

SCOTT CO. PCC PAVEMENT-GRADE AND REPLACE IM-74-1(226)4--13-82

LETTING DATE 09/16/2014

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B.8	Typical Details
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C.**	Estimated Project Quantities
C.**	Estimate Reference Information
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C.**	Tabulations
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PLANS OF PROPOSED IMPROVEMENT ON THE  
**PRIMARY ROAD SYSTEM**  
**SCOTT COUNTY**  
**PCC PAVEMENT - GRADE AND REPLACE**  
**US 67 FROM 10TH STREET**  
**TO 17TH STREET IN BETTENDORF**

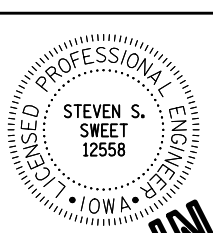
SCALES: As Noted

Refer to the Proposal Form for list of applicable specifications.  
 Value Engineering Saves. Refer to Article 1105.15 of the Specifications.

MILEAGE SUMMARY			
Div.	Location	Lin. Ft.	Miles
	US 67 Southbound Sta. 197+00.00 to Sta. 217+40.00	2040.00	0.386
	US 67 Northbound Sta. 1593+00.00 to Sta. 1624+15.00	3115.00	0.590
	Total Length US 67 Southbound and Northbound	5155.00	0.976
	Average Length of US 67 Roadway (Division 1)	2577.50	0.488

DESIGN DATA URBAN	
2010 AADT	14,000 V.P.D.
2035 AADT	18,740 V.P.D.
2035 DHV	V.P.H.
TRUCKS	5 %
Total Design ESALS	--

INDEX OF SEALS	
A.1	Steven S. Sweet
G.1	Coventine Fidi... Reference Ties/Benchmarks
N.1	Todd... Traffic Signal Design
N.8	Steven S. Sweet... ITS Design
Q.1	Okochi K. Chepkoi... Geotechnical Design



I hereby certify that this engineering document was prepared by me or under my direct personal supervision and that I am a duly licensed Professional Engineer under the laws of the State of Iowa.

Signature: Steven S. Sweet Date: \_\_\_\_\_  
 Printed Name: Steven S. Sweet  
 My license renewal date is December 31, 14

Pages or sheets covered by this seal: \_\_\_\_\_

REVISIONS

TOTAL
487
PROJECT IDENTIFICATION NUMBER
03-82-074-010-03
PROJECT NUMBER
IM-74-1(226)4--13-82
R.O.W. PROJECT NUMBER
IM-074-1(144)5--13-82

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P.5 - 8	Lighting Removals
P.9 - 13	Proposed Lighting Plan
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<b>Q Sheets</b>	<b>Soils Sheets</b>
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Q.11 - 32	Side Roads Soils Sheets
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* S .2 - 16	Sidewalk Plan Details
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<b>T Sheets</b>	<b>Earthwork Quantity Sheets</b>
T.1 - 4	Earthwork Quantity Sheets
<b>U Sheets</b>	<b>500 Series, Mod.Stds. and Detail Sheets</b>
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W.1	Cross Sections Legend & Symbol Information Sheet
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<b>Y Sheets</b>	<b>Ramp Cross Sections</b>
Y.1 - 5	Ramp Cross Sections
Y.6 - 17	I-74 Grading Cross Sections

