

**HAMILTON CO.**

PCC PAVEMENT-GRADE AND REPLACE  
IM-035-5(110)133--13-40

LETTING DATE  
10/16/2018



**Highway Division**

PLANS OF PROPOSED IMPROVEMENT ON THE

INTERSTATE ROAD SYSTEM

**HAMILTON COUNTY**

PCC PAVEMENT-GRADE AND REPLACE

I-35 over IA 175

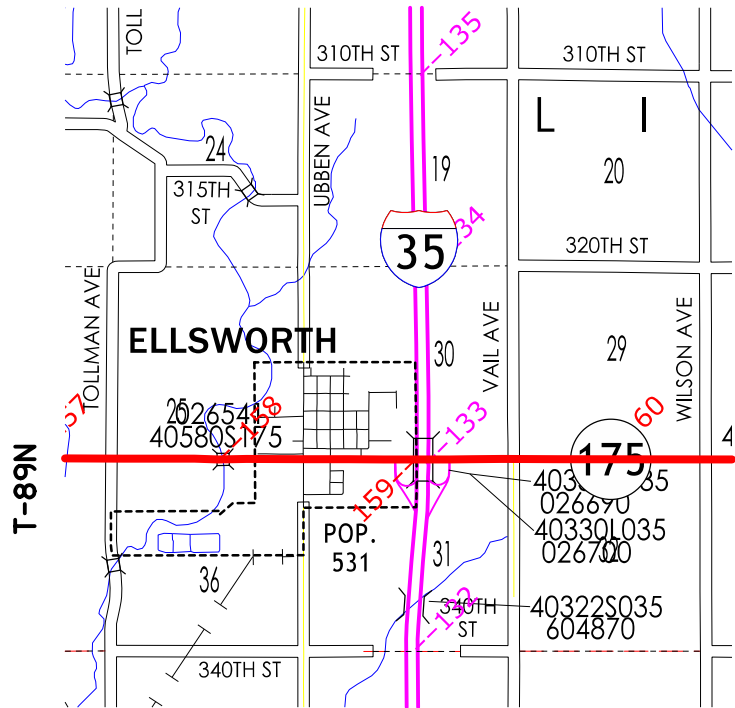
SCALES: As Noted

Refer to the Proposal Form for list of applicable specifications.

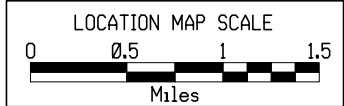
Value Engineering Saves. Refer to Article 1105.15 of the Specifications.



**NO MILEAGE SUMMARY**



R-23W



DESIGN DATA RURAL			
2013	AADT	22,700	V.P.D.
2033	AADT	31,512	V.P.D.
20--	DHV	--	V.P.H.
	TRUCKS	12.1	%
Total			
Design	ESALs	18,070,052	

INDEX OF SEALS		
SHEET NO.	NAME	TYPE
A.1	X	Primary Signature Block
X	X	X

REVISIONS	TOTAL
	X
PROJECT IDENTIFICATION NUMBER	
15-40-035-020	
PROJECT NUMBER	
IM-035-5(110)133--13-40	
R.O.W. PROJECT NUMBER	

SCHEDULED PROJECTS	
<i>Ellsworth Interchange Projects</i>	
PCC Pavement - Grade and New	10/16/18
Traffic Signs	10/16/18
Lighting	10/16/18
Erosion Control	12/17/19
<i>Nearby Projects</i>	
Bridge Deck Overlay on Co Rd D-41	10/18/16
Median Cable - Story City to Ellsworth	07/19/16
Median Cable - Ellsworth to US 20	07/18/17
Resurfacing - I-35 SB at Ellsworth	10/17/17

SCHEDULE SUMMARY		
D0 - Concept	9/10/2015	Complete
D2 - Field Exam	5/9/2016	Complete
D3 - Bridge Submittal	6/10/2016	Complete
D5 - ROW Submittal	10/7/2016	Current
R1 - ROW Layout	1/16/2017	Scheduled
S3 - Soils Design	4/6/2018	Scheduled
DM5 - Review Plans	7/3/2018	Scheduled
D8 - Contracts Submittal	8/7/2018	Scheduled
L3 - Letting	10/16/2018	Scheduled

see A02 for a detailed schedule

PRELIMINARY PLANS

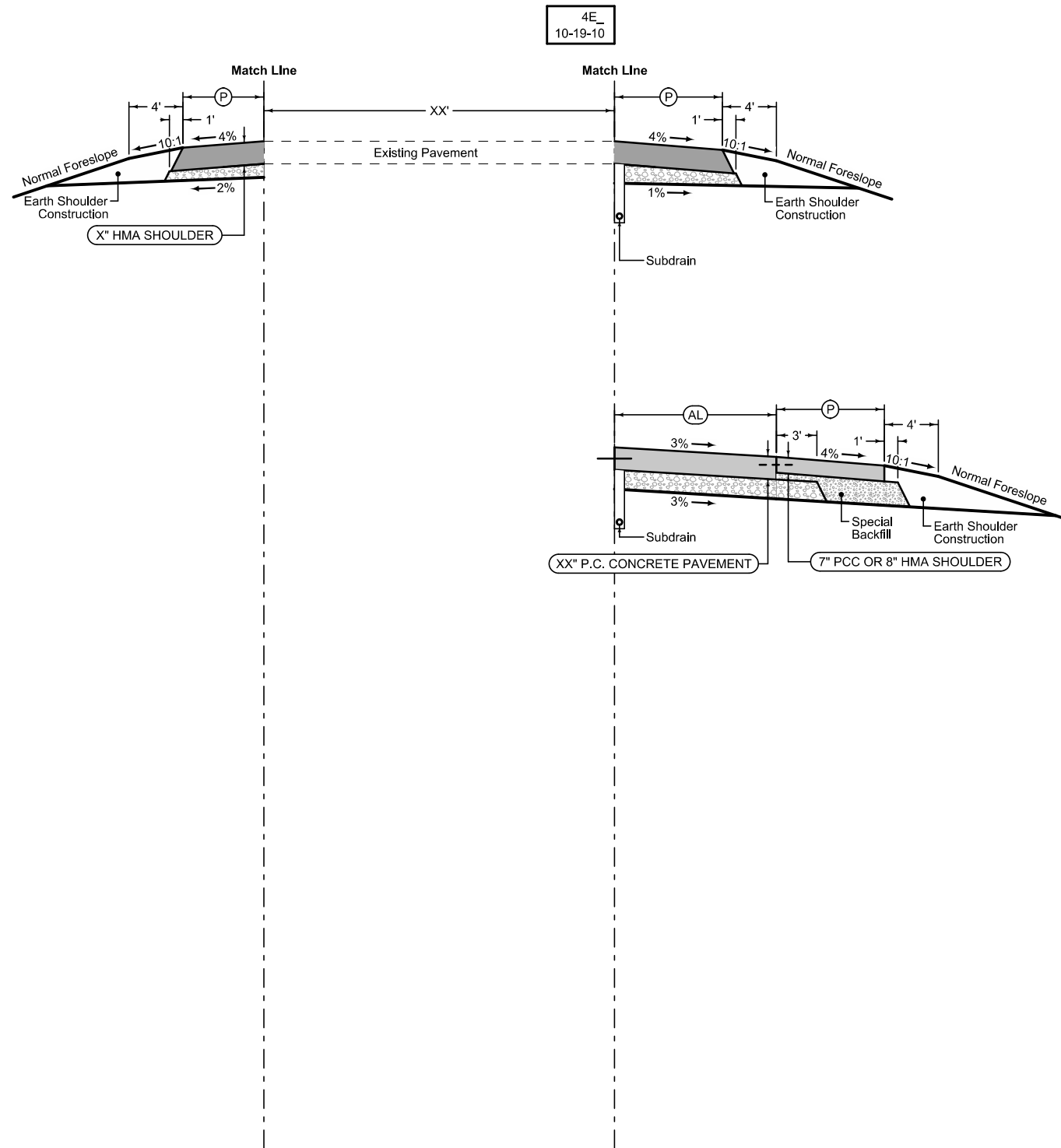
Subject to change by final design.

D5 PLAN - Date: 10/1/2016

### Full Depth HMA Shoulder

Shoulder Jointing:  
Longitudinal joint: B

4_P_FullHMA_10-19-10			
Direction of Travel	BEGIN STATION	END STATION	(P) Feet



### Full Depth HMA Shoulder

Shoulder Jointing:  
Longitudinal joint: B

4_P_FullHMA_10-19-10			
Direction of Travel	BEGIN STATION	END STATION	(P) Feet

### Auxiliary Lane

Longitudinal joint: L or KT  
Transverse joint: Match Mainline

4_AuxLane_PCC_10-19-10			
Direction of Travel	BEGIN STATION	END STATION	(AL) Feet

### Auxiliary Lane

#### Paved Shoulder Alternates

PCC Shoulder Jointing:  
Longitudinal joint: BT-1 or BT-5  
Transverse joints: C at 20' spacing  
HMA Shoulder Jointing:  
Longitudinal joint: B

4_AL_Shldr_ALT_10-21-14	
(P) Feet	

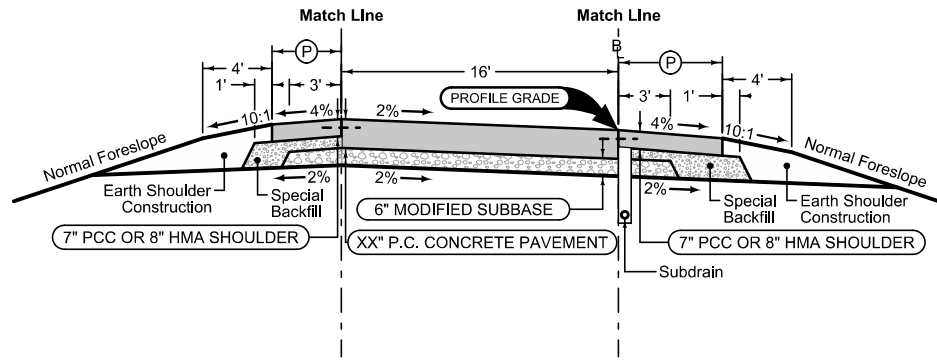
See Tab 100-24 or 100-25 for pavement quantities.  
See Tab 112-9 for shoulder quantities.

## I-35 SHOULDER & TAPER CONSTRUCTION

**Paved Shoulder Alternates**

PCC Shoulder Jointing:  
 Longitudinal joint: BT-1 or BT-5  
 Transverse joints: C at 20' spacing  
 HMA Shoulder Jointing:  
 Longitudinal joint: B

1R_P_ALT_10-21-14		
BEGIN STATION	END STATION	(P) Feet



Section shown in the direction of traffic.

Ramp Jointing:  
 Transverse joints: CD at 20' spacing.

1RP_10-19-10	
BEGIN STATION	END STATION

**Paved Shoulder Alternates**

PCC Shoulder Jointing:  
 Longitudinal joint: BT-1 or BT-5  
 Transverse joints: C at 20' spacing  
 HMA Shoulder Jointing:  
 Longitudinal joint: B

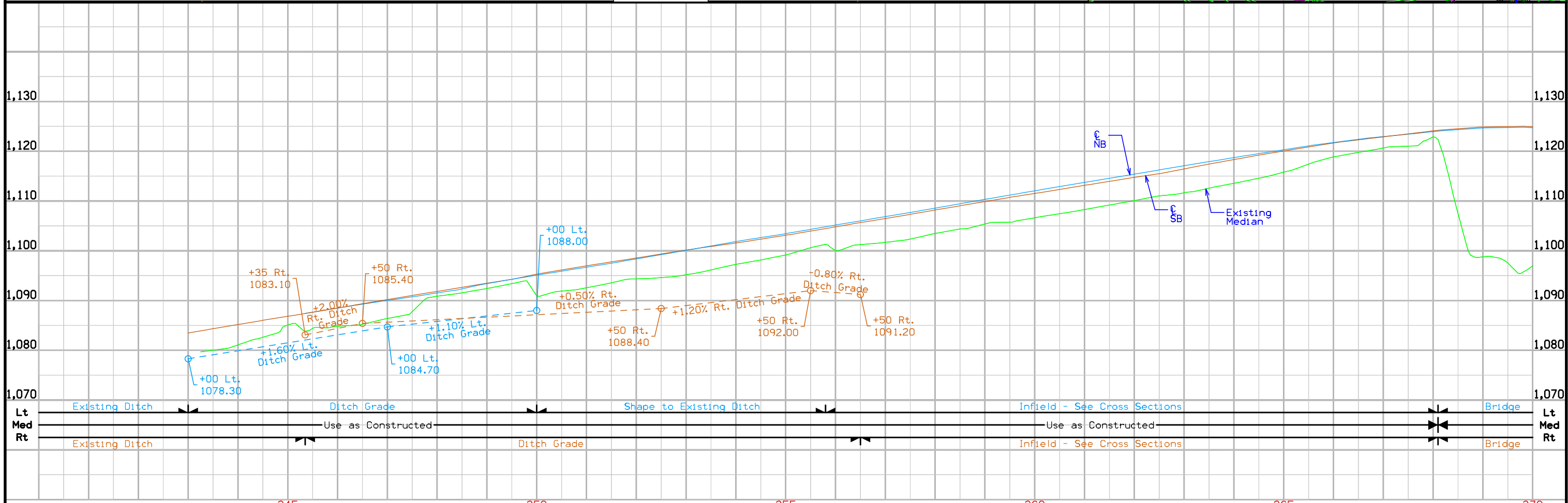
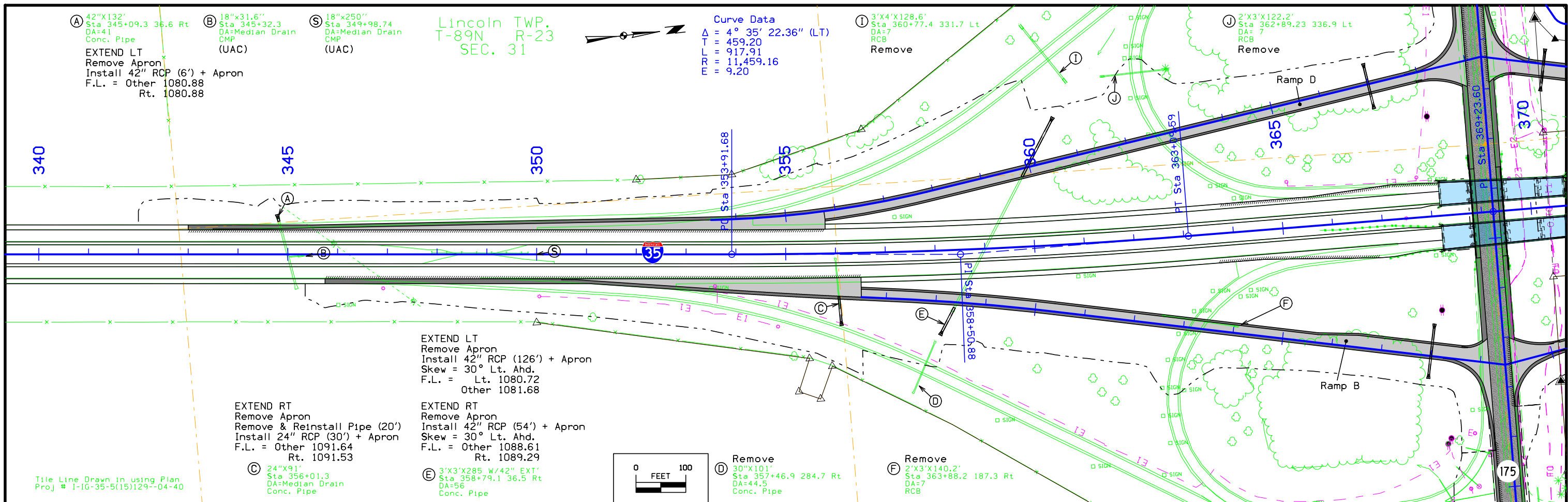
1R_P_ALT_10-21-14		
BEGIN STATION	END STATION	(P) Feet

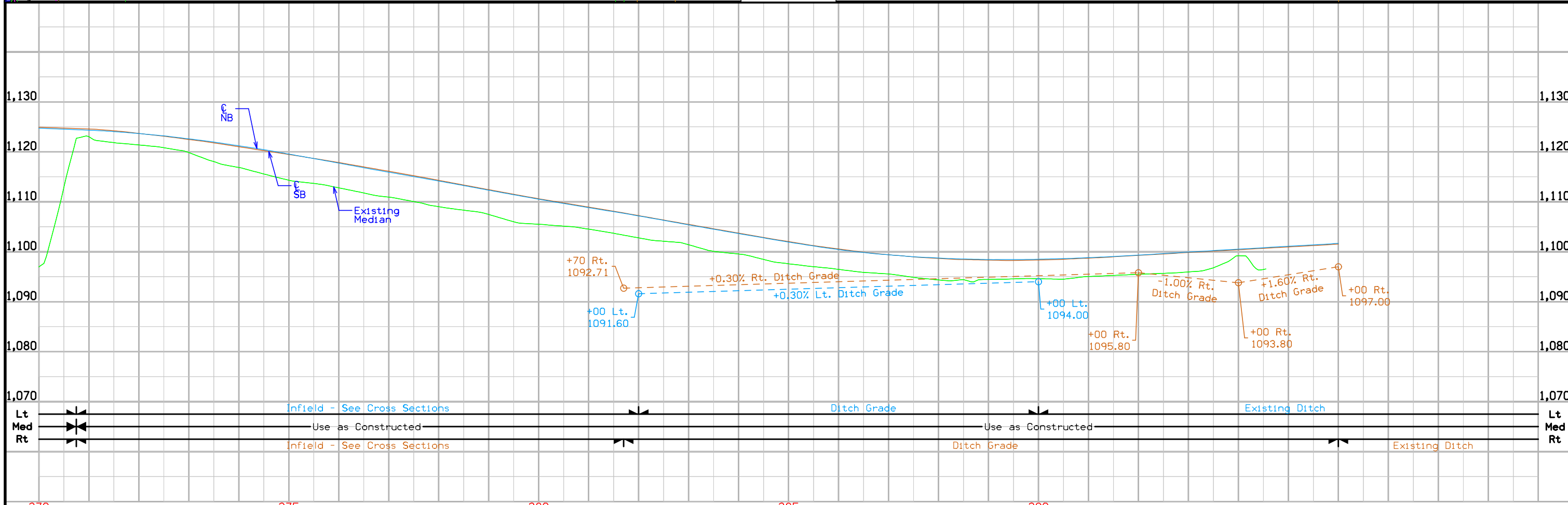
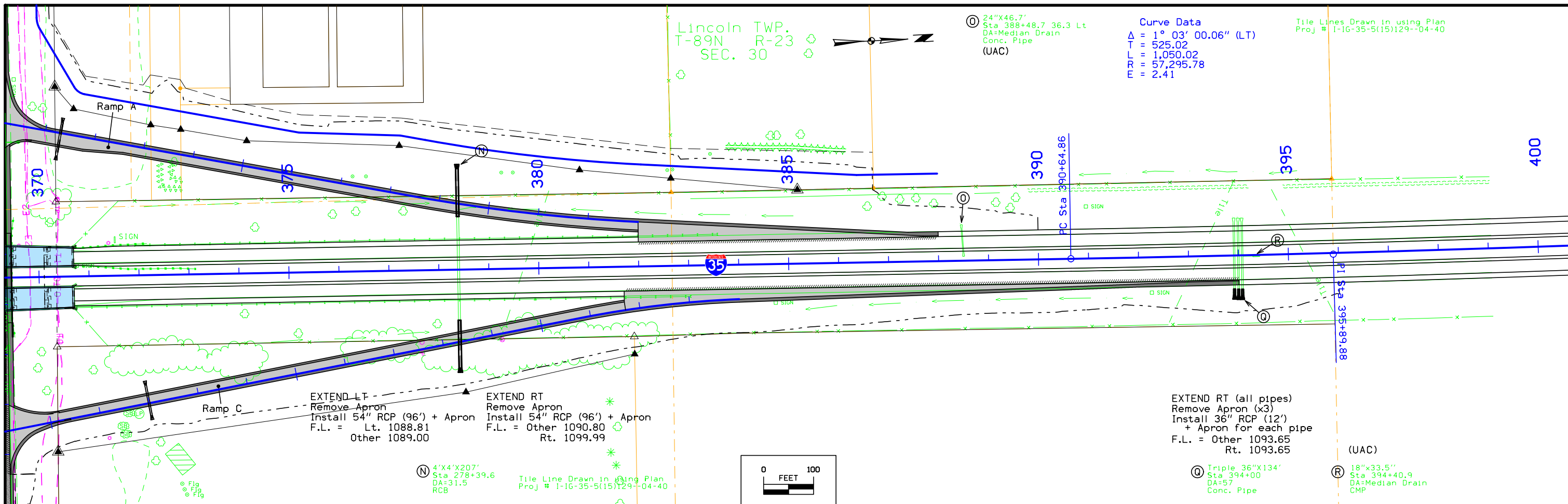
See Tab 100-24 or 100-25 for pavement quantities.  
 See Tab 112-9 for shoulder quantities.

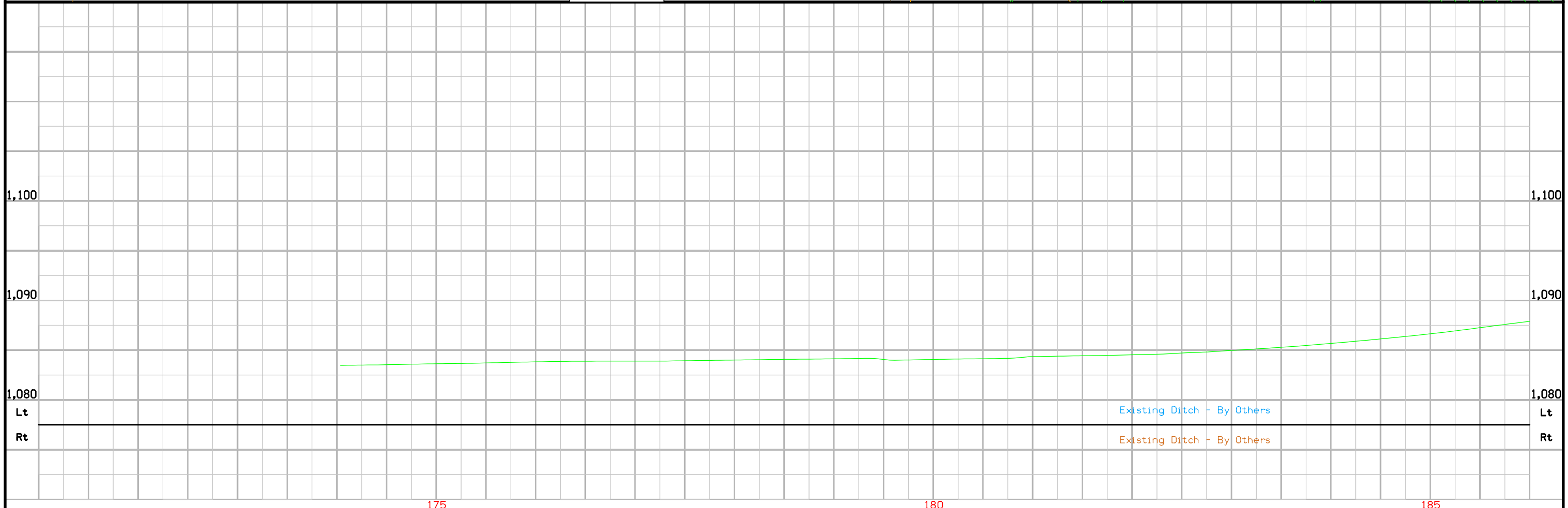
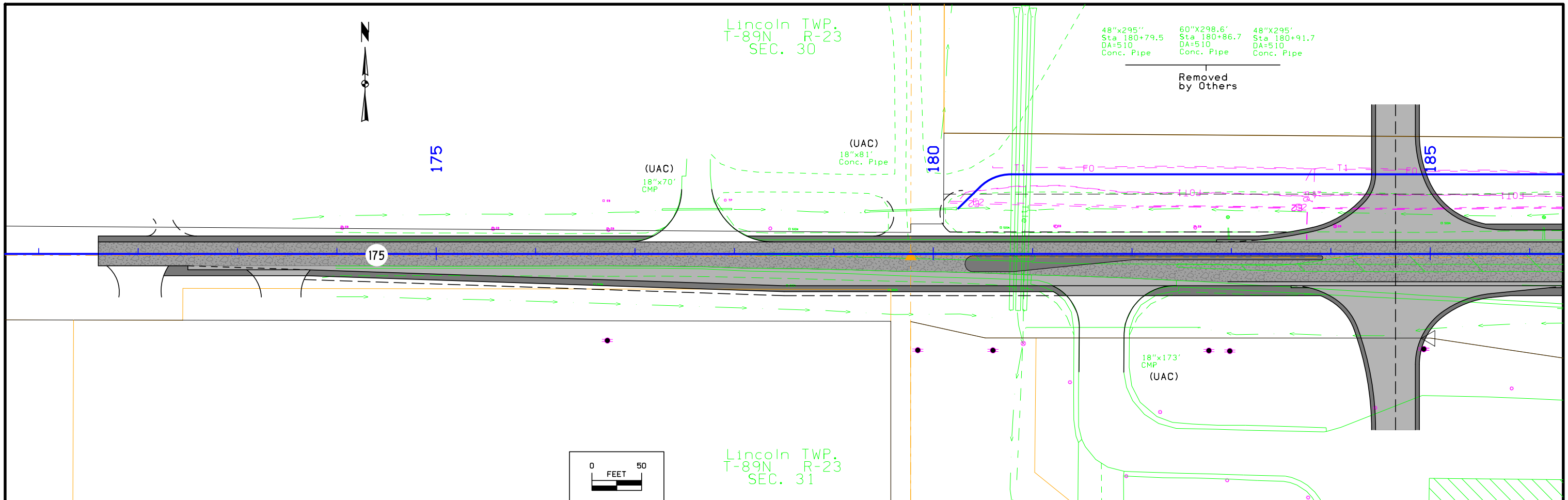
**RAMP CONSTRUCTION**

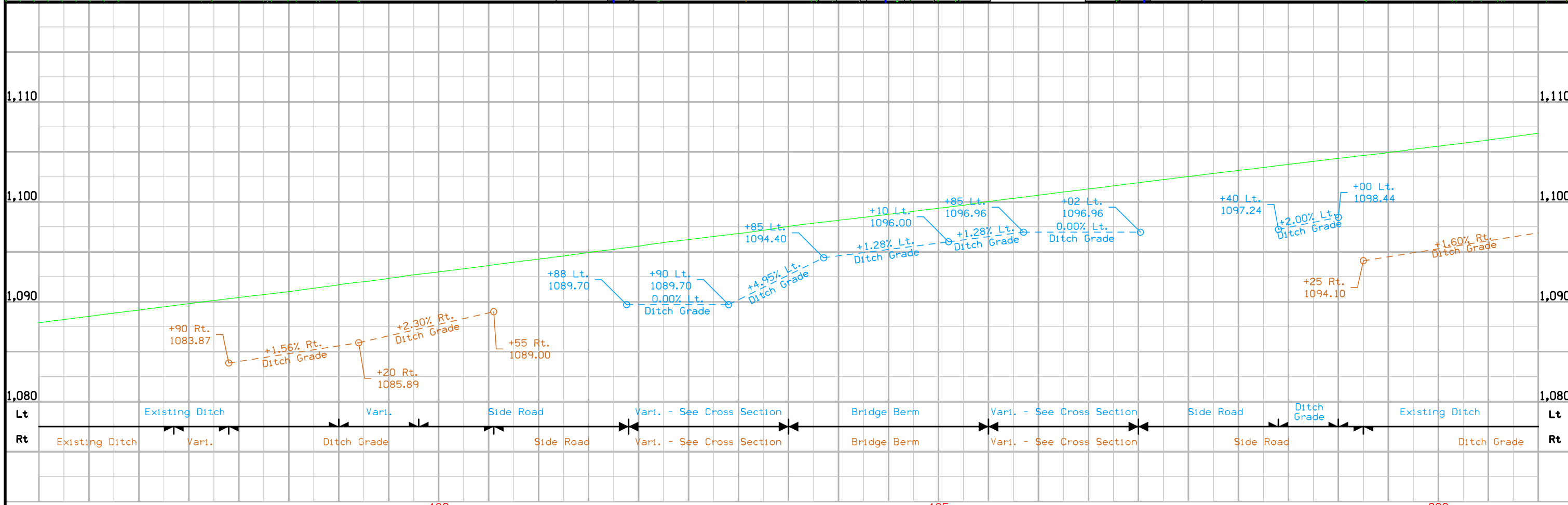
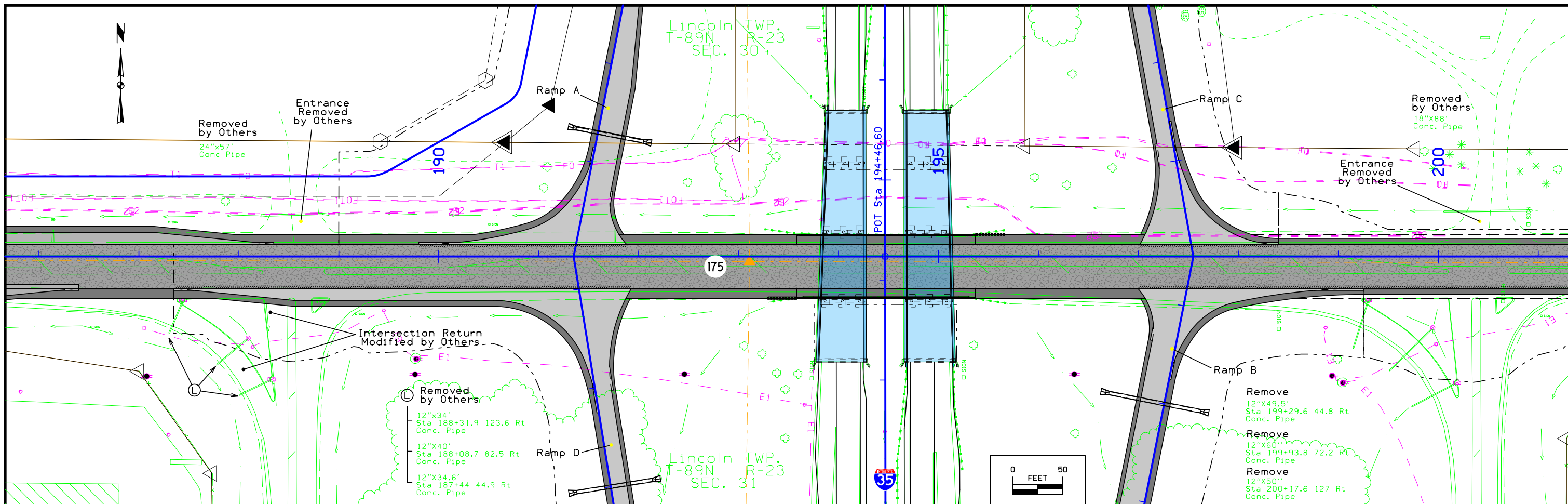


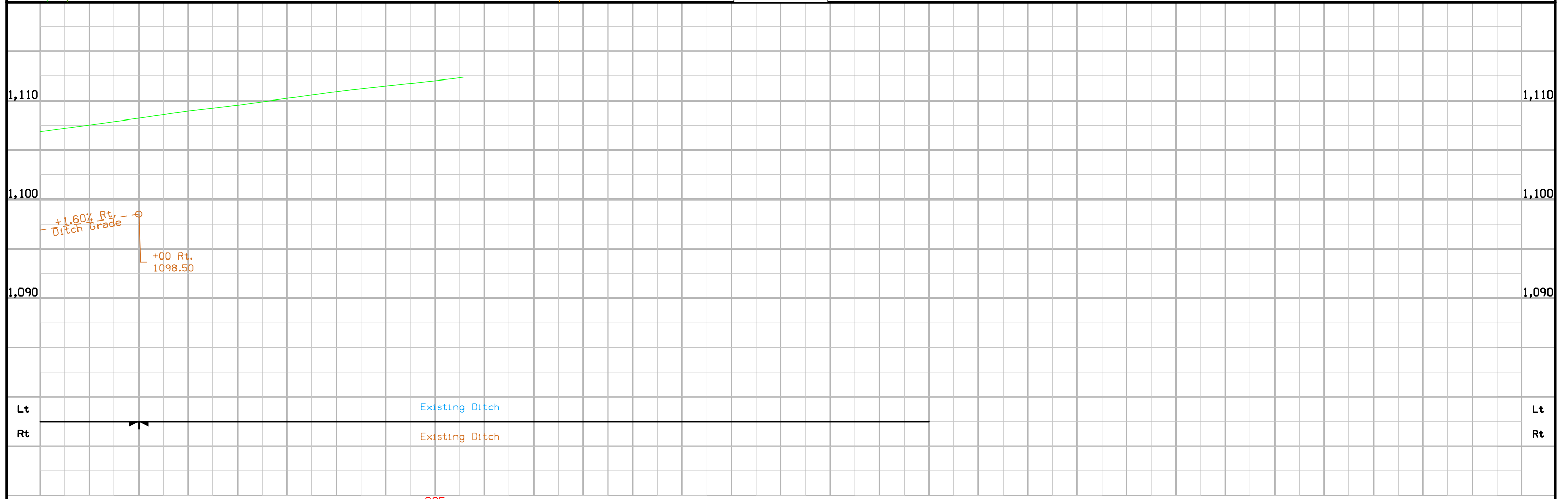
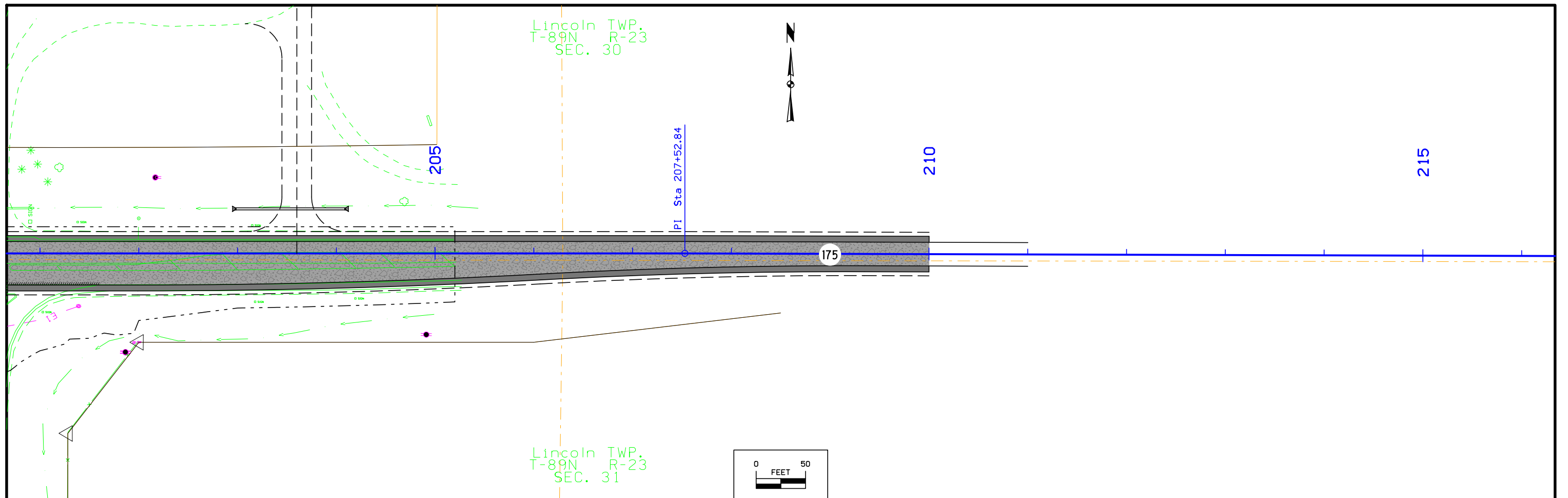












FILE NO.	ENGLISH	DESIGN TEAM	T. Gustafson/J. Vortherms	HAMILTON COUNTY	PROJECT NUMBER	IM-035-5(110)133--13-40	SHEET NUMBER	E.3
----------	---------	-------------	---------------------------	-----------------	----------------	-------------------------	--------------	-----

## Survey Information

Hamilton County  
 IM-035-5(110)133--13-40  
 I-35 and IA 175 Interchange  
 PIN 15-40-035-020  
 Sap-0858

### General Information

Measurement units for this survey are US survey feet. This survey is for proposed interchange reconstruction at Iowa Highway 175 and Interstate 35. Project datum and control information is provided by Design Survey Office. This project is a Partial DTM with Photo control.

### Vertical Control

Vertical control was established by running a 6 hour static observation on FENO Monument No. 1. and processing the static observation using OPUS solution.

This survey also observed 1 Hamilton County Control Monument with published NAVD88 height to compare with observed survey height: Mark 47 is located 1.1 miles SW of the project.

Palo Alto County Control mark GPS 47 published height = 1068.82  
 RTK NAVD88 height computed using Geoid 12A = 1068.77

### Horizontal Control

The project coordinate system is IARCS Zone 4 (U.S. Survey Feet). Project coordinates were established by running a 6 hour static observation on FENO No.1 and processing the static observation using OPUS solution.

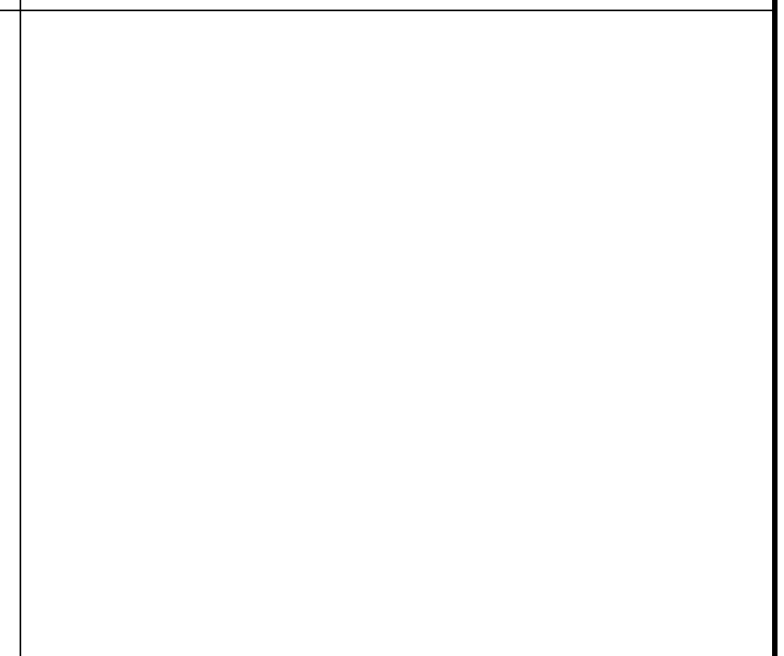
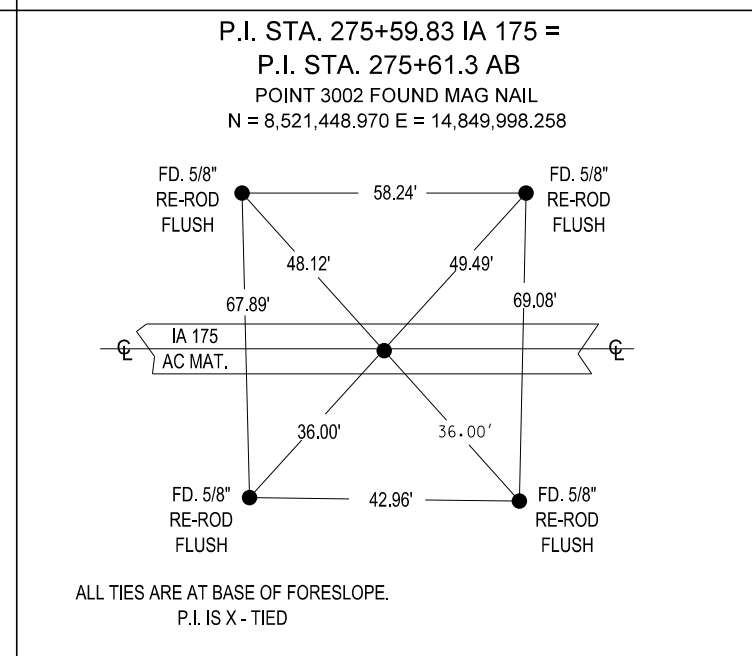
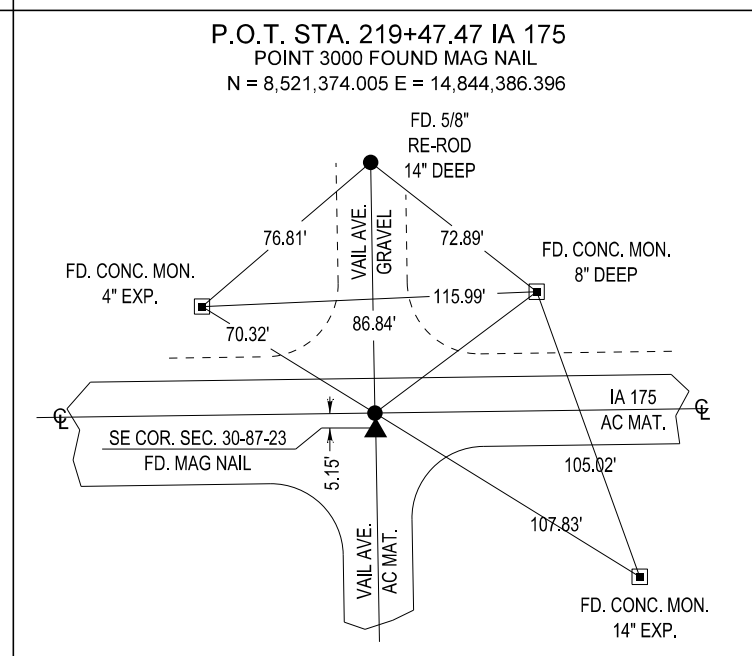
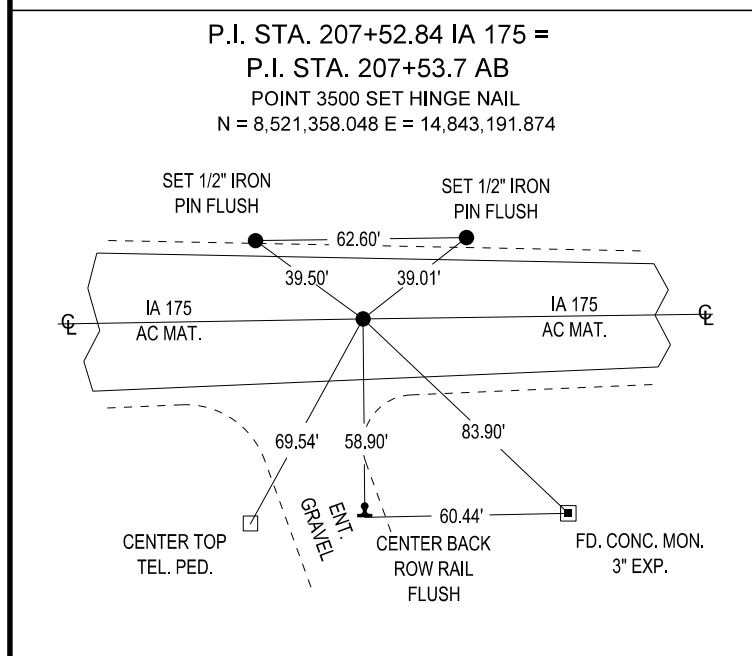
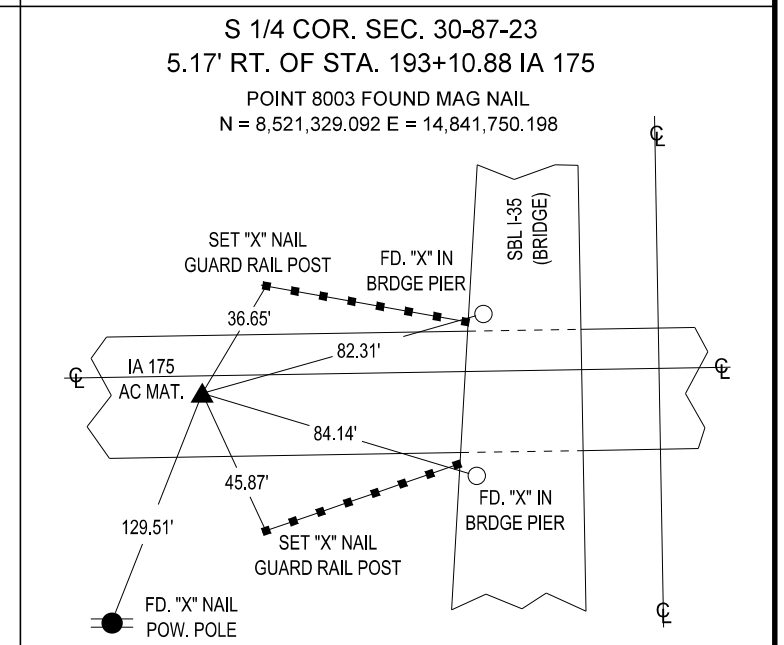
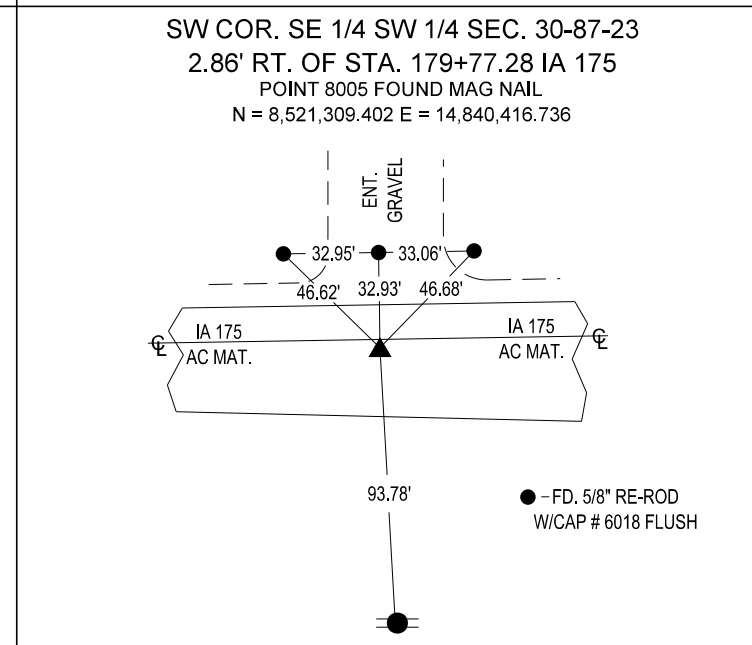
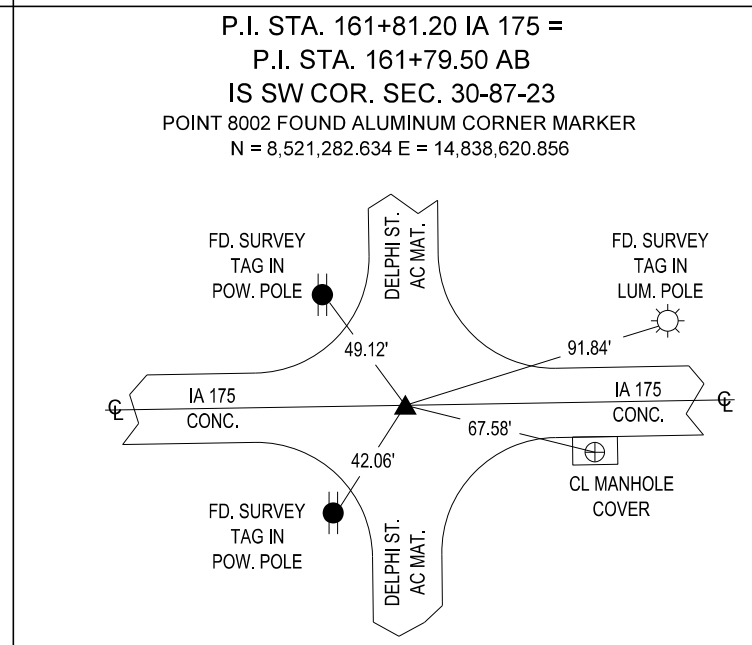
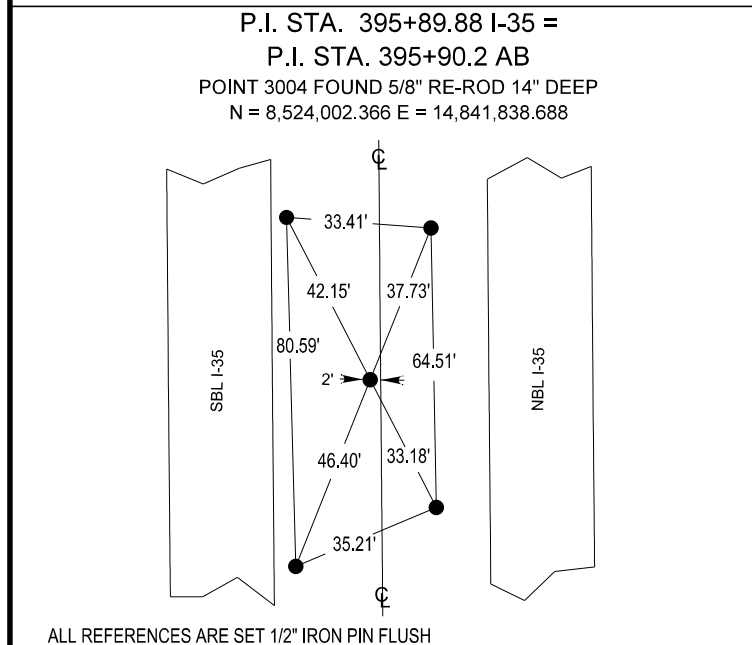
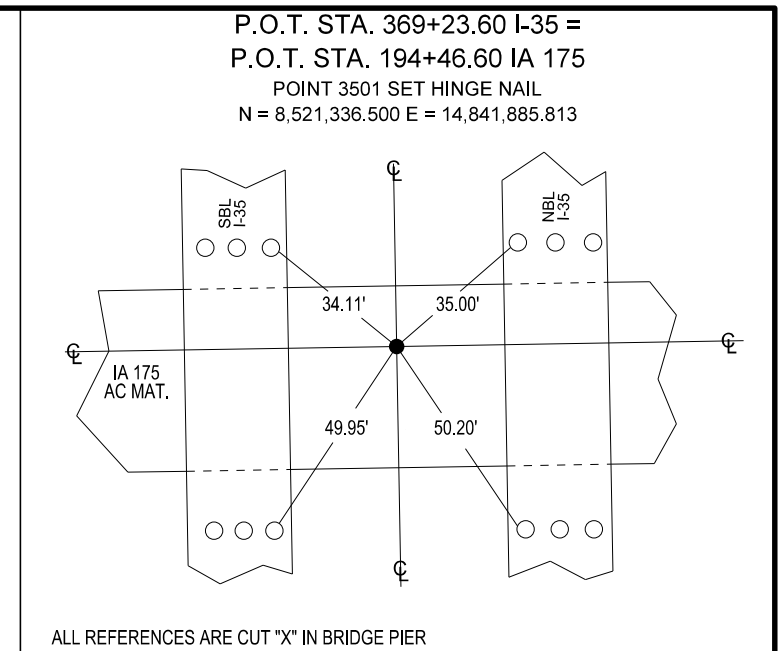
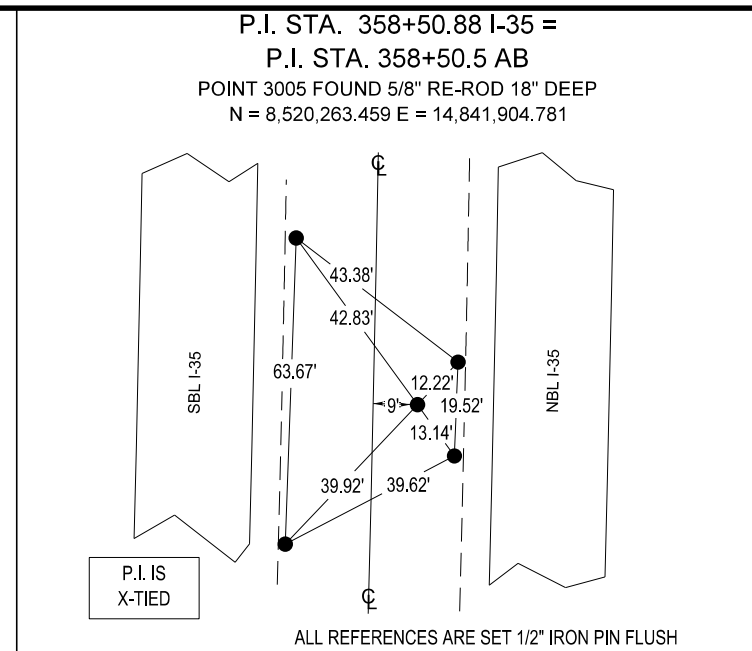
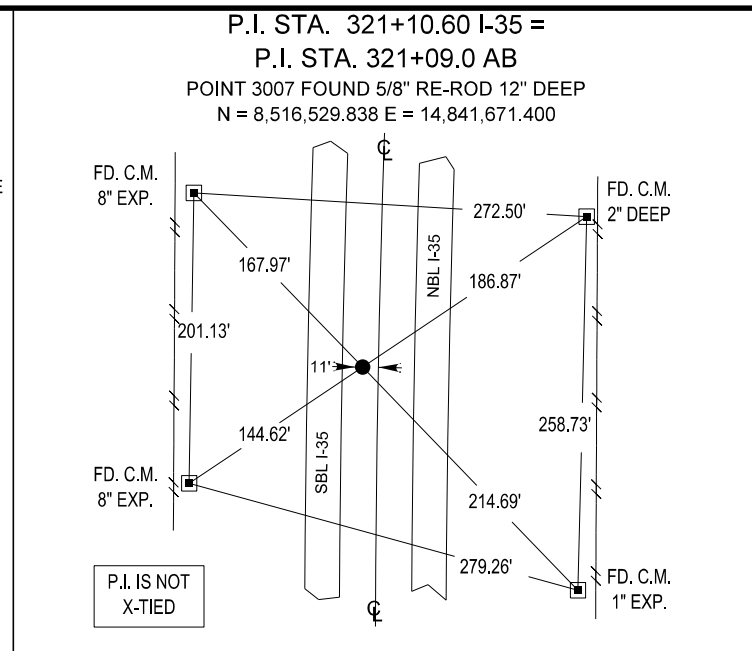
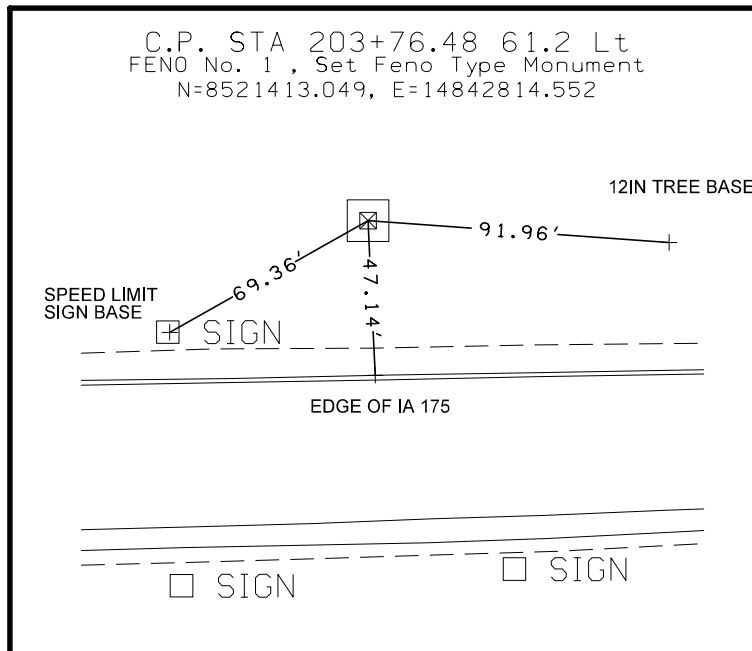
### Alignment Information

Alignment was provided by District 1 ROW office.

## VERTICAL CONTROL

Point	North	East	Elevation	Station	Offset	Feature	Description
1	8521413.049	14842814.552	1102.797	269+86.48	-61.2	FENO	FENO TYPE MONUMENT
47	8515975.415	14838644.702	1068.768	OFF CHAIN	OFF CHAIN	CP	CP COUNTY GPS
500	8521232.184	14841958.730	1123.936	368+18.01	71.062	BM	DOT BUTTON SE CORNER NORTH BOUND BRIDGE
501	8521481.910	14841823.460	1124.642	370+70.09	-59.773	BM	DOT BUTTON NW CORNER SOUTH BOUND BRIDGE

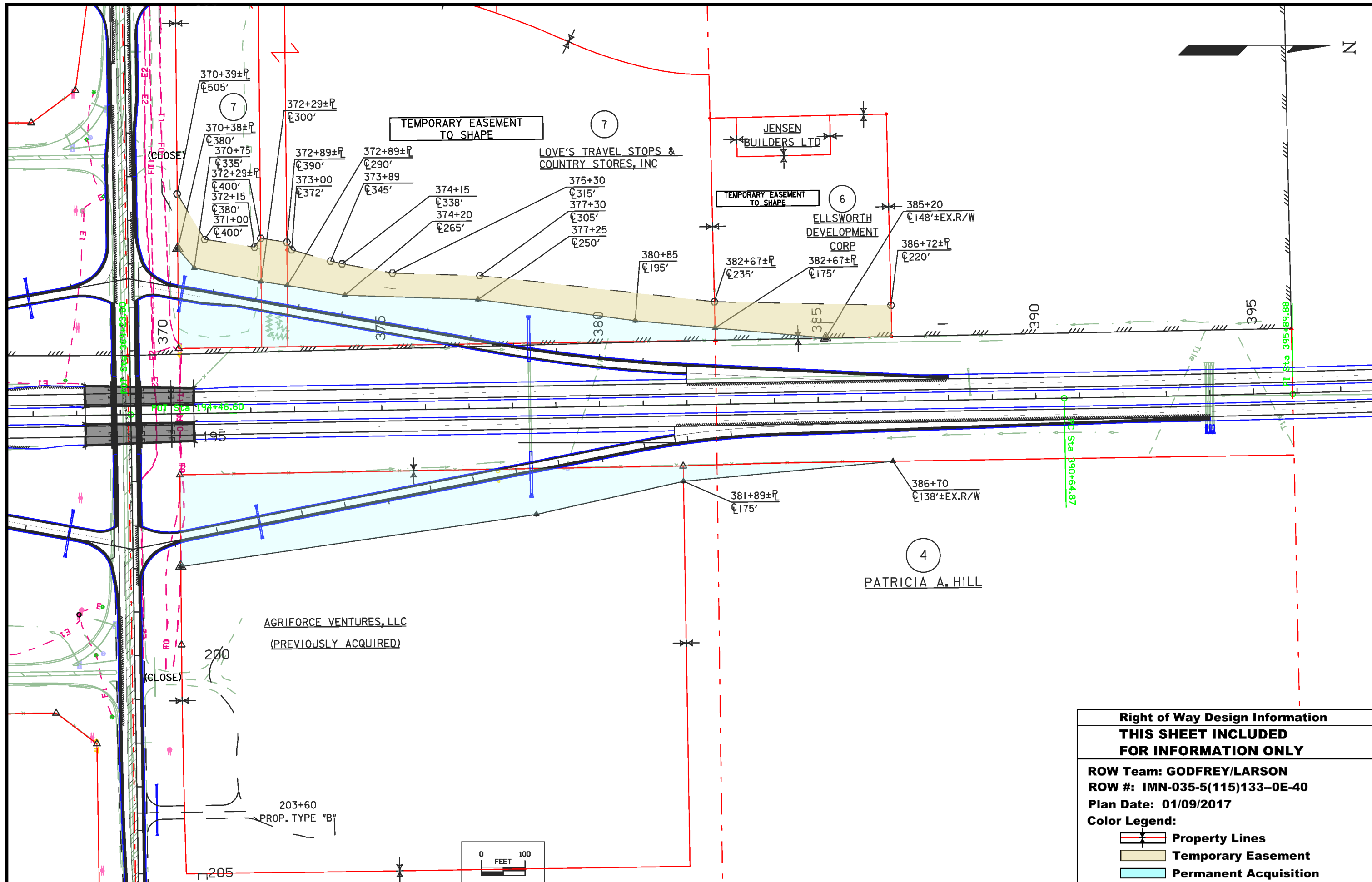




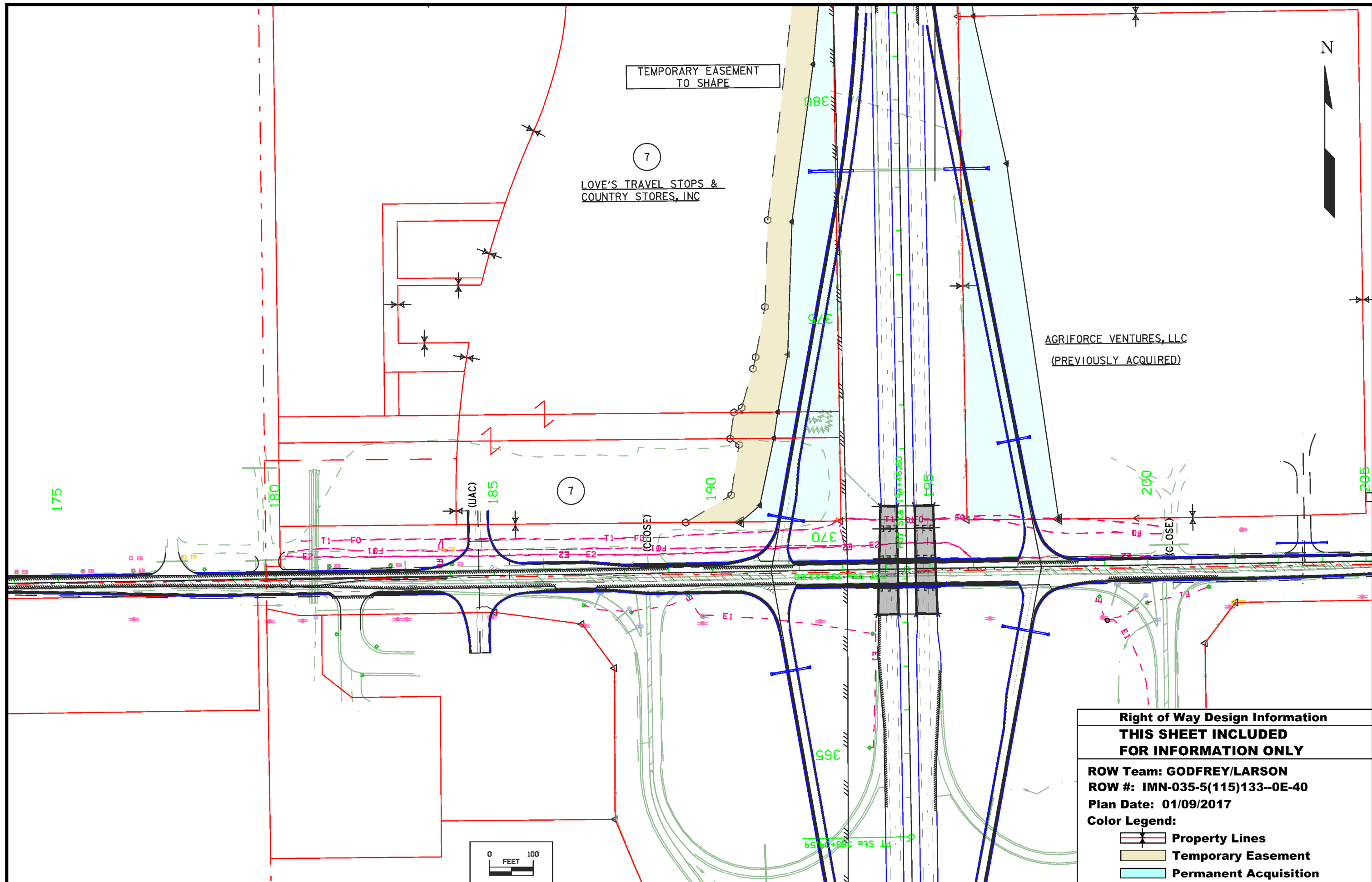
NO ACCESS RIGHTS ARE TO BE ACQUIRED ON THIS PROJECT.

ACCESS CONTROL PREVIOUSLY ACQUIRED





<b>Right of Way Design Information</b>	
<b>THIS SHEET INCLUDED FOR INFORMATION ONLY</b>	
ROW Team: GODFREY/LARSON	
ROW #: IMN-035-5(115)133--0E-40	
Plan Date: 01/09/2017	
Color Legend:	
	Property Lines
	Temporary Easement
	Permanent Acquisition



TEMPORARY EASEMENT  
TO SHAPE

7  
LOVE'S TRAVEL STOPS &  
COUNTRY STORES, INC

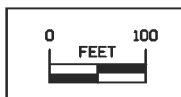
AGRIFORCE VENTURES, LLC  
(PREVIOUSLY ACQUIRED)

**Right of Way Design Information**  
**THIS SHEET INCLUDED**  
**FOR INFORMATION ONLY**

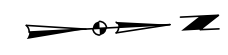
ROW Team: GODFREY/LARSON  
 ROW #: IMN-035-5(115)133--0E-40  
 Plan Date: 01/09/2017

**Color Legend:**

- Property Lines
- Temporary Easement
- Permanent Acquisition

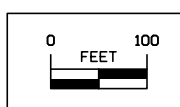
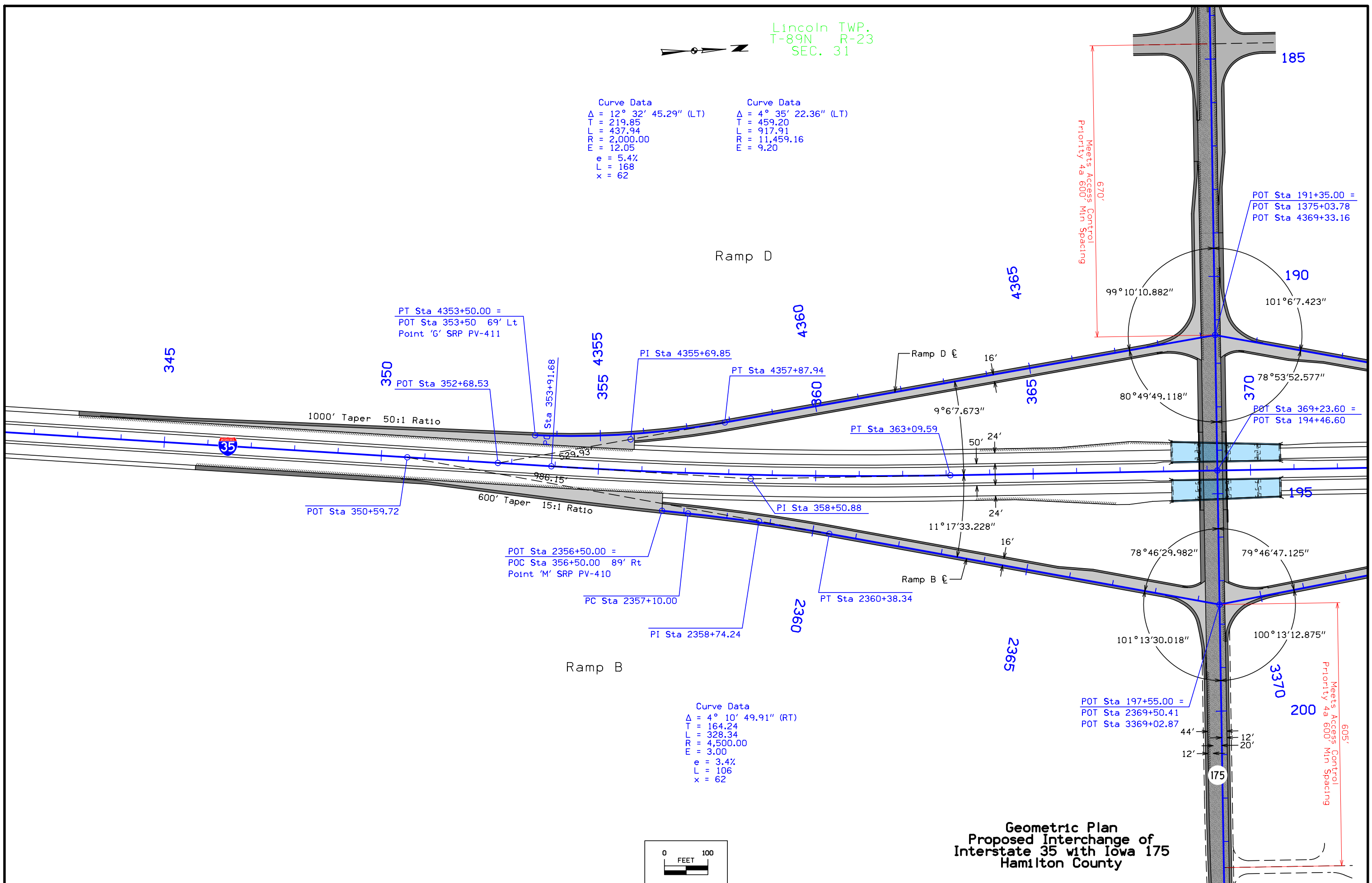


Lincoln TWP.  
T-89N R-23  
SEC. 31



Curve Data  
 $\Delta = 12^\circ 32' 45.29''$  (LT)  
 $T = 219.85$   
 $L = 437.94$   
 $R = 2,000.00$   
 $E = 12.05$   
 $e = 5.4\%$   
 $L = 168$   
 $x = 62$

Curve Data  
 $\Delta = 4^\circ 35' 22.36''$  (LT)  
 $T = 459.20$   
 $L = 917.91$   
 $R = 11,459.16$   
 $E = 9.20$



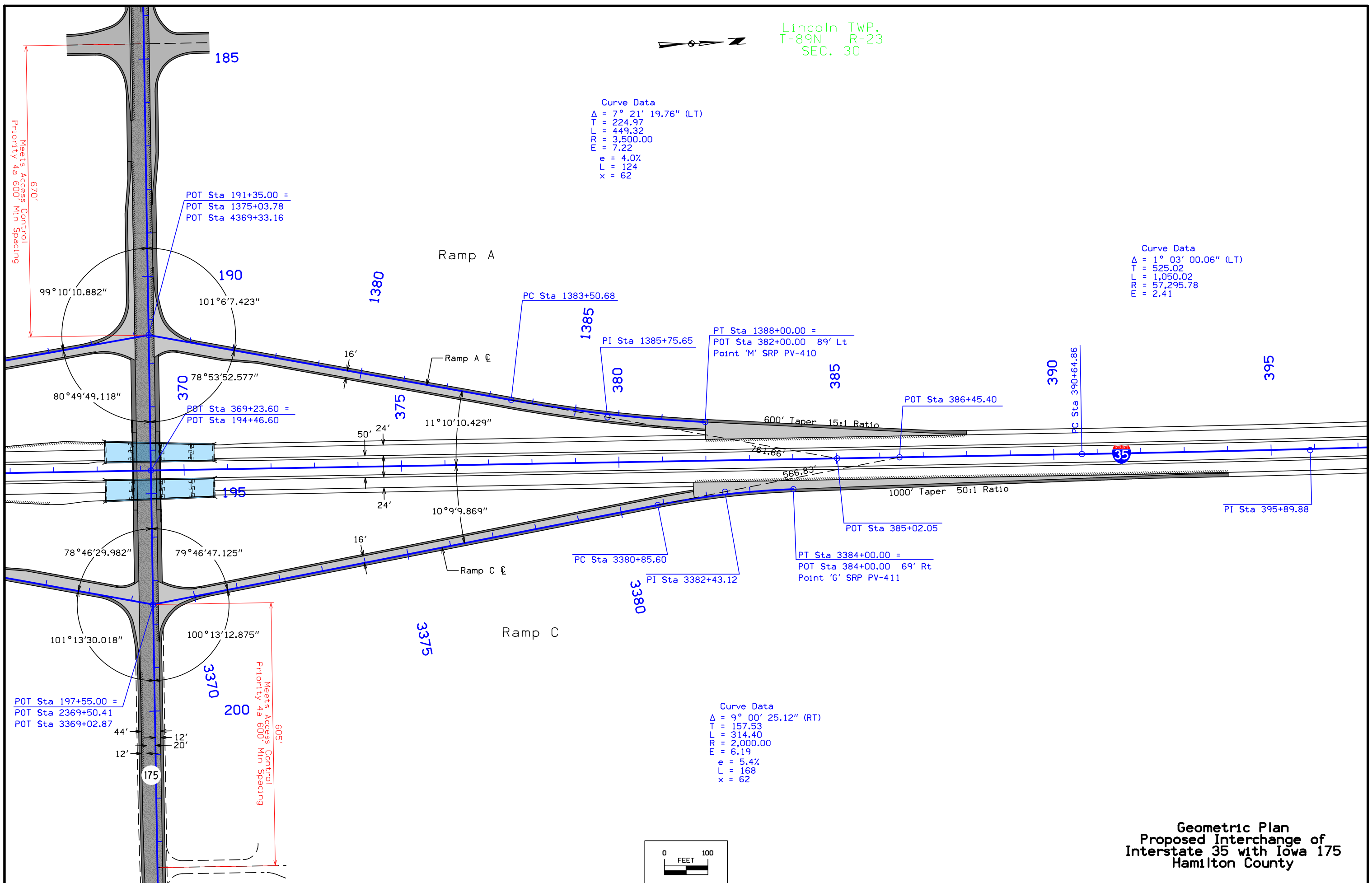
**Geometric Plan  
Proposed Interchange of  
Interstate 35 with Iowa 175  
Hamilton County**

Lincoln TWP.  
T-89N R-23  
SEC. 30

Curve Data  
 $\Delta = 7^\circ 21' 19.76''$  (LT)  
 T = 224.97  
 L = 449.32  
 R = 3,500.00  
 E = 7.22  
 e = 4.0%  
 L = 124  
 x = 62

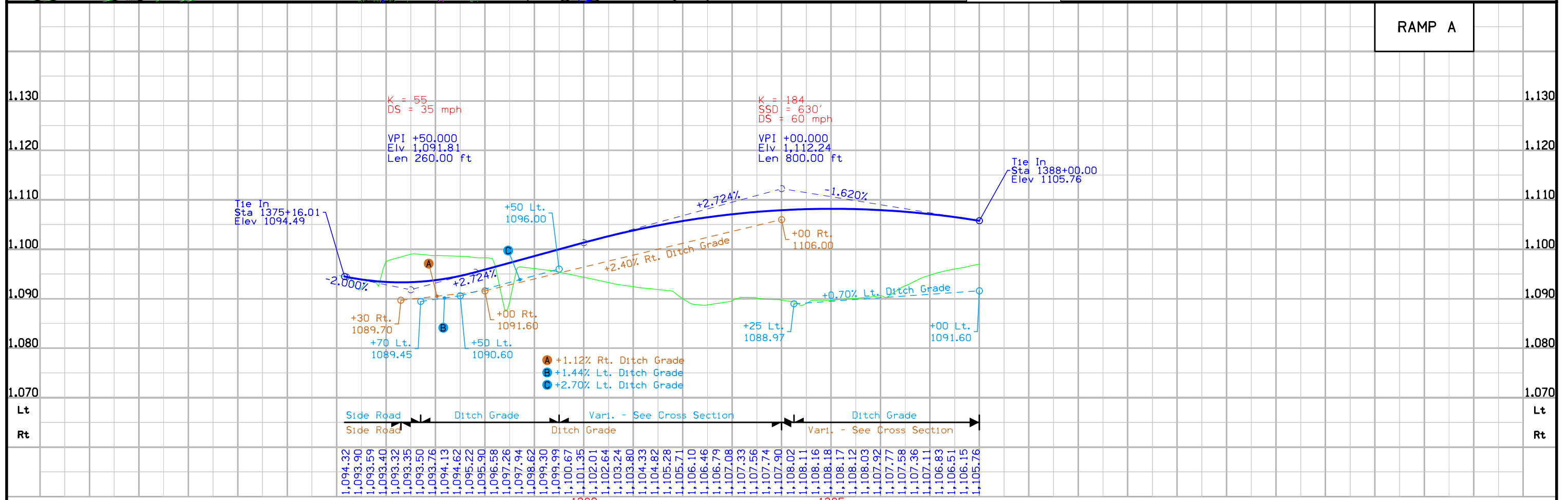
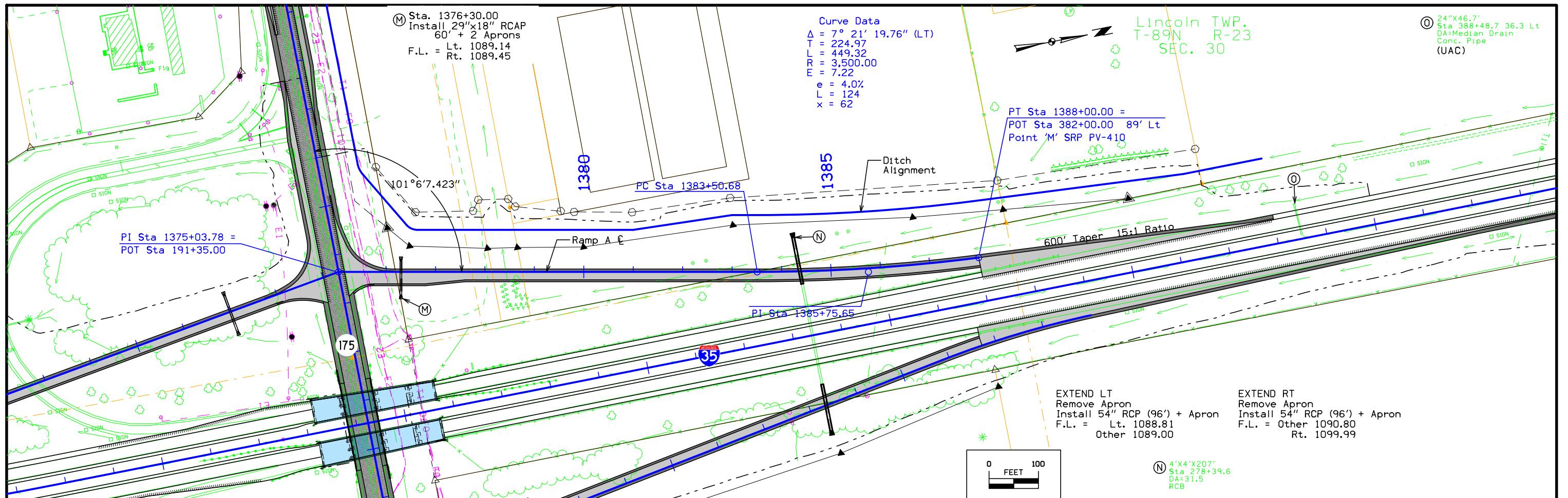
Curve Data  
 $\Delta = 1^\circ 03' 00.06''$  (LT)  
 T = 525.02  
 L = 1,050.02  
 R = 57,295.78  
 E = 2.41

