			
2015 AADT	8,400	V.P.D.	
2035 AADT	13,000	V.P.D.	
2035 DHV	--	V.P.H.	
TRUCKS	11	%	
Total Design ESALs	--		

INDEX OF SEALS		
SHEET NO.	NAME	TYPE
A.1	X	Primary Signature Block
	X	

PRELIMINARY PLANS

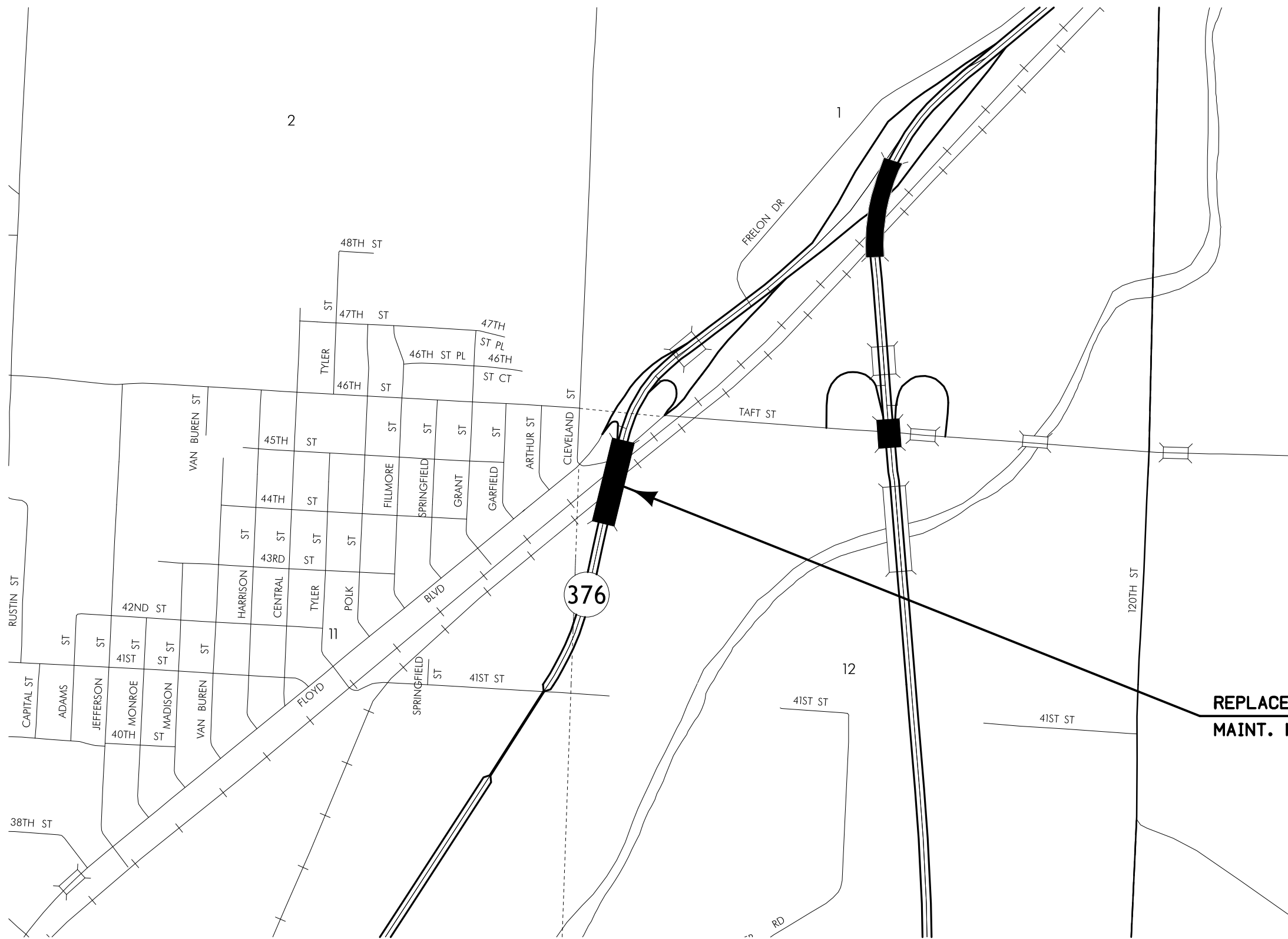
Subject to change by final design.

D5 PLAN - Date: Mar. 13, 2013





T-89N

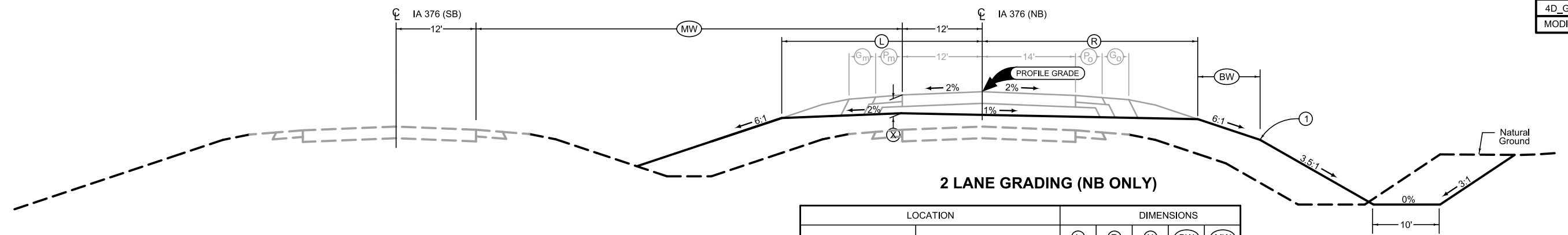


REPLACE NORTHBOUND BRIDGE
MAINT. NO. 9799.OR376

R-47W

Location Map





LOCATION		DIMENSIONS				
ROAD IDENTIFICATION	STATION TO STATION	(L) Feet	(R) Feet	(X) Inches	(BW) Feet	(MW) Feet
IA 376	196+00.00 - 206+45.86	28	32	16	12	66
IA 376	212+19.37 - 213+31.10	28	32	16	12	66

Normal section shown may be modified appropriately in areas of superelevated curves or other locations specifically designated by the Engineer.

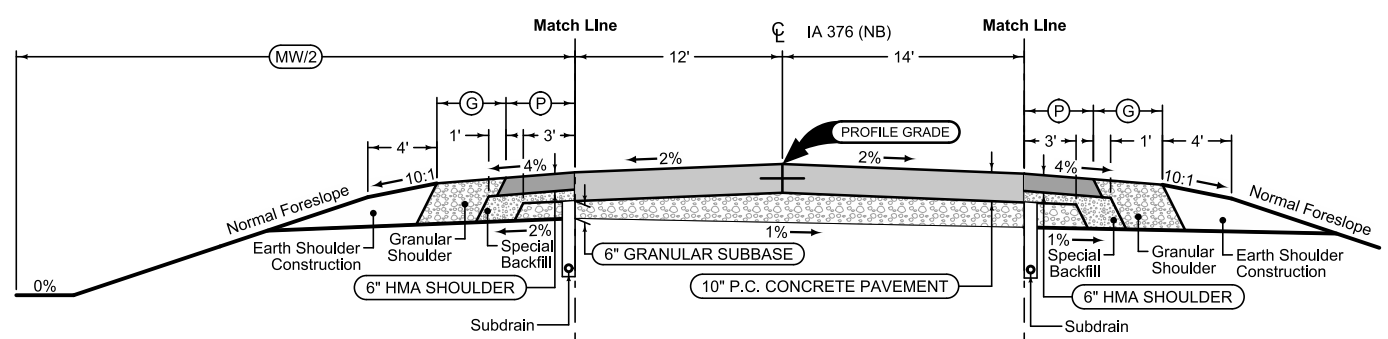
① Refer to project plan and cross sections for specific location of foreslope change.

See Plan & Profile sheets and cross sections for additional details of ditches and backslopes.

Combination Shoulder
Shoulder Jointing:
Longitudinal joint: B

STATION TO STATION		(P) Feet	(G) Feet
196+00.00	204+44.44	4	2
212+68.92	213+31.10	4	Varies

2_C_
10-19-10



Combination Shoulder
Shoulder Jointing:
Longitudinal joint: B

Direction of Travel	STATION TO STATION		(P) Feet	(G) Feet
		BEGIN STATION	END STATION	
	196+00.00	205+10.18	4	4
	213+10.83	213+31.10	4	Varies

4_C_
10-19-10

Section shown in the direction of traffic.

Mainline Jointing:
Transverse joints: CD at 20' spacing
Longitudinal joint: L-2

Direction of Travel	STATION TO STATION		(MW) Feet
		BEGIN STATION	END STATION
NB	196+00.00	205+64.39	66

4DP_
10-19-10

See Tab 100-24 for pavement quantities.
See Tab 112-9 for shoulder quantities.

IA 376 RECONSTRUCTION (NB LANES ONLY)

F02

SURVEY SYMBOLS

- ENU edge of unpaved entrance&parking
- x- FW fence wire
- ← FLD Flowline of Ditch
- TOE toe of slope
- TB top of bank
- EP edge of paved roads
- C center of roadway
- OHE Overhead electric line
- PP power pole
- GW GW Guy Wire
- Default.Point Default Point Feature
- TP TPD telephone pedestal
- St.S. --- STA storm sewer 1st co.
- CMP corrugated metal pipes
- MB MB Mailbox
- SIGN SI sign
- RET retaining walls
- FWD wood fence
- ⊕ MHE Electrical Manhole
- T1 --- TLA buried telephone line 1st co.
- ⊕ CPT Control Point Temporary
- CC control check
- CS concrete slab
- GR Ground Field Survey
- PIP pipes(cast iron,steel,tile,etc)
- ⊕ MHSN Sanitary Manhole
- ⊕ CPS Control Point Set
- ⊕ CPF Control Point Found
- ⊕ TDC tree deciduous
- TLN tree line
- RCP reinforced conc.pipe
- ▲ SCR section corner
- ROW right of way rails
- HDG hedge
- LC LC lot corner
- SHG granular shoulder
- REF reference tie points
- BD Bridge Deck
- BCL bridge centerline
- EG edge of gravel road
- ⊕ SHR shrub
- SH shoulder
- CON concrete or a/c slab
- MIS miscellaneous
- SHP paved shoulder

UTILITY LEGEND

Where public utility fixtures are shown as existing on the plans or encountered within the construction area, it shall be the responsibility of the contractor to notify the owners of those utilities prior to the beginning of any construction. The Contractor shall afford access to these facilities for necessary modification of services. Underground facilities, structures and utilities have been plotted from available surveys and records, and therefore their locations must be considered approximate only. It is possible there may be others, the existence of which presently not known or shown. It is the Contractor's responsibility to determine their existence and exact location and to avoid damage thereto. No claims for additional compensation will be allowed to the Contractor for any interference or delay caused by such work.

The Contractor is required to utilize the utility One-Call service at (800) 292-8989 at least 48 hours prior to excavating anywhere on the project.

The following utility companies are known to have facilities on the project:

CITY OF SIOUX CITY
 Timothy J. Higgins
 712-279-6164
 thiggins@sioux-city.org

MIDAMERICAN ENERGY
 Barb Parks
 712-233-4866
 BTParks@midamerican.com

Verizon
 Janette Harris
 972-729-6650
 janette.l.harris@verizonbusiness.com

QWEST LOCAL NET
 qwest.lcall@qwest.com

FIBERCOMM
 Mike Roarson
 712-224-2020
 mroarson@fibercom.net

LONGLINES (PIONEER HOLDING)
 Miles Patton
 712-271-5577

CABLE ONE
 Charles (Bud) Kumpf V
 712-224-7705

PAETEC (FORMERLY MCLEODUSA)
 Steven Gritman
 319-790-7510
 Steven.Gritman@paetec.com

MAGELLAN MIDSTREAM PARTNERS
 Tim Kassen
 918-574-7351
 tim.kassen@magellanlp.com

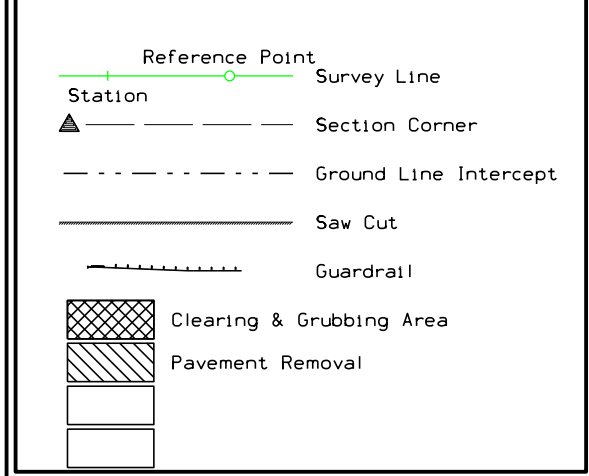
- San. ---
- St.S. ---
- W ---
-
- E1 ---
- G ---
- F03 ---
- F02 ---
- F0 ---
- F04 ---

PLAN VIEW COLOR LEGEND OF PLAN AND PROFILE SHEETS

LINEWORK	Design Color No.	
Green	(2)	Existing Topographic Features and Labels
Blue	(1)	Proposed Alignment, Stationing, Tic Marks, and Alignment Annotation
Magenta	(5)	Existing Utilities
SHADING		
Design Color No.		
Yellow	(4)	Highlight for Critical Notes or Features
Red	(3)	Delineates Restricted Areas
Lavender	(9)	Temporary Pavement Shading
Gray, Light	(48)	Proposed Pavement Shading
Gray, Med	(80)	Proposed Granular Shading
Gray, Dark	(112)	Proposed Grade and Pave Shading
Brown, Light	(236)	Grading Shading
Tan	(8)	Proposed Sidewalk Shading
Blue, Light	(230)	Proposed Sidewalk Landing Shading
Pink	(11)	Proposed Sidewalk Ramp Shading

PROFILE VIEW COLOR LEGEND OF PLAN AND PROFILE SHEETS

LINEWORK	Design Color No.	
Green	(2)	Existing Ground Line Profile
Blue	(1)	Proposed Profile and Annotation
Magenta	(5)	Existing Utilities
Blue, Light	(230)	Proposed Ditch Grades, Left
Black	(0)	Proposed Ditch Grades, Median
Rust	(14)	Proposed Ditch Grades, Right



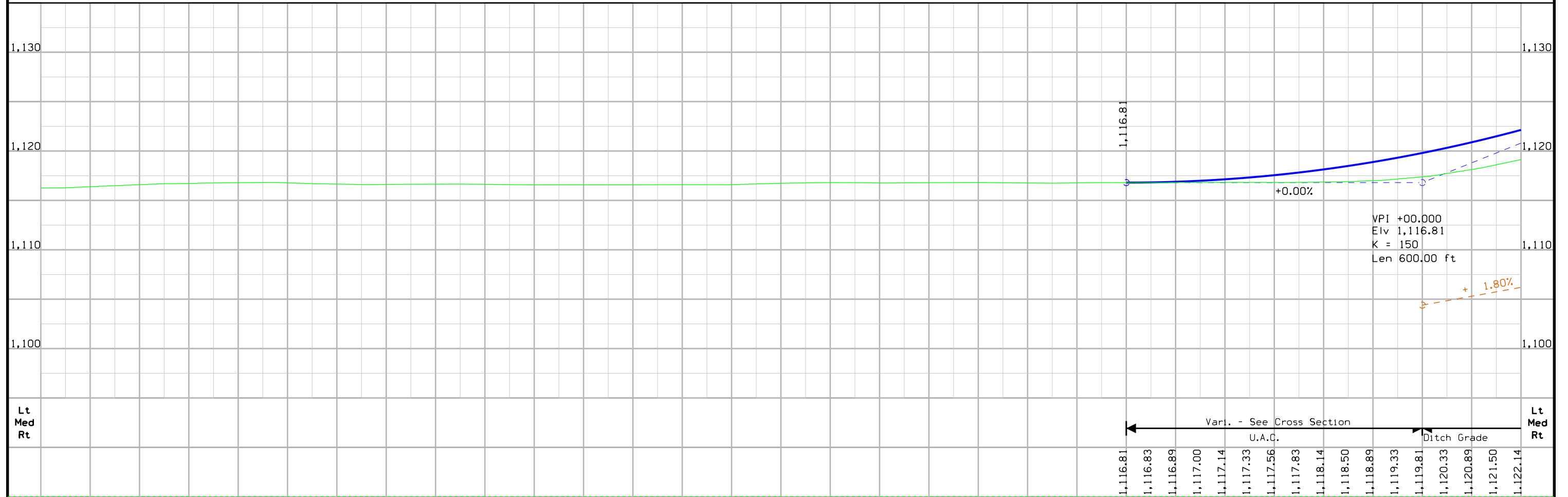
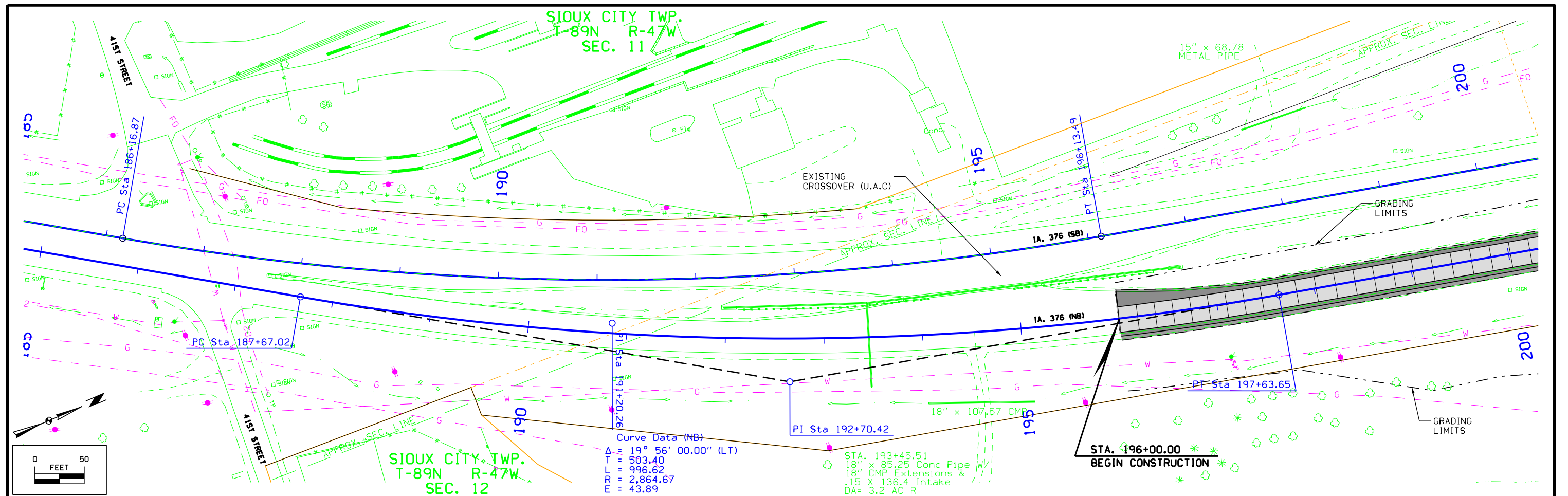
RIGHT-OF-WAY LEGEND

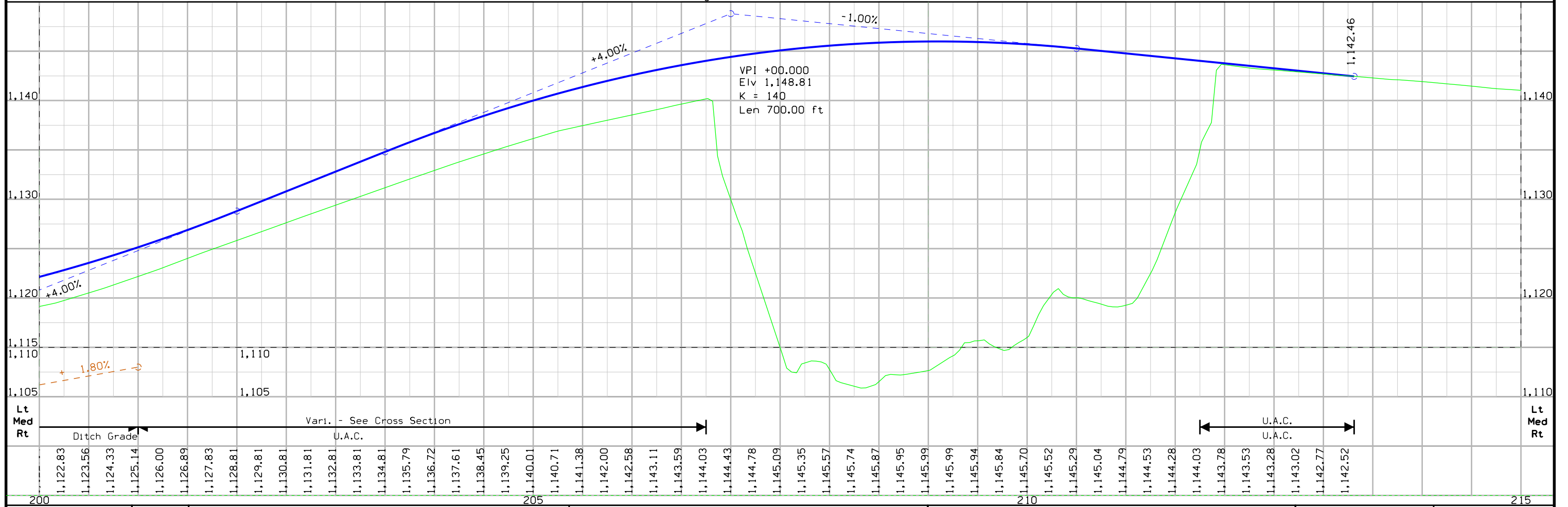
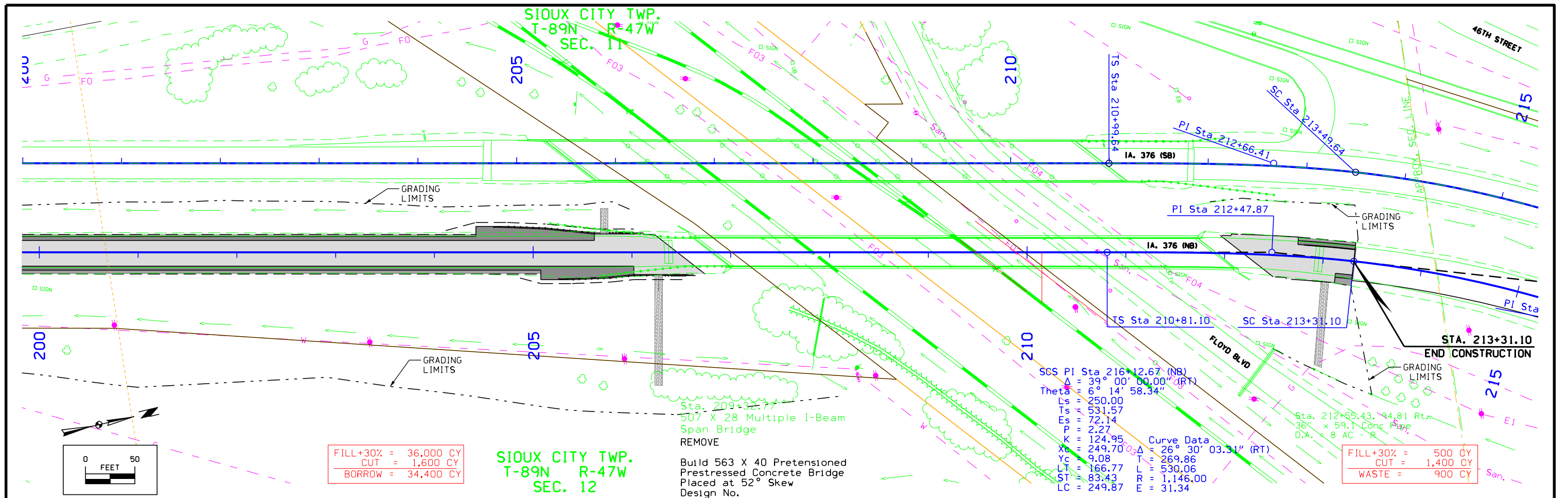
- ▲ Proposed Right-of-Way
- ▲ Existing and Proposed Right-of-Way
- ▲ Easement and Existing Right-of-Way
- Borrow
- Easement (Temporary)
- ⊕ Easement
- X Excess
- A/C Access Control

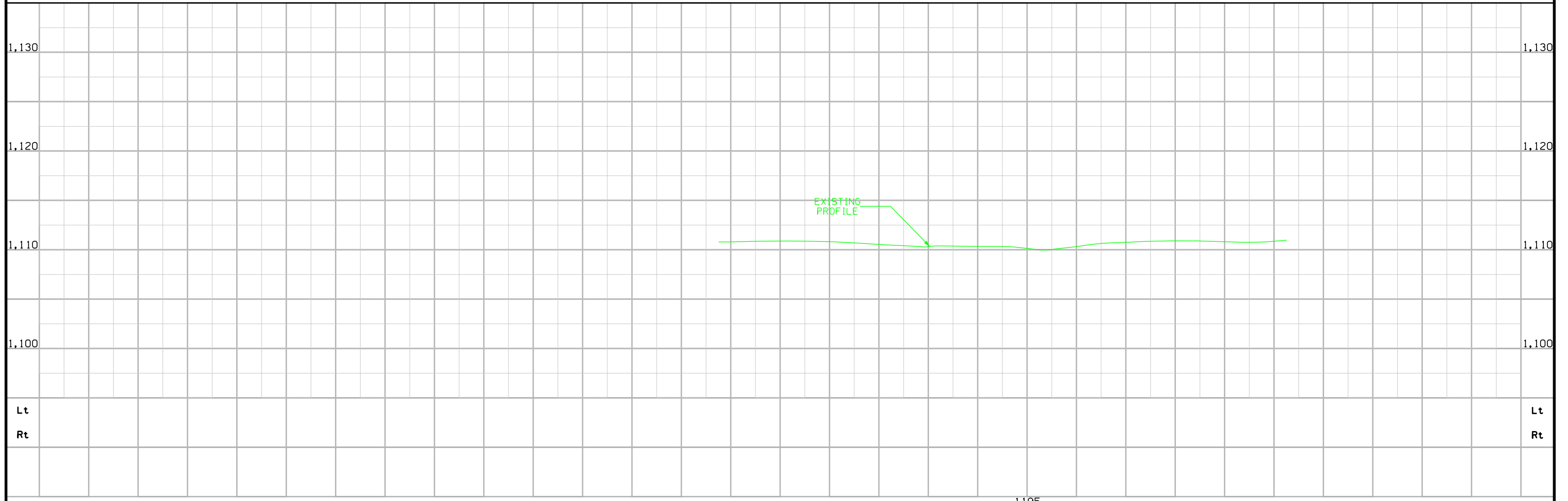
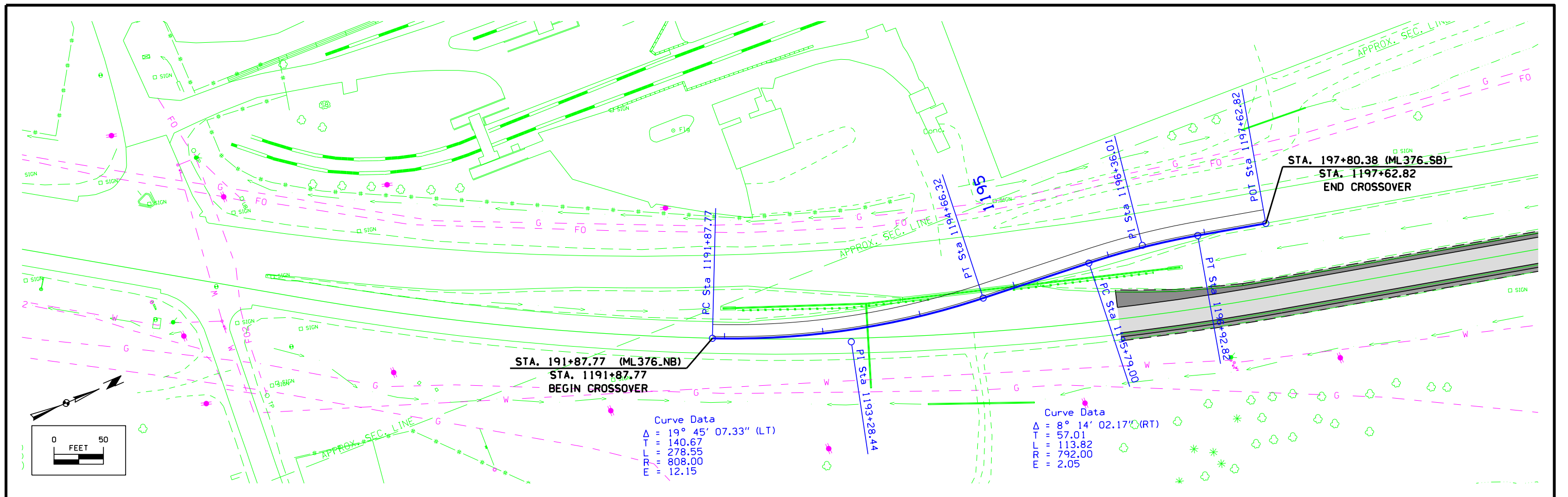
PLAN AND PROFILE LEGEND AND SYMBOL INFORMATION SHEET