\* Color Plan Sheets  
\*\* To Be Inserted Later



For Project Location Map Refer to Sheet A.2

UNAPPROVED PLANS

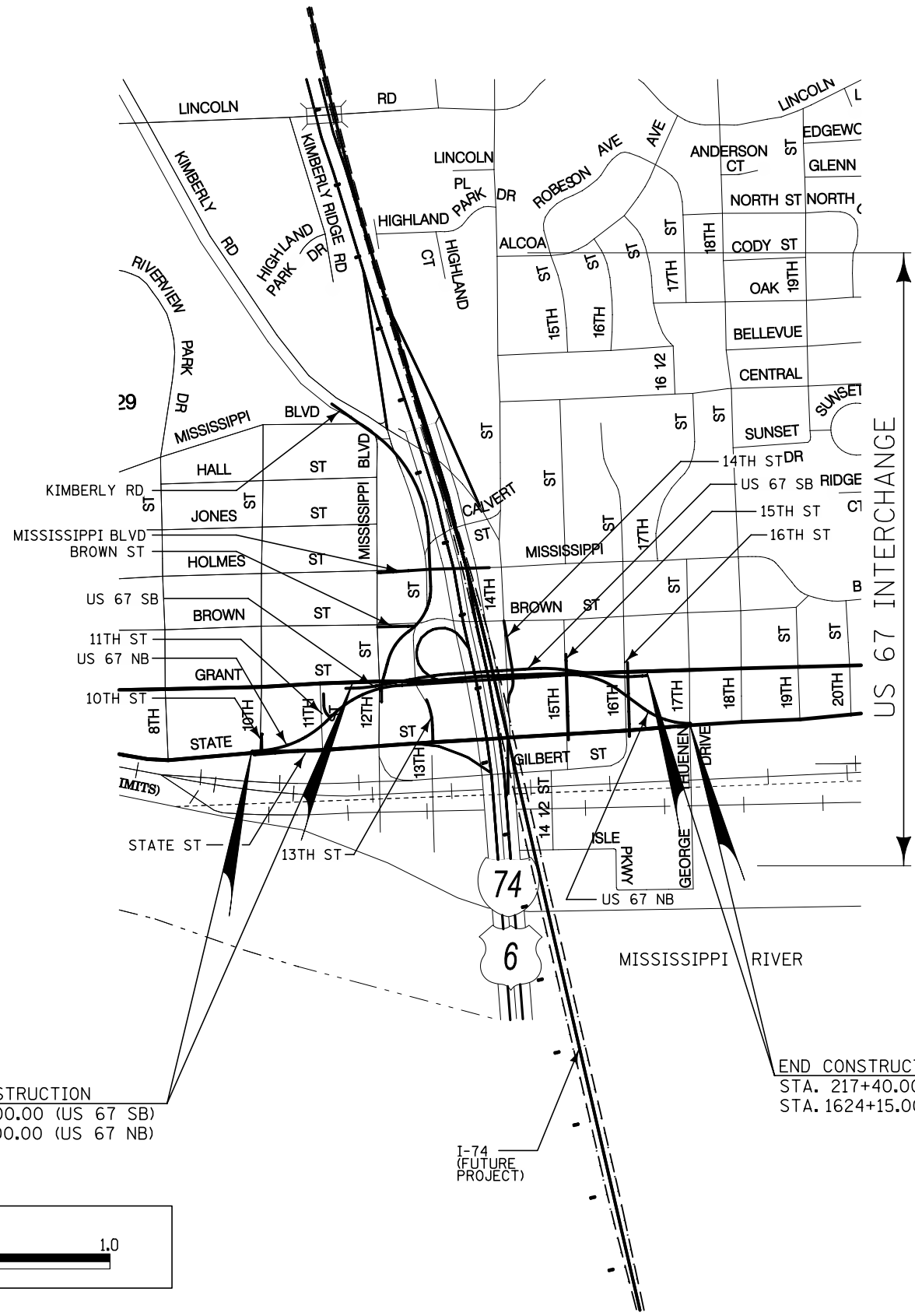
Subject to change by final design.