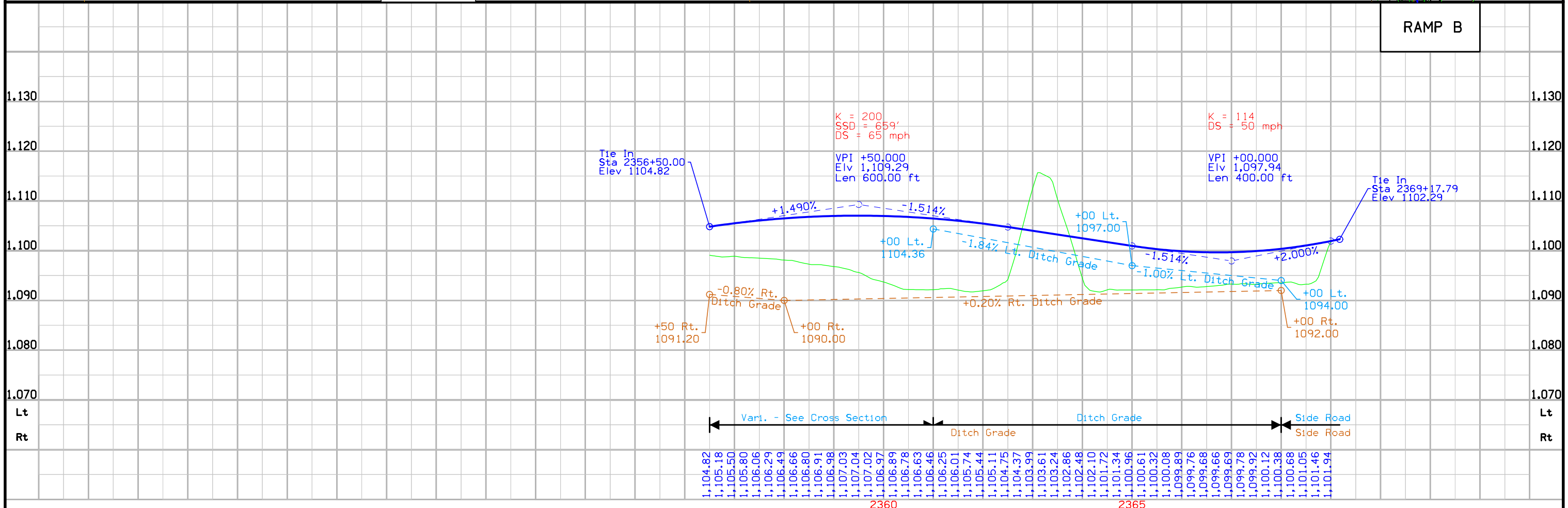
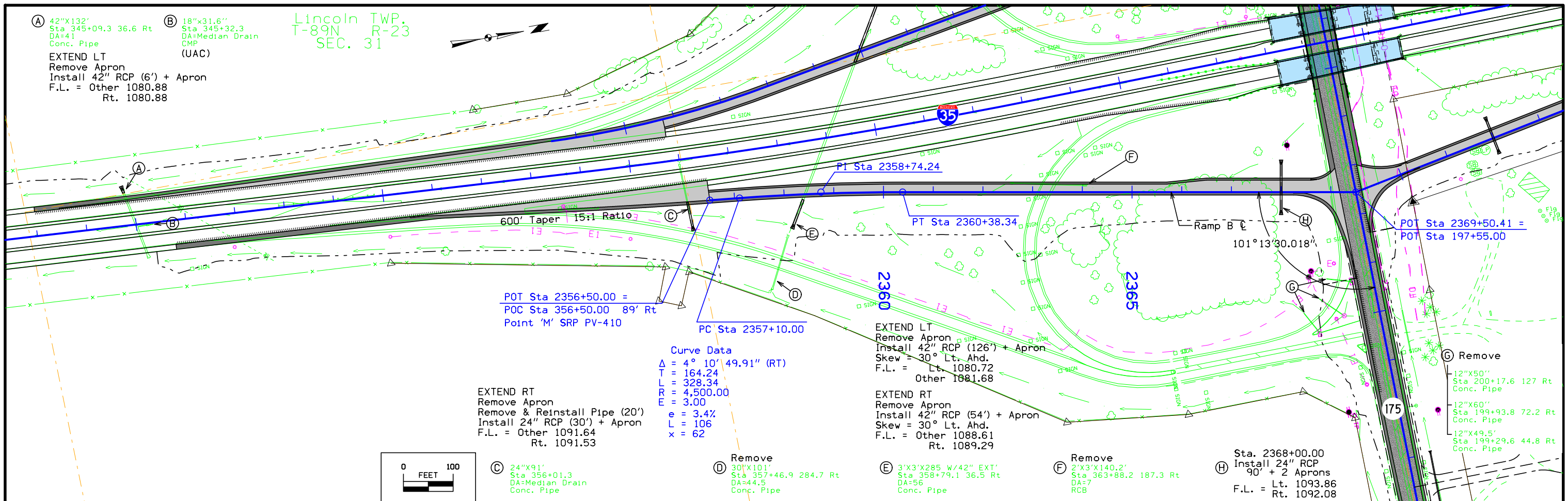
Curve Data  
 $\Delta = 9^\circ 00' 25.12''$  (RT)  
 T = 157.53  
 L = 314.40  
 R = 2,000.00  
 E = 6.19  
 e = 5.4%  
 L = 168  
 x = 62

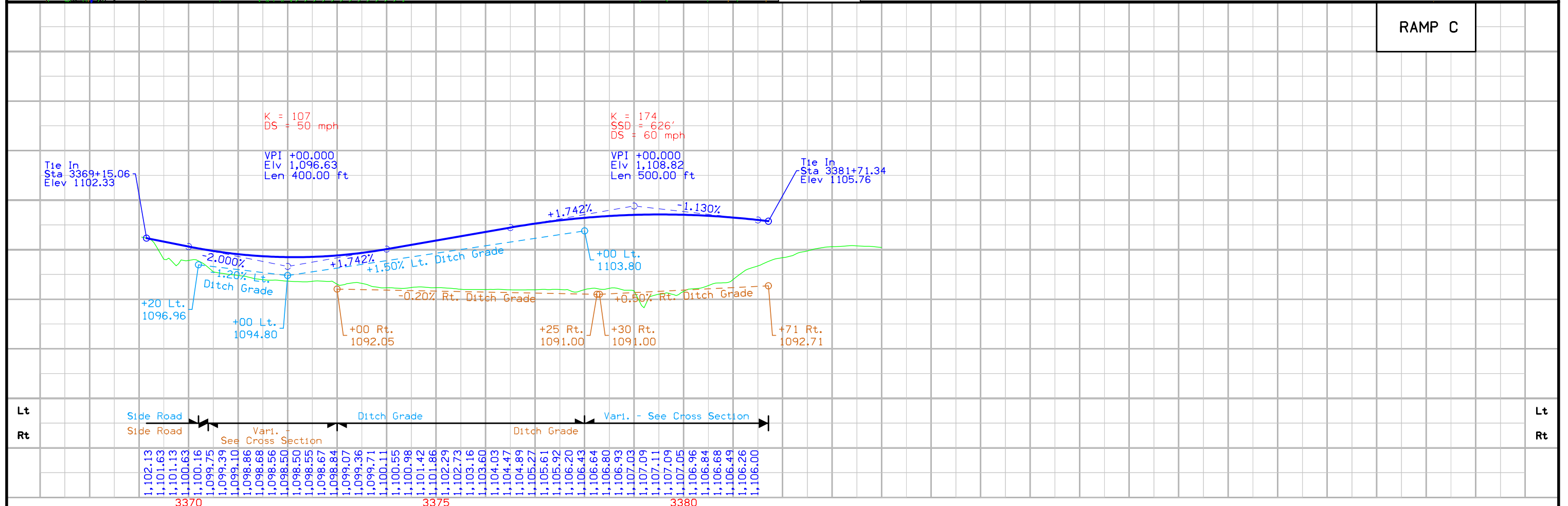
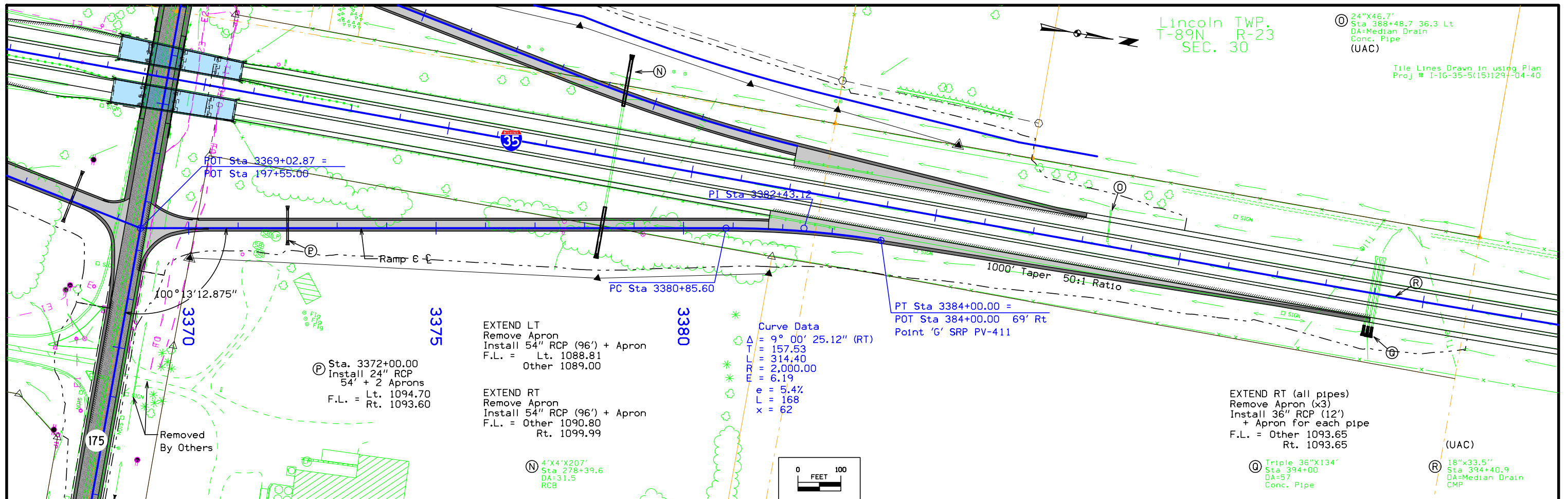


Geometric Plan  
 Proposed Interchange of  
 Interstate 35 with Iowa 175  
 Hamilton County









FILE NO.	ENGLISH	DESIGN TEAM	T. Gustafson/J. Vortherms	HAMILTON COUNTY	PROJECT NUMBER	IM-035-5(110)133--13-40	SHEET NUMBER	K.5
----------	---------	-------------	---------------------------	-----------------	----------------	-------------------------	--------------	-----



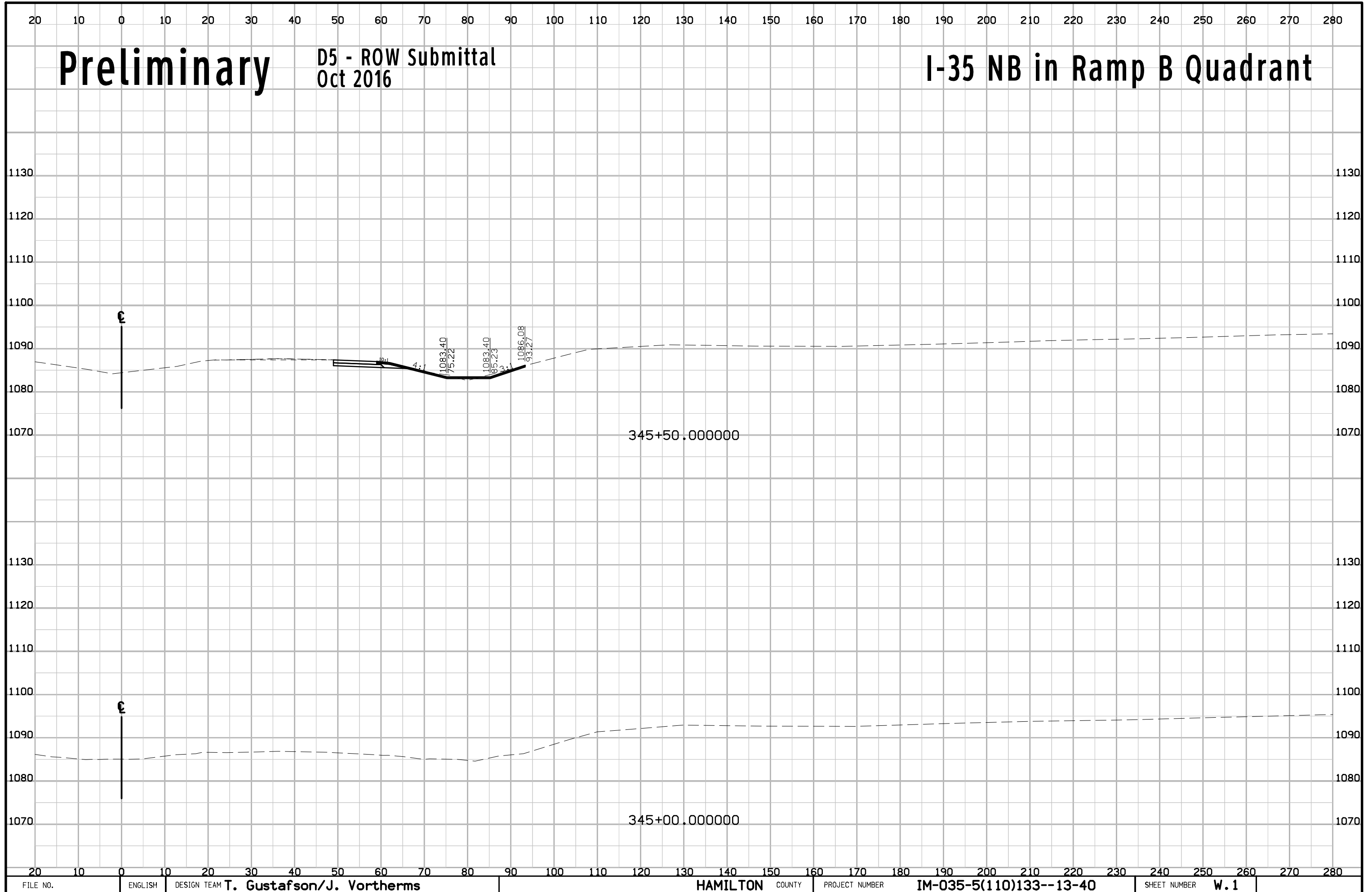




# Preliminary

D5 - ROW Submittal  
Oct 2016

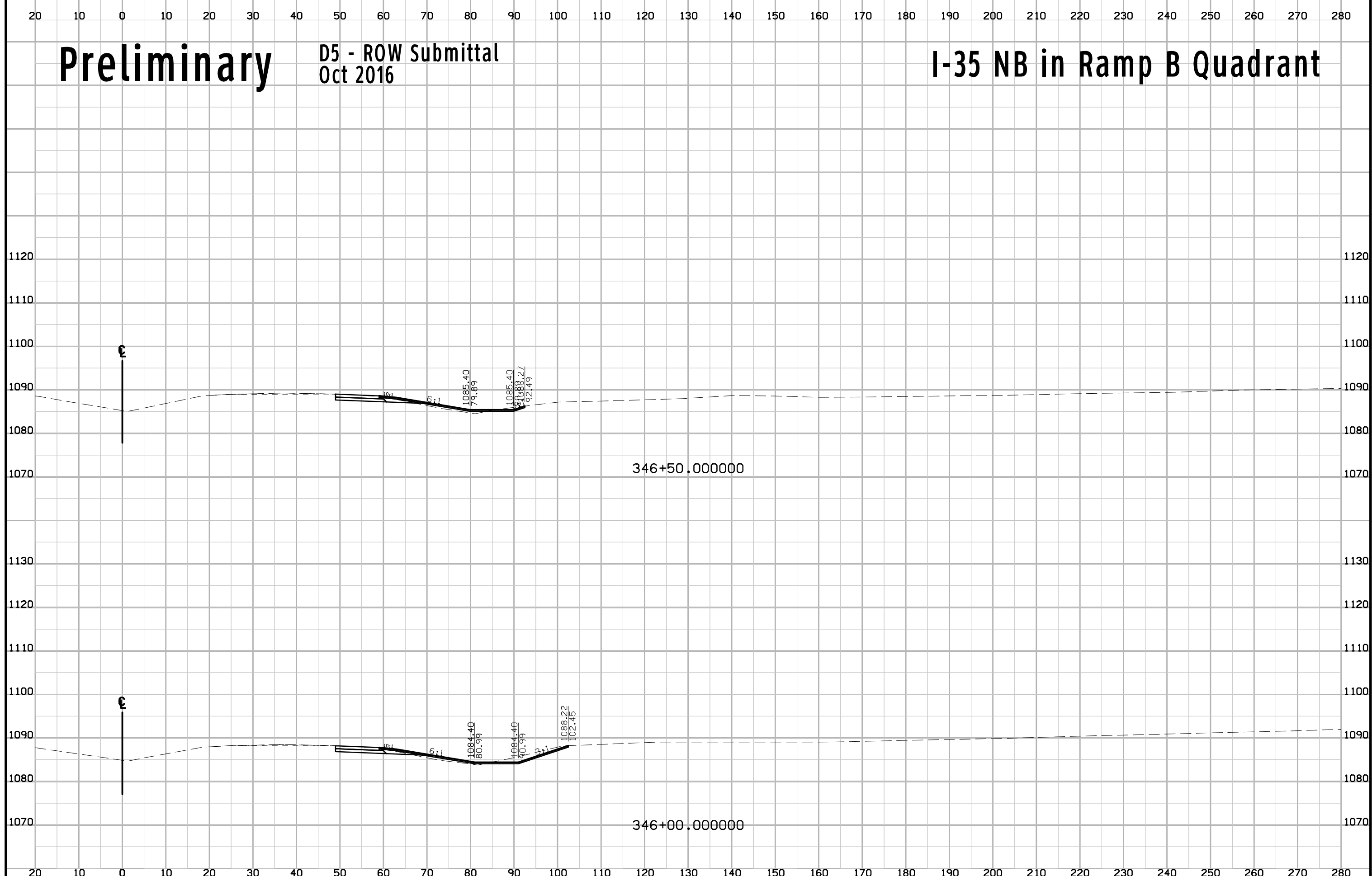
## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

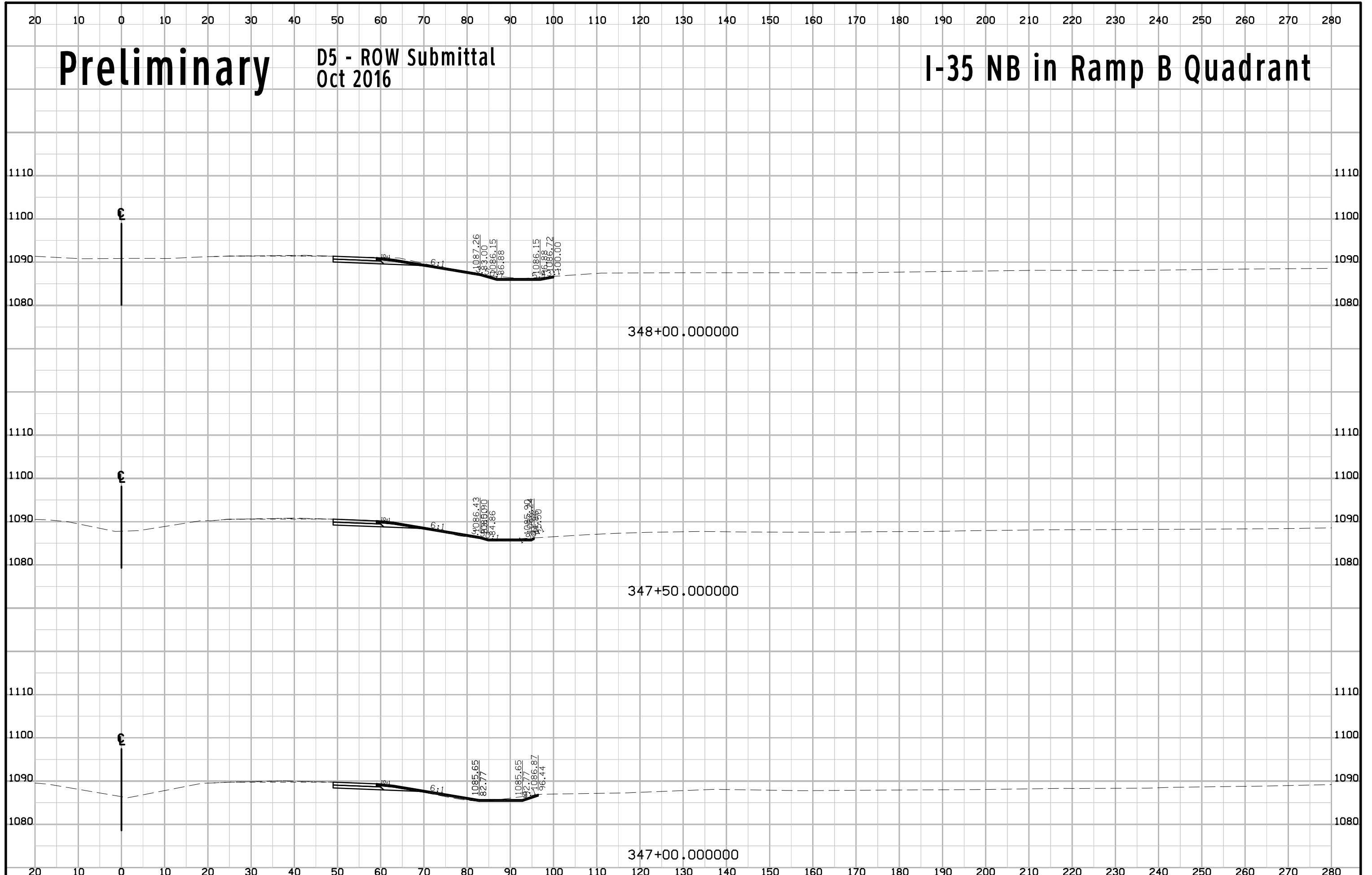
## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

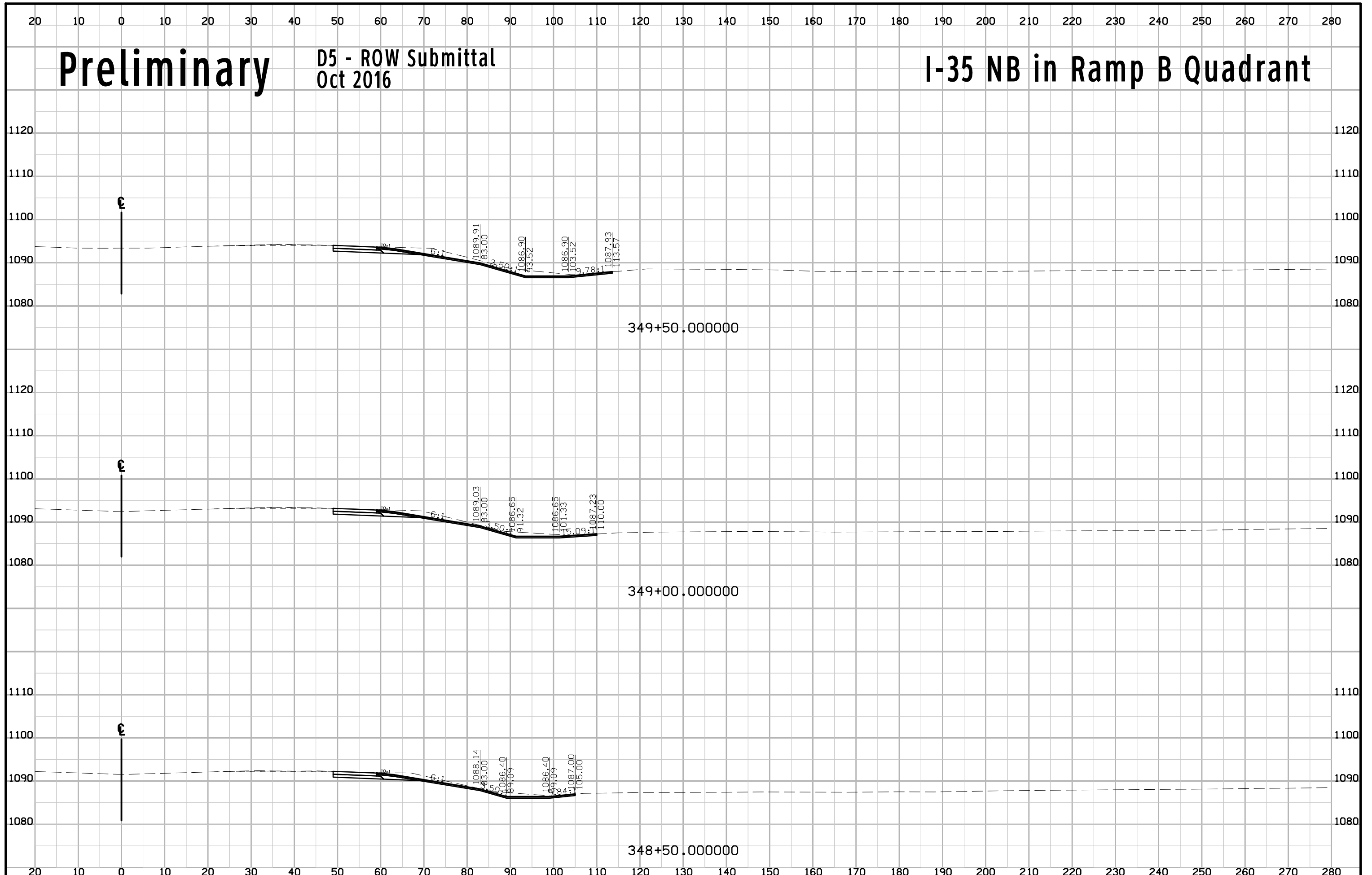
## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

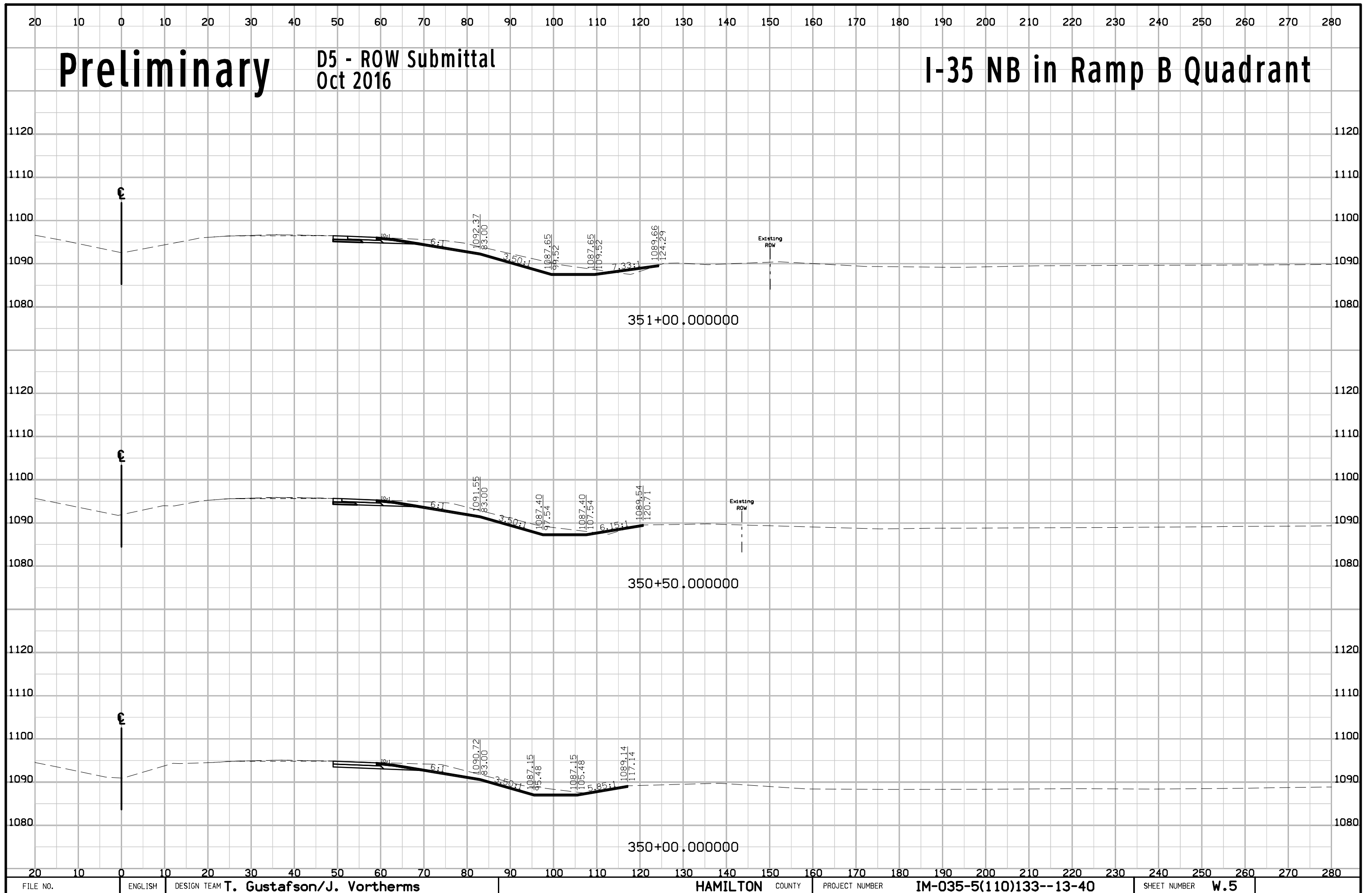
## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

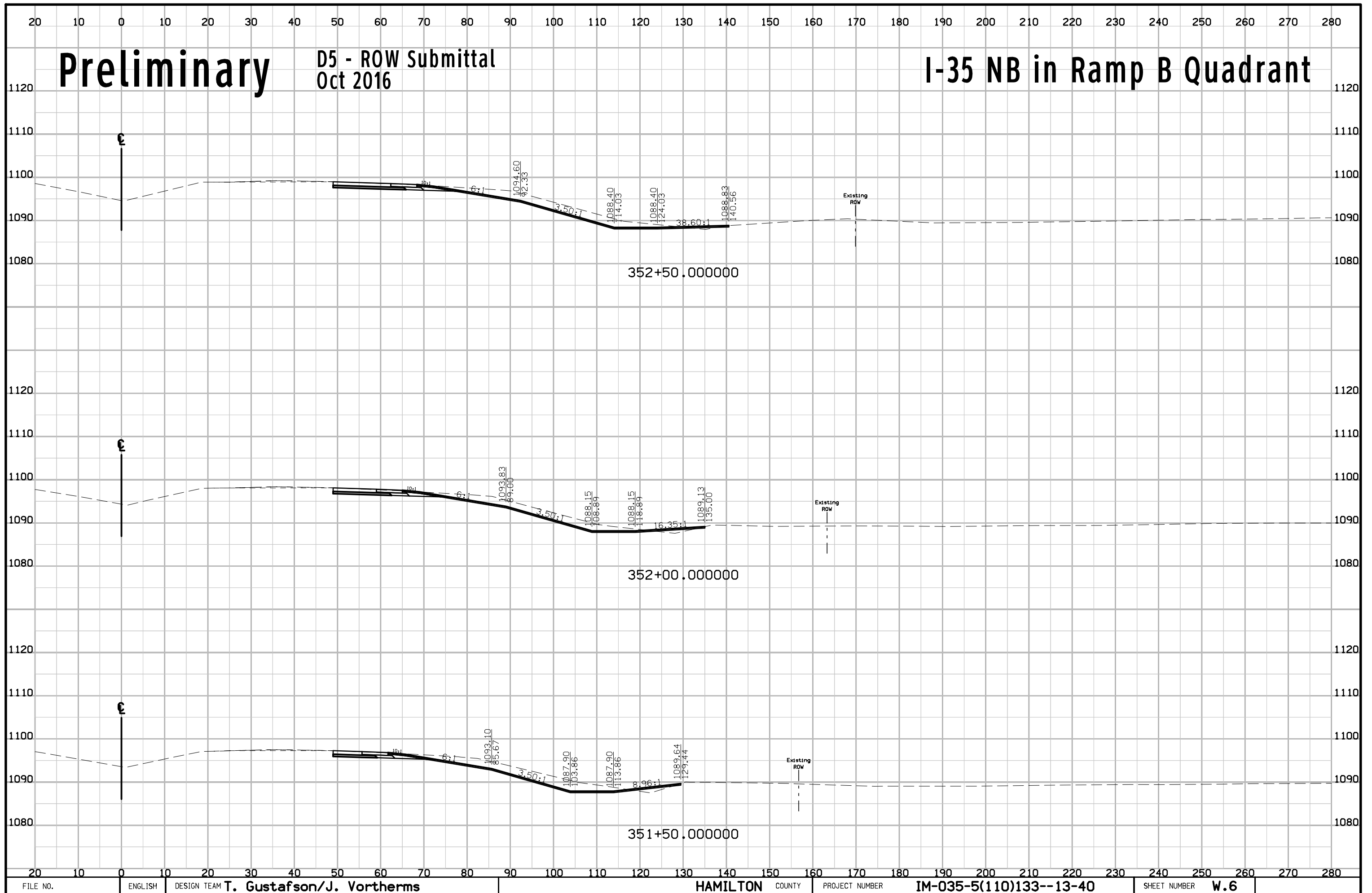
## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

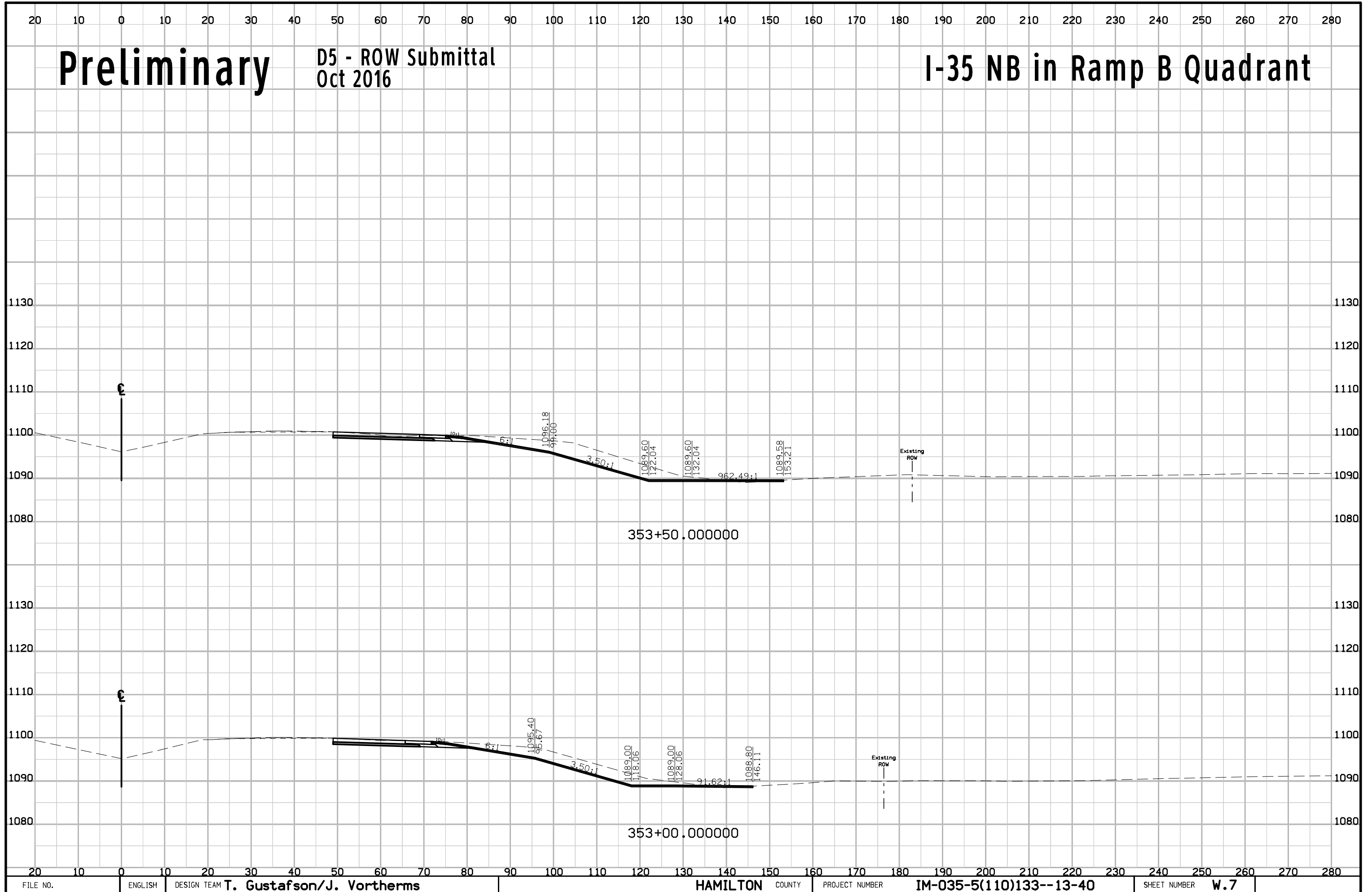
## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

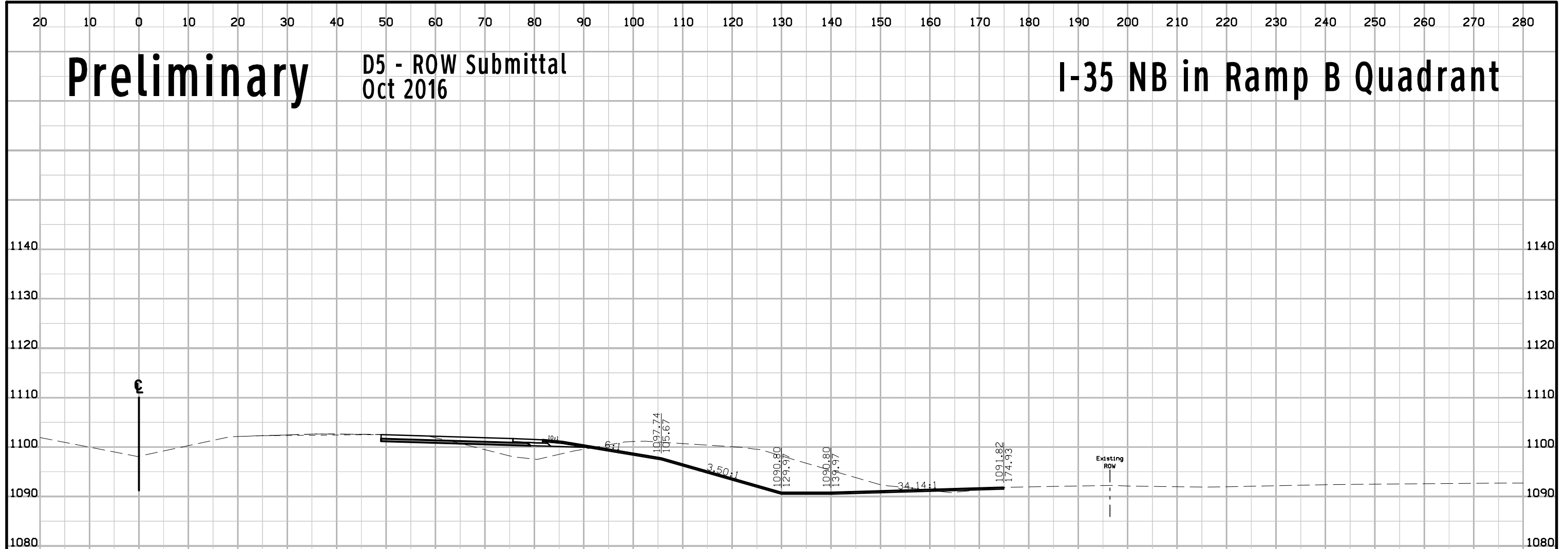
## I-35 NB in Ramp B Quadrant



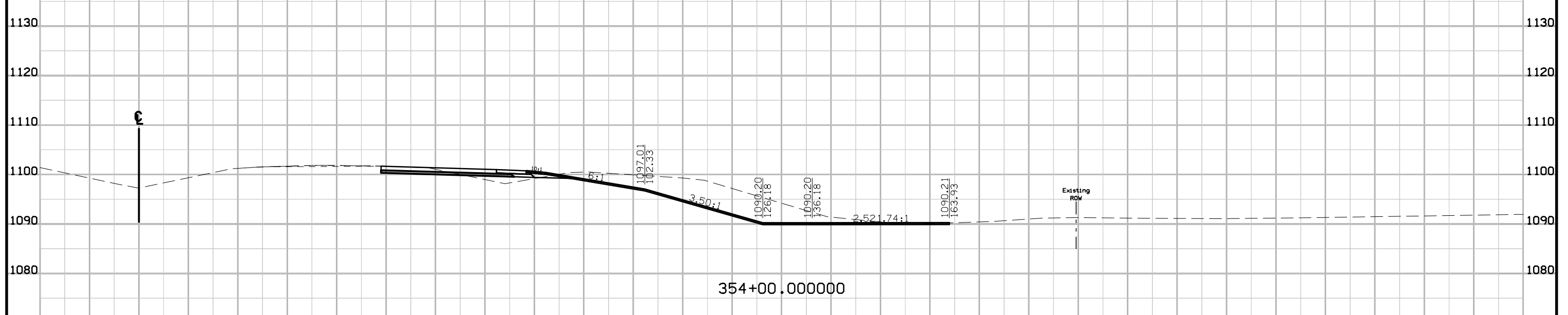
# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant



354+50.00000



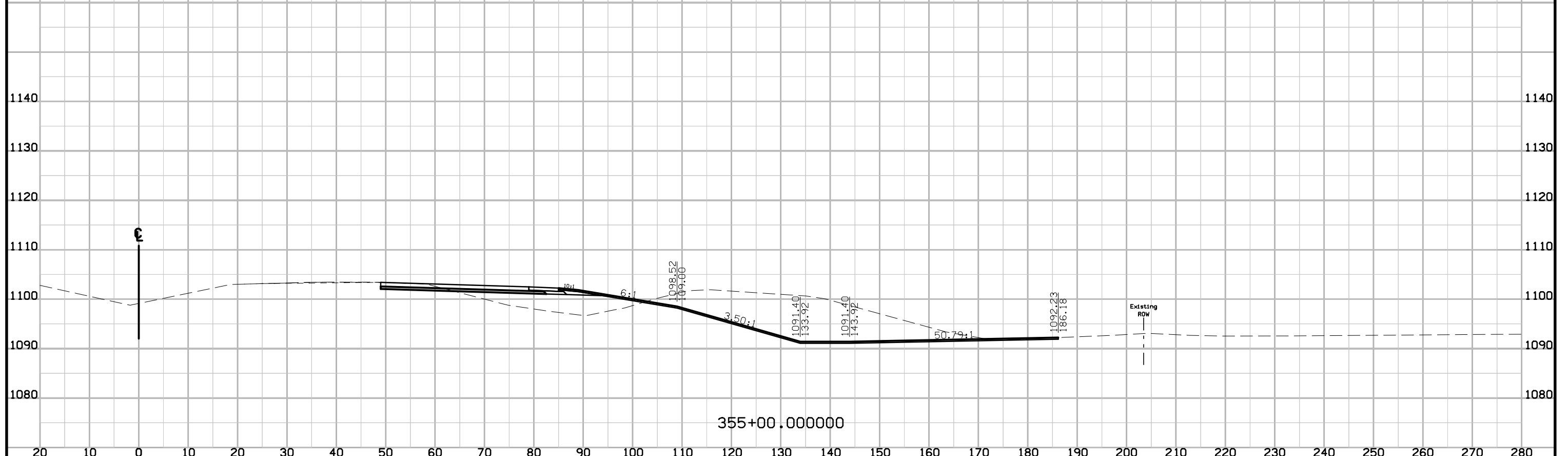
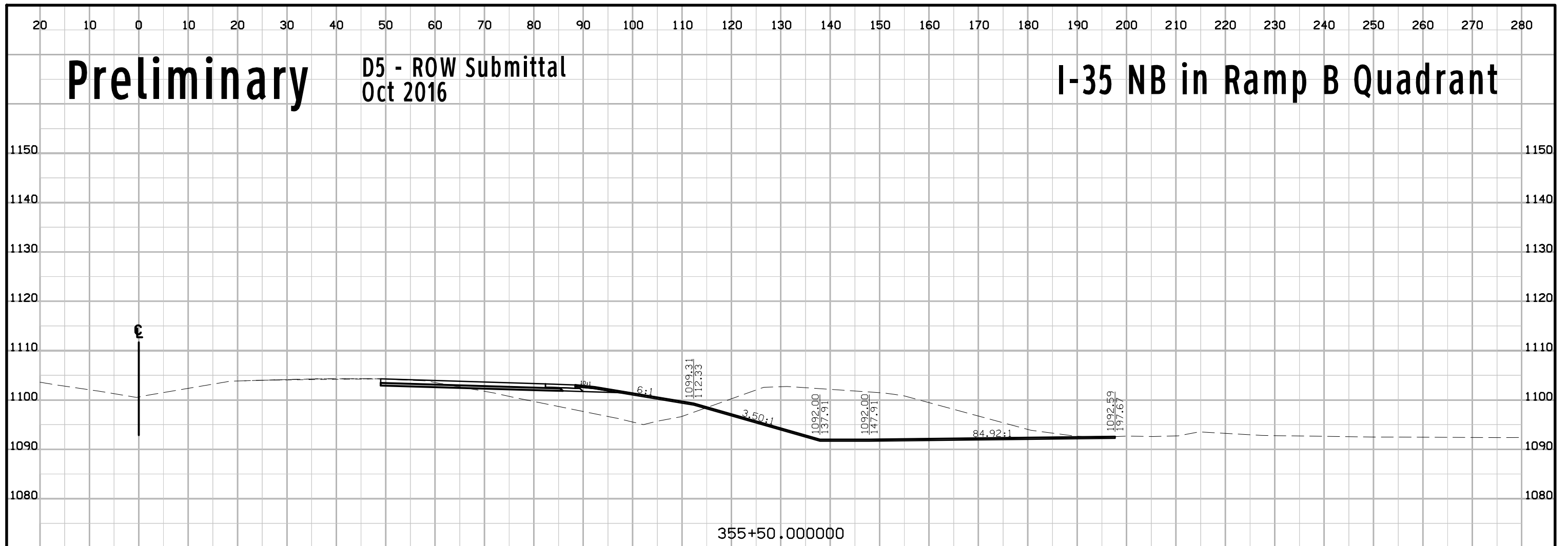
354+00.00000



# Preliminary

D5 - ROW Submittal  
Oct 2016

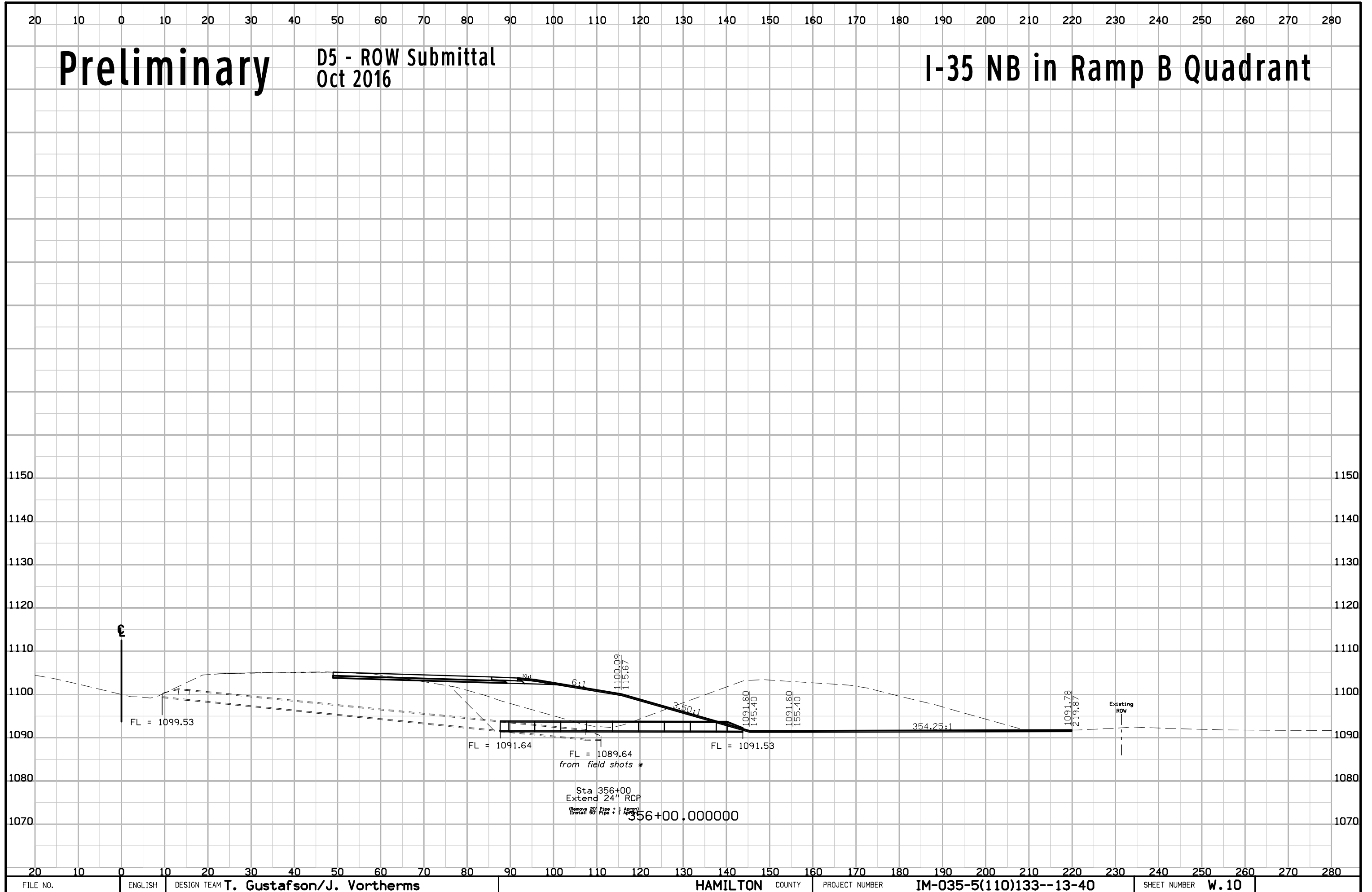
## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

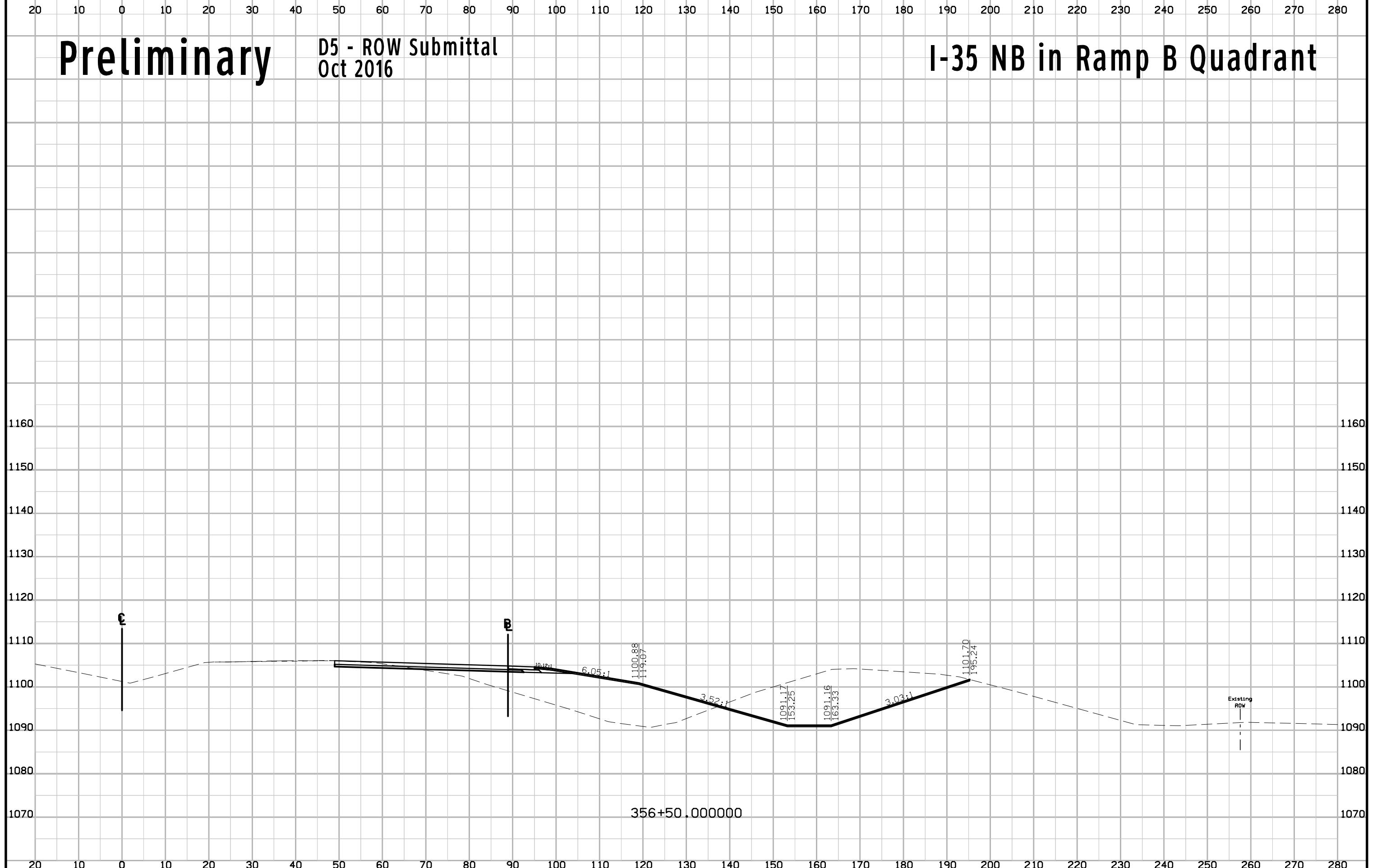
## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant

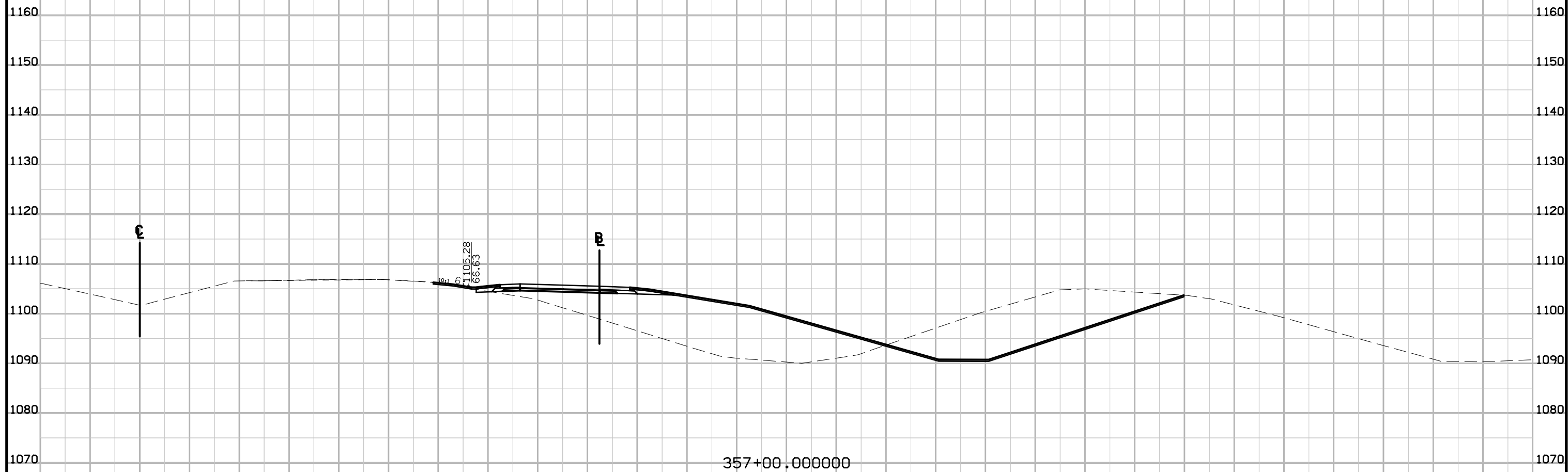


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant



20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant



20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

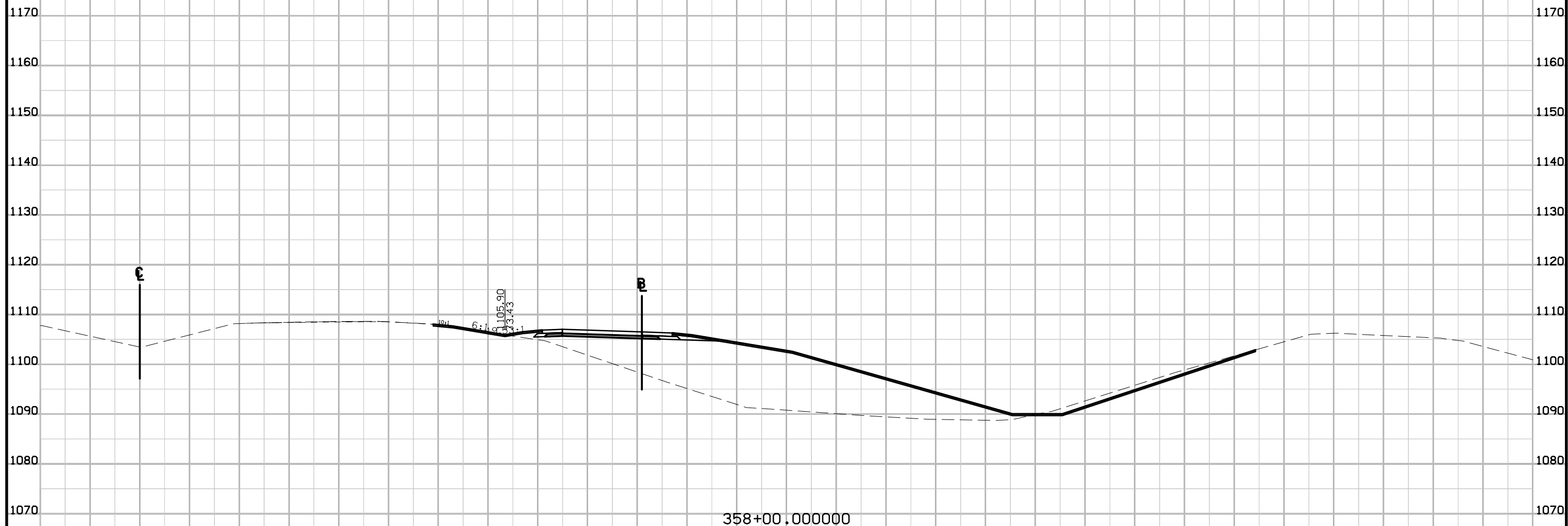
FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	<b>HAMILTON</b> COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.13</b>
----------	---------	--	------------------------	---	--------------------------

20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant



20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

## D5 - ROW Submittal Oct 2016

# I-35 NB in Ramp B Quadrant



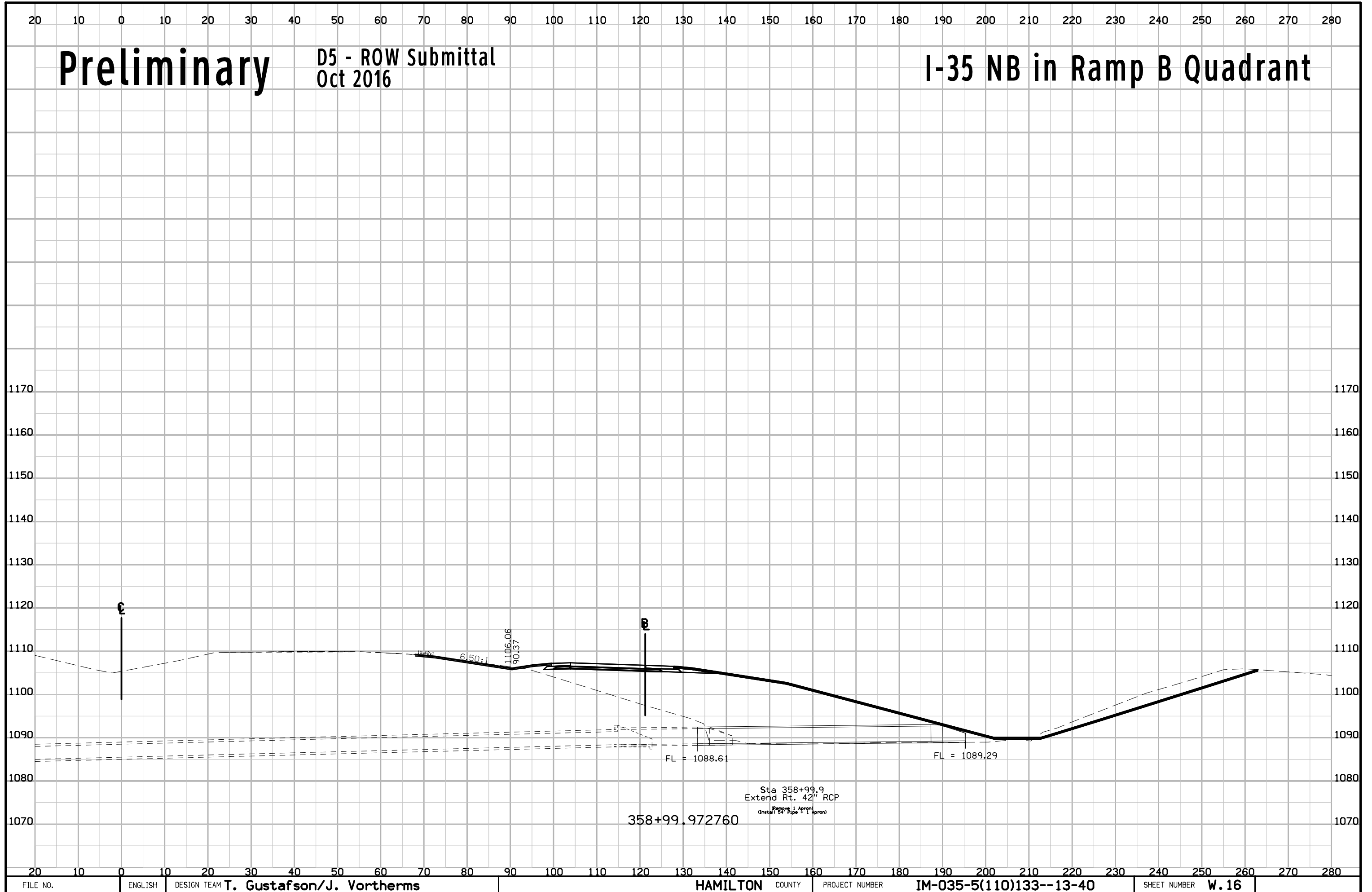
20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	<b>HAMILTON</b> COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.15</b>
----------	---------	--	------------------------	---	--------------------------

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant



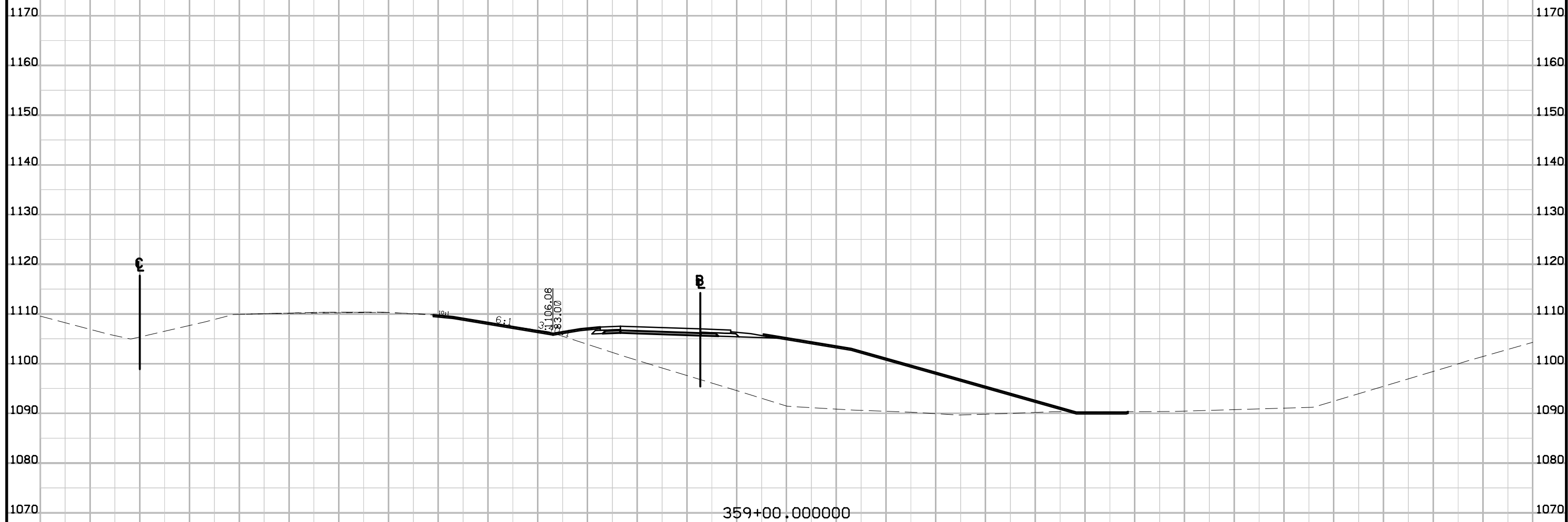


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant



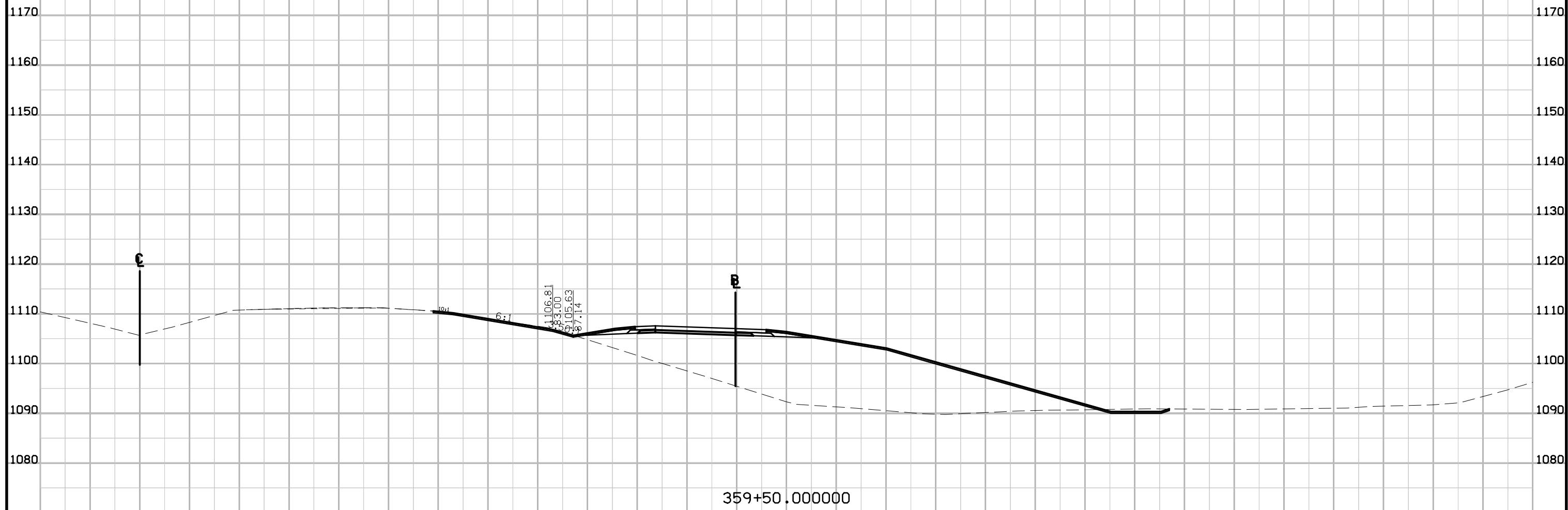
20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant