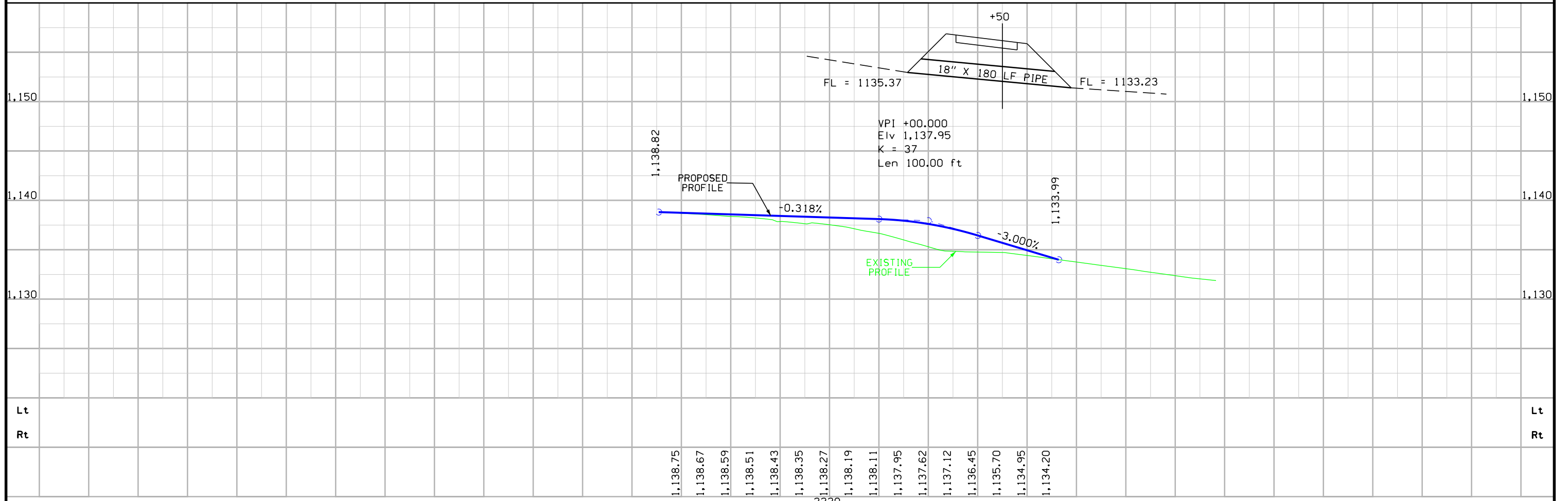
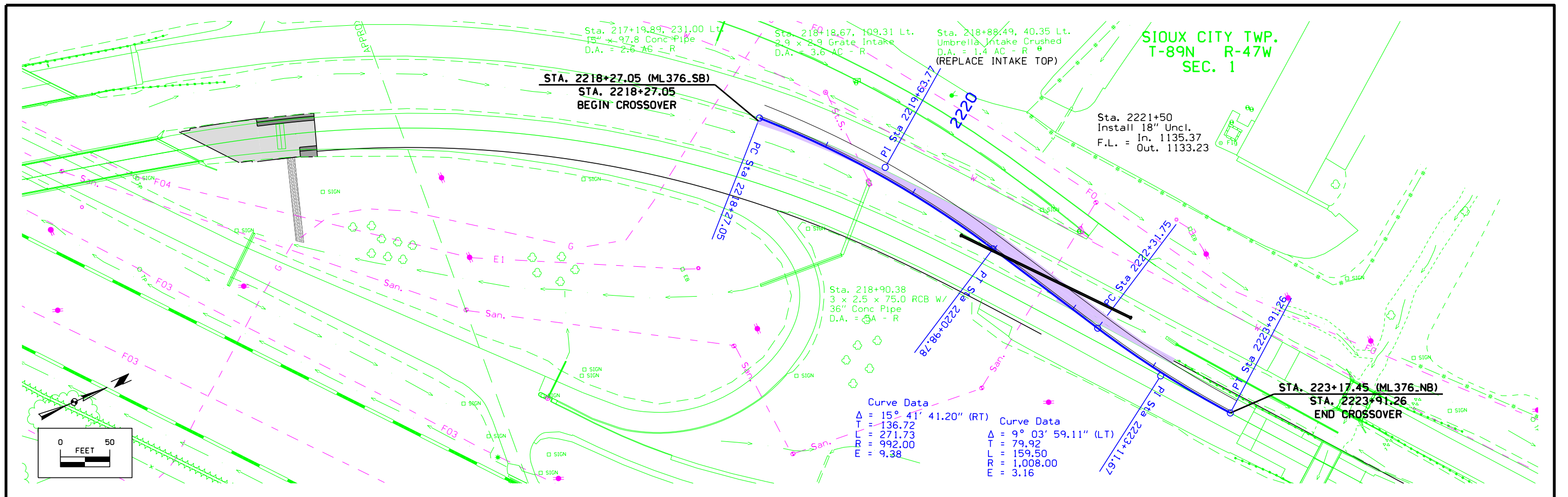
(COVERS SHEET SERIES D, E, F, & K)

IOWA 1-CALL# 1-800-292-8989









DATUM INFORMATION

THE DATUM PLANE FOR THIS SURVEY IS RELATIVE TO N.A.V.D. 88 DATUM. BENCHES WERE RAN FROM IADOT BM NO. ___ TO NO. ___ FOR A CHECK THEN A CLOSED LOOP WAS RAN FROM NO. ___ TO NO. ____.

ALL CONTROL POINT COORDINATES SHOWN ARE LOCAL PROJECT PLANE (GROUND) COORDINATES.
 CONVERSION EQUATION GRID TO GROUND: GROUND COORD = (STATE PLANE - HOLD POINT) 1/GRID FACTOR + HOLD POINT
 CONVERSION EQUATION GROUND TO GRID: GRID COORD = (GROUND - HOLD POINT) GRID FACTOR + HOLD POINT

HOLD POINT = G021 NORTH EAST GRID FACTOR 1/GRID FACTOR
 580322.54 2455353.37 0.999936506 1.000063498

BENCH MARKS

IOWA BENCHMARKS:				ELEVATION
No. 312	Sta. 177+05.18	24.02 RT	SET IRON P IN -----	1113.796
No. 313	Sta. 187+26.17	47.29 RT	FND 1/2 REBAR -----	1114.554
No. 314	Sta. 205+83.31	42.58 LT	FND 1/2 REBAR -----	1135.385
No. 315	Sta. 223+89.28	34.59 LT	FND 1/2 REBAR -----	1130.743
No. 316	Sta. 211+74.92	43.59 LT	FND 1/2 REBAR -----	1140.025
No. 317	Sta. 241+67.79	23.96 RT	SET IRON P IN -----	1122.514
No. 500	Sta. 176+05.06	32.52 RT	CUT X ON TOP BOLT OF FIRE HYDRANT -----	1114.503
No. 501	Sta. 186+46.07	46.53 RT	CUT X ON N BOLT OF FIRE HYDRANT -----	1114.341
No. 502	Sta. 205+64.52	71.46 LT	CUT SQUARE SE HANDRAIL OF SB BRIDGE -----	1142.188
No. 503	Sta. 211+75.19	18.43 LT	FND IHC BUTTON NW WHEELGUARD NB BRIDGE -----	1144.435
No. 504	Sta. 224+46.01	34.02 RT	FND IHC BUTTON NE HANDRAIL NB BRIDGE -----	1132.281
No. 505	Sta. 211+02.63	71.16 LT	FND CUT SQUARE -----	1146.007
No. 506	Sta. 239+99.42	44.60 LT	FND DOT BUTTON INLET HDWL RCB -----	1119.286

DETAILS OF REFERENCE INFORMATION

All References are Plumb Distances
 unless otherwise noted.

PARCEL CHECK BY PROJ UPDATED 04/25/13 10:58

PAGE: 1

R2360003 PARCEL CHECK LIST BY PROJECT NUMBER

COUNTY : WOODBURY PROJECT NO. :STPN-376-1(8)--2J-97

PIN: 10-97-376020-00

CONSTRUCTION NO.:BRF-376-1(6)--38-97

ASSIGNED TO: SJD

DESCRIPTION : In Sioux City Over R.R. and 0.1 Mile S. Of Co. Rd. D-12(NBL)

PARCEL	KEY	OWNER	TYPE	R/W W.D OR EASE.	BORROW W.D OR EASE.	HOUSE OR OTHER	COMMERCIAL	OCC ENVIRONMENTAL CONCERNS
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0001	26733	AVERY BROTHERS LLC	FEE STATE OF IOWA	69,459.00	WD SQFT			
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0002 R	26734	CHICAGO CENTRAL & PACIFIC	FEE STATE OF IOWA	30,035.00	EASE SQFT			
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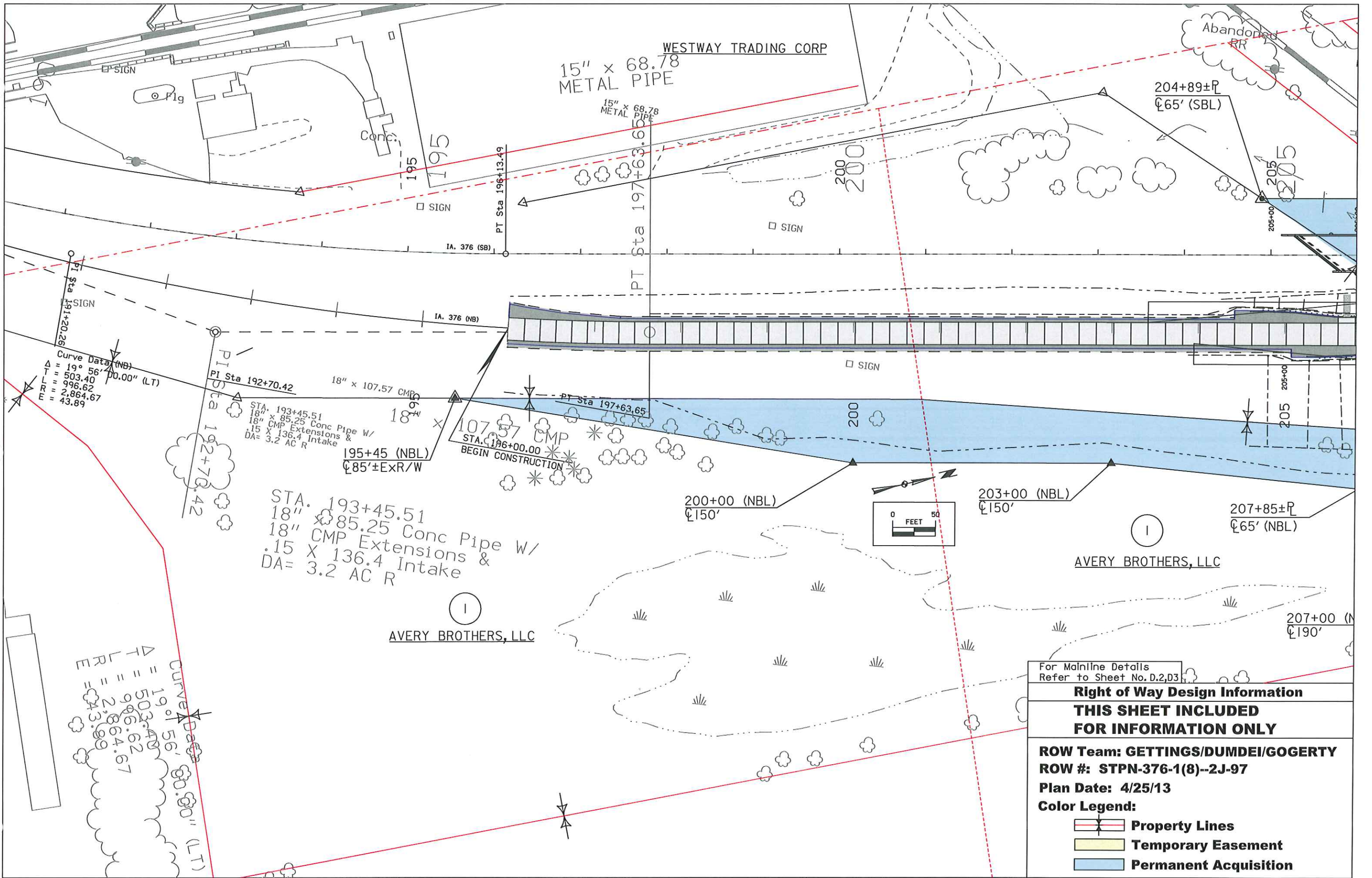
0003 R	26735	BNSF RAILWAY CO	FEE STATE OF IOWA	39,218.00	EASE SQFT			
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STATE OF IOWA

69,253.00 EASEMENT SQFT

69,459.00 WARRANTY DEED SQFT

3 TOTAL PARCELS ON PROJECT



For Mainline Details
Refer to Sheet No. D.2,D3

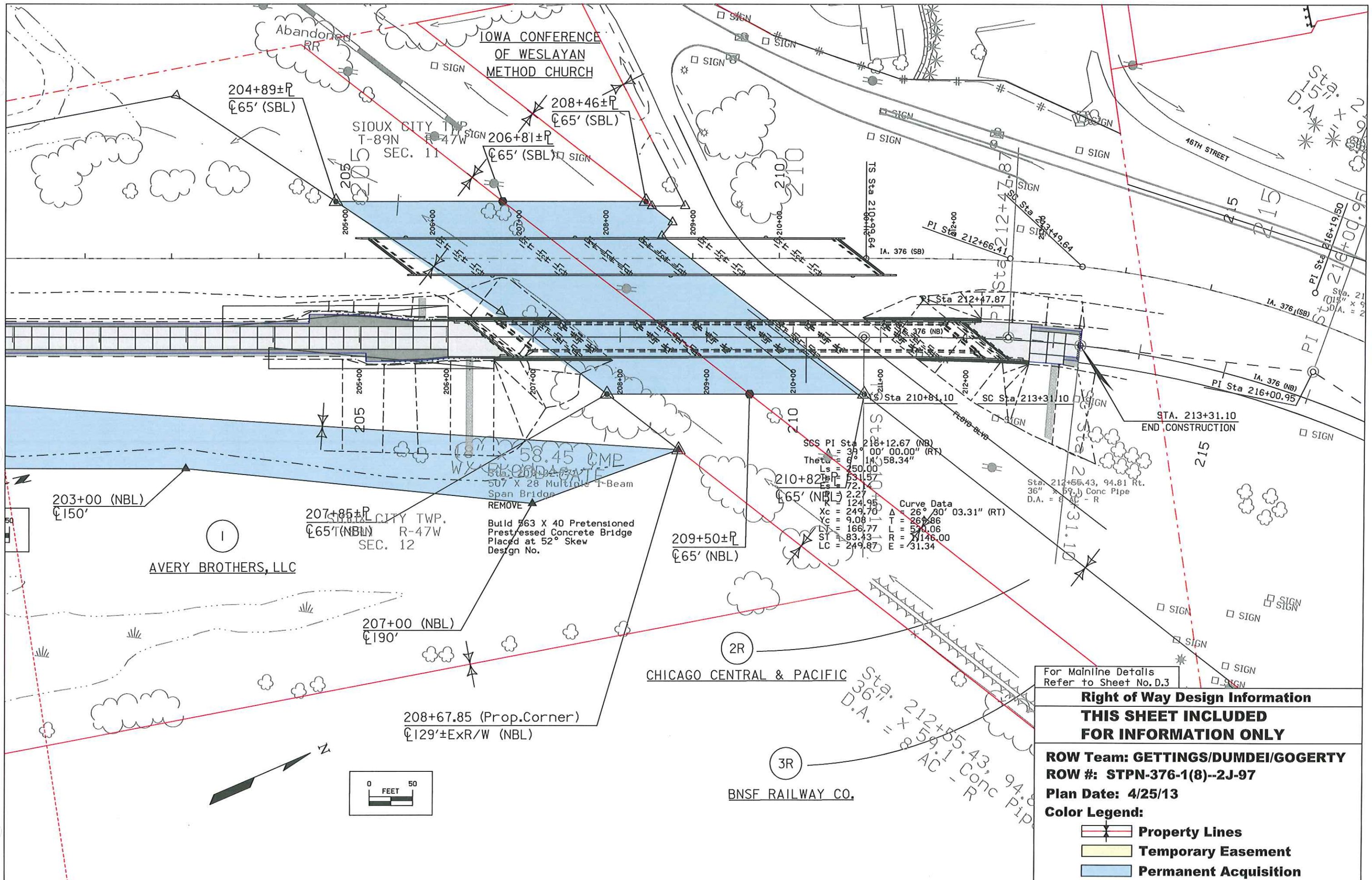
Right of Way Design Information

THIS SHEET INCLUDED FOR INFORMATION ONLY

ROW Team: GETTINGS/DUMDEI/GOGERTY
ROW #: STPN-376-1(8)--2J-97
Plan Date: 4/25/13

Color Legend:

- Property Lines
- Temporary Easement
- Permanent Acquisition



SCS PI Sta 216+12.67 (NB)
 $\Delta = 39^{\circ} 00' 00.00''$ (RT)
 Theta = $6^{\circ} 14' 58.34''$
 Ls = 250.00
 Es = 531.57
 E = 72.10
 K = 124.95
 Xc = 249.70
 Yc = 9.08
 LI = 166.77
 ST = 83.43
 LC = 249.87
 E = 31.34
 Curve Data
 $\Delta = 26^{\circ} 30' 03.31''$ (RT)
 T = 267.86
 L = 530.06
 R = 7146.00
 Sta. 212+55.43, 94.81 Rt.
 36" x 59" Conc Pipe
 D.A. = 8 AC - R

For Mainline Details Refer to Sheet No. D.3

Right of Way Design Information

THIS SHEET INCLUDED FOR INFORMATION ONLY

ROW Team: GETTINGS/DUMDEI/GOGERTY
ROW #: STPN-376-1(8)--2J-97
Plan Date: 4/25/13
Color Legend:

- Property Lines
- Temporary Easement
- Permanent Acquisition

STAGING NOTES

108-26A
08-01-08

TRAFFIC CONTROL PLAN

108-23A
08-01-08

Traffic on IA 376 will be maintained as two-lane, two-way traffic on the SB lanes via NB lane median crossovers. The existing exit loop from NB IA 376 to Floyd Blvd/Taft St will be closed for the duration of the project. Access to Floyd Blvd/Taft St will be detoured via Outer Drive.

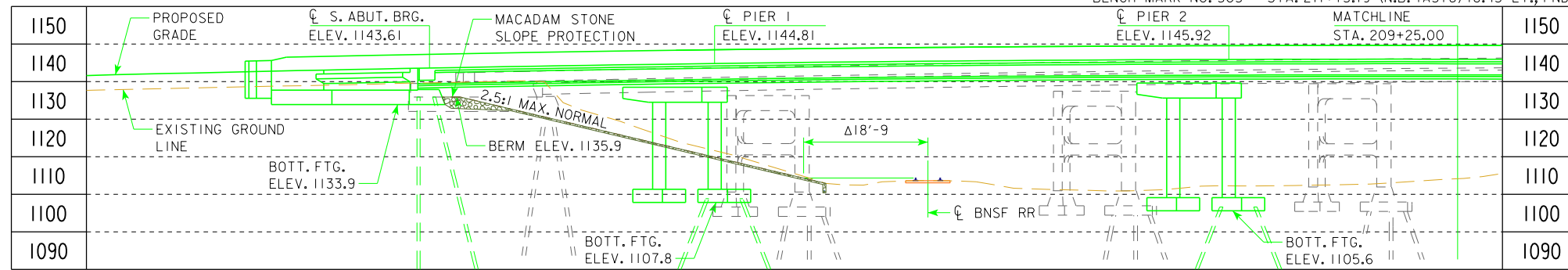
Traffic control shall be in accordance with Standard Road Plan TC-61.

TABULATION OF SPECIAL EVENTS

102-15
08-01-08

Event	Location	Date

NOTES TO FINAL DESIGNER:
 1. TL-5 BRIDGE RAILING PROPOSED.
 2. CONSTANT 2% CROSS SLOPE FOR FULL BRIDGE LENGTH.
 3. PIER TYPE - FRAME.
 4. BEAM TYPE - BTD.
 5. TOP OF DECK IS COINCIDENT TO PROFILE GRADE AT \bar{C} RDWY.



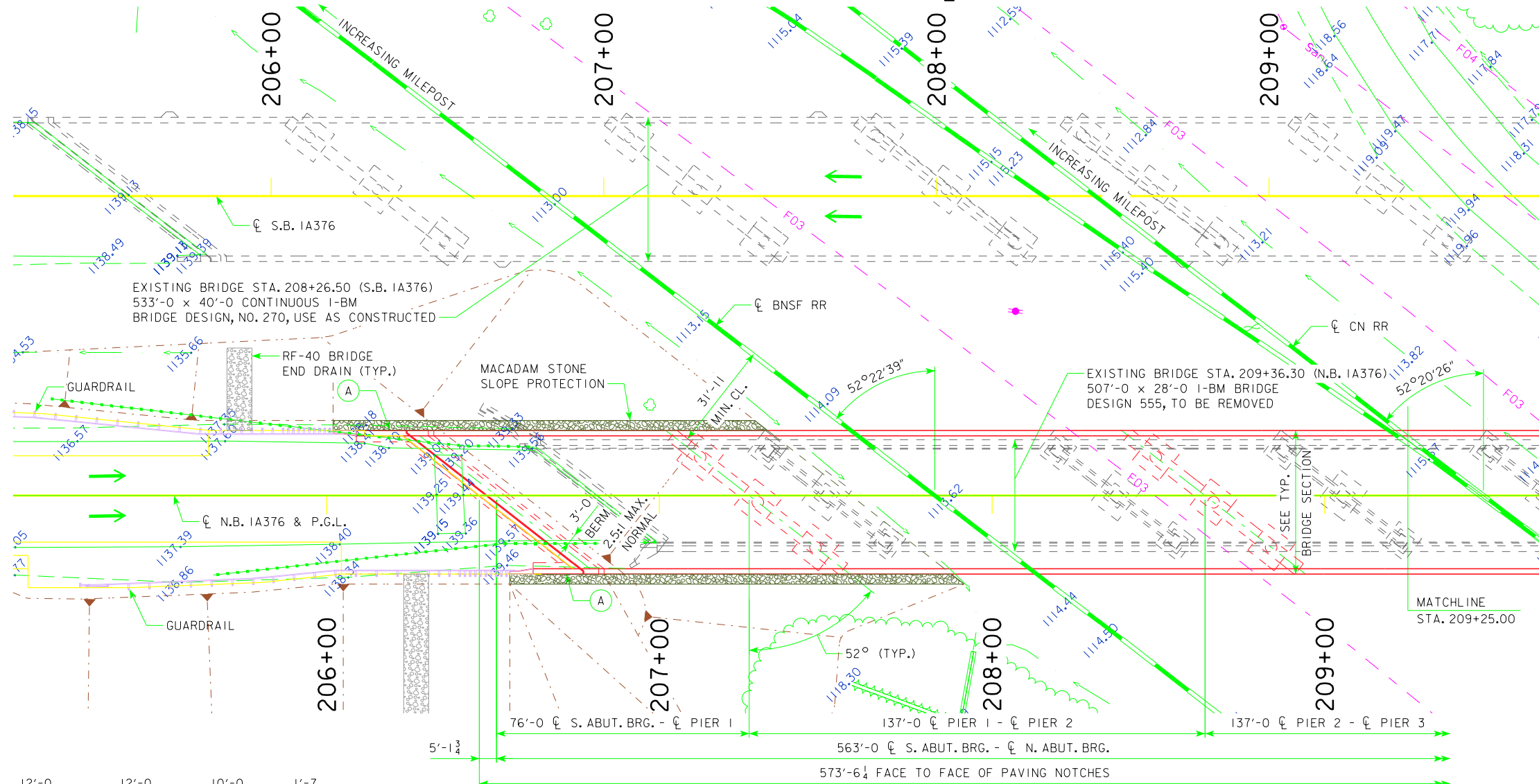
PROPOSED PROFILE GRADE ON N.B. IA376

TRAFFIC ESTIMATE

2015 AADT	8400	V.P.D.
2035 AADT	13,100	V.P.D.
2035 DHV	-	V.P.H.
TRUCKS	11	%

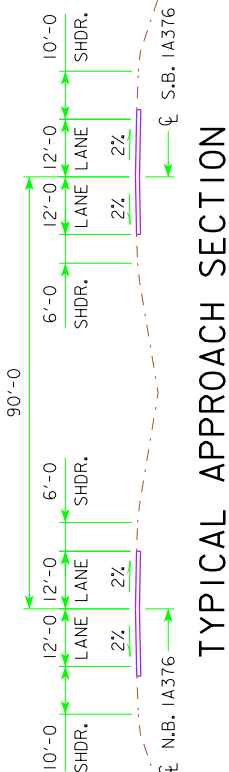
LONGITUDINAL SECTION ALONG \bar{C} N.B. IA376

Δ MEASURED PERPENDICULAR TO \bar{C} BNSF RR. PROPOSED DIMENSION IS GREATER THAN EXISTING DIMENSION.

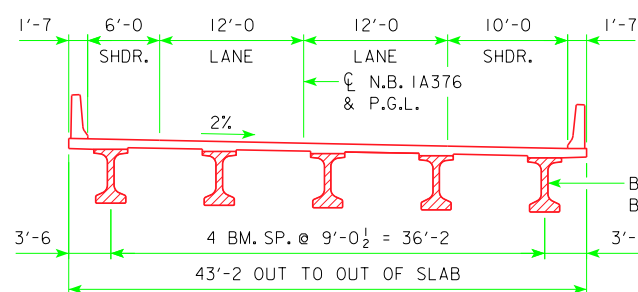


UTILITIES LEGEND:

San. - SANITARY SEWER CITY OF SIOUX CITY
 F03 - FIBER OPTIC MCI/VERISON



TYPICAL APPROACH SECTION



TYPICAL BRIDGE CROSS SECTION

(A) 15'-0 WING EXTENSION FOR S. ABUT.