UNAPPROVED PLAN

Date: 02-21-14



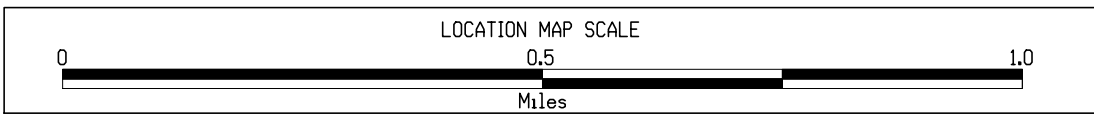
SCOTT COUNTY, IA  
CITY OF BETTENDORF



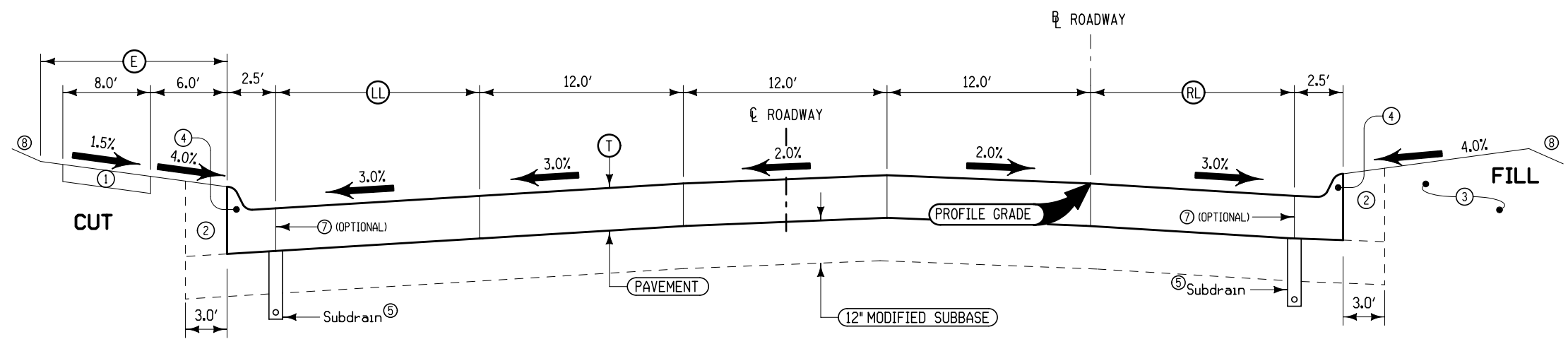
BEGIN CONSTRUCTION  
STA 197+00.00 (US 67 SB)  
STA 1593+00.00 (US 67 NB)

END CONSTRUCTION  
STA. 217+40.00 (US 67 SB)  
STA. 1624+15.00 (US 67 NB)

I-74  
(FUTURE PROJECT)



PROJECT LOCATION

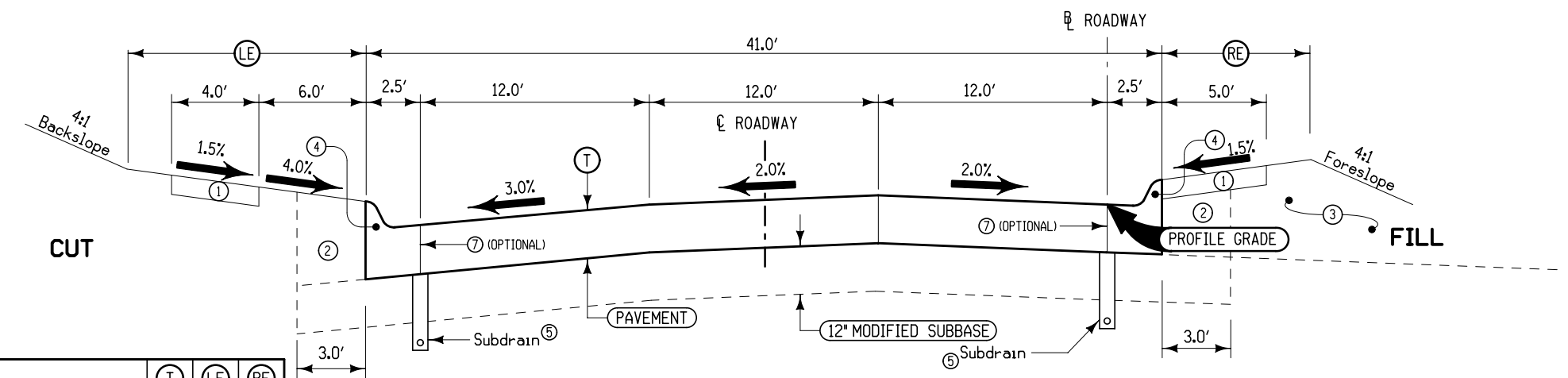


LOCATION		(T)	(E)	(LL)	(RL)
ROAD IDENTIFICATION	STATION TO STATION	Inches	Feet	Feet	Feet
US 67/GRANT STREET	198+30.18	200+04.20	10	16	0
US 67/GRANT STREET	200+04.20	203+20.00	10	16	0
US 67/GRANT STREET	203+20.00	206+03.67	10	16	0
US 67/GRANT STREET	206+23.67	209+14.88	10	16	0
US 67/GRANT STREET	209+14.88	211+40.00	10	16	12
US 67/GRANT STREET	211+40.00	212+59.74	10	16	0
US 67/GRANT STREET	212+59.74	216+13.94	10	16	0

Notes:  
Normal sections shown may be appropriately modified for areas specifically designated by the Engineer, such as intersections or super-elevated curves.  
Refer to other drawings for details of shoulder design and construction.