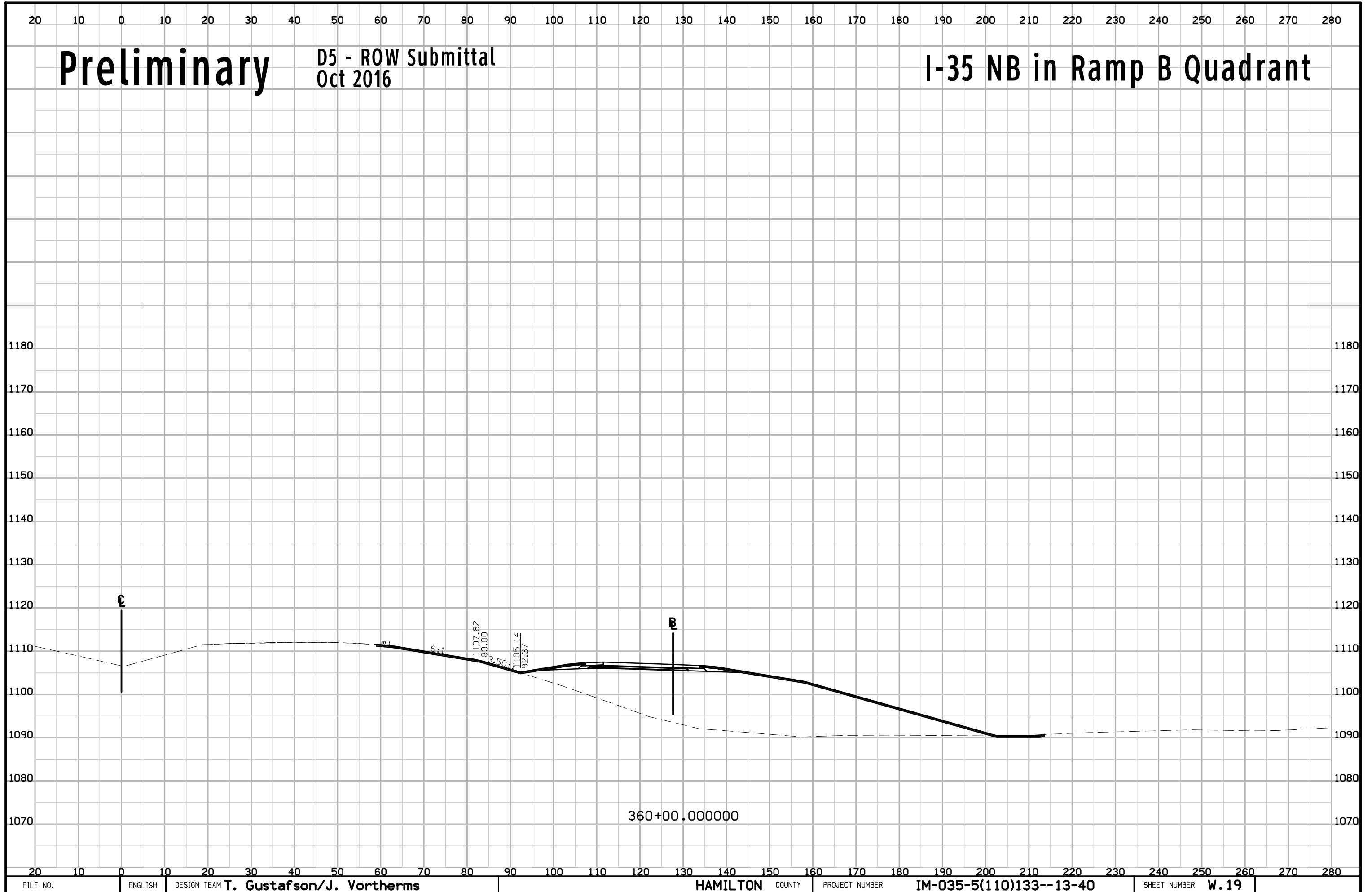


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant

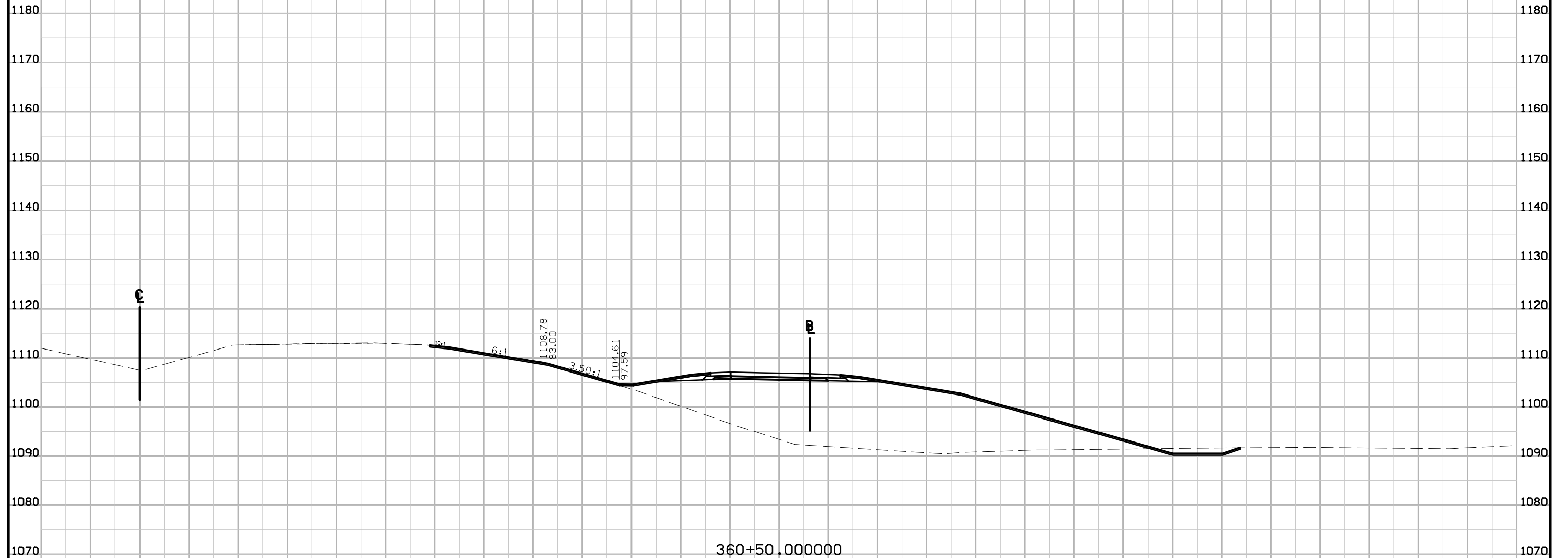


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant

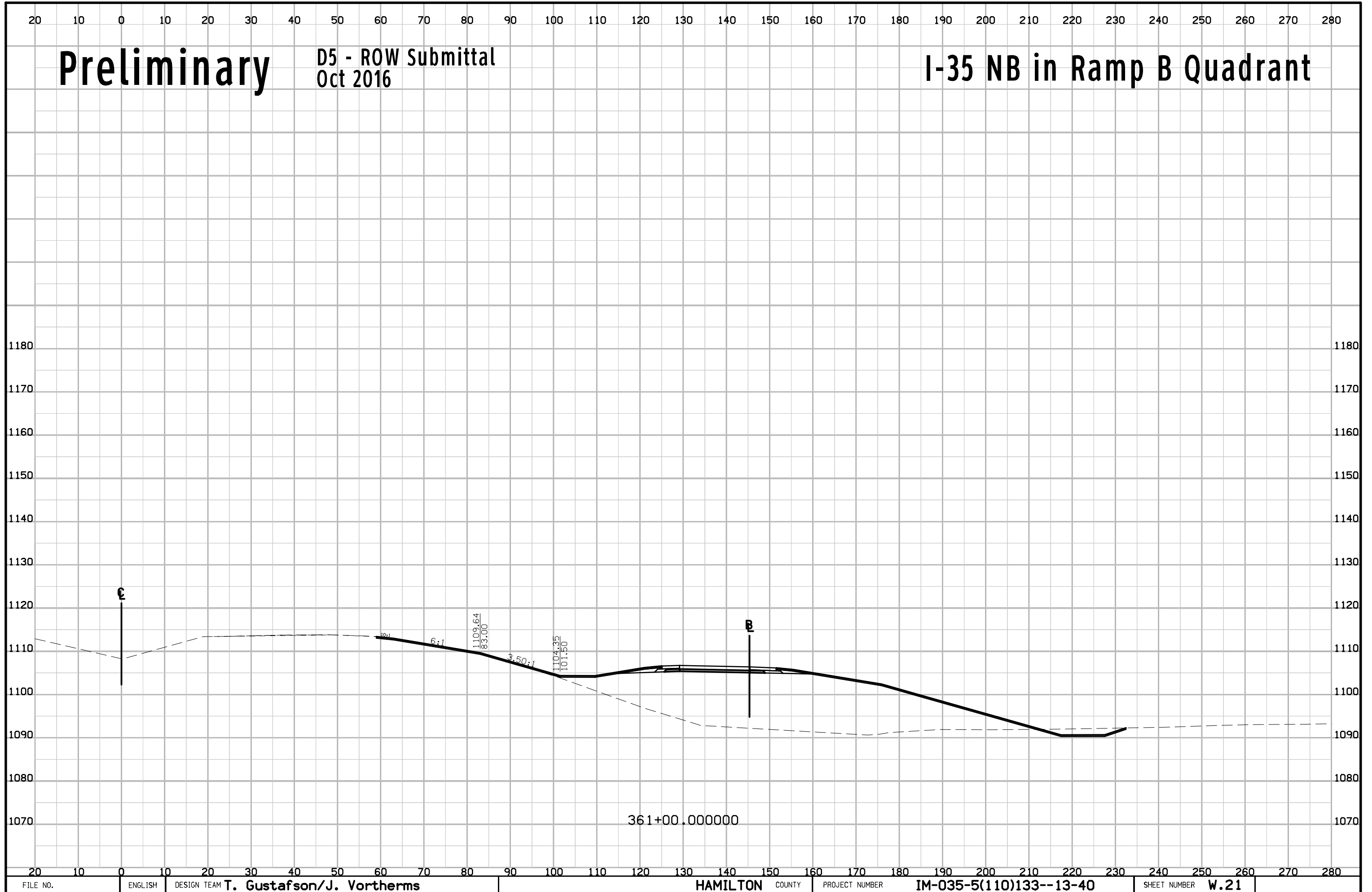


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant

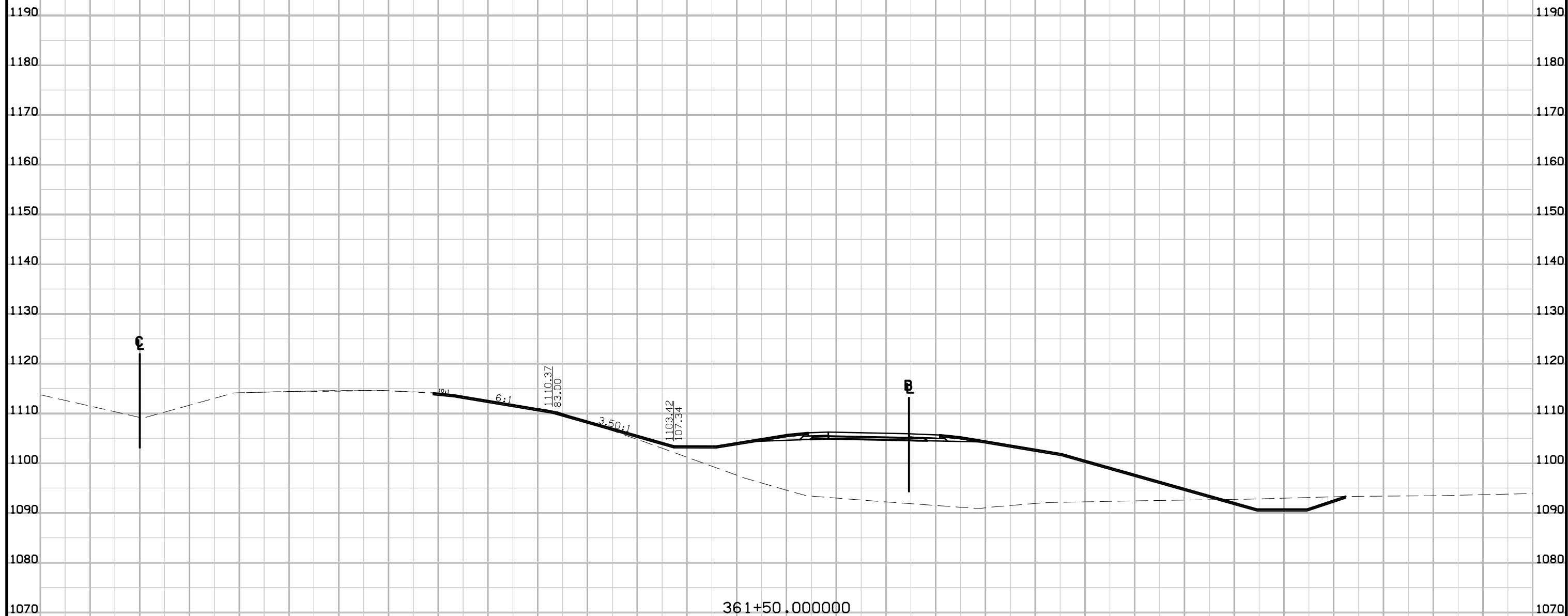


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant

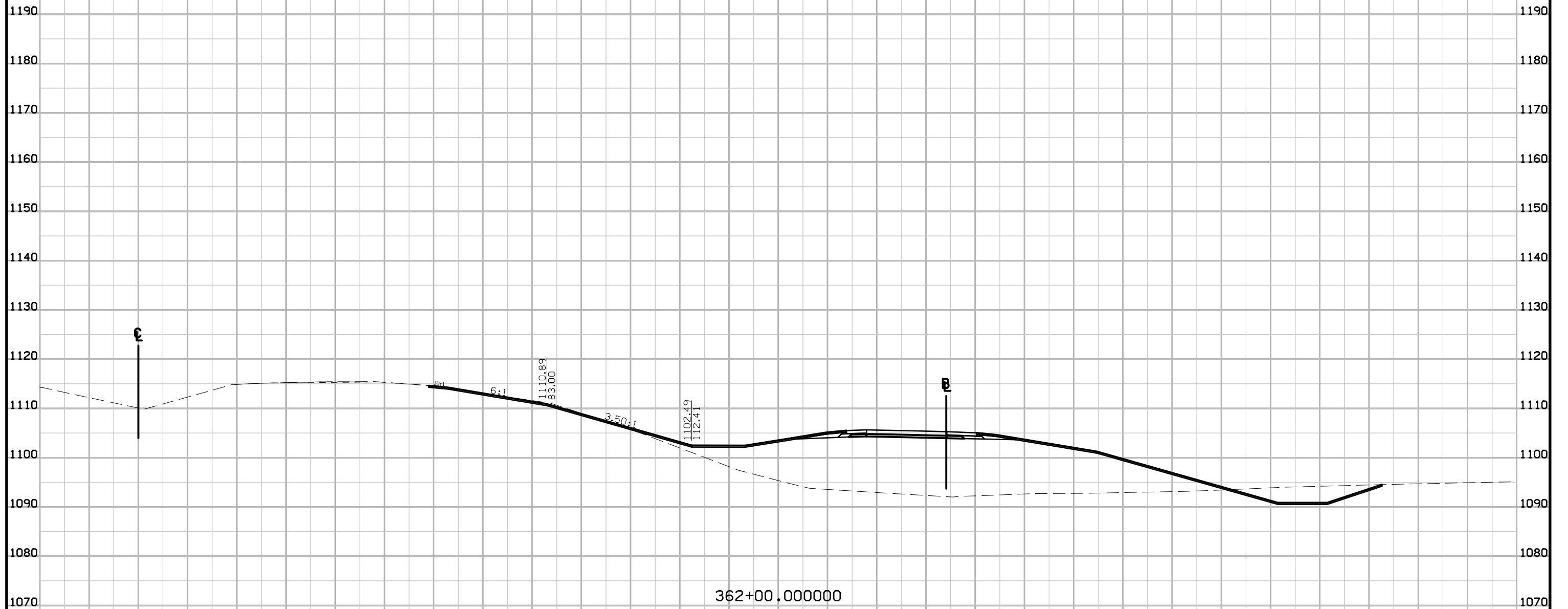


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

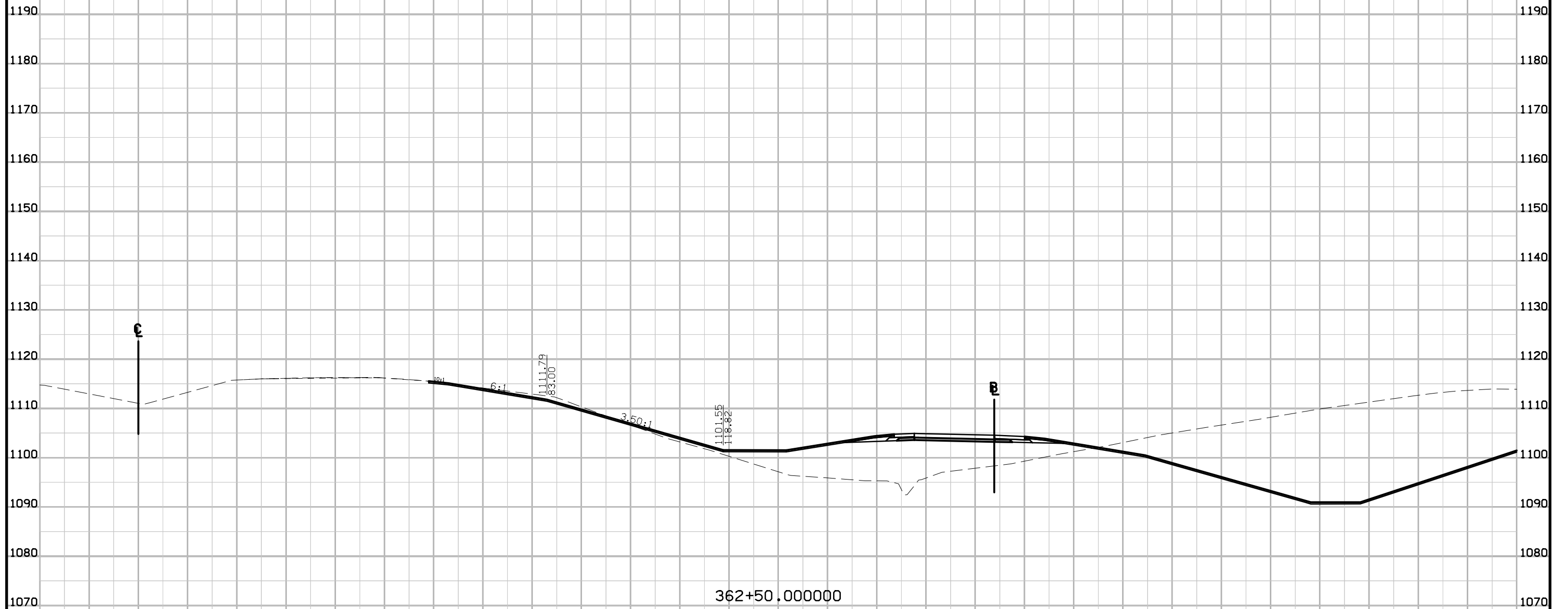
## I-35 NB in Ramp B Quadrant



Preliminary

D5 - ROW Submittal  
Oct 2016

I-35 NB in Ramp B Quadrant





# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant

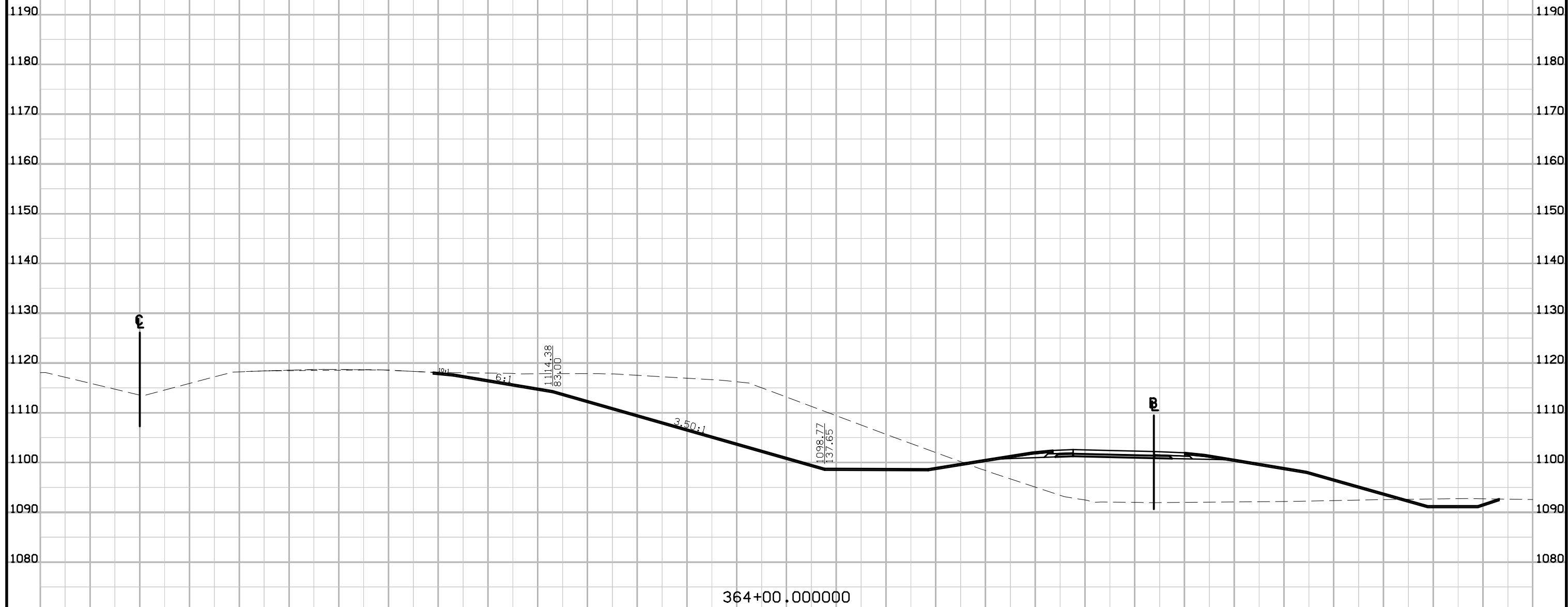


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

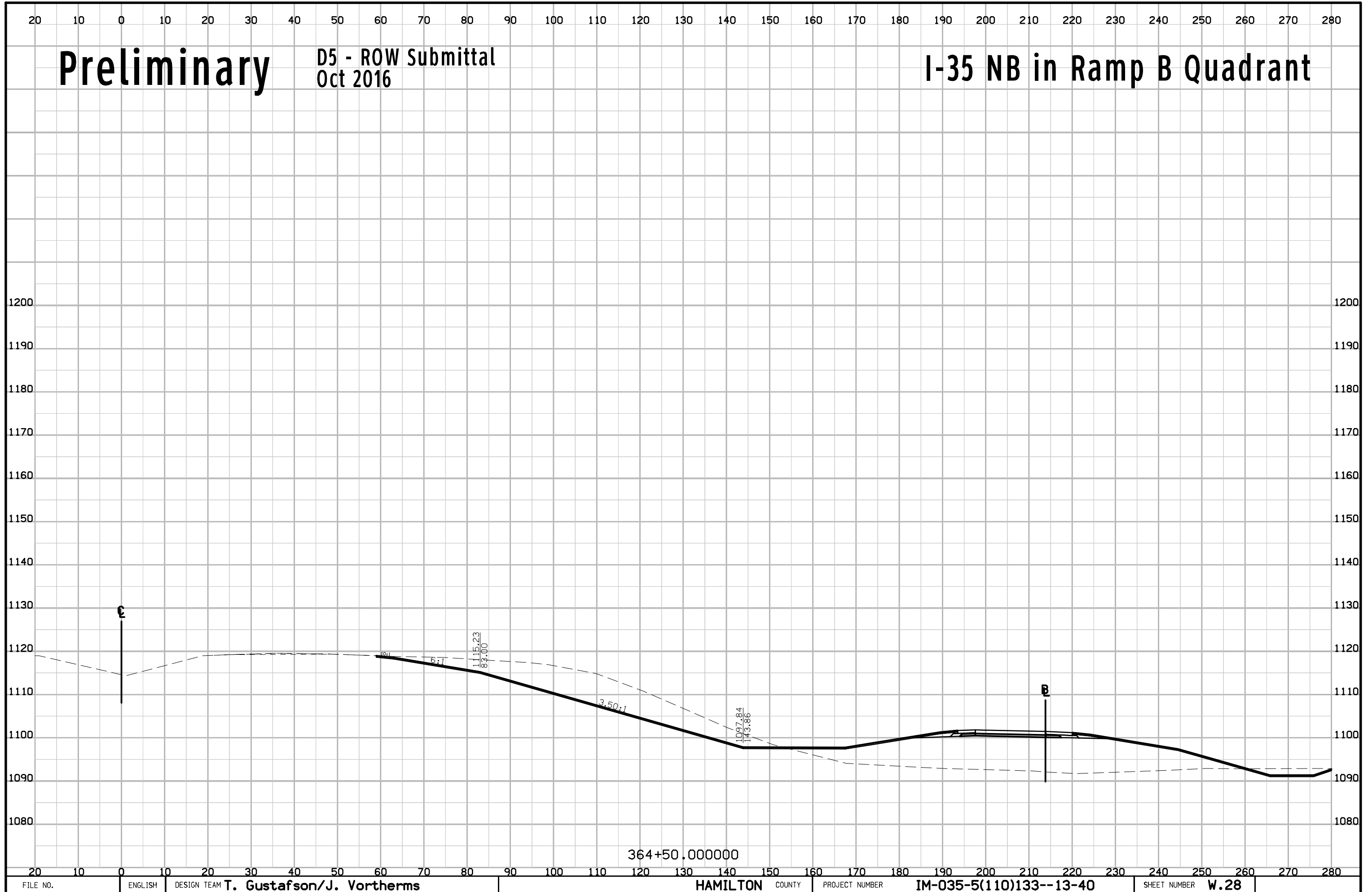
## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

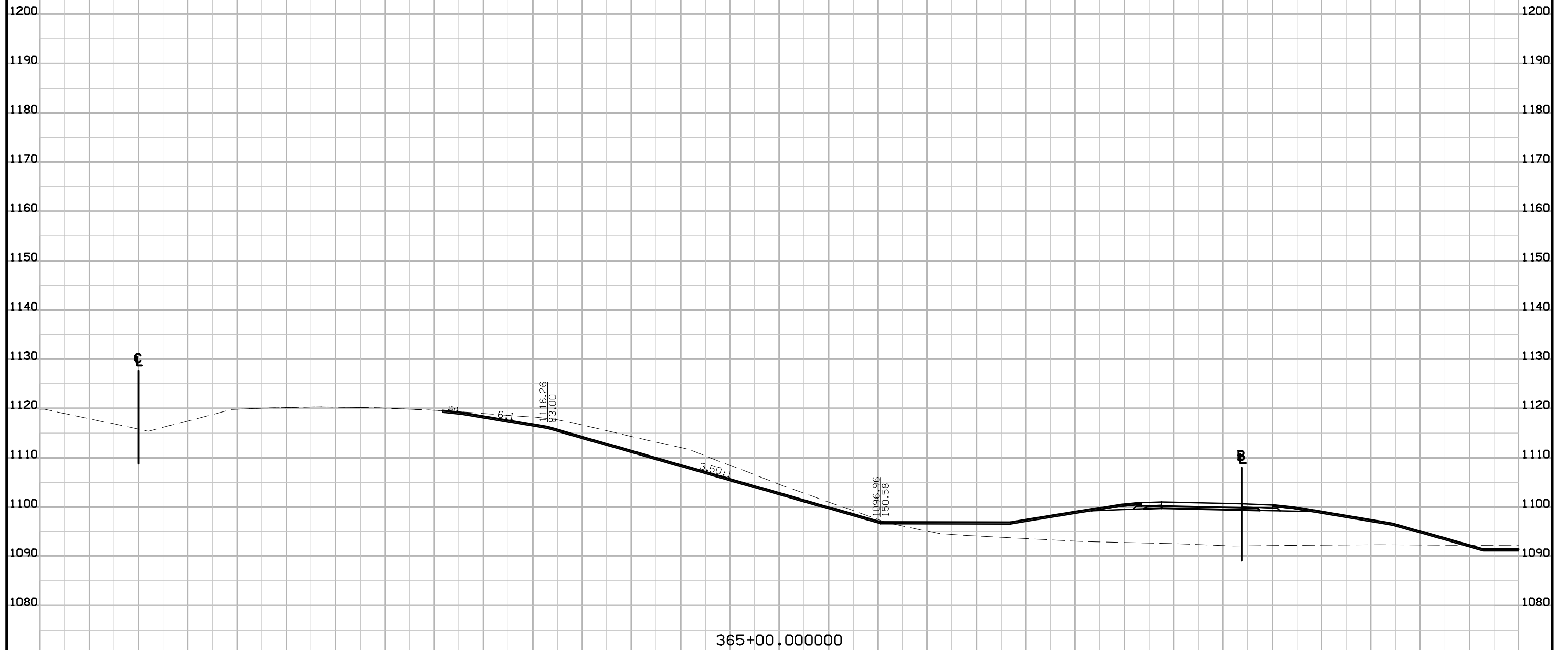
## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

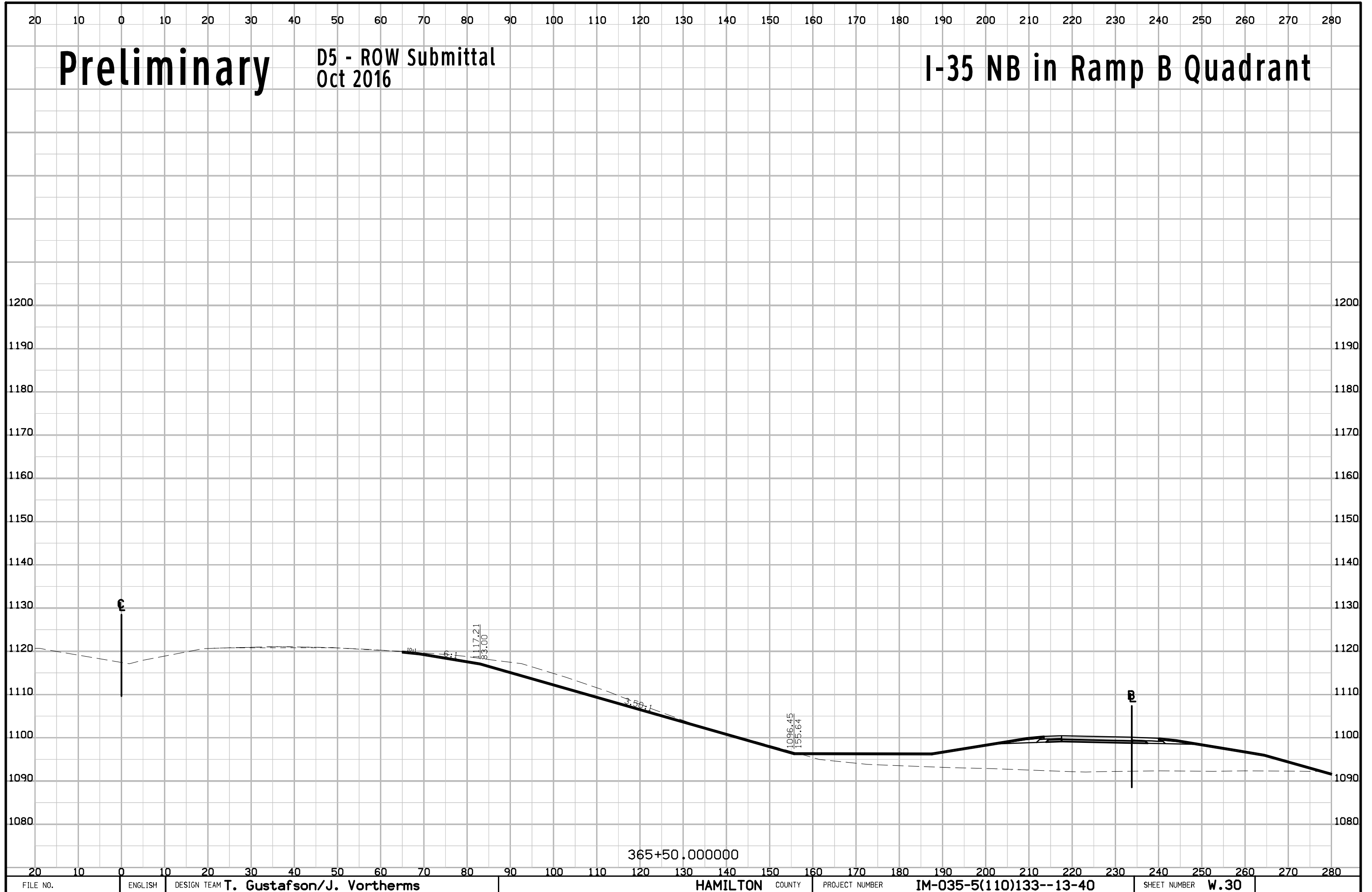
## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

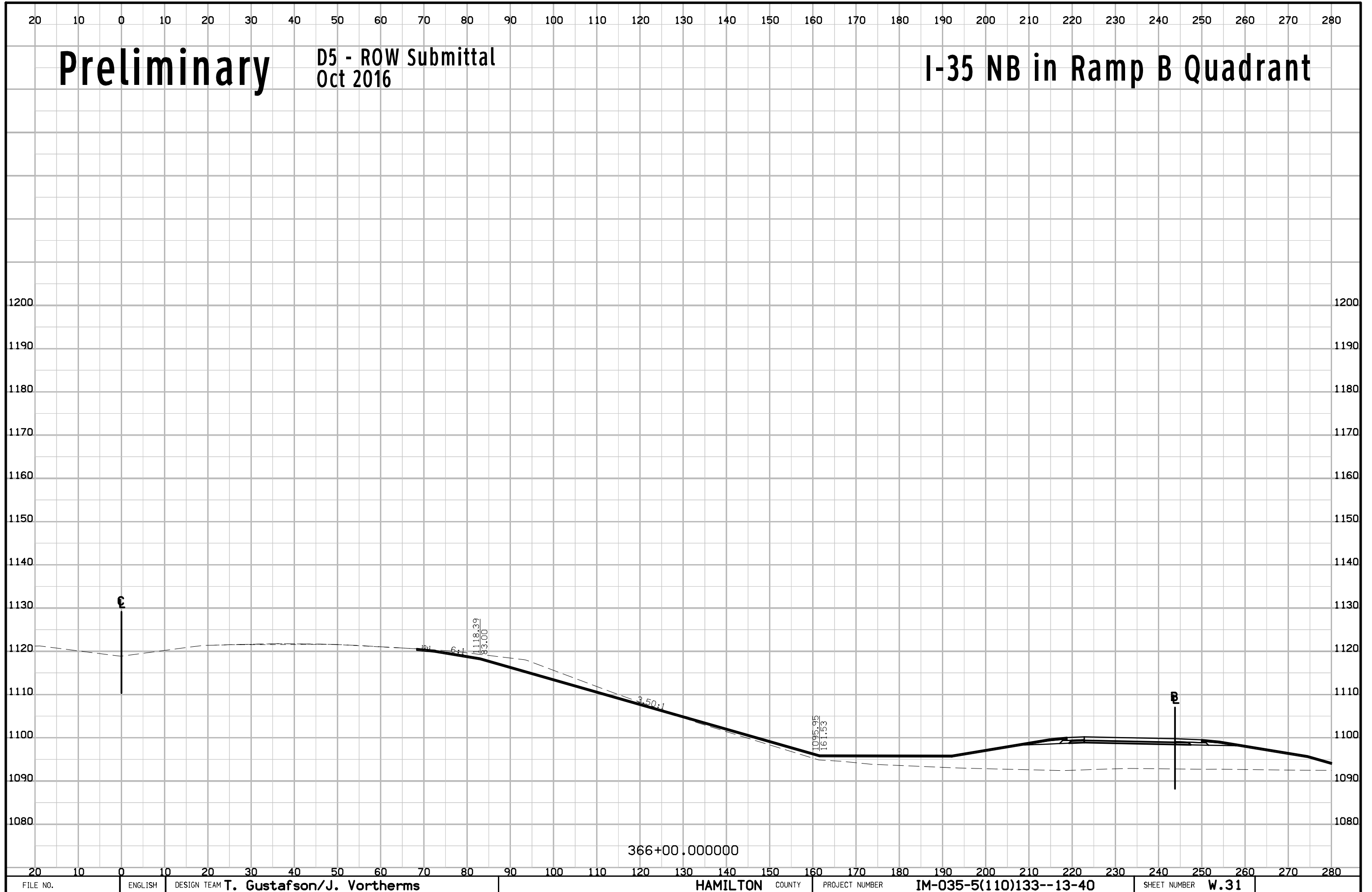
## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

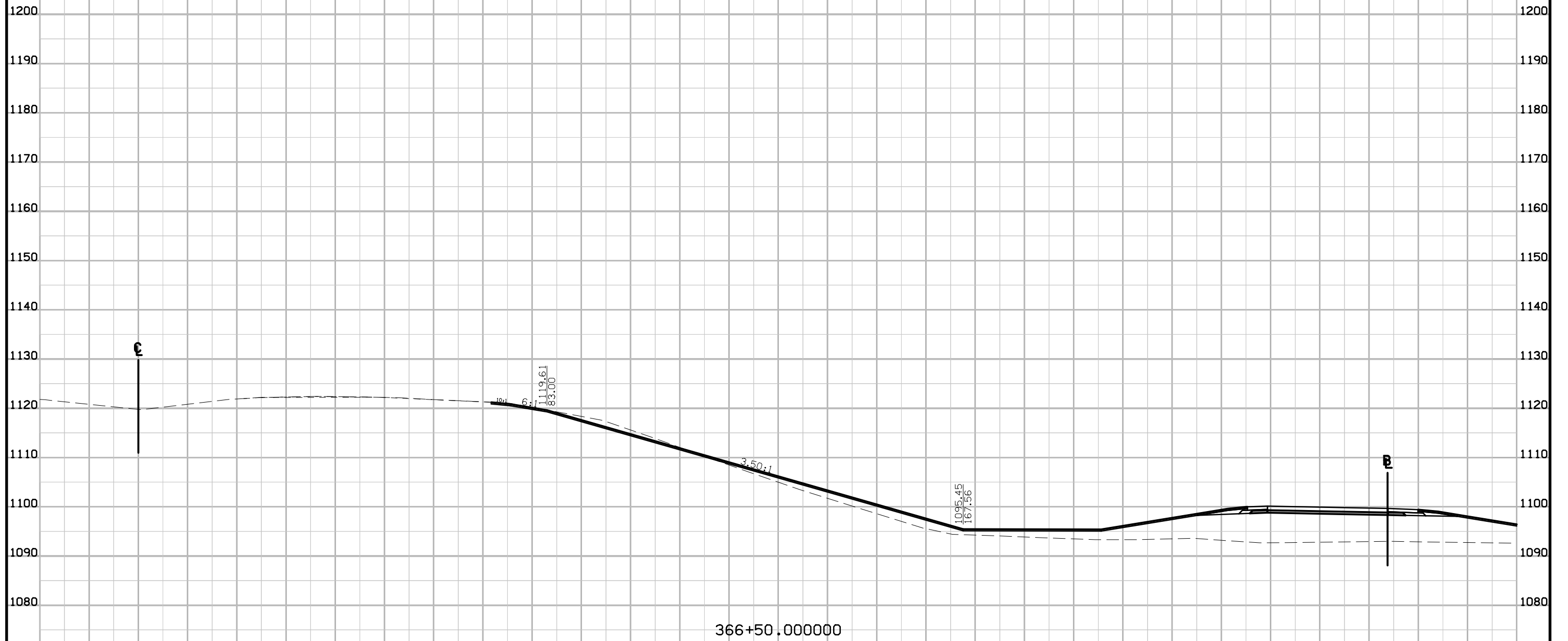
## I-35 NB in Ramp B Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant



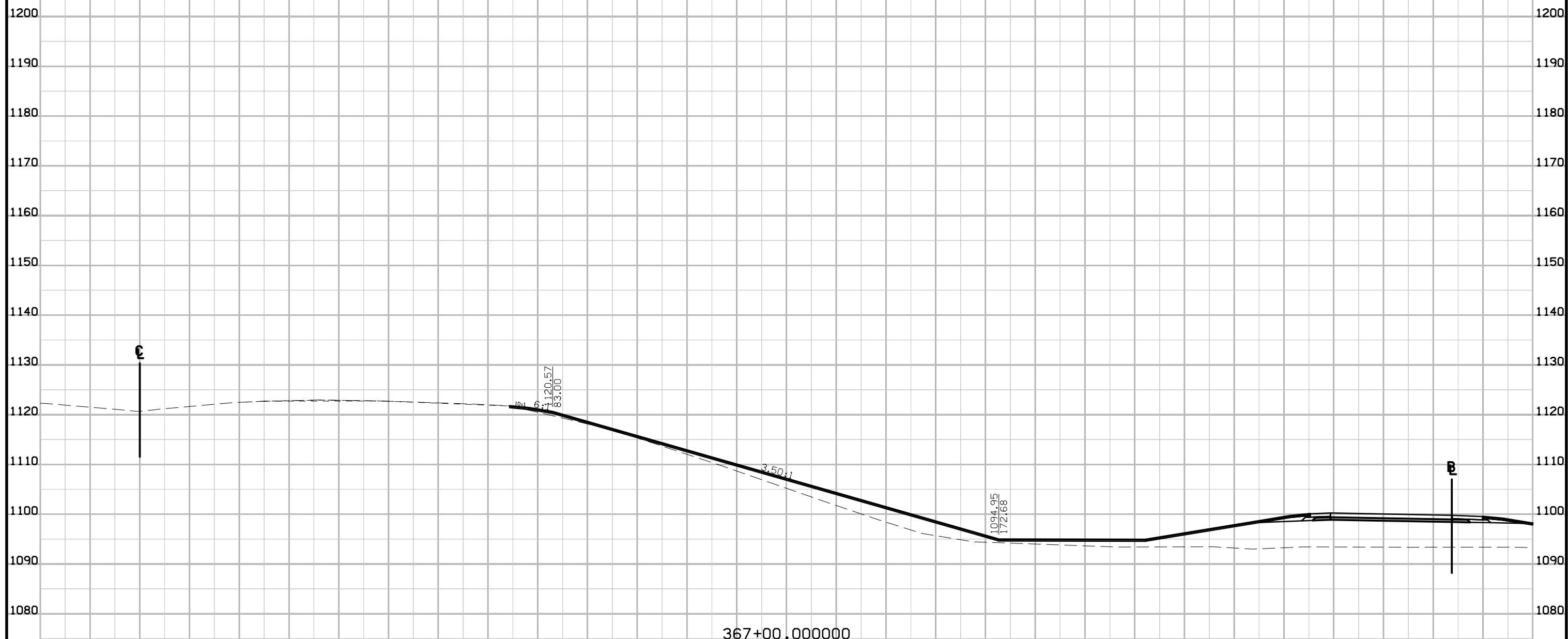


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant



367+00.000000

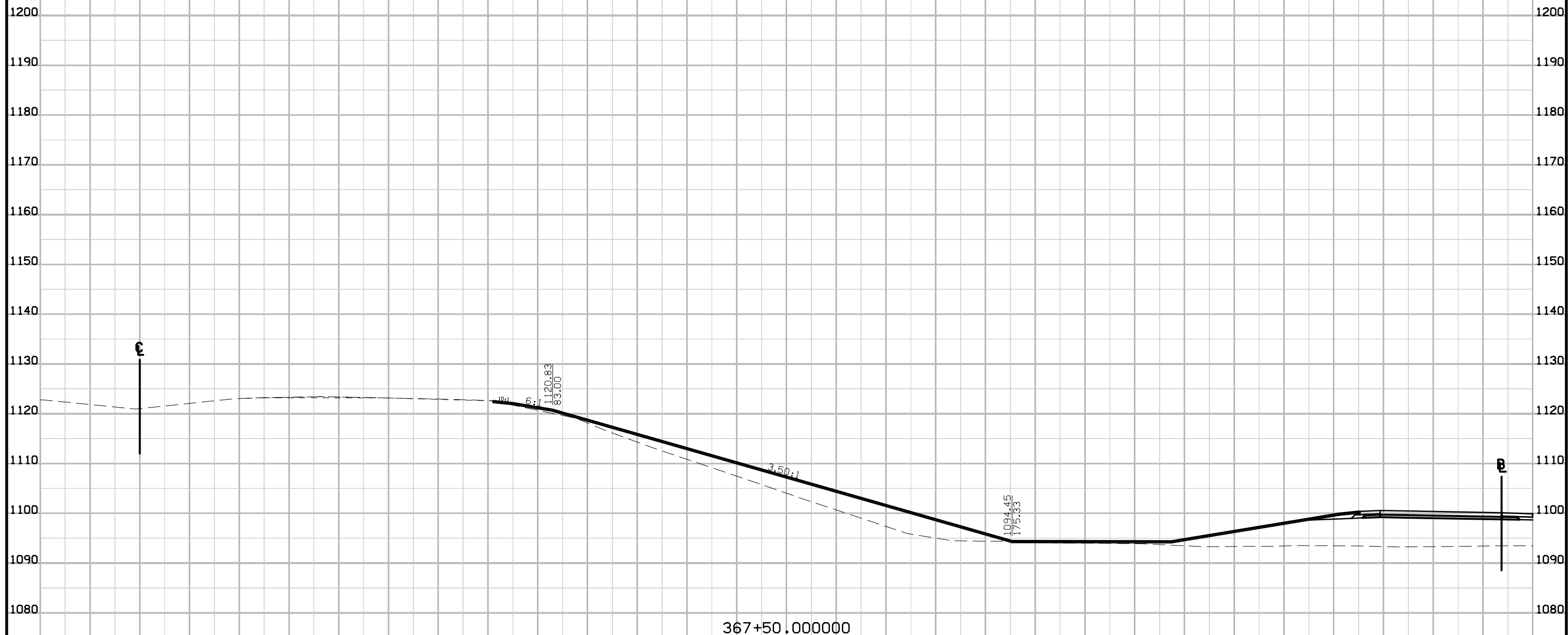
20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant



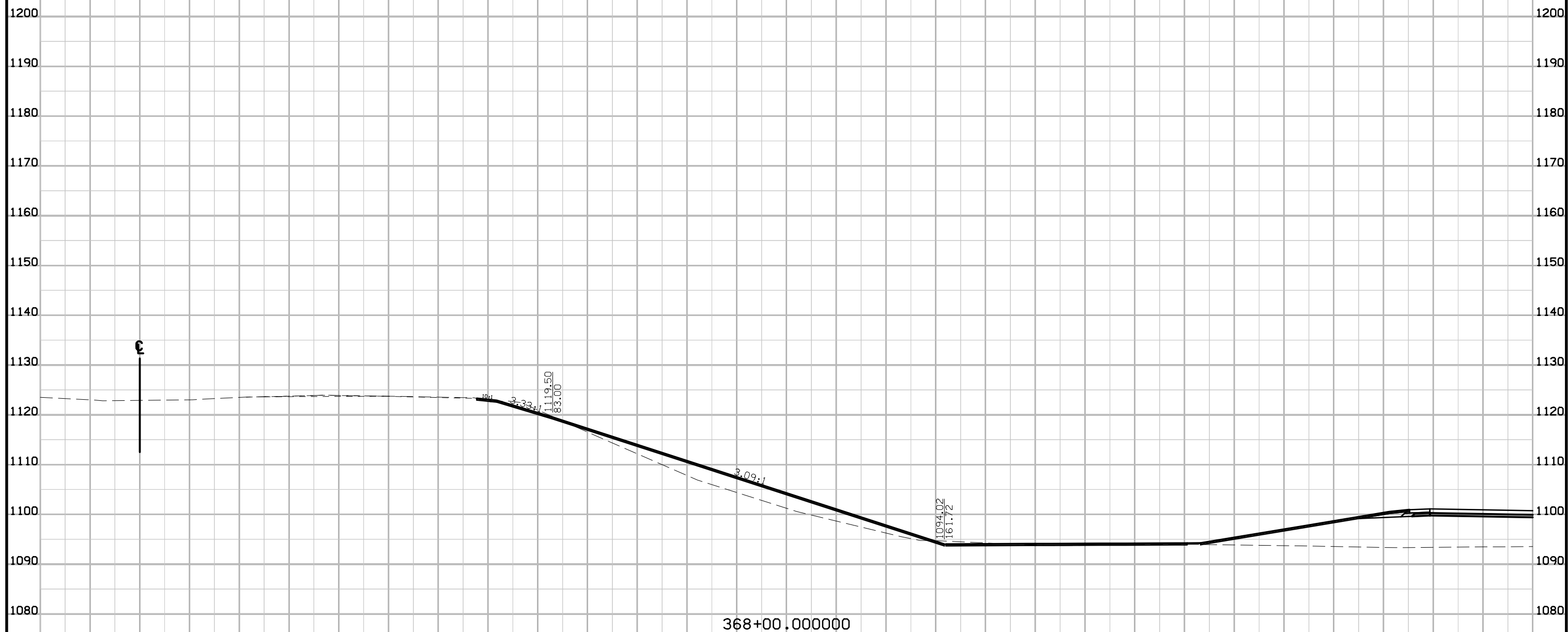
20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp B Quadrant



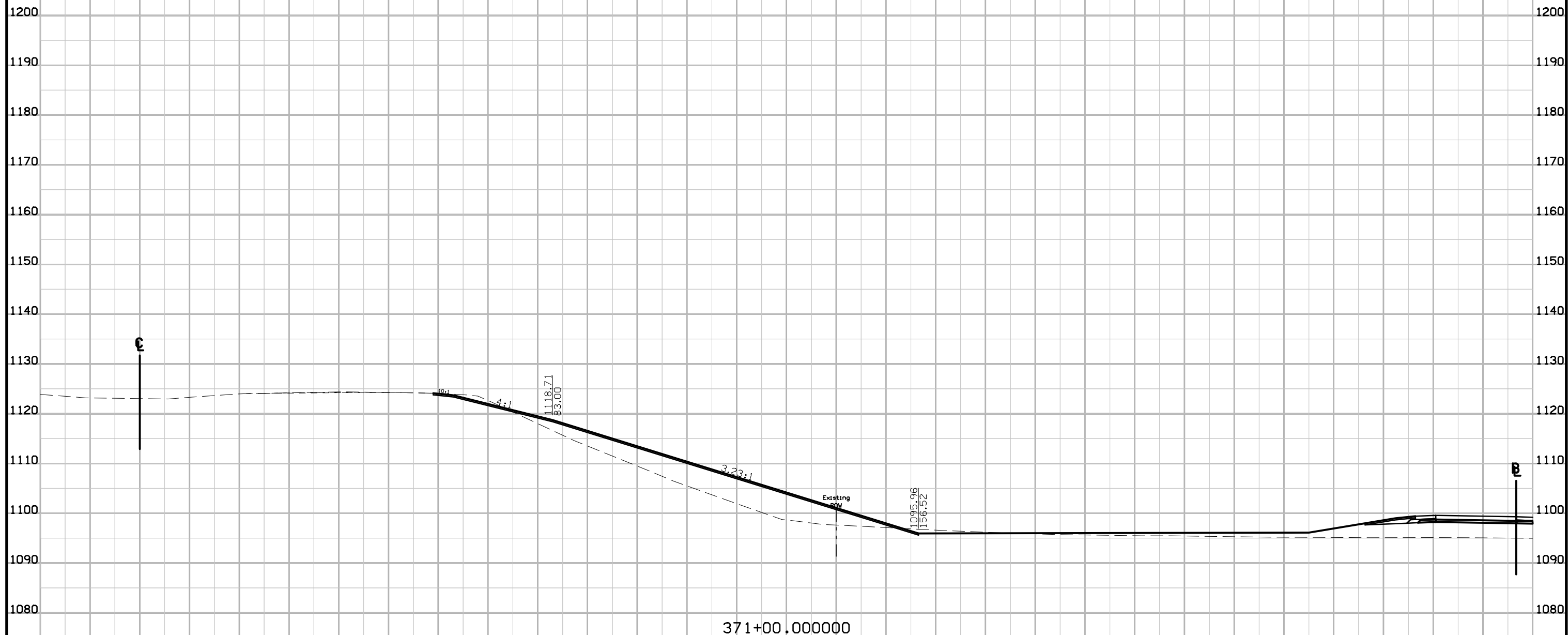
20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant



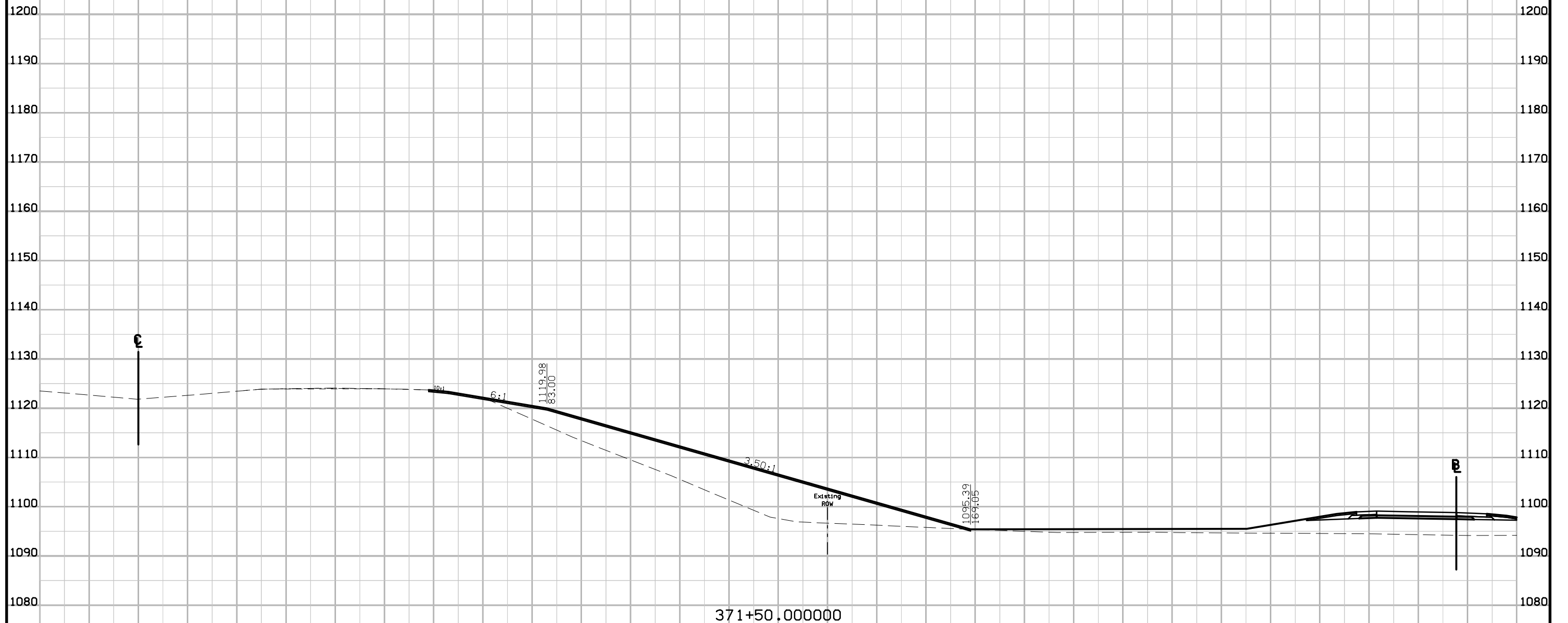
371+00.000000

20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

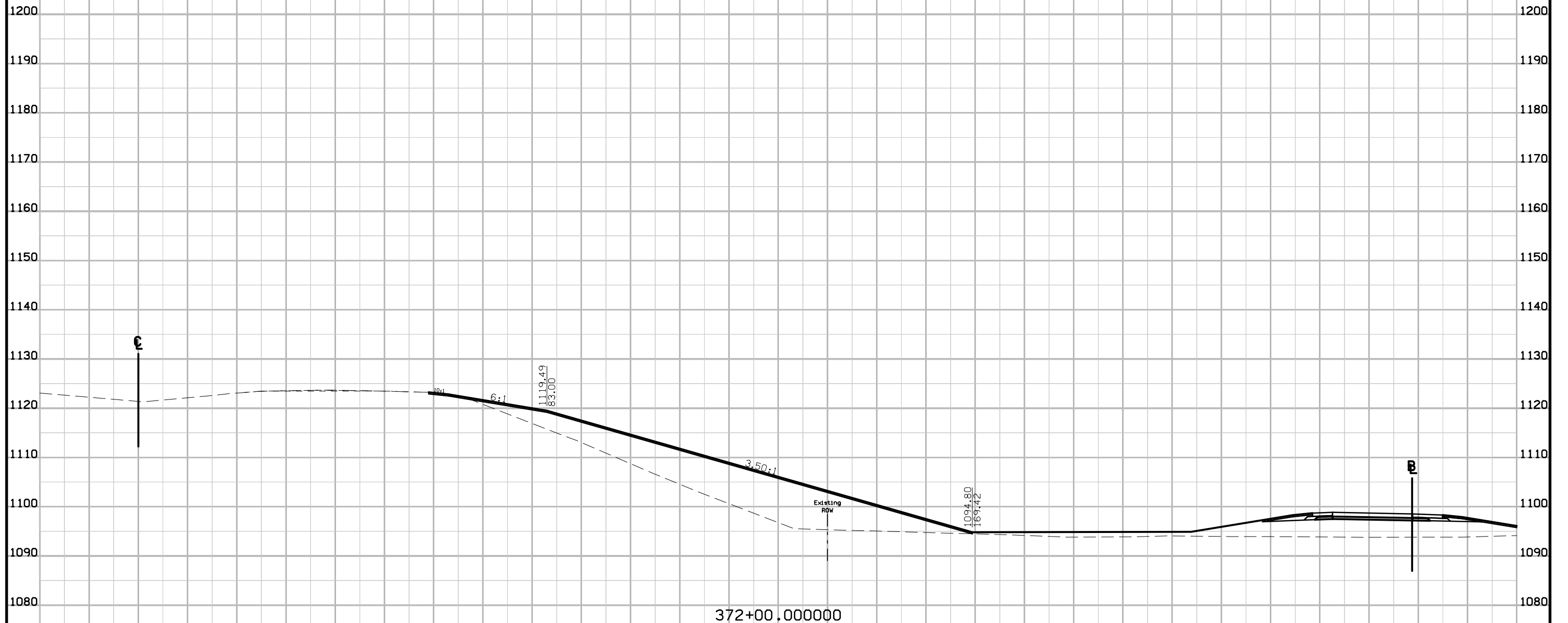
## I-35 NB in Ramp C Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

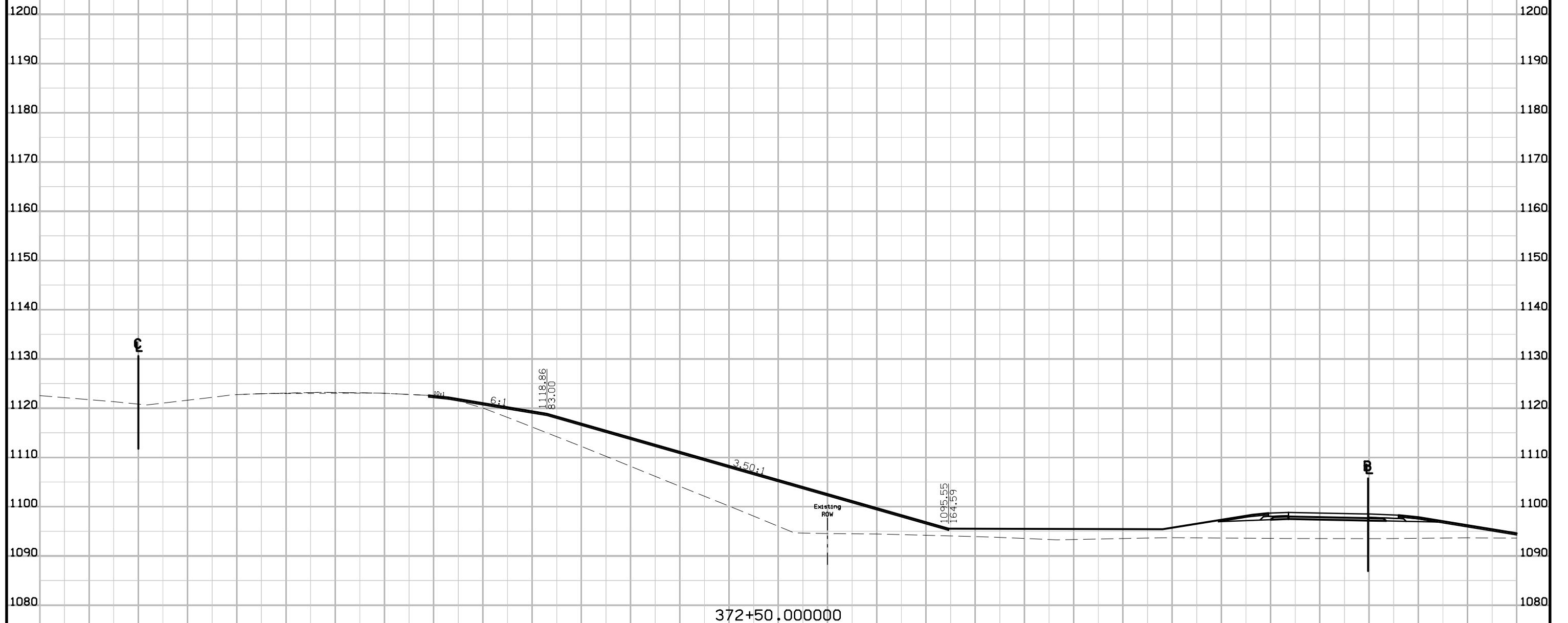
## I-35 NB in Ramp C Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

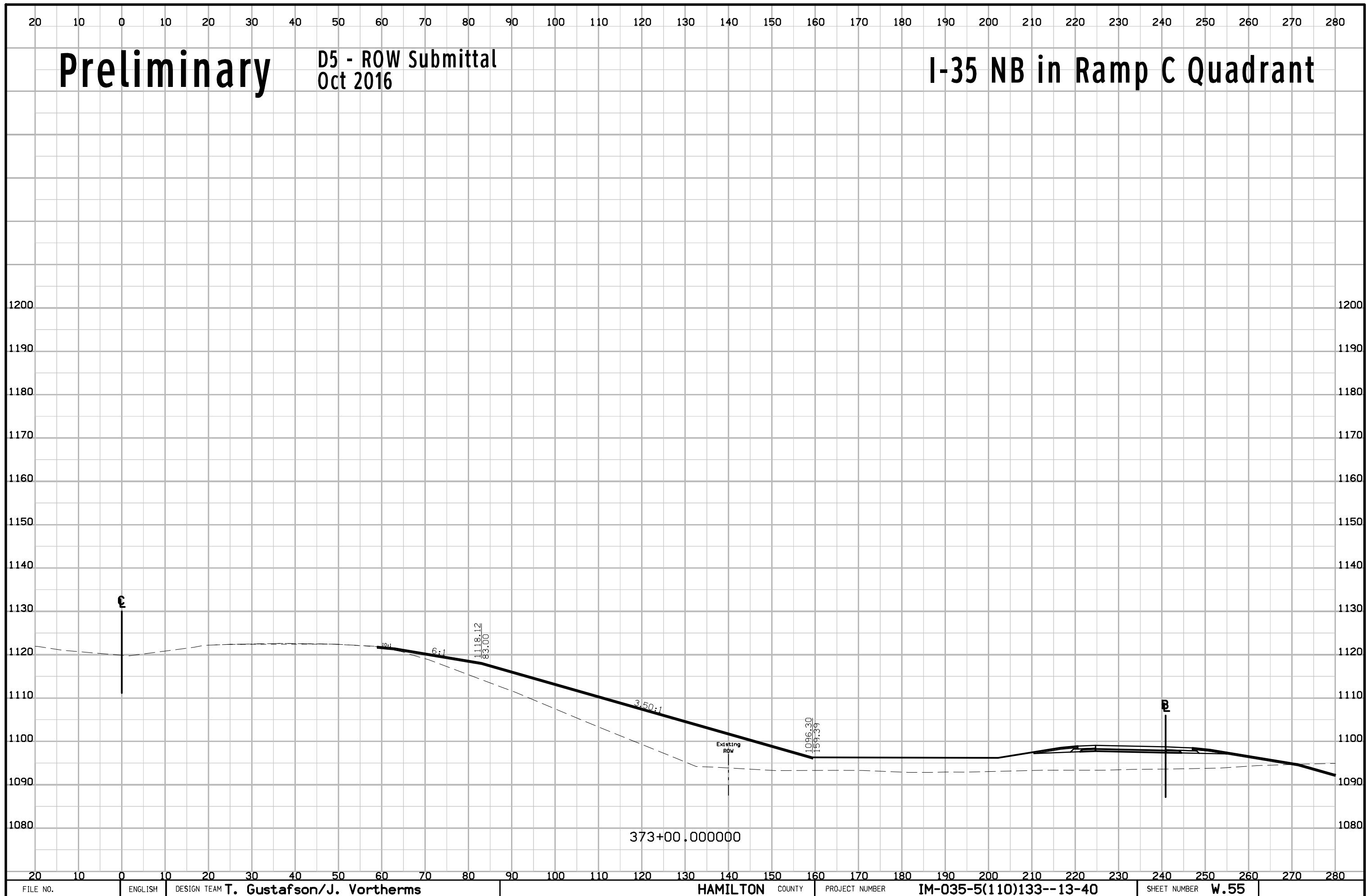
## I-35 NB in Ramp C Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant



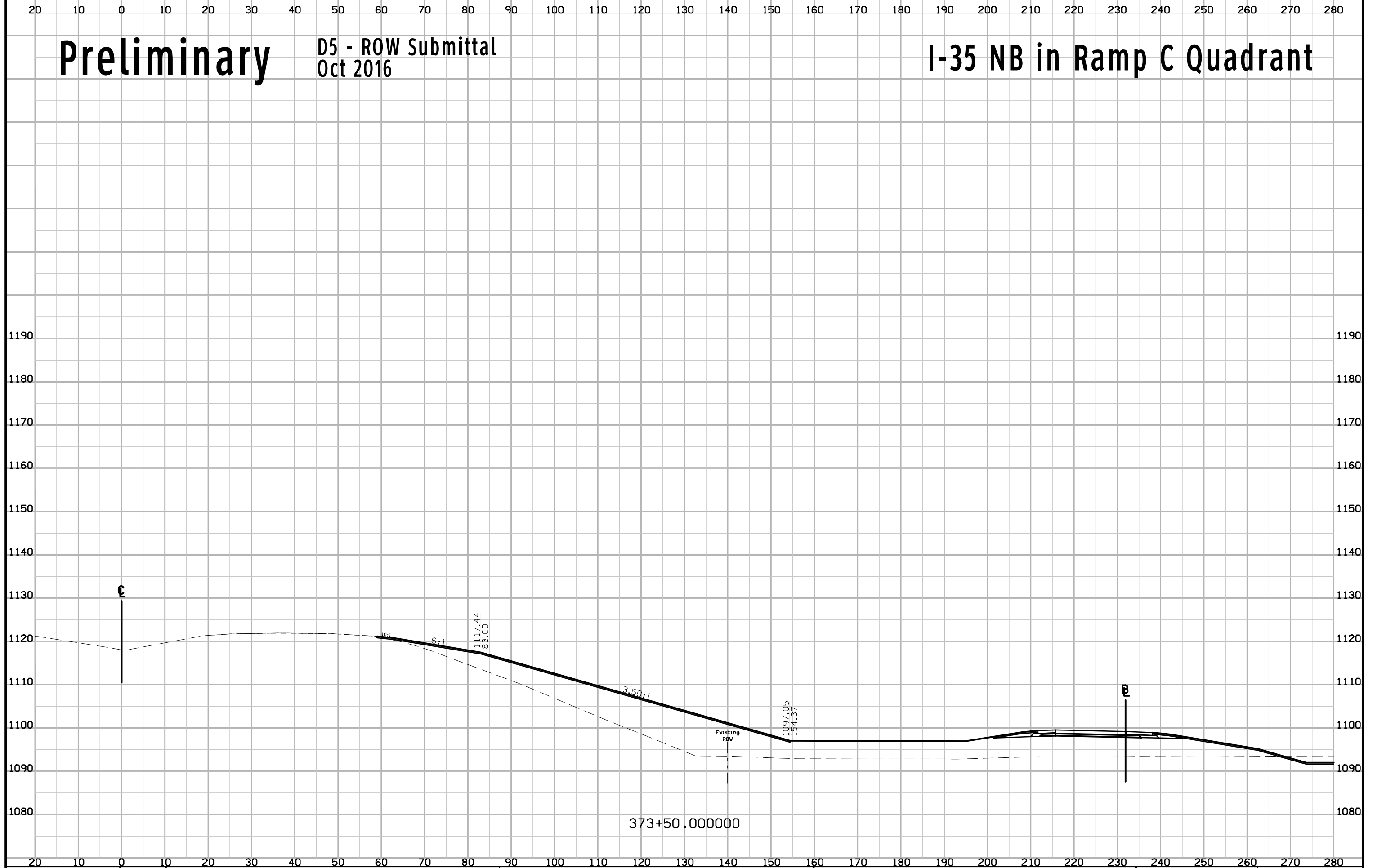
373+00.000000



Preliminary

D5 - ROW Submittal  
Oct 2016

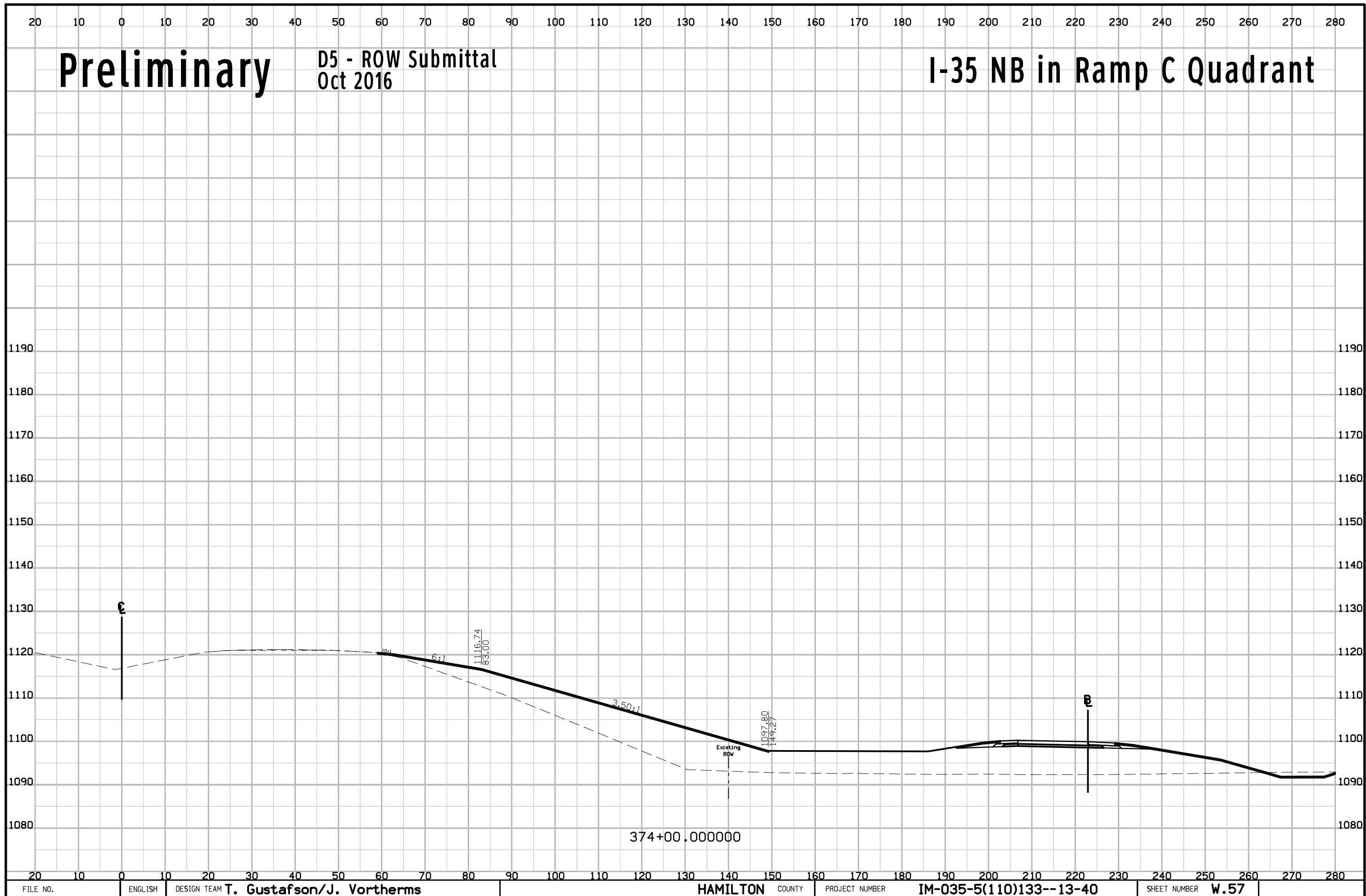
I-35 NB in Ramp C Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

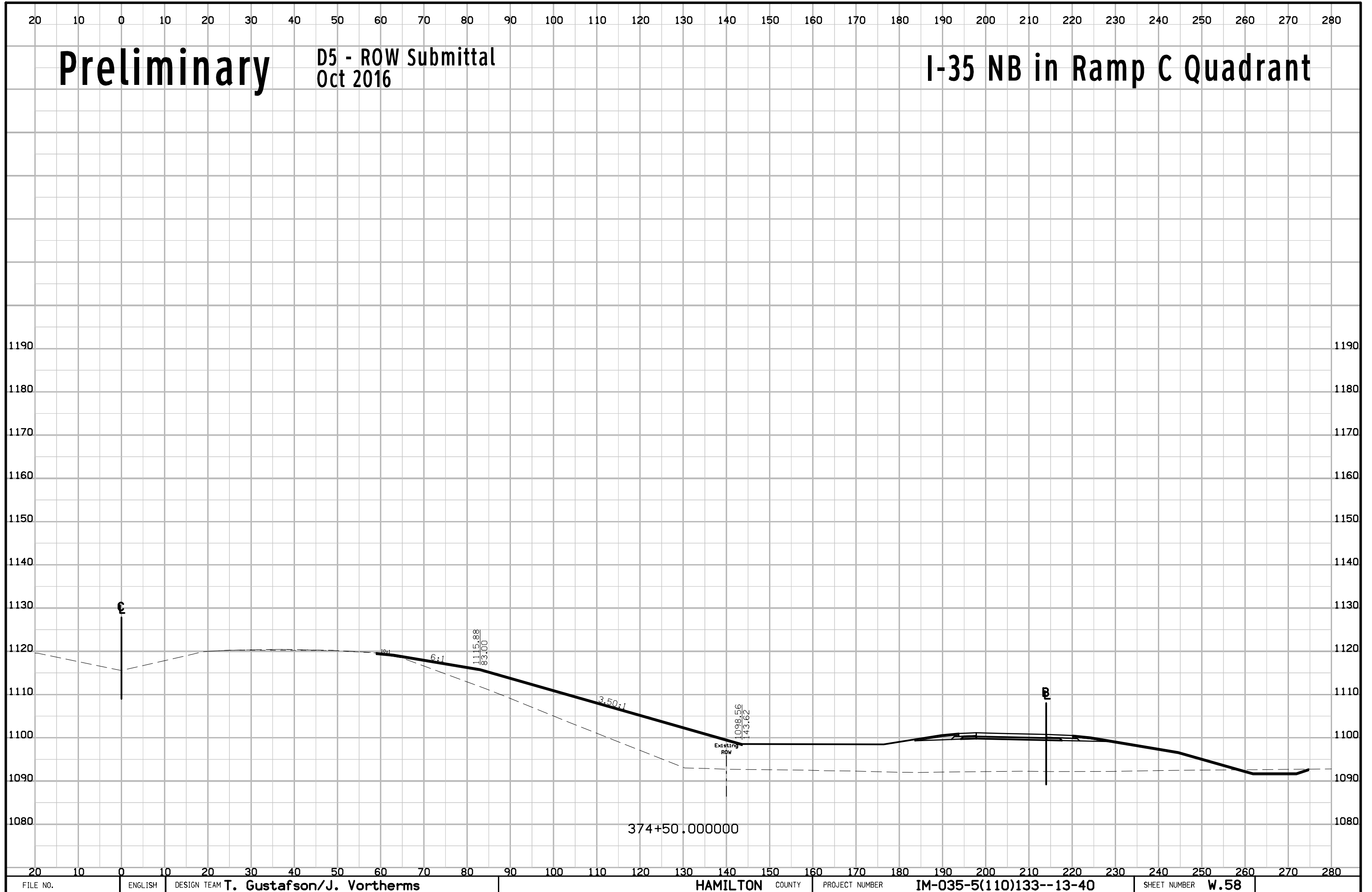
## I-35 NB in Ramp C Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant



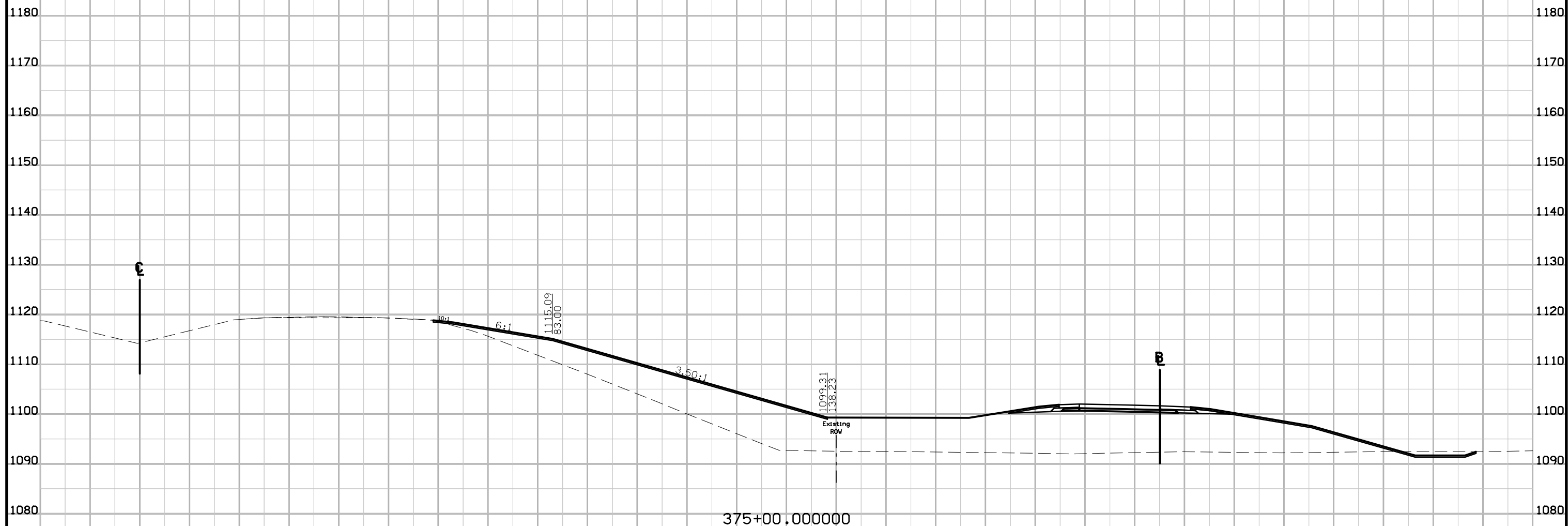
374+50.000000

20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant

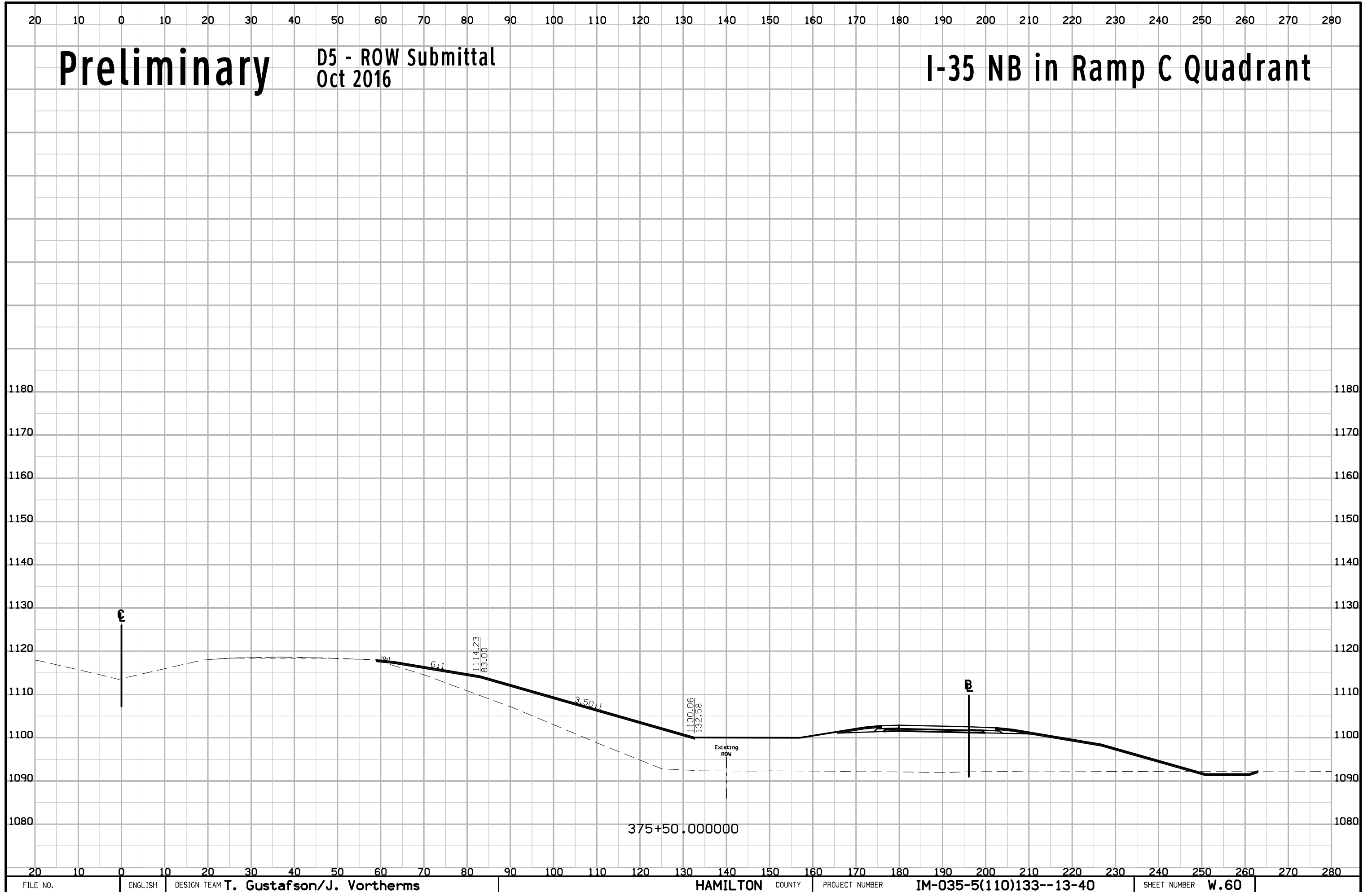


375+00.000000

# Preliminary

D5 - ROW Submittal  
Oct 2016

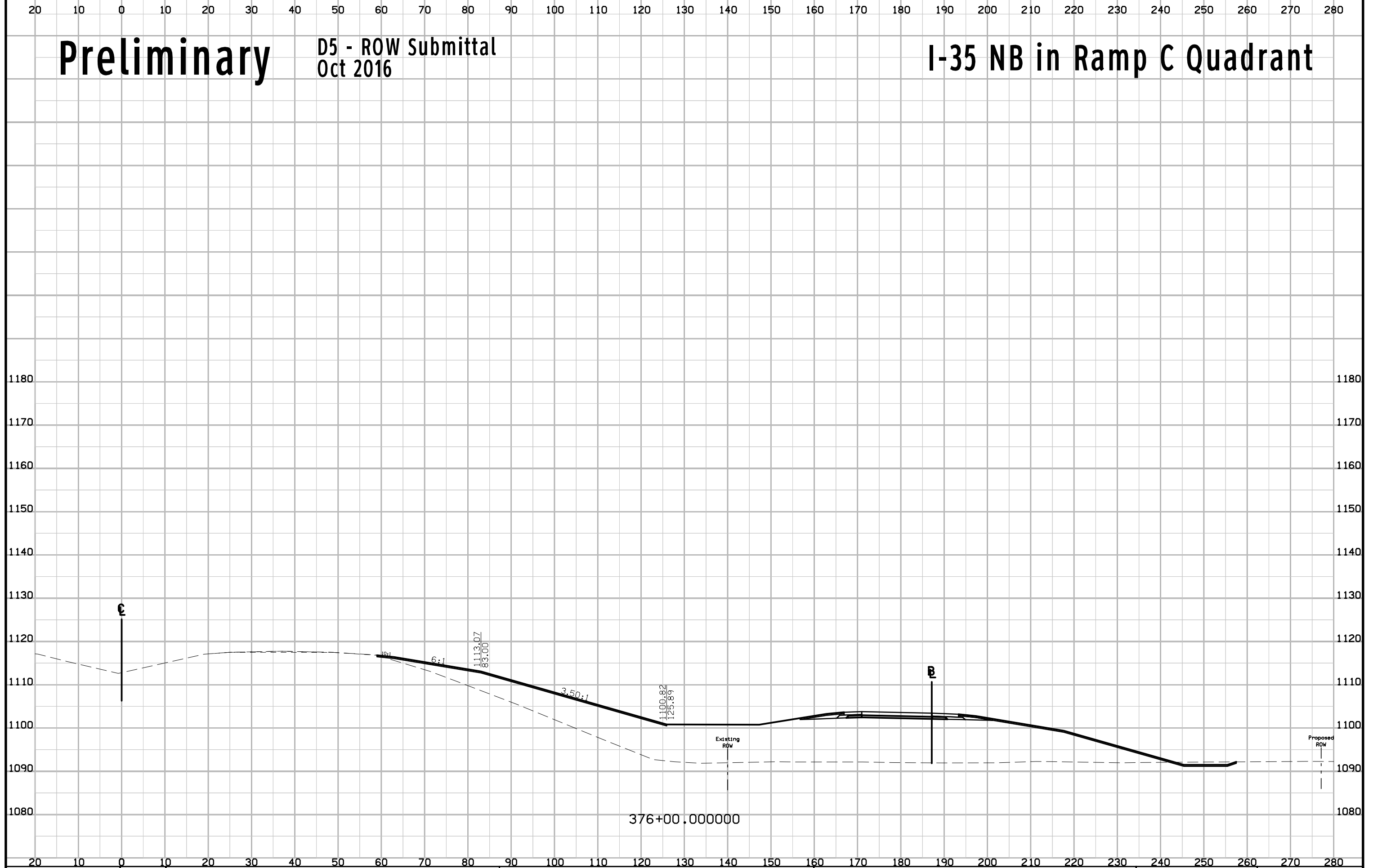
## I-35 NB in Ramp C Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

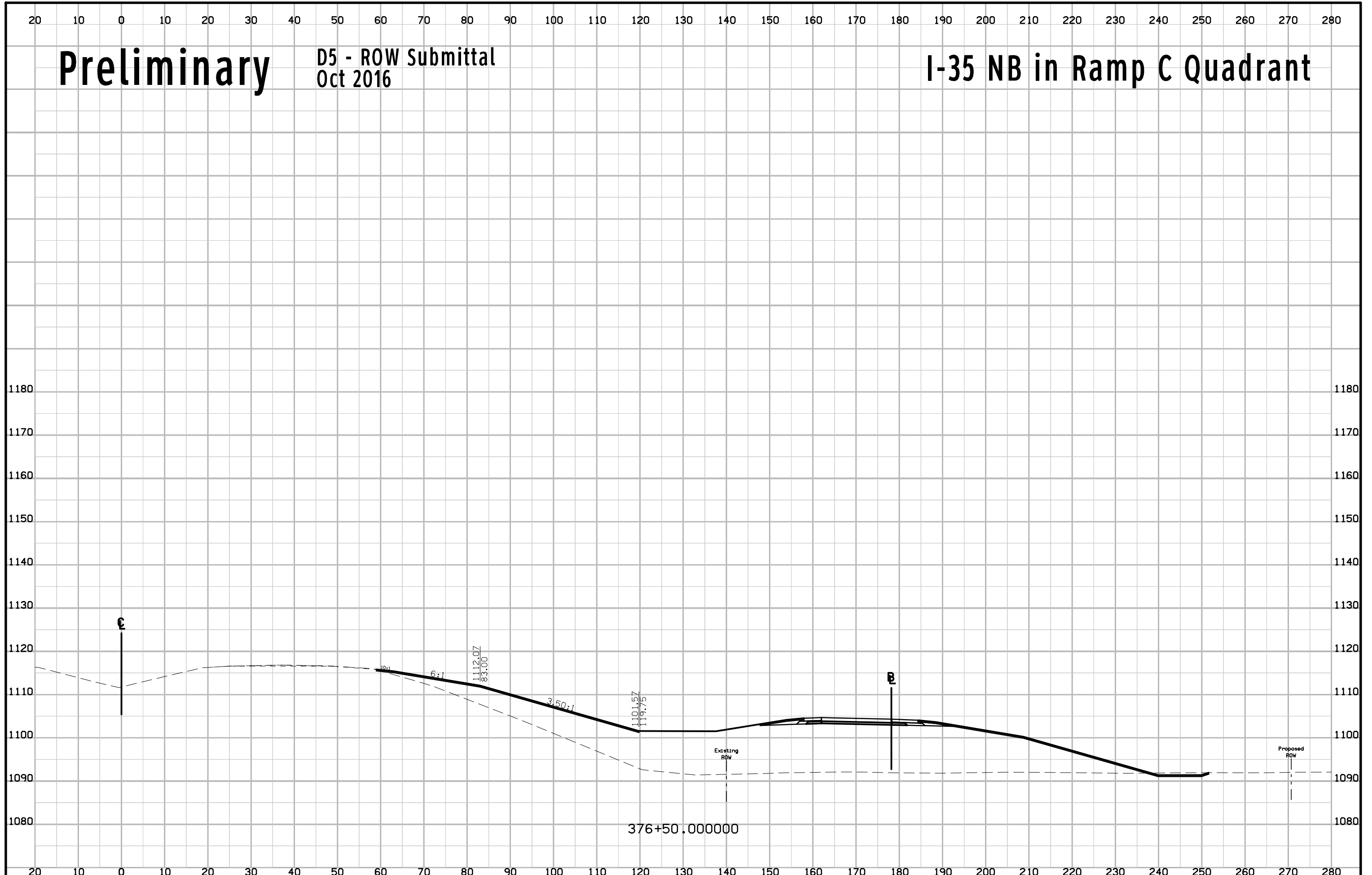
## I-35 NB in Ramp C Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant

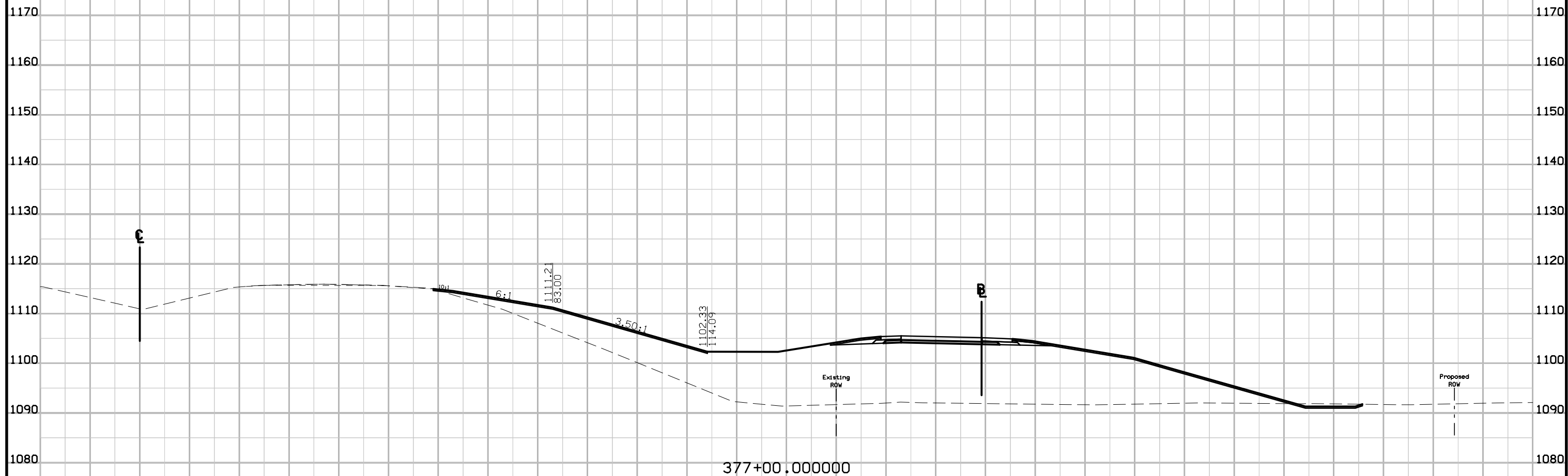


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant



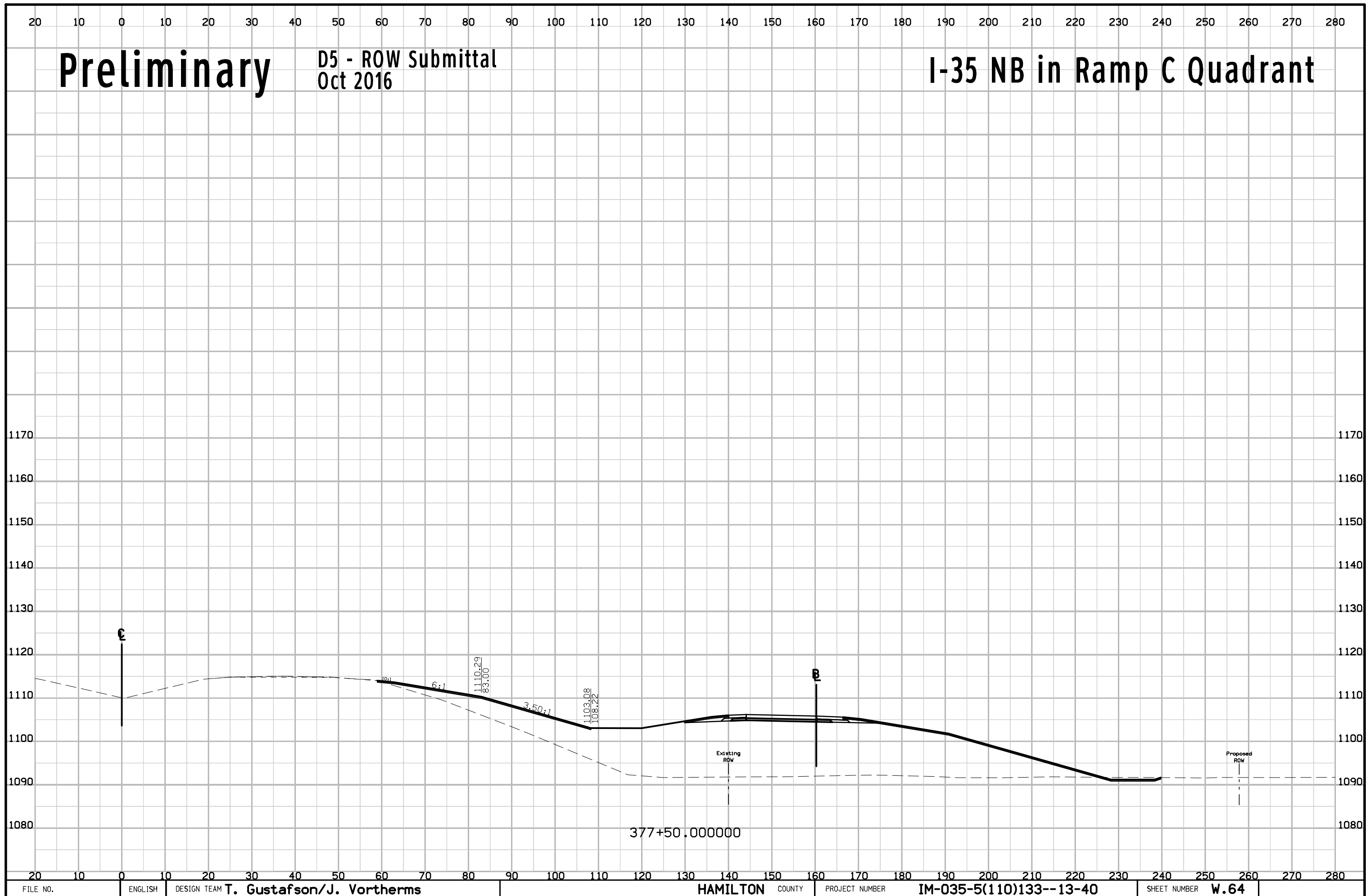
20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant