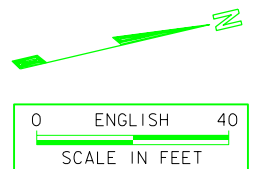
SITUATION PLAN

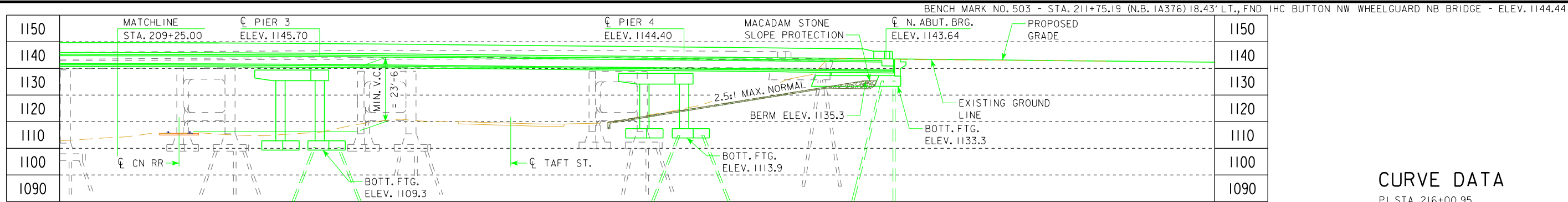
LOCATION

N.B. IA376 OVER
 BNSF RR, CN RR & TAFT ST.
 FRA CROSSING NO. 070183L & 307686R
 IOWA RR CROSSING NO. 970
 T-89N R-47W
 SECTION 12
 SIOUX CITY TOWNSHIP
 WOODBURY COUNTY
 BRIDGE MAINT. NO. 9799.0R376
 LATITUDE 42.546309°
 LONGITUDE -96.350328°

PRELIMINARY
 DESIGN FOR 52° SKEW (R.A.)
**563'-0 X 40'-0 PRETENSIONED
 PRESTRESSED CONCRETE BEAM BRIDGE**

76'-0 END SPANS (3) 137'-0 INTERIOR SPANS
SITUATION PLAN
 STATION: 209+32.50
WOODBURY COUNTY
 IOWA DEPARTMENT OF TRANSPORTATION - HIGHWAY DIVISION
 DESIGN SHEET NO. OF FILE NO. 30859 DESIGN NO. 215





LONGITUDINAL SECTION ALONG \bar{C} N.B. IA376

CURVE DATA

PI STA.	216+00.95
Δ	26°30'03.31" (RT)
D	4°59'58.67"
T	269.86
L	530.06
E	31.34
R	1146.00
e	6.00%
I	250.00
x	77.00

SPIRAL DATA

SCS PI STA.	216+12.67
Δ	39°00'00.00" (RT)
θ_s	6°14'58.34"
Ls	250.00
Ts	531.57
Es	72.14
P	2.27
K	124.95
Xc	249.70
Yc	9.08
LT	166.77
ST	83.43
LC	249.87
TS STA.	210+81.10
SC STA.	213+31.10

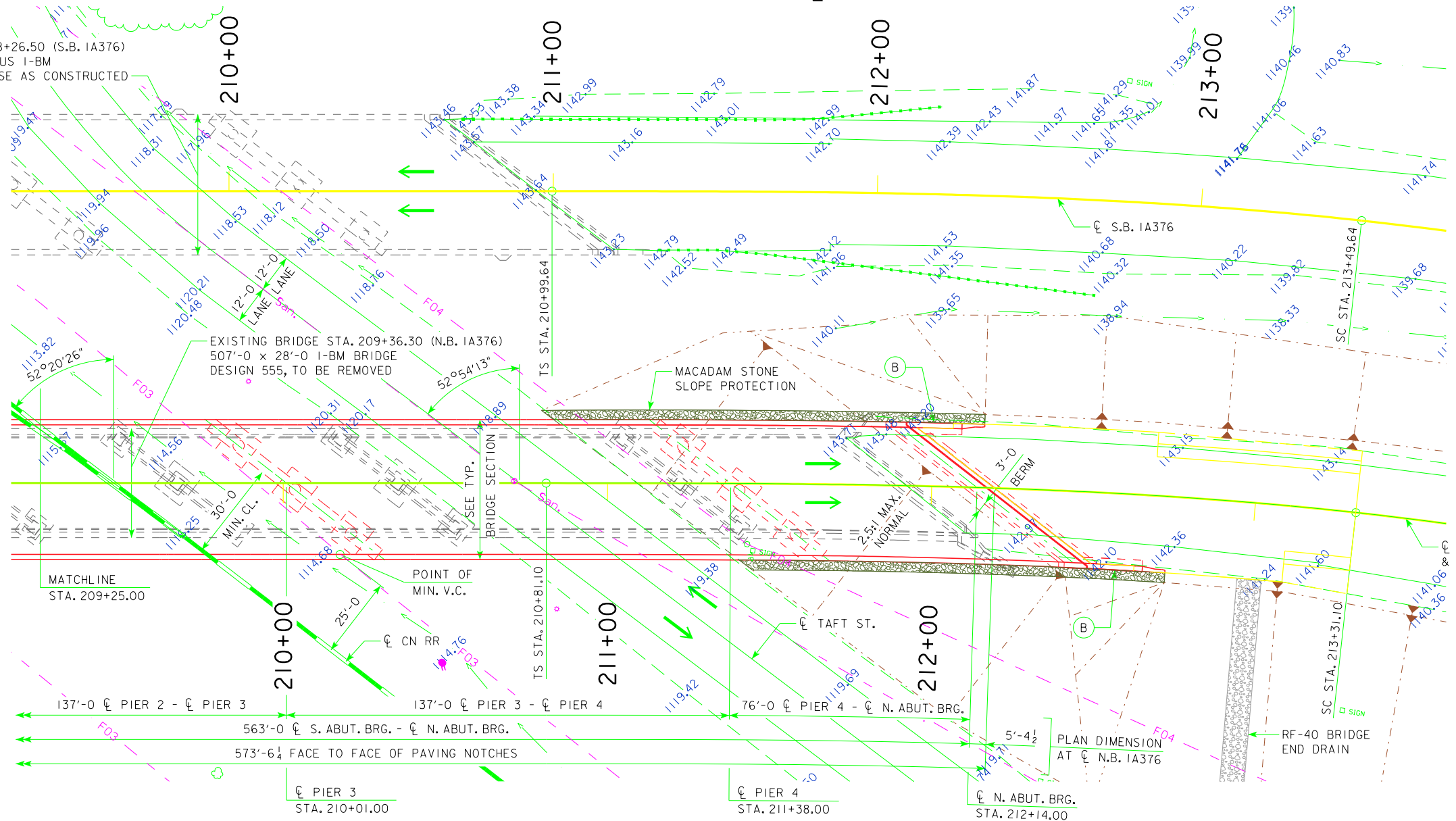
EXISTING BRIDGE STA. 208+26.50 (S.B. IA376)
533'-0" x 40'-0" CONTINUOUS I-BM
BRIDGE, DESIGN NO. 270, USE AS CONSTRUCTED

210+00

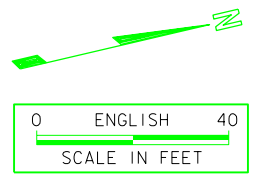
211+00

212+00

213+00



UTILITIES LEGEND:
San. - SANITARY SEWER CITY OF SIOUX CITY
F03 - FIBER OPTIC MCI/VERISON



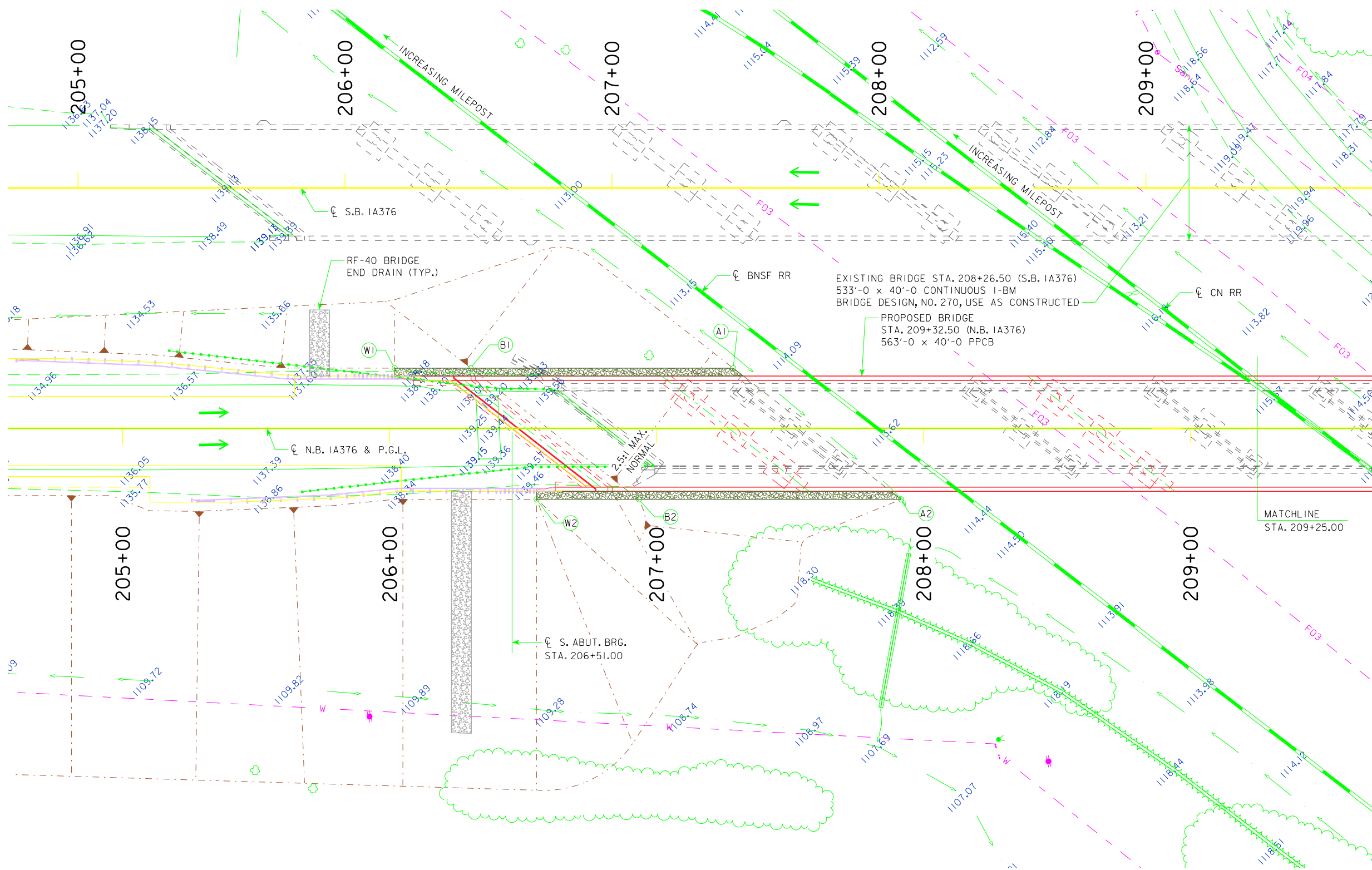
MINIMUM VERTICAL CLEARANCE

OVERHEAD STATION = 210+17.50, OFFSET 22.00' RT.
OVERHEAD ELEVATION = 1145.14
DEPTH OF SUPERSTRUCTURE = 5.38'
UNDERPASS STATION = NA, OFFSET 25.00' LT.
UNDERPASS ELEVATION = 1116.32'
MINIMUM VERTICAL CLEARANCE = 23.44'

(B) 17'-0" WING EXTENSION FOR N. ABUT.

SITUATION PLAN

PRELIMINARY
DESIGN FOR 52° SKEW (R.A.)
**563'-0" X 40'-0" PRETENSIONED
PRESTRESSED CONCRETE BEAM BRIDGE**
76'-0" END SPANS (3) 137'-0" INTERIOR SPANS
SITUATION PLAN
STATION: 209+32.50 JAN. 2013
WOODBURY COUNTY
IOWA DEPARTMENT OF TRANSPORTATION - HIGHWAY DIVISION
DESIGN SHEET NO. ___ OF ___ FILE NO. 30859 DESIGN NO. 215



SITE PLAN

BERM SLOPE LOCATION TABLE						
	SOUTH ABUTMENT			NORTH ABUTMENT		
	STATION	OFFSET	ELEV	STATION	OFFSET	ELEV
A1	207+28.65	22.58' LT	1113.07	210+79.65	22.58' LT	1119.35
A2	207+91.58	26.58' RT	1111.82	211+44.94	26.58' RT	1120.12
B1	206+30.35	22.58' LT	1135.90	211+75.37	22.58' LT	1135.30
B2	206+93.28	26.58' RT	1135.90	212+42.35	26.58' RT	1135.30
W1	206+01.89	22.58' LT	1142.54	212+15.77	22.58' LT	1143.54
W2	206+55.01	26.58' RT	1143.10	212+73.51	26.58' RT	1141.97

W - END WING / EDGE BRIDGE WING ARMORING
BERM SLOPE ELEVATIONS REFLECT GRADING SURFACE

PRELIMINARY

DESIGN FOR 52° SKEW (R.A.)

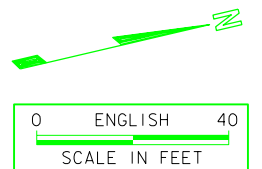
**563'-0" X 40'-0" PRETENSIONED
PRESTRESSED CONCRETE BEAM BRIDGE**

76'-0" END SPANS (3) 137'-0" INTERIOR SPANS

SITUATION PLAN - SITE
STATION: 209+32.50 JAN. 2013

WOODBURY COUNTY

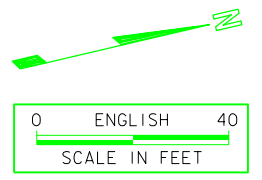
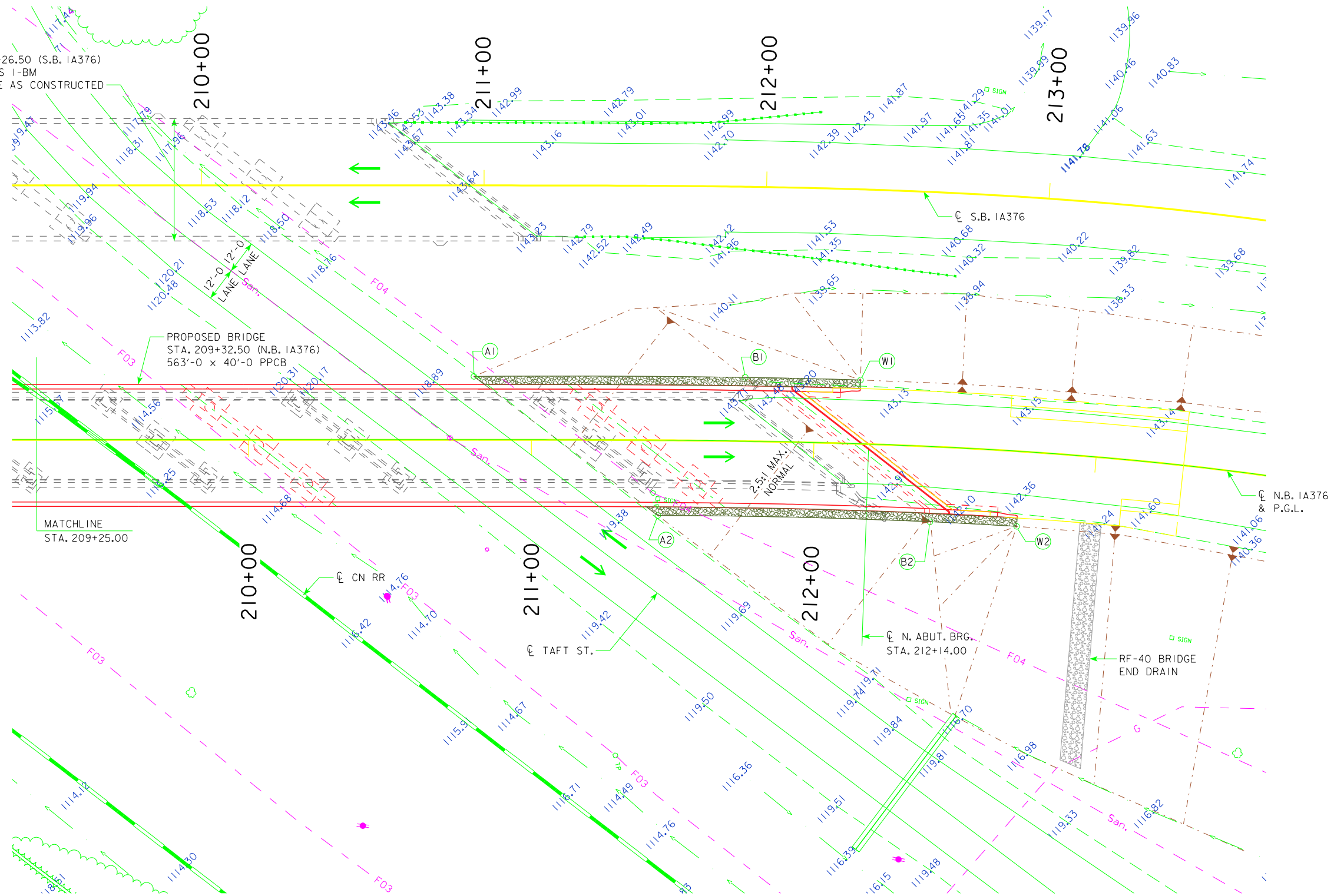
IOWA DEPARTMENT OF TRANSPORTATION - HIGHWAY DIVISION
DESIGN SHEET NO. ___ OF ___ FILE NO. 30859 DESIGN NO. 215



EXISTING BRIDGE STA. 208+26.50 (S.B. 1A376)
533'-0" x 40'-0" CONTINUOUS I-BM
BRIDGE, DESIGN NO. 270, USE AS CONSTRUCTED

PROPOSED BRIDGE
STA. 209+32.50 (N.B. 1A376)
563'-0" x 40'-0" PPCB

MATCHLINE
STA. 209+25.00



SITE PLAN

BERM SLOPE LOCATION TABLE						
	SOUTH ABUTMENT			NORTH ABUTMENT		
	STATION	OFFSET	ELEV	STATION	OFFSET	ELEV
A1	207+28.65	22.58' LT	1113.07	210+79.65	22.58' LT	1119.35
A2	207+91.58	26.58' RT	1111.82	211+44.94	26.58' RT	1120.12
B1	206+30.35	22.58' LT	1135.90	211+75.37	22.58' LT	1135.30
B2	206+93.28	26.58' RT	1135.90	212+42.35	26.58' RT	1135.30
W1	206+01.89	22.58' LT	1142.54	212+15.77	22.58' LT	1143.54
W2	206+55.01	26.58' RT	1143.10	212+73.51	26.58' RT	1141.97

W - END WING / EDGE BRIDGE WING ARMORING
BERM SLOPE ELEVATIONS REFLECT GRADING SURFACE

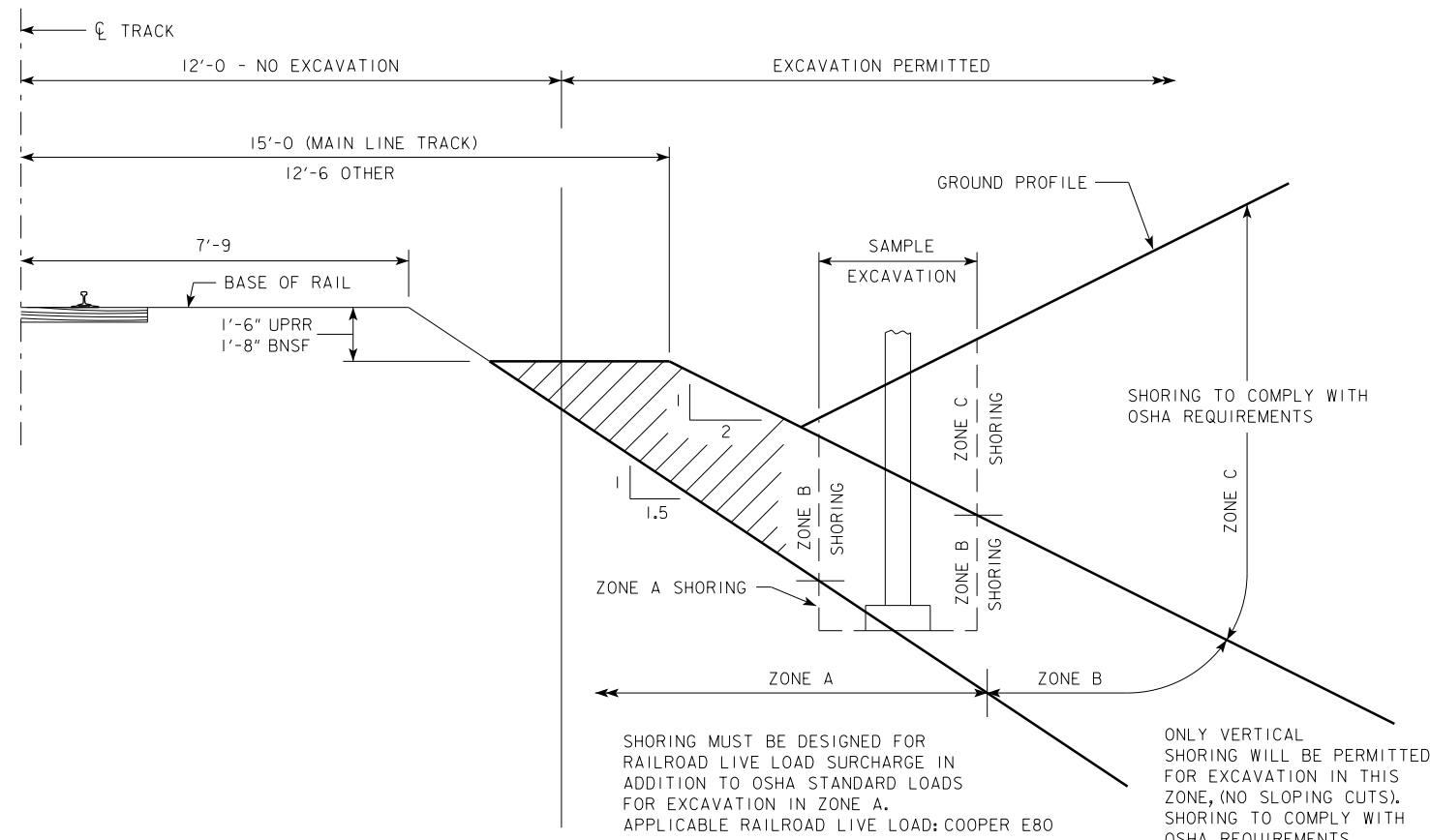
PRELIMINARY
DESIGN FOR 52° SKEW (R.A.)

**563'-0" X 40'-0" PRETENSIONED
PRESTRESSED CONCRETE BEAM BRIDGE**

76'-0" END SPANS (3) 137'-0" INTERIOR SPANS

SITUATION PLAN - SITE
STATION: 209+32.50 JAN. 2013

WOODBURY COUNTY
IOWA DEPARTMENT OF TRANSPORTATION - HIGHWAY DIVISION
DESIGN SHEET NO. ___ OF ___ FILE NO. 30859 DESIGN NO. 215



GENERAL EXCAVATION ZONES

RAILROAD GENERAL NOTES:

- RAILROAD REVIEW AND APPROVAL OF SHORING, ERECTION, DEMOLITION, AND FALSEWORK IS REQUIRED. ALLOW A MINIMUM OF FOUR WEEKS FOR THE REVIEW AND APPROVAL OF EACH SUBMITTAL.
- THE PROPOSED GRADE SEPARATION PROJECT SHALL NOT INCREASE THE QUANTITY AND/OR CHARACTERISTICS OF THE FLOW IN THE RAILROAD'S DITCHES AND/OR DRAINAGE STRUCTURES.
- THE ELEVATION OF THE EXISTING TOP-OF-RAIL PROFILE SHALL BE VERIFIED BEFORE BEGINNING CONSTRUCTION. ALL DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE RAILROAD PRIOR TO CONSTRUCTION.
- THE CONTRACTOR MUST SUBMIT A PROPOSED METHOD OF EROSION AND SEDIMENT CONTROL AND HAVE THE METHOD APPROVED BY THE RAILROAD.
- ALL SHORING SYSTEMS THAT IMPACT THE RAILROAD'S OPERATIONS AND/OR SUPPORTS THE RAILROAD'S EMBANKMENT SHALL BE DESIGNED AND CONSTRUCTED PER CURRENT RAILROAD GUIDELINES FOR TEMPORARY SHORING.
- ALL DEMOLITIONS WITHIN THE RAILROAD'S RIGHT-OF-WAY AND/OR DEMOLITION THAT MAY IMPACT THE RAILROAD'S TRACKS OR OPERATIONS SHALL BE IN COMPLIANCE WITH THE RAILROAD'S DEMOLITION GUIDELINES.
- ERECTION OVER THE RAILROAD'S RIGHT-OF-WAY SHALL BE DESIGNED TO CAUSE NO INTERRUPTION TO THE RAILROAD'S OPERATION, ENABLING THE TRACK(S) TO REMAIN OPEN TO TRAFFIC PER THE RAILROAD'S REQUIREMENTS.
- ALL CONSTRUCTION PHASING THAT MAY IMPACT THE RAILROAD OPERATIONS SHALL BE DESIGNED TO CAUSE NO INTERRUPTION TO THE RAILROAD'S OPERATION, ENABLING THE TRACK(S) TO REMAIN OPEN TO TRAFFIC PER THE RAILROAD'S REQUIREMENTS.
- FALSE-WORK CLEARANCES SHALL COMPLY WITH MINIMUM CONSTRUCTION CLEARANCES.
- ALL PERMANENT CLEARANCES SHALL BE VERIFIED BEFORE PROJECT CLOSING.
- FOR RAILROAD COORDINATION PLEASE REFER TO THE RAILROAD COORDINATION REQUIREMENTS AS PART OF SPECIAL PROVISIONS.

GENERAL SHORING NOTES:

- ALL DIMENSIONS ARE MEASURED PERPENDICULAR TO TRACK.
- PRIOR TO COMMENCING ANY WORK, THE CONTRACTOR SHALL SUBMIT FOR APPROVAL BY THE RAILROAD DETAILED PLANS INDICATING THE NATURE AND EXTENT OF THE TRACK PROTECTION SHORING PROPOSED. THE CONTRACTOR SHALL INSTALL THE TEMPORARY SHORING SYSTEM PER THE APPROVED PLANS. DESIGN OF THE TEMPORARY SHORING SYSTEM TO COMPLY WITH GUIDELINES FOR TEMPORARY SHORING.
- FOR EXCAVATIONS WHICH ENCROACH INTO ZONE A OR B, SHORING PLANS SHALL BE ACCOMPANIED BY DESIGN CALCULATIONS. PLANS AND CALCULATIONS MUST BE SIGNED AND STAMPED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF IOWA.

NOTE:
 BNSF = BURLINGTON NORTHERN SANTA FE RAILROAD
 UPRR = UNION PACIFIC RAILROAD
 CN = CANADIAN NATIONAL RAILROAD

(STATIONS INCREASE WITH MILEPOST INCREASE)

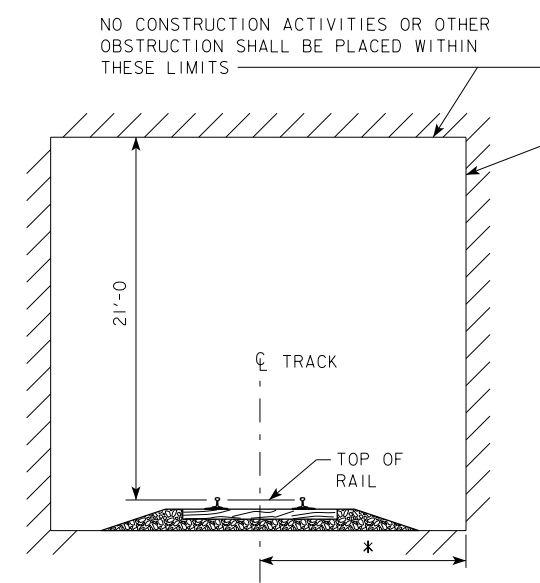
BNSF		CN	
ALIGNMENT: LEFT RAIL	ALIGNMENT: RIGHT RAIL	ALIGNMENT: LEFT RAIL	ALIGNMENT: RIGHT RAIL
STATION	ELEVATION	STATION	ELEVATION
0+00	1118.70	0+00	1118.63
1+00	1118.15	1+00	1118.16
2+00	1117.64	2+00	1117.63
3+00	1116.81	3+00	1116.81
4+00	1116.14	4+00	1116.18
5+00	1115.73	5+00	1115.71
6+00	1115.31	6+00	1115.33
7+00	1115.08	7+00	1115.11
8+00	1114.76	8+00	1114.82
9+00	1114.56	9+00	1114.59
① 10+00	1114.27	① 10+00	1114.30
11+00	1113.75	11+00	1113.82
12+00	1113.43	12+00	1113.54
13+00	1113.46	13+00	1113.46
14+00	1113.06	14+00	1113.12
15+00	1112.74	15+00	1112.74
16+00	1112.29	16+00	1112.36
17+00	1112.00	17+00	1112.00
18+00	1111.99	18+00	1111.99
19+00	1111.77	19+00	1111.80
20+00	1111.55	20+00	1111.54

① EXISTING TRACK STA. 10+00

(STATIONS INCREASE WITH MILEPOST INCREASE)

BNSF		CN	
ALIGNMENT: LEFT RAIL	ALIGNMENT: RIGHT RAIL	ALIGNMENT: LEFT RAIL	ALIGNMENT: RIGHT RAIL
STATION	ELEVATION	STATION	ELEVATION
0+00	1119.92	0+00	1119.82
1+00	1119.38	1+00	1119.34
2+00	1119.07	2+00	1119.09
3+00	1119.00	3+00	1118.93
4+00	1118.39	4+00	1118.37
5+00	1117.92	5+00	1117.93
6+00	1117.39	6+00	1117.39
7+00	1116.94	7+00	1116.94
8+00	1116.66	8+00	1116.69
9+00	1116.37	9+00	1116.39
① 10+00	1116.24	① 10+00	1116.26
11+00	1116.17	11+00	1116.19
12+00	1116.07	12+00	1116.02
13+00	1115.79	13+00	1115.76
14+00	1115.68	14+00	1115.64
15+00	1115.48	15+00	1115.49
16+00	1115.35	16+00	1115.33
17+00	1115.28	17+00	1115.28
18+00	1115.13	18+00	1115.09
19+00	1114.78	19+00	1114.72
20+00	1114.43	20+00	1114.42

① EXISTING TRACK STA. 10+00



MINIMUM CONSTRUCTION CLEARANCE ENVELOPE

(NORMAL TO RAILROAD)

* 15'-0" for BNSF and 12'-0" for UPRR

PRELIMINARY

DESIGN FOR 52° SKEW (R.A.)