- ① Refer to other Detail Drawings for details of shoulder and possible sidewalk construction.
- ② Excavate and Backfill 3.0'.
- ③ Backfill
- ④ 6" Standard Curb (PV-102)
- ⑤ Refer to Standard Road Plan RF-19C.
- ⑥ Refer to other drawings for limits of construction for this project.
- ⑦ \*KT-2" or \*L-2" Joint
- ⑧ Normal Foreslope

**TYPICAL CROSS SECTION  
3-LANE DIVIDED ROADWAY  
PAVEMENT WITH CURBS**

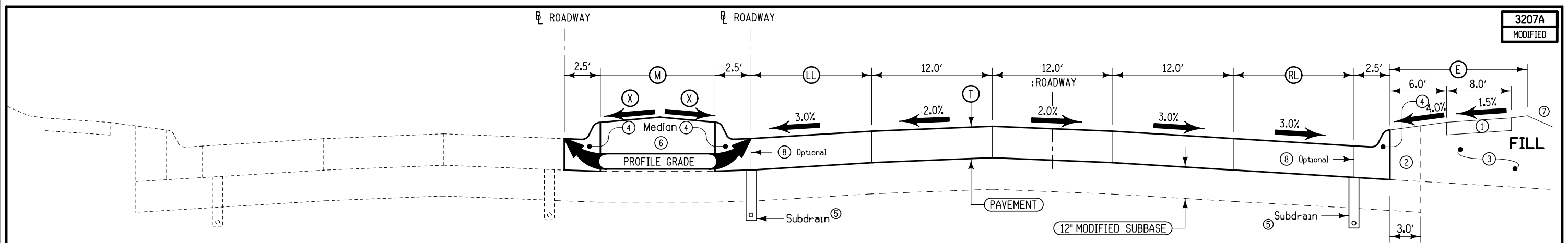


Notes:  
Normal section shown may be appropriately modified for areas specifically designated by the engineer such as intersections or super-elevated curves.  
Refer to other drawings for details of shoulder design and construction

LOCATION ⑥		(T)	(LE)	(RE)
ROAD IDENTIFICATION	STATION TO STATION	Inches	Feet	Feet
US 67/GRANT STREET	197+00.00	198+30.18	10	12
US 67/GRANT STREET	216+13.94	217+40.00	10	12

- ① Refer to other drawings for details of shoulder and possible sidewalk construction
- ② Excavate and backfill 3.0'
- ③ Backfill
- ④ 6" Standard Curb (PV-102)
- ⑤ Refer to Standard Road Plan RF-19C
- ⑥ Refer to other drawings for limits of construction for this project.
- ⑦ \*KT-2" or \*L-2" Joint

**TYPICAL CROSS SECTION  
3 LANE 41' B-B  
WITH 2.5' CURB SECTION**

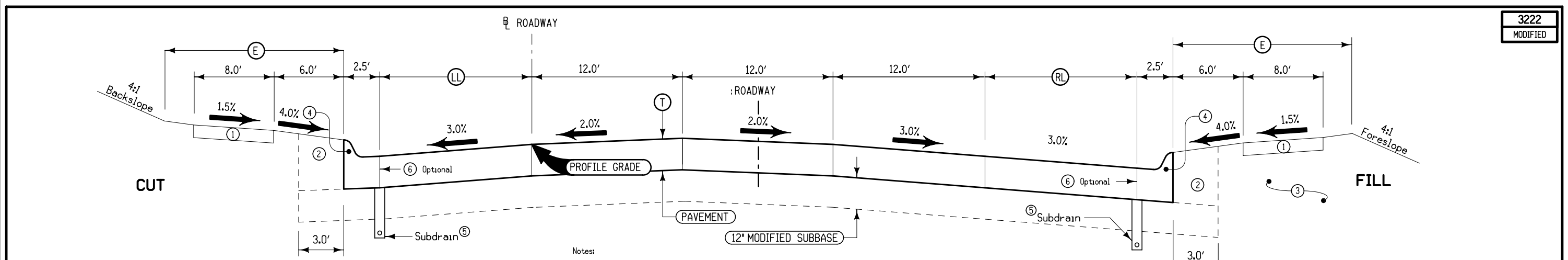


Notes:  
Normal sections shown may be appropriately modified for areas specifically designated by the Engineer, such as intersections or super-elevated curves.  
Refer to other drawings for details of shoulder design and construction.