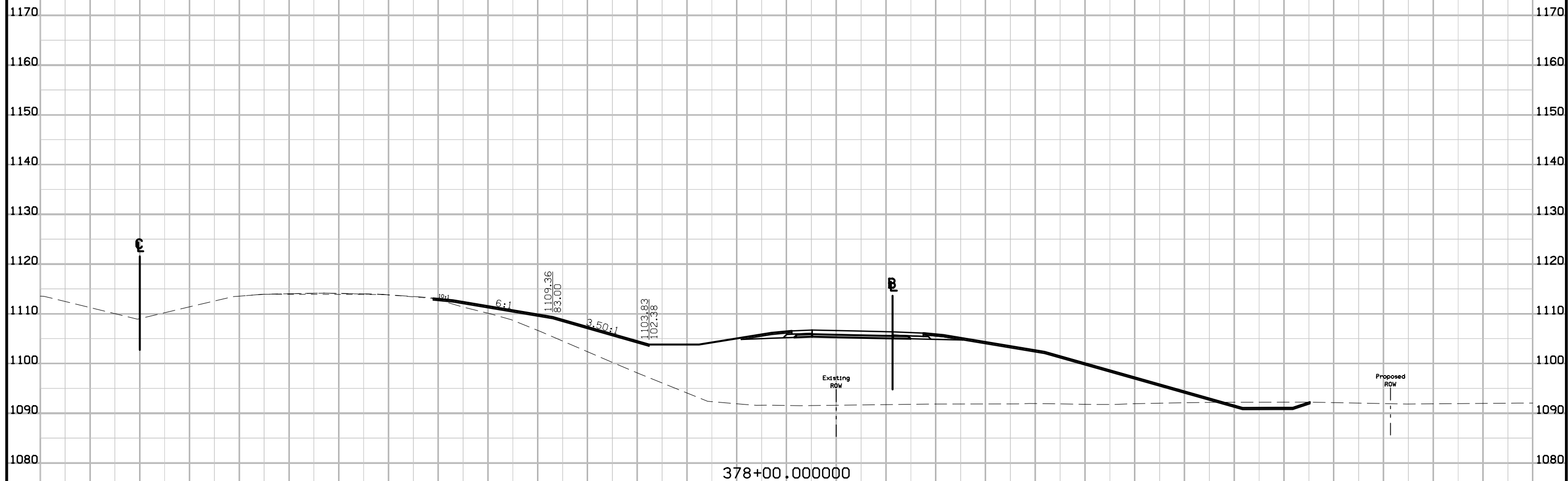


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant

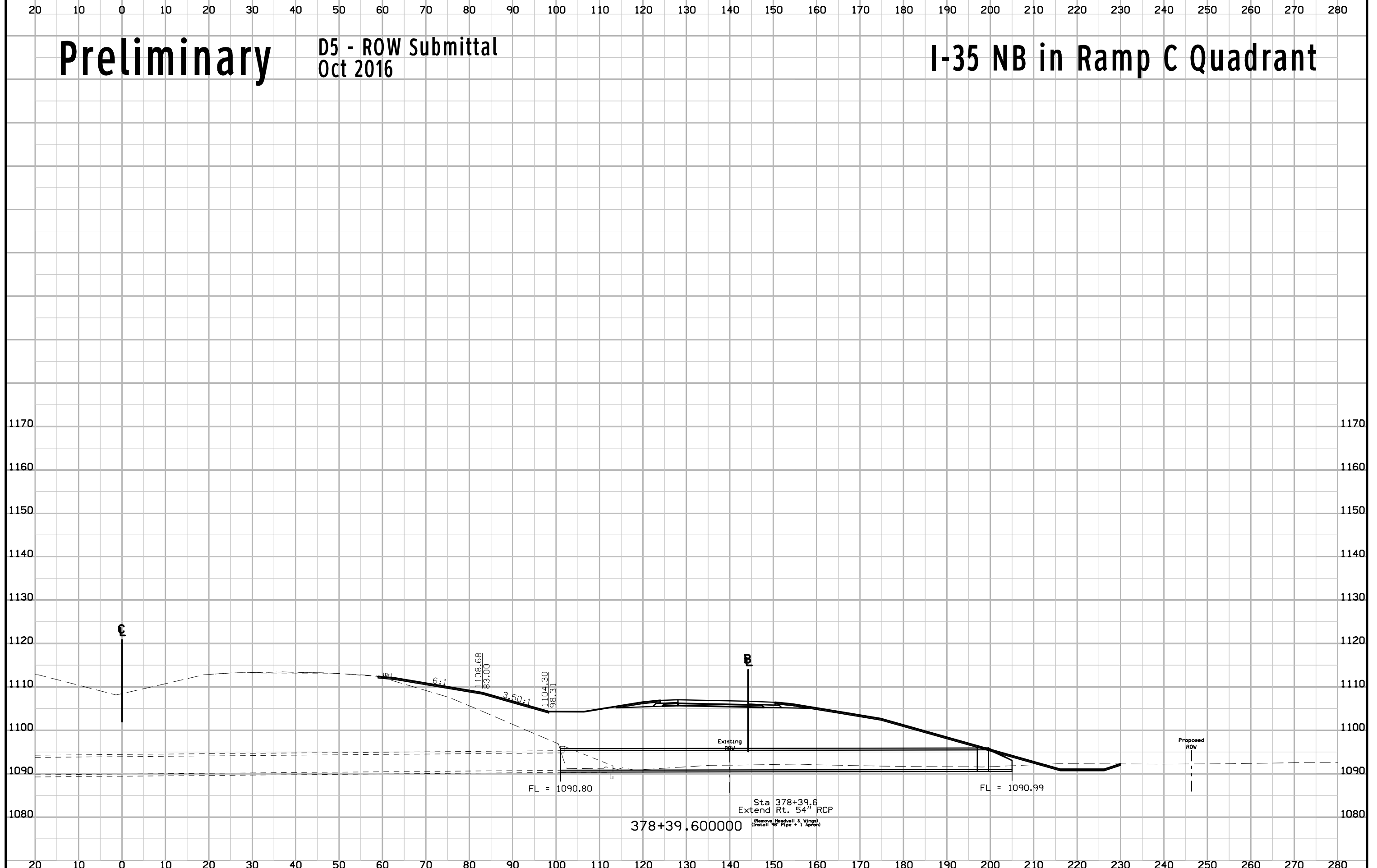


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

# I-35 NB in Ramp C Quadrant

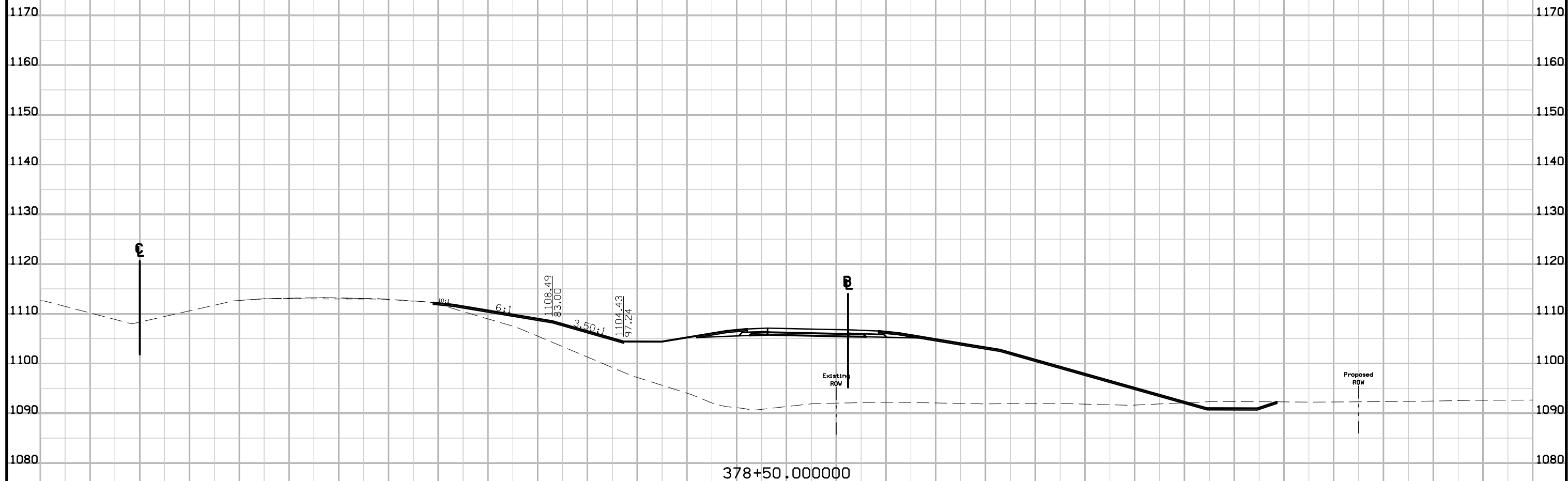


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

# I-35 NB in Ramp C Quadrant



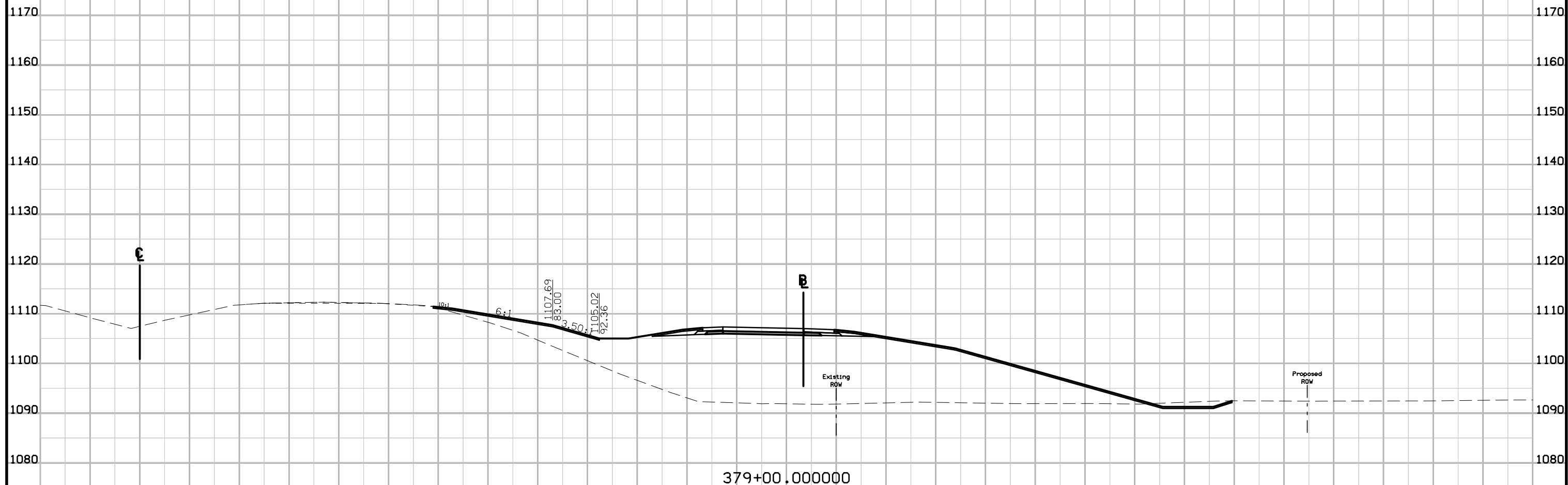
20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant



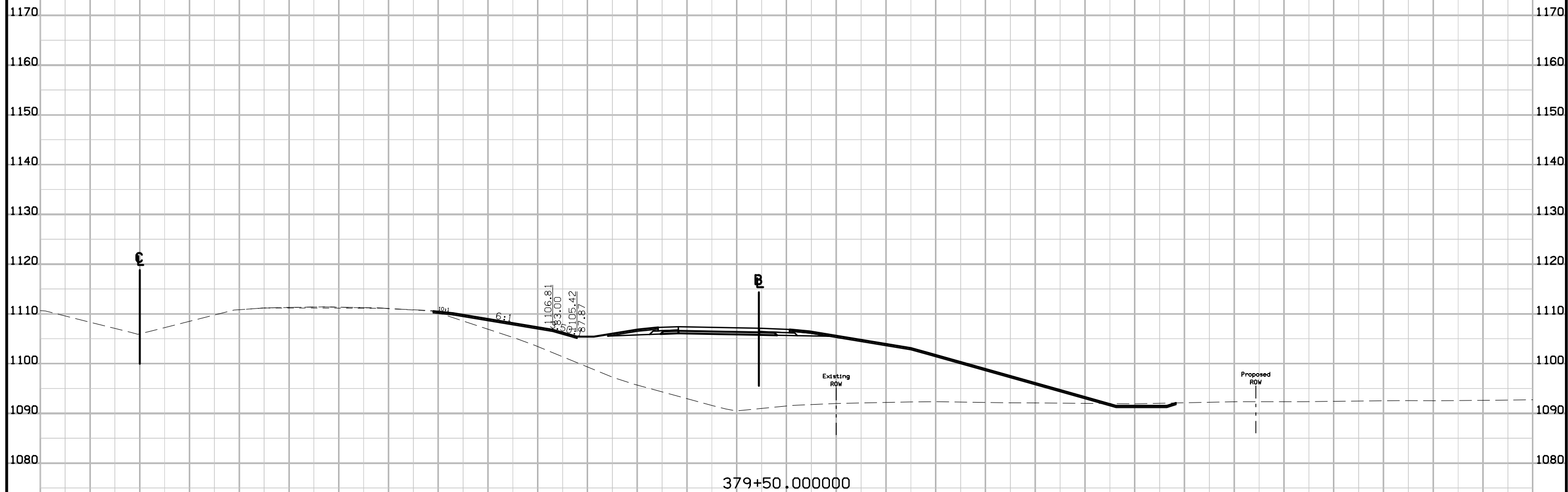
20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant



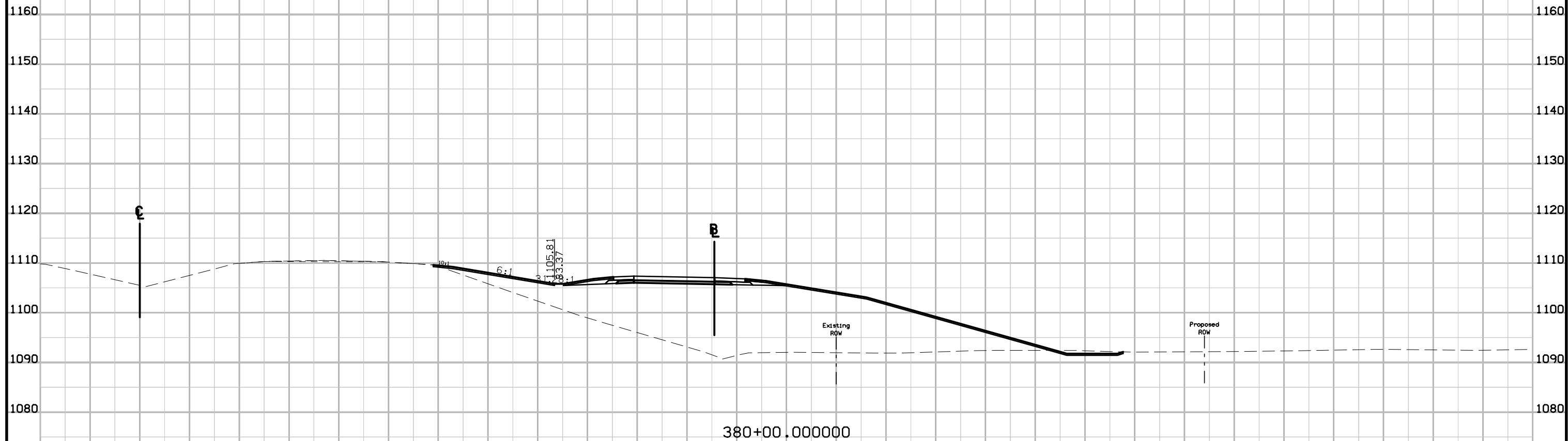
20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

# I-35 NB in Ramp C Quadrant

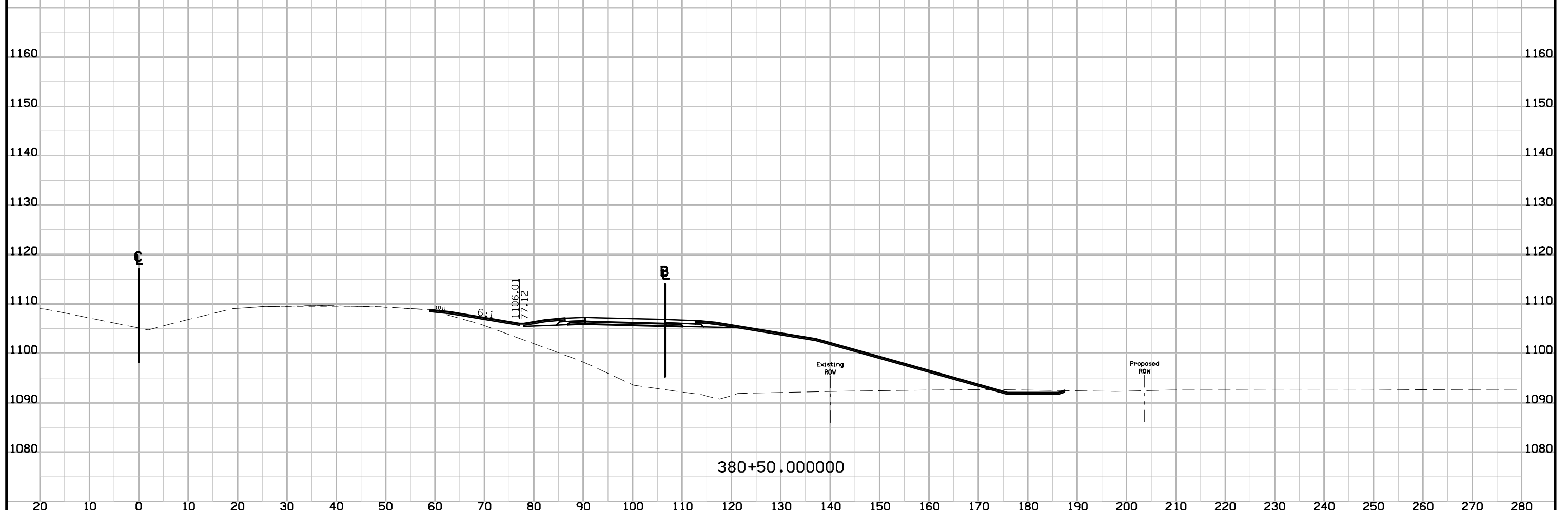
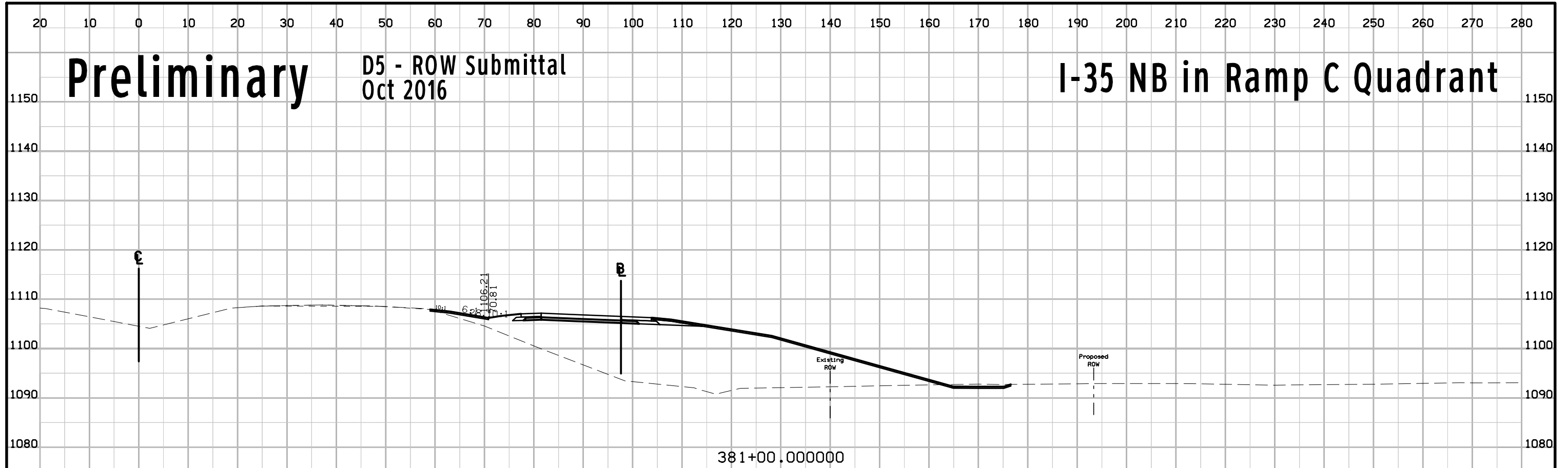


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant

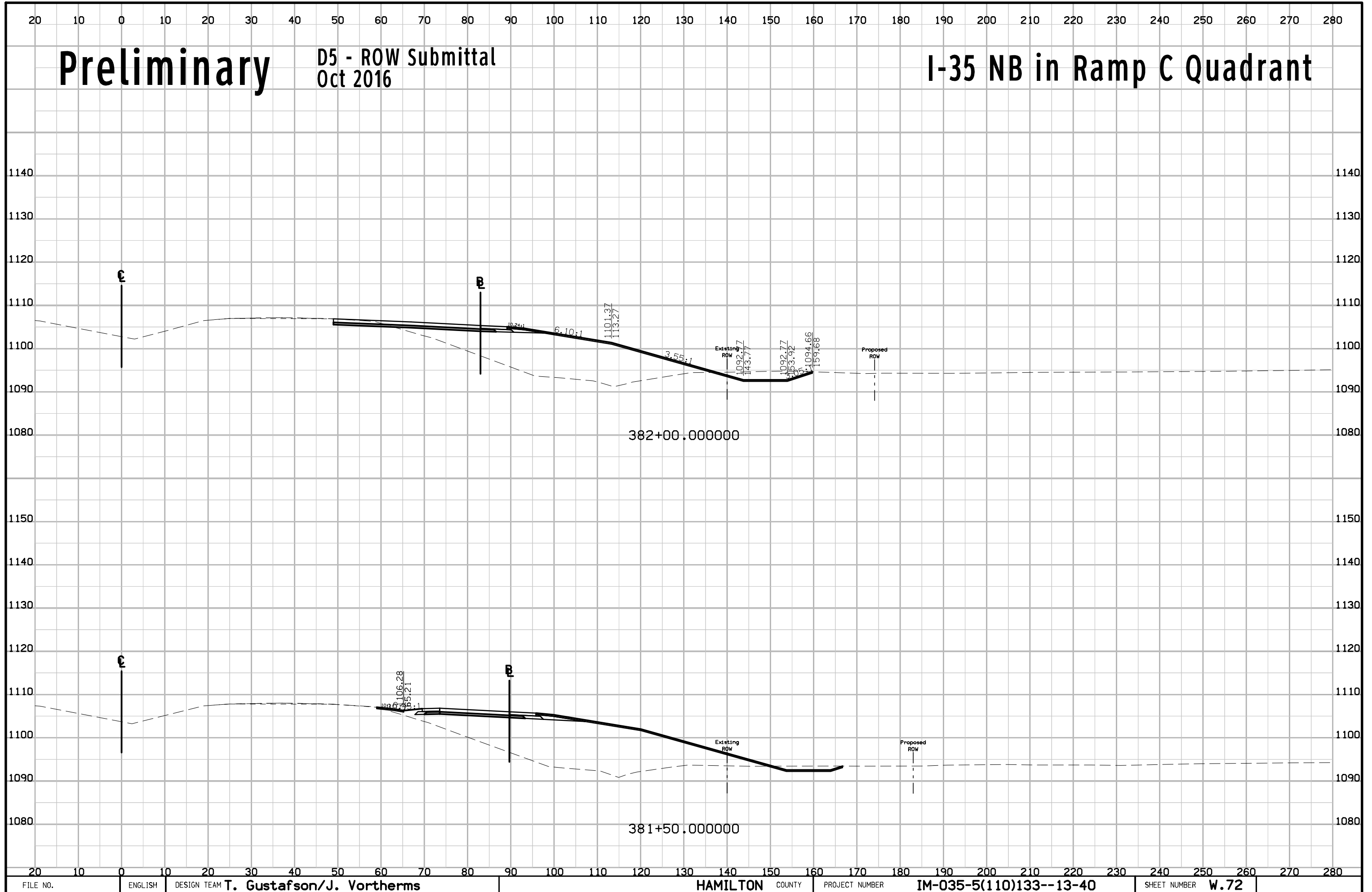




Preliminary

D5 - ROW Submittal  
Oct 2016

I-35 NB in Ramp C Quadrant

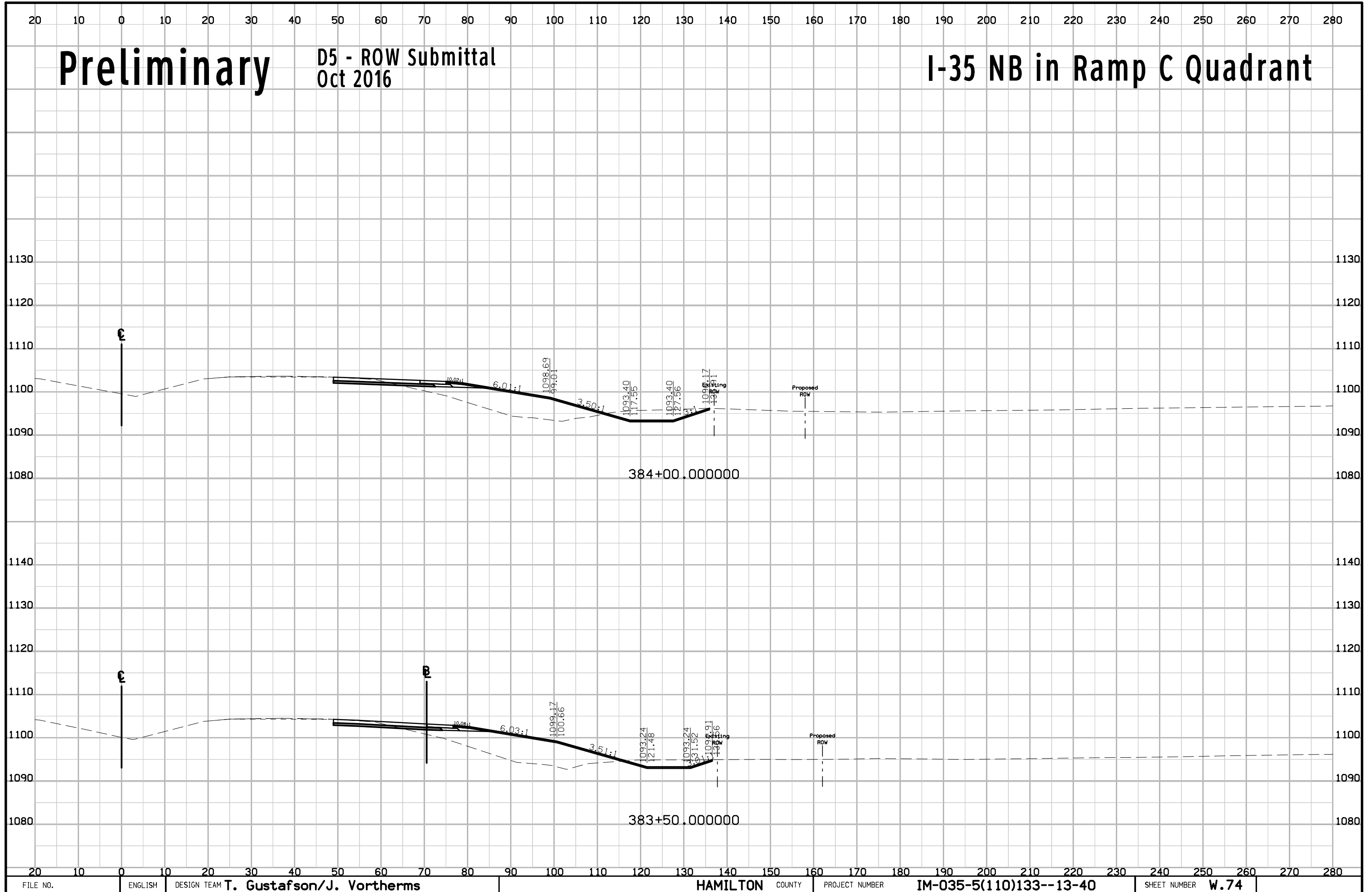




# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant

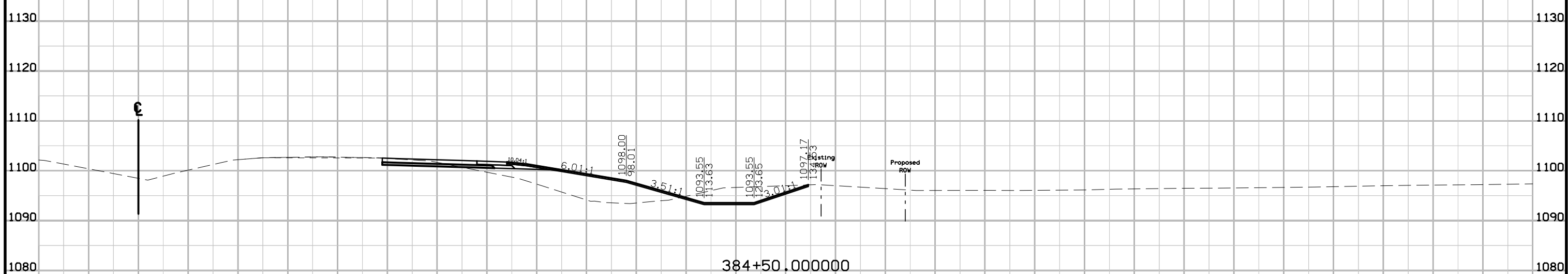
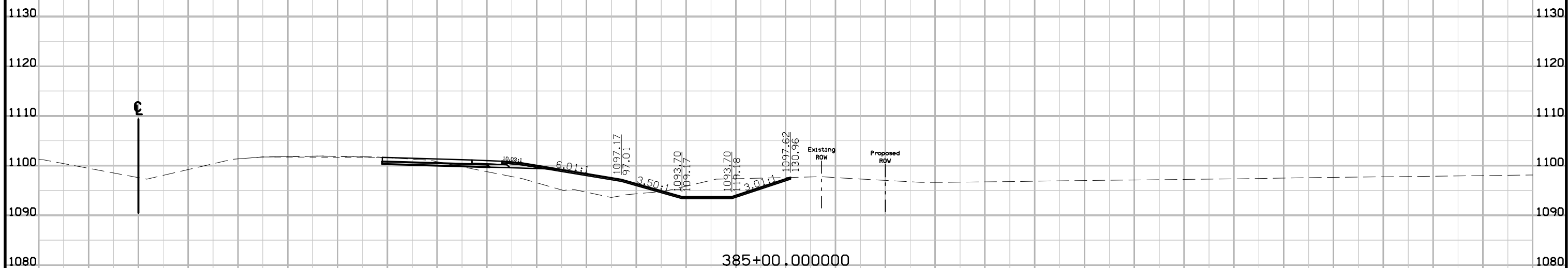


# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant

20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

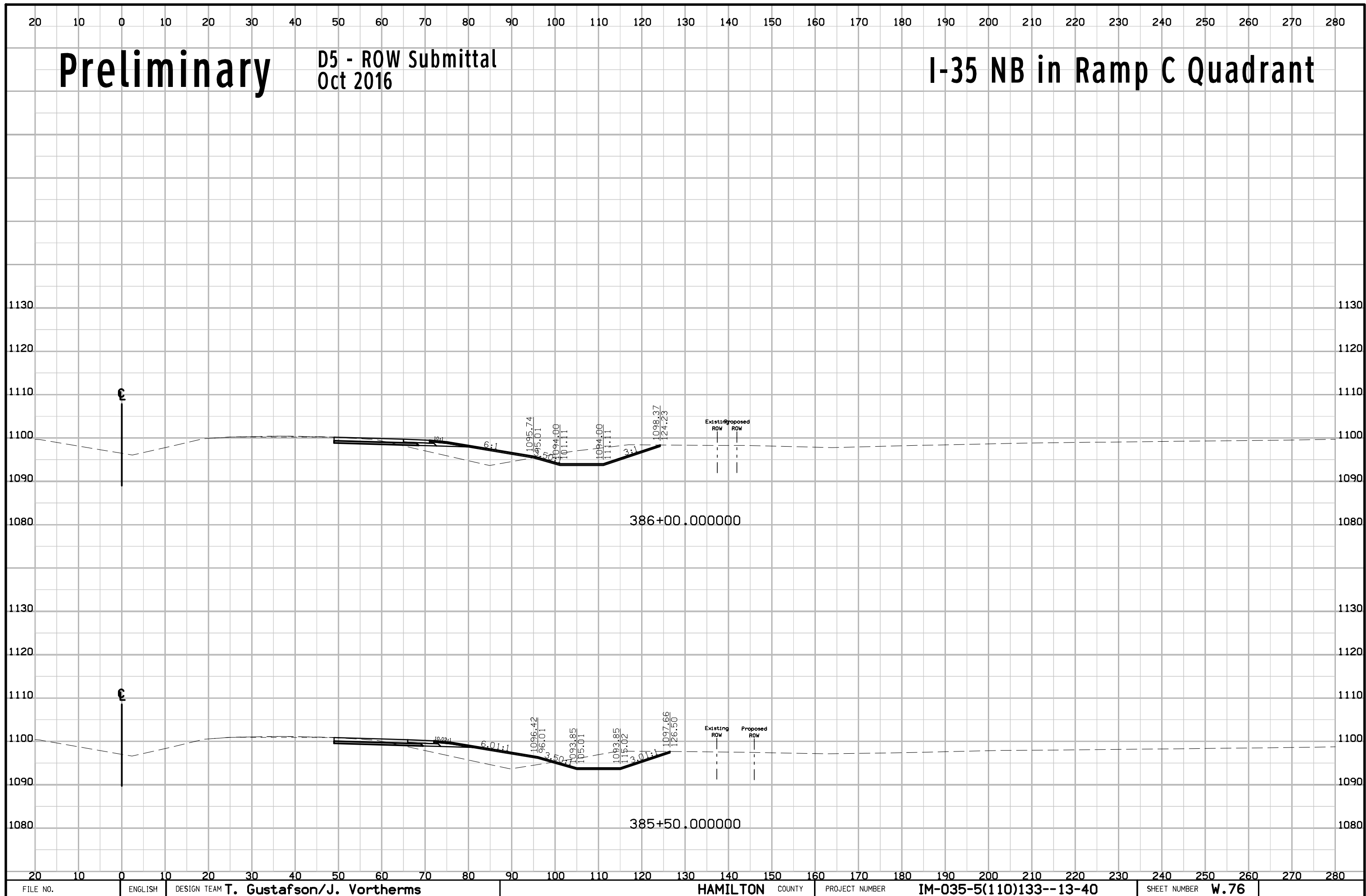


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

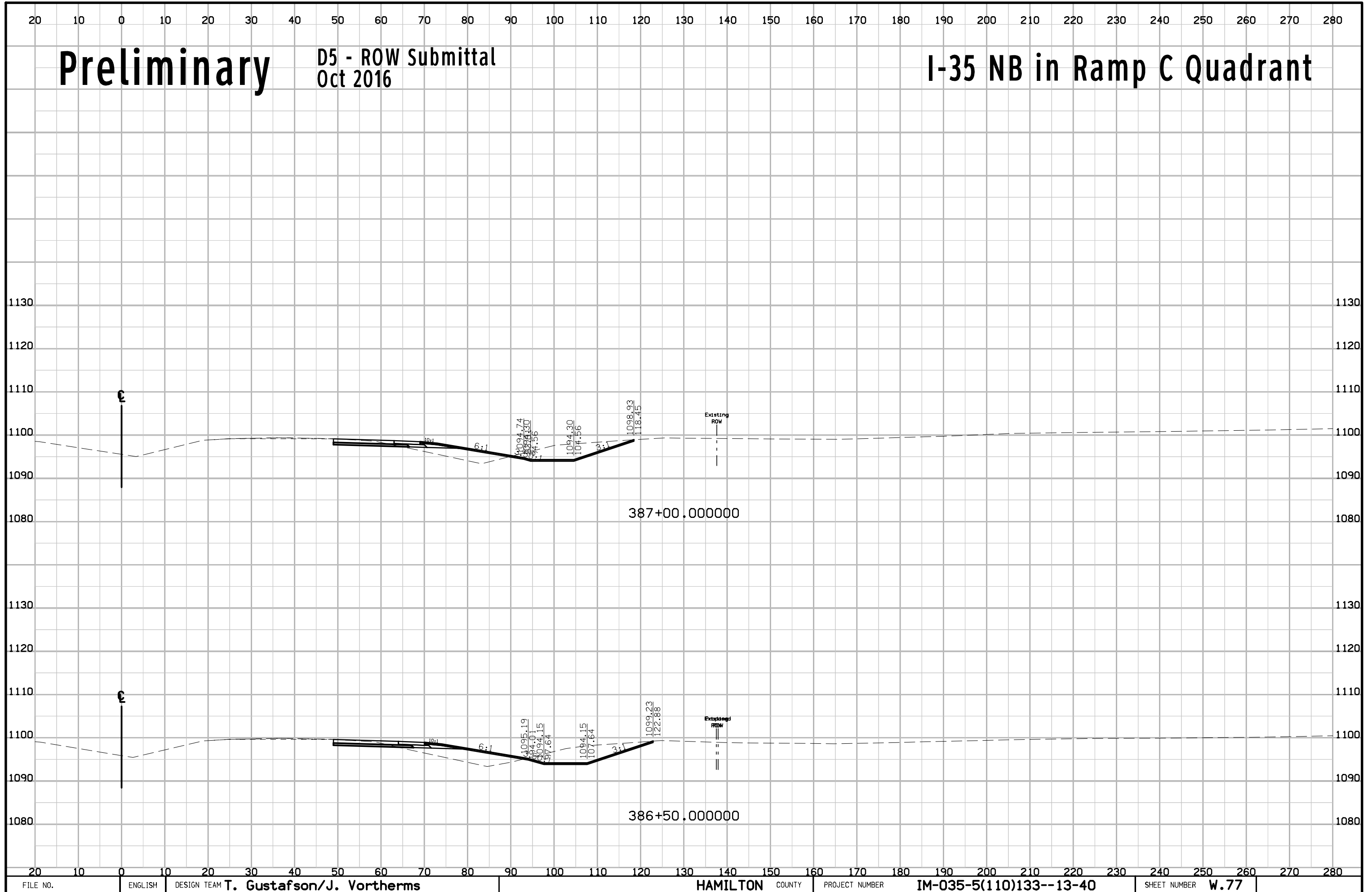
## I-35 NB in Ramp C Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

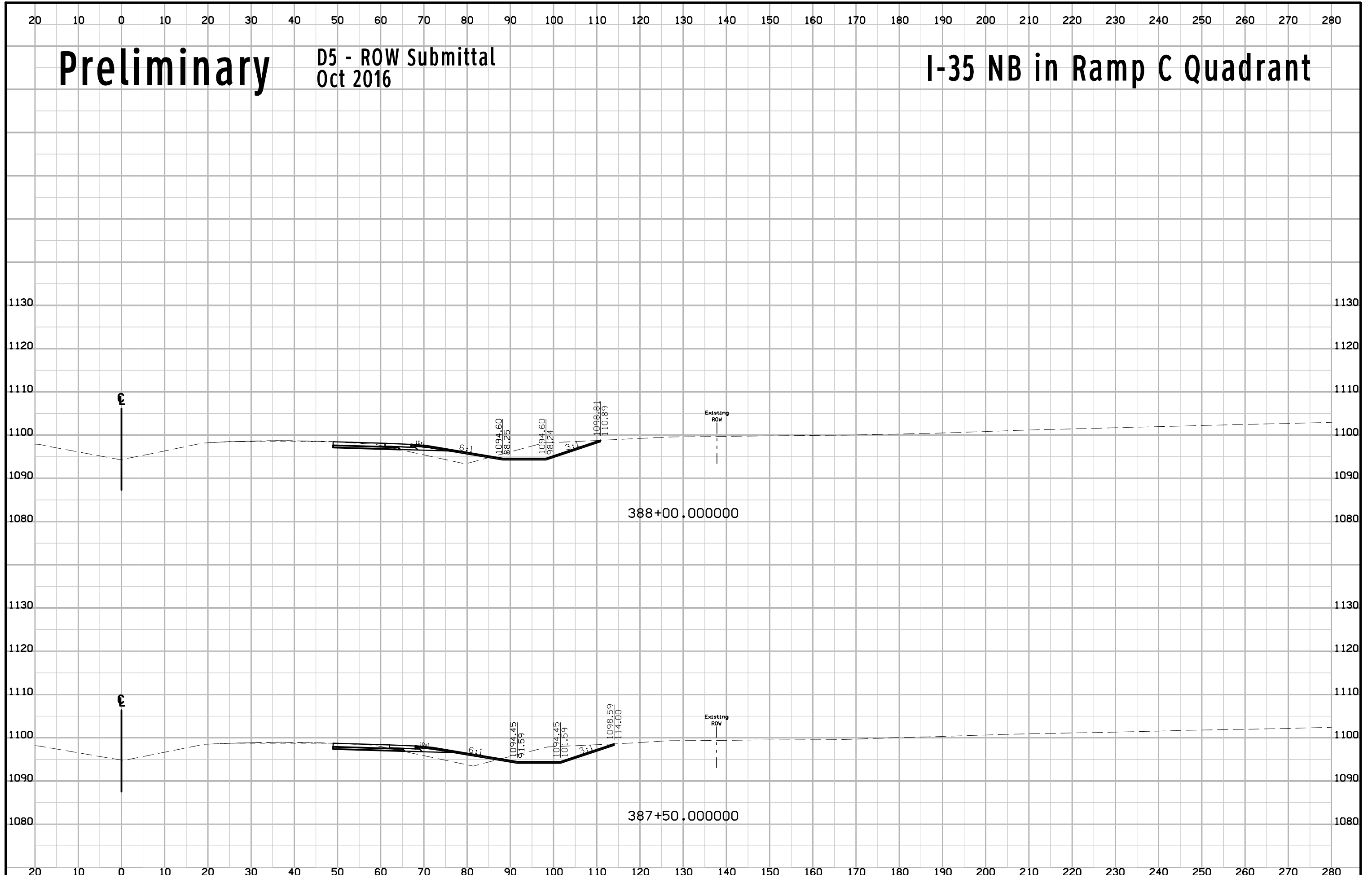
## I-35 NB in Ramp C Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant





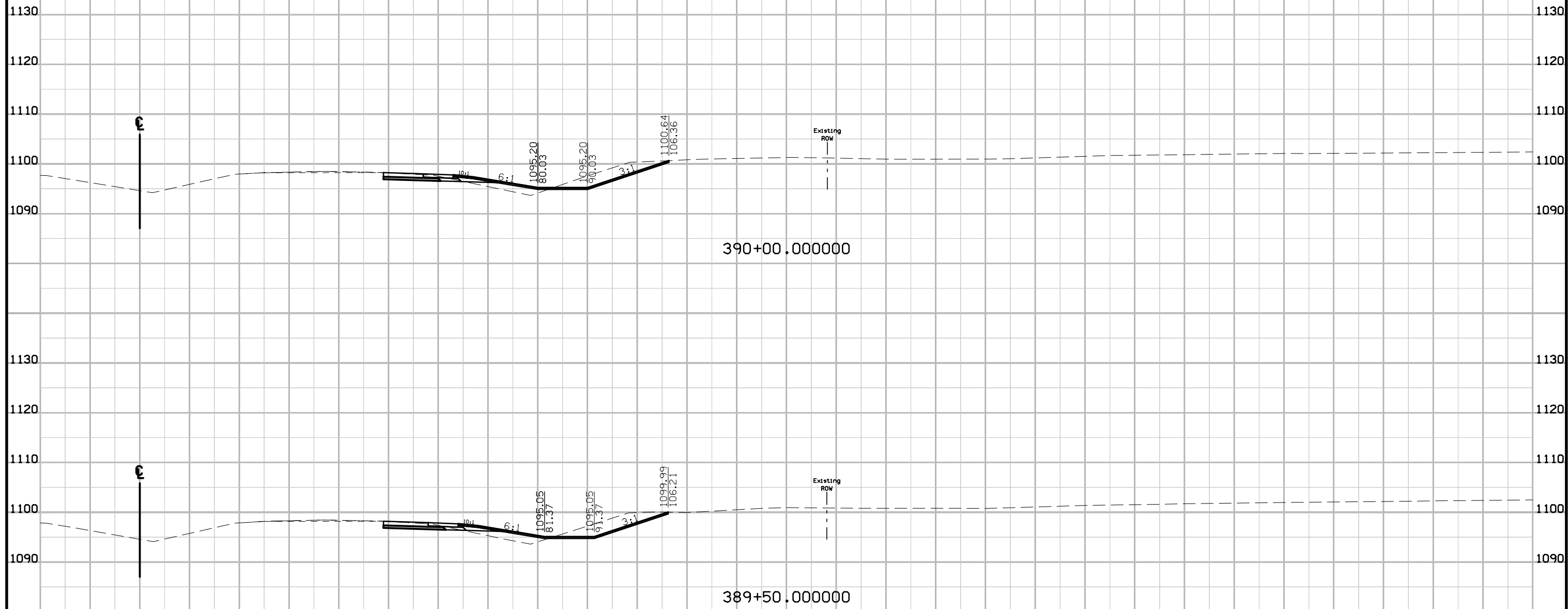


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant

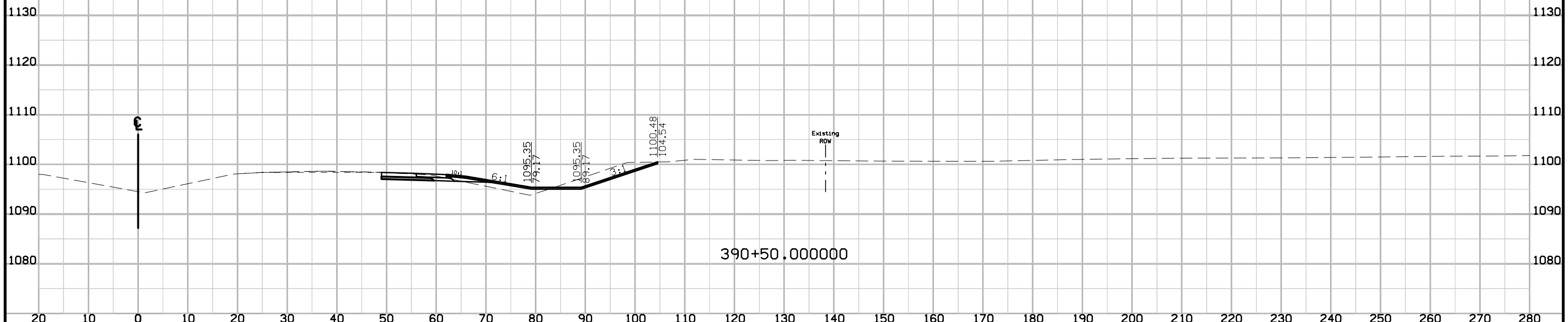


20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280

# Preliminary

D5 - ROW Submittal  
Oct 2016

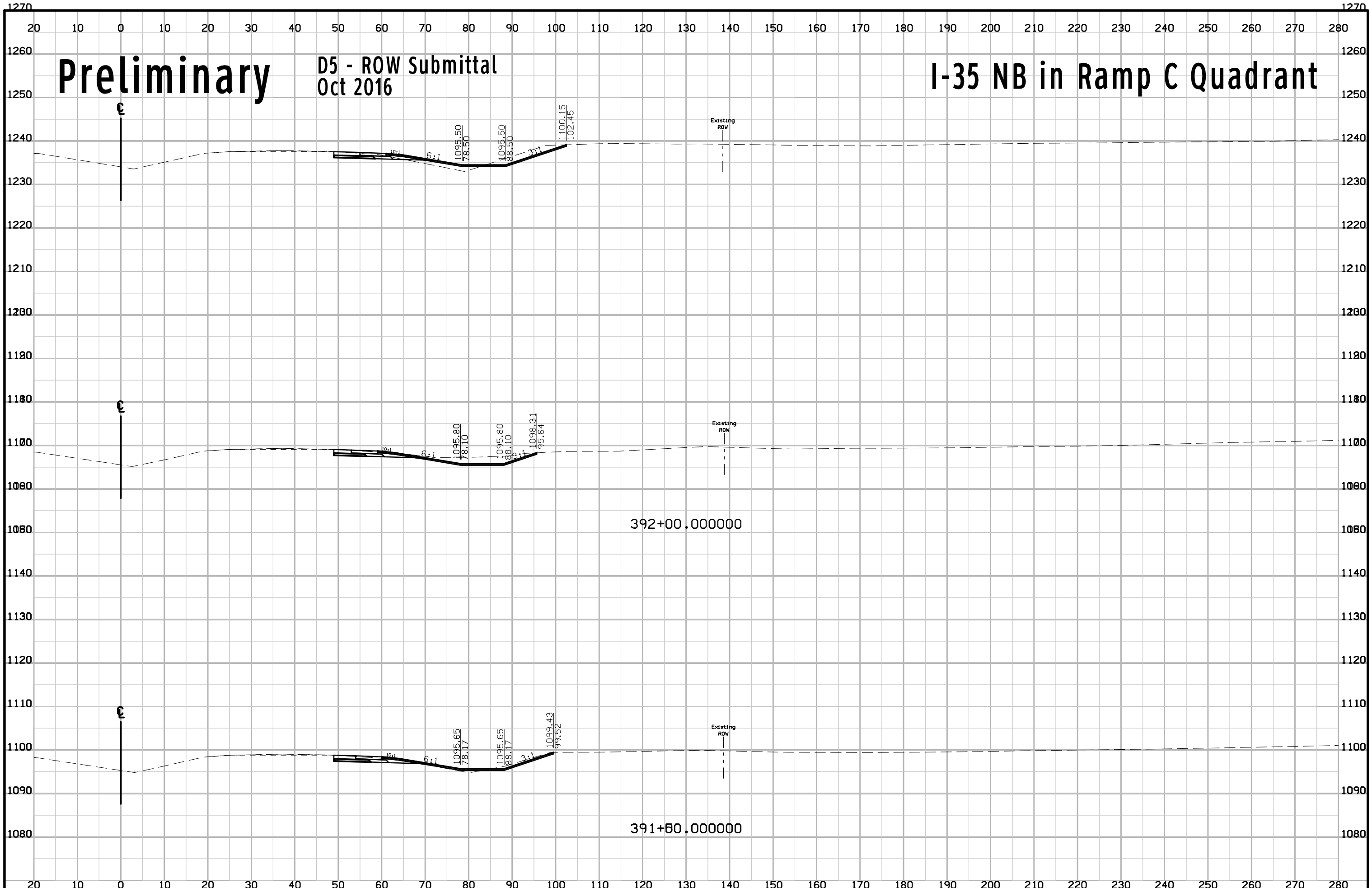
## I-35 NB in Ramp C Quadrant



# Preliminary

## D5 - ROW Submittal Oct 2016

# I-35 NB in Ramp C Quadrant



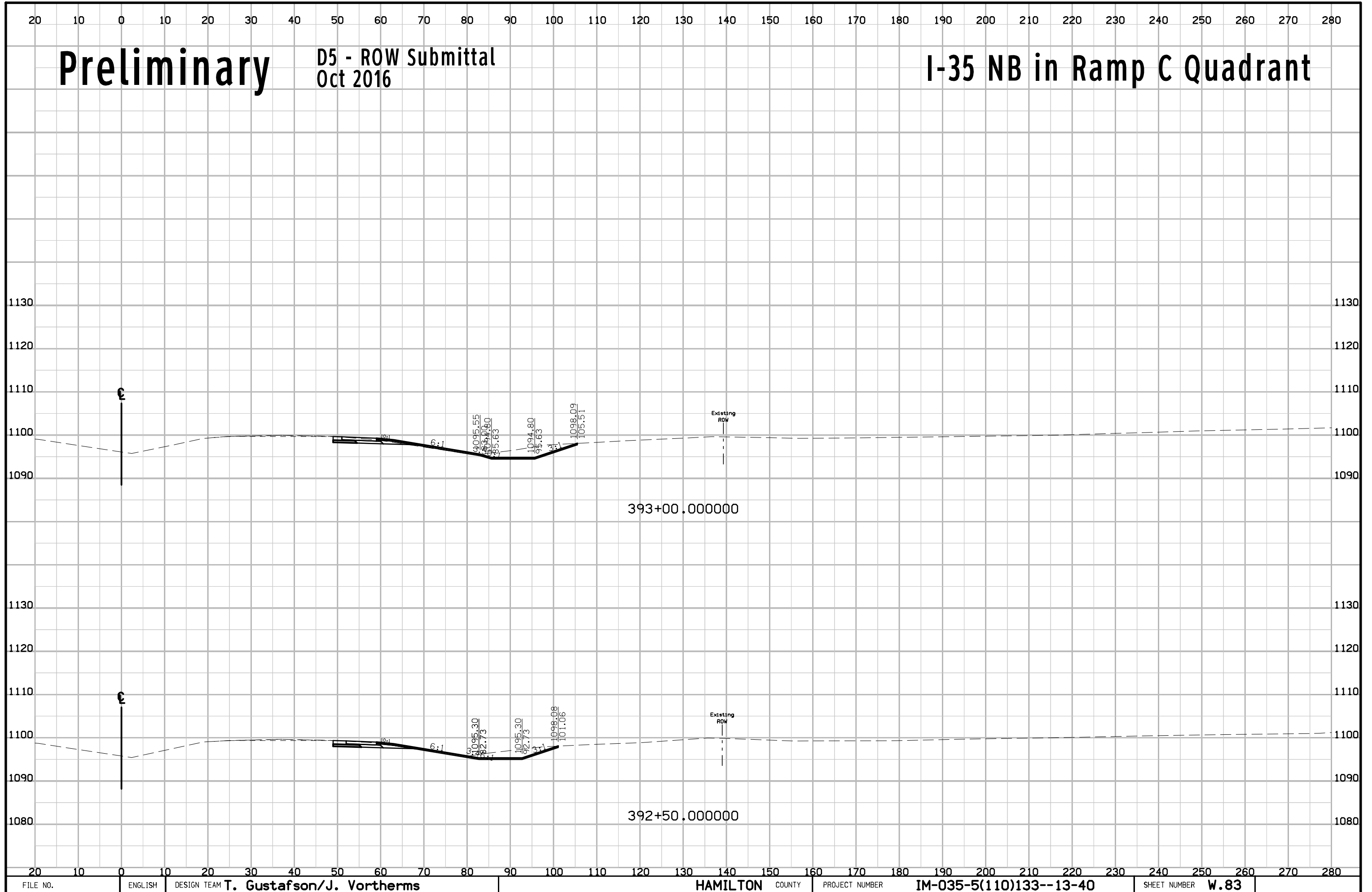
392+00.000000

391+80.000000

# Preliminary

D5 - ROW Submittal  
Oct 2016

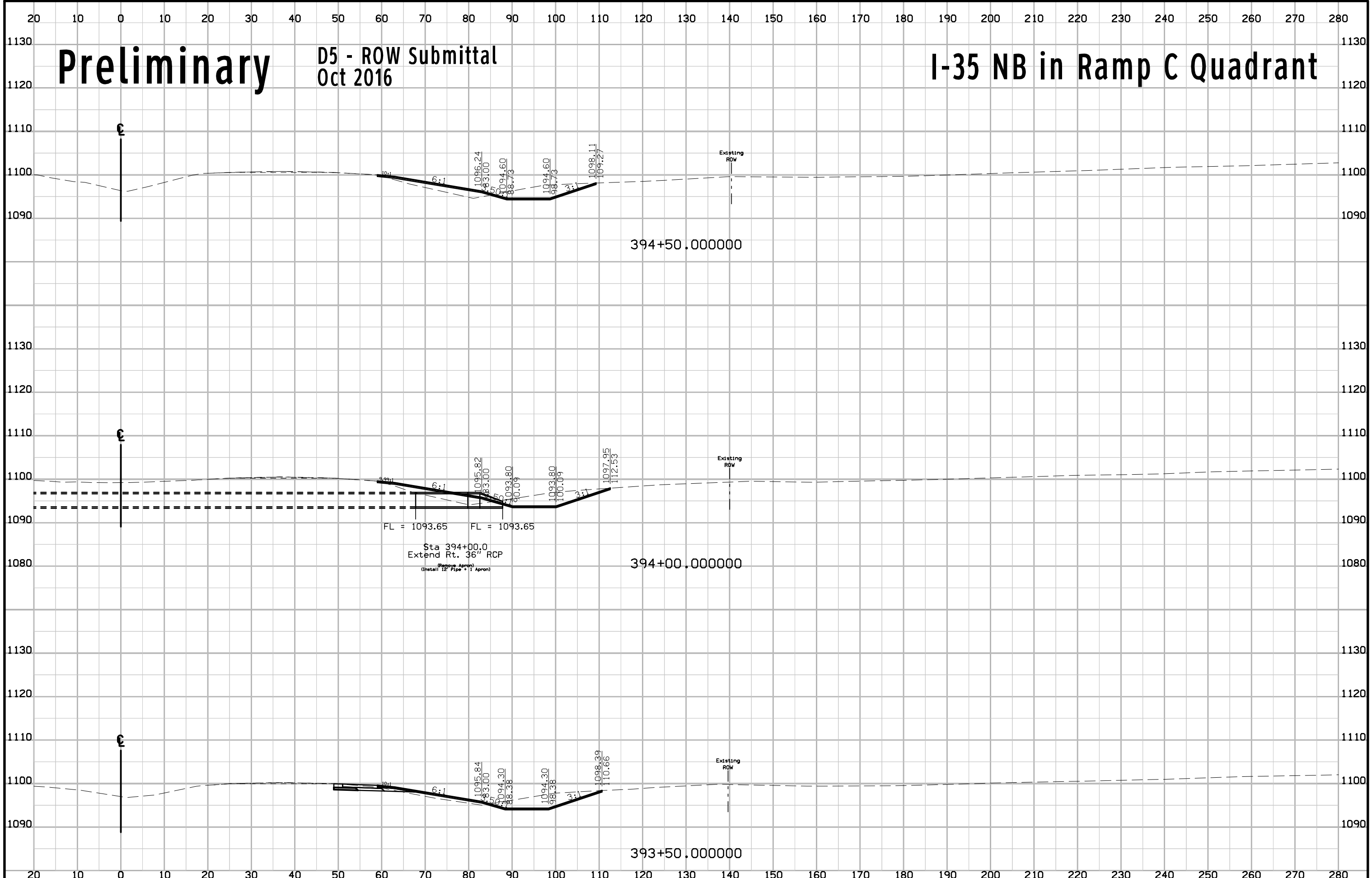
## I-35 NB in Ramp C Quadrant



# Preliminary

## D5 - ROW Submittal Oct 2016

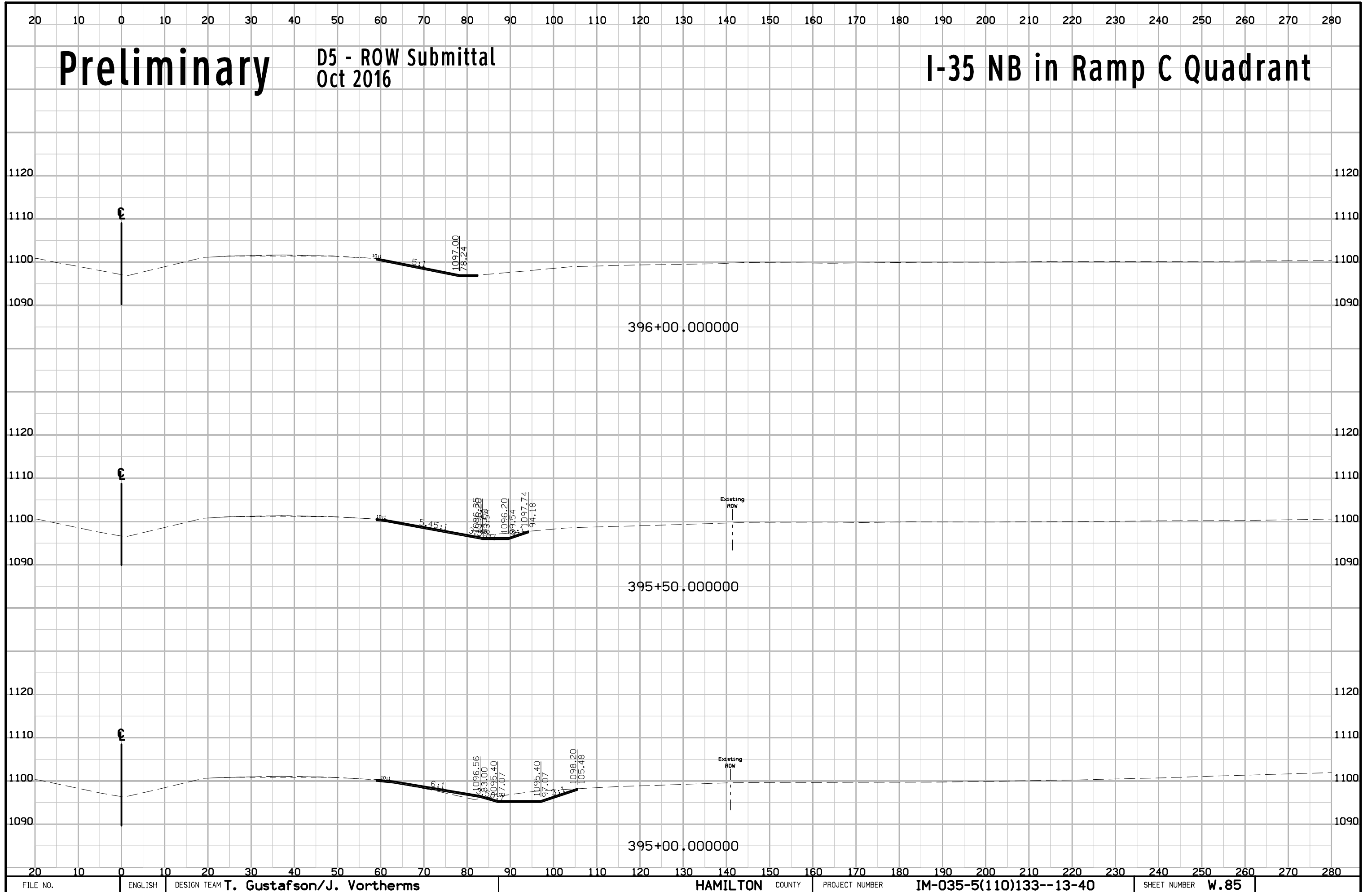
# I-35 NB in Ramp C Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 NB in Ramp C Quadrant

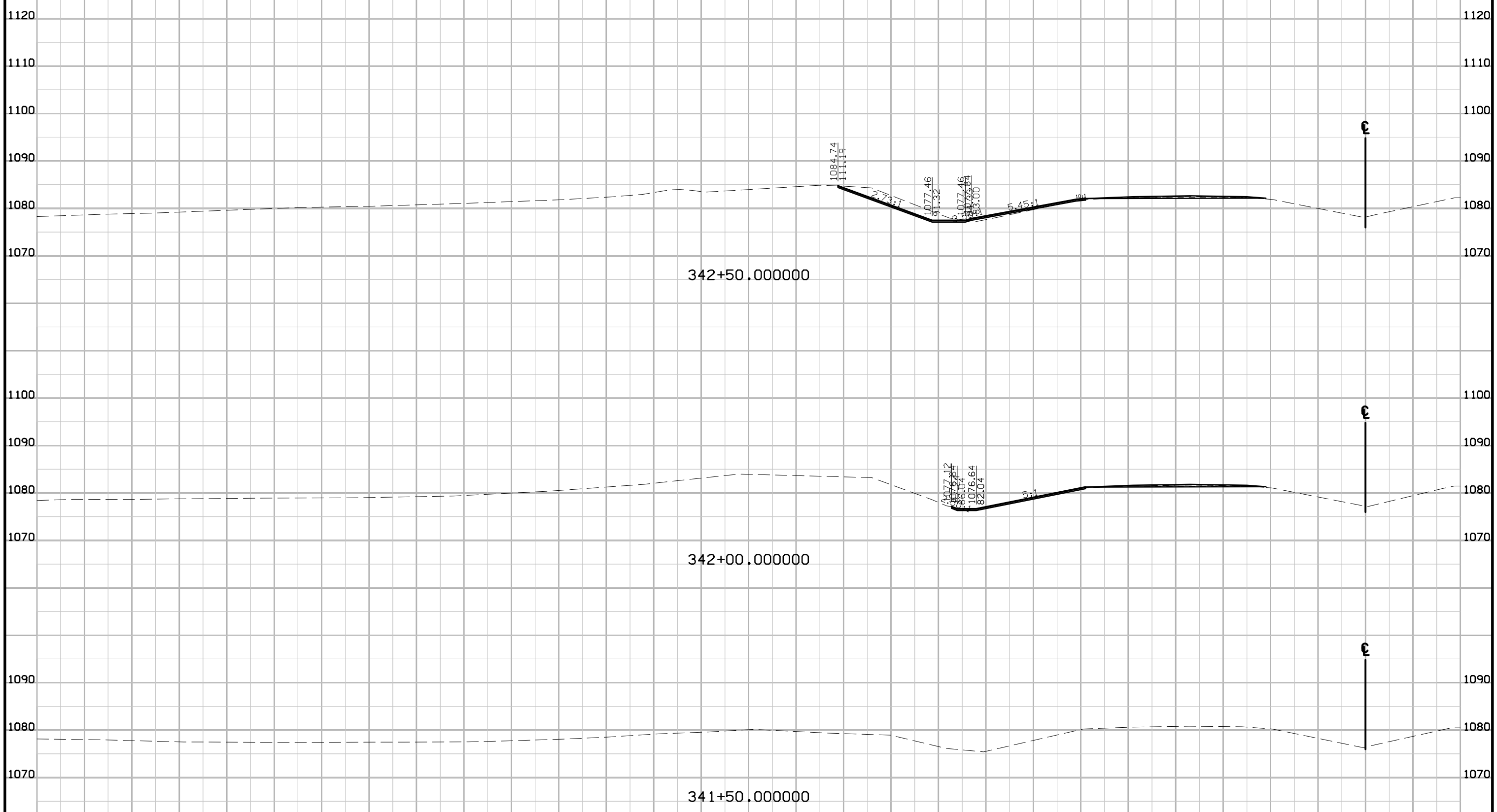


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant

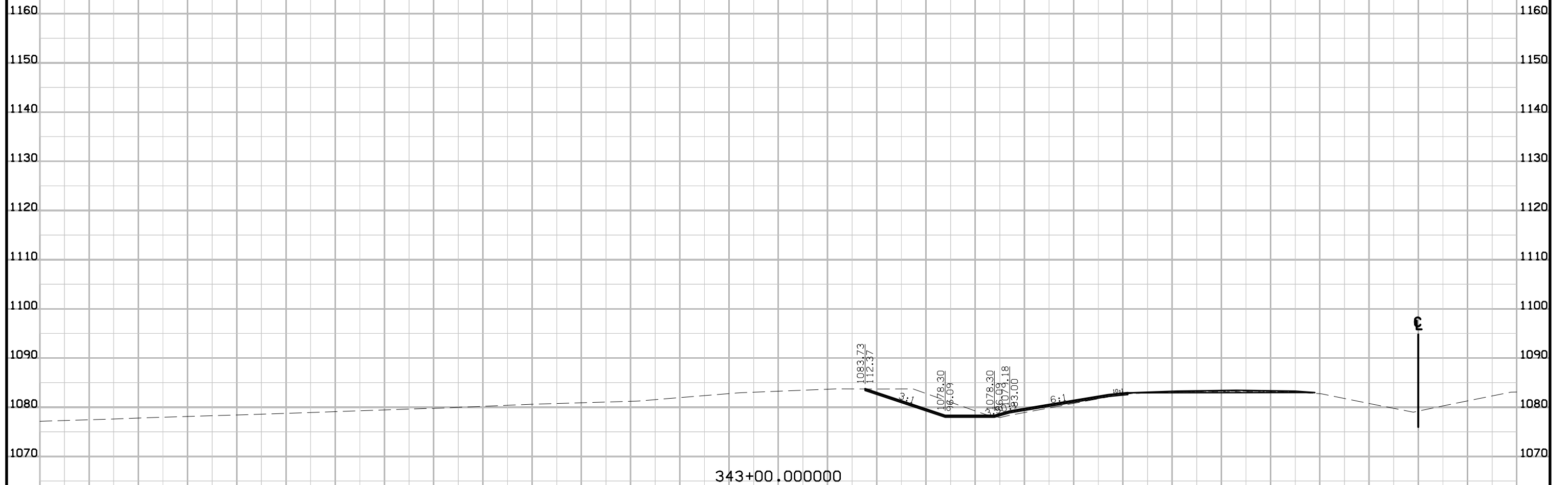


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



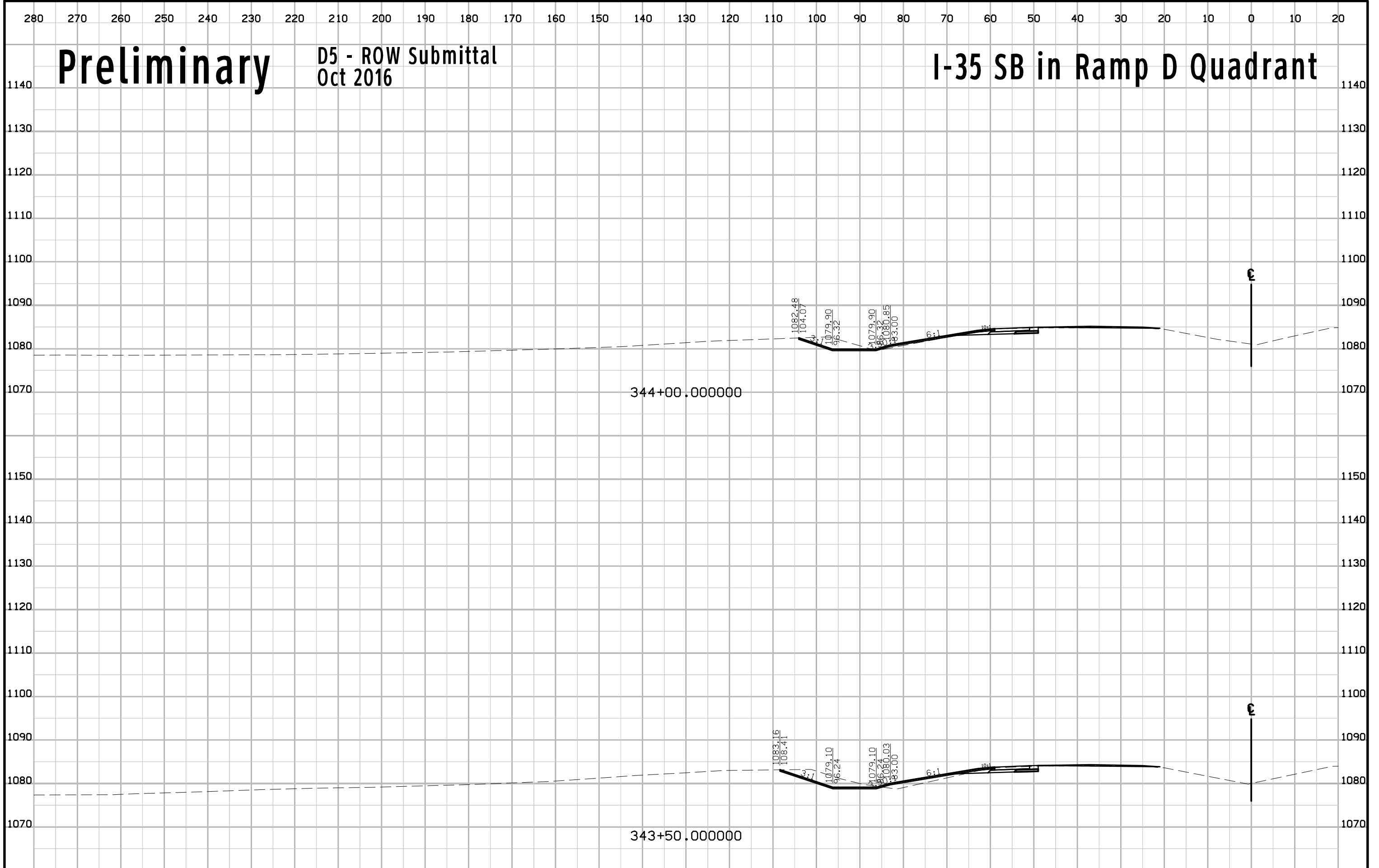
FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	<b>HAMILTON</b> COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.102</b>
----------	---------	--	------------------------	---	---------------------------



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant

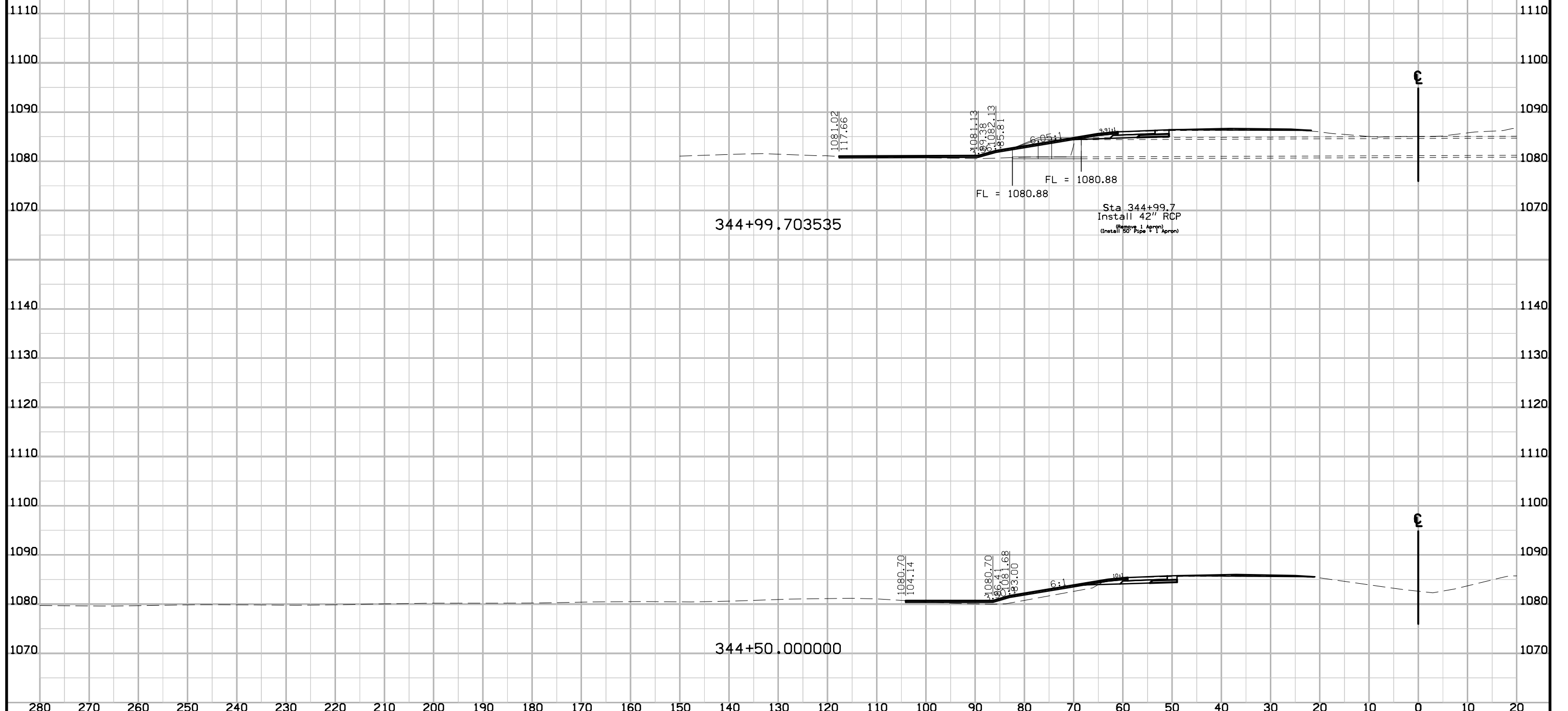


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

## D5 - ROW Submittal Oct 2016

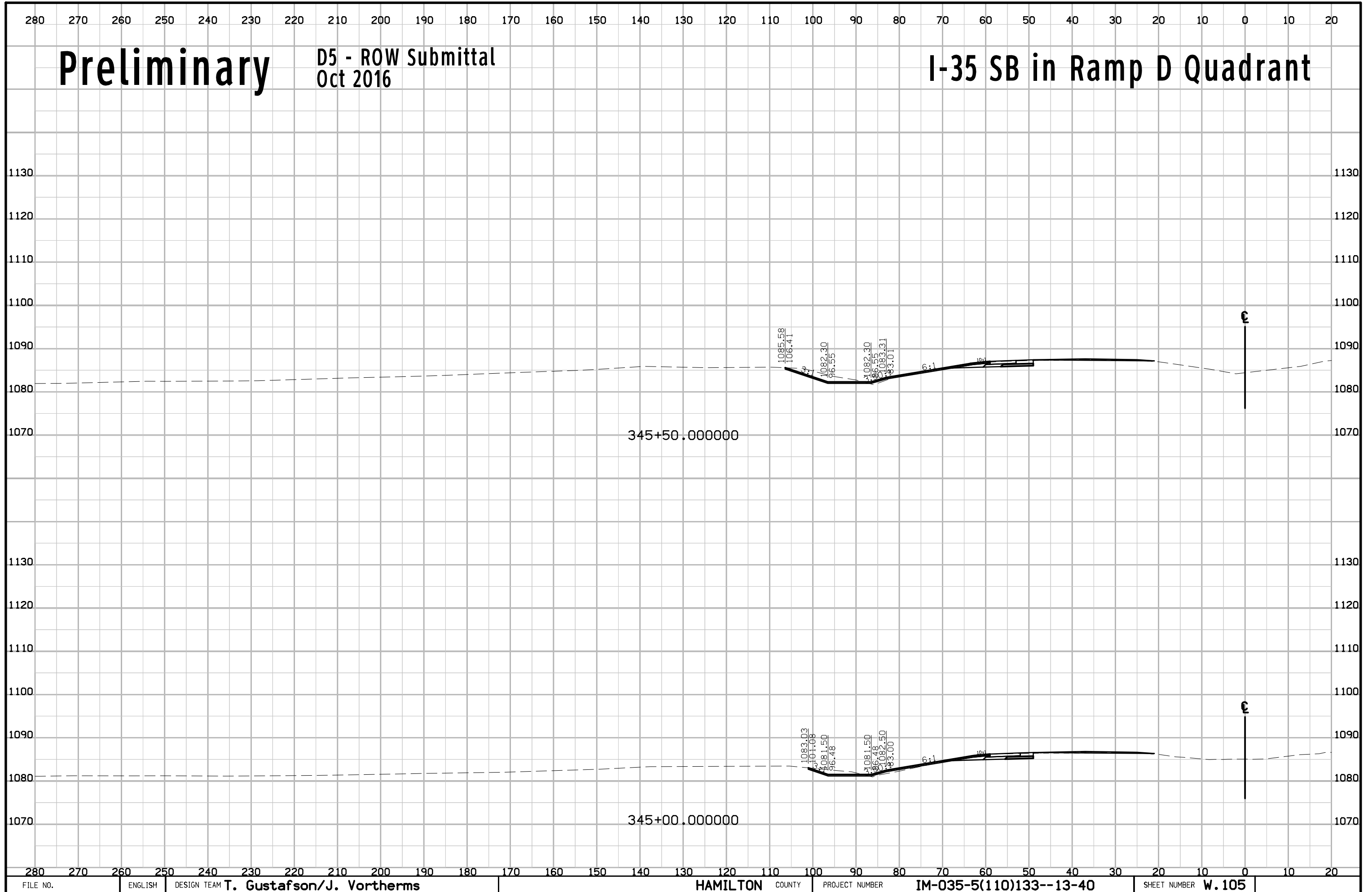
# I-35 SB in Ramp D Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