**563'-0 X 40'-0 PRETENSIONED
 PRESTRESSED CONCRETE BEAM BRIDGE**

76'-0 END SPANS (3) 137'-0 INTERIOR SPANS

RAILROAD DETAILS

STATION: 209+32.50 JAN. 2013

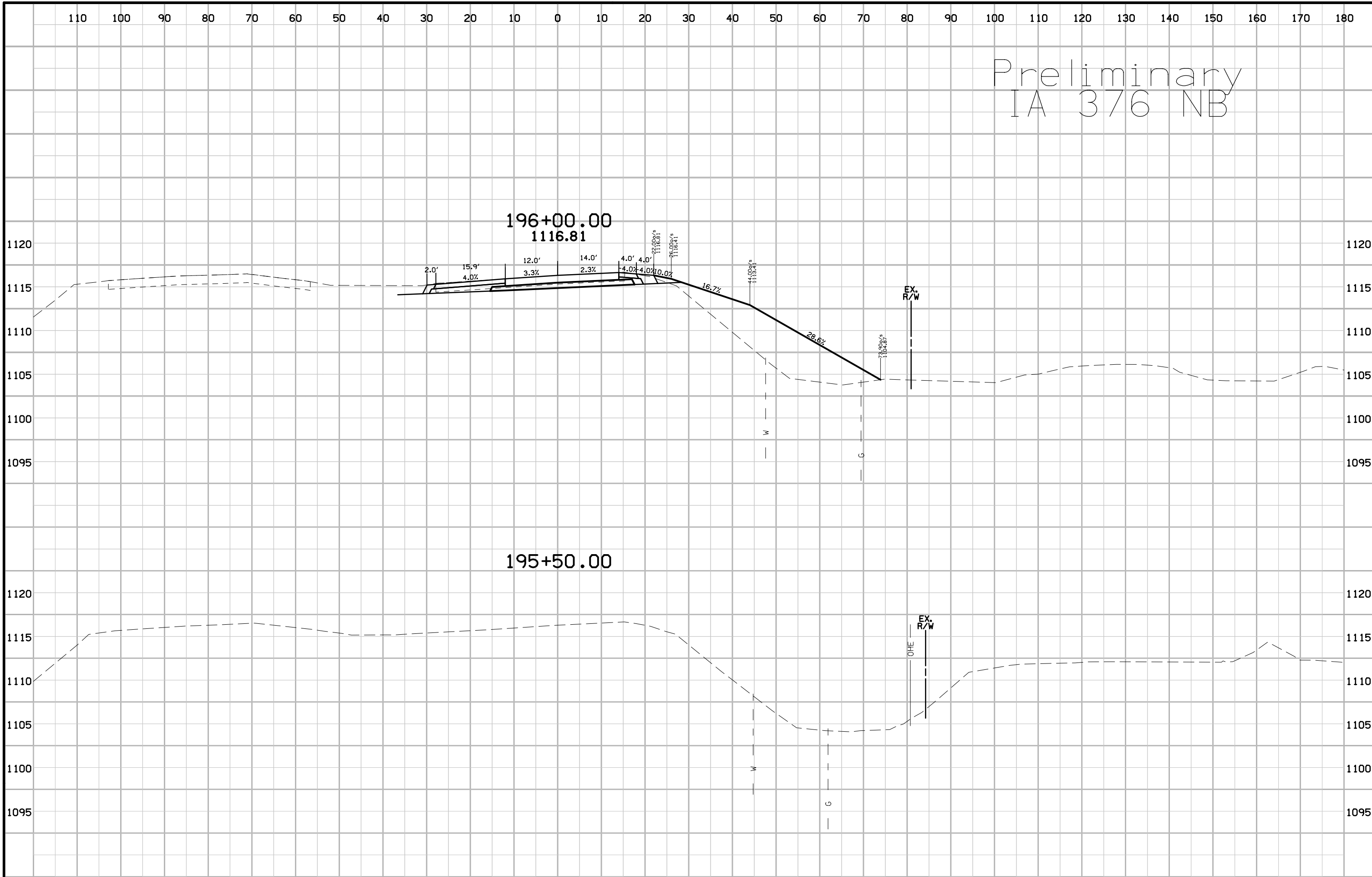
WOODBURY COUNTY

IOWA DEPARTMENT OF TRANSPORTATION - HIGHWAY DIVISION

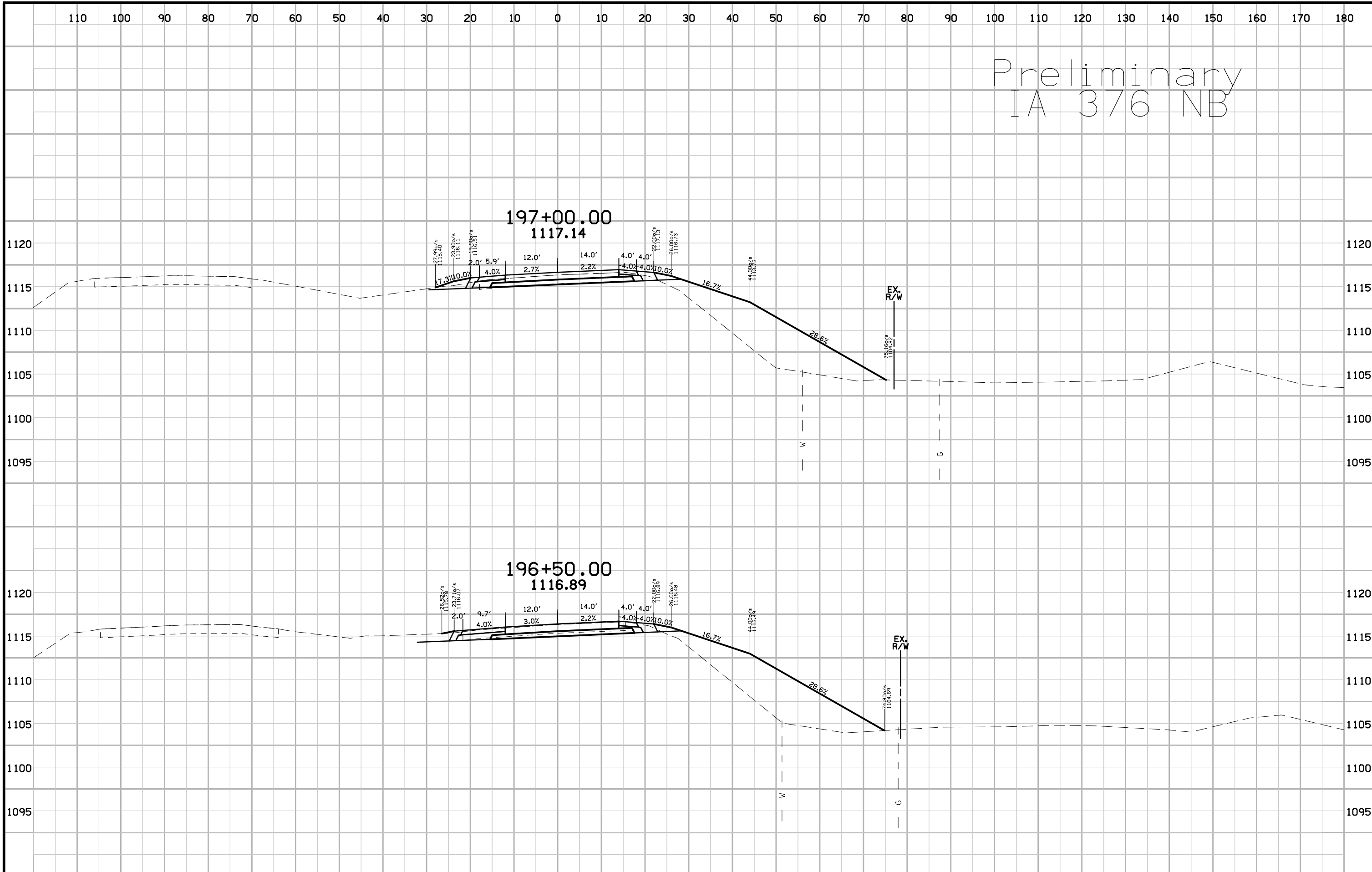
DESIGN SHEET NO. ___ OF ___ FILE NO. 30859 DESIGN NO. 215

ENGLISHMISCELLANEOUSBRIDGES.DGN - 1067 - THIS SHEET ISSUED 12-08.

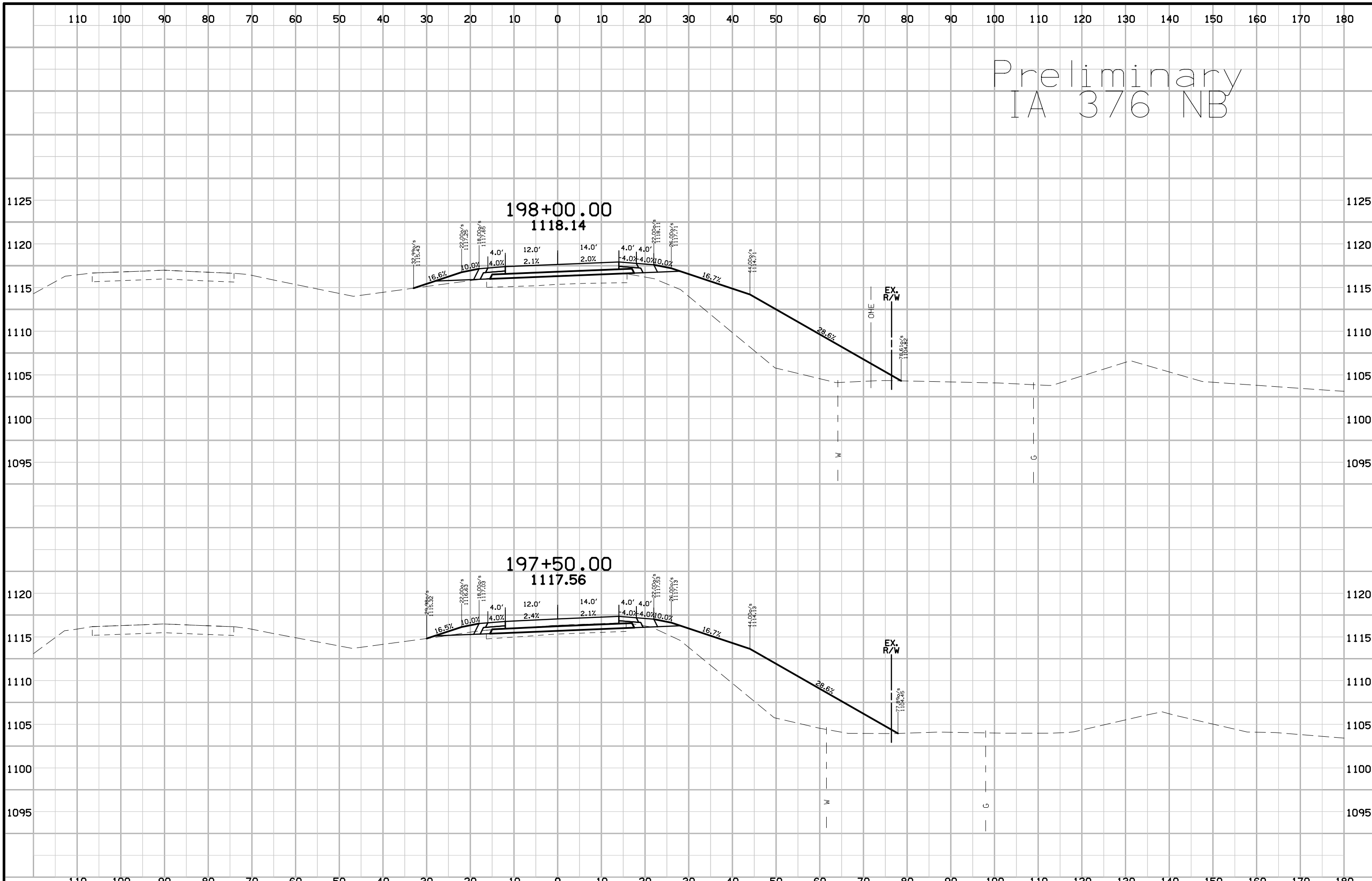
Preliminary
IA 376 NB



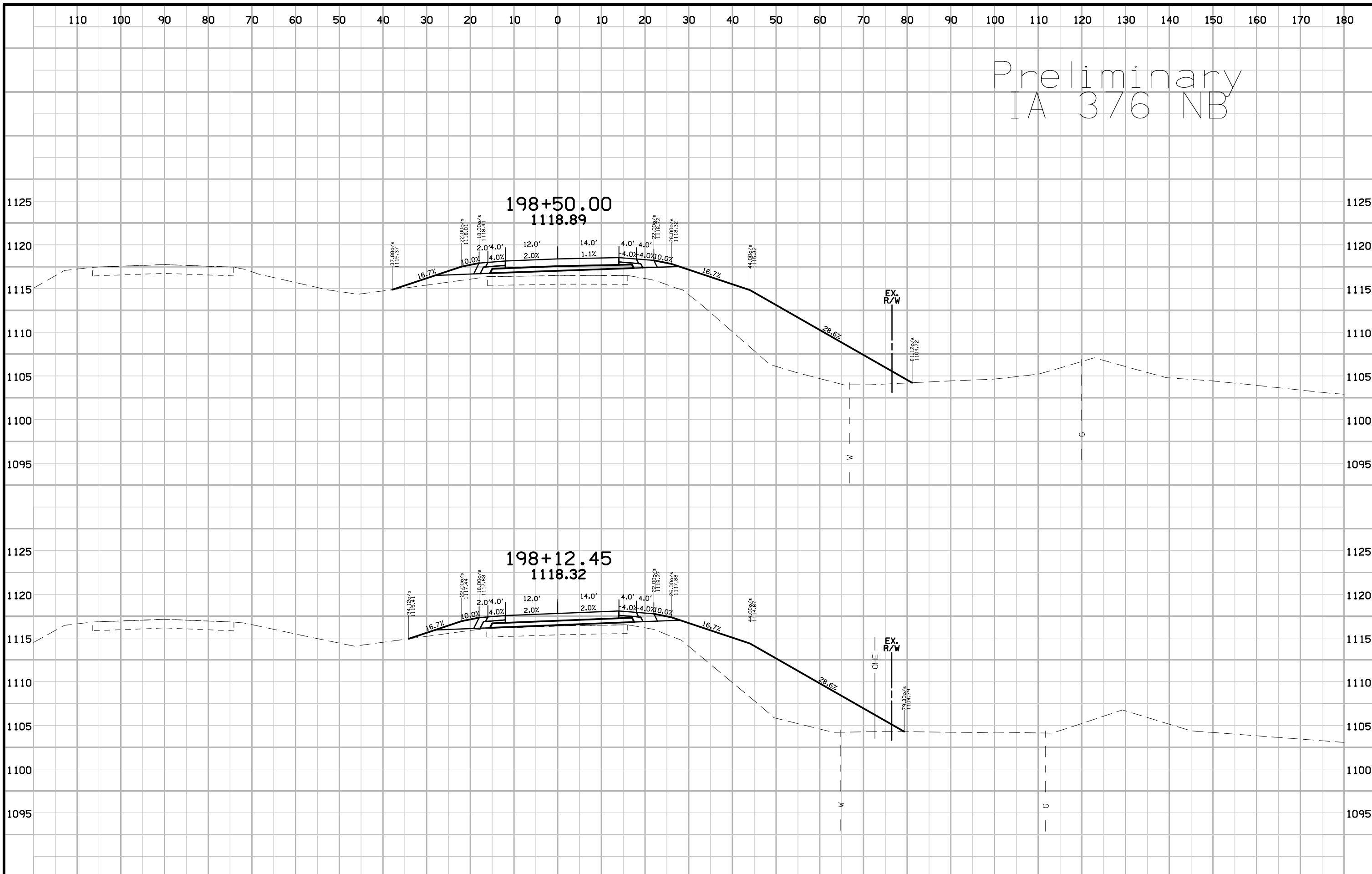
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IA 376 NB



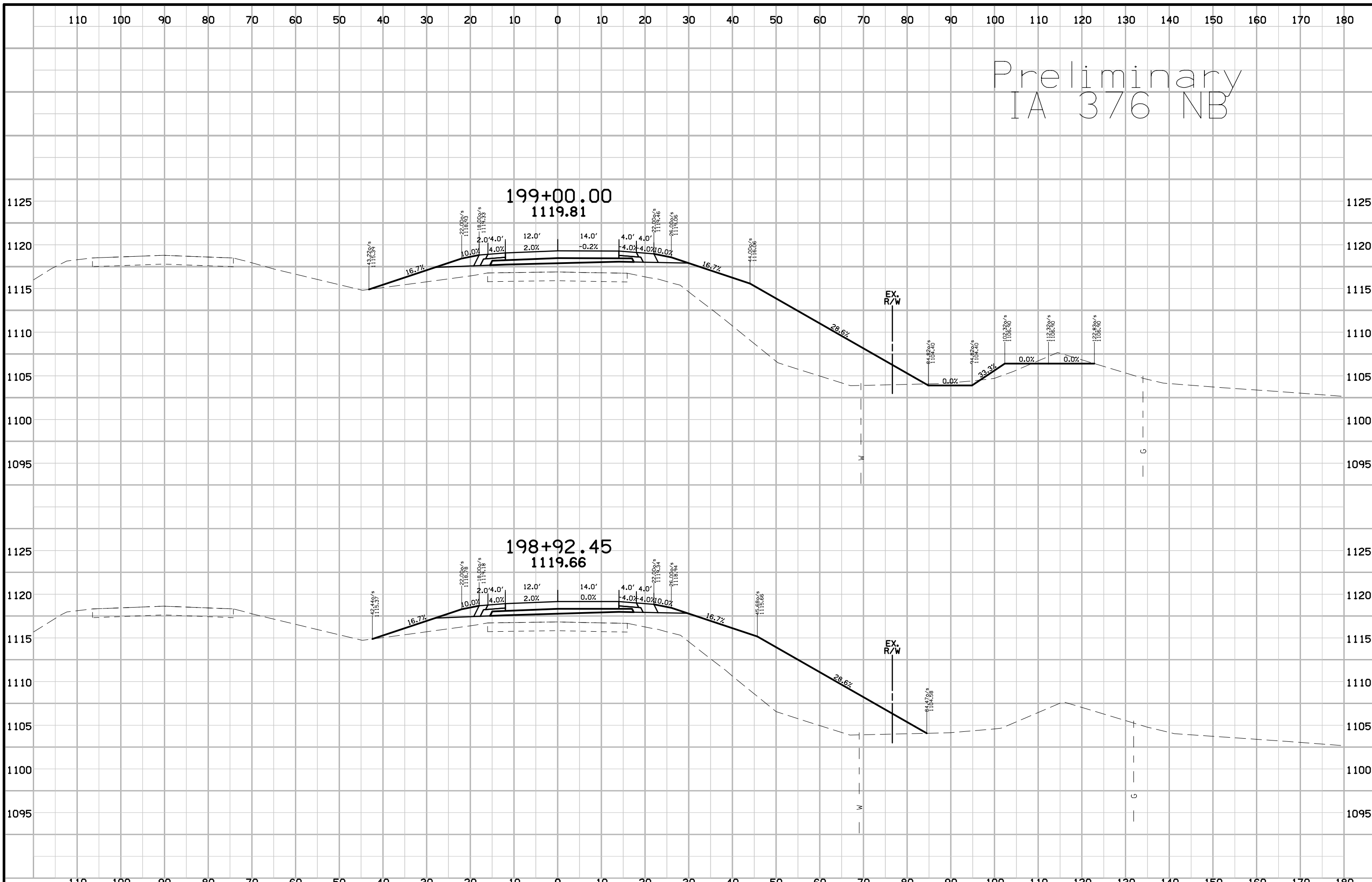
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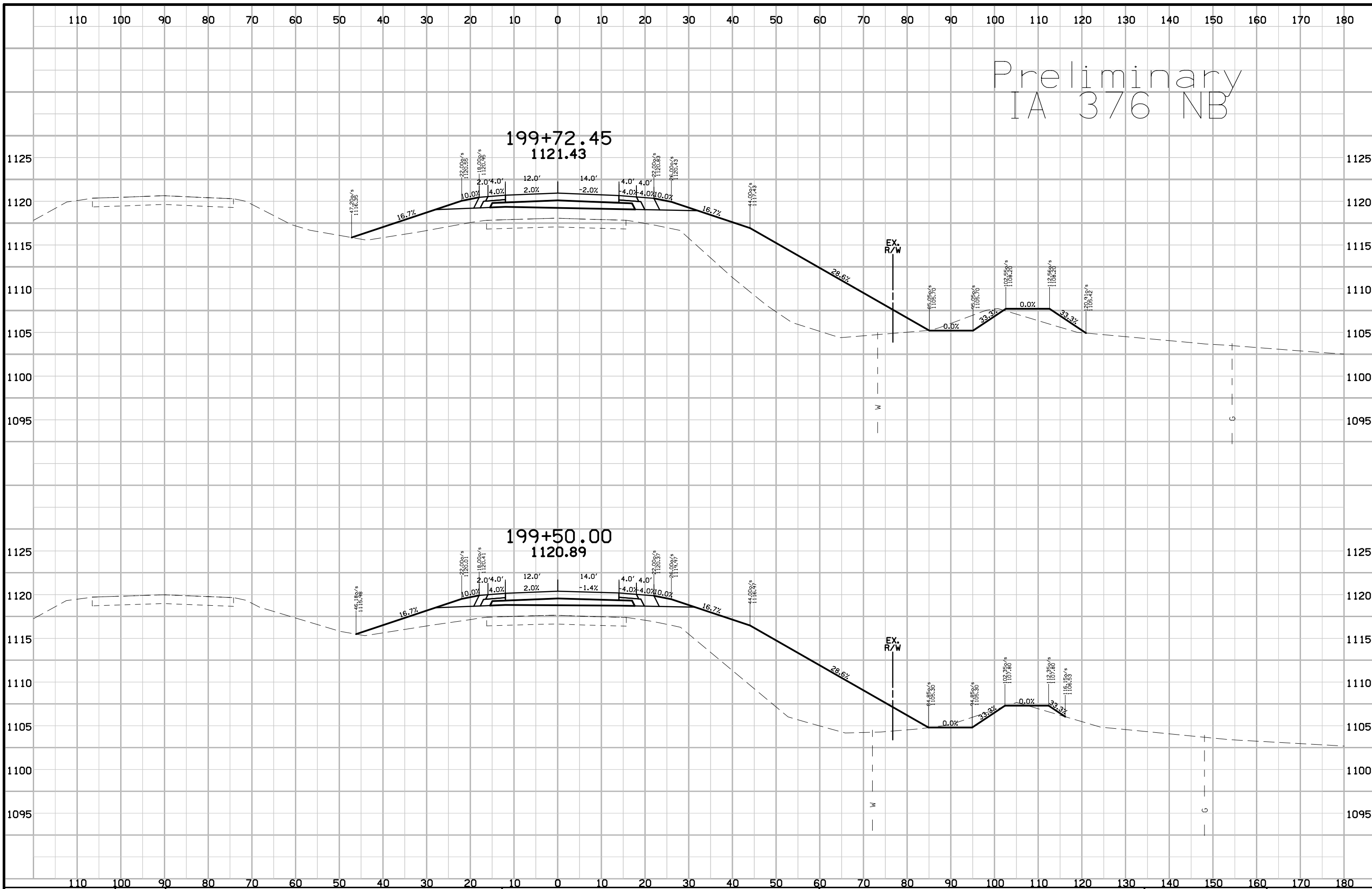
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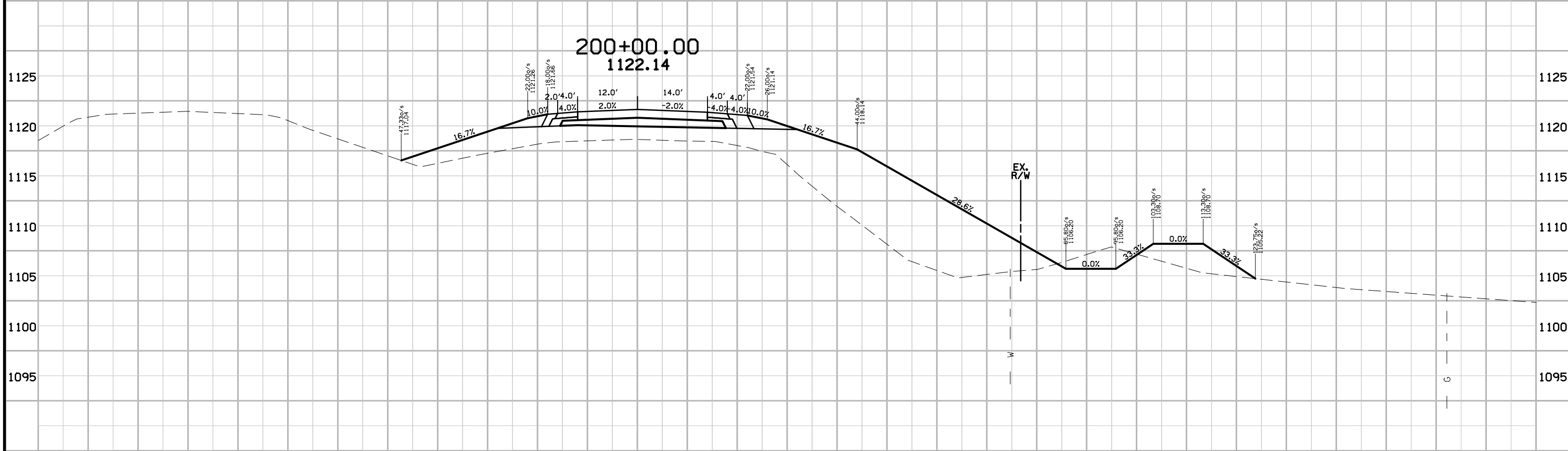
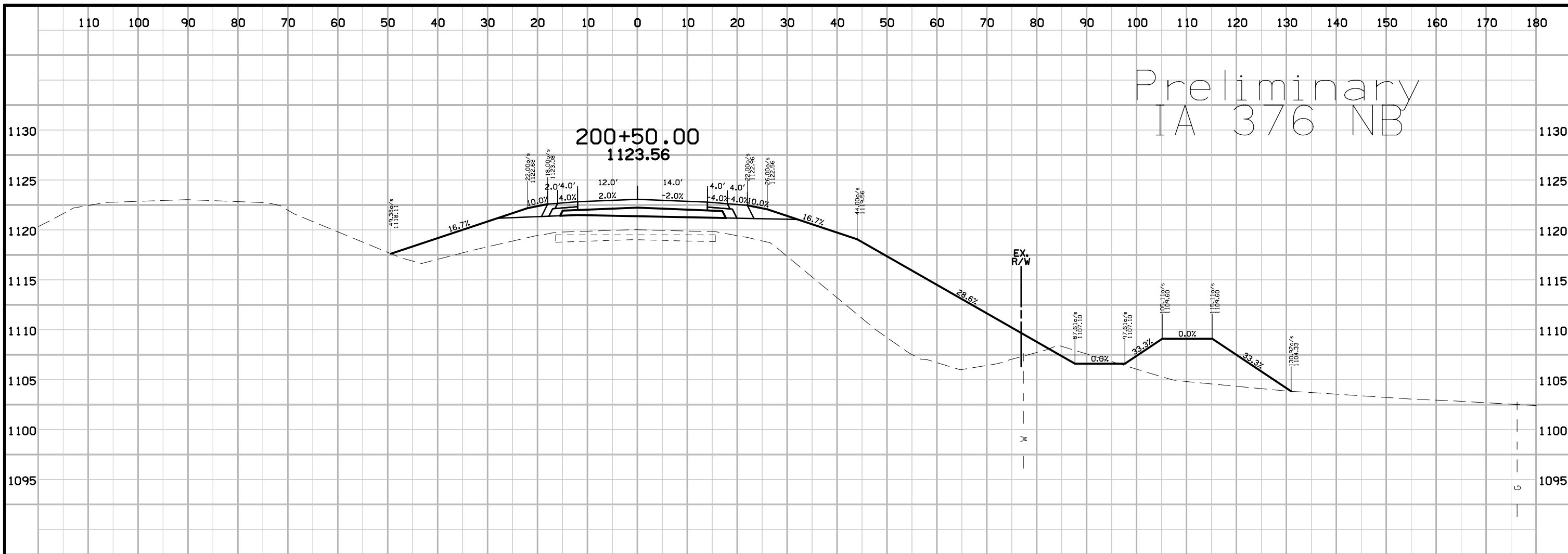
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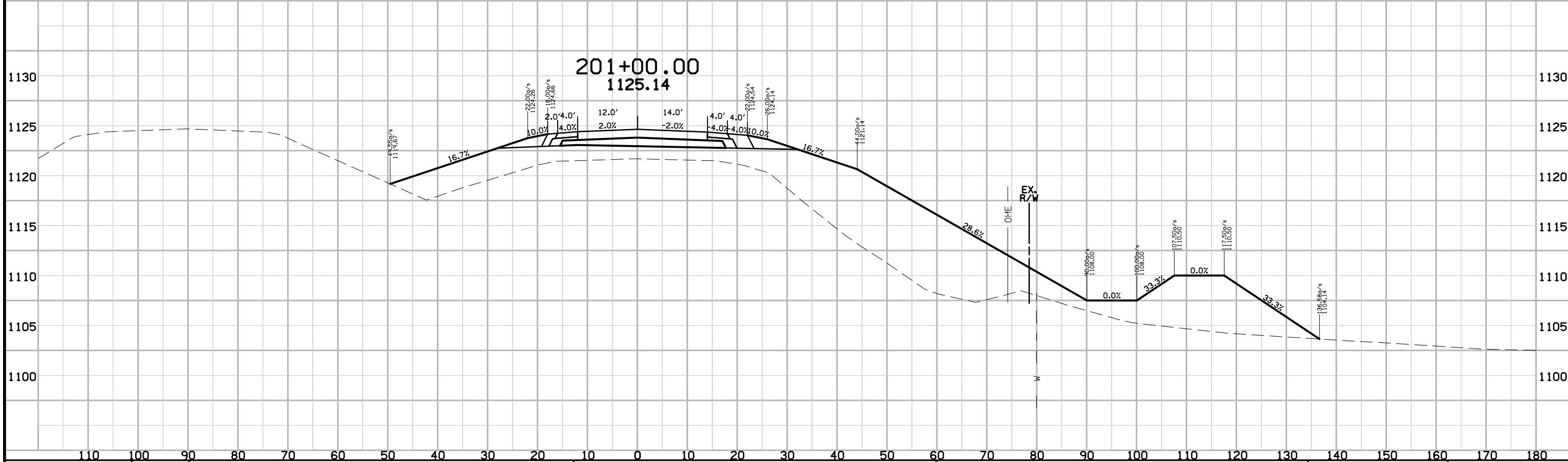
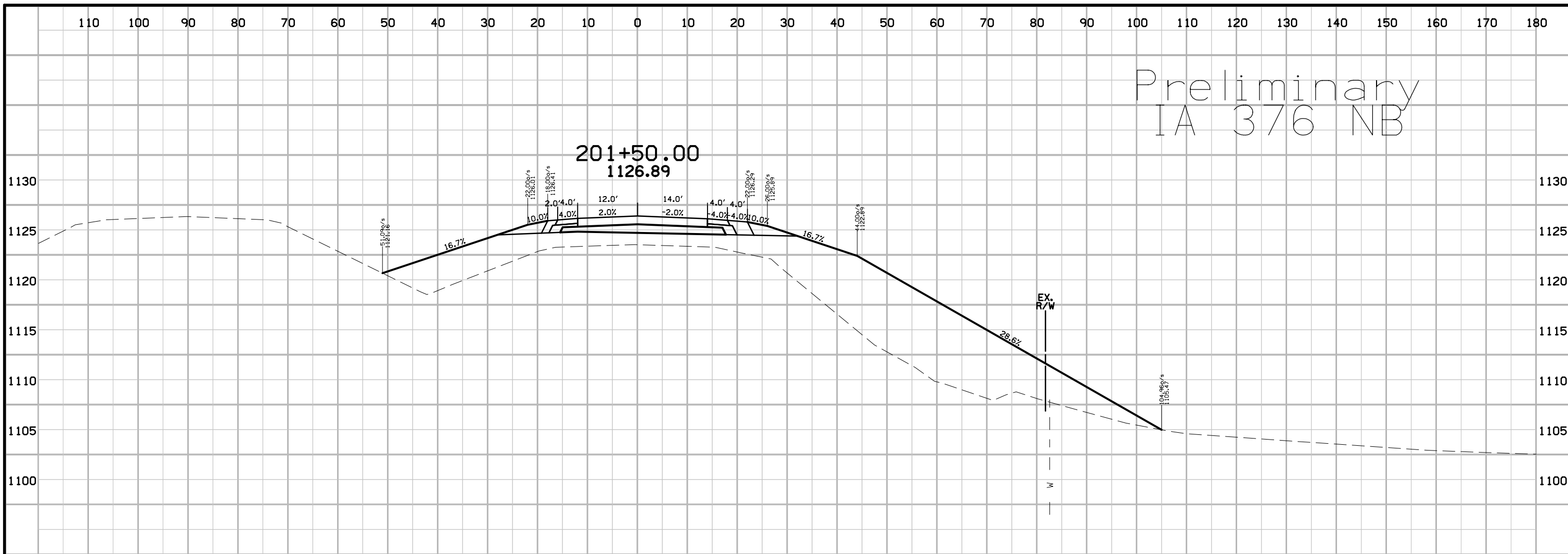
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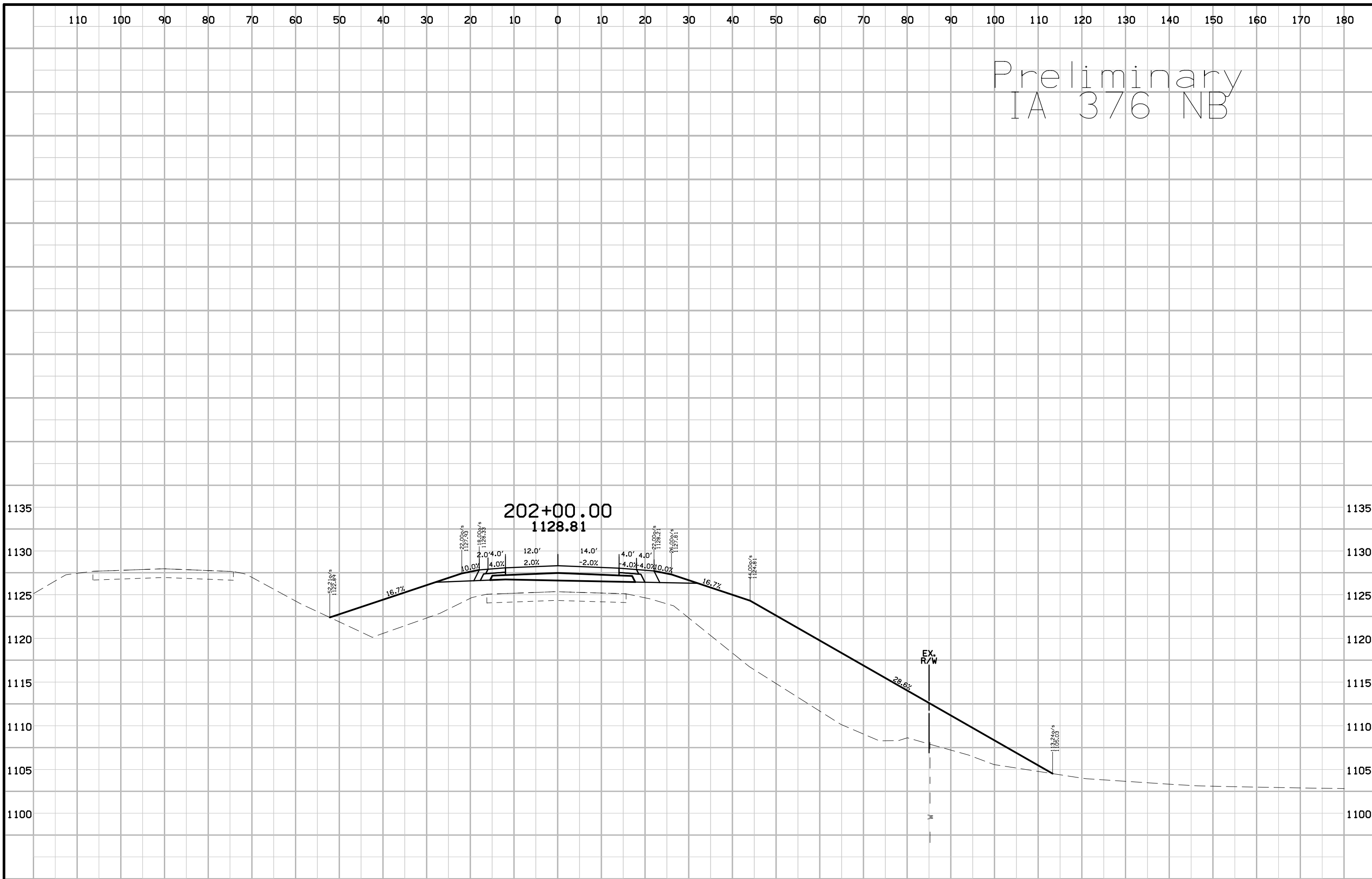
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IA 376 NB