- ① Refer to other Detail Drawings for details of shoulder and possible sidewalk construction.
- ② Excavate and Backfill 3.0'.
- ③ Backfill
- ④ 6" Standard Curb
- ⑤ Refer to Standard Road Plan RF-19C.
- ⑥ Refer to other drawings for details of raised median.
- ⑦ Normal Foreslope
- ⑧ "KT-2" or "L-2" Joint

**TYPICAL CROSS SECTION  
6-LANE DIVIDED ROADWAY  
PAVEMENT WITH CURBS**

LOCATION		T	E	M	X	MEDIAN TYPE	LL	RL
ROAD IDENTIFICATION	STATION TO STATION	Inches	Feet	Feet	%		Feet	Feet
US 67/STATE STREET =	1601+75.00 =	10	16	46.2-	4	EARTH	0.0-	0.0
US 67/GRANT STREET	198+40.71			20.0			12.0	
US 67/STATE STREET =	1603+18.99 =	10	16	0.0-	2	PAVED	0.0	0.0
US 67/GRANT STREET	200+04.16			8.6				
US 67/STATE STREET =	1604+00.00 =	10	16	8.56	2	PAVED	0.0-	0.0-
US 67/GRANT STREET	200+84.94						24.0	24.0
US 67/STATE STREET =	1608+21.53 =	10	16	0.0-	2	PAVED	24.0-	0.0
US 67/GRANT STREET	205+11.11			3.0			0.0	
US 67/STATE STREET =	1612+11.25 =	10	16	27.0-	2	PAVED	0.0	0.0
US 67/GRANT STREET	209+00.83			17.6				
US 67/STATE STREET =	1614+75.00 =	10	16	17.6-	2	PAVED	0.0-	0.0
US 67/GRANT STREET	211+72.80			73.2			12.0	



Notes:  
Normal section shown may be appropriately modified for areas specifically designated by the engineer such as intersections or super-elevated curves.  
Refer to other drawings for details of shoulder design and construction.

- ① Refer to other drawings for details of shoulder and possible sidewalk construction
- ② Excavate and backfill 3.0'
- ③ Backfill
- ④ 6" Standard Curb
- ⑤ Refer to Standard Road Plan RF-19C
- ⑥ "KT-2" or "L-2" Joint

**TYPICAL CROSS SECTION  
3 LANE 41' B-B  
WITH 2.5' CURB SECTION**

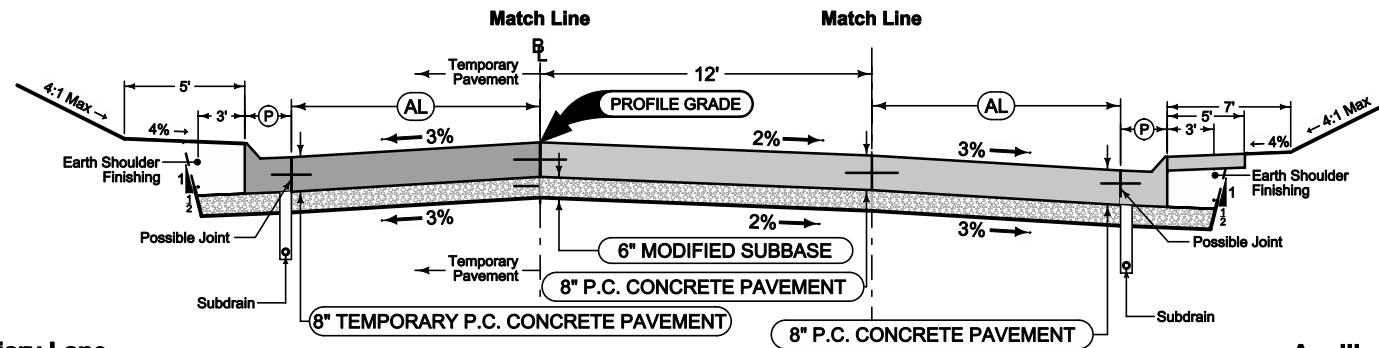
LOCATION		T	E	LL	RL
ROAD IDENTIFICATION	STATION TO STATION	Inches	Feet	Feet	Feet
US 67/STATE STREET	1593+00.00	10	16	0	20.3-0
US 67/STATE STREET	1594+40.10			0	0
US 67/STATE STREET	1599+55.00	10	16	0-12	0
US 67/STATE STREET	1619+56.47	10	16	0	0
US 67/STATE STREET	1620+75.00			0	0
US 67/STATE STREET	1620+75.00	10	16	0	0-12