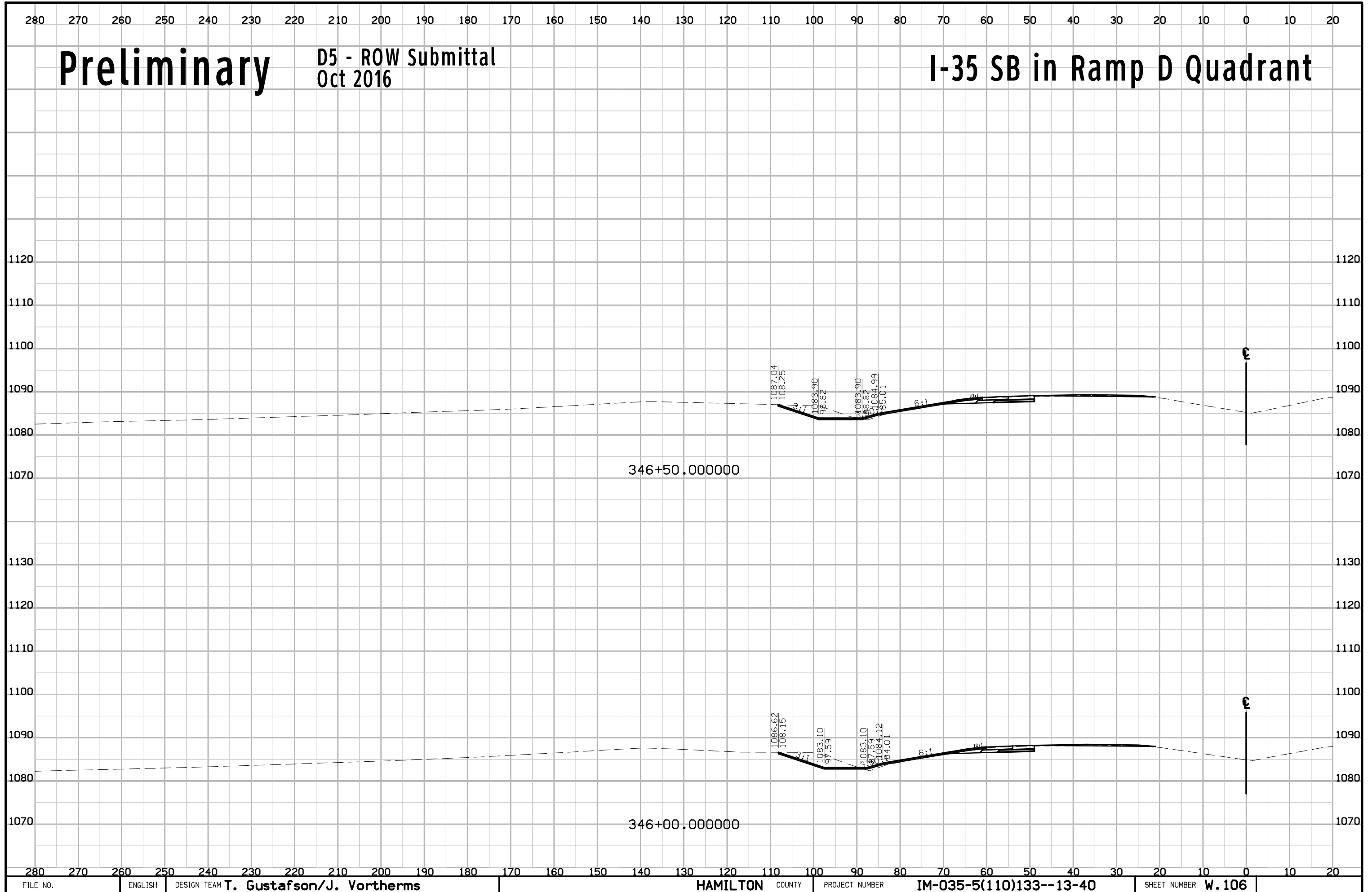
## I-35 SB in Ramp D Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

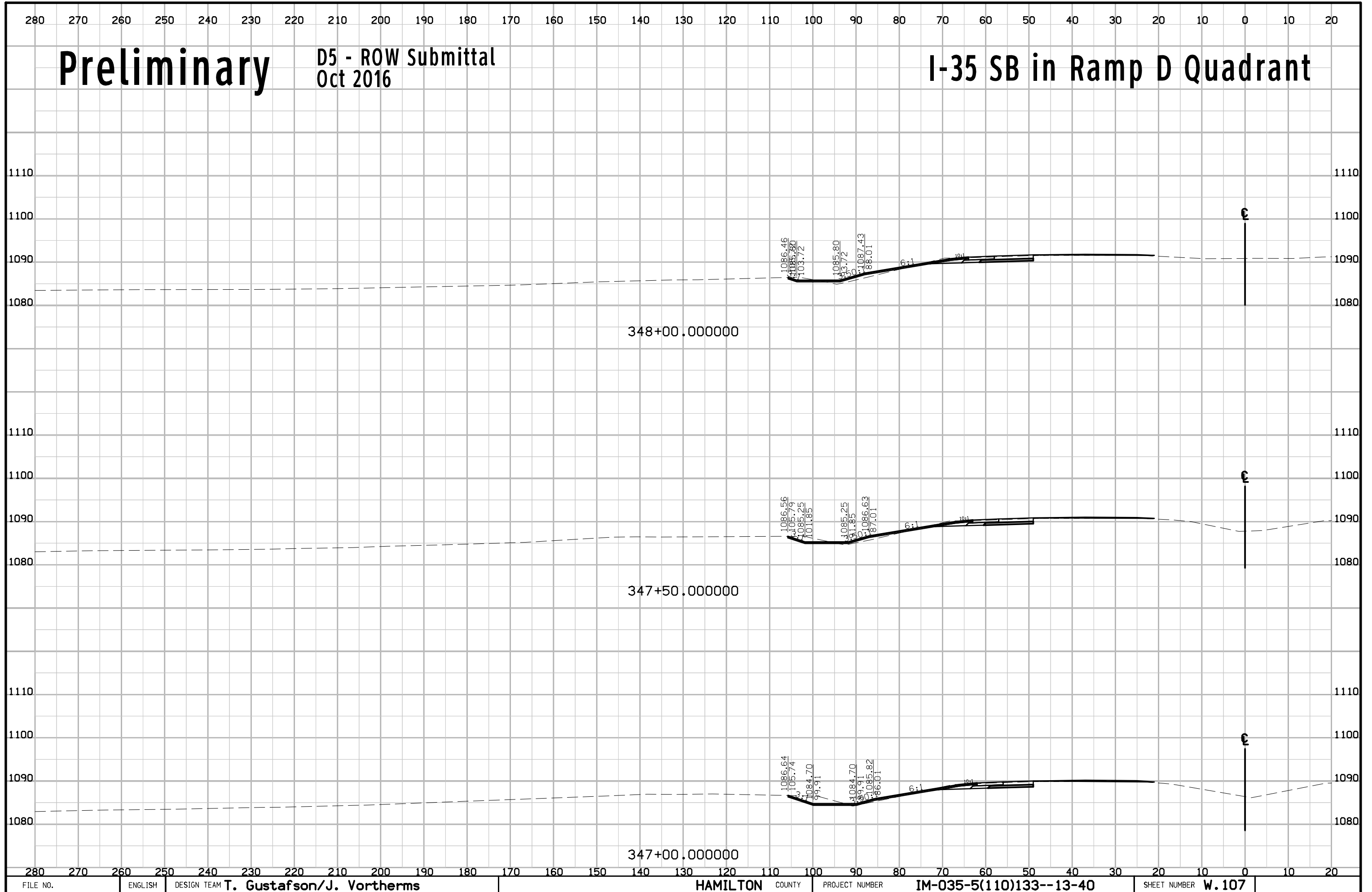
## I-35 SB in Ramp D Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

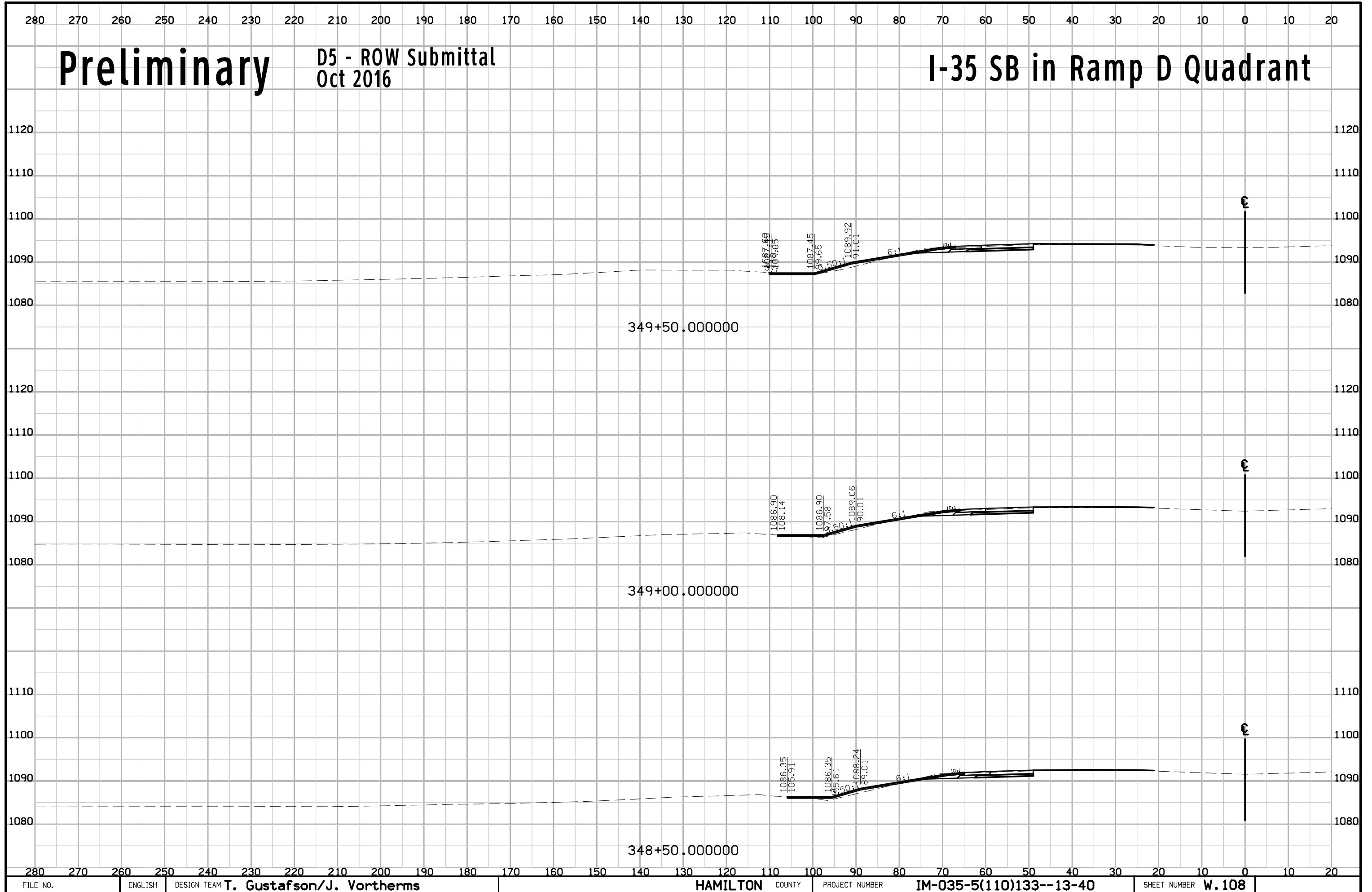
## I-35 SB in Ramp D Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

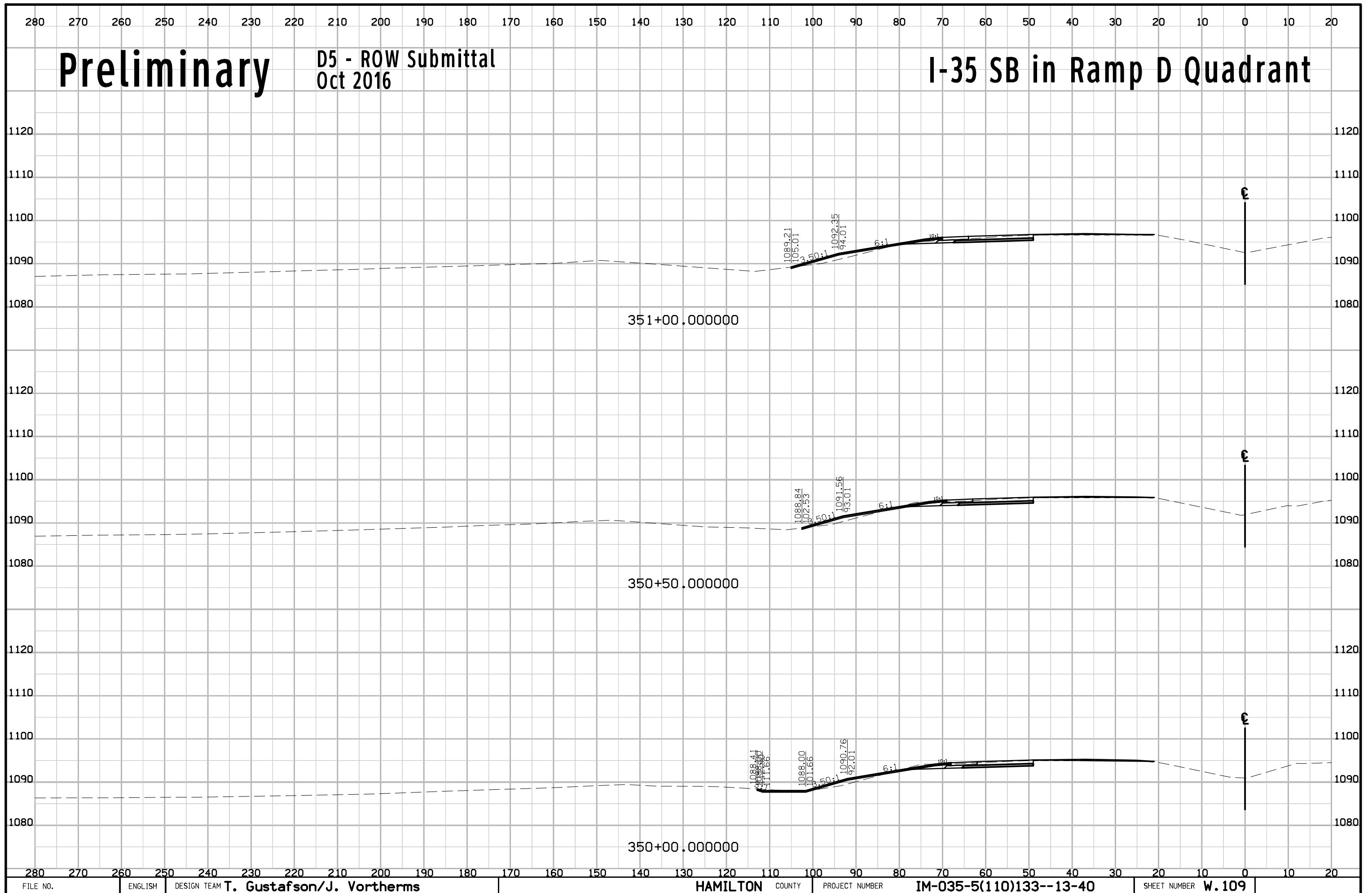
## I-35 SB in Ramp D Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

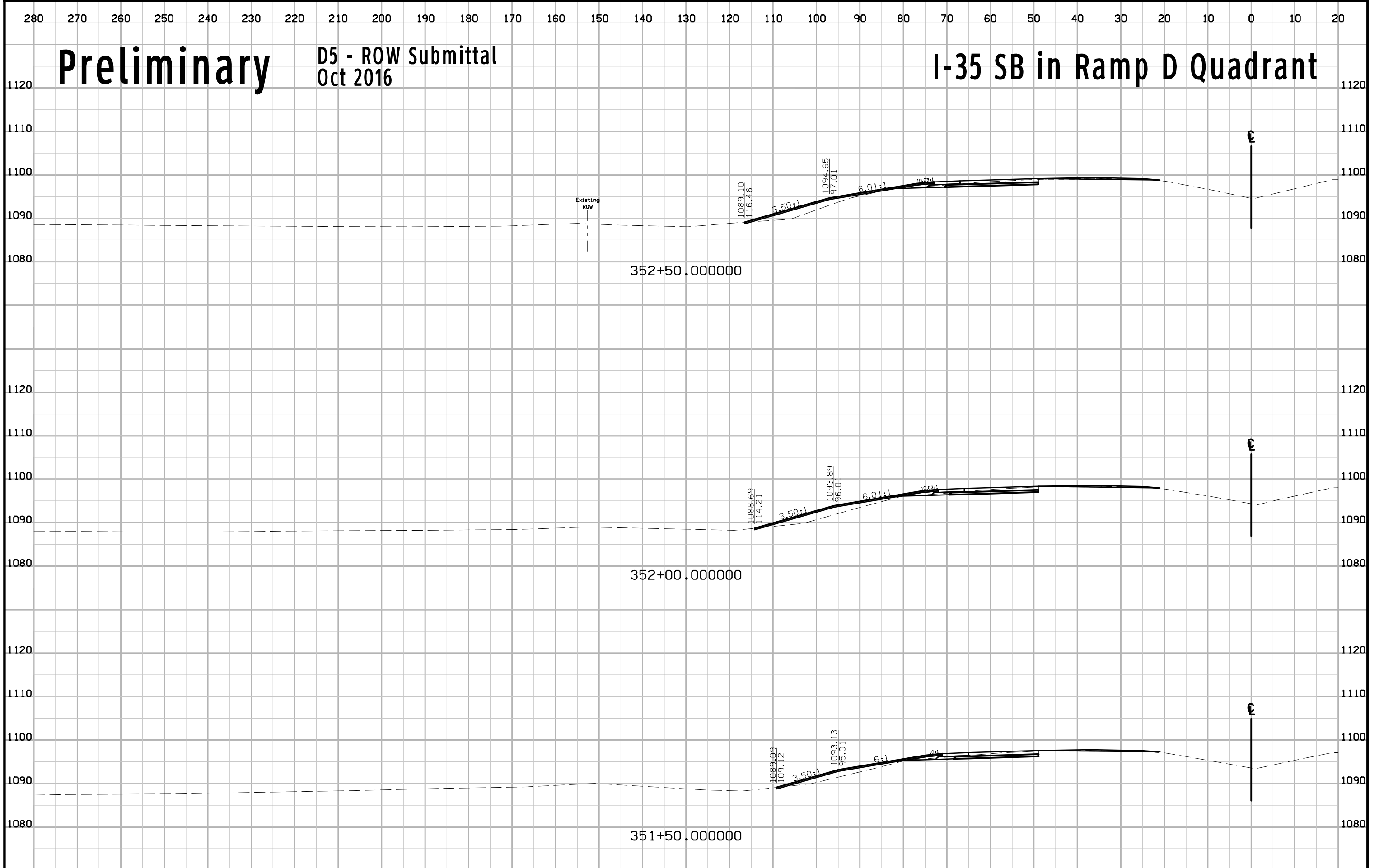
## I-35 SB in Ramp D Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



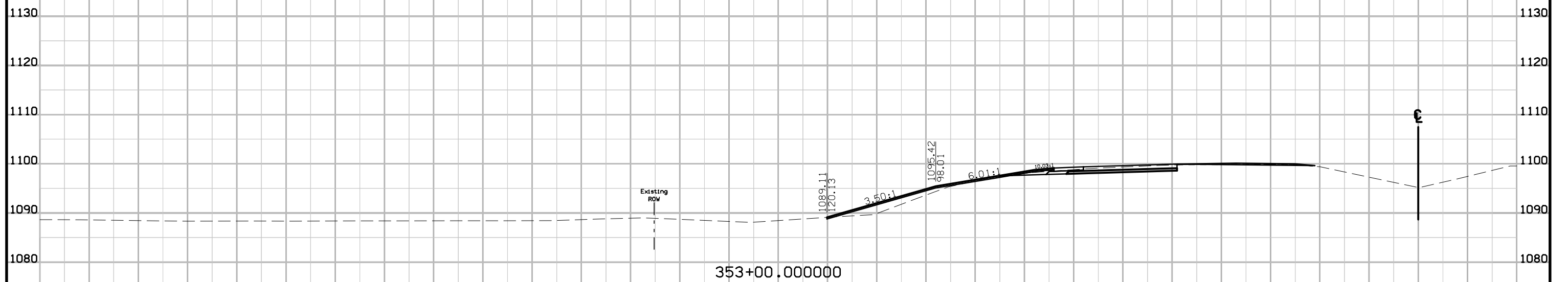
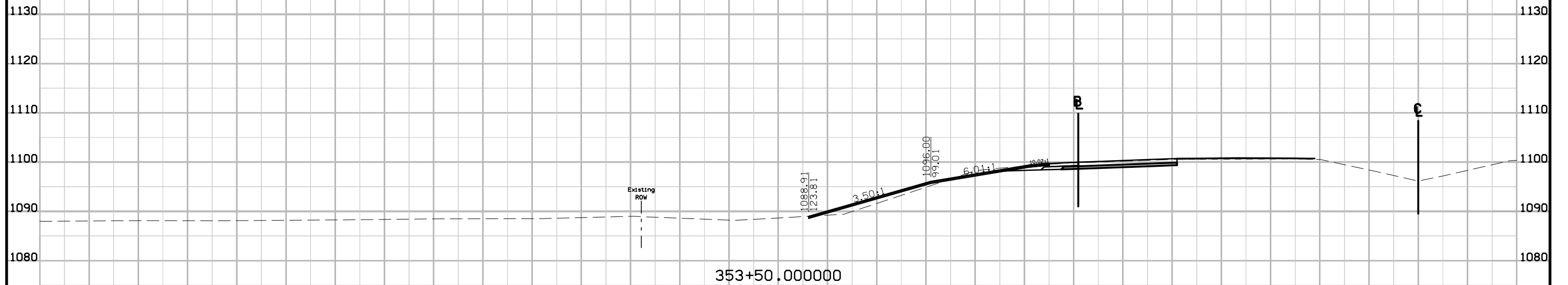


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



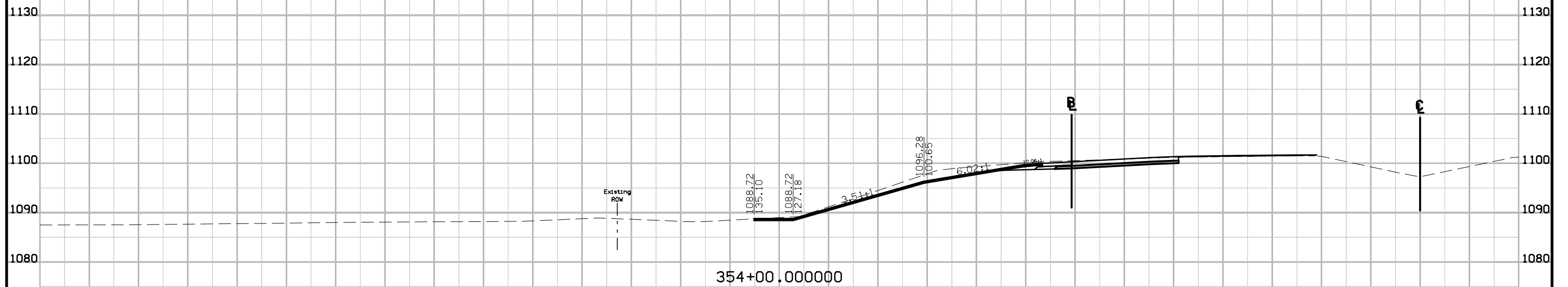
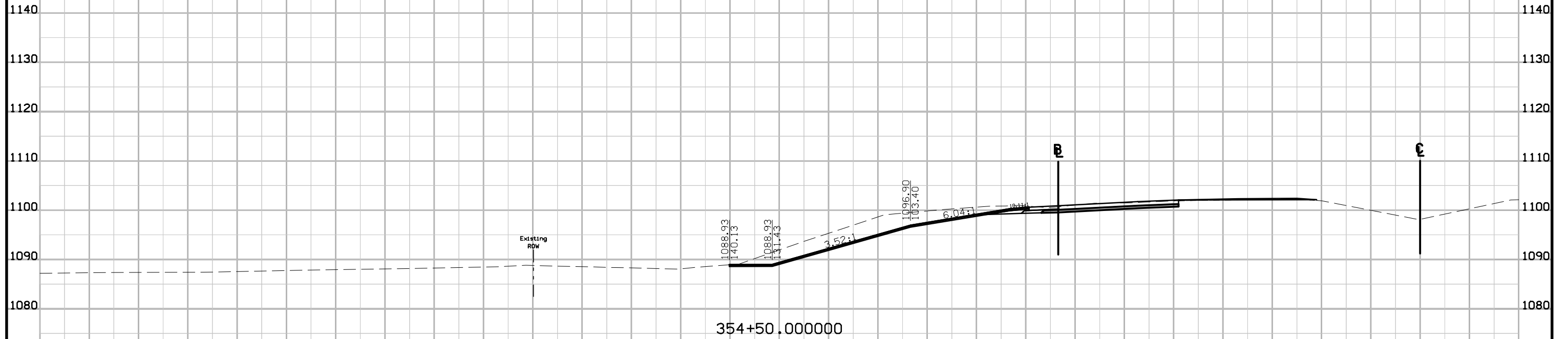
280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

## D5 - ROW Submittal Oct 2016

# I-35 SB in Ramp D Quadrant

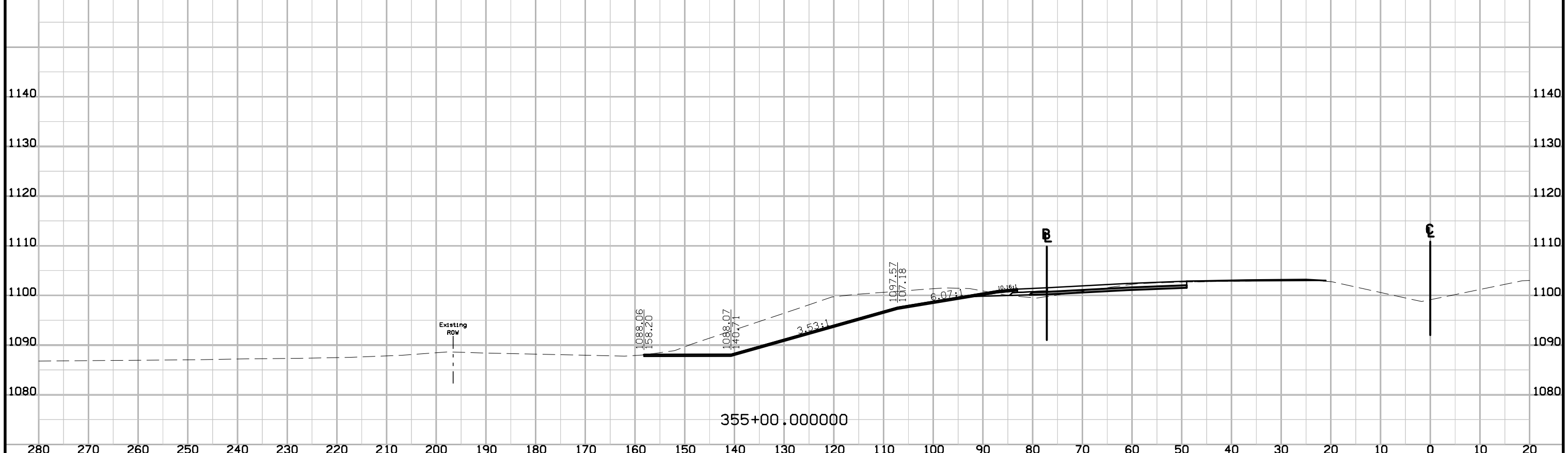
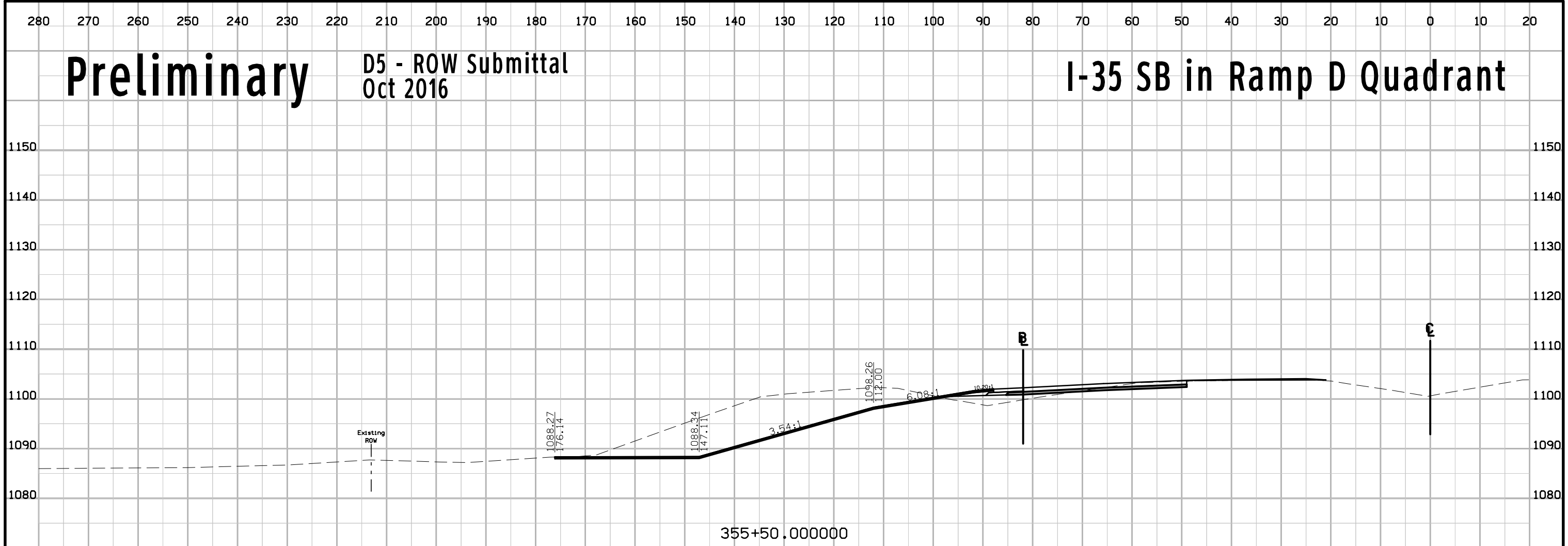


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant

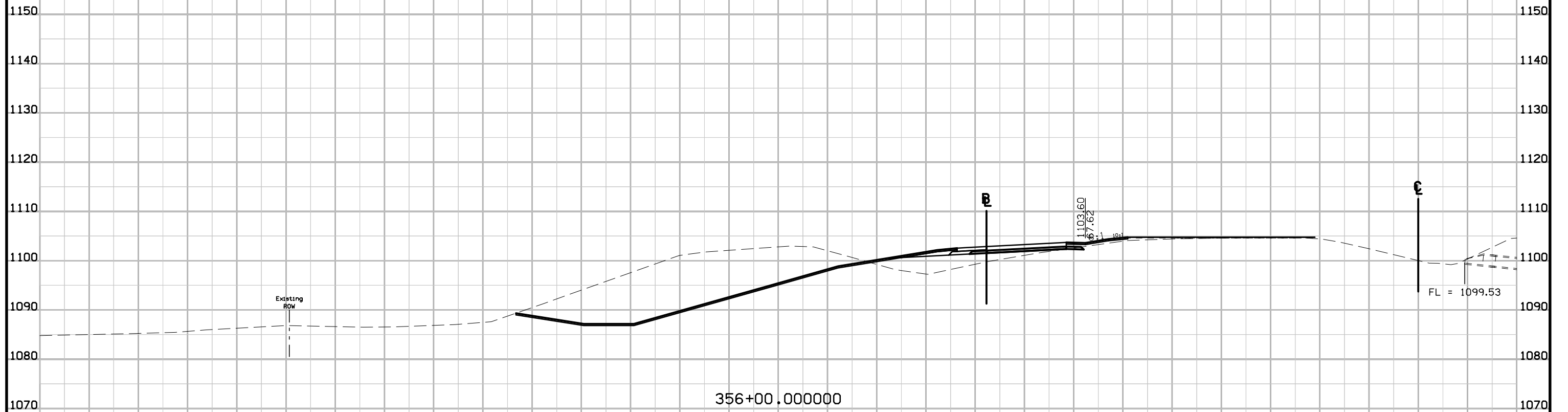


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



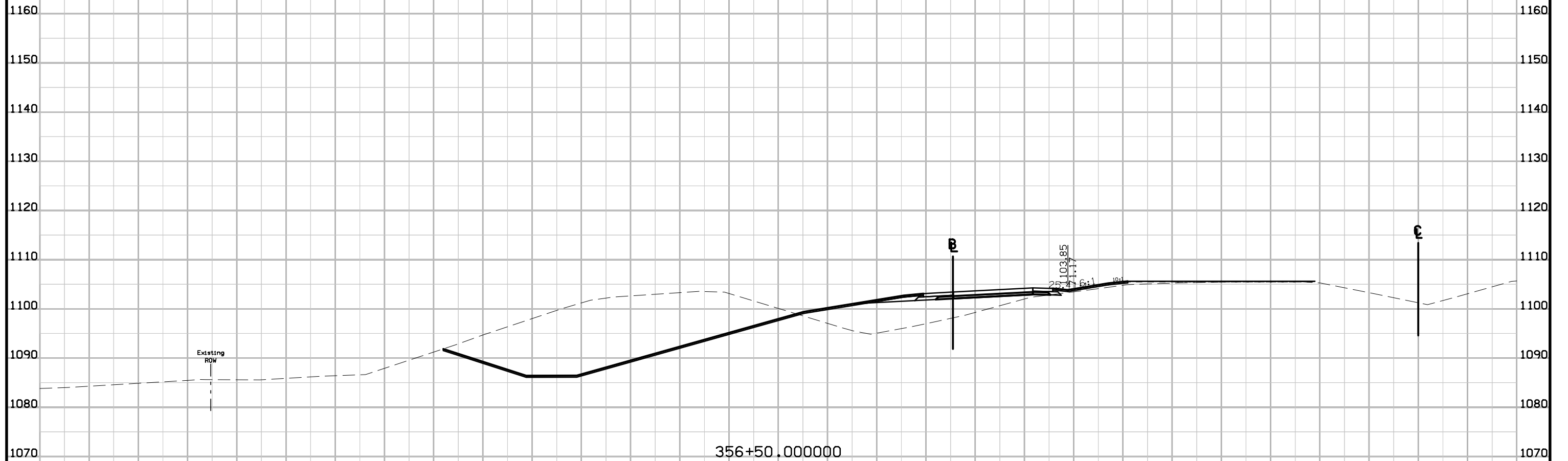
280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



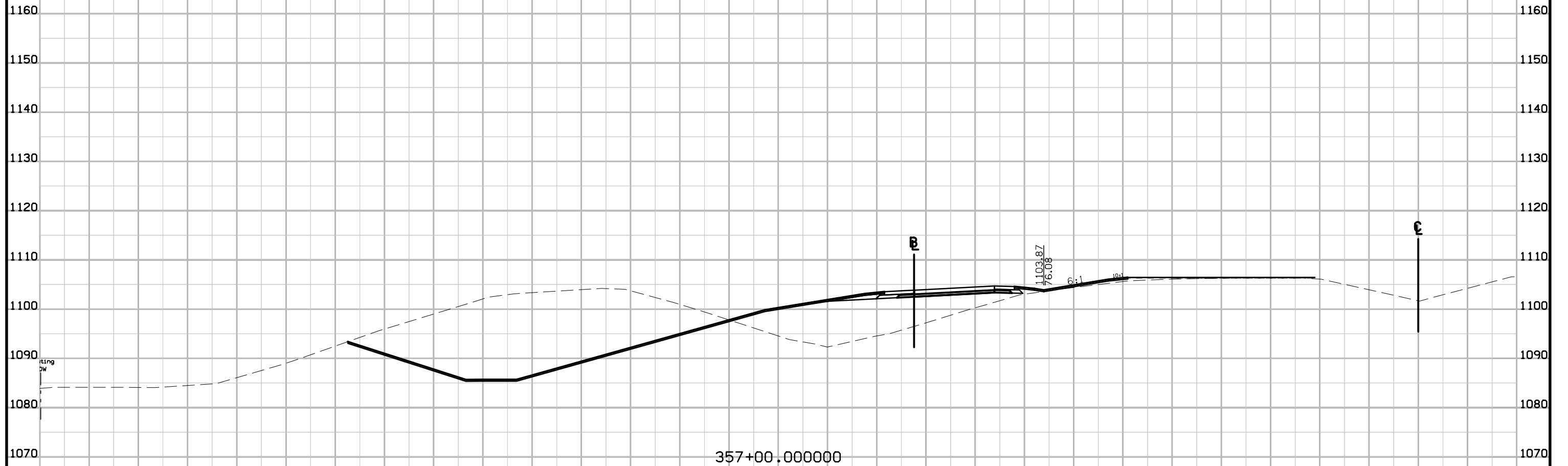
280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



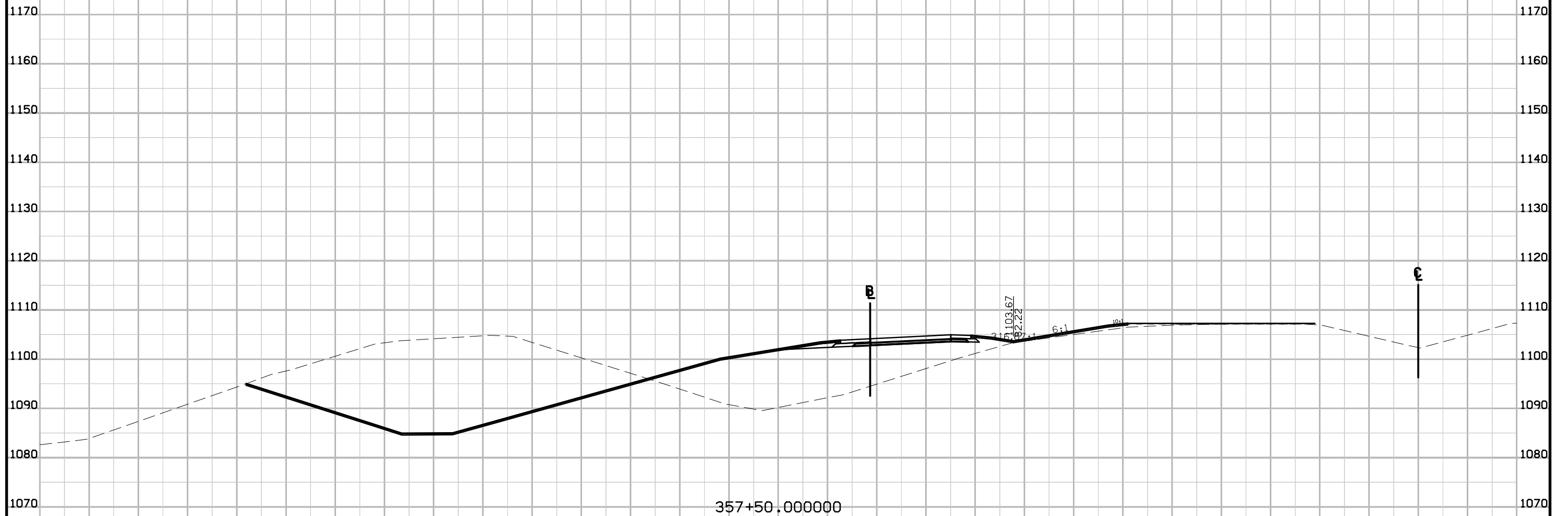
280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



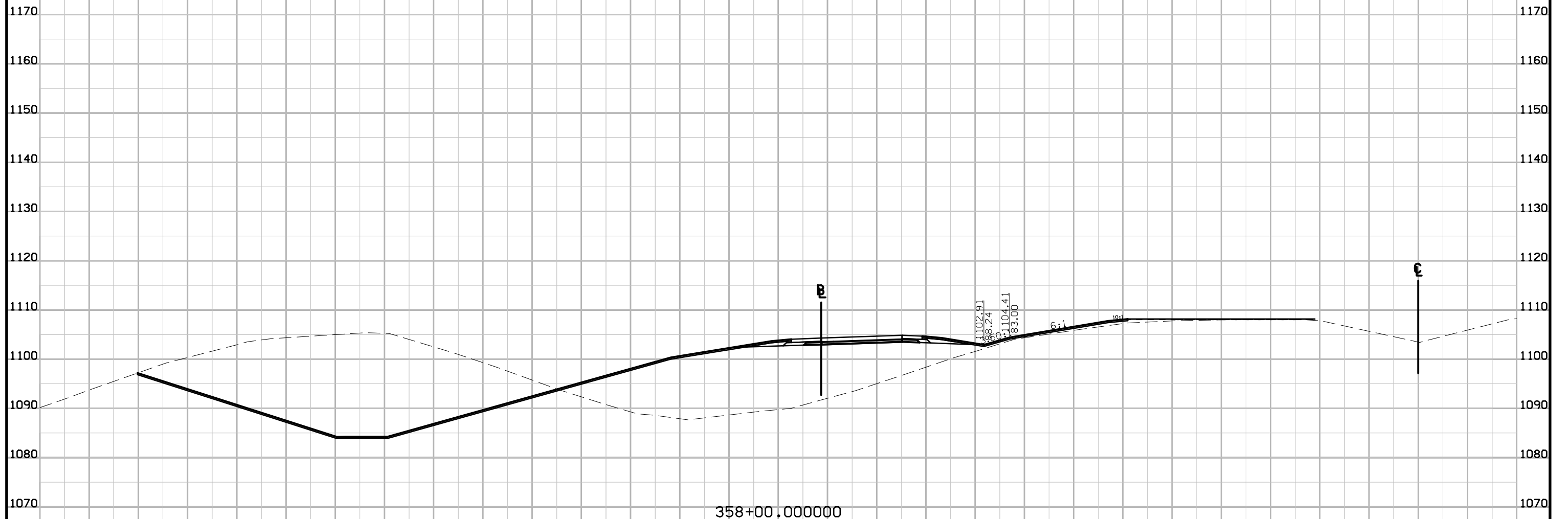
280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

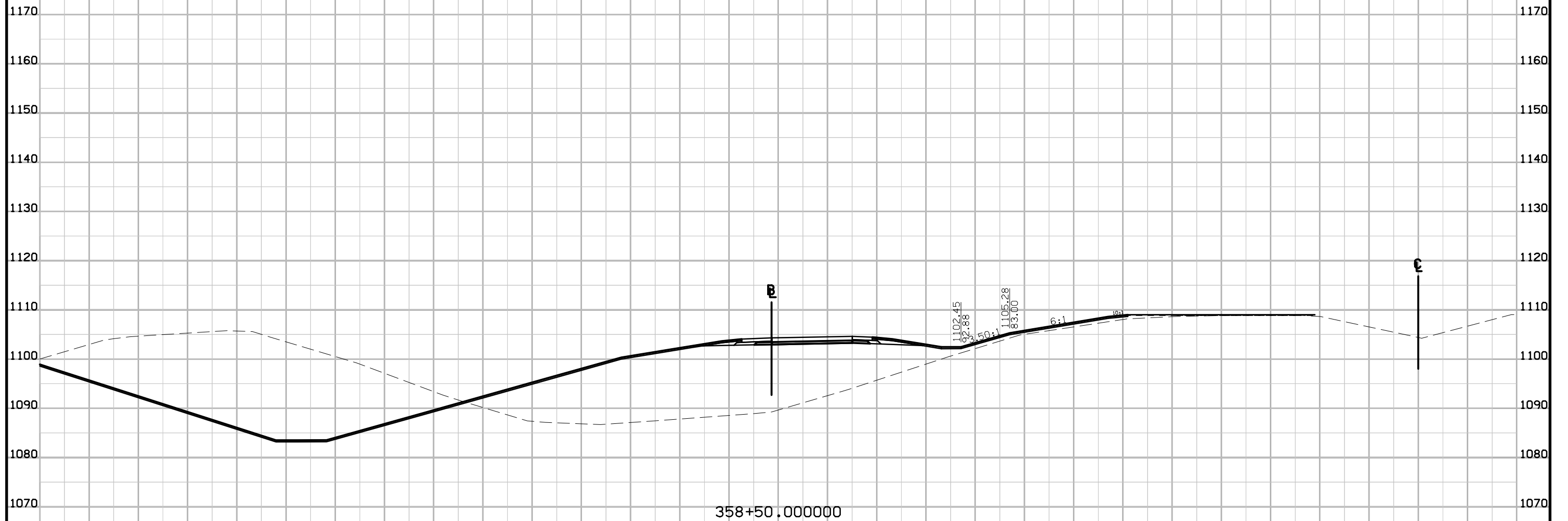


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

# I-35 SB in Ramp D Quadrant



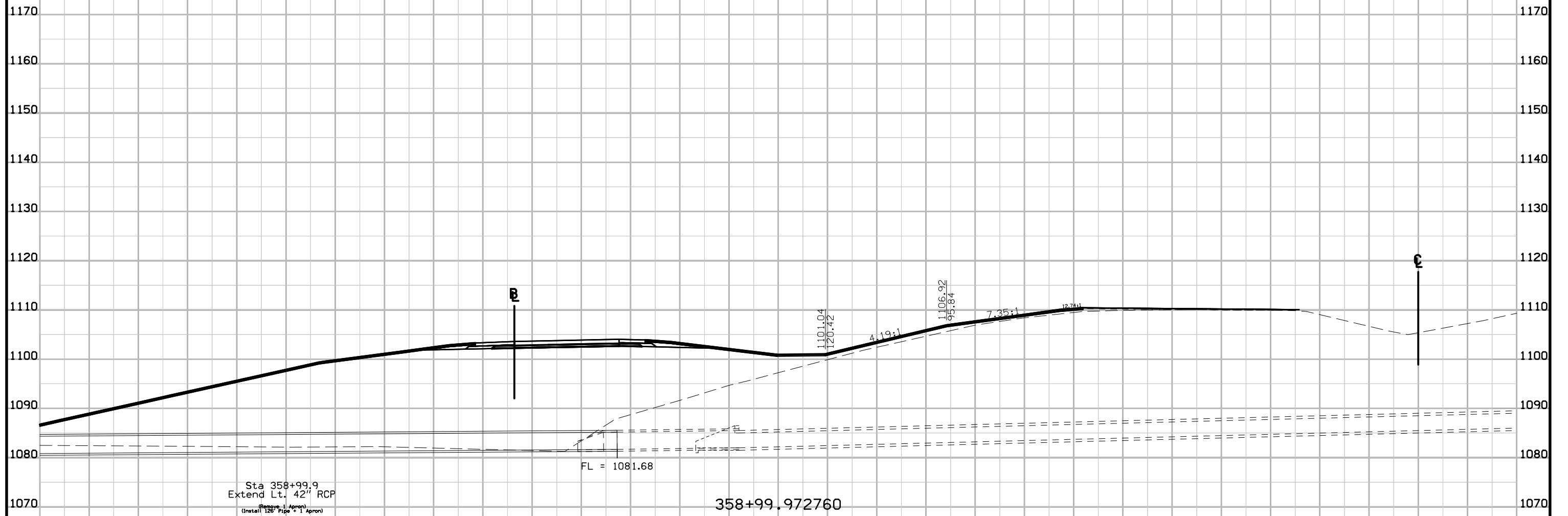
FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	HAMILTON COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.119</b>
----------	---------	--	-----------------	---	---------------------------

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

## D5 - ROW Submittal Oct 2016

# I-35 SB in Ramp D Quadrant



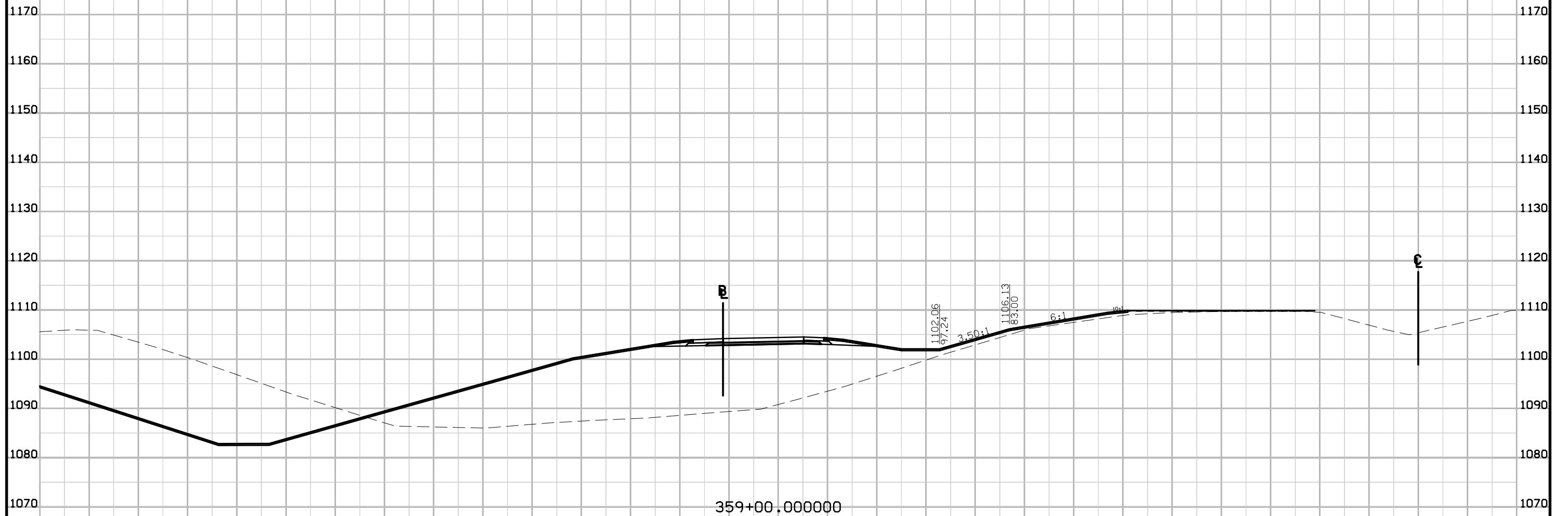
FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	HAMILTON COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.120</b>
----------	---------	--	-----------------	---	---------------------------

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant

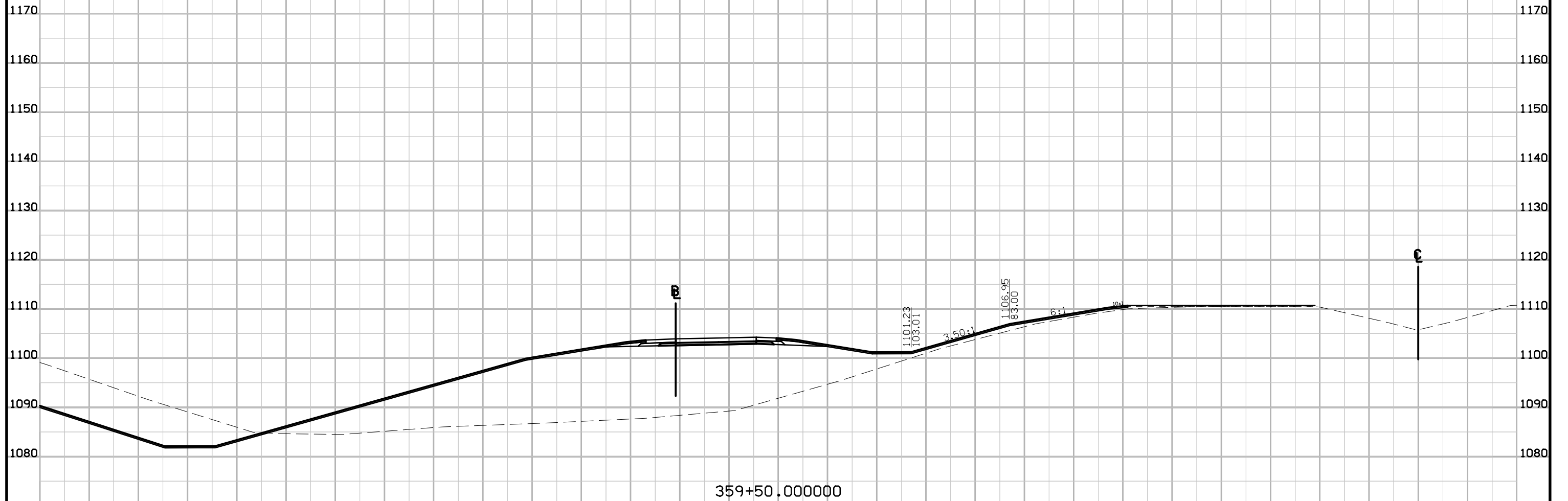


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant

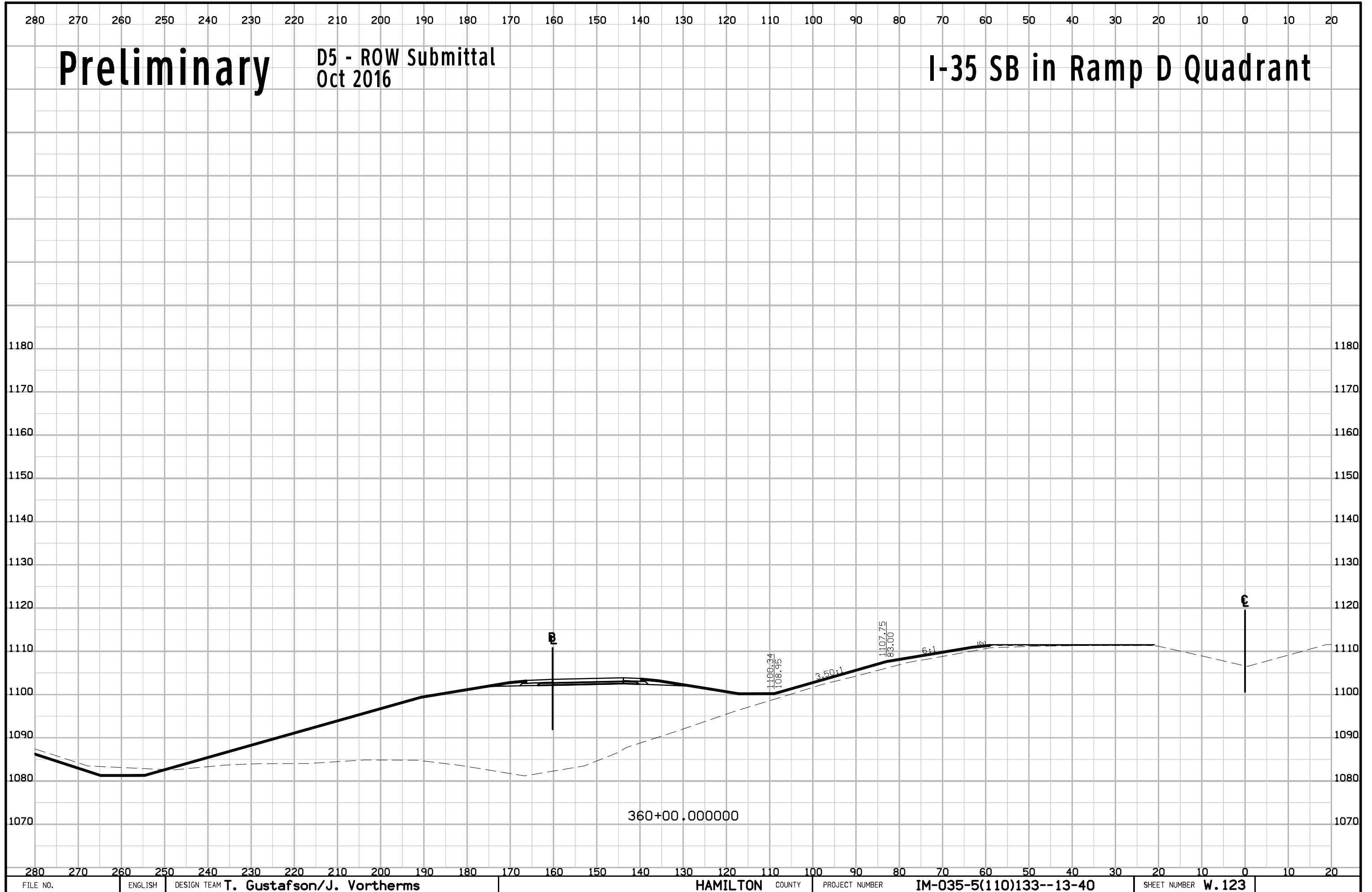


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant

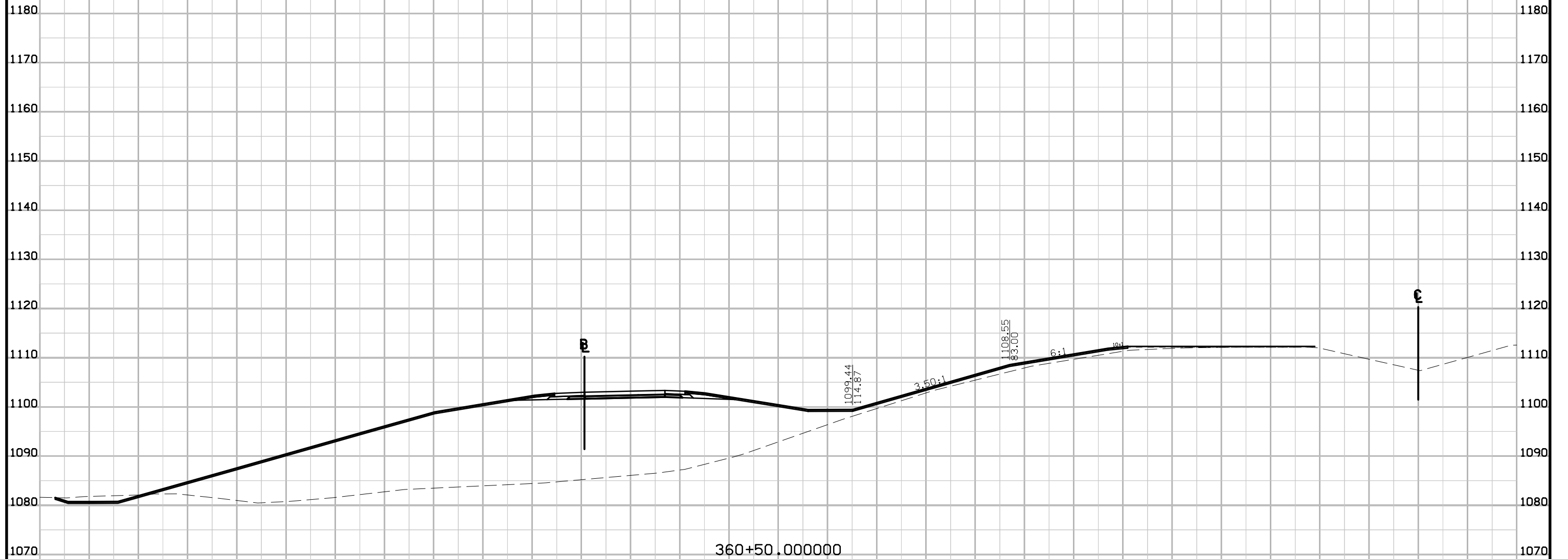


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



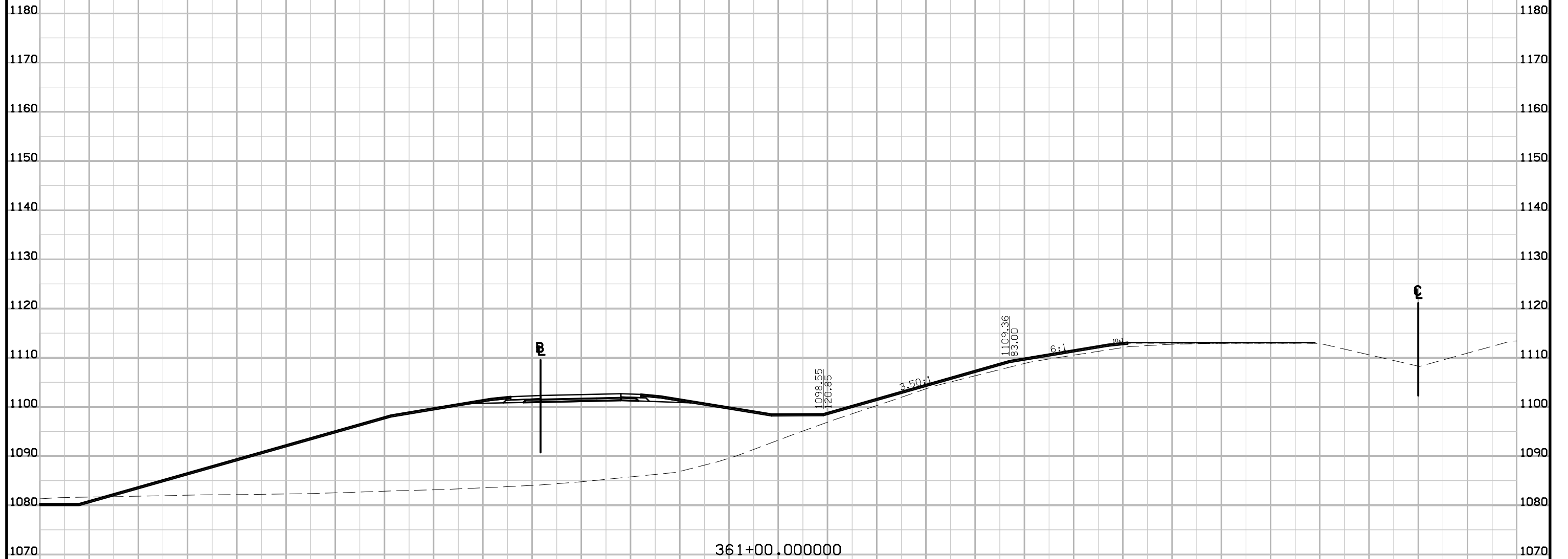
280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



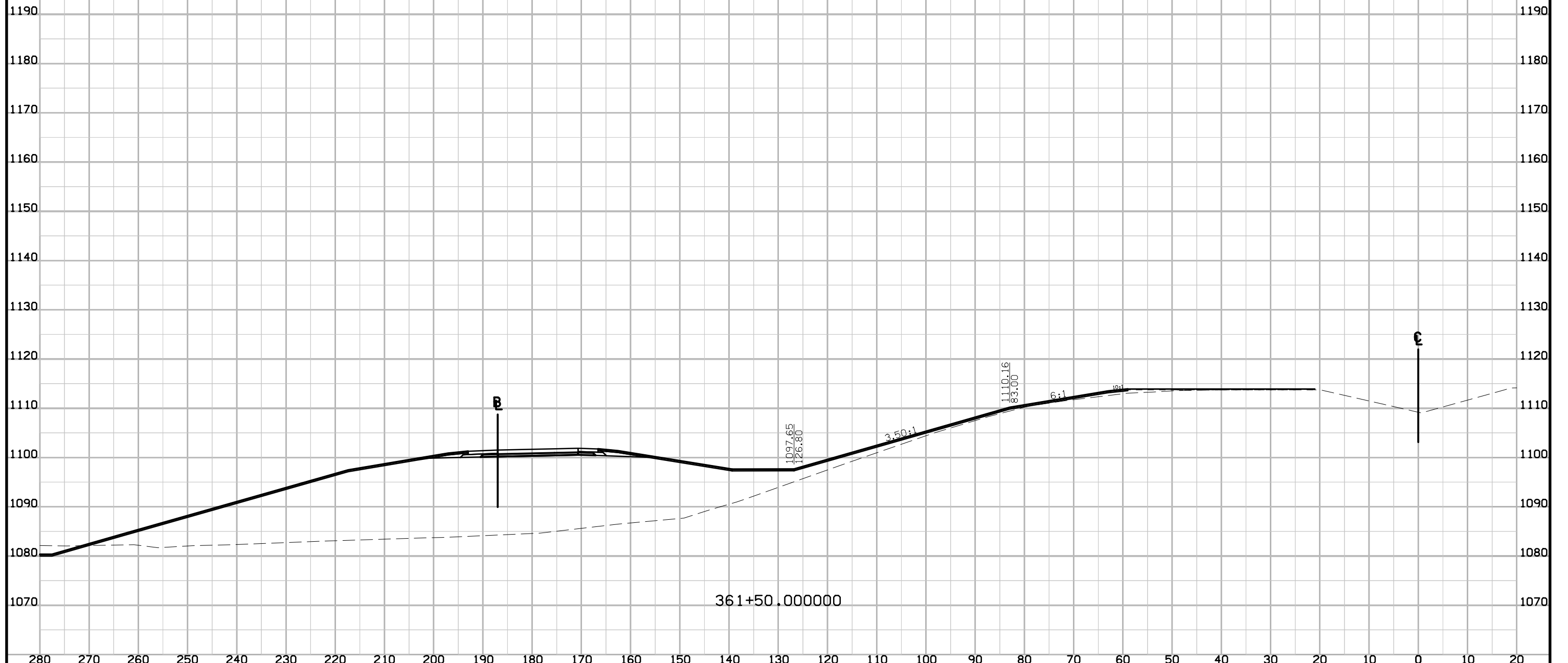
FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	<b>HAMILTON</b> COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.125</b>
----------	---------	--	------------------------	---	---------------------------

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	HAMILTON COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.126</b>
----------	---------	--	-----------------	---	---------------------------



280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



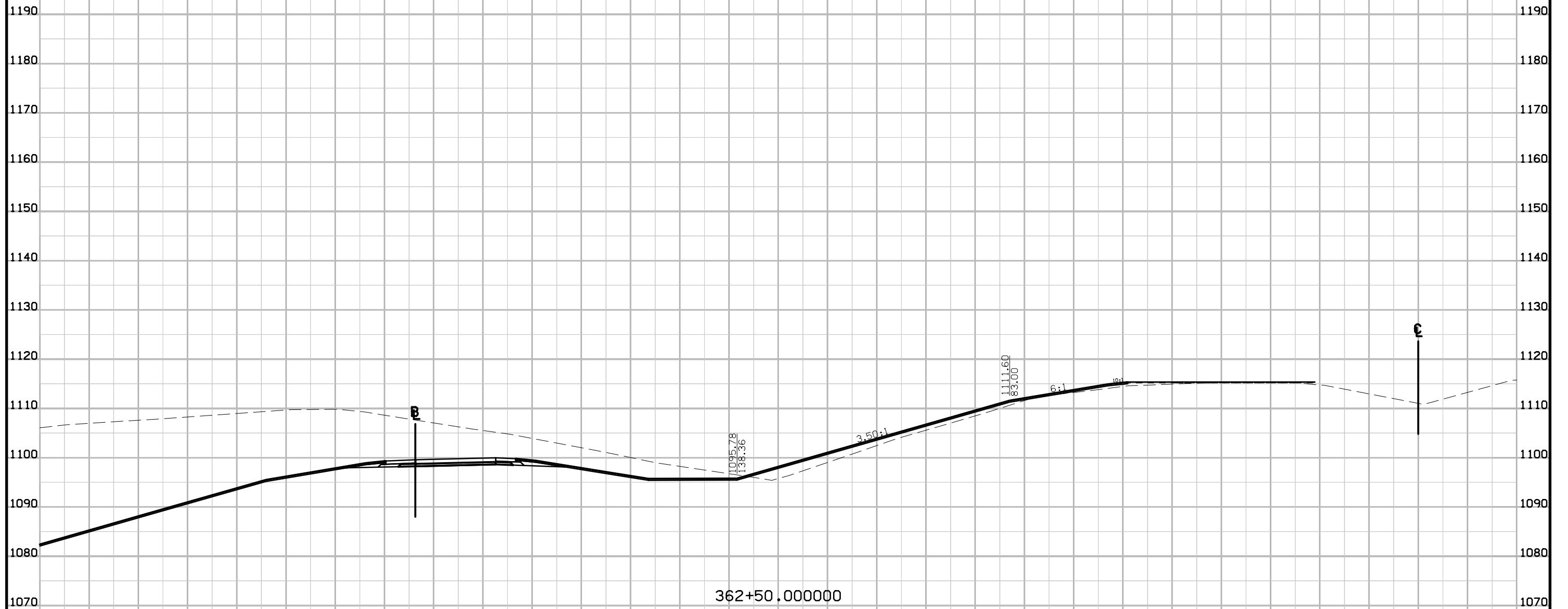
362+00.000000

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



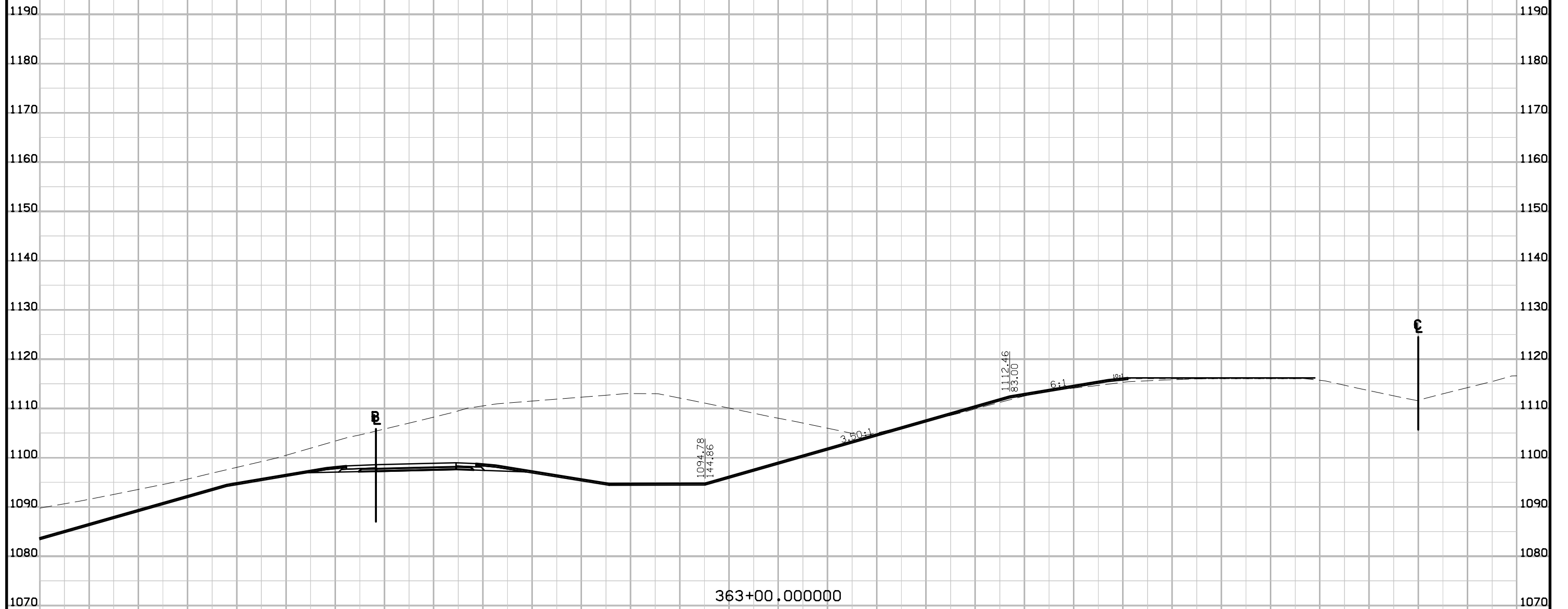
FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	HAMILTON COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.128</b>
----------	---------	--	-----------------	---	---------------------------