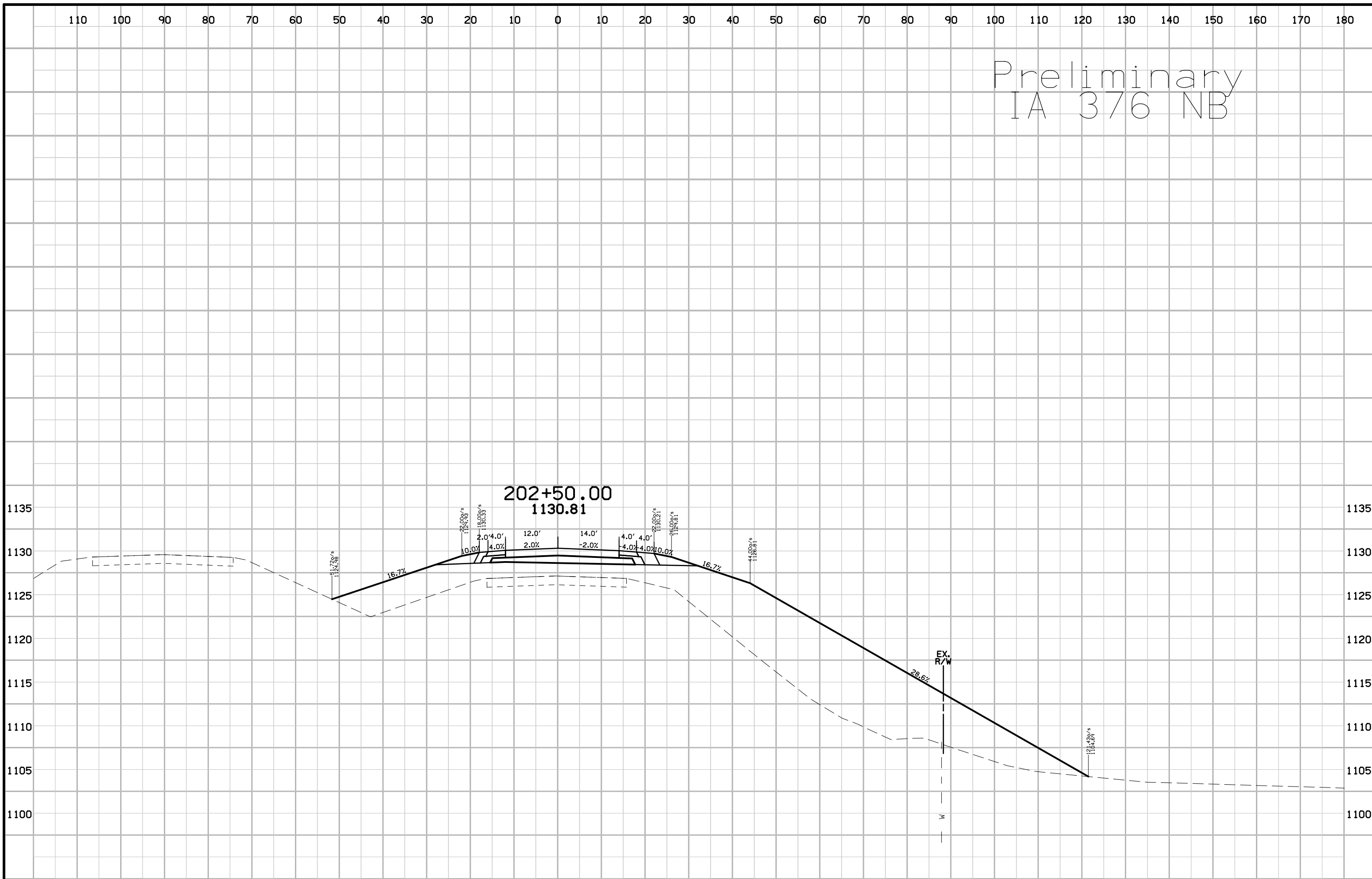
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IA 376 NB



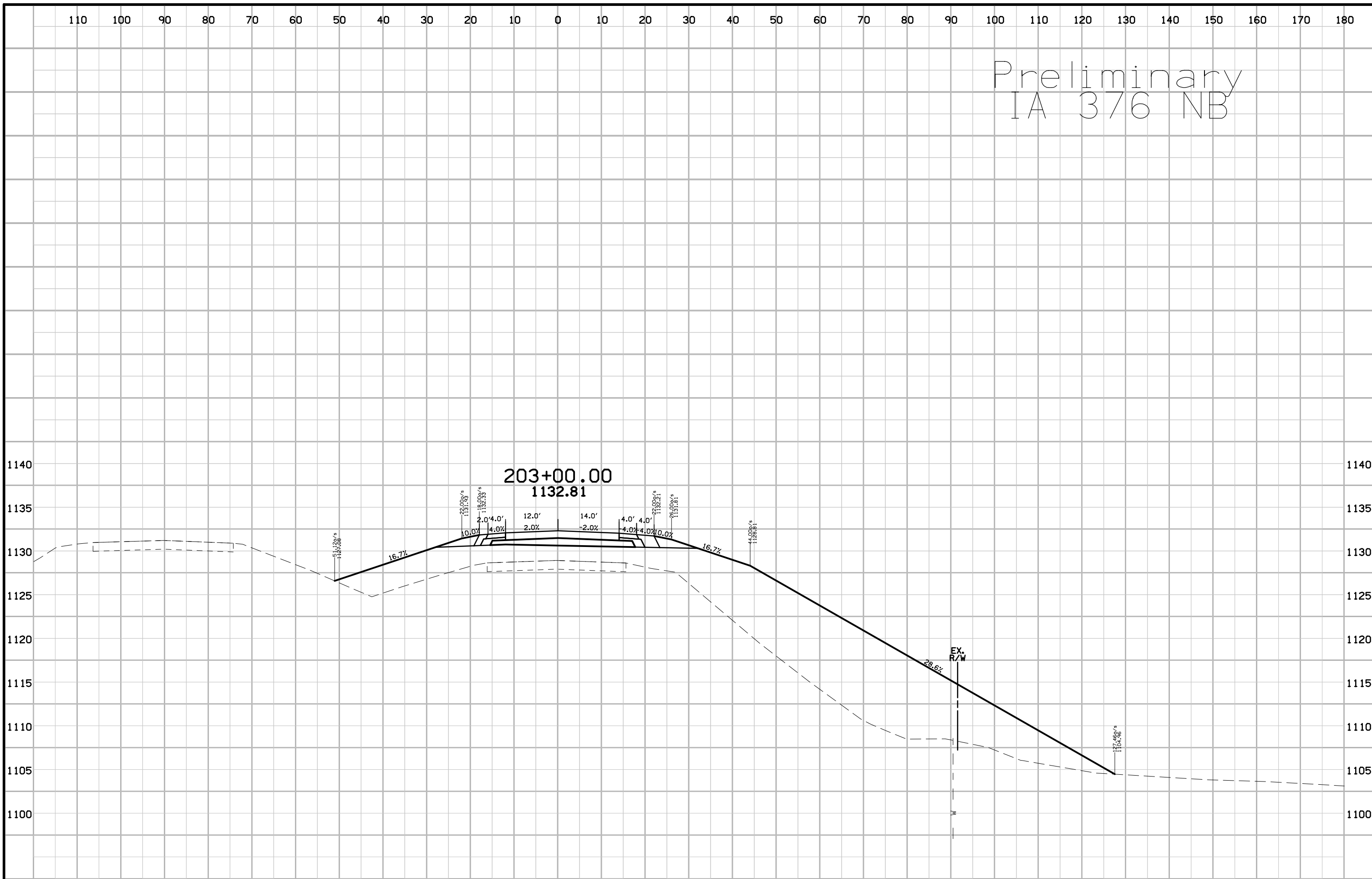
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IA 376 NB



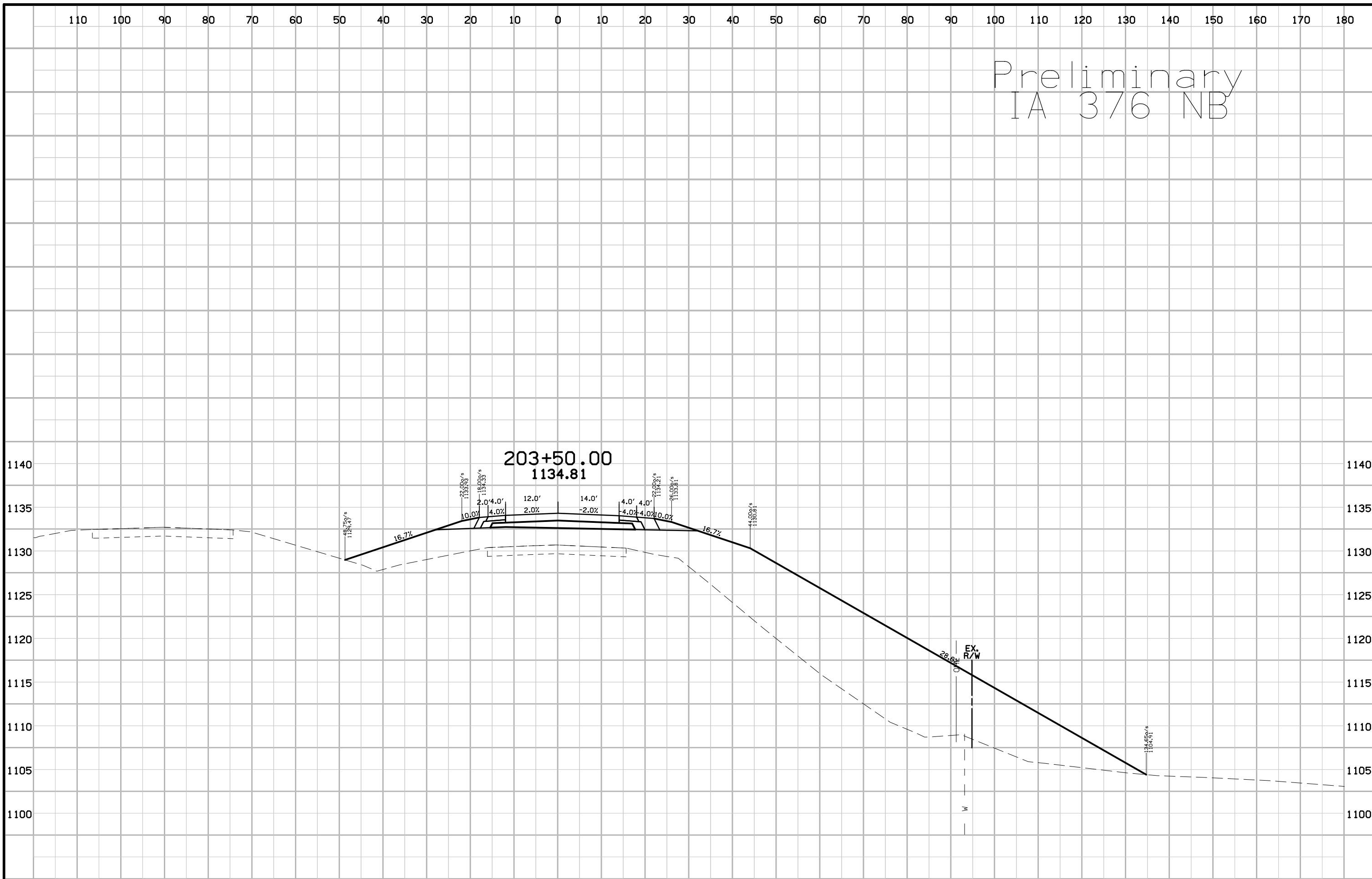
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IA 376 NB