**Curbed Shoulder**

Shoulder Jointing:  
Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
Staged : KT-2  
Transverse:C at 20' spacing

STATION TO STATION	(P) Feet	Curb Type See PV-102
480+31.28	482+03.52	2.5
		6" Std



**Auxiliary Lane**

Longitudinal joint: L or KT  
Transverse joint: Match Mainline

STATION TO STATION	(AL) Feet
480+31.28	482+03.52
	0-14.49

Section shown in the direction of traffic.

Jointing:  
Transverse joints: CD at 20' spacing.

BEGIN STATION	END STATION
480+31.28	482+03.52

**Auxiliary Lane**

Longitudinal joint: L or KT  
Transverse joint: Match Mainline

STATION TO STATION	(AL) Feet
480+31.28	482+03.52
	8

**Curbed Shoulder**

Shoulder Jointing:  
Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
Staged : KT-2  
Transverse:C at 20' spacing

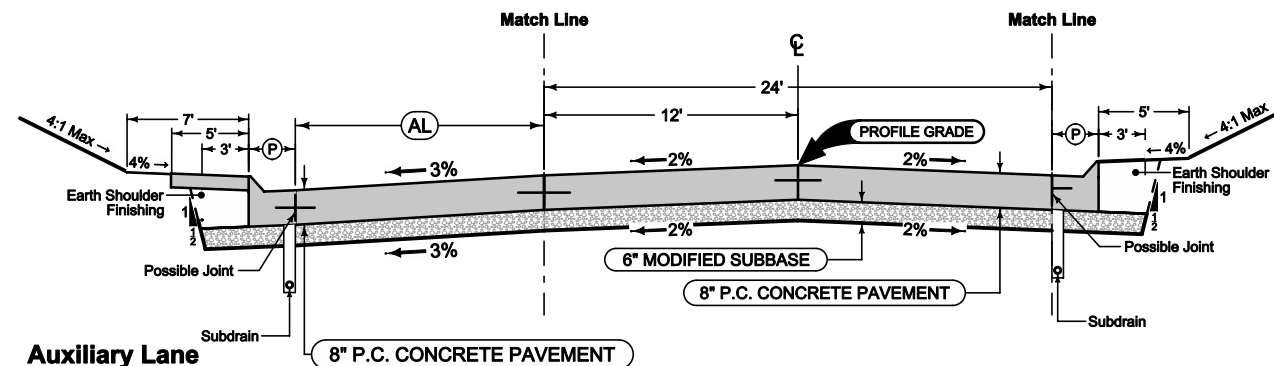
STATION TO STATION	(P) Feet	Curb Type See PV-102
480+31.28	482+03.52	2.5
		6" Std

**Curbed Shoulder**

Shoulder Jointing:  
Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
Staged : KT-2  
Transverse:C at 20' spacing

STATION TO STATION	(P) Feet	Curb Type See PV-102
489+52.36	493+02.50	2.5
		6" Std



**Auxiliary Lane**

Longitudinal joint: L or KT  
Transverse joint: Match Mainline

STATION TO STATION	(AL) Feet
489+52.36	491+66.98
491+66.98	493+02.50
	8
	0

Section shown in the direction of traffic.

Jointing:  
Transverse joints: CD at 20' spacing.