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant

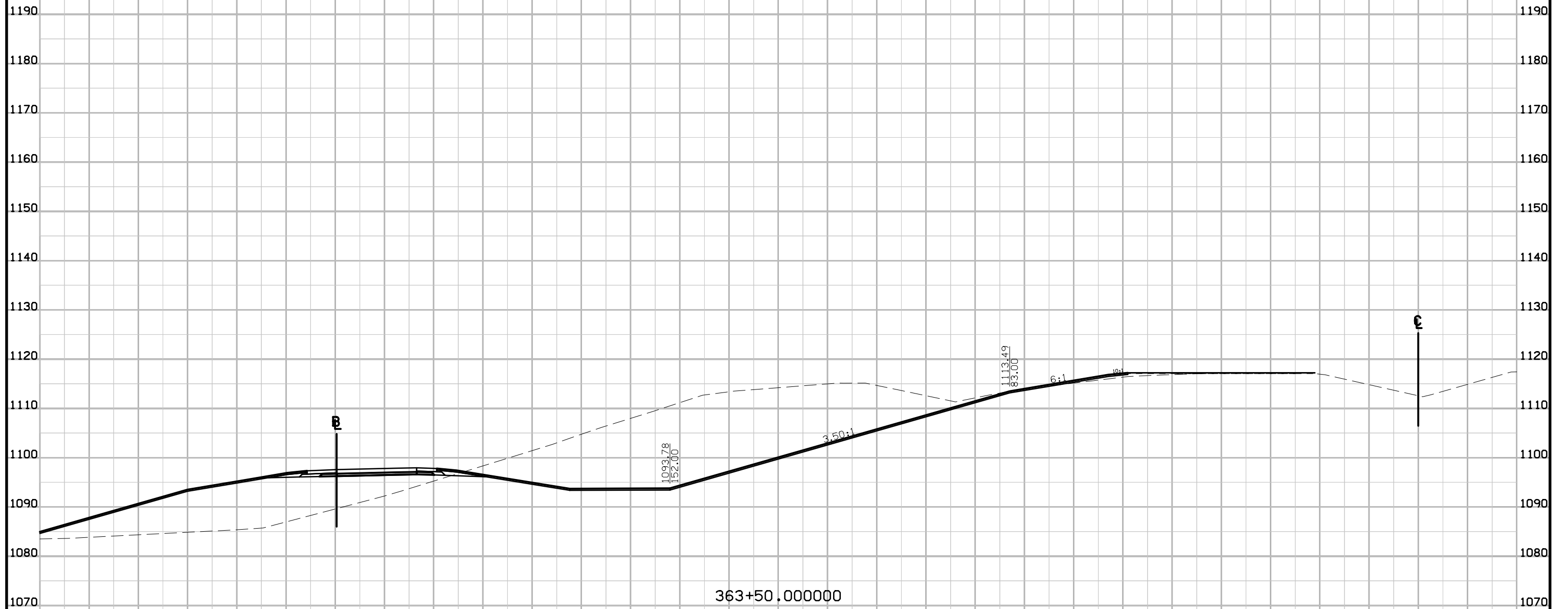


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

Preliminary

D5 - ROW Submittal  
Oct 2016

I-35 SB in Ramp D Quadrant

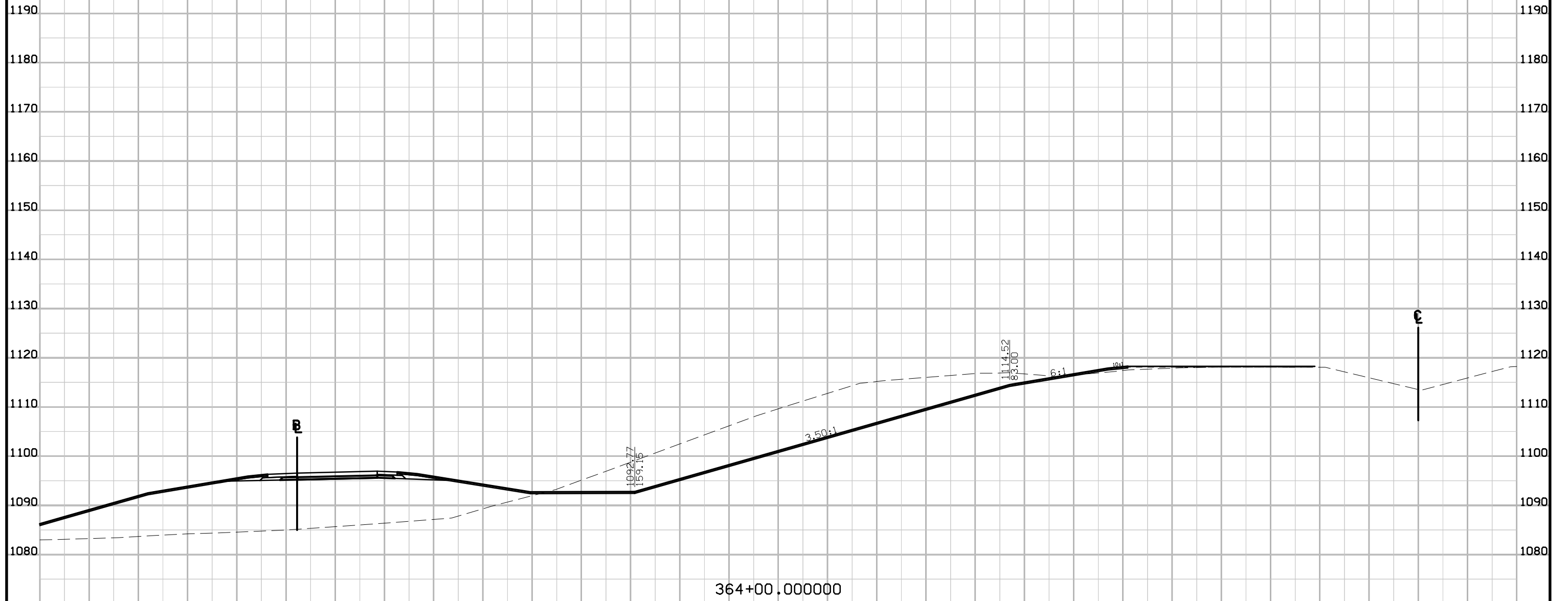


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

## D5 - ROW Submittal Oct 2016

# I-35 SB in Ramp D Quadrant

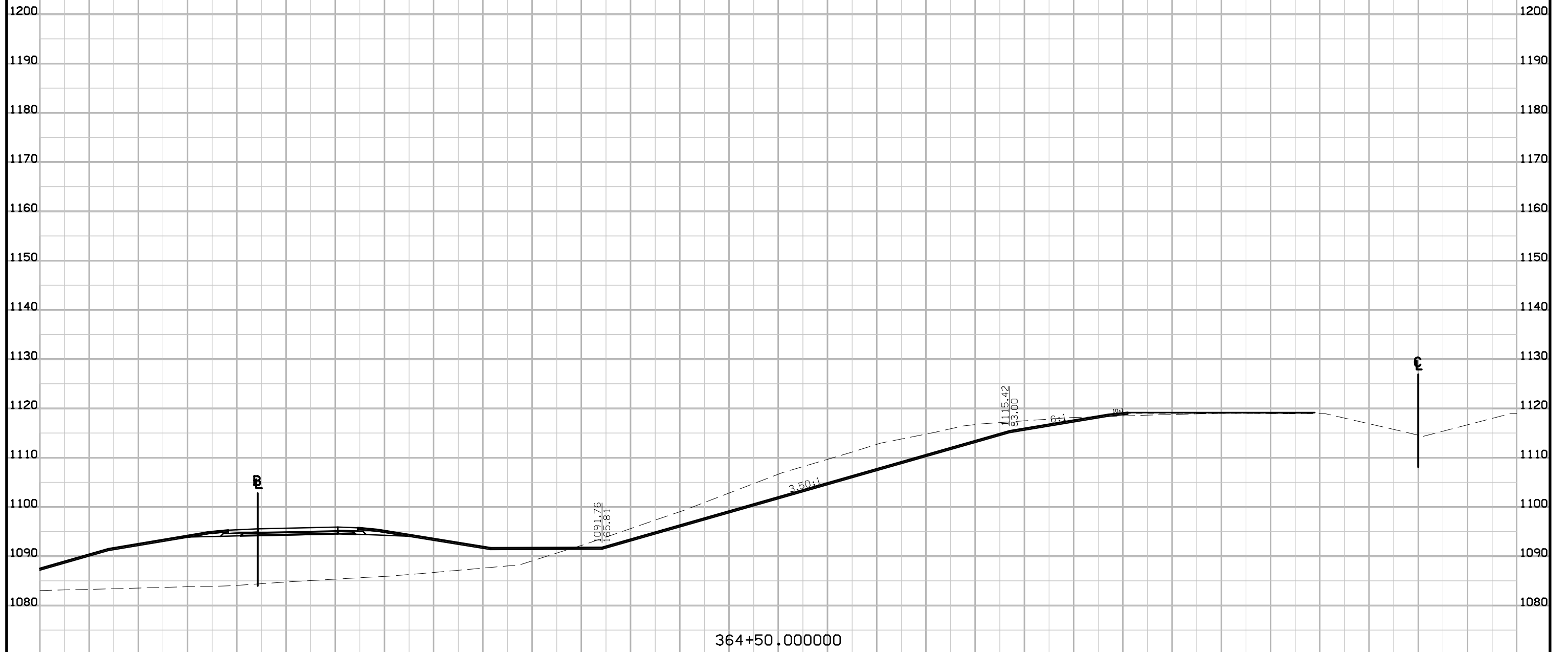


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



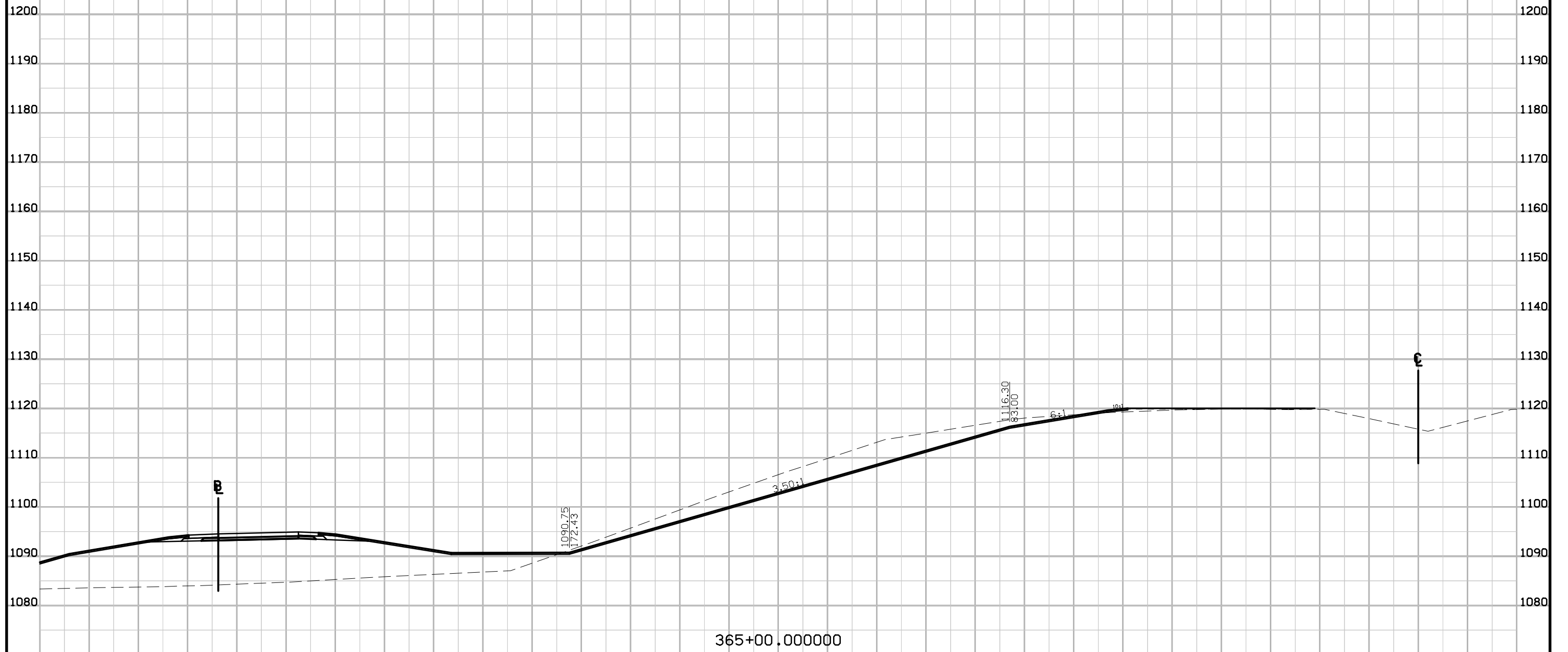
364+50.000000

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant

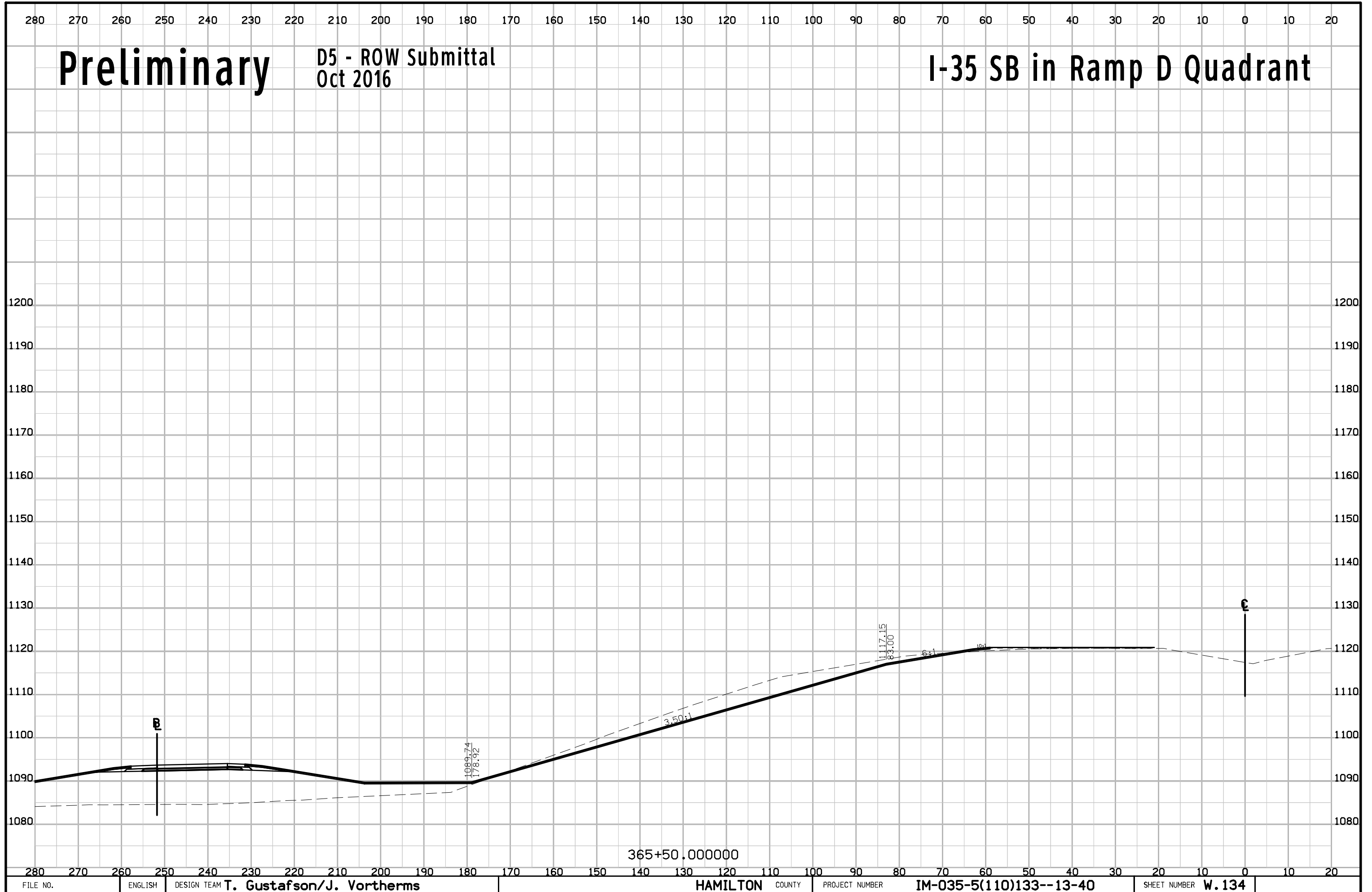


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



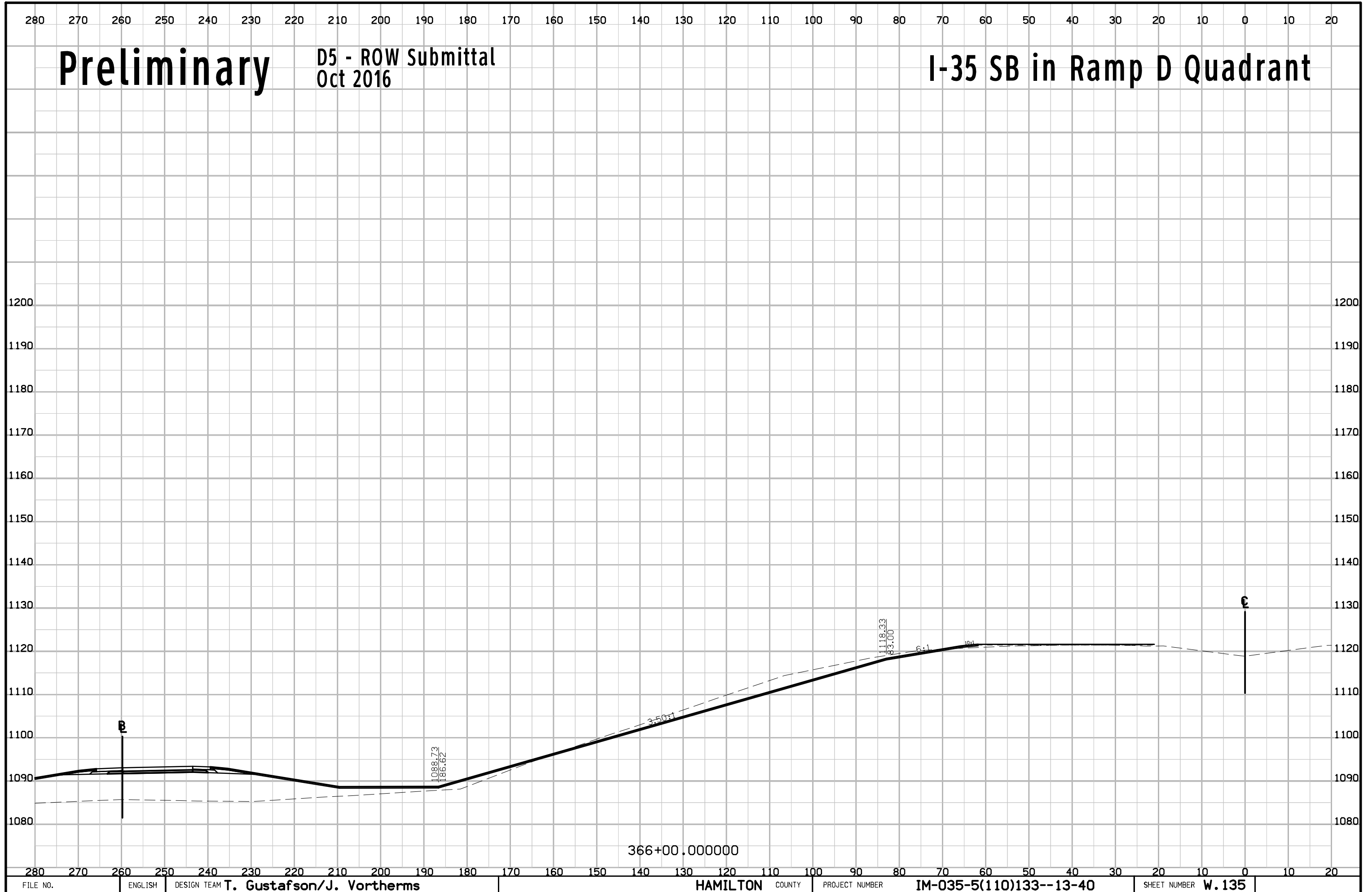
365+50.000000



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant

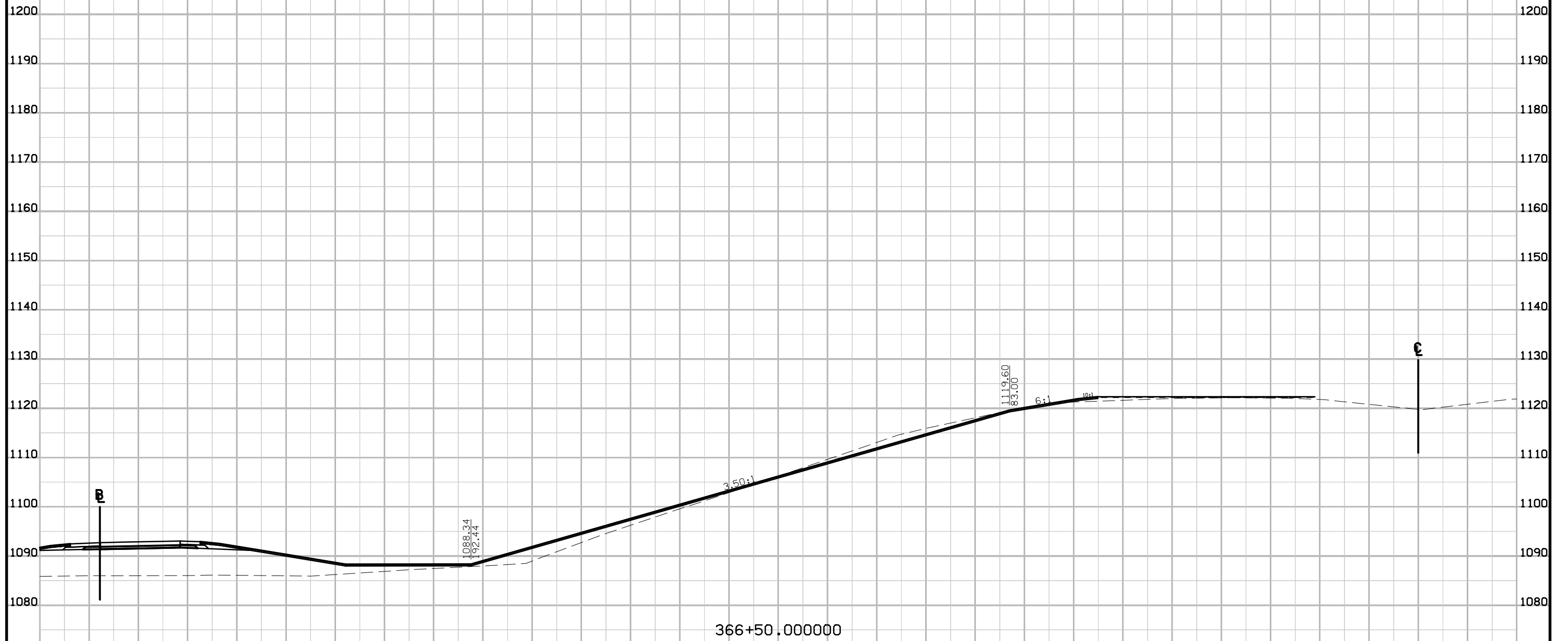


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



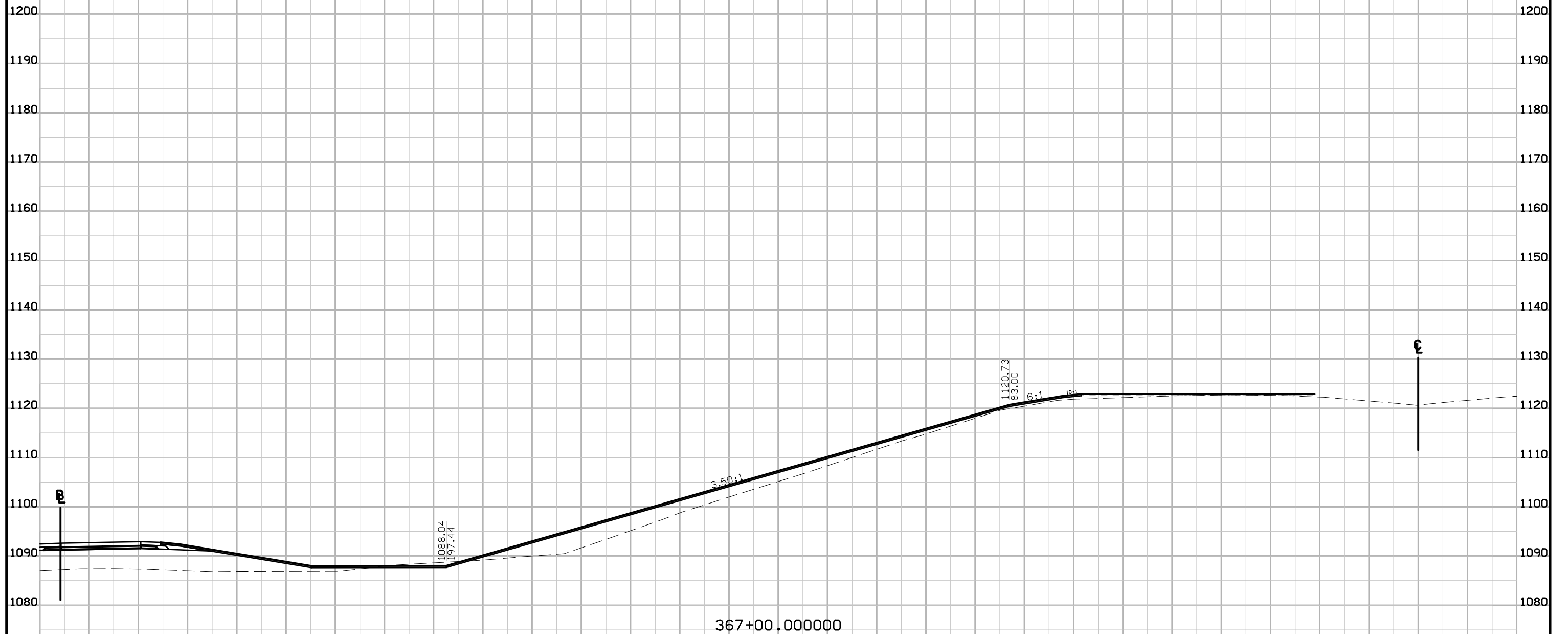
366+50.000000

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

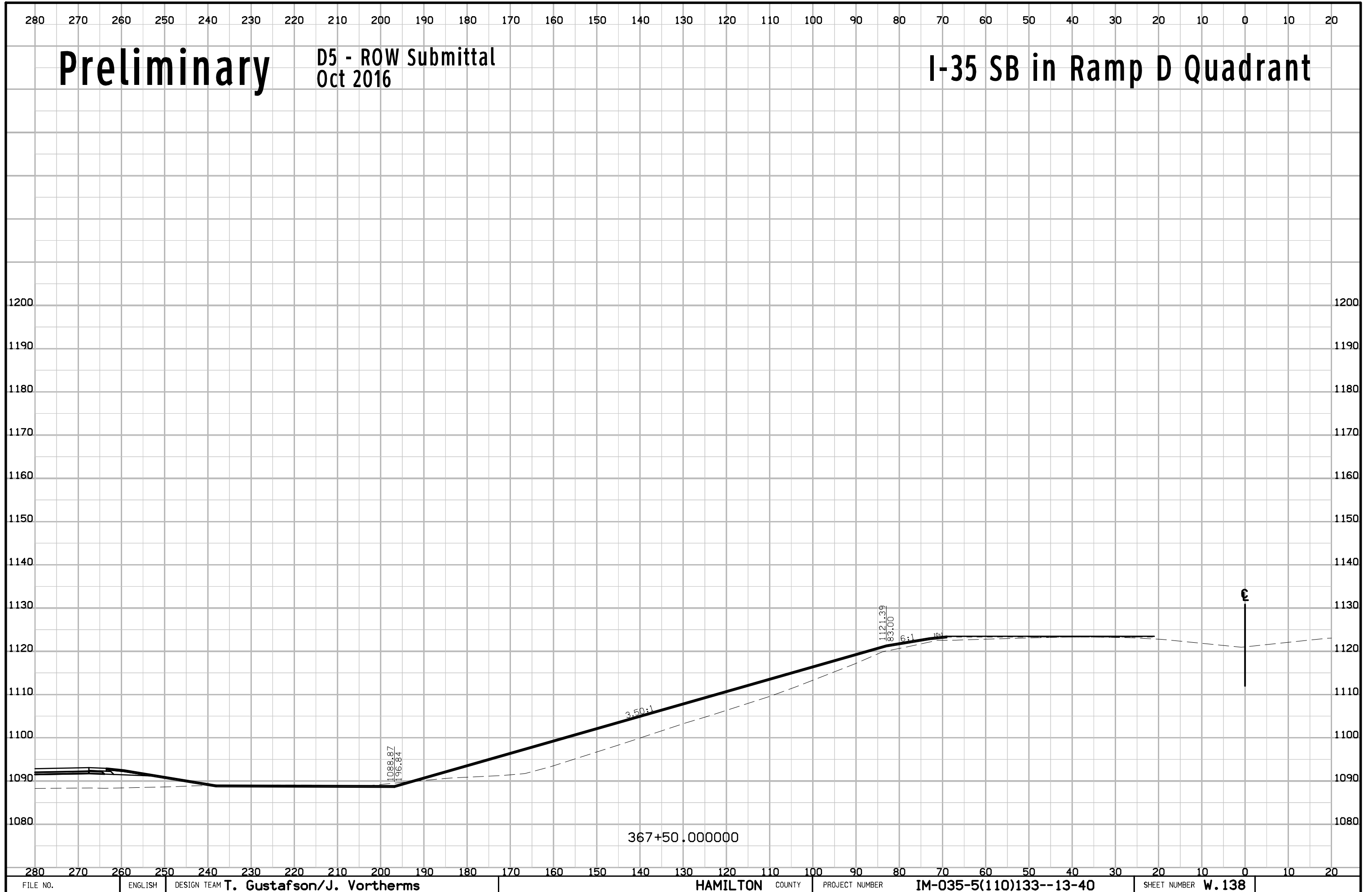
## I-35 SB in Ramp D Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant

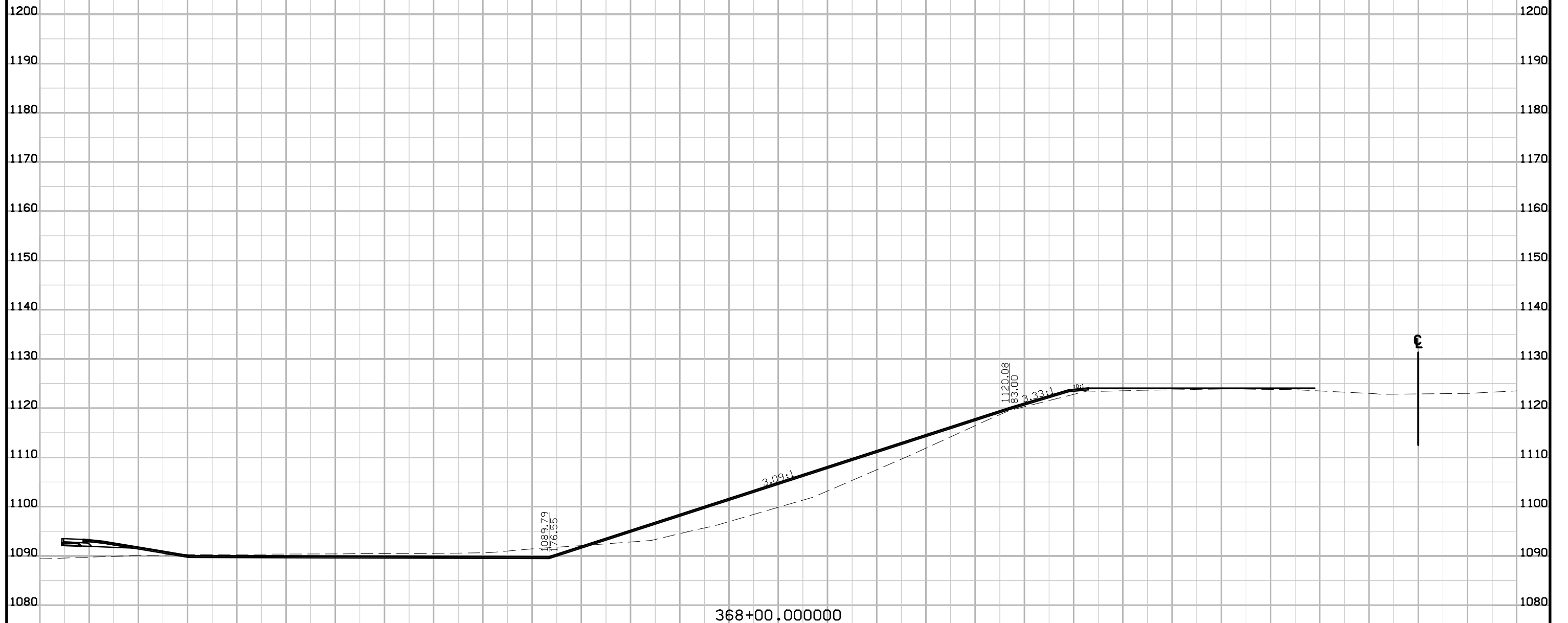


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp D Quadrant



368+00.000000

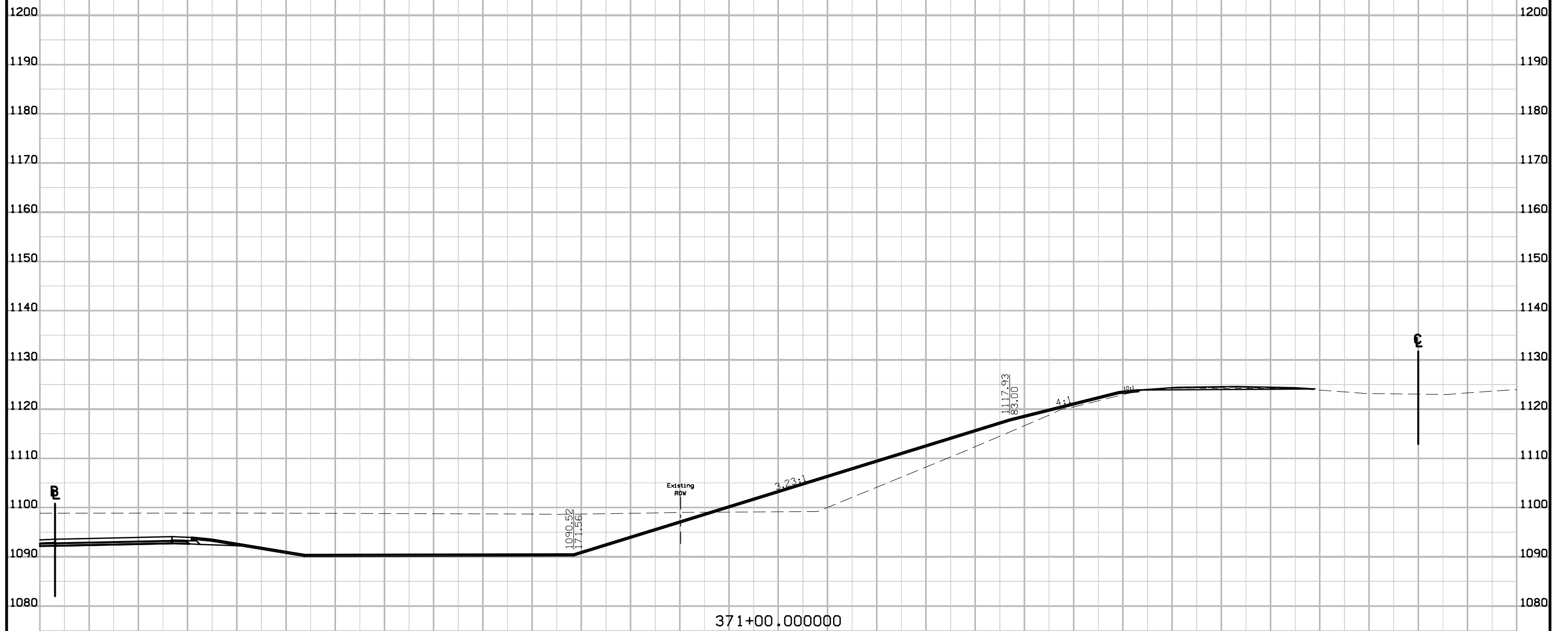
280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

# I-35 SB in Ramp A Quadrant



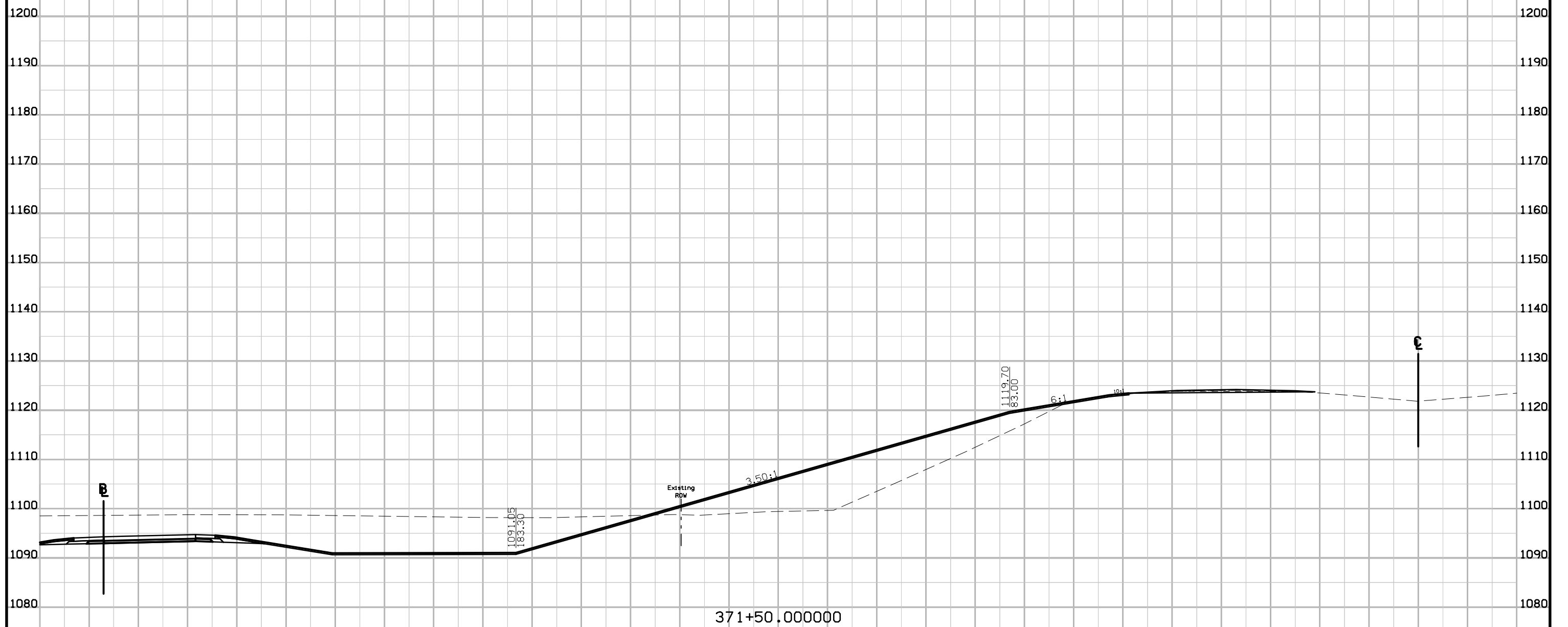
280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant

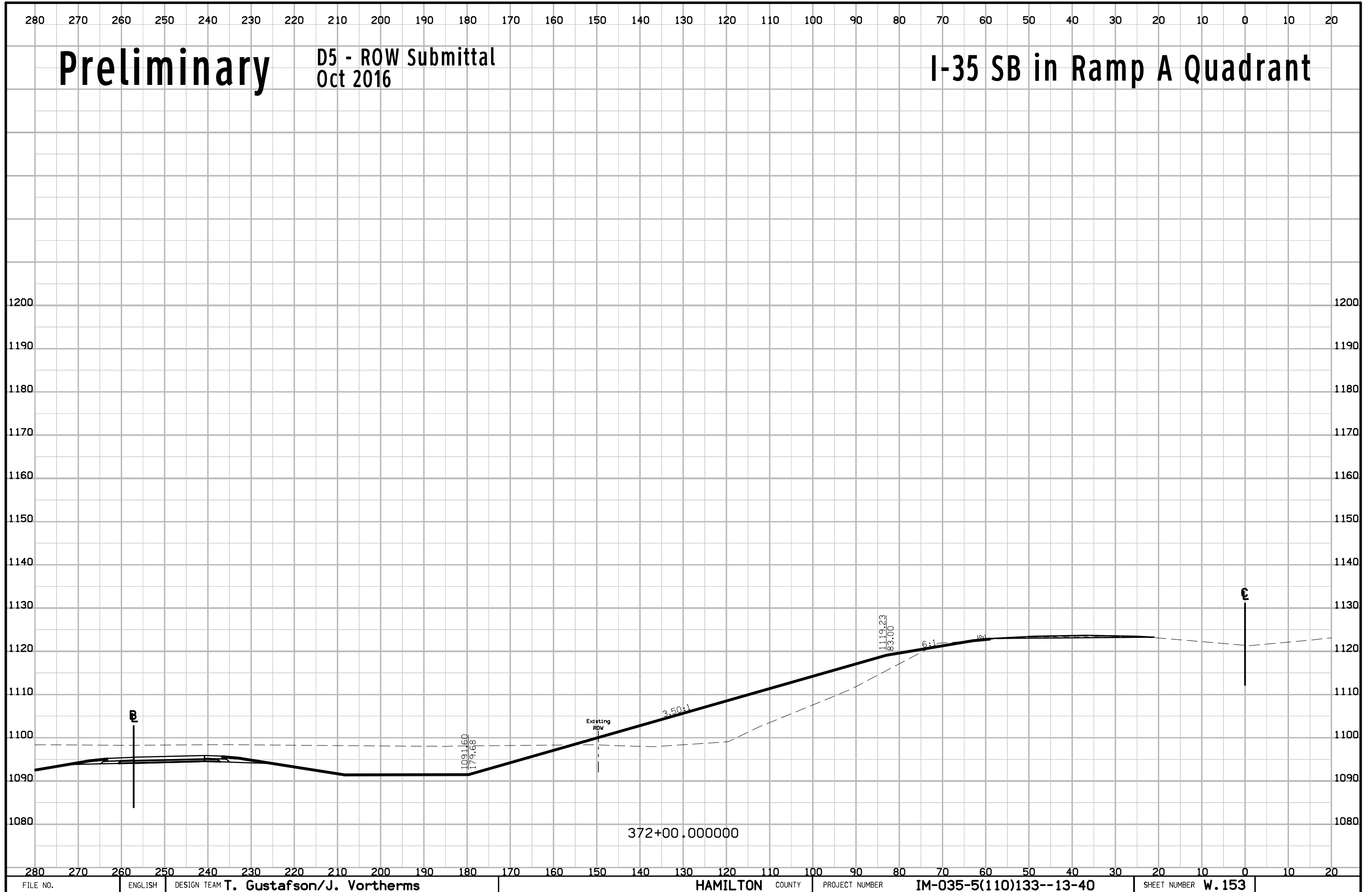


FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	HAMILTON COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.152</b>
----------	---------	--	-----------------	---	---------------------------

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant



372+00.000000

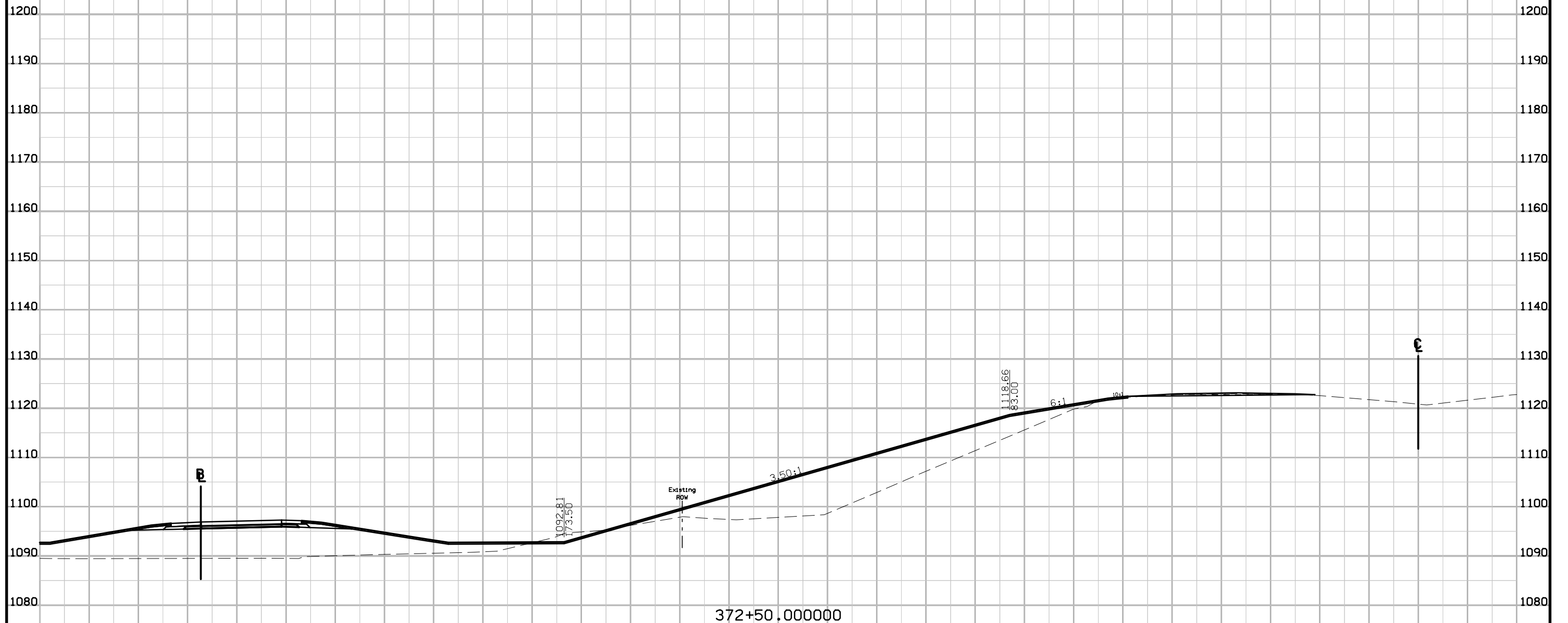


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant



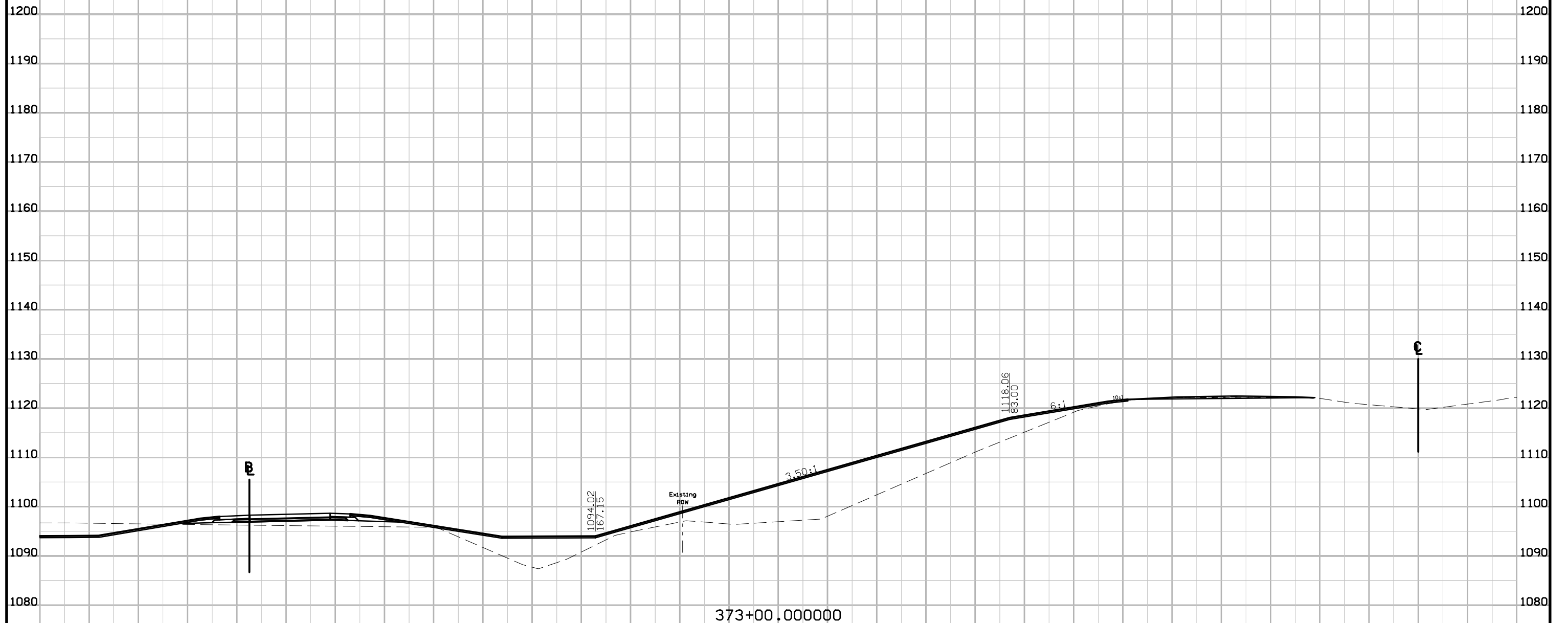
FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	<b>HAMILTON</b> COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.154</b>
----------	---------	--	------------------------	---	---------------------------

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant



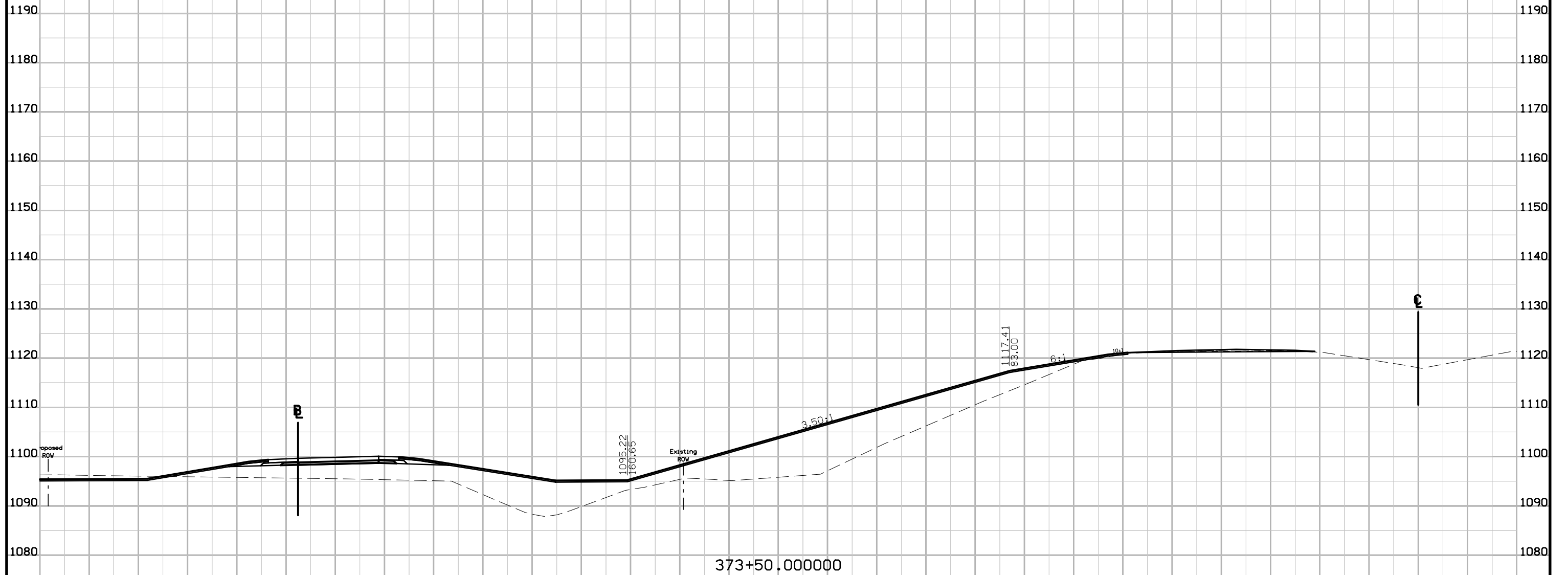
373+00.000000

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

## D5 - ROW Submittal Oct 2016

# I-35 SB in Ramp A Quadrant

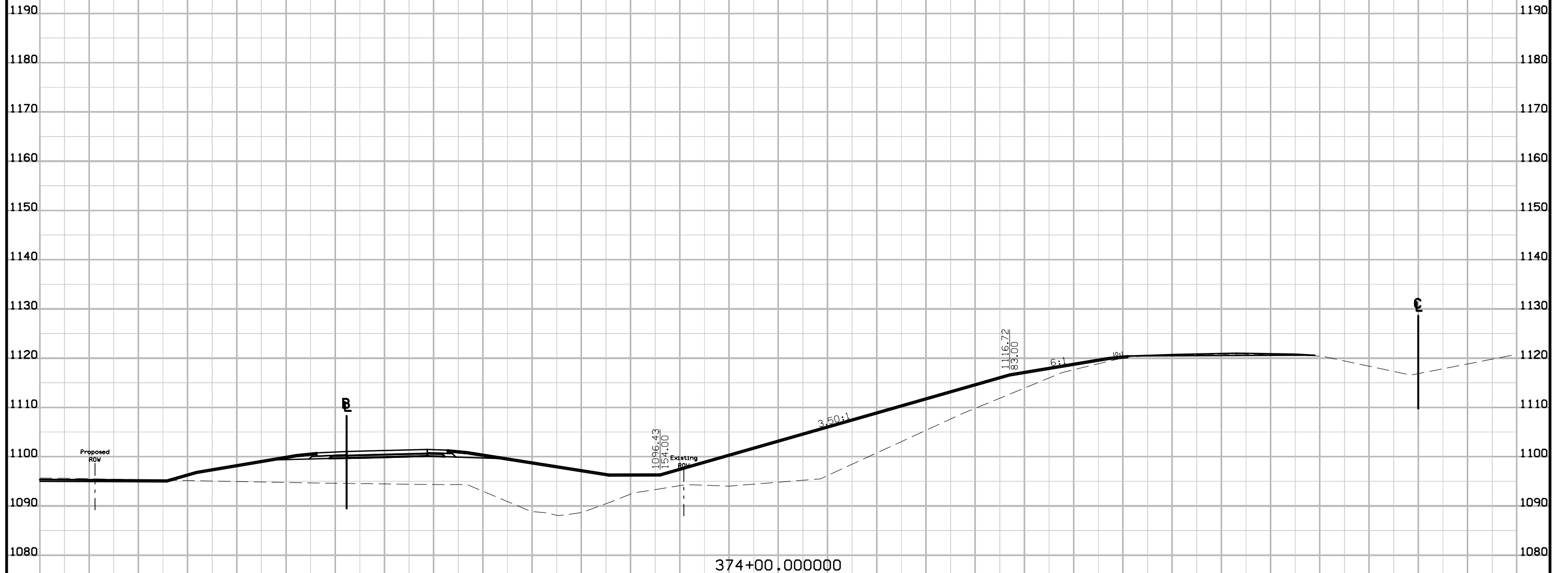


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant



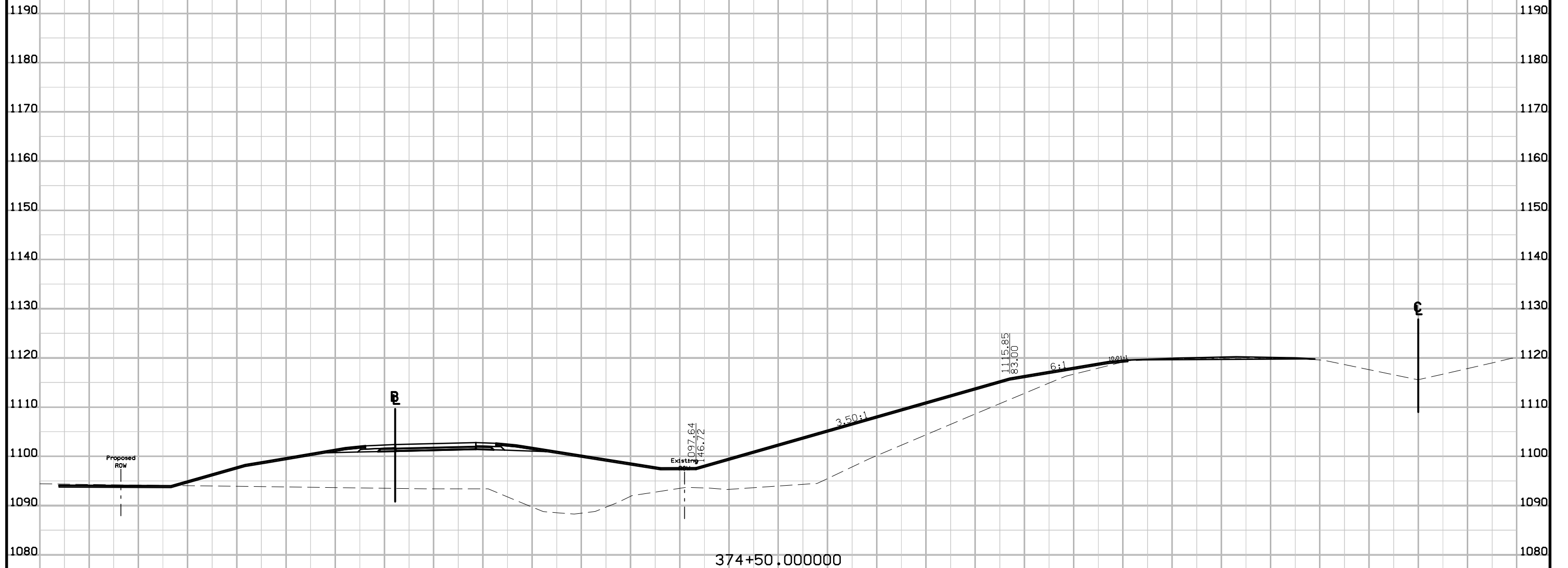
FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	<b>HAMILTON</b> COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.157</b>
----------	---------	--	------------------------	---	---------------------------

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant

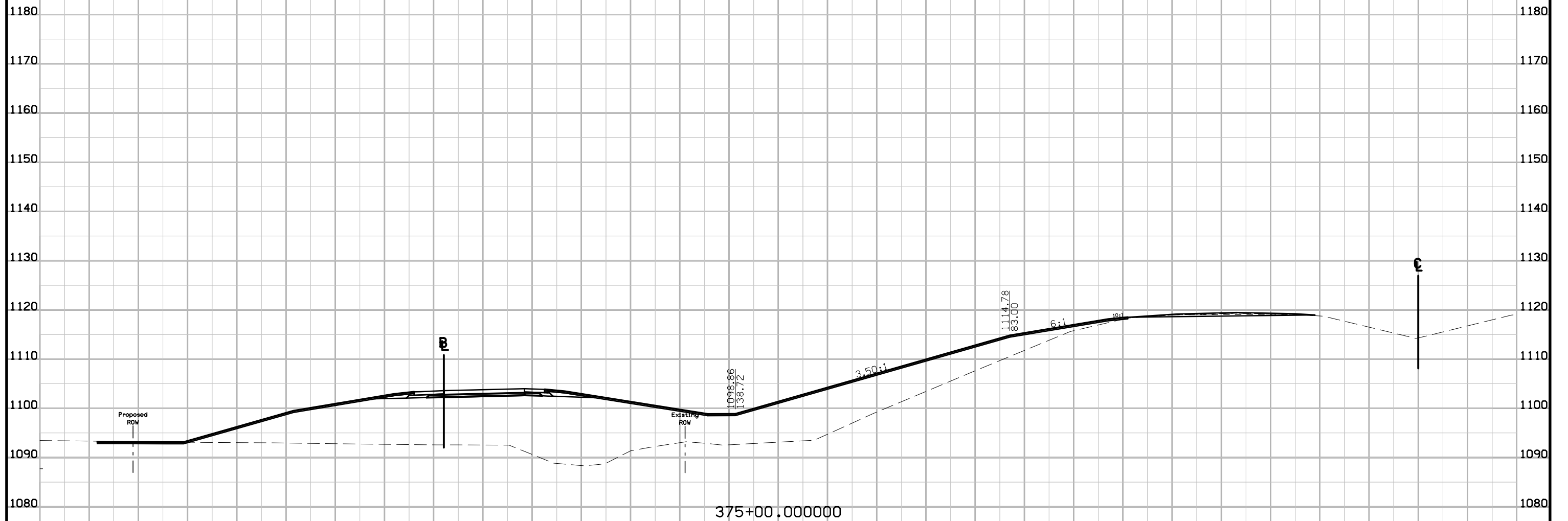


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant



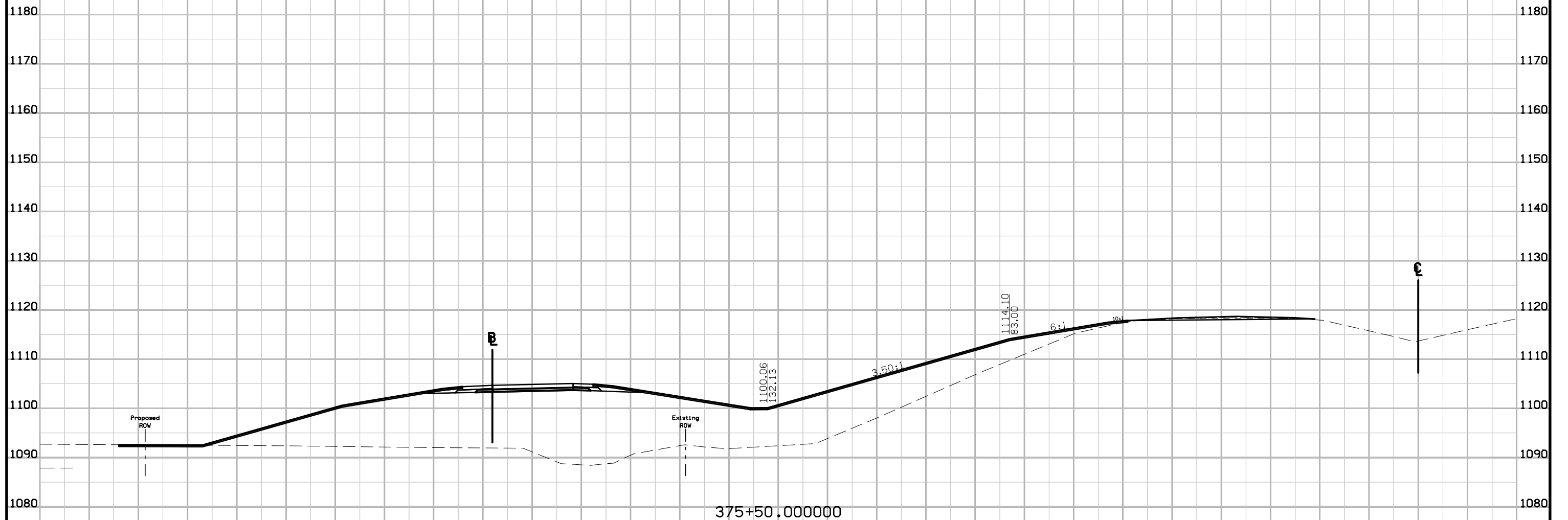
FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	<b>HAMILTON</b> COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.159</b>
----------	---------	--	------------------------	---	---------------------------

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant



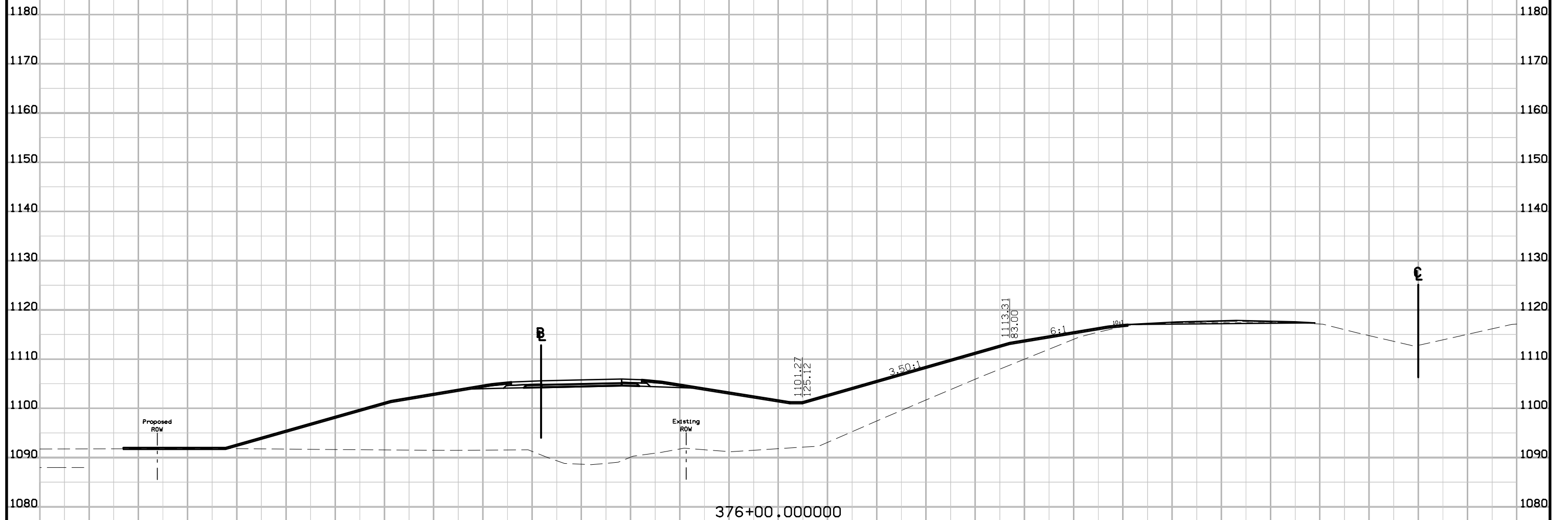
280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

## D5 - ROW Submittal Oct 2016

# I-35 SB in Ramp A Quadrant



FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	<b>HAMILTON</b> COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.161</b>
----------	---------	--	------------------------	---	---------------------------

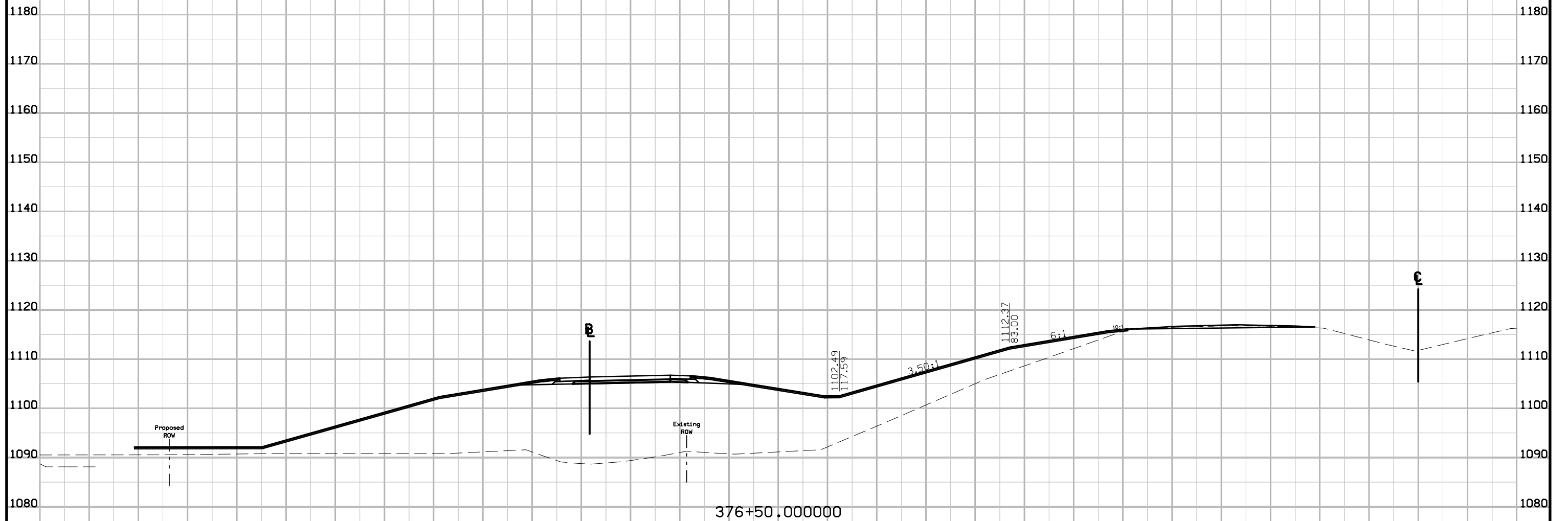


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant



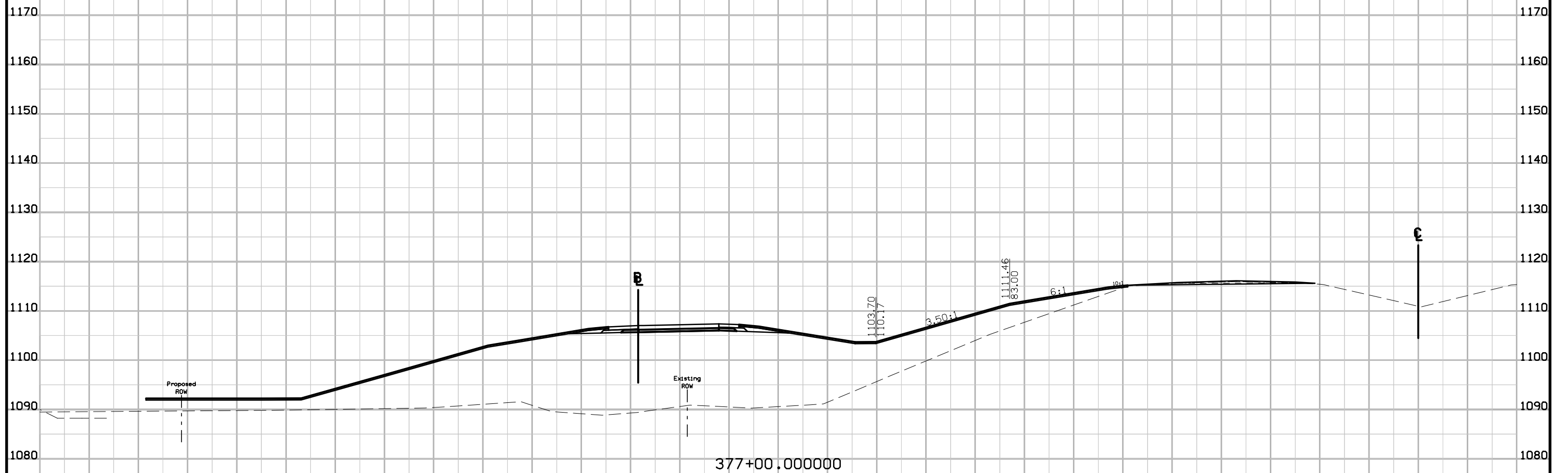
280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant



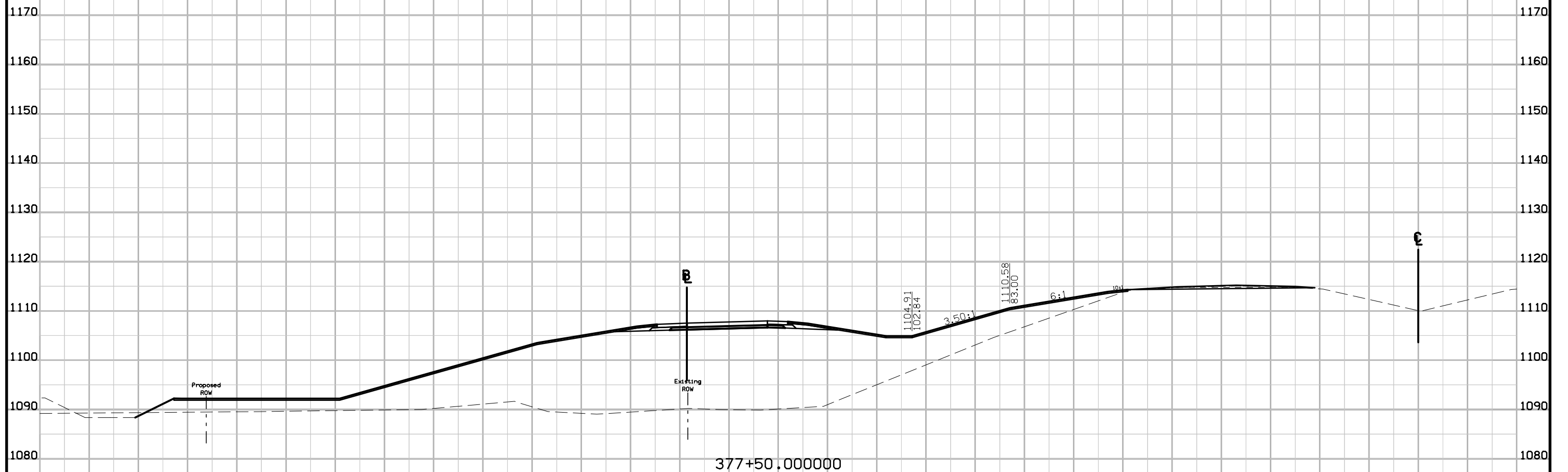
377+00.000000

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant

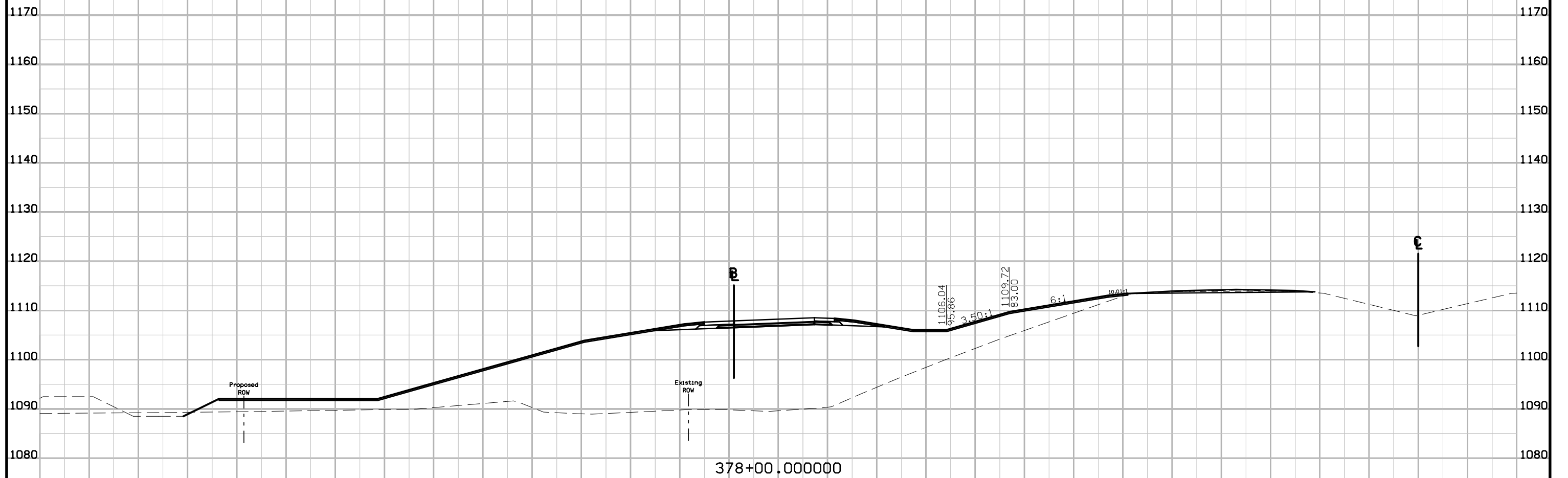


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant



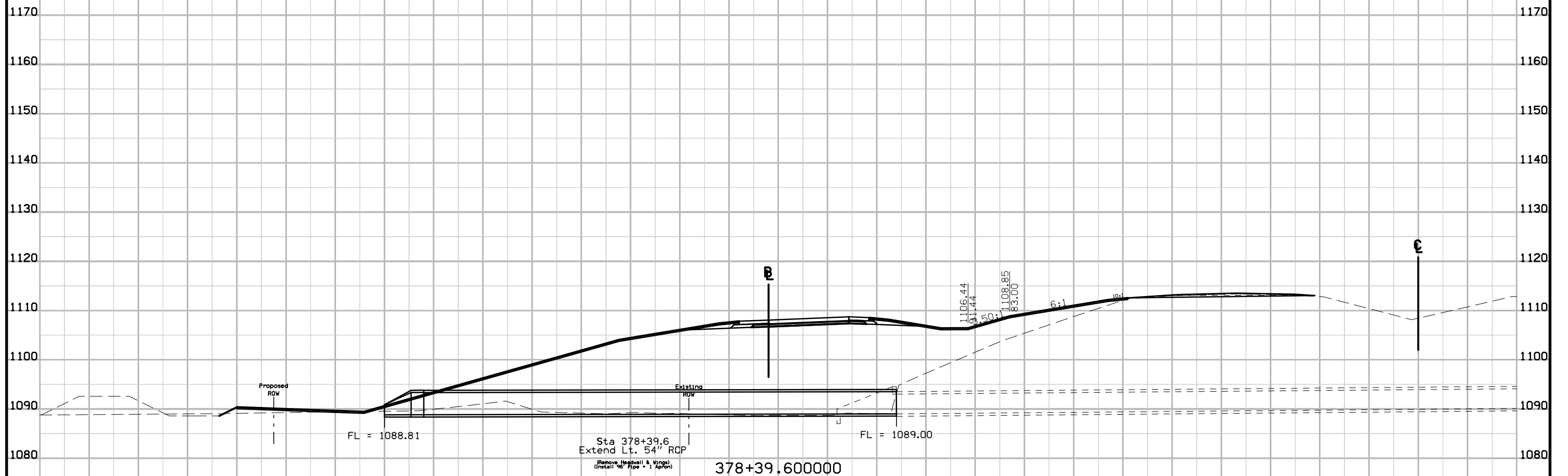
FILE NO.	ENGLISH	DESIGN TEAM	T. Gustafson/J. Vortherms	HAMILTON COUNTY	PROJECT NUMBER	IM-035-5(110)133--13-40	SHEET NUMBER	W.165
----------	---------	-------------	---------------------------	-----------------	----------------	-------------------------	--------------	-------

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

## D5 - ROW Submittal Oct 2016

# I-35 SB in Ramp A Quadrant



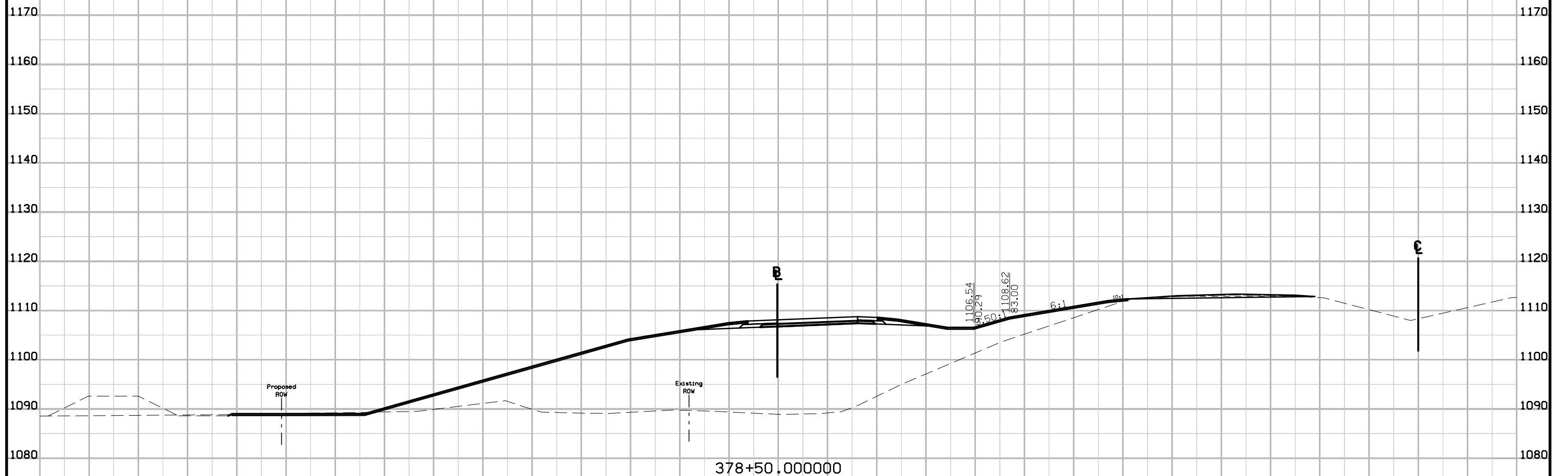
FILE NO.	ENGLISH	DESIGN TEAM <b>T. Gustafson/J. Vortherms</b>	HAMILTON COUNTY	PROJECT NUMBER <b>IM-035-5(110)133--13-40</b>	SHEET NUMBER <b>W.166</b>
----------	---------	--	-----------------	---	---------------------------

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant

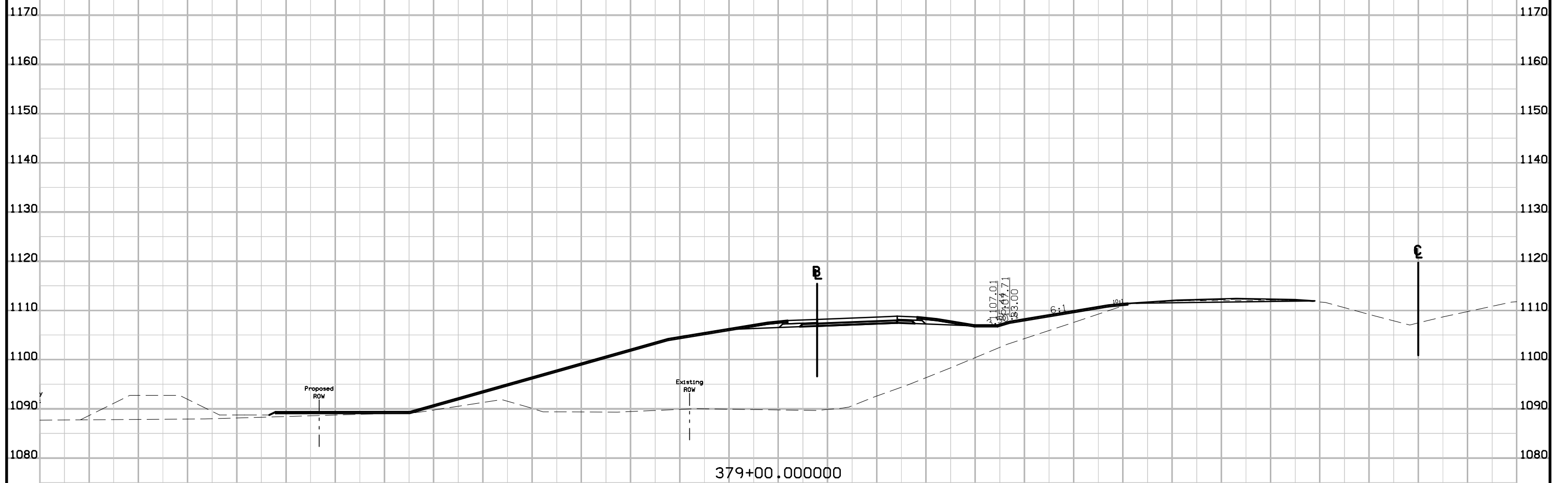


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant

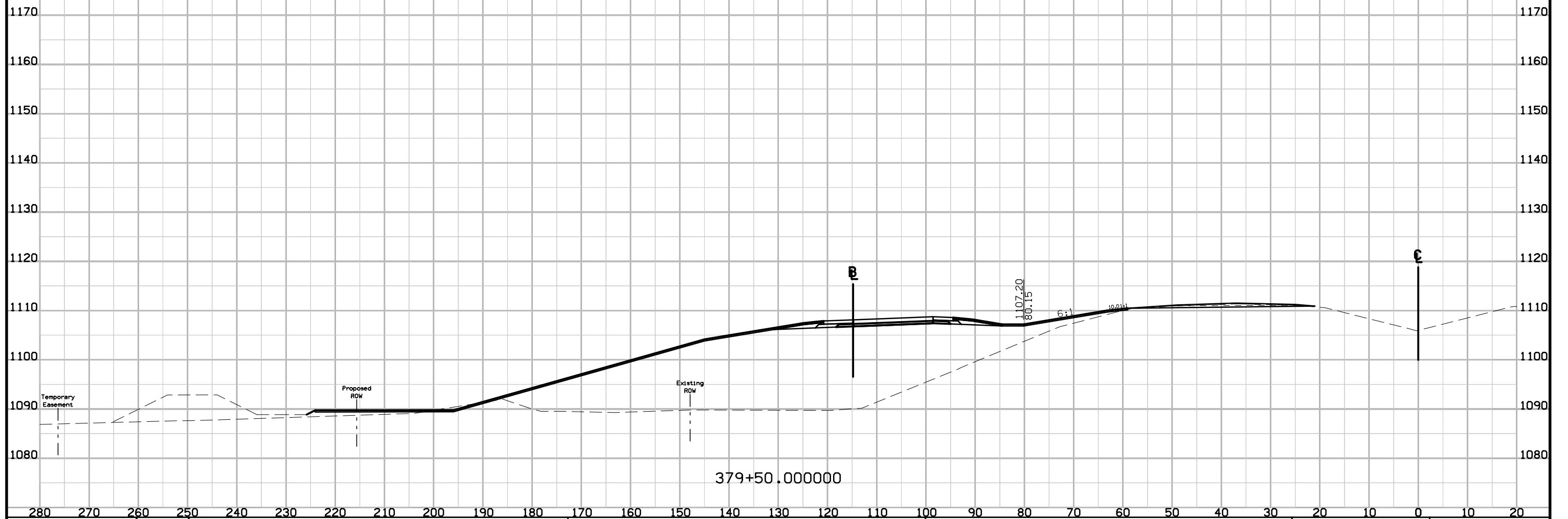


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

## D5 - ROW Submittal Oct 2016

# I-35 SB in Ramp A Quadrant



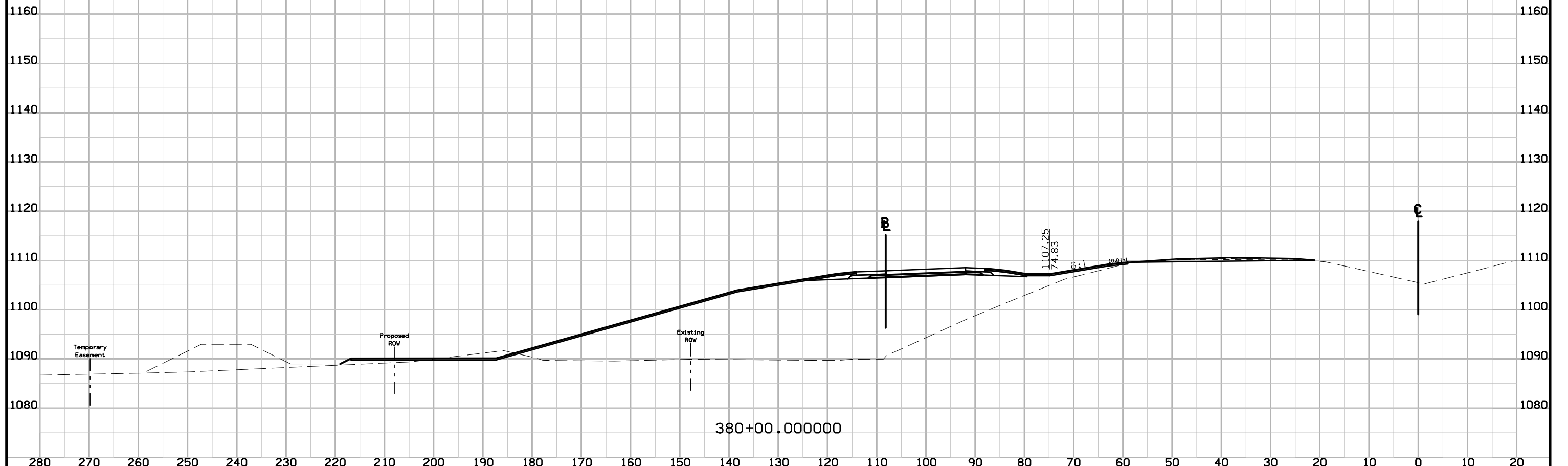


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

## D5 - ROW Submittal Oct 2016

# I-35 SB in Ramp A Quadrant

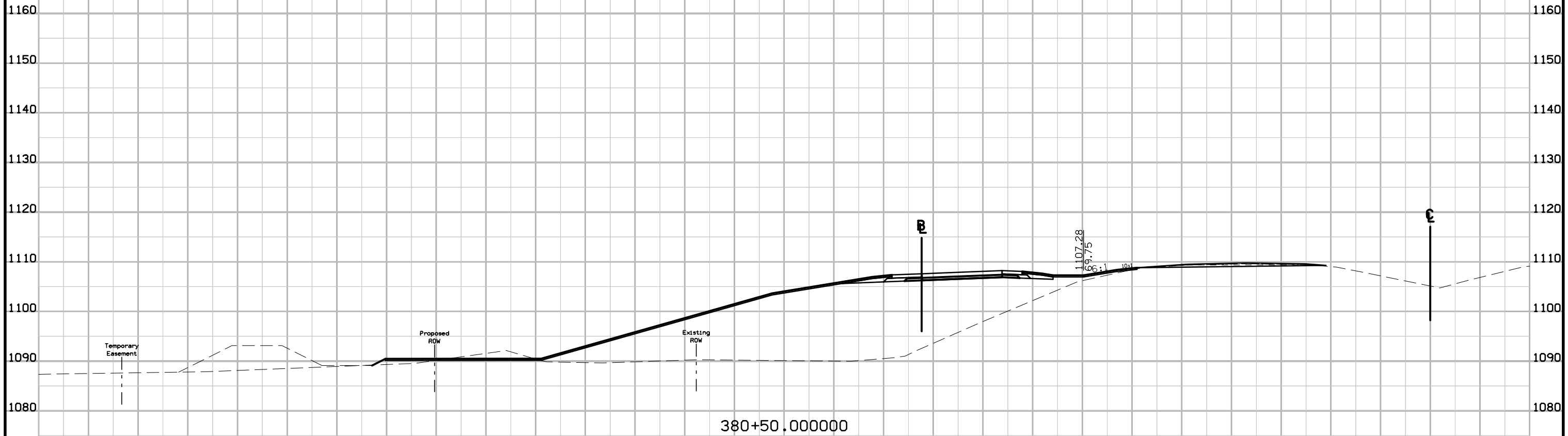
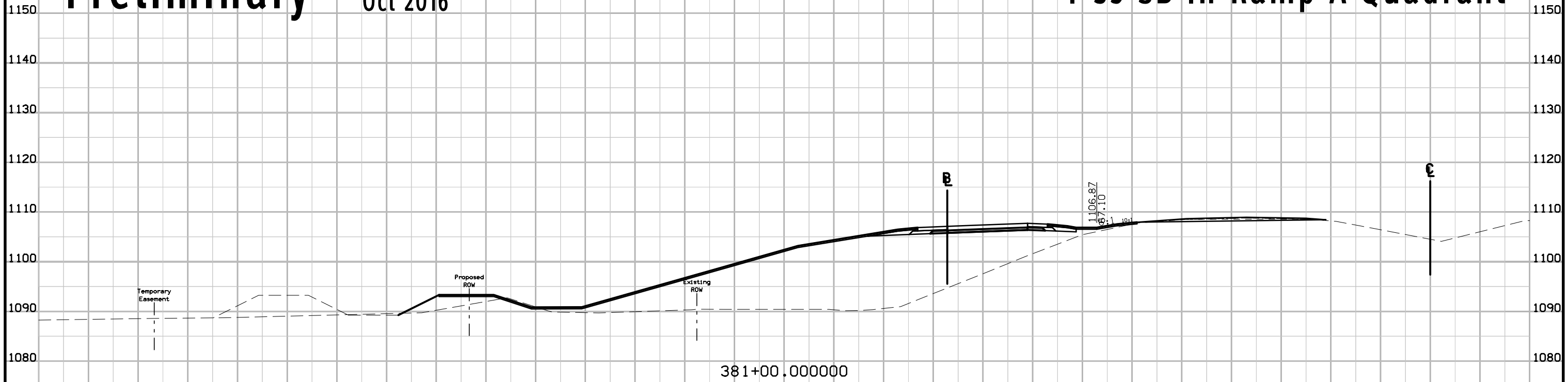