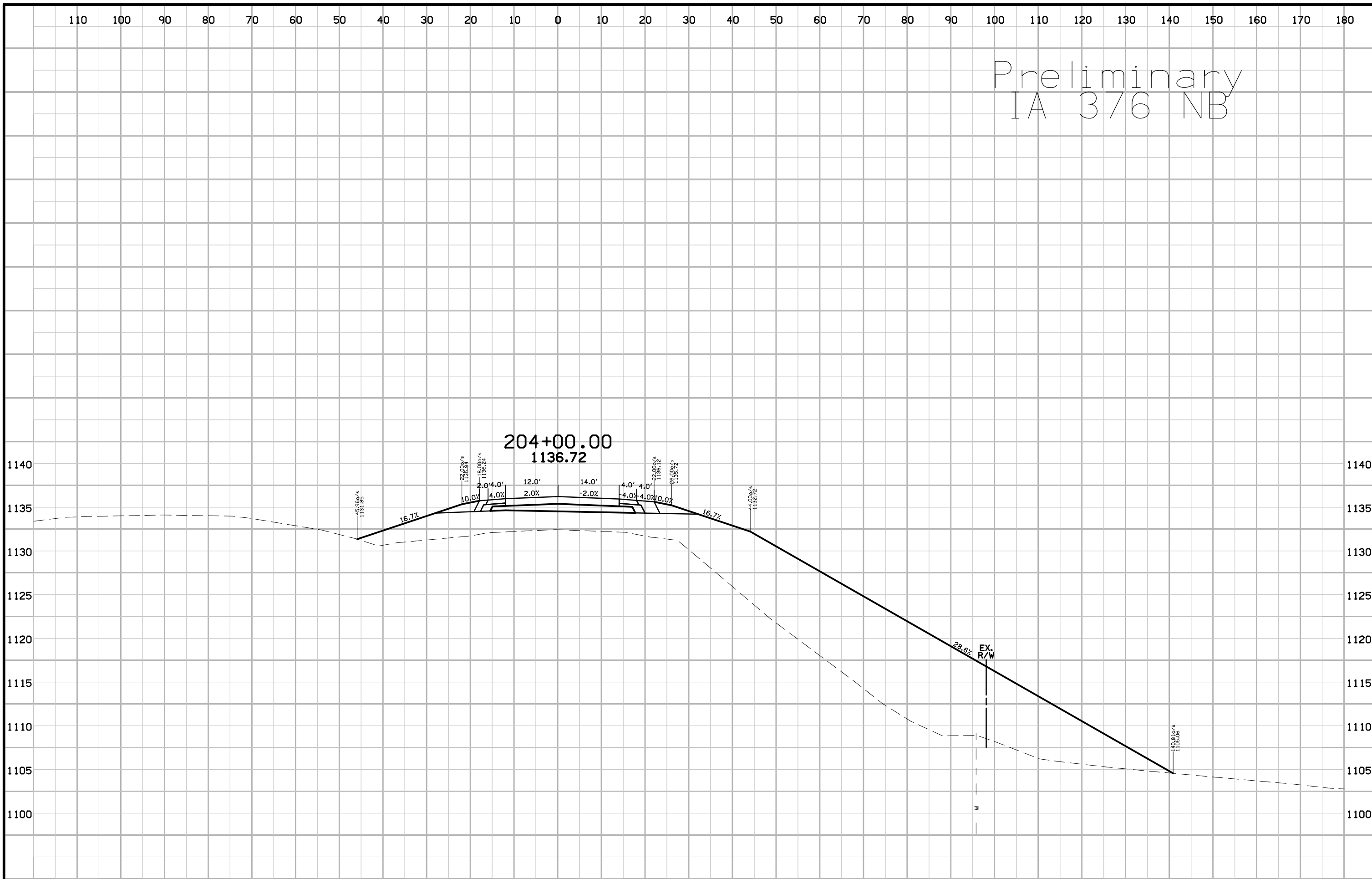
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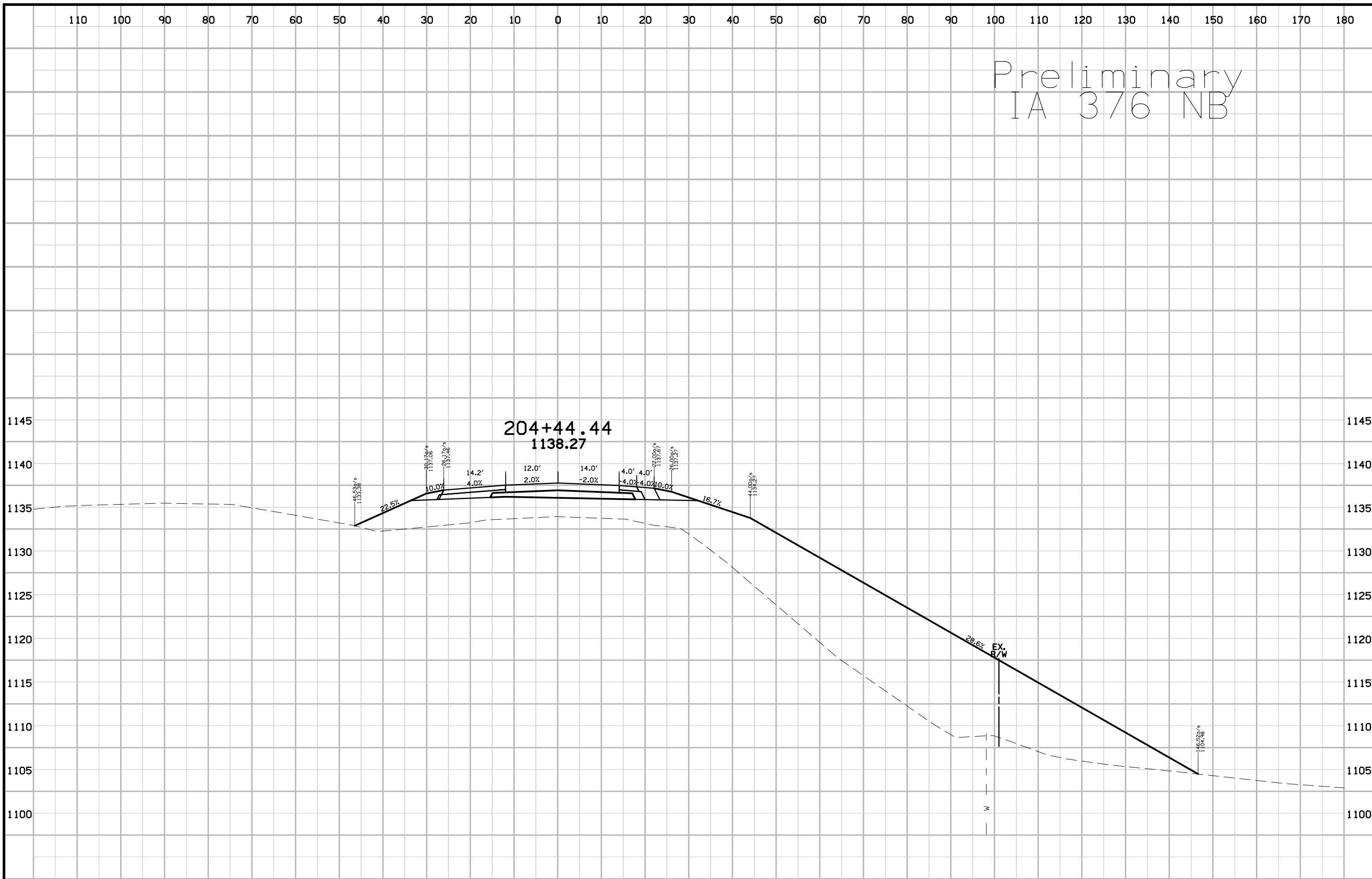
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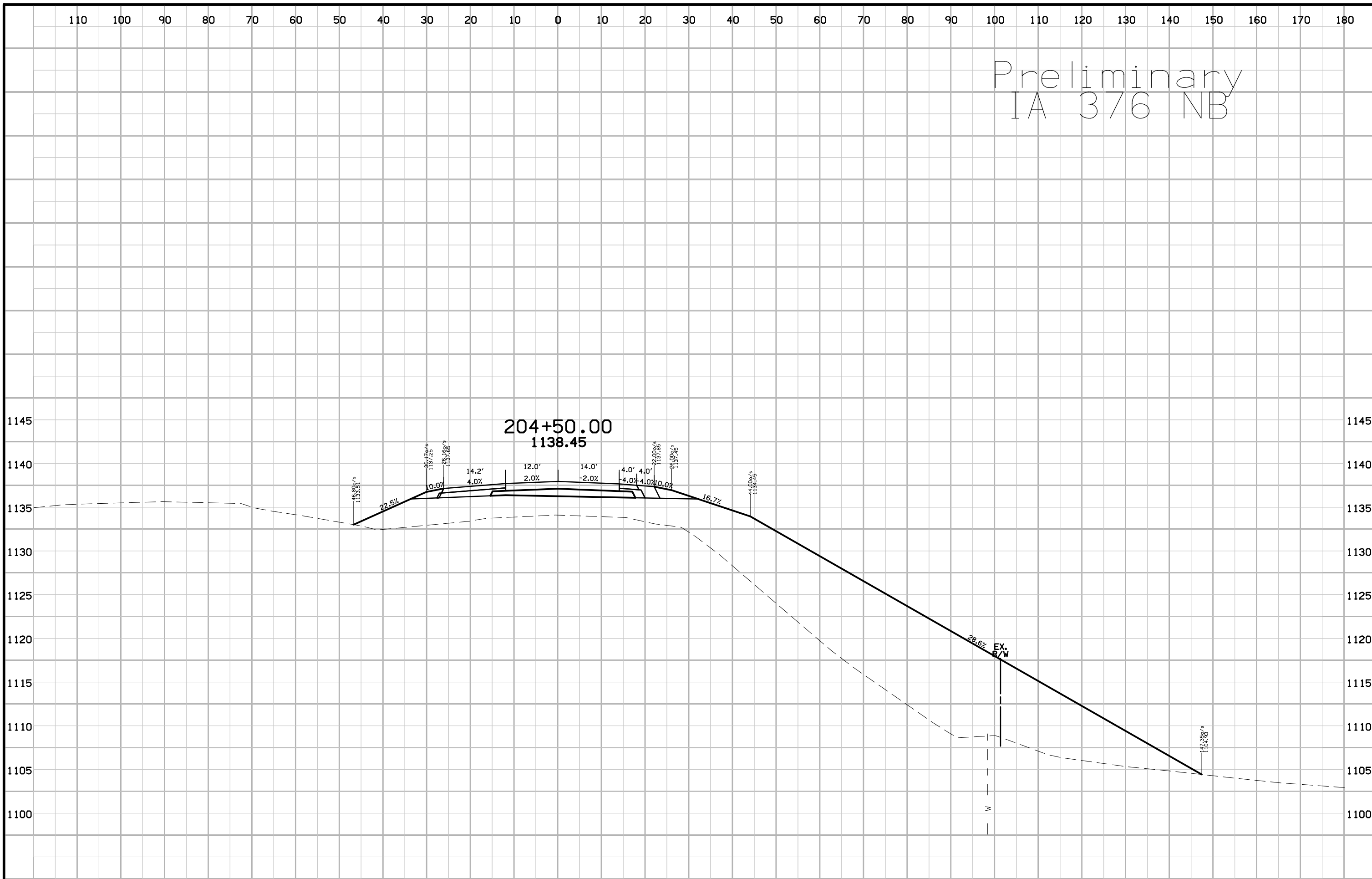
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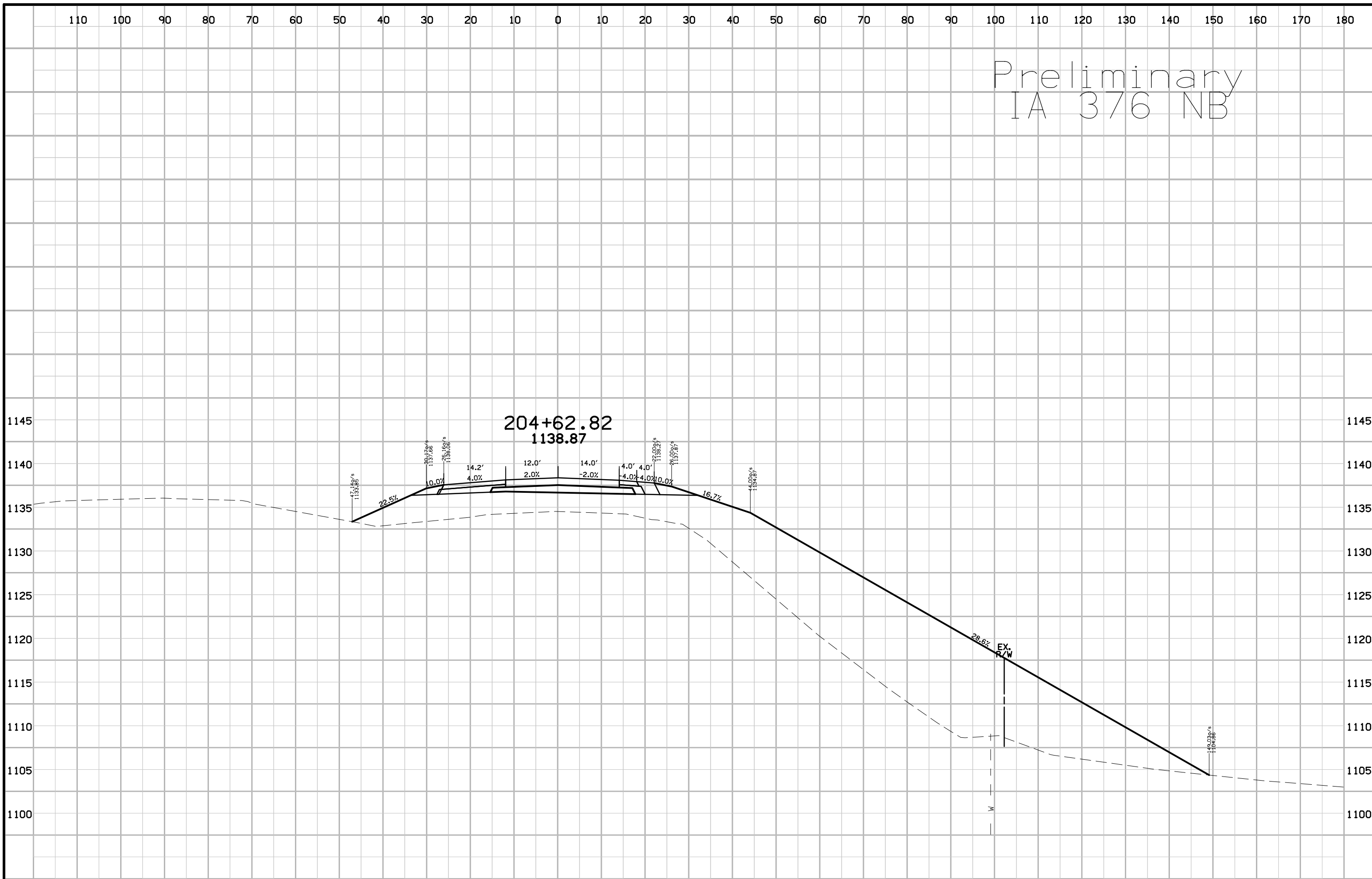
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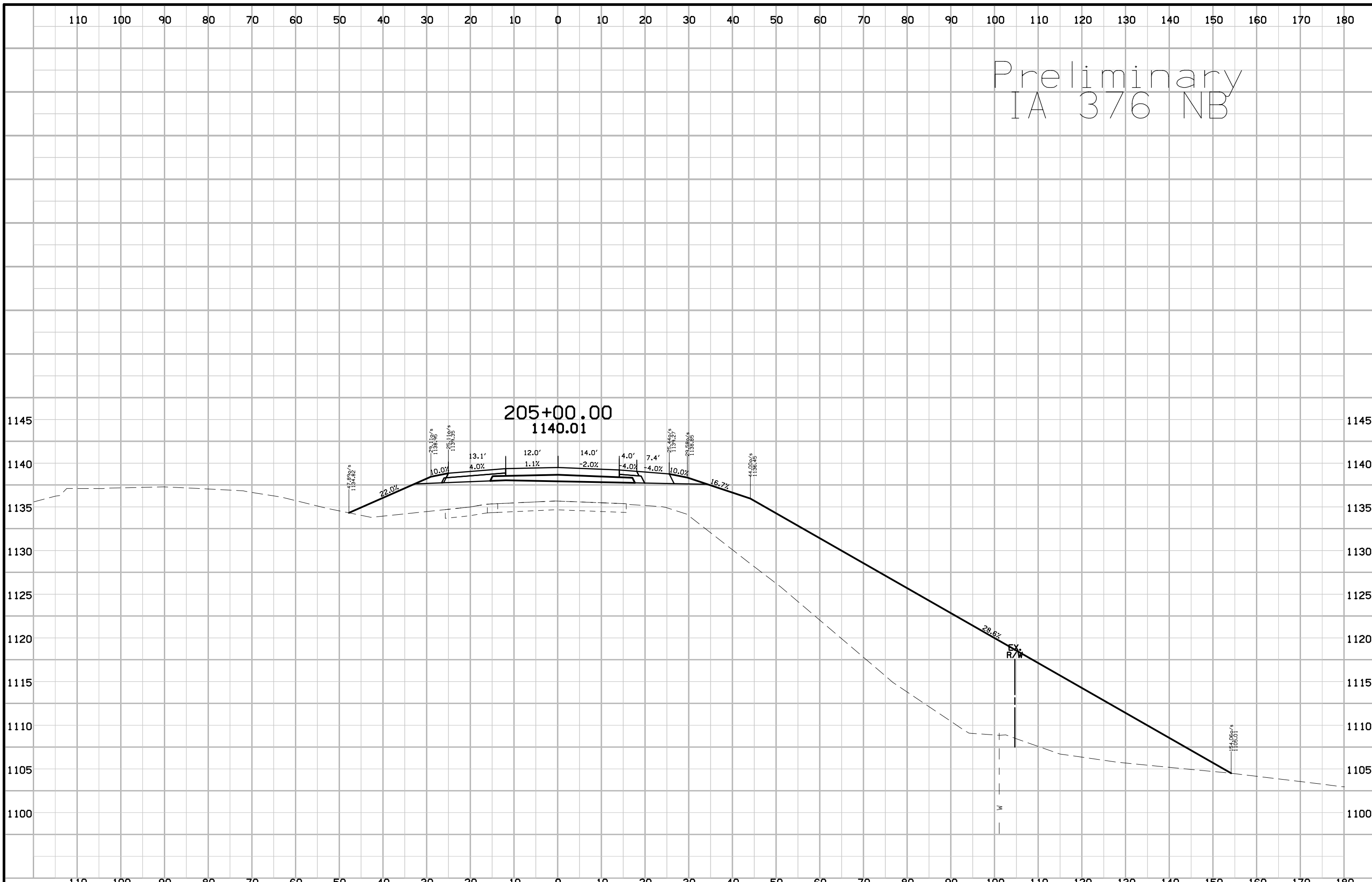
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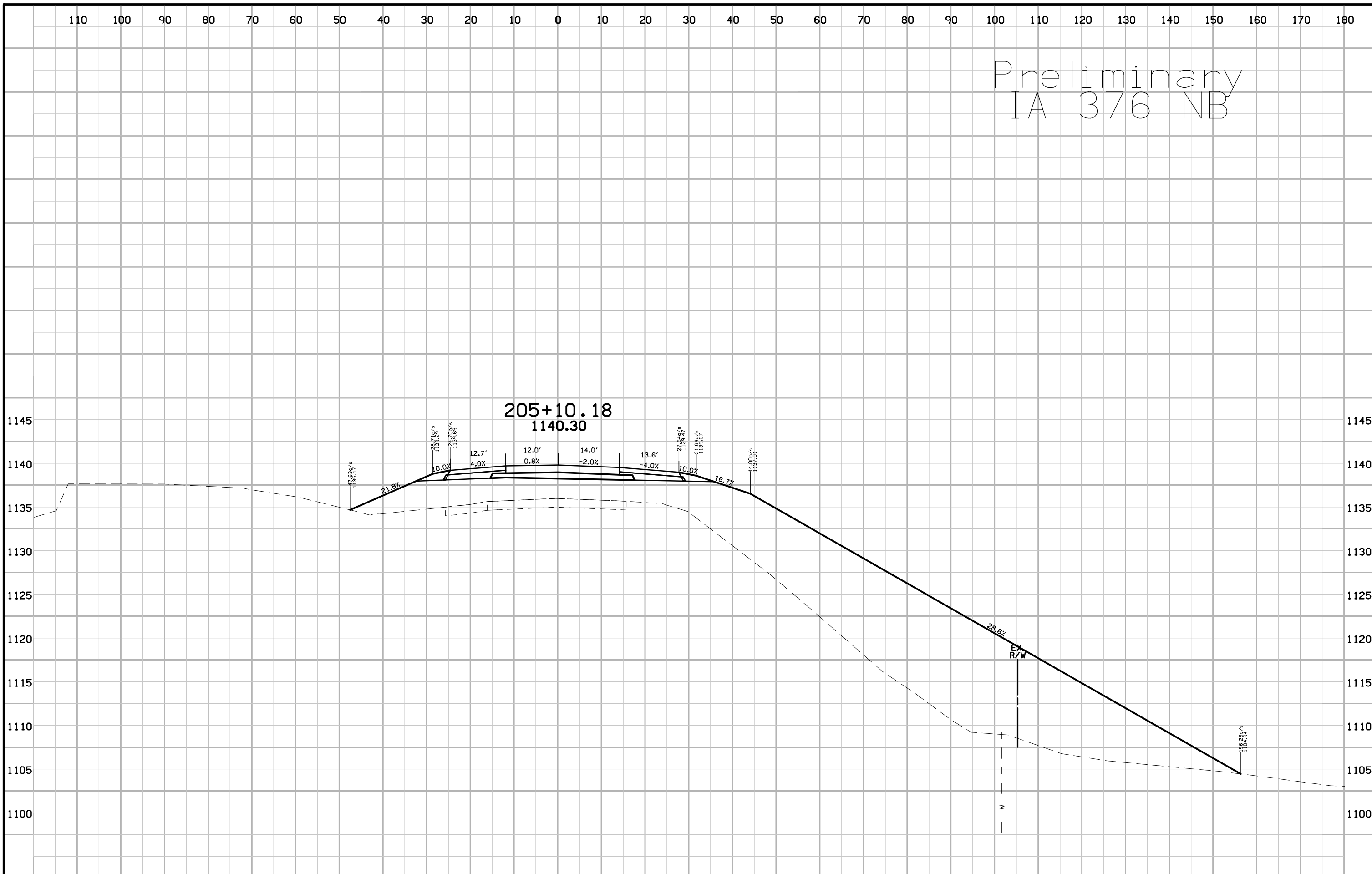
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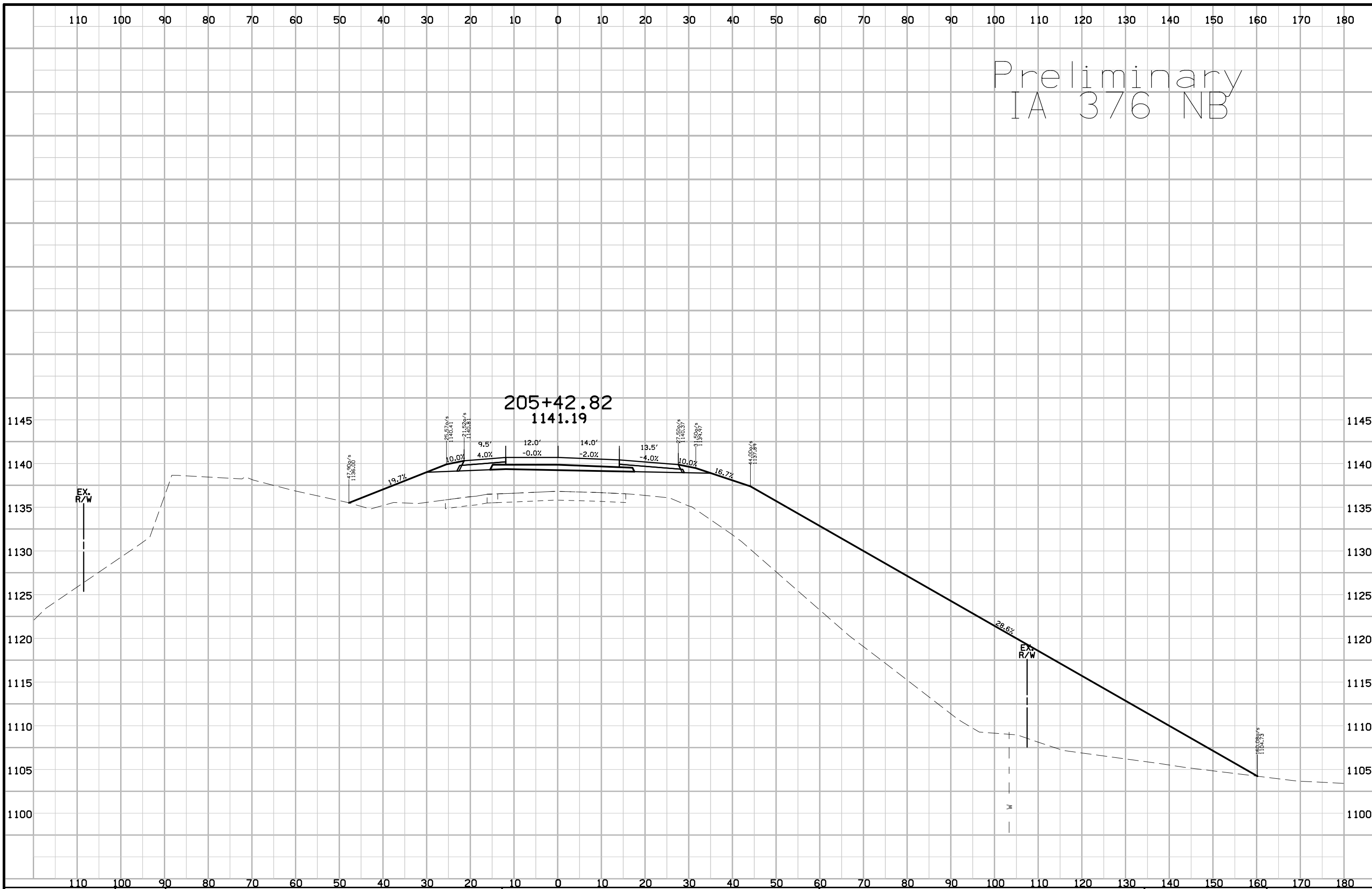
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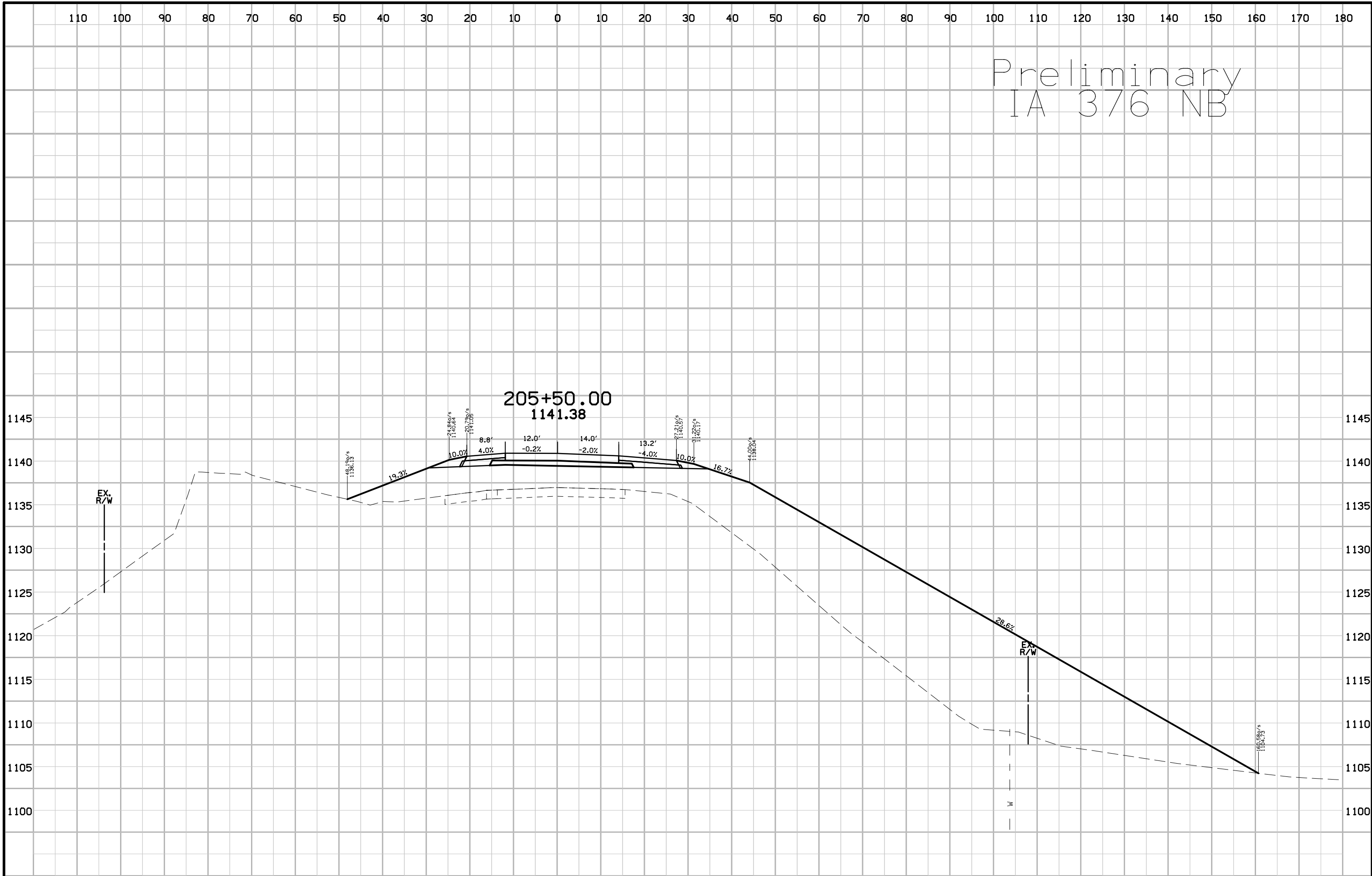
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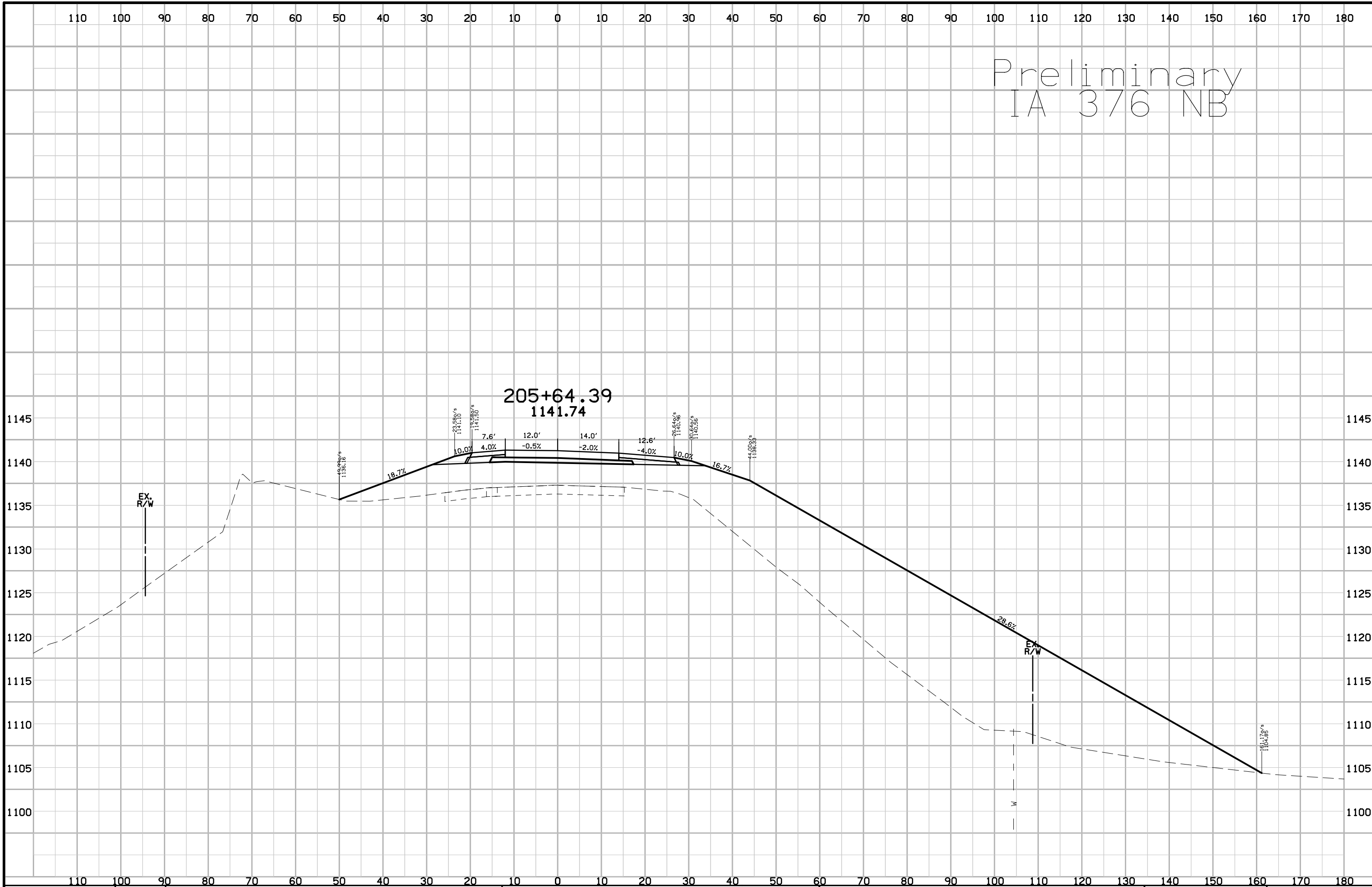
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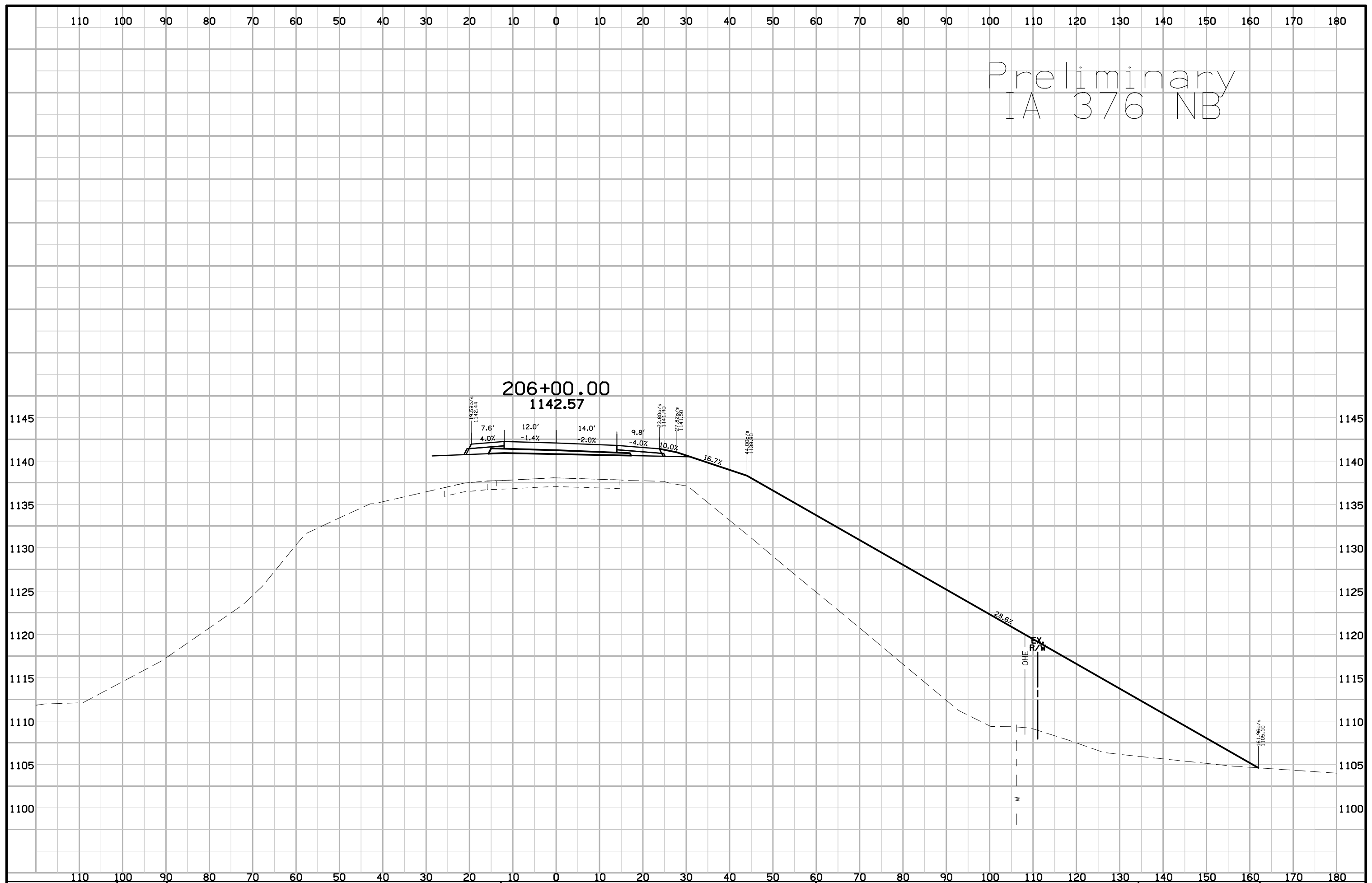
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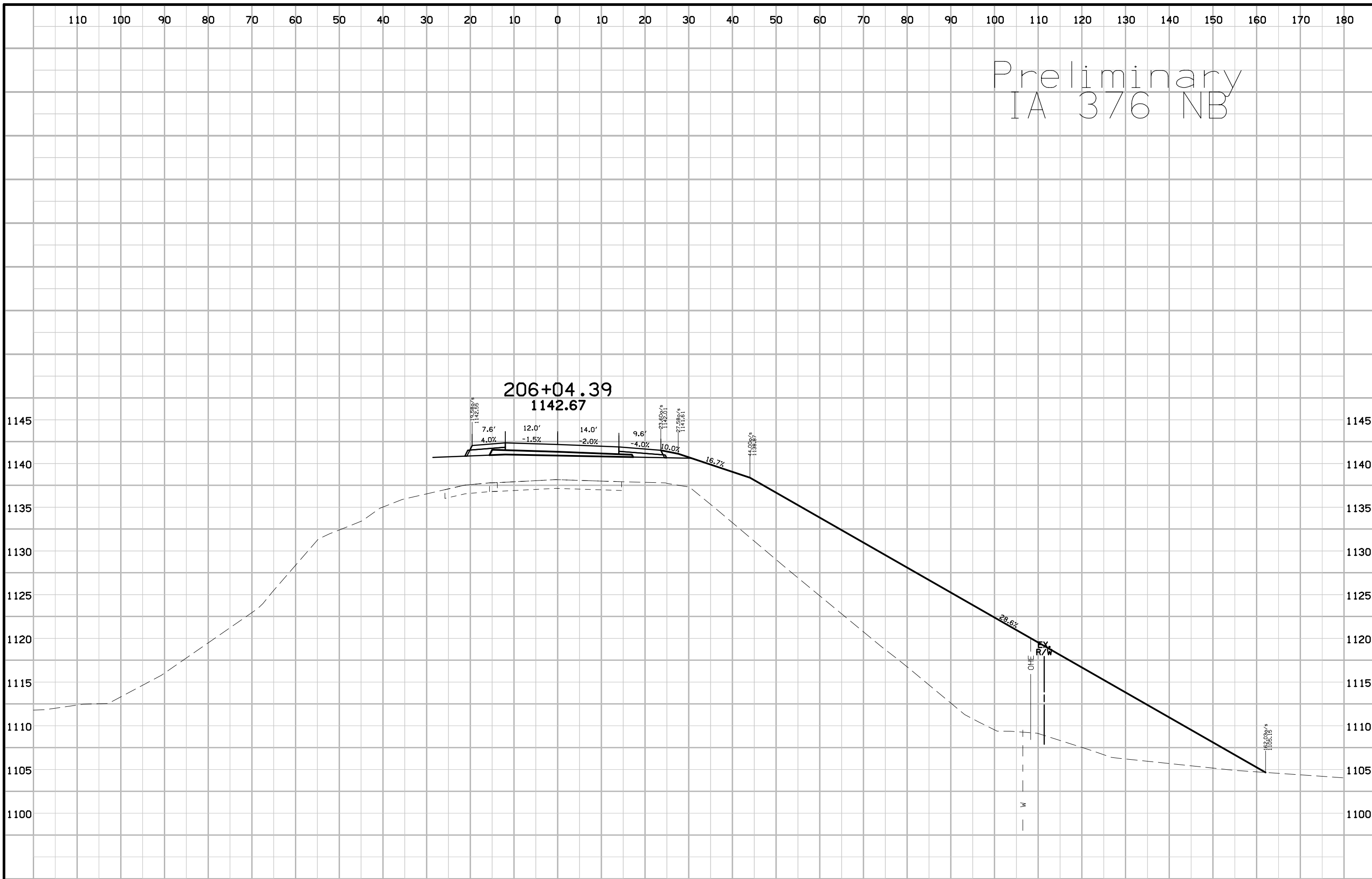
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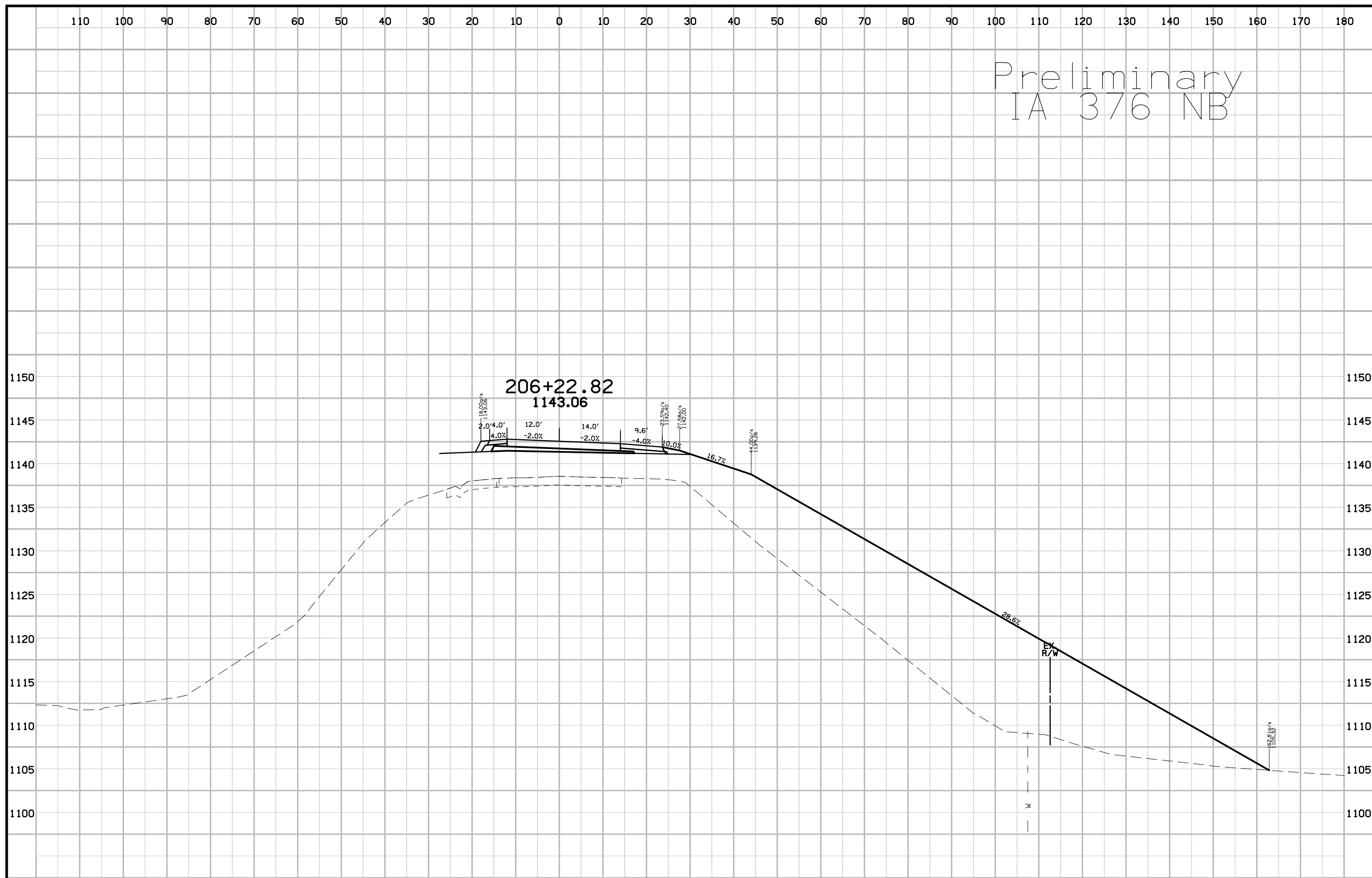
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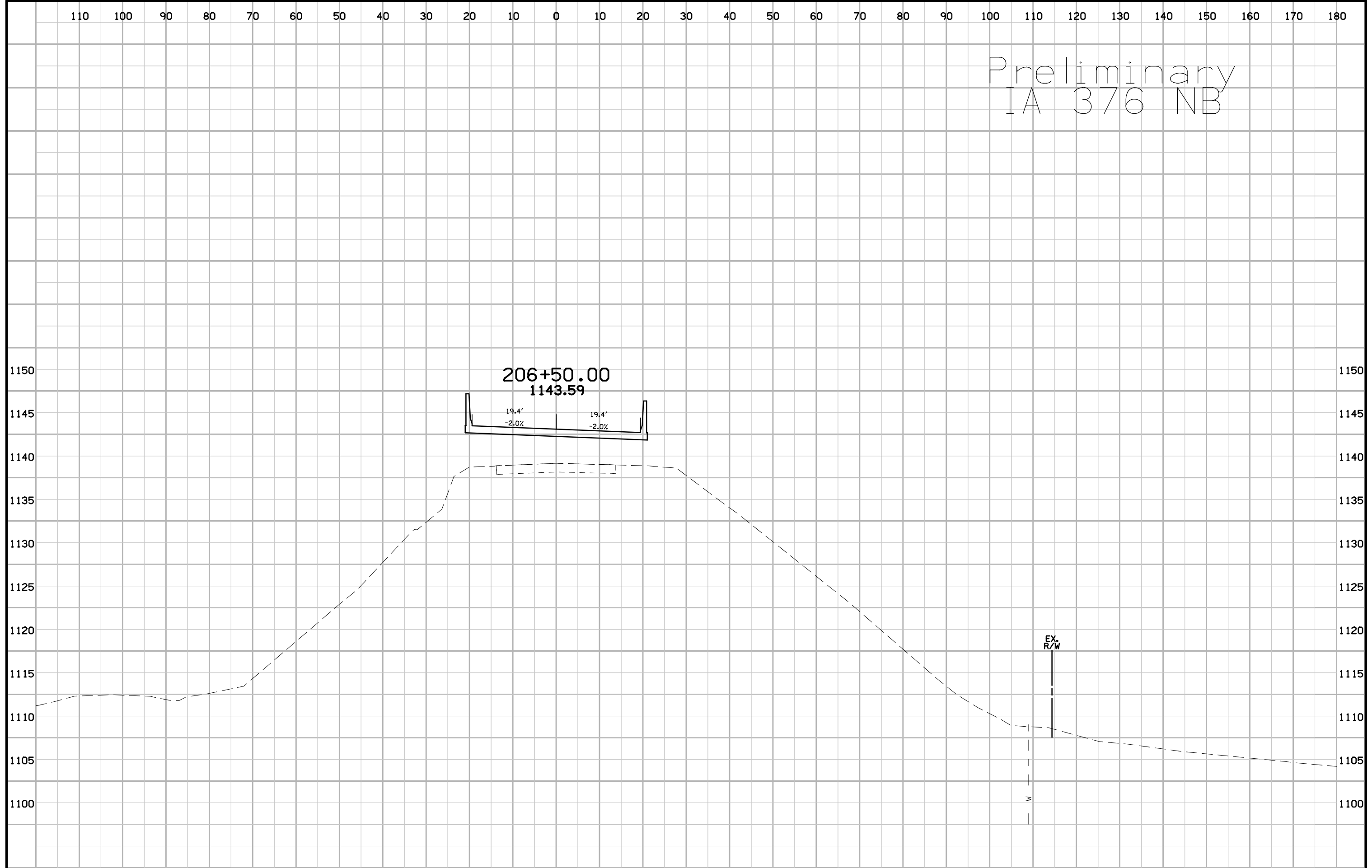
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Preliminary
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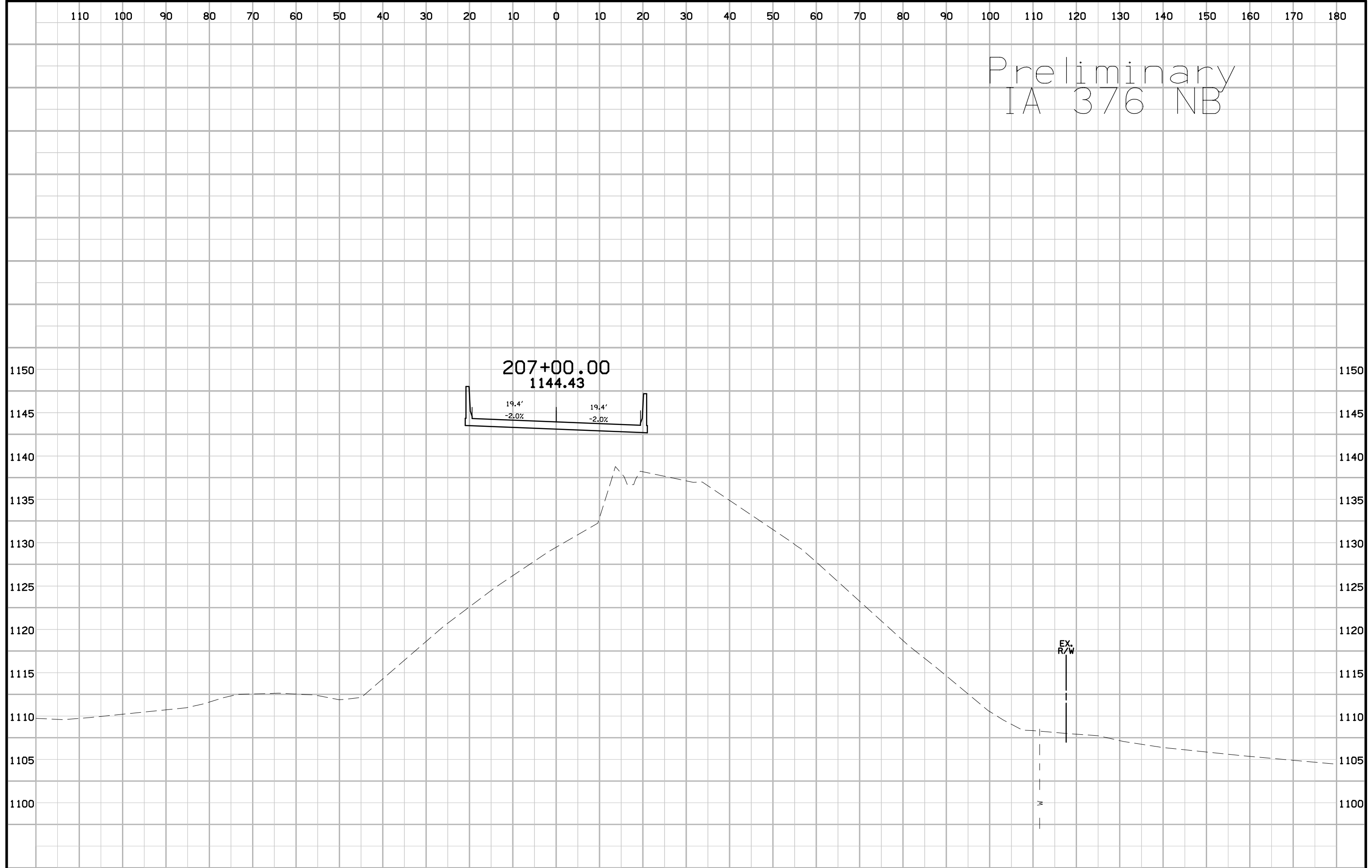