BEGIN STATION	END STATION
489+52.36	493+02.50

**Curbed Shoulder**

Shoulder Jointing:  
Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
Staged : KT-2  
Transverse:C at 20' spacing

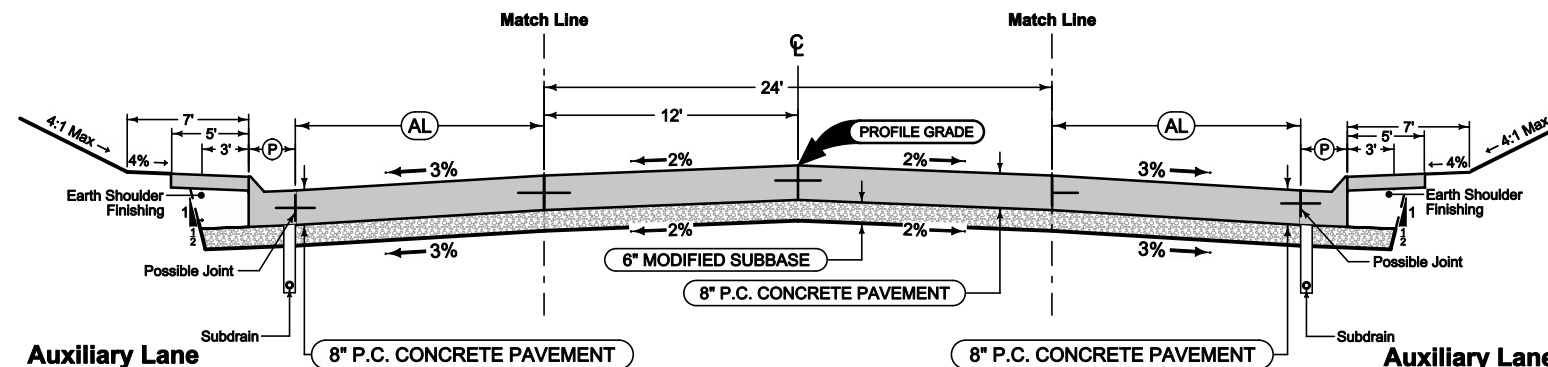
STATION TO STATION	(P) Feet	Curb Type See PV-102
489+52.36	493+02.50	2.5
		6" Std

**Curbed Shoulder**

Shoulder Jointing:  
Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
Staged : KT-2  
Transverse:C at 20' spacing

STATION TO STATION	(P) Feet	Curb Type See PV-102
499+69.53	503+44.95	2.5
		6" Std



**Auxiliary Lane**

Longitudinal joint: L or KT  
Transverse joint: Match Mainline

STATION TO STATION	(AL) Feet
499+69.53	503+44.95
	8

Section shown in the direction of traffic.

Jointing:  
Transverse joints: CD at 20' spacing.

BEGIN STATION	END STATION
499+69.53	503+44.95

Longitudinal joint: L or KT  
Transverse joint: Match Mainline

STATION TO STATION	(AL) Feet
499+69.53	503+44.95
	8

**Curbed Shoulder**

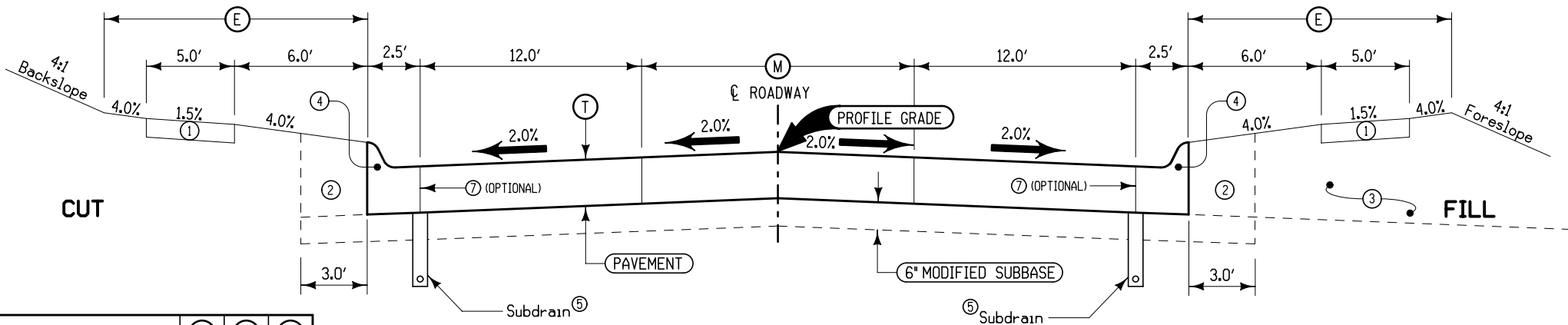
Shoulder Jointing:  
Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
Staged : KT-2  
Transverse:C at 20' spacing