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

## D5 - ROW Submittal Oct 2016

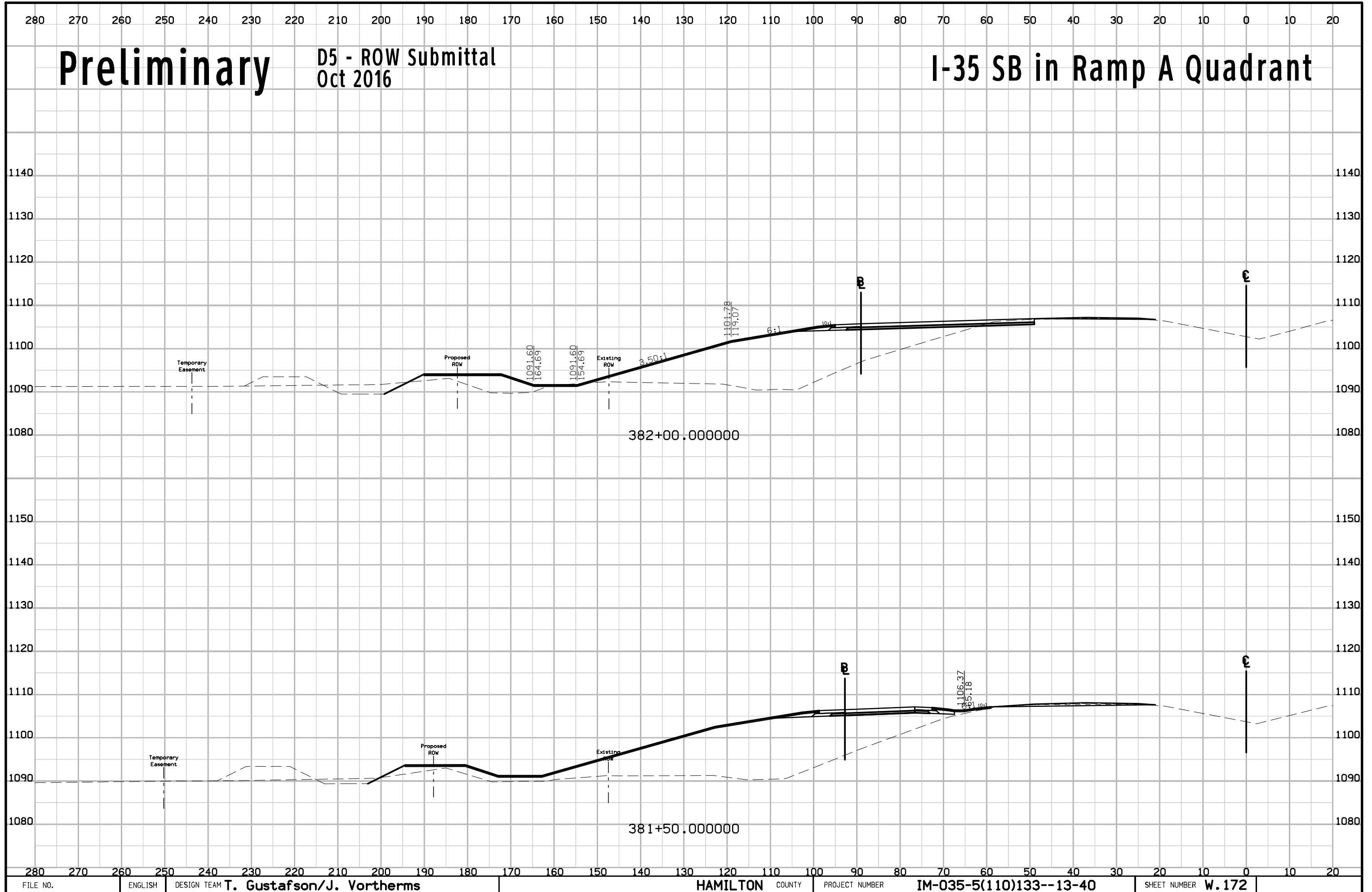
# I-35 SB in Ramp A Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

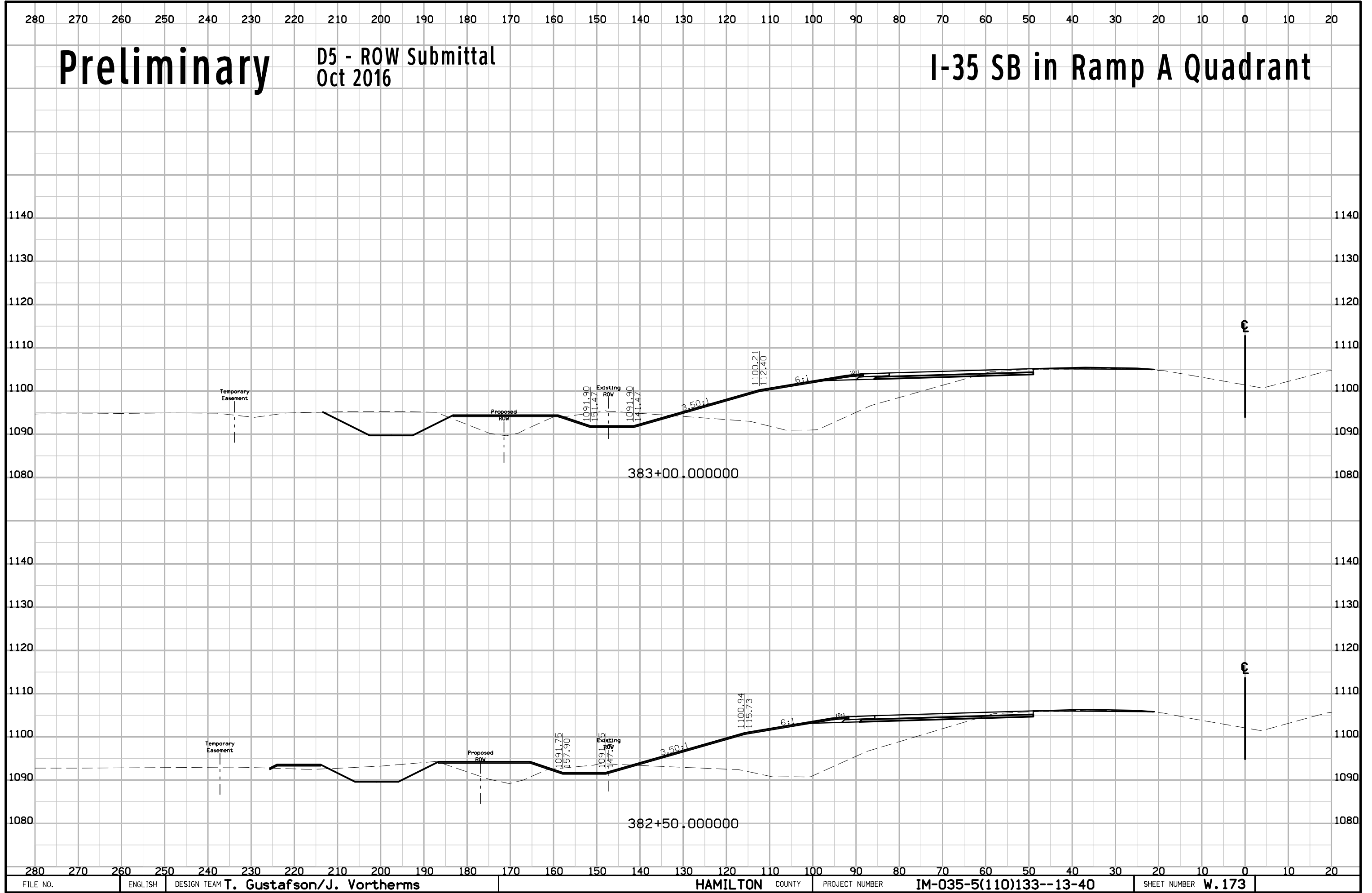
## I-35 SB in Ramp A Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

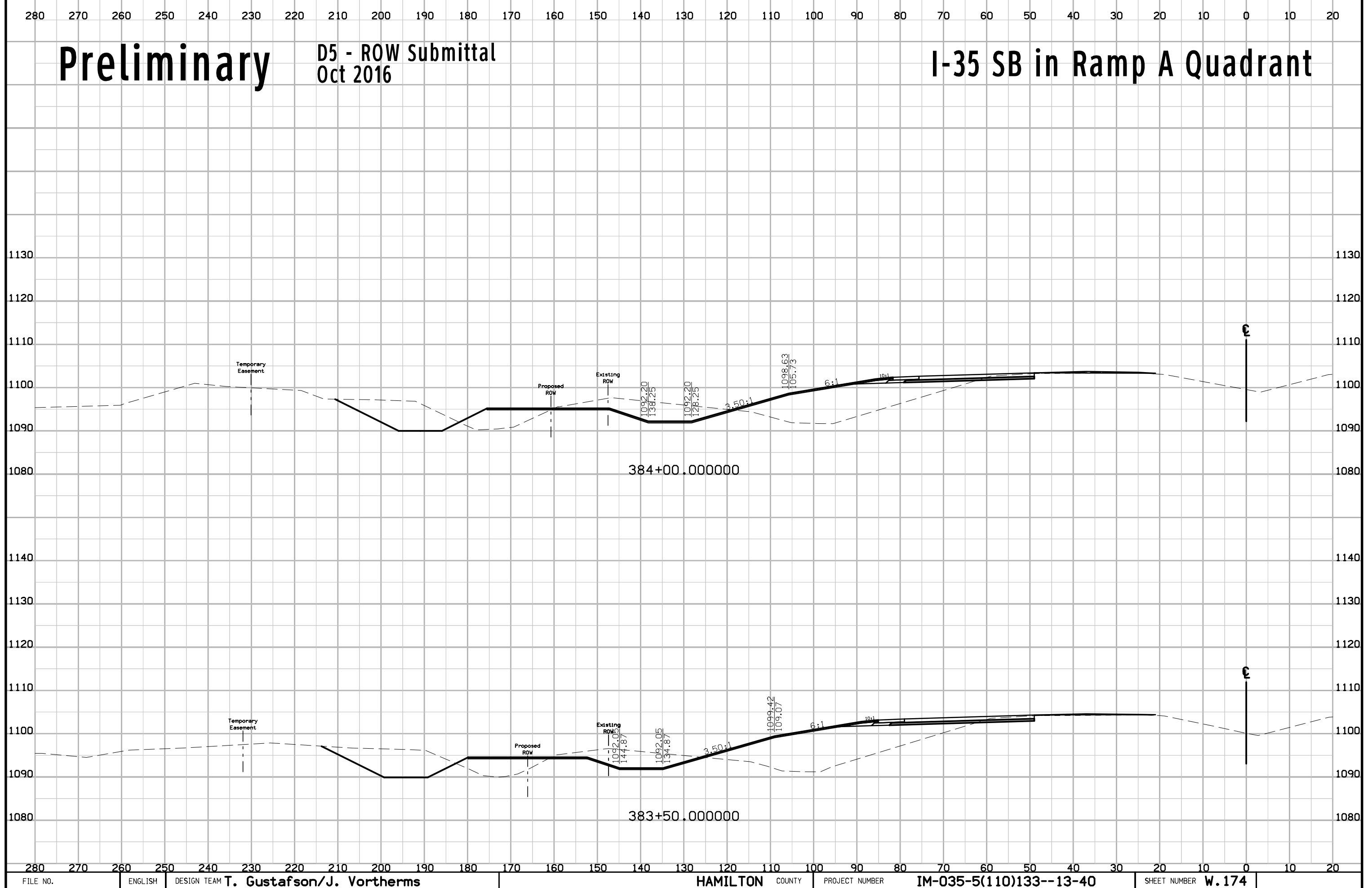
## I-35 SB in Ramp A Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

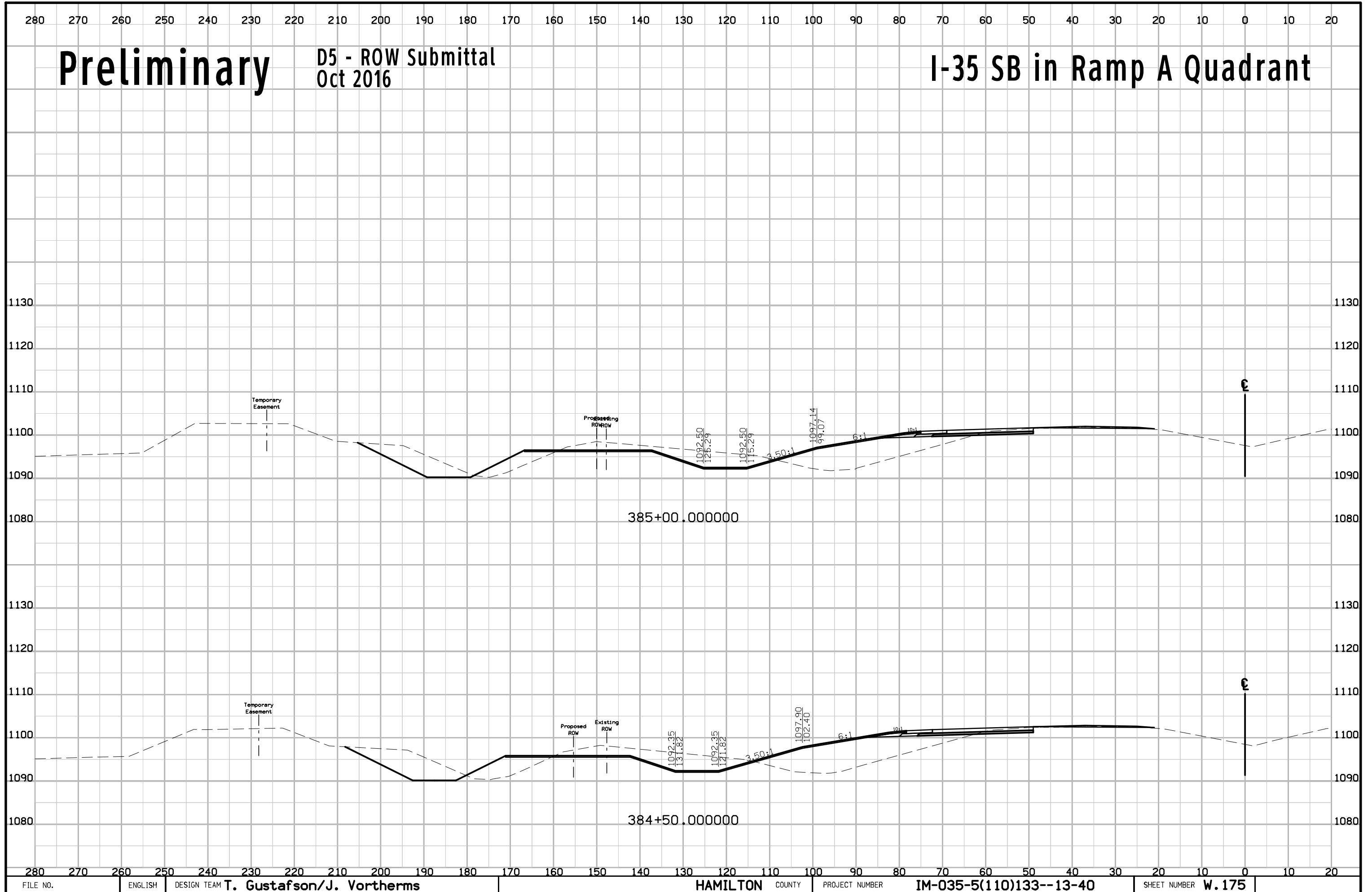
## I-35 SB in Ramp A Quadrant



# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant

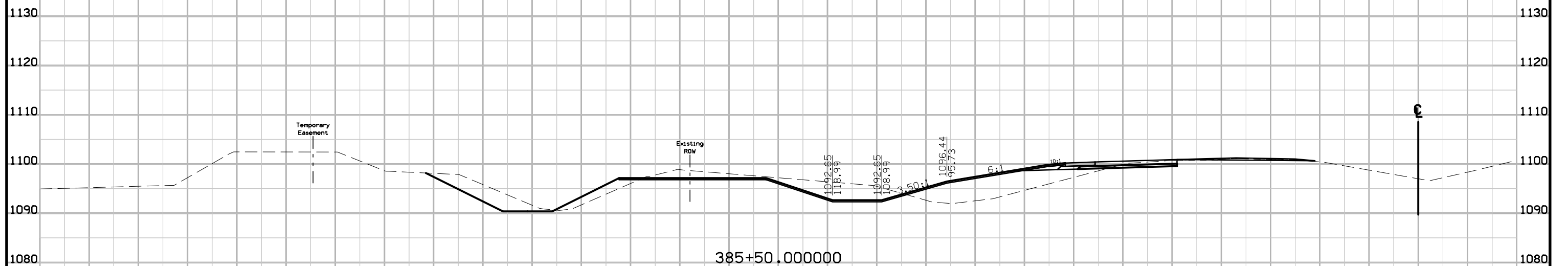
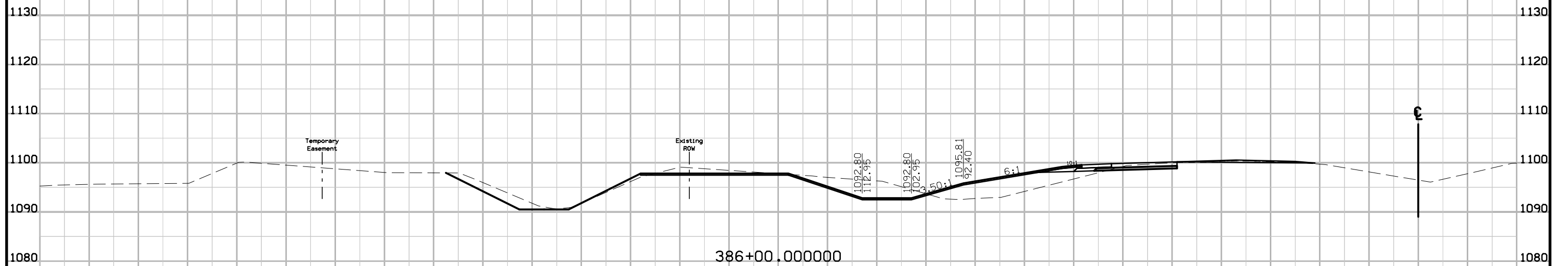


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant

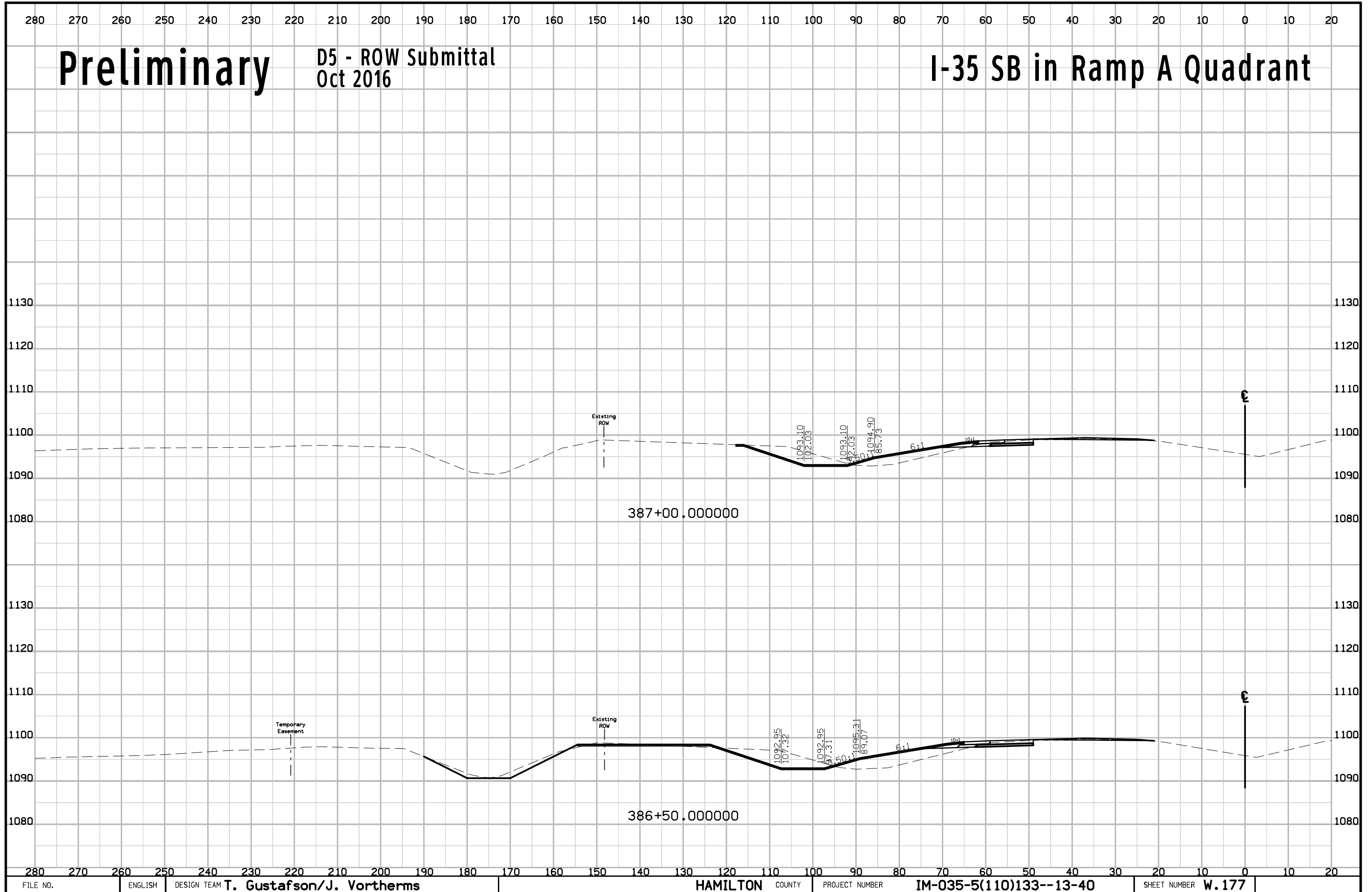


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant

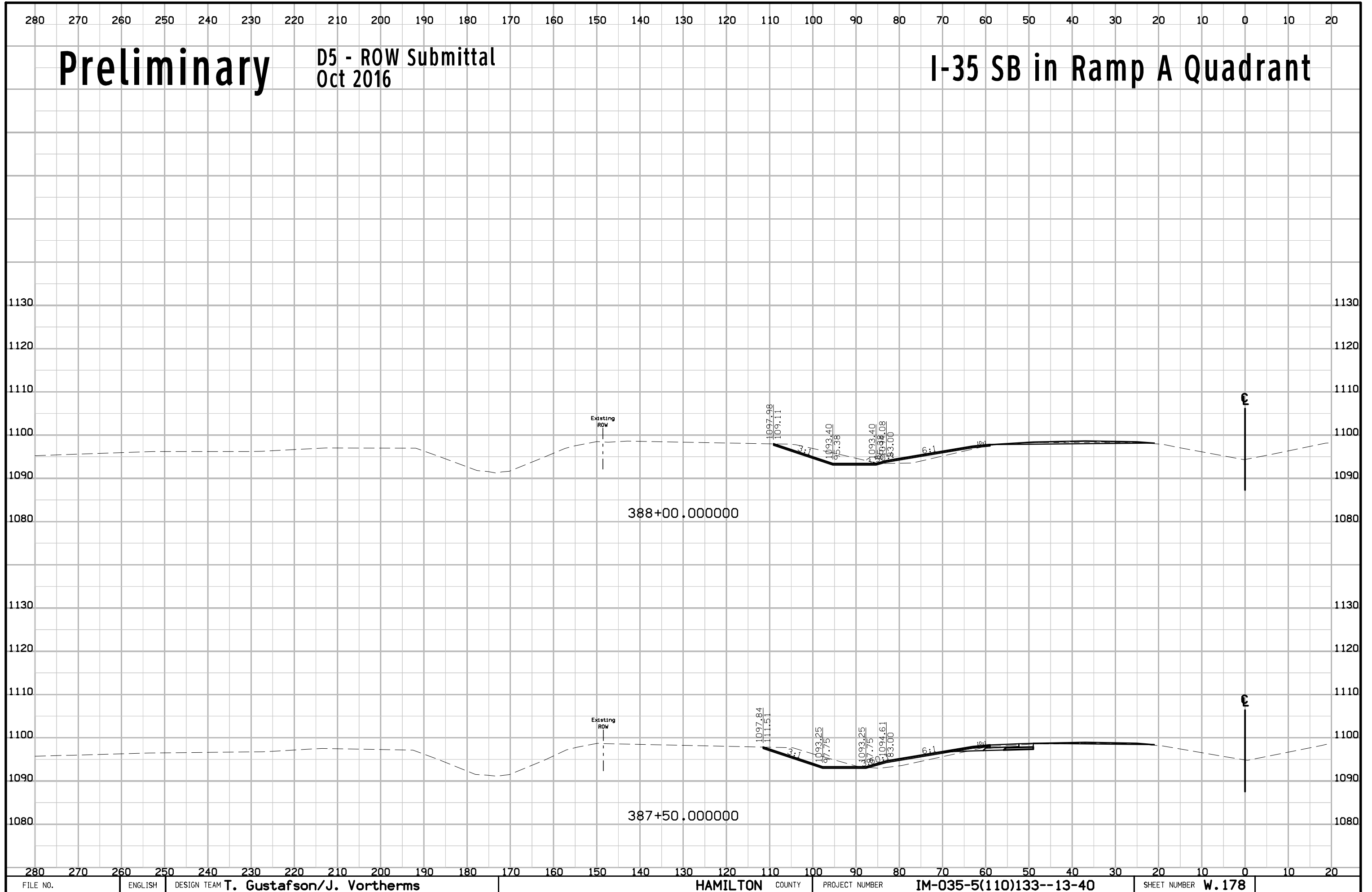




# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant

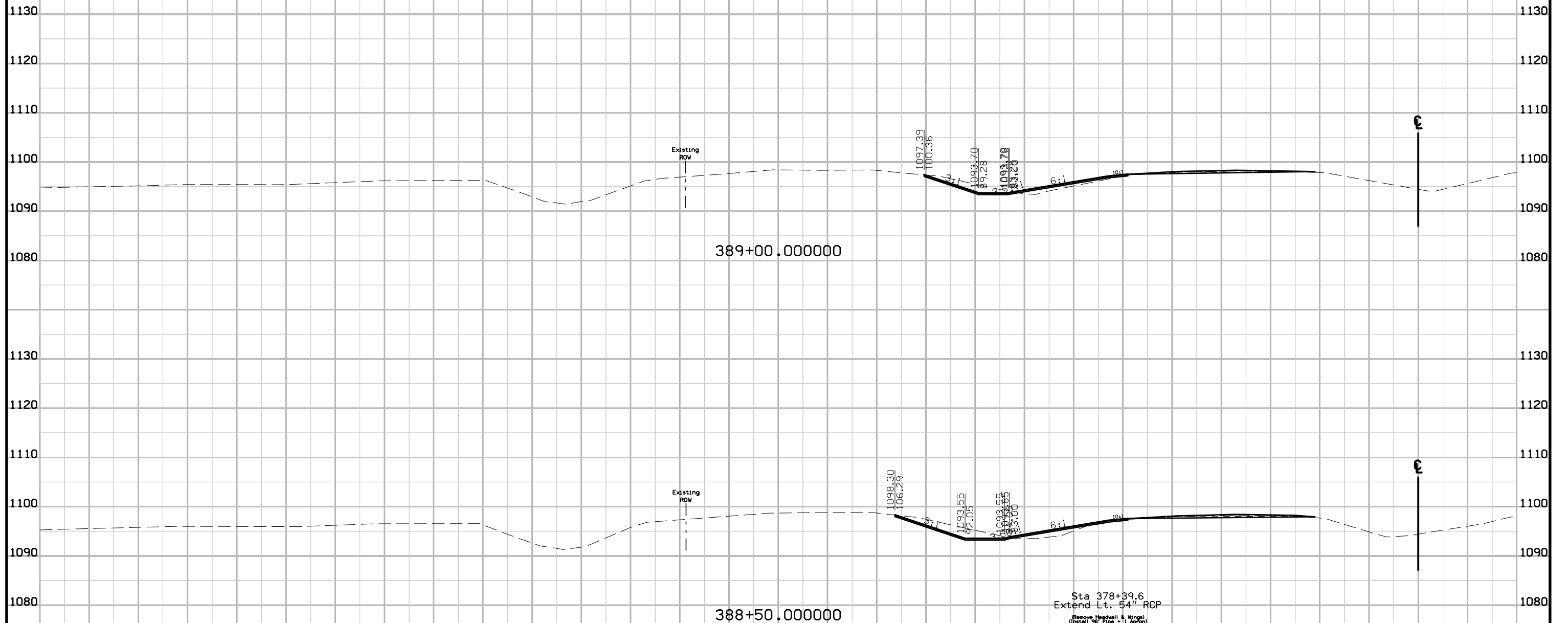


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

## D5 - ROW Submittal Oct 2016

# I-35 SB in Ramp A Quadrant



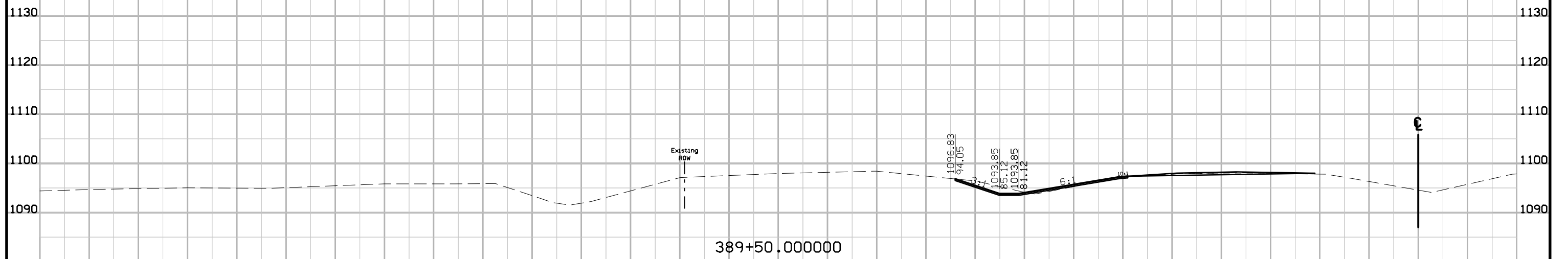
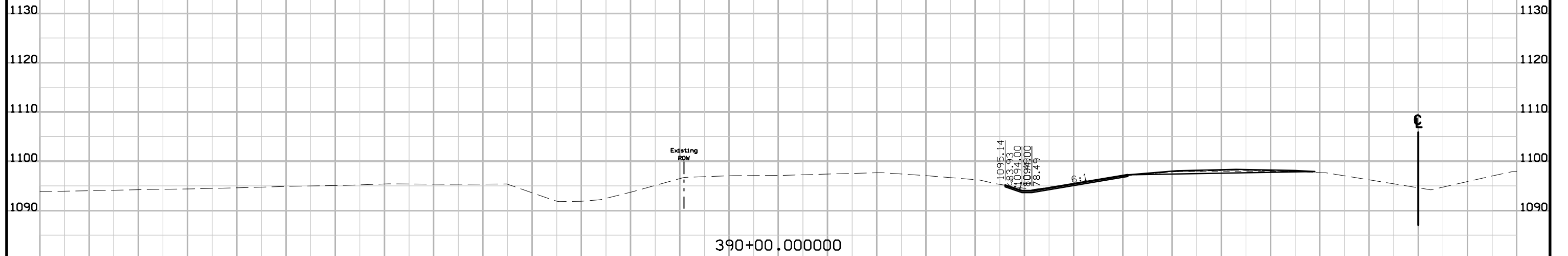
280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant

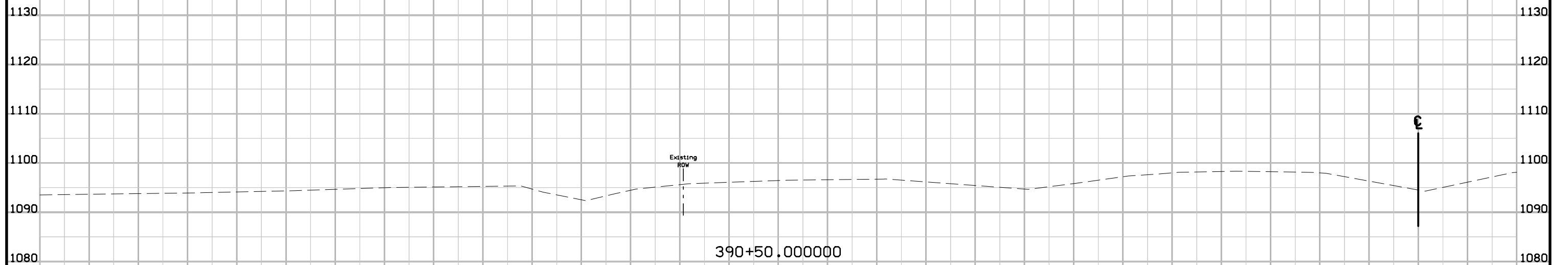
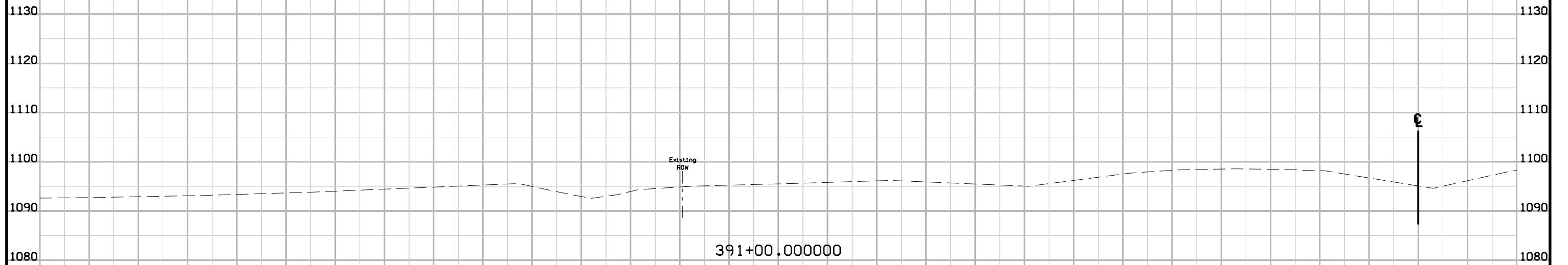


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

D5 - ROW Submittal  
Oct 2016

## I-35 SB in Ramp A Quadrant

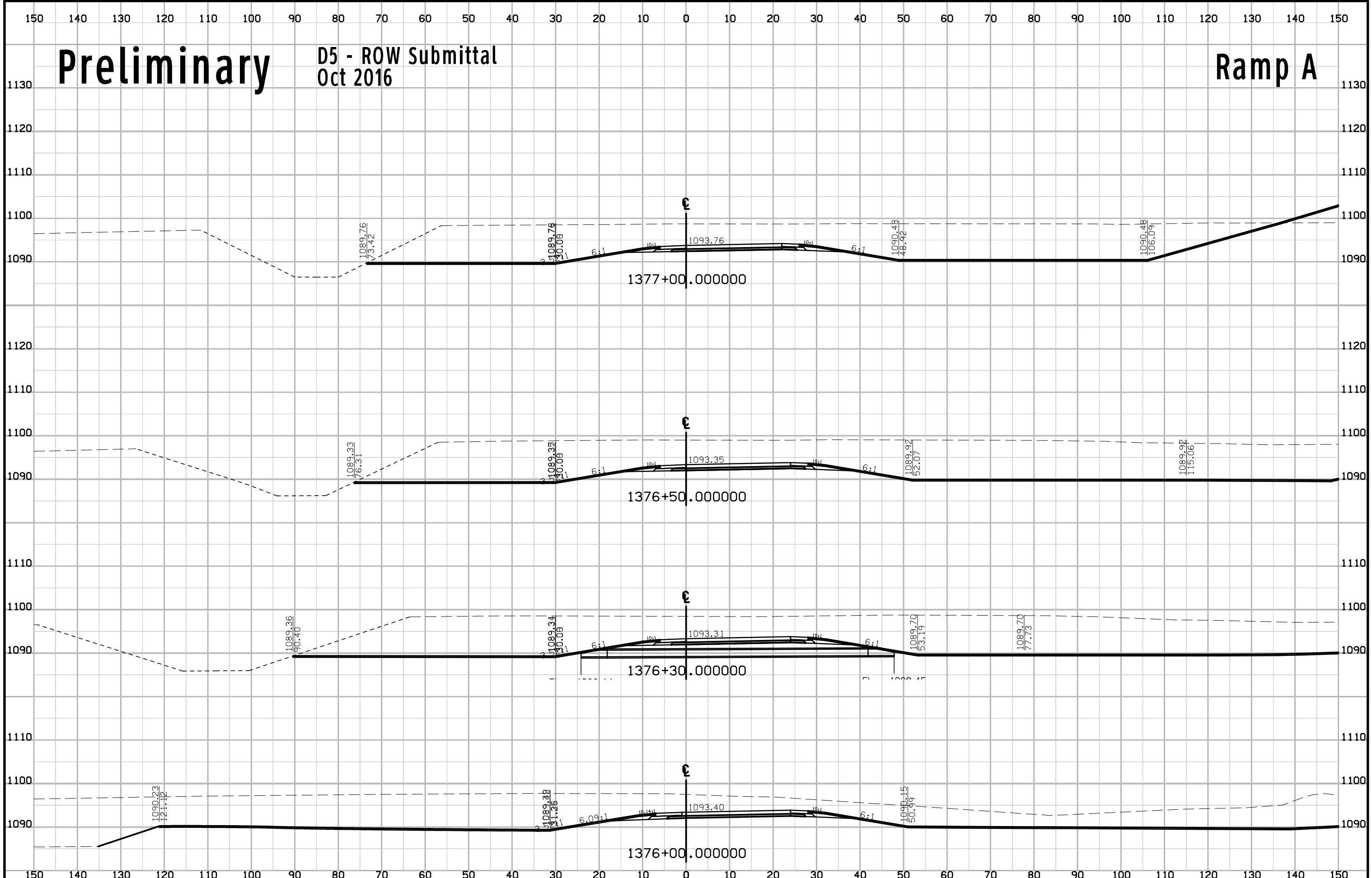


280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

# Preliminary

## D5 - ROW Submittal Oct 2016

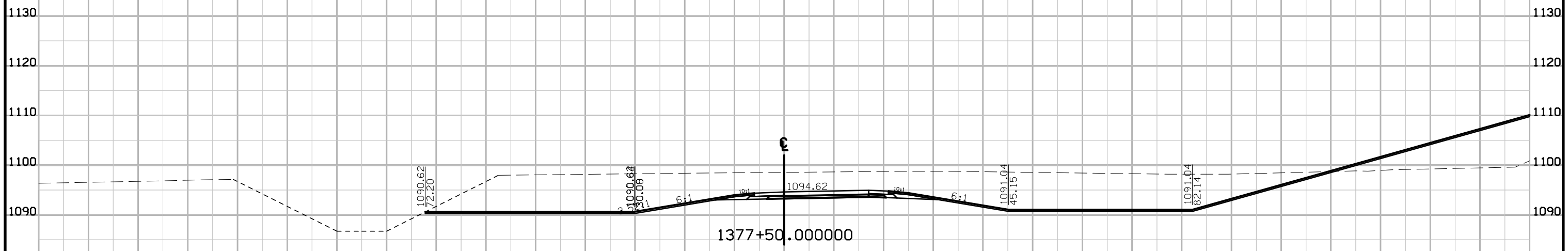
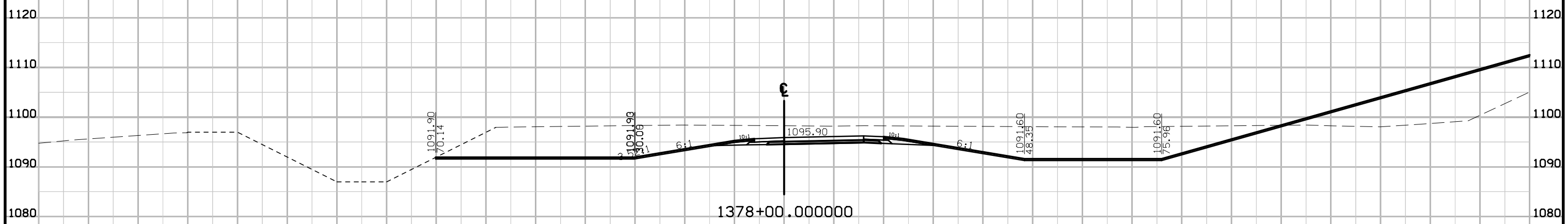
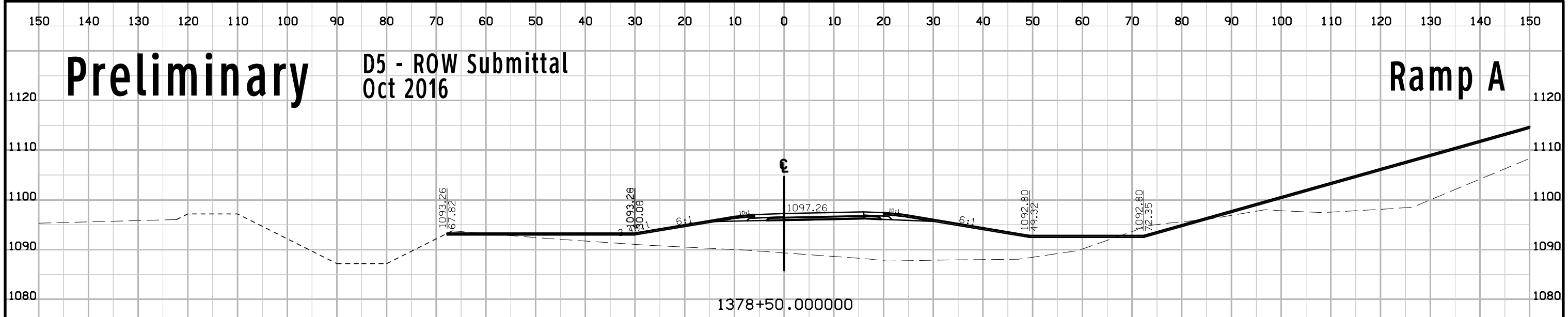
# Ramp A



# Preliminary

## D5 - ROW Submittal Oct 2016

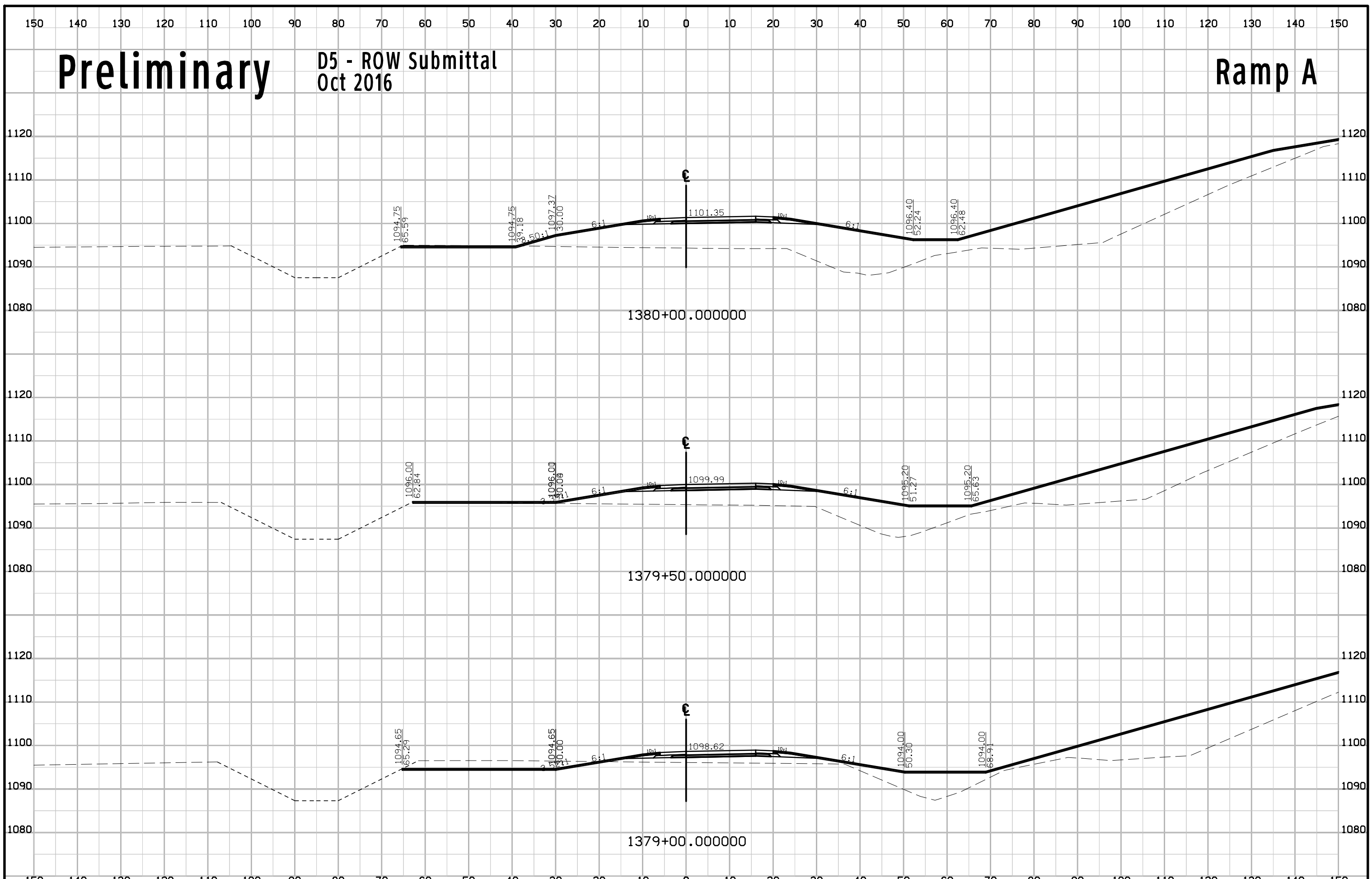
# Ramp A



# Preliminary

## D5 - ROW Submittal Oct 2016

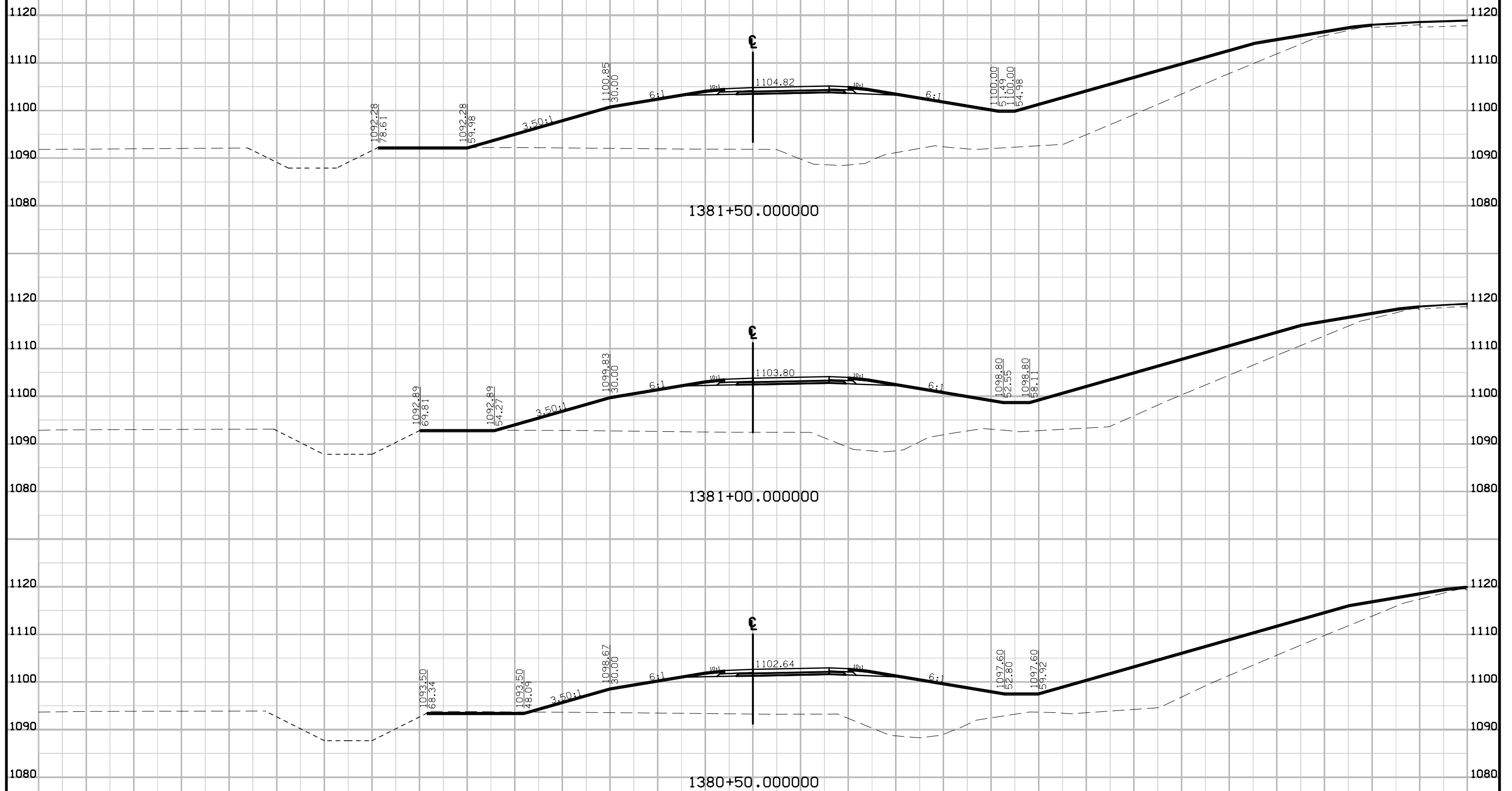
# Ramp A



# Preliminary

## D5 - ROW Submittal Oct 2016

# Ramp A

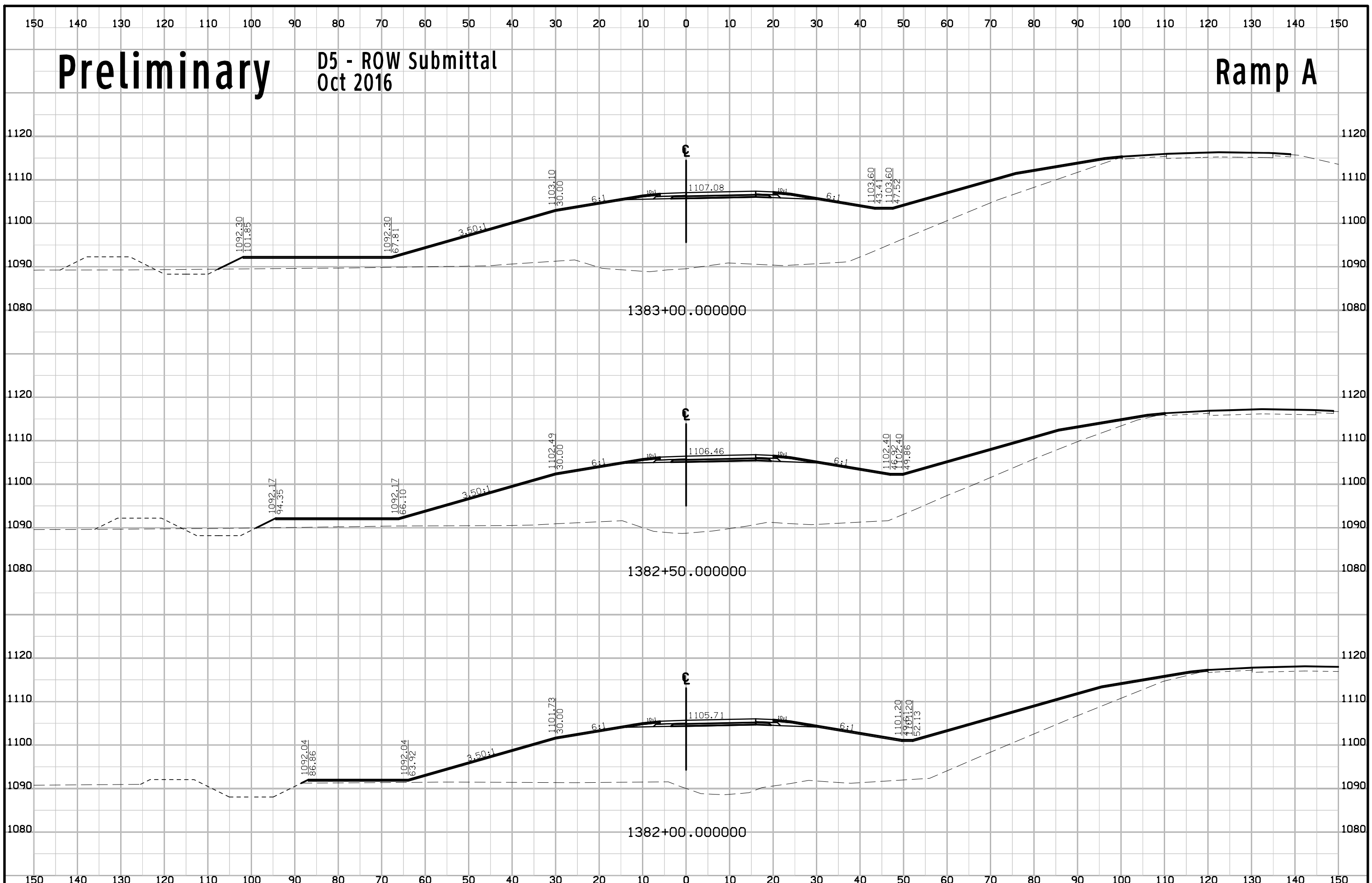




# Preliminary

## D5 - ROW Submittal Oct 2016

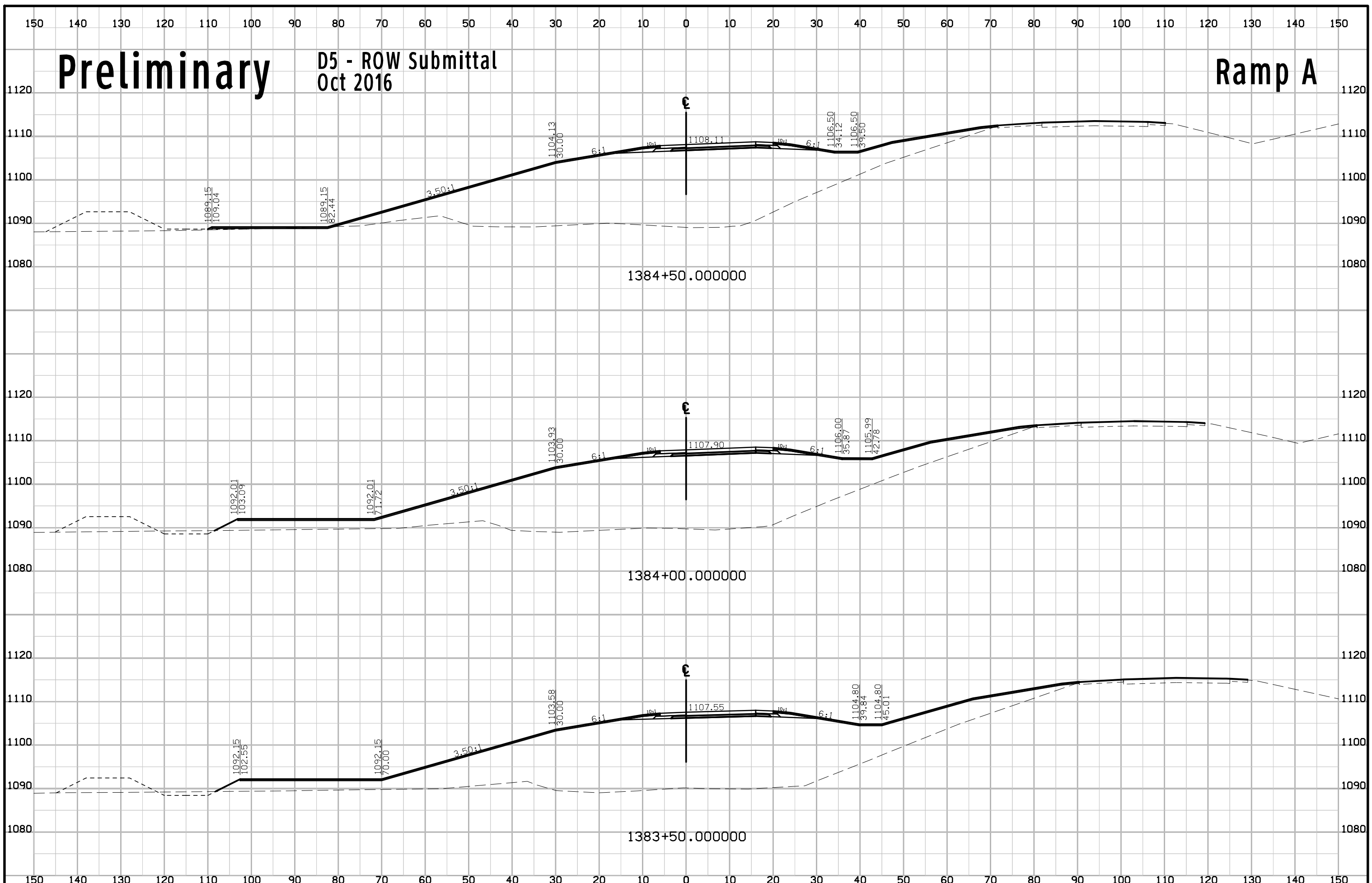
# Ramp A



# Preliminary

## D5 - ROW Submittal Oct 2016

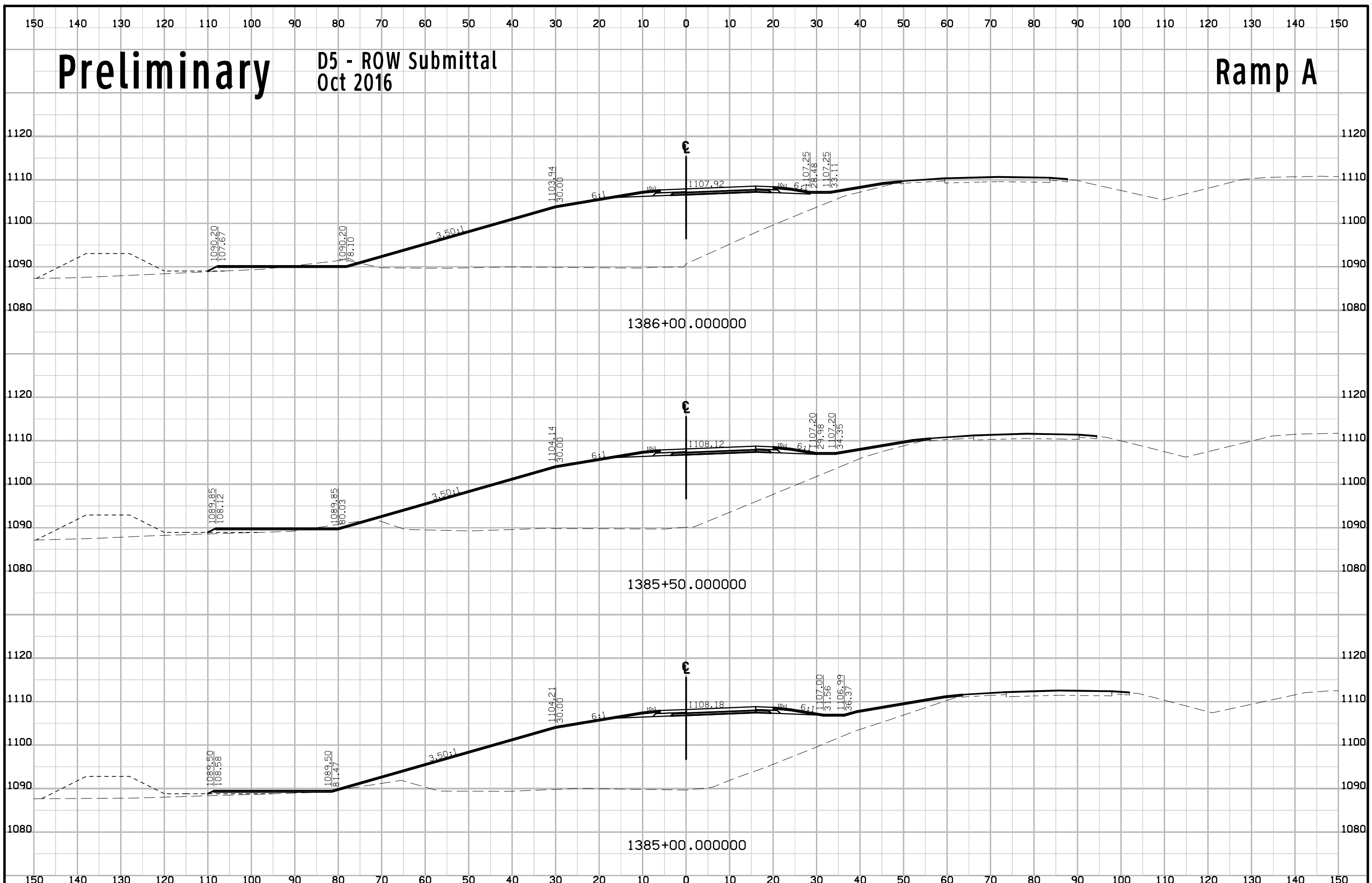
# Ramp A



# Preliminary

## D5 - ROW Submittal Oct 2016

# Ramp A

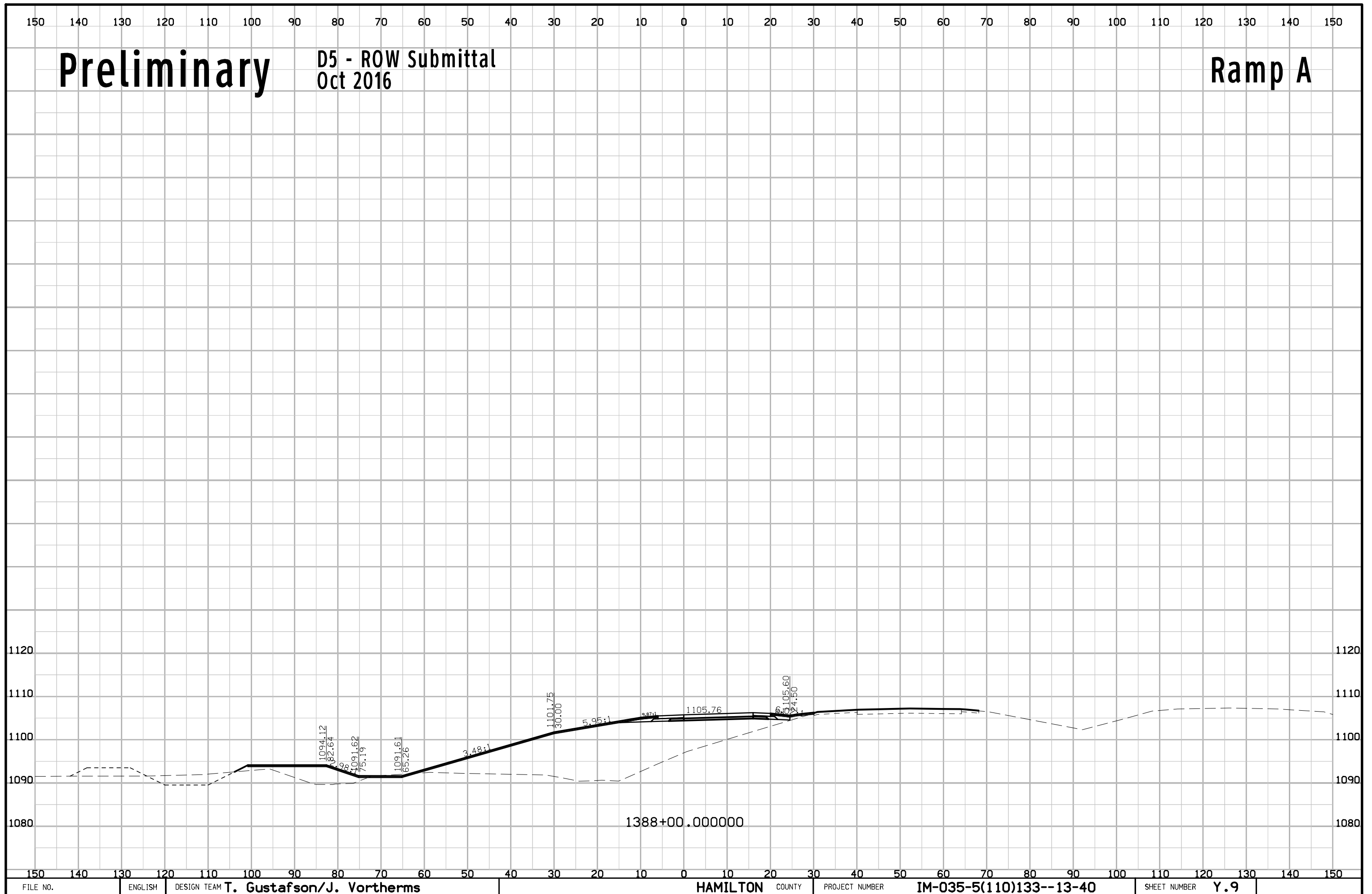




# Preliminary

## D5 - ROW Submittal Oct 2016

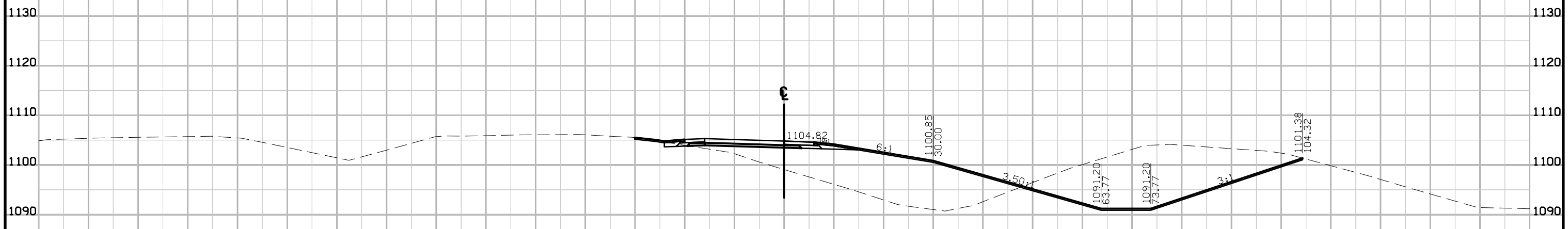
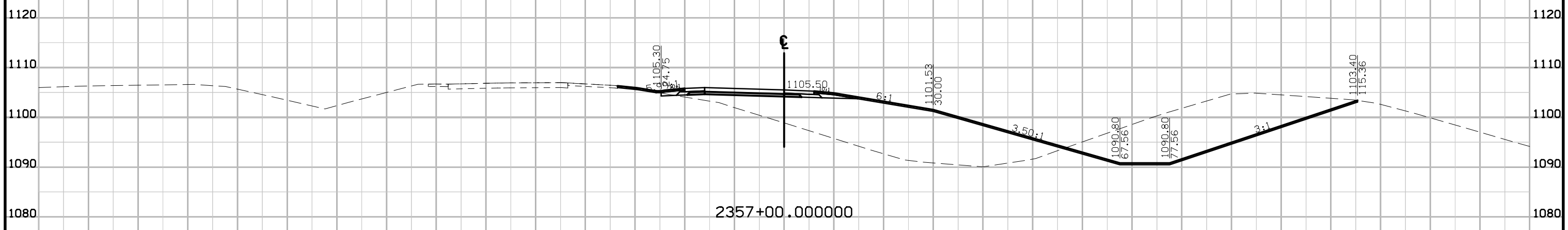
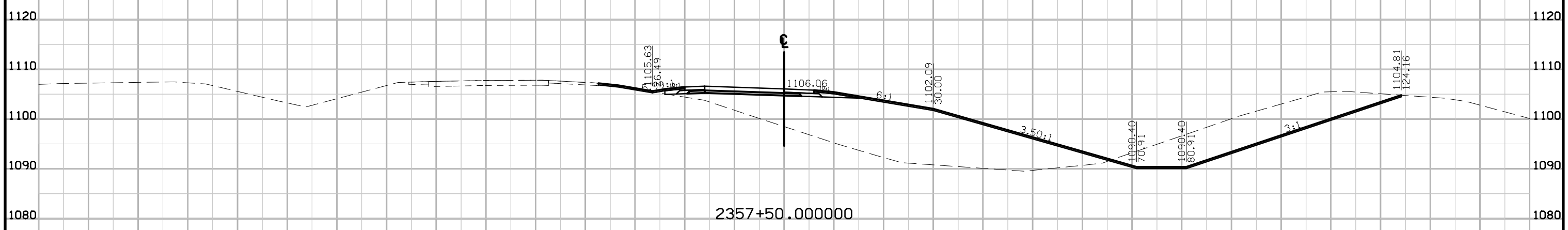
# Ramp A