206+50.00
1143.59

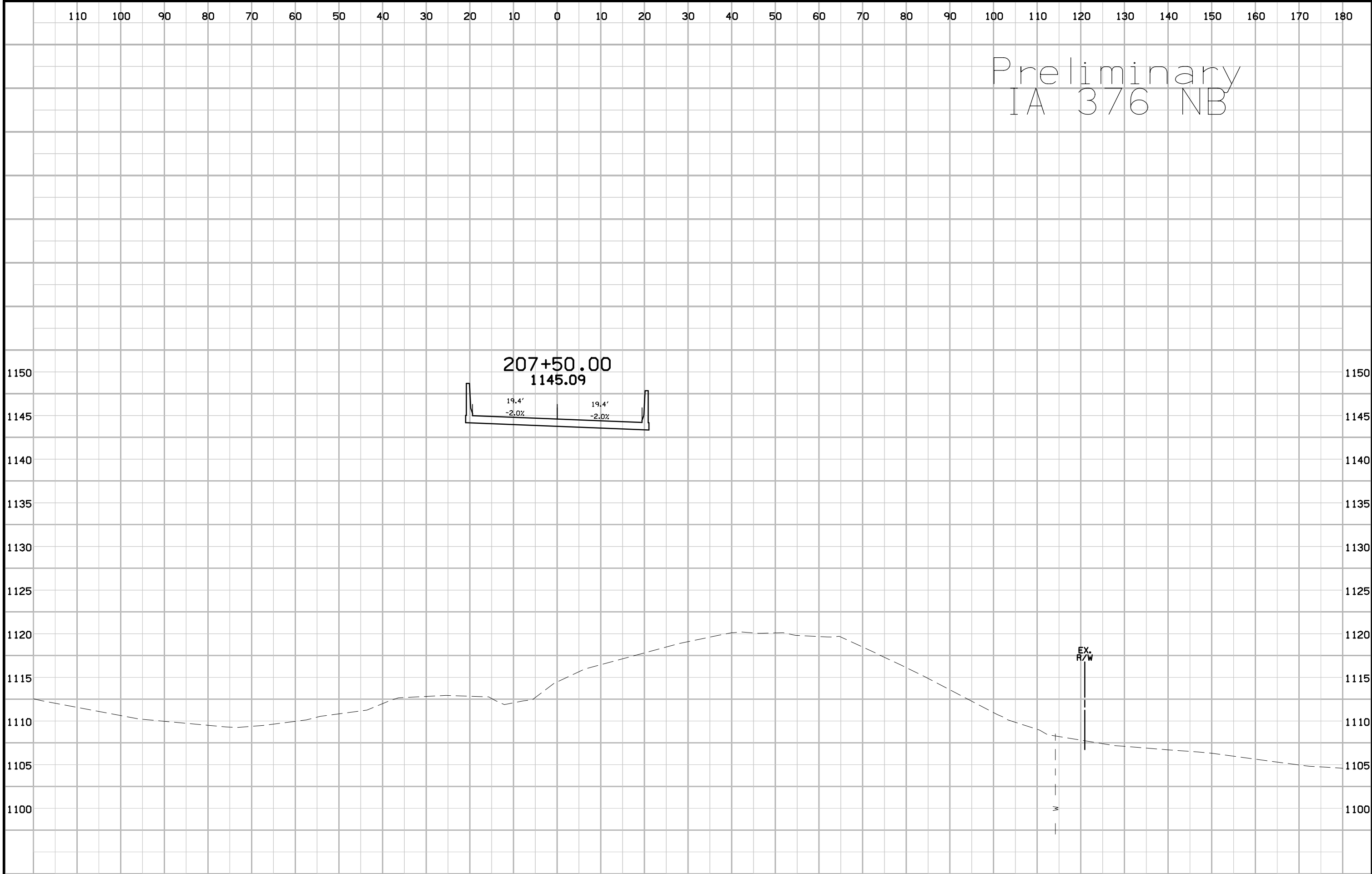
19.4'
-2.0% 19.4'
-2.0%

EX.
R/W

Preliminary
IA 376 NB



Preliminary
IA 376 NB



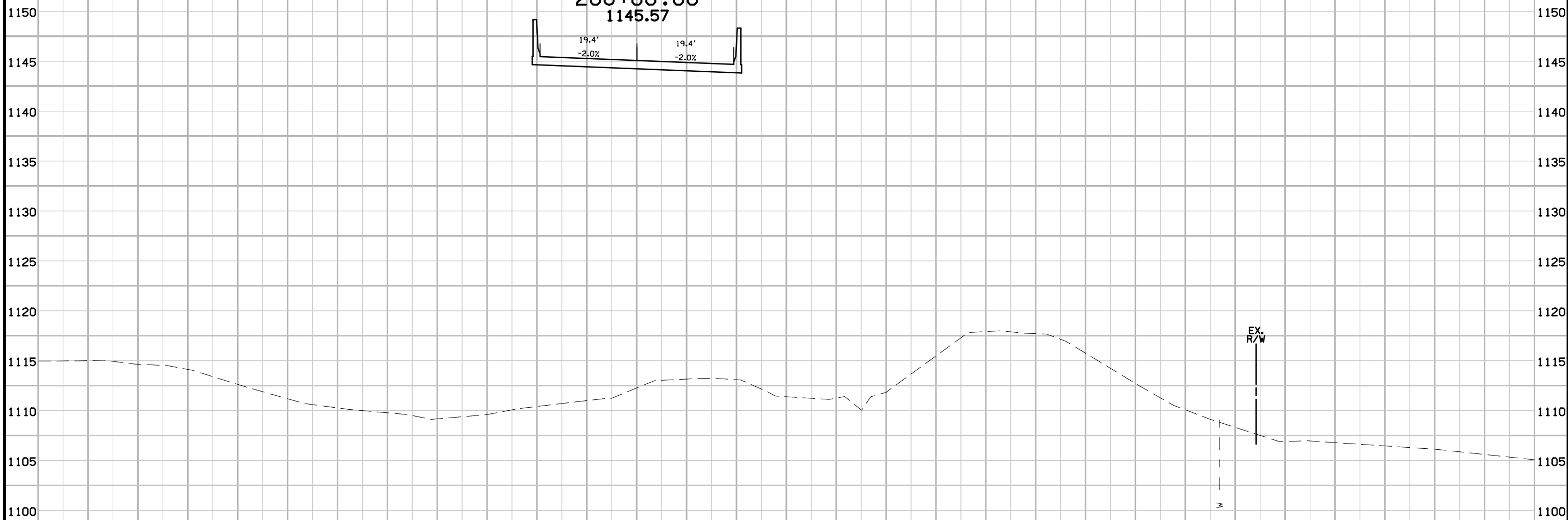
207+50.00
1145.09

19.4' 19.4'
-2.0% -2.0%

EX.
R/W

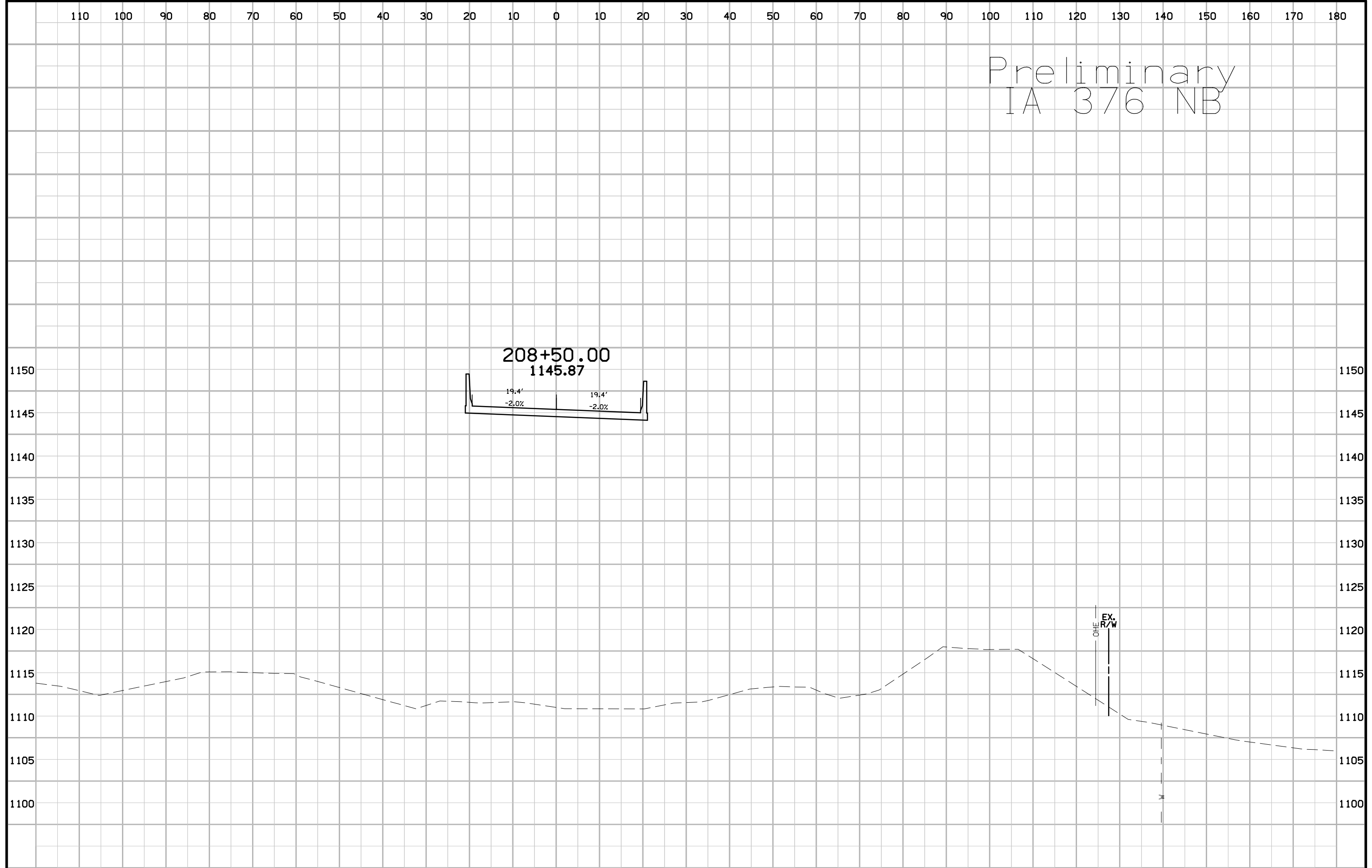
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Preliminary
IA 376 NB