# Preliminary

## D5 - ROW Submittal Oct 2016

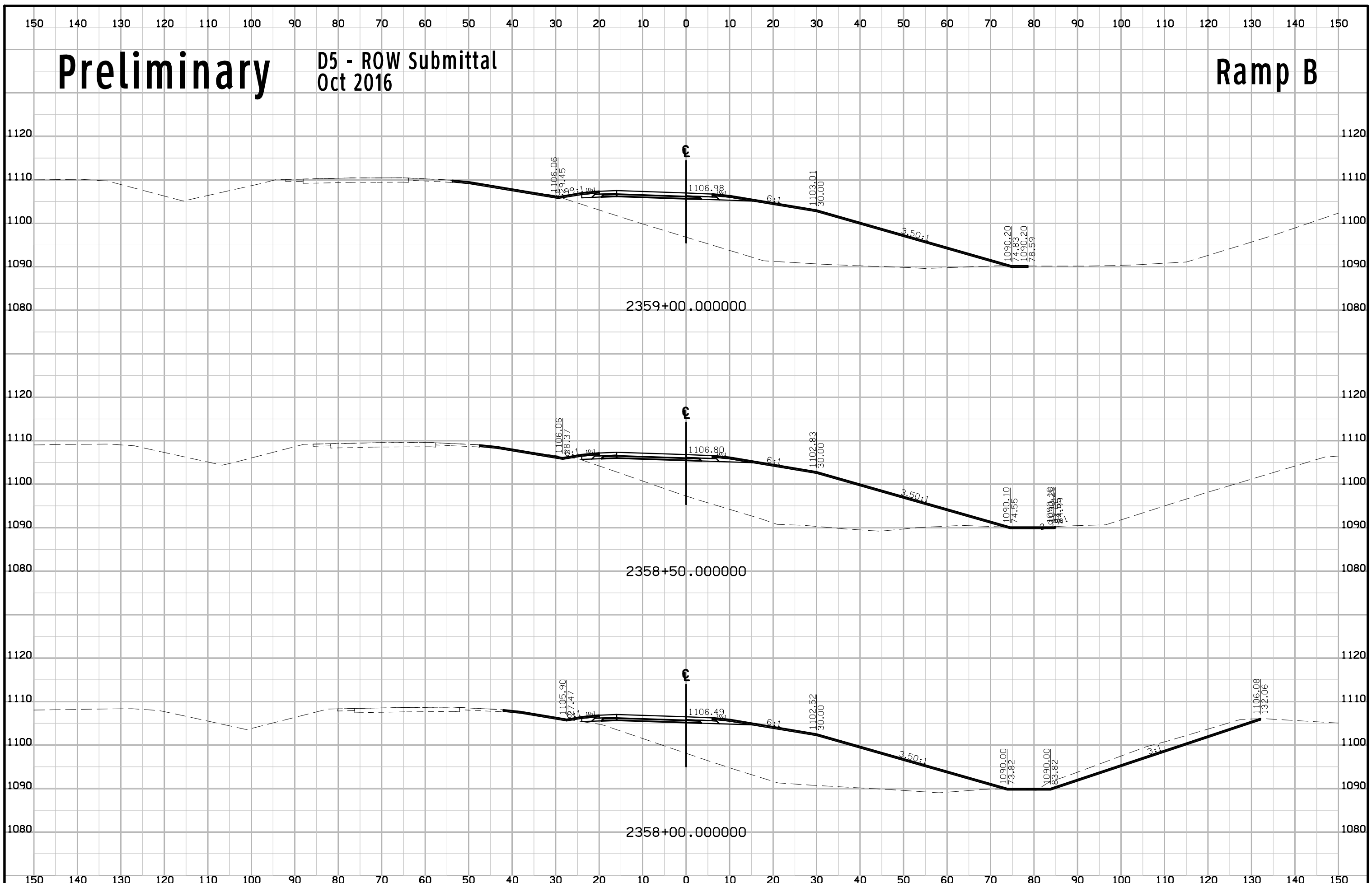
# Ramp B



# Preliminary

## D5 - ROW Submittal Oct 2016

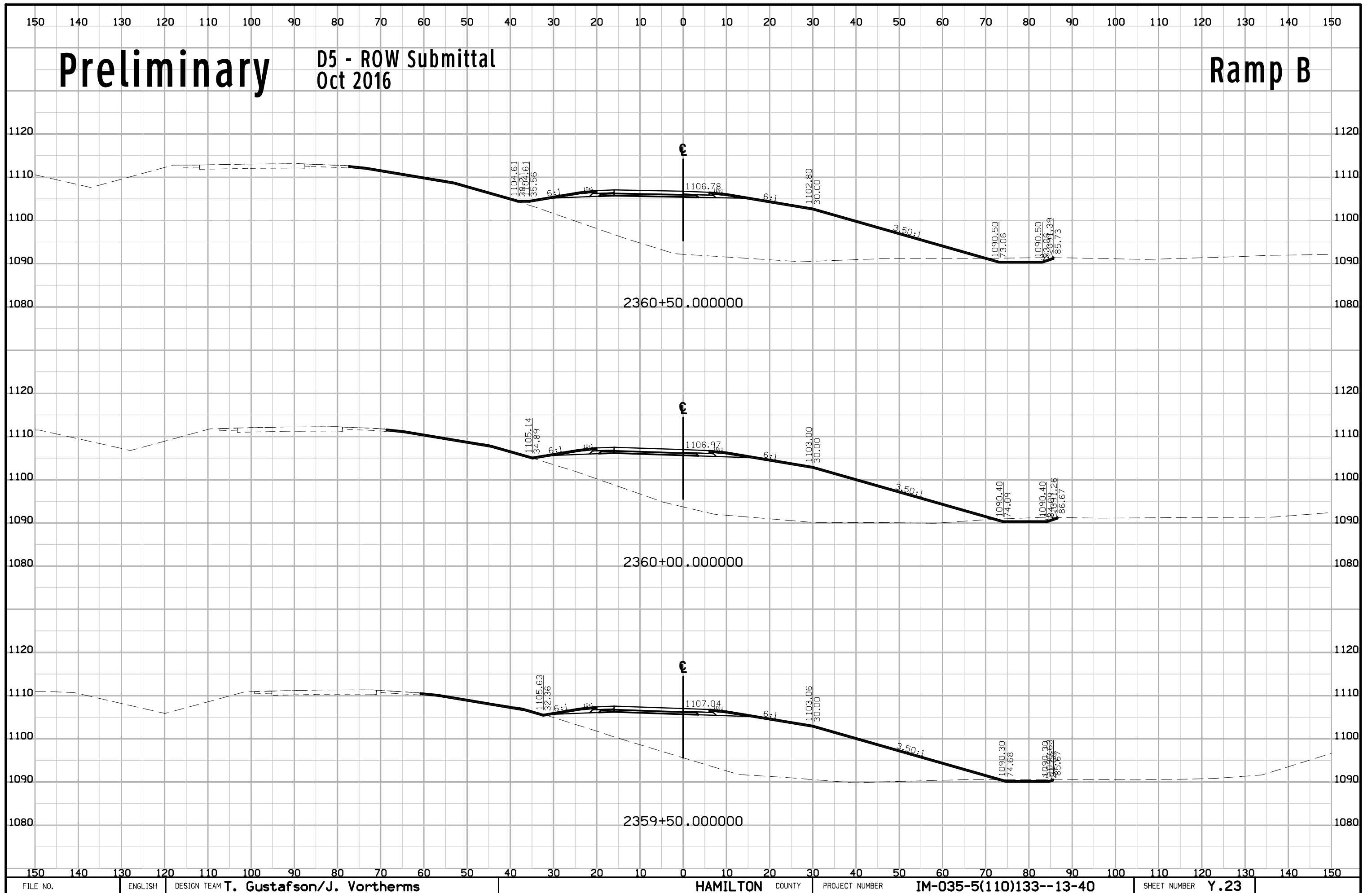
# Ramp B



# Preliminary

## D5 - ROW Submittal Oct 2016

# Ramp B

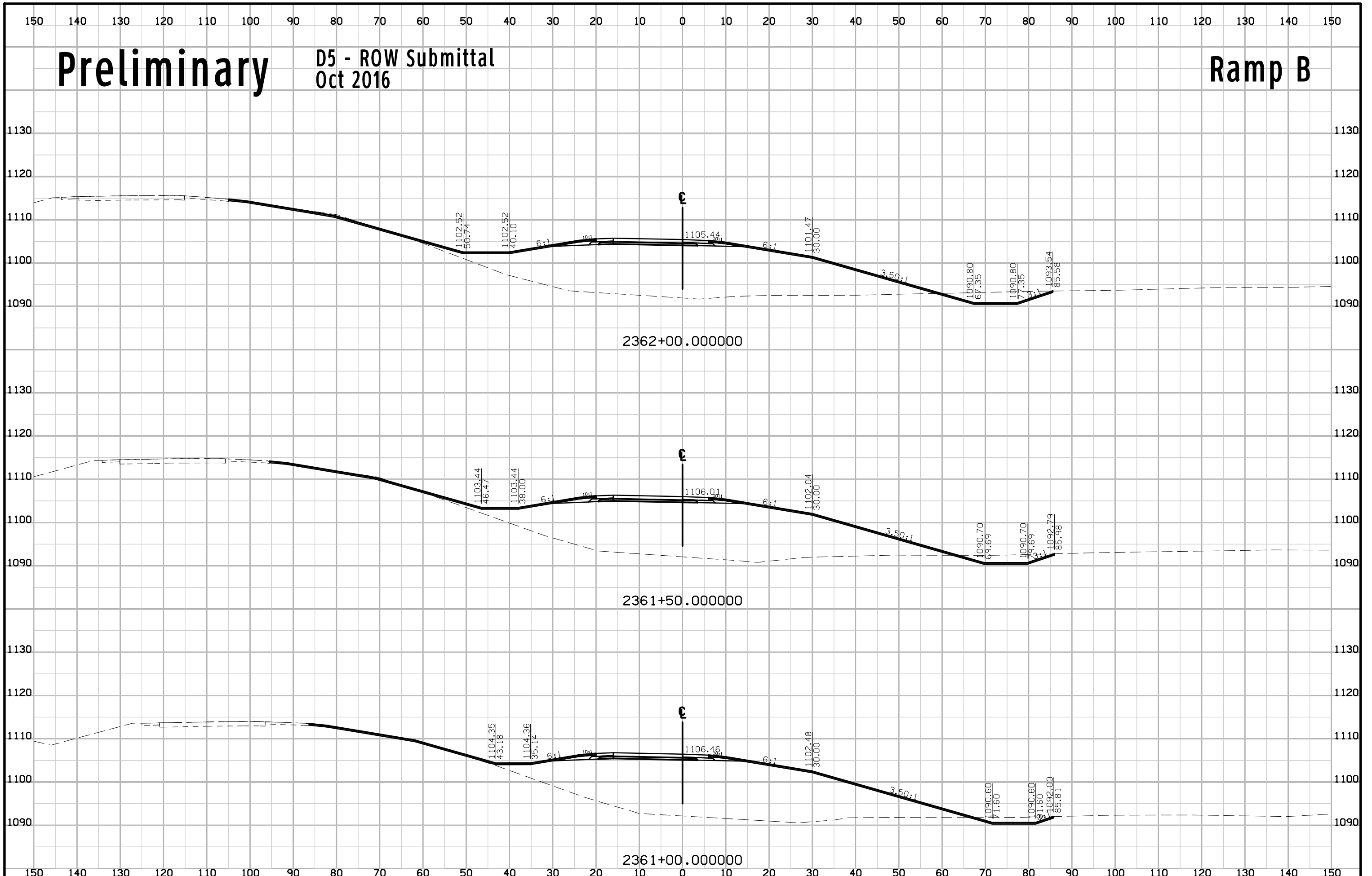




# Preliminary

## D5 - ROW Submittal Oct 2016

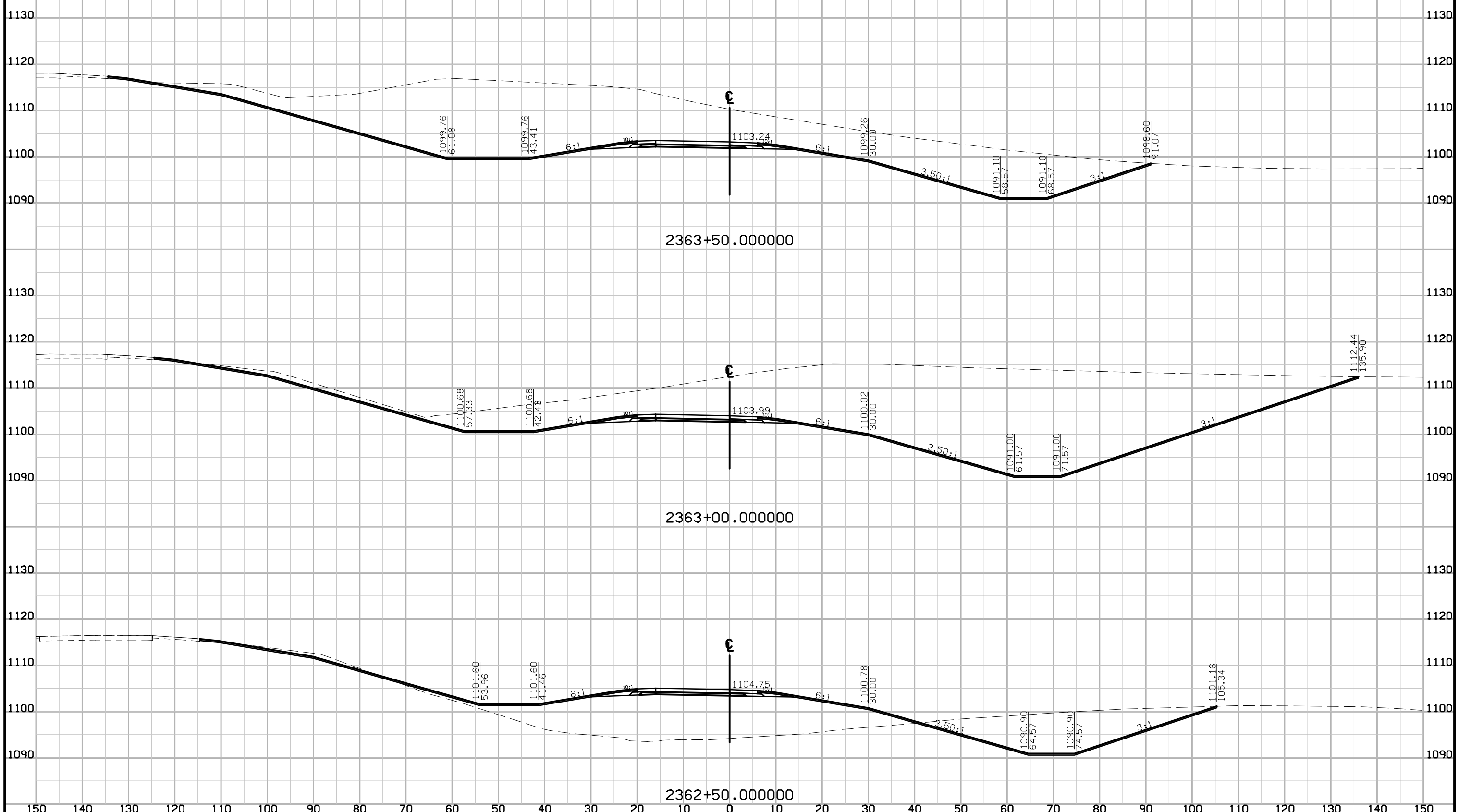
# Ramp B



# Preliminary

## D5 - ROW Submittal Oct 2016

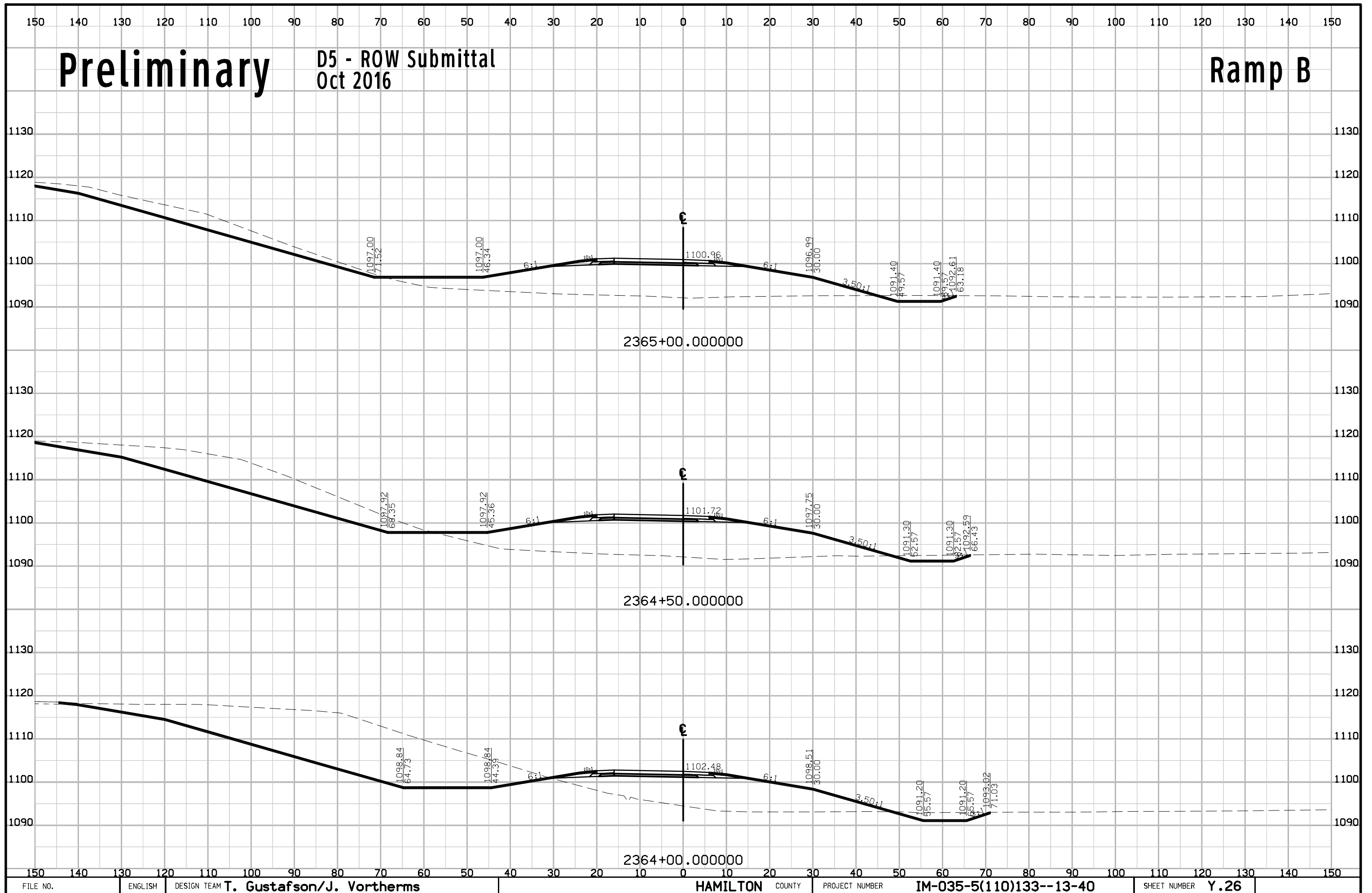
# Ramp B



# Preliminary

## D5 - ROW Submittal Oct 2016

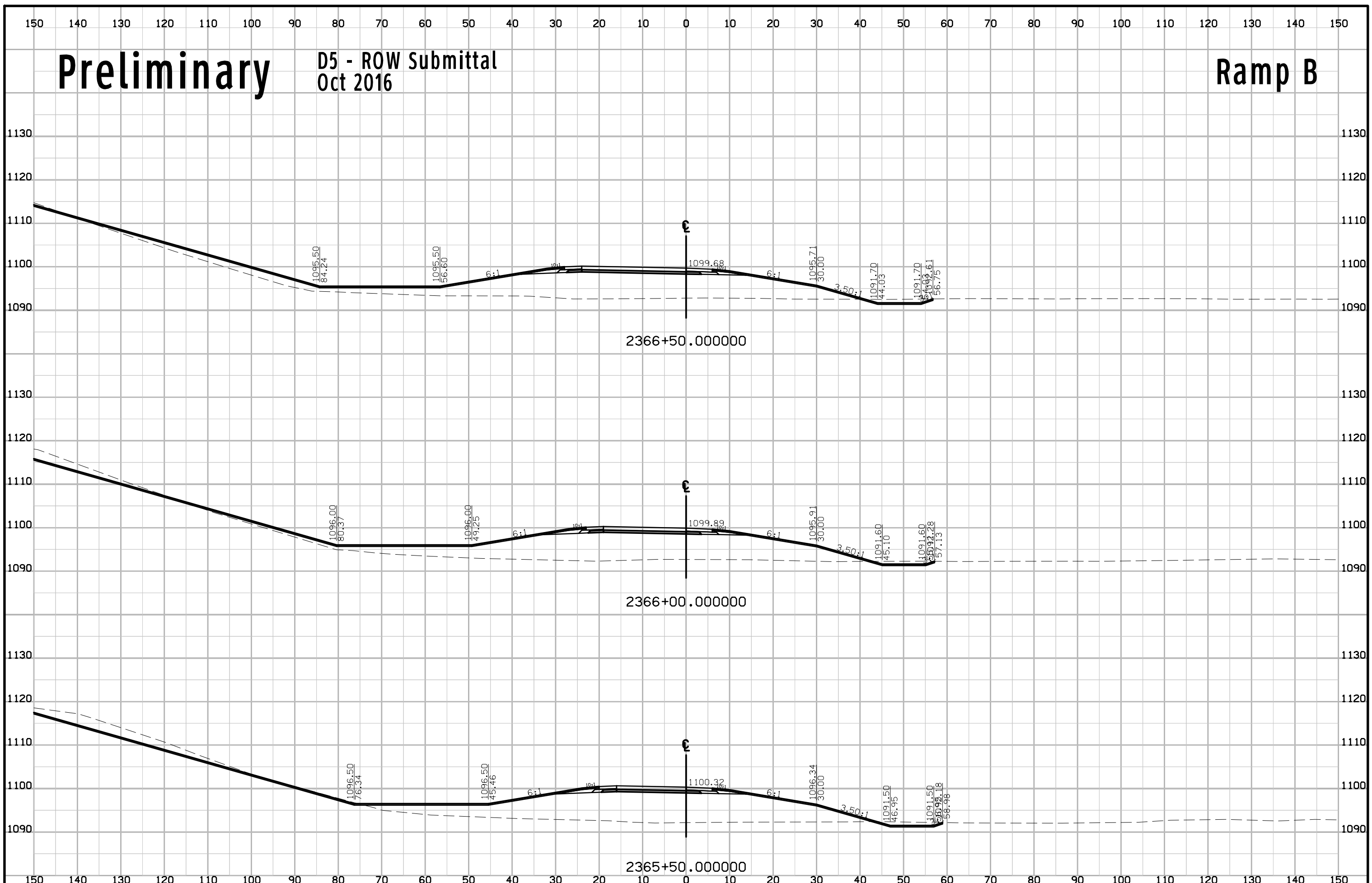
# Ramp B



# Preliminary

## D5 - ROW Submittal Oct 2016

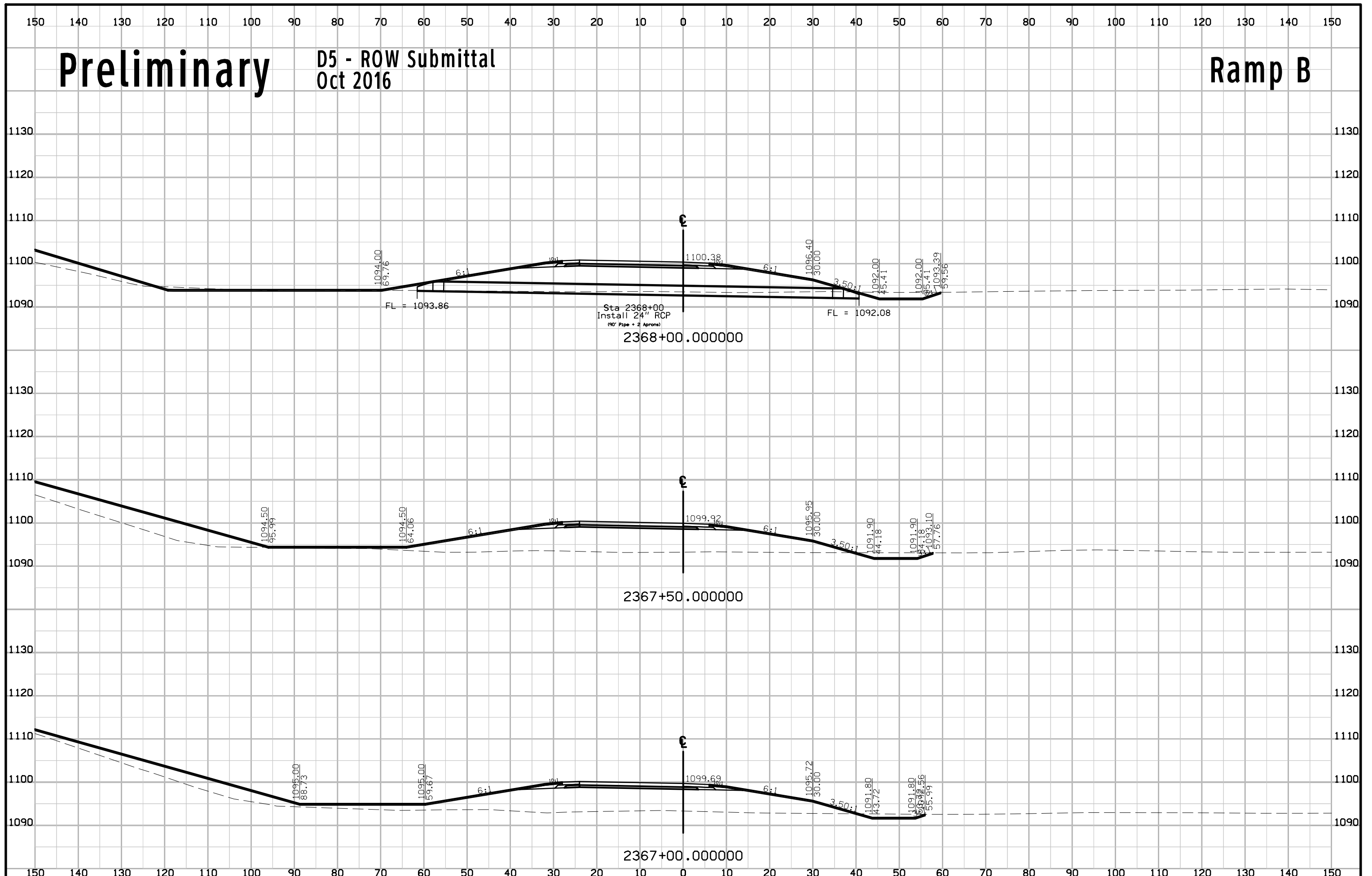
# Ramp B



# Preliminary

## D5 - ROW Submittal Oct 2016

# Ramp B

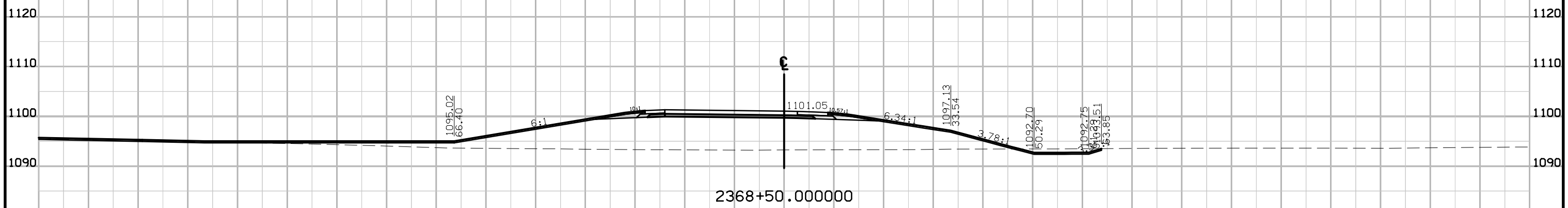


150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

# Preliminary

## D5 - ROW Submittal Oct 2016

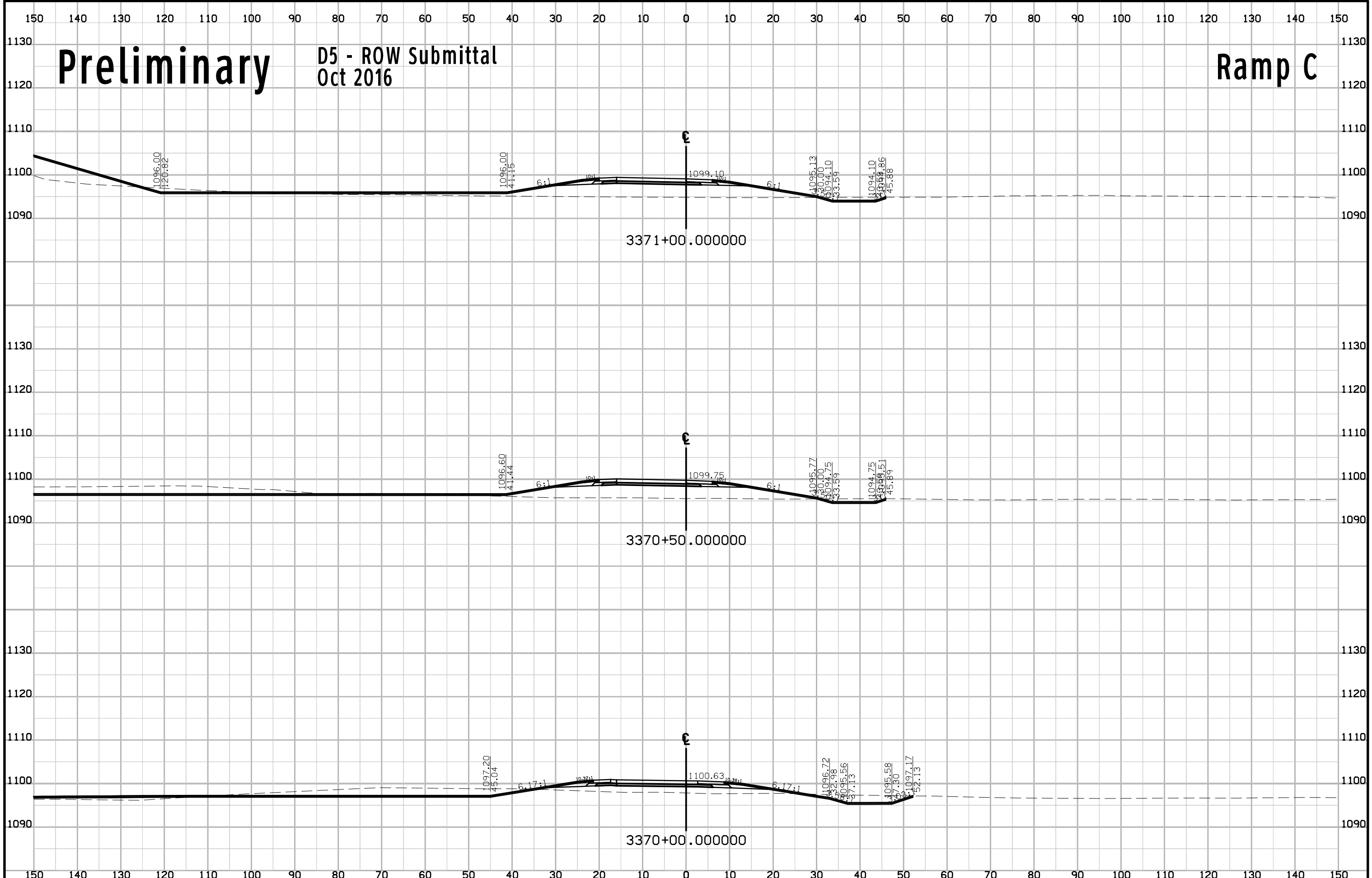
# Ramp B



# Preliminary

## D5 - ROW Submittal Oct 2016

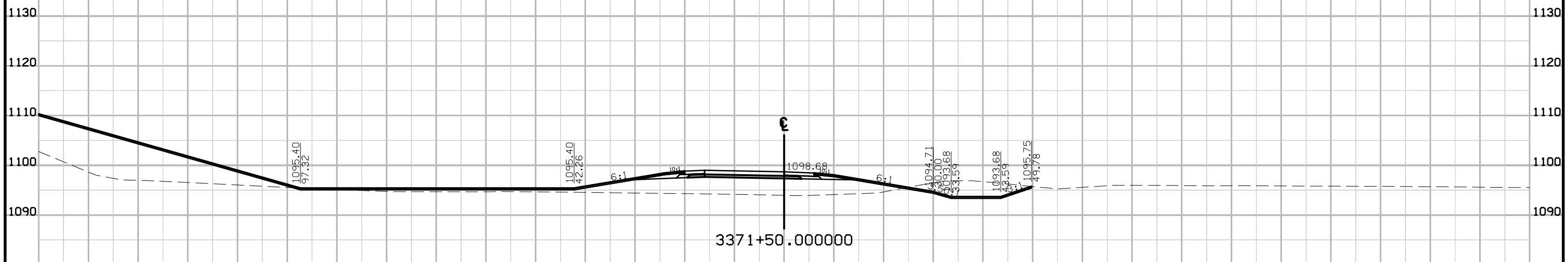
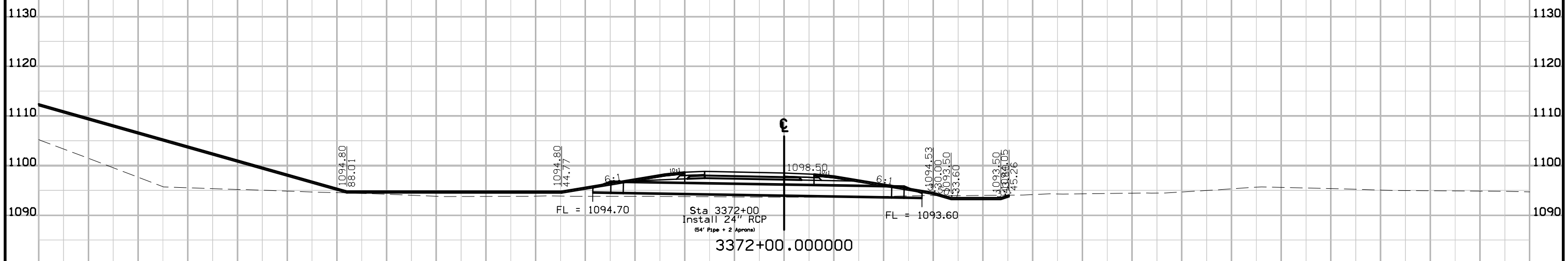
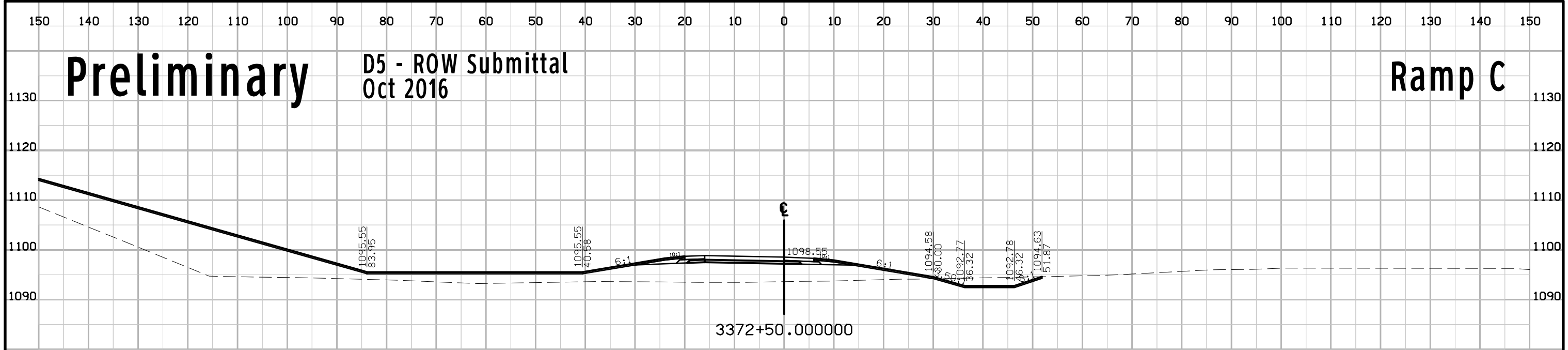
# Ramp C



# Preliminary

## D5 - ROW Submittal Oct 2016

# Ramp C

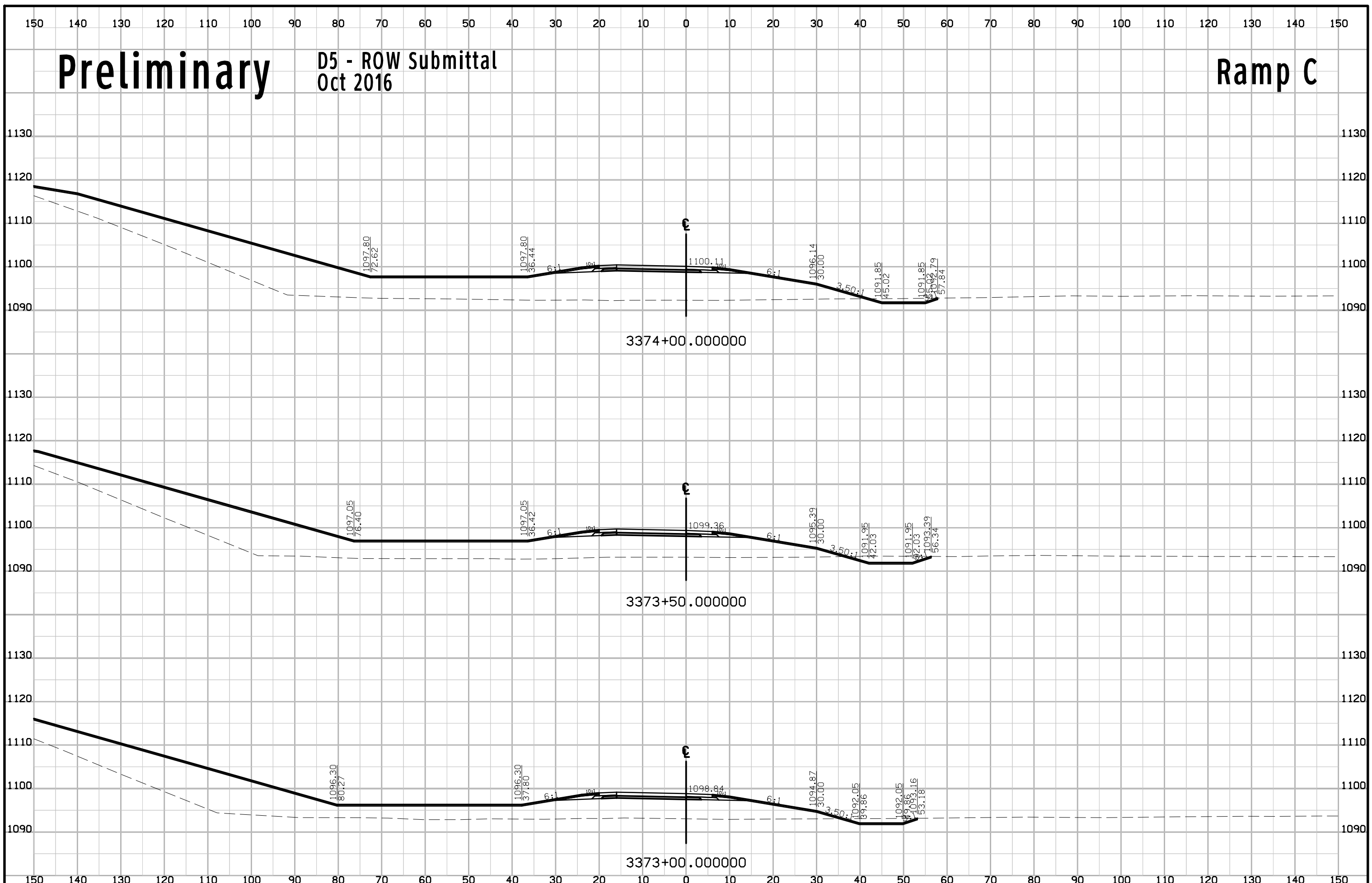




# Preliminary

## D5 - ROW Submittal Oct 2016

# Ramp C

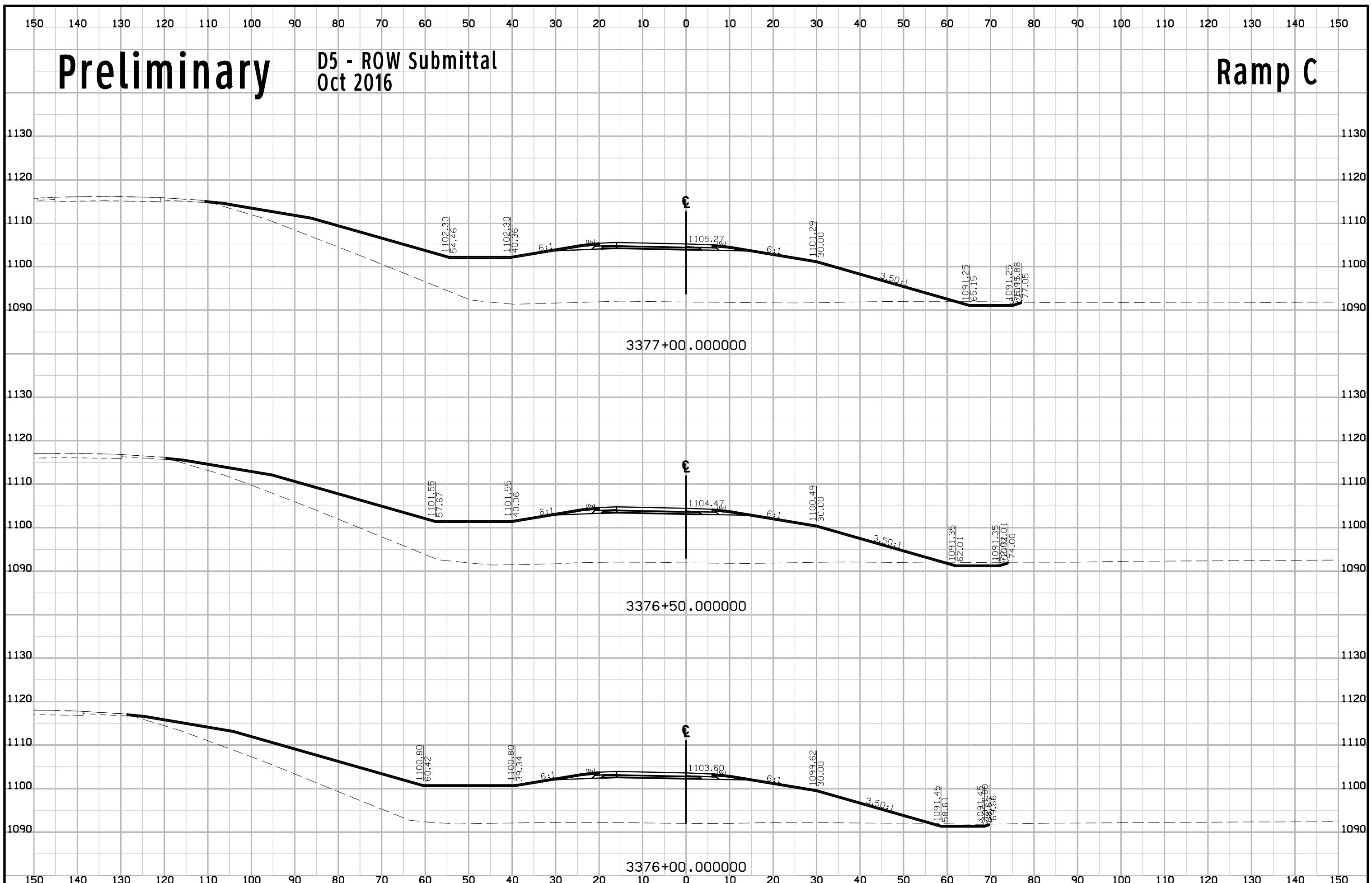




# Preliminary

## D5 - ROW Submittal Oct 2016

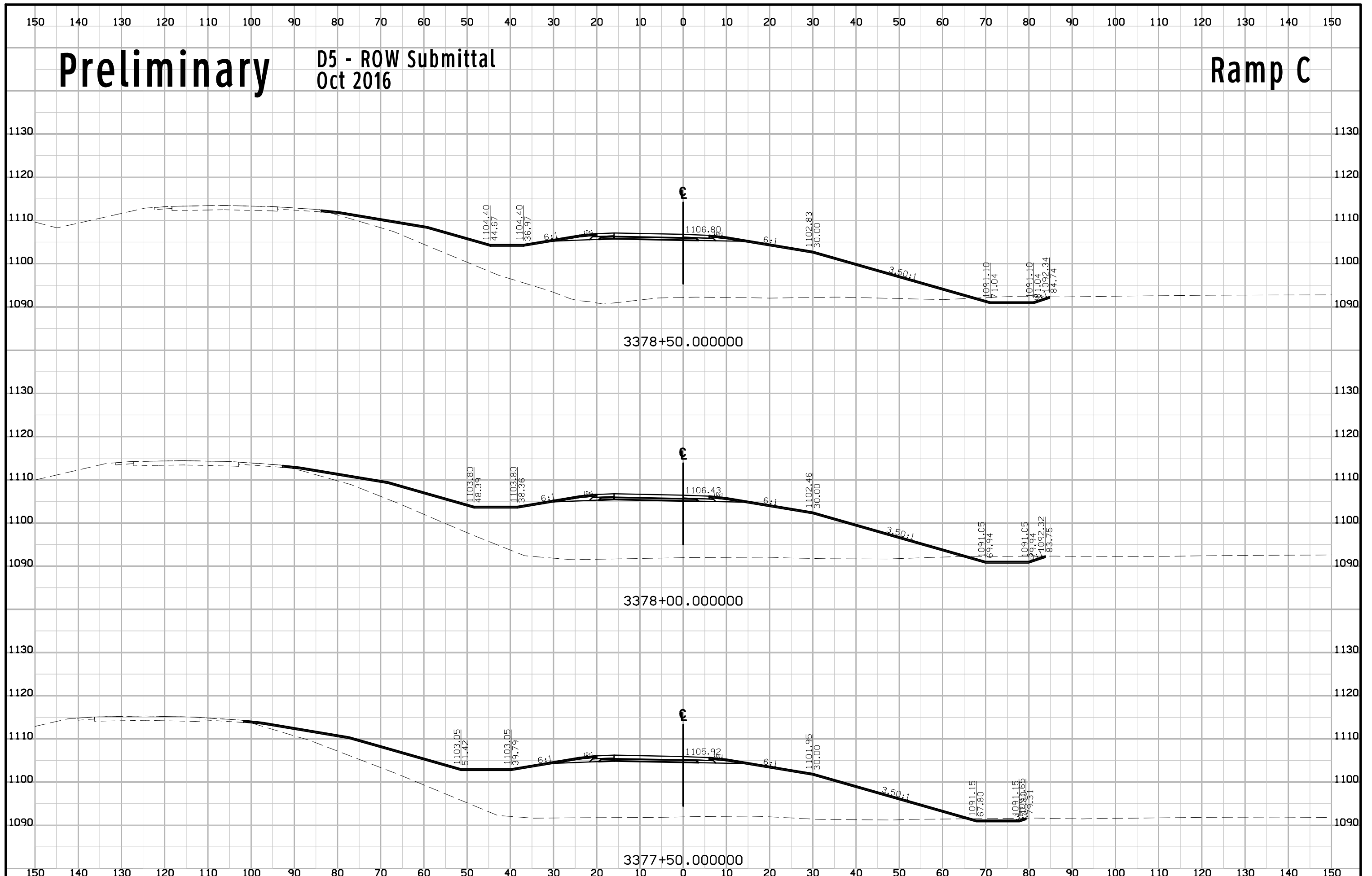
# Ramp C



# Preliminary

## D5 - ROW Submittal Oct 2016

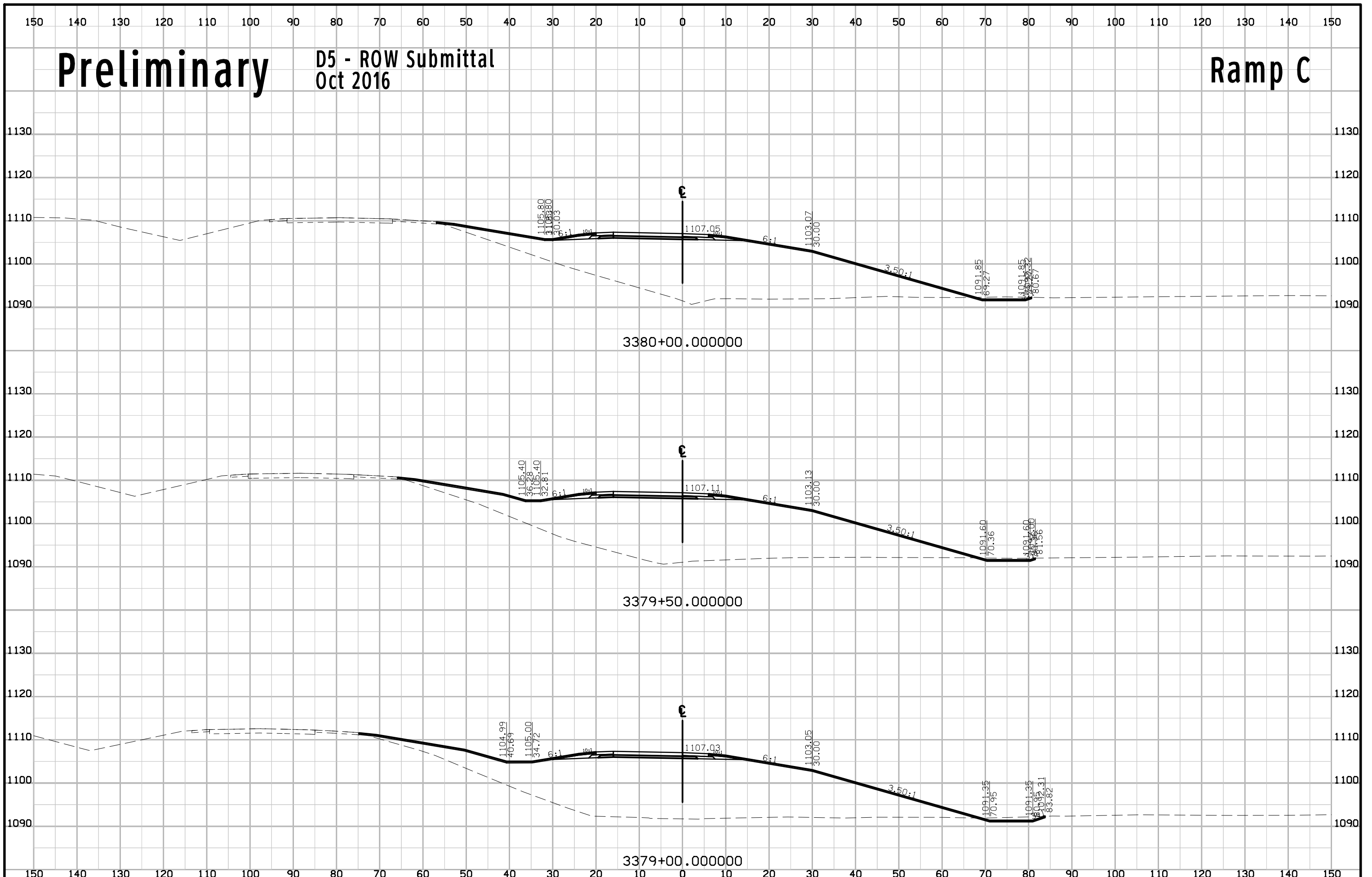
# Ramp C



# Preliminary

## D5 - ROW Submittal Oct 2016

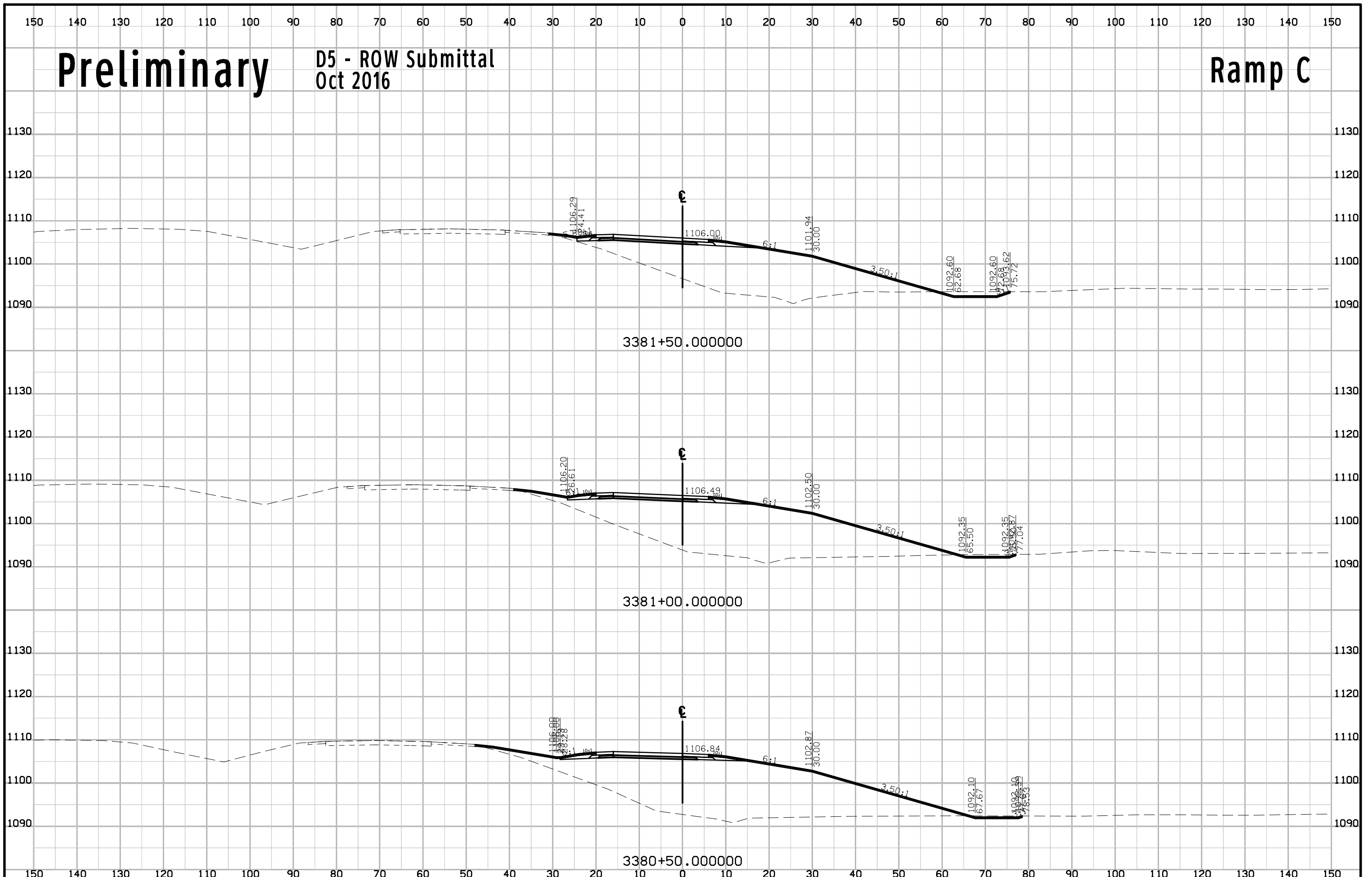
# Ramp C



# Preliminary

## D5 - ROW Submittal Oct 2016

# Ramp C



# Preliminary

D5 - ROW Submittal  
Oct 2016

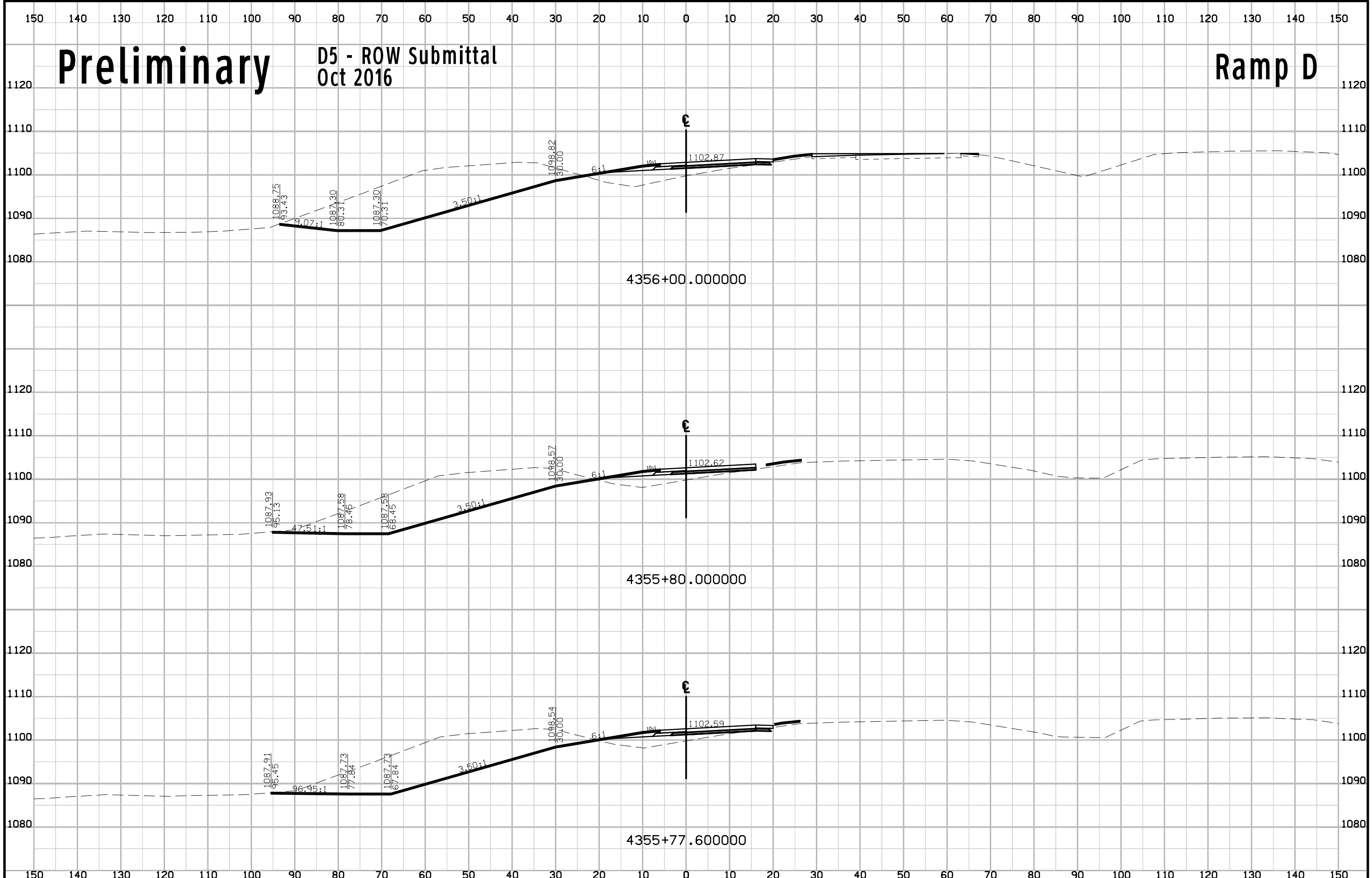
# Ramp C



# Preliminary

## D5 - ROW Submittal Oct 2016

# Ramp D

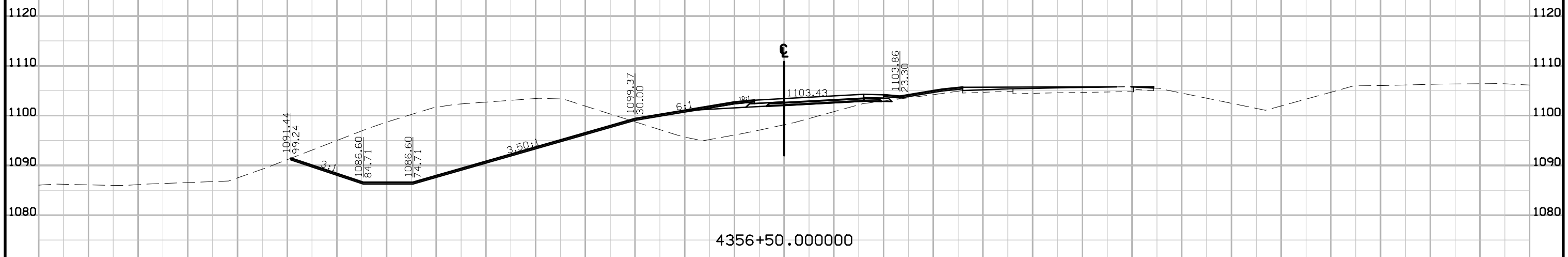
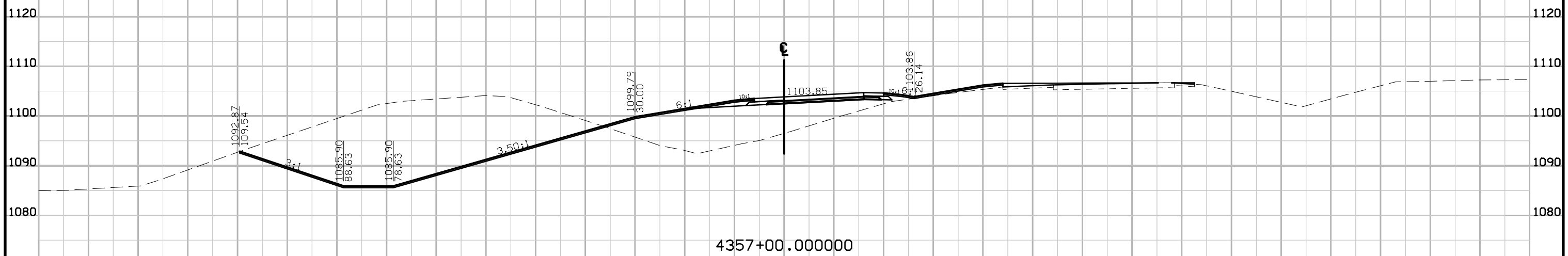
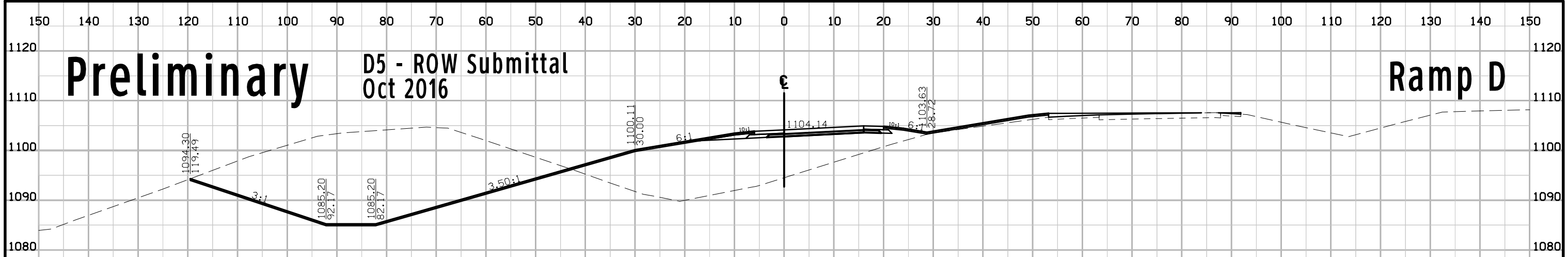




# Preliminary

## D5 - ROW Submittal Oct 2016

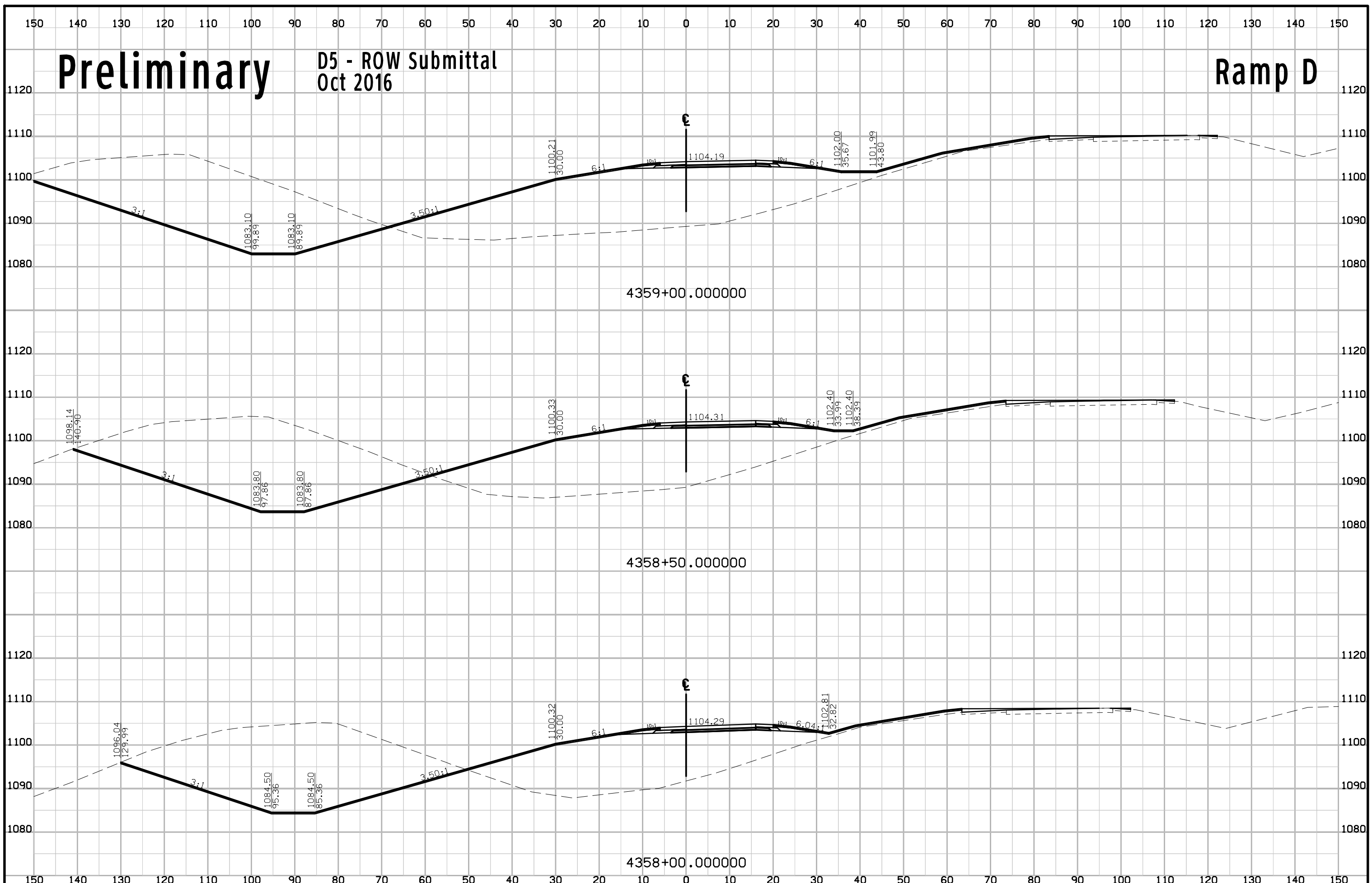
# Ramp D



# Preliminary

## D5 - ROW Submittal Oct 2016

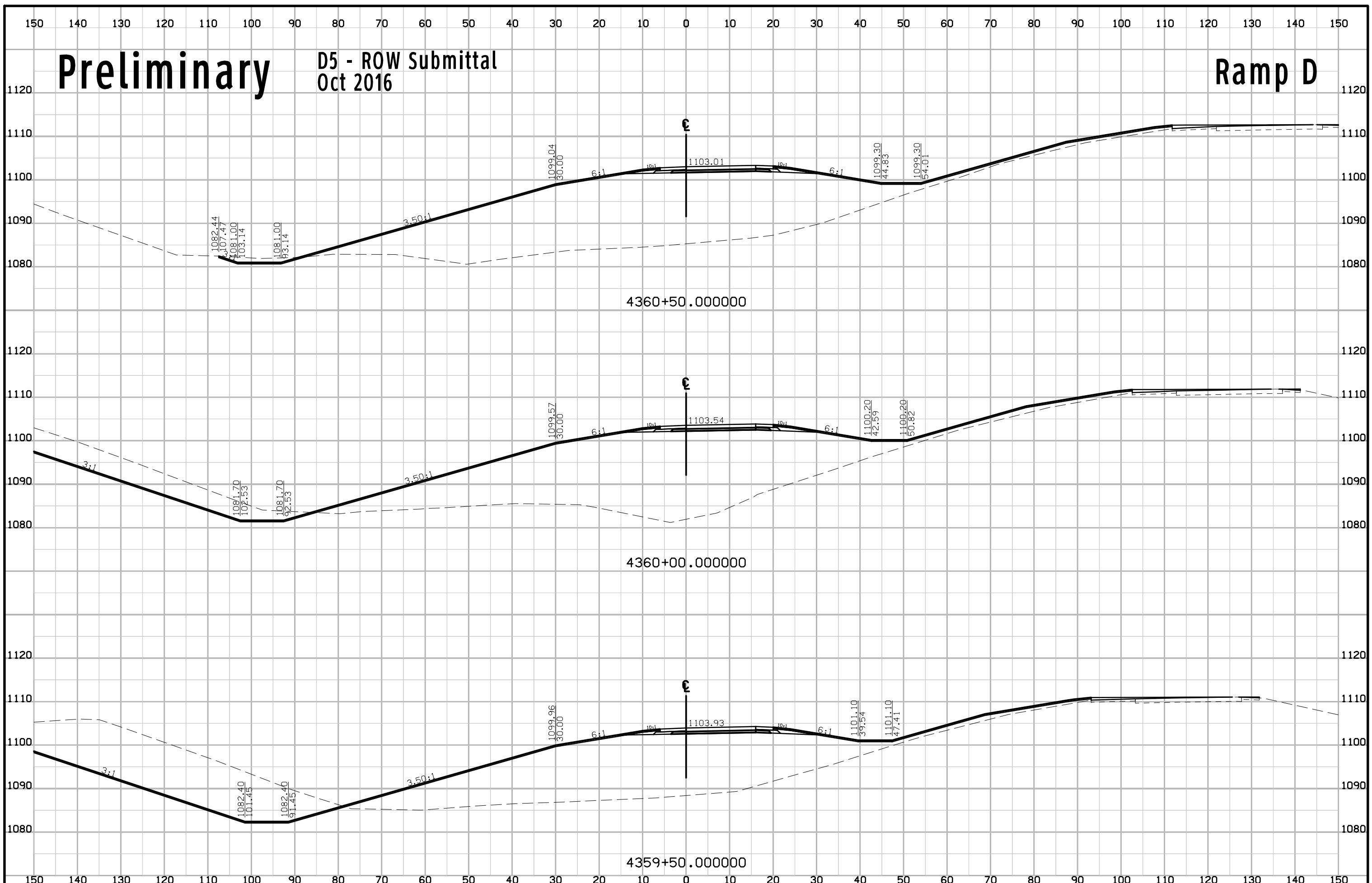
# Ramp D



# Preliminary

## D5 - ROW Submittal Oct 2016

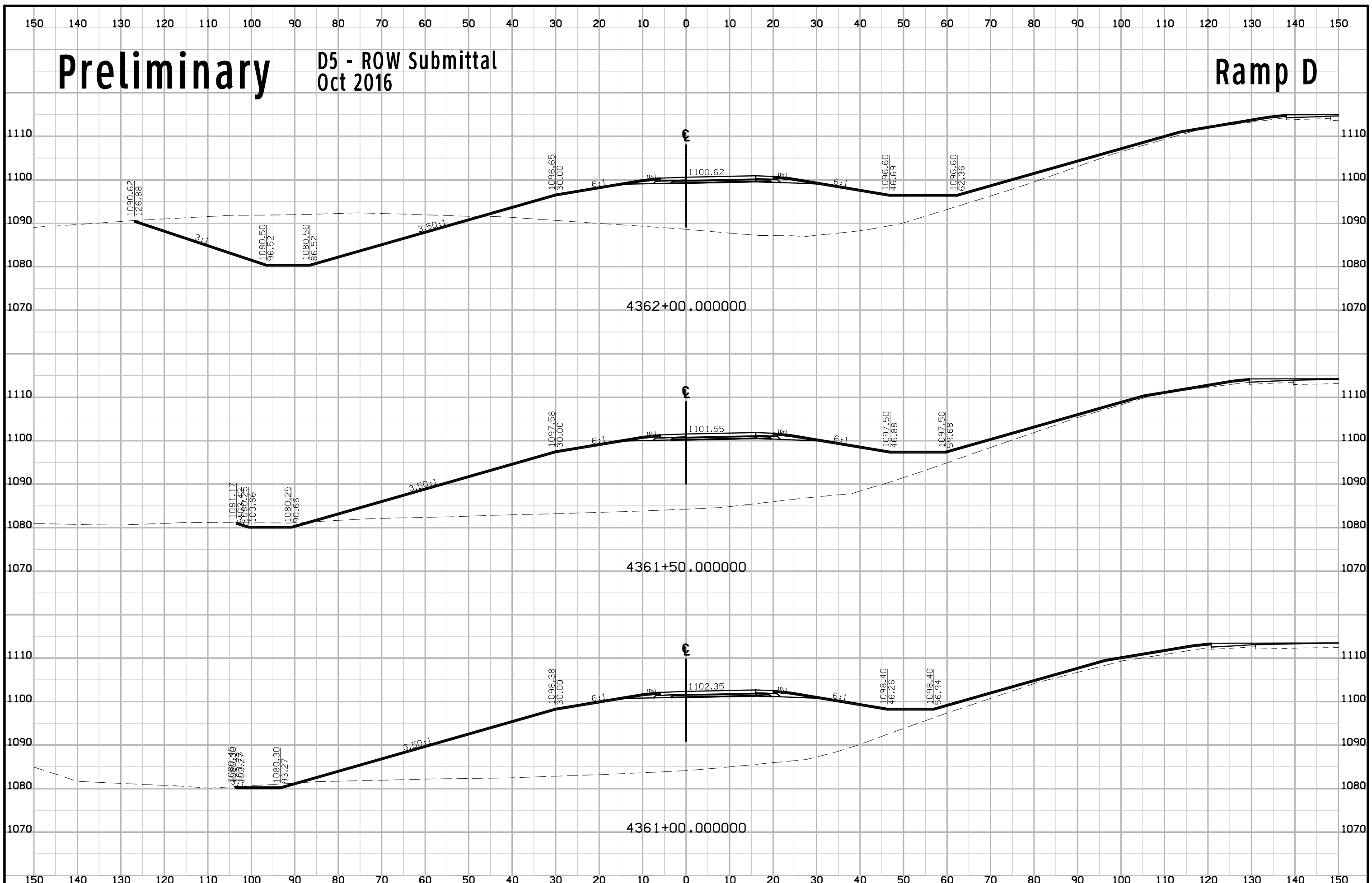
# Ramp D



# Preliminary

## D5 - ROW Submittal Oct 2016

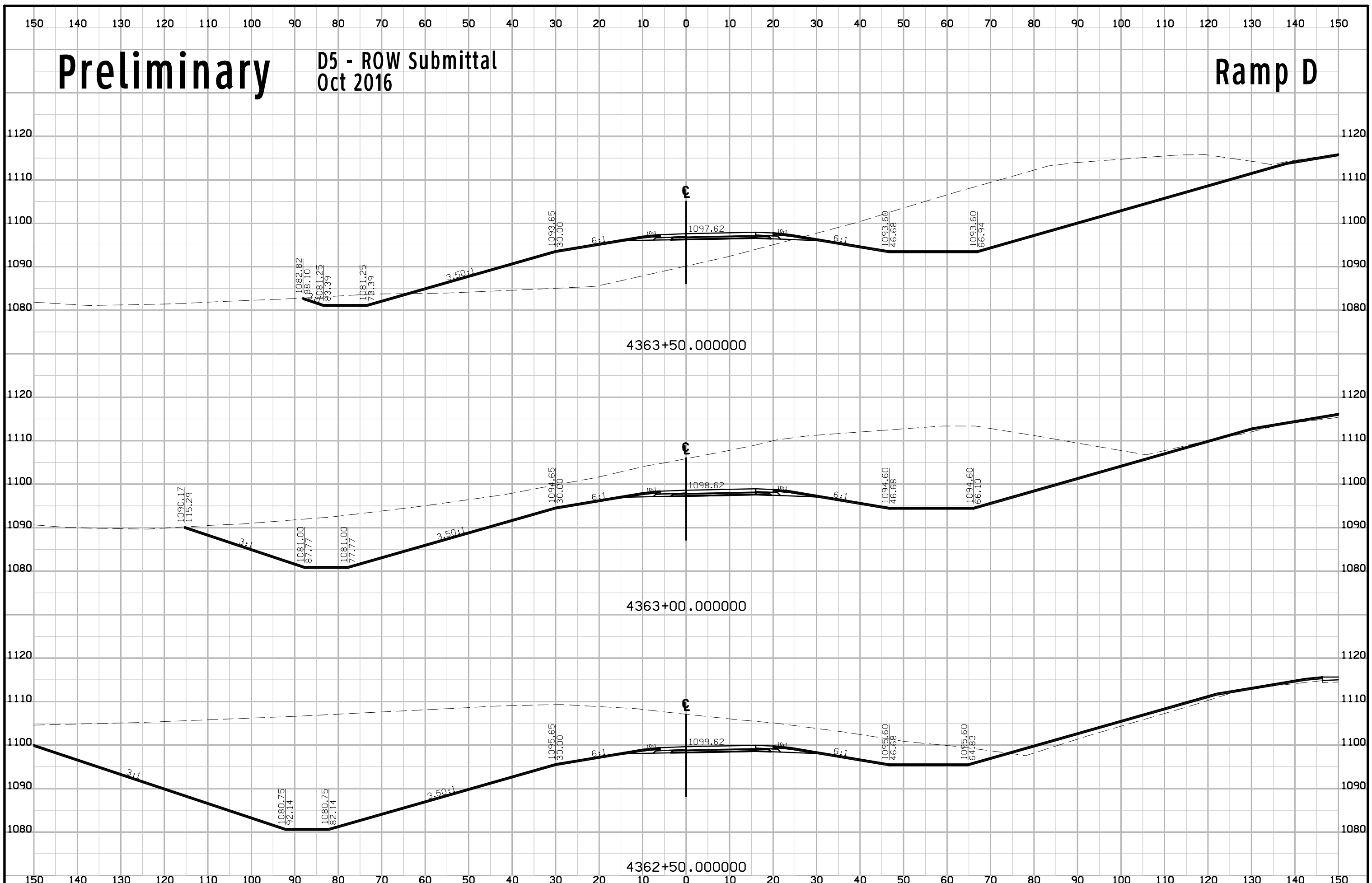
# Ramp D



# Preliminary

## D5 - ROW Submittal Oct 2016

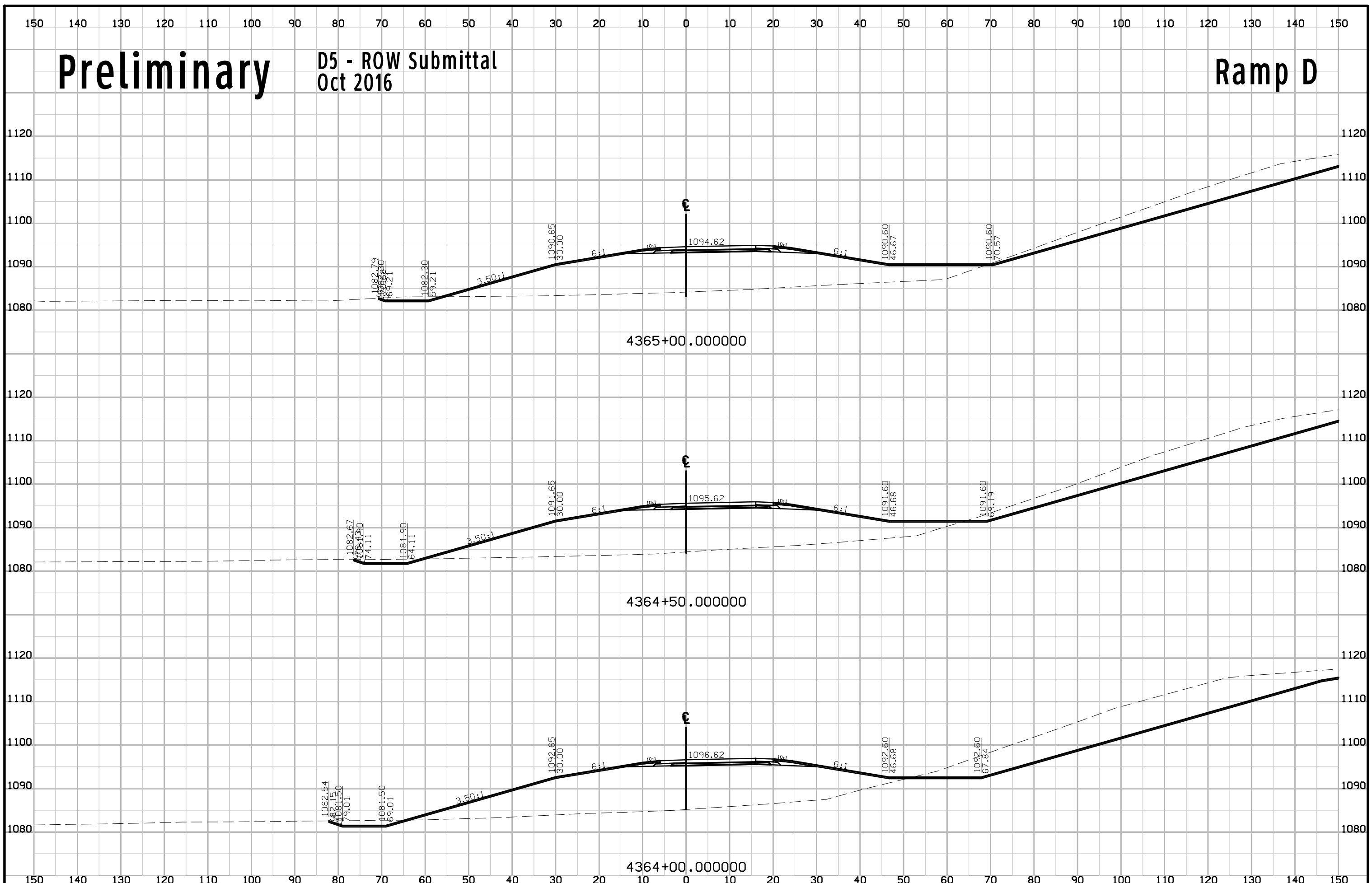
# Ramp D



# Preliminary

## D5 - ROW Submittal Oct 2016

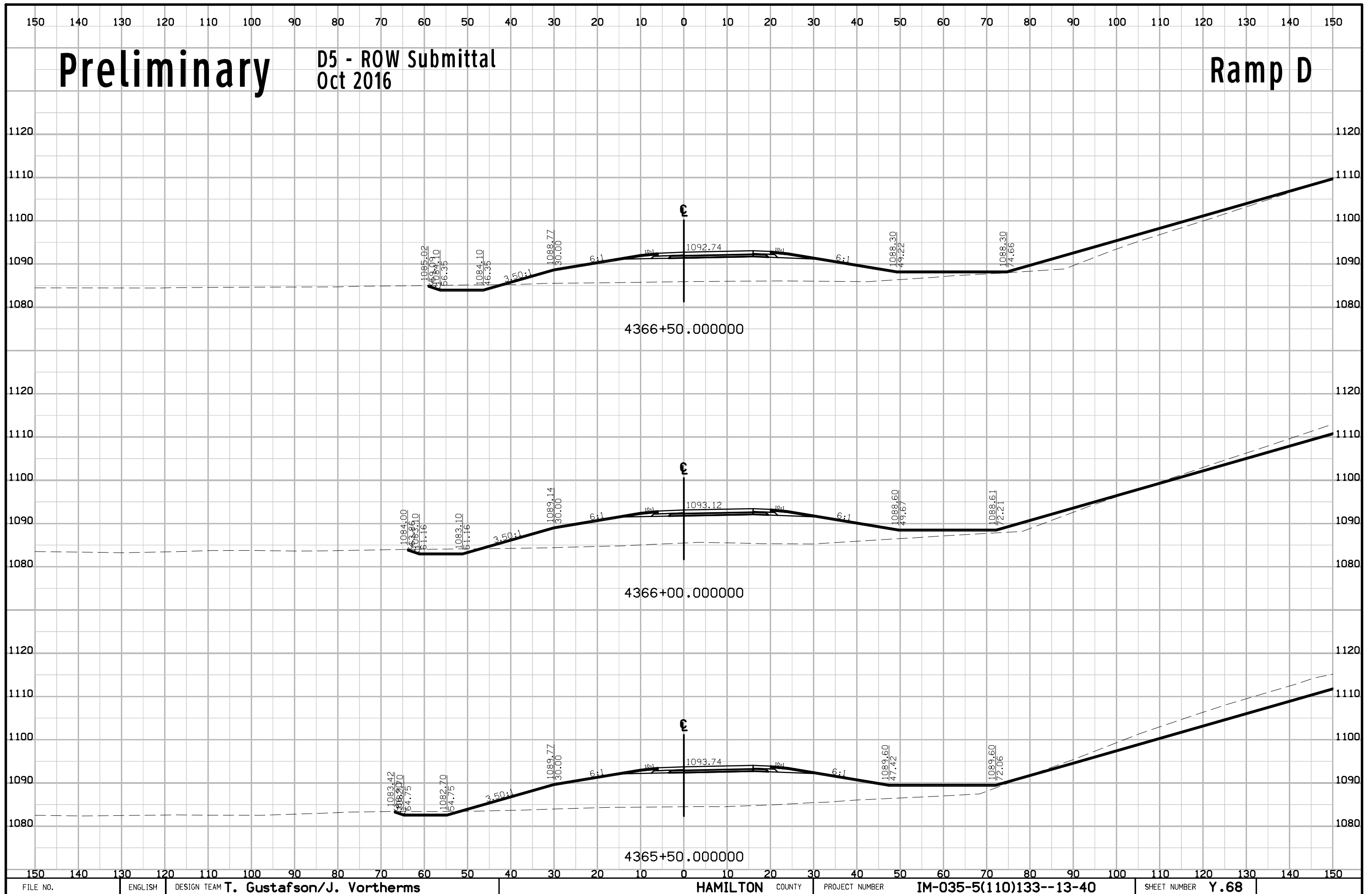
# Ramp D



# Preliminary

## D5 - ROW Submittal Oct 2016

# Ramp D



# Preliminary

## D5 - ROW Submittal Oct 2016

# Ramp D