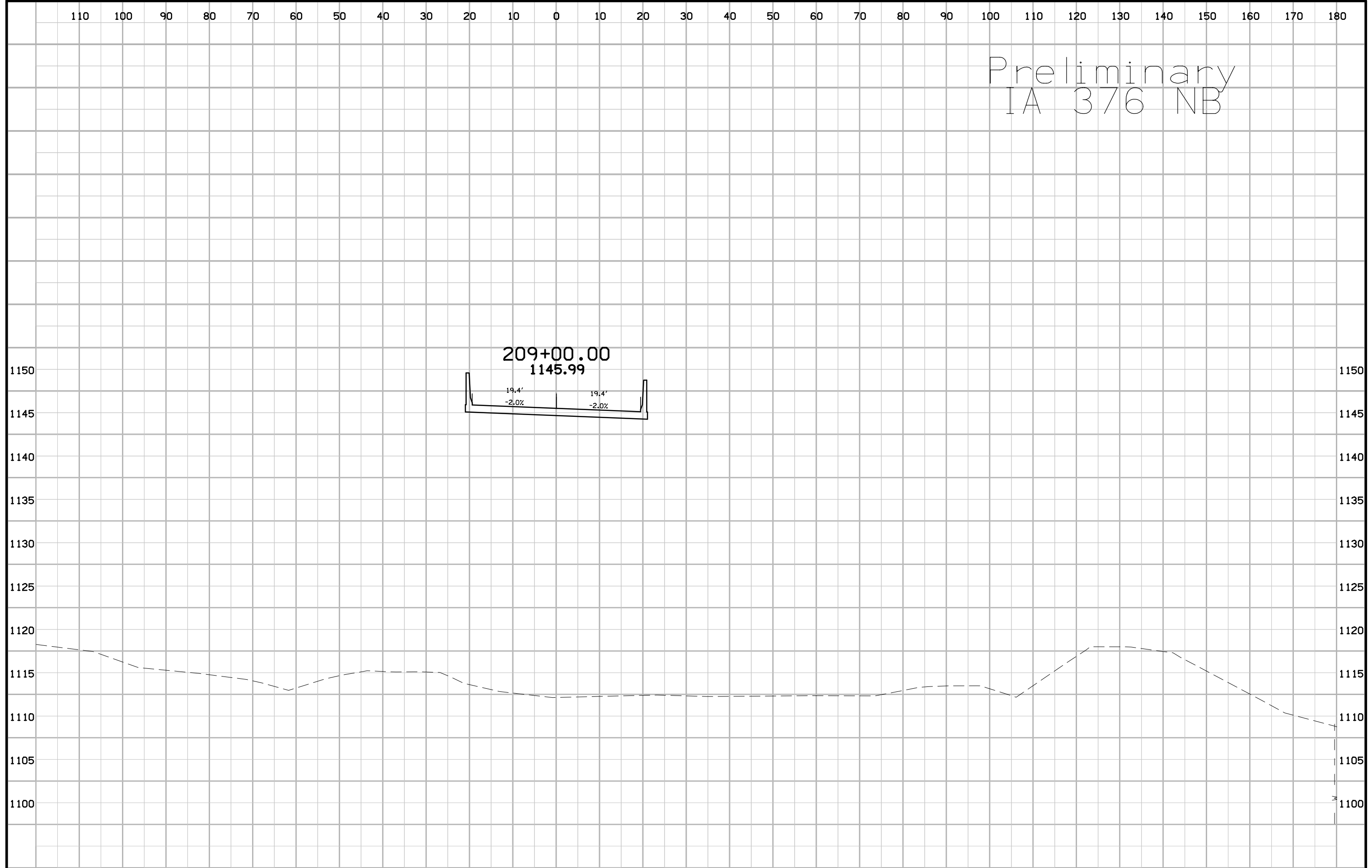


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Preliminary
IA 376 NB

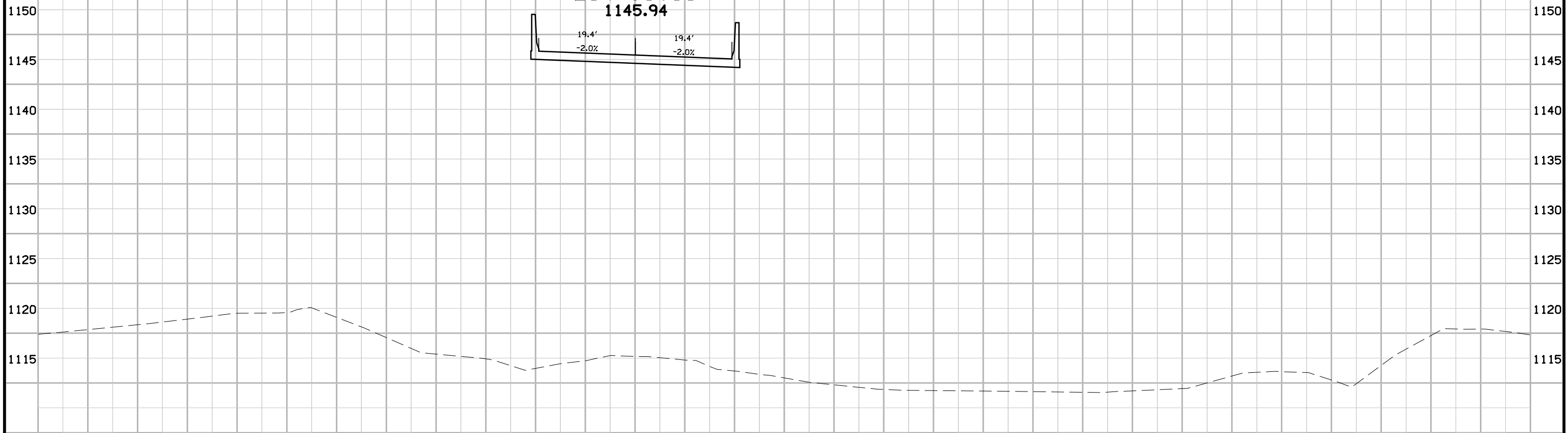


Preliminary
IA 376 NB



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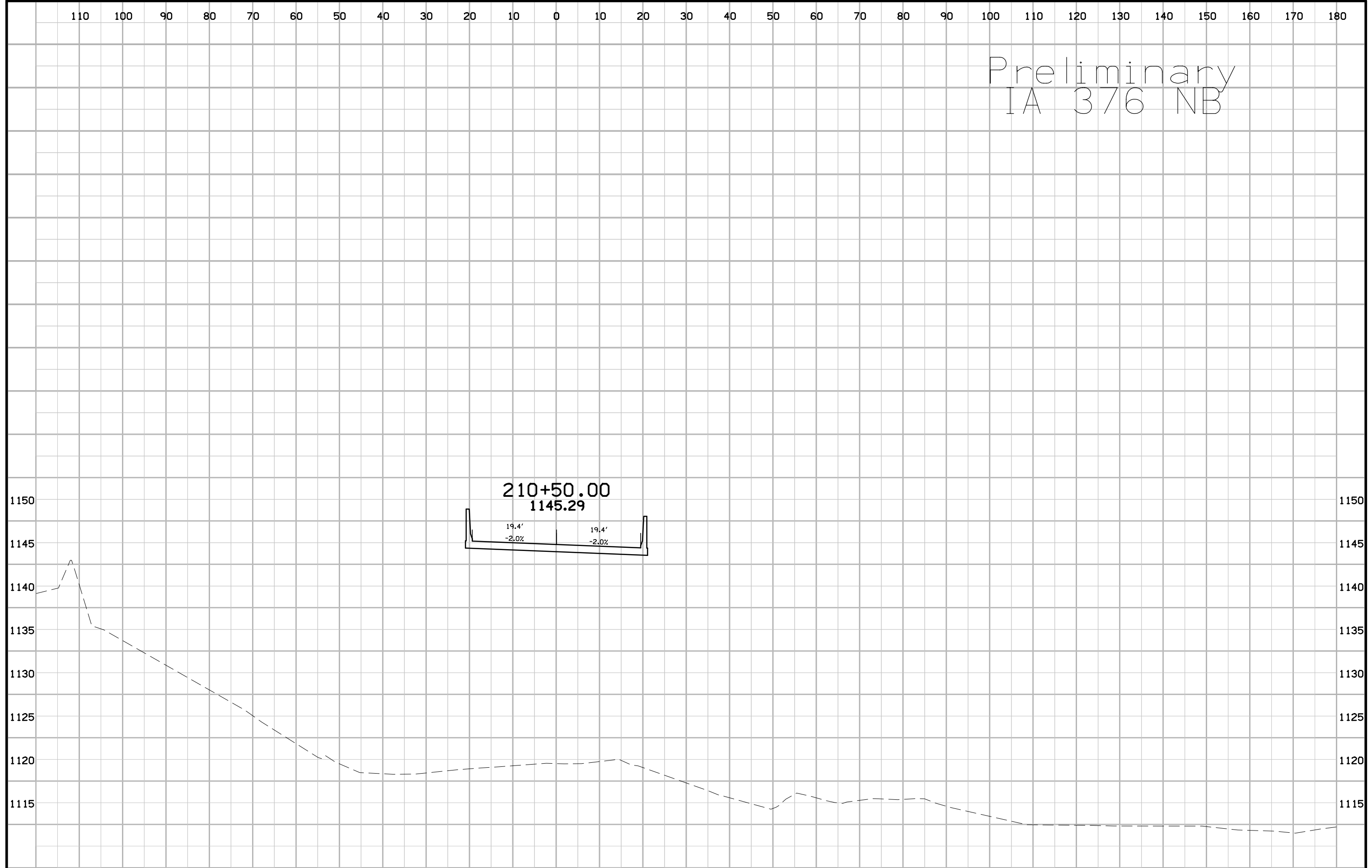


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Preliminary
IA 376 NB

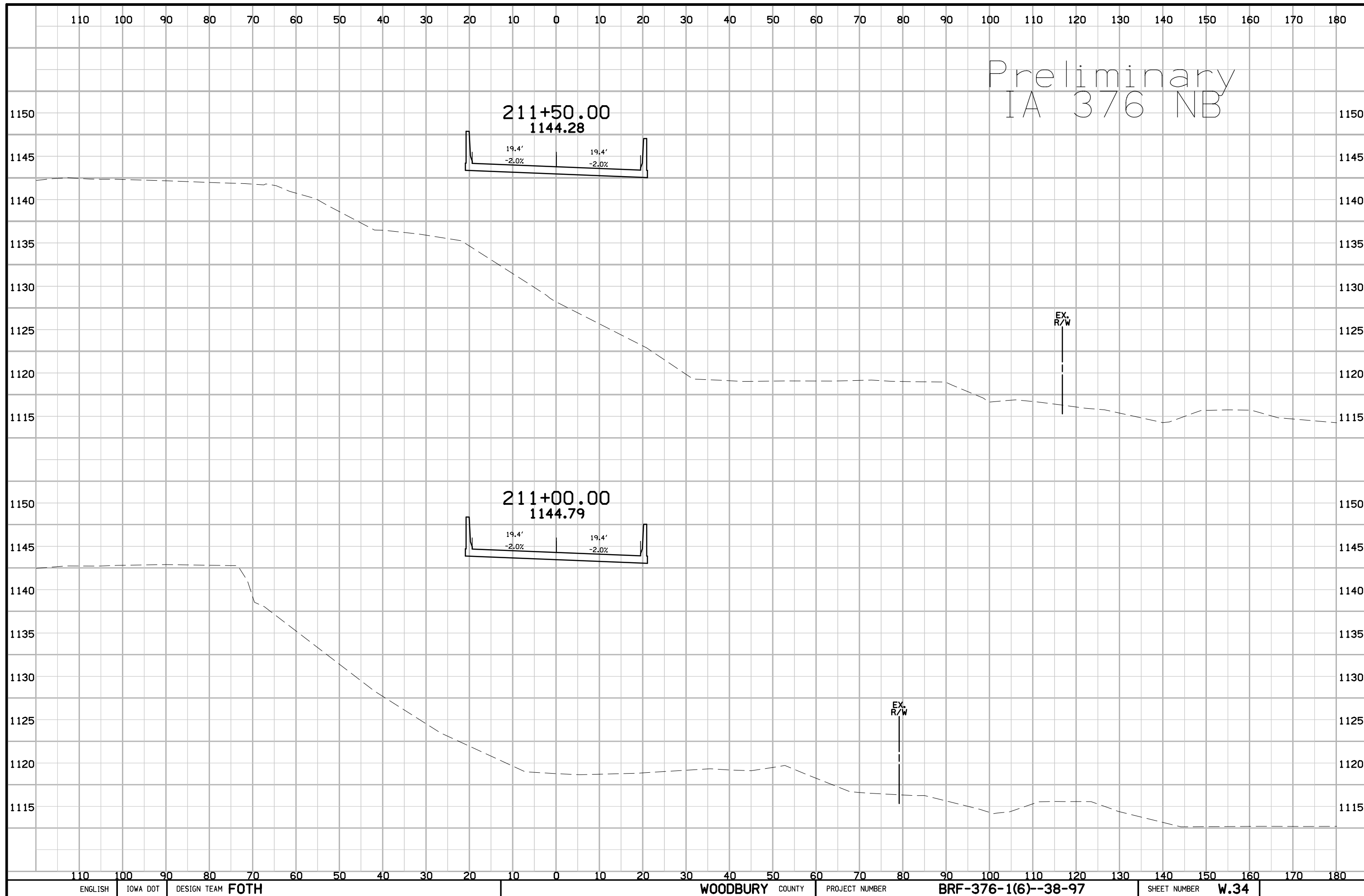


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IA 376 NB



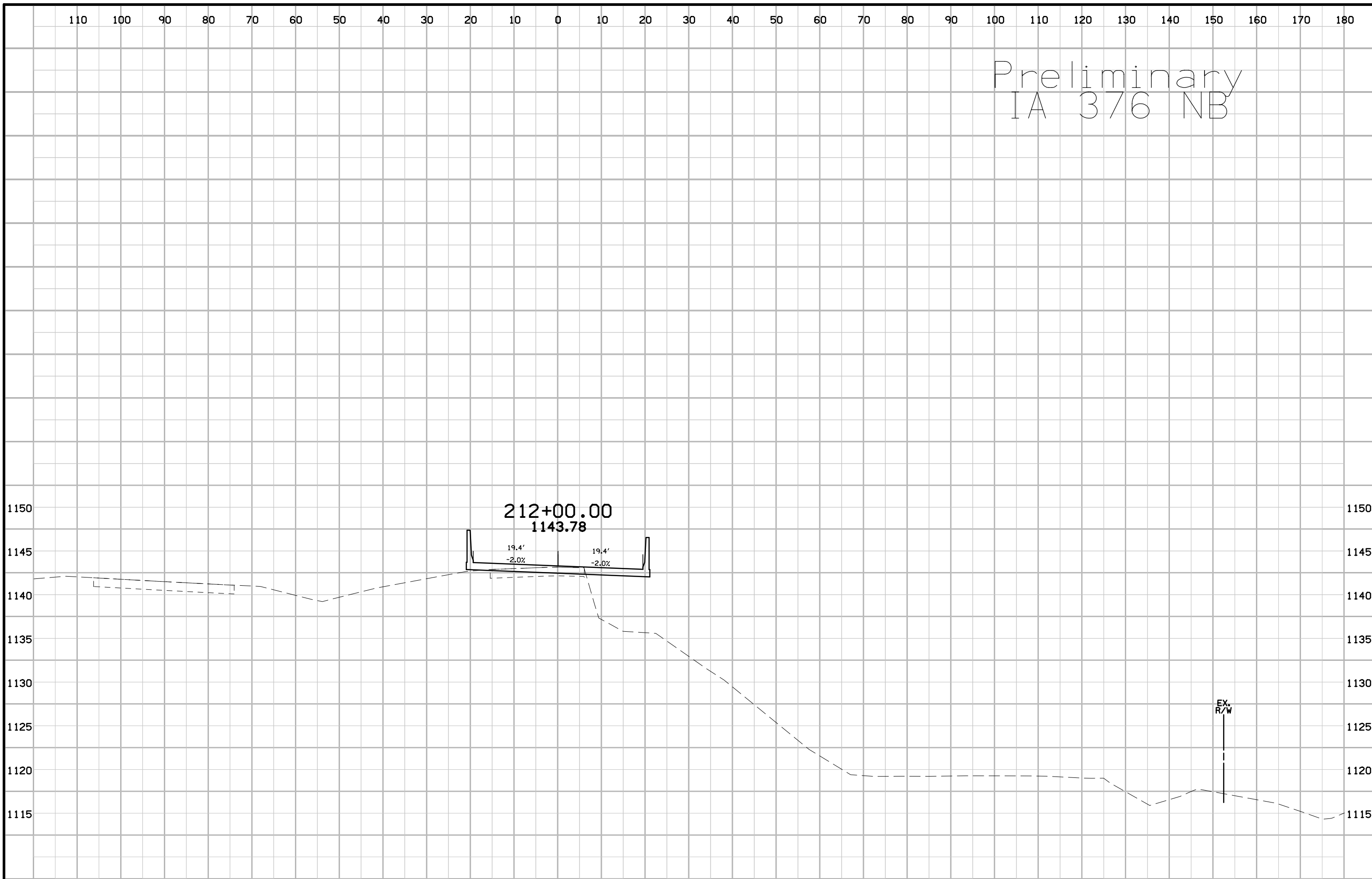
210+50.00
1145.29

19.4'
-2.0% 19.4'
-2.0%

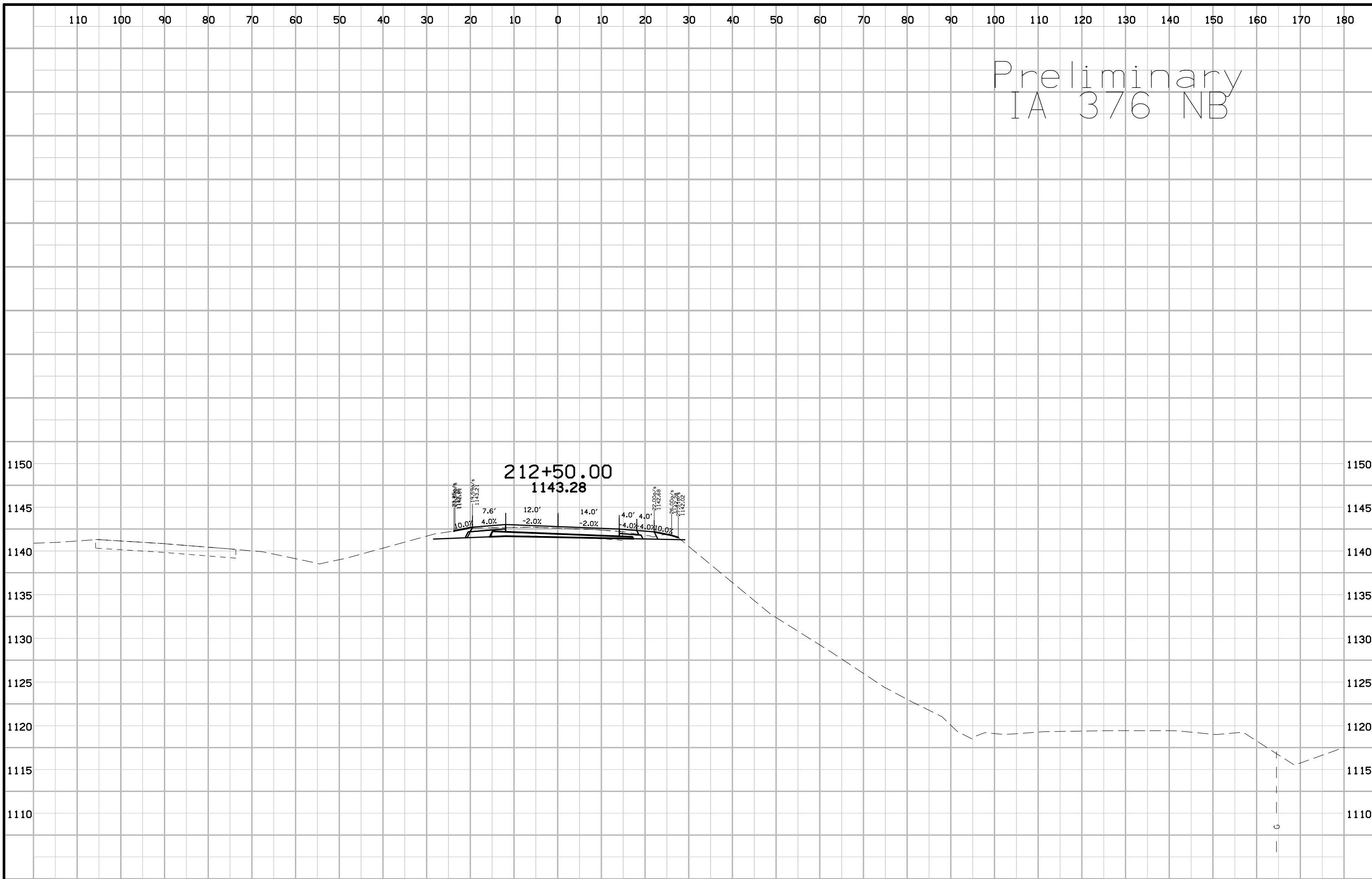


Preliminary
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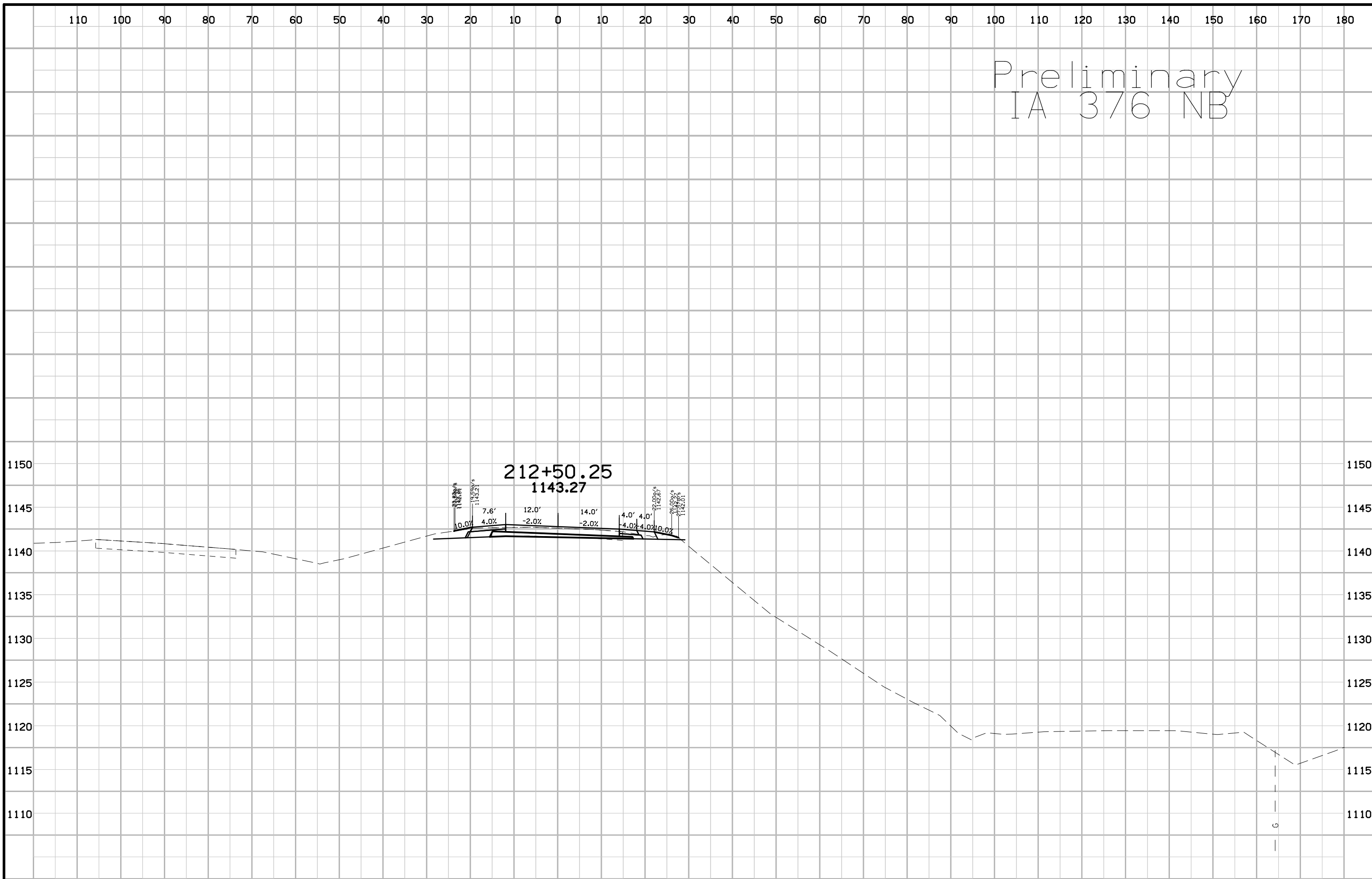
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IA 376 NB



Preliminary
IA 376 NB



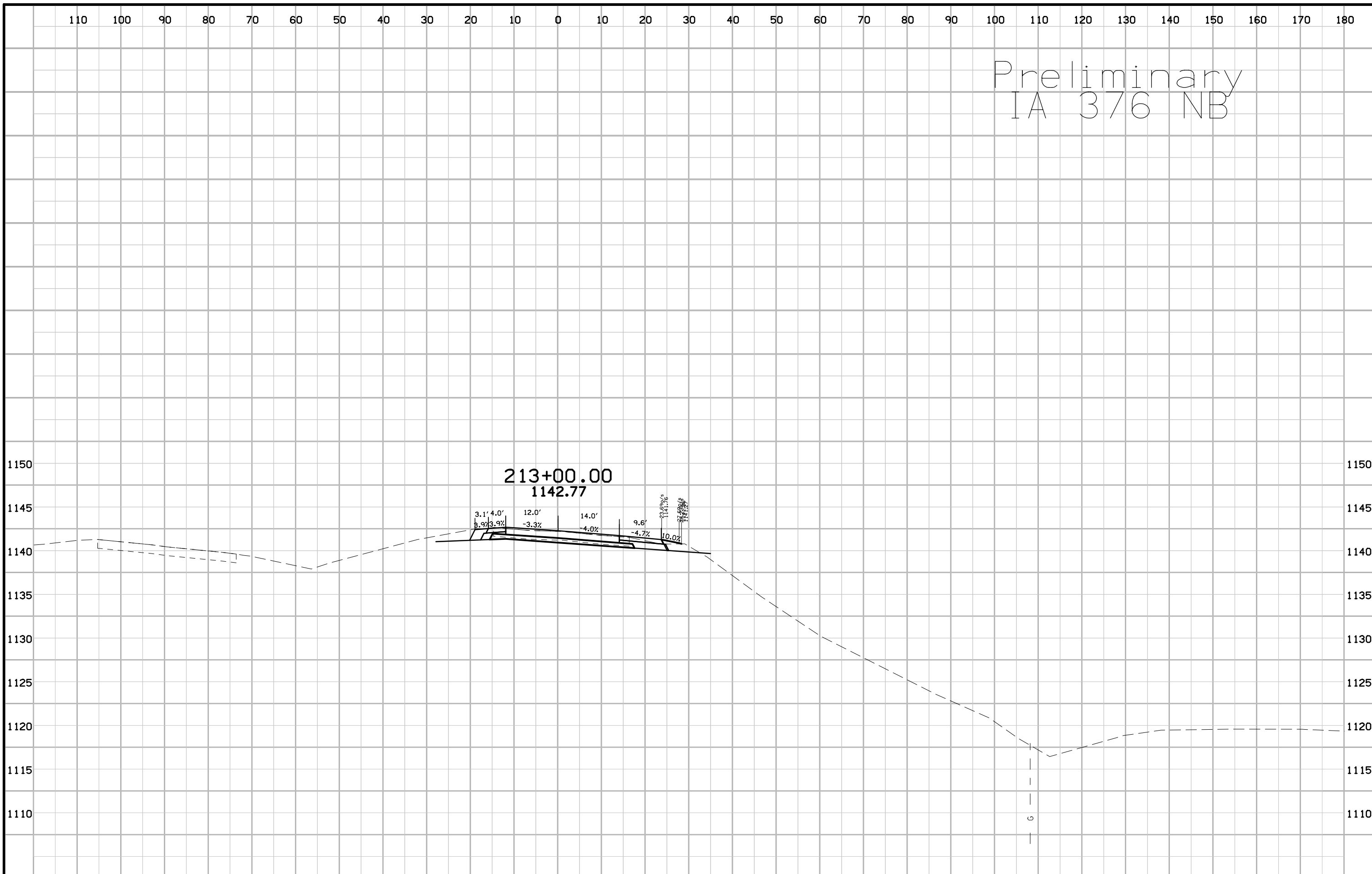
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IA 376 NB



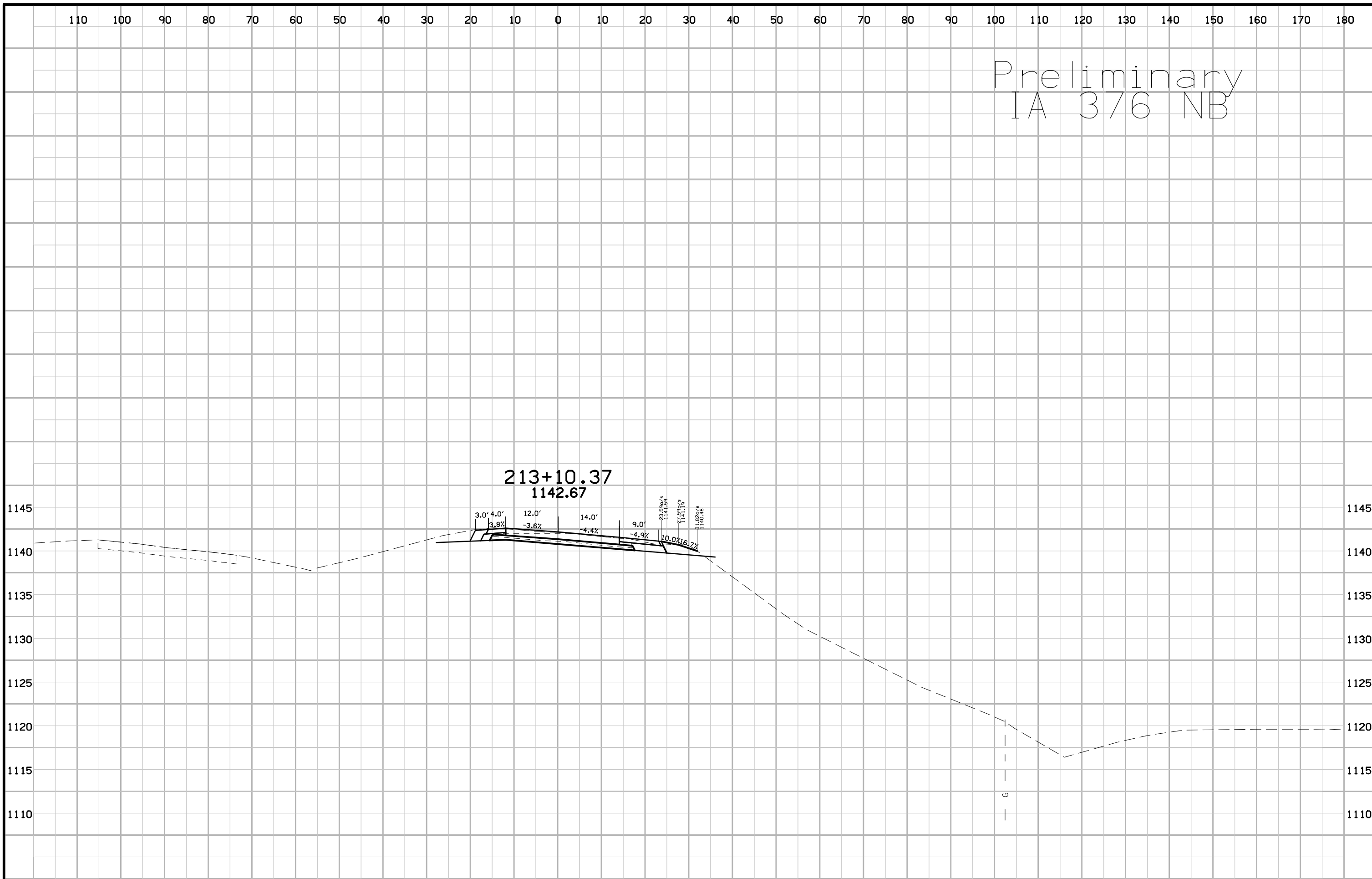
Preliminary
IA 376 NB



Preliminary
IA 376 NB



Preliminary
IA 376 NB



213+10.37
1142.67

3.0' 4.0' 12.0' 14.0' 9.0'
3.8% -3.6% -4.4% -4.9% 10.0% 16.7%

21.5%
1141.59
27.5%
1141.19
1140.86

6

Preliminary
IA 376 NB

213+50.00

213+31.10
1142.46