STATION TO STATION	(P) Feet	Curb Type See PV-102
499+69.53	503+44.95	2.5
		6" Std

**STATE STREET**

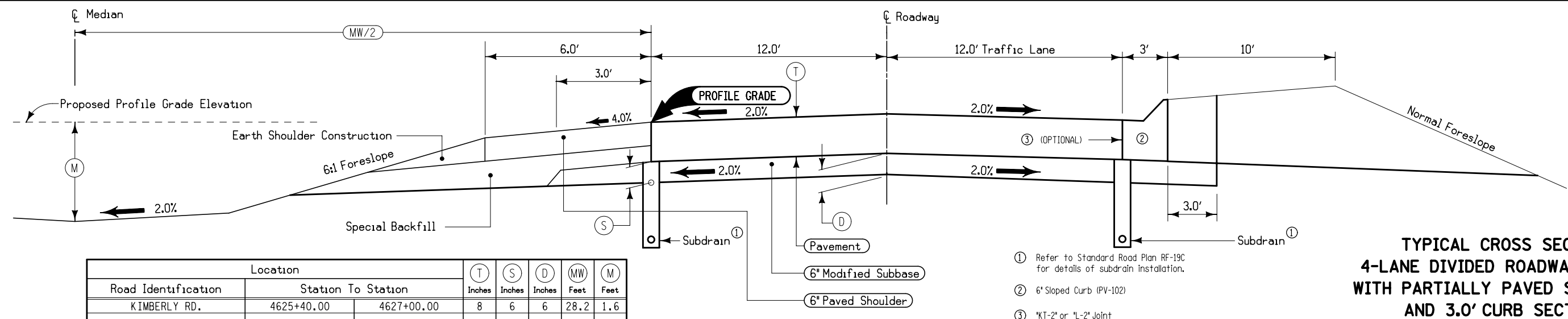
Notes:  
Normal section shown may be appropriately modified for areas specifically designated by the engineer such as intersections or super-elevated curves.  
Refer to other drawings for details of shoulder design and construction



- ① Refer to other drawings for details of shoulder and possible sidewalk construction
- ② Excavate and backfill 3.0'
- ③ Backfill
- ④ 6" Standard Curb (PV-102)
- ⑤ Refer to Standard Road Plan RF-19C
- ⑥ Refer to other drawings for limits of construction for this project.
- ⑦ \*KT-2" or \*L-2" Joint

**TYPICAL CROSS SECTION  
3 LANE  
WITH 2.5' CURB SECTION**

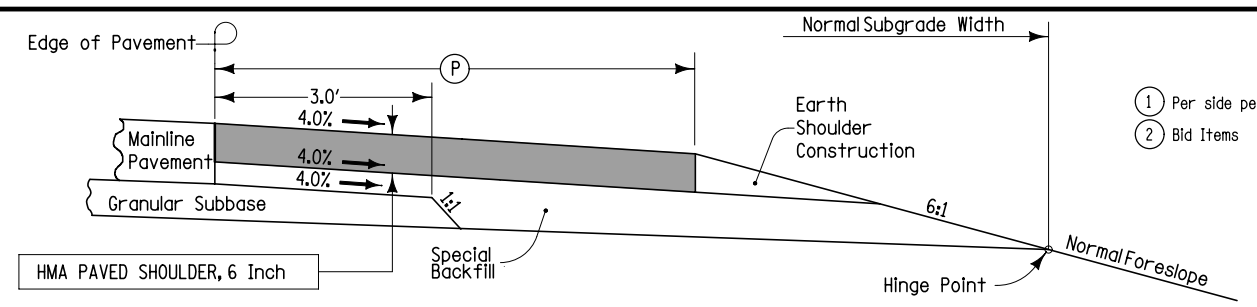
LOCATION ⑥		T	E	M
ROAD IDENTIFICATION	STATION TO STATION	Inches	Feet	Feet
KIMBERLY ROAD	4606+09.39 - 4616+26.99	8	13	16.0
KIMBERLY ROAD	4616+26.99 - 4620+06.00	8	13	16.0
				4.1
KIMBERLY ROAD	4620+06.00 - 4625+40.00	8	13	4.1 - 28.2



- ① Refer to Standard Road Plan RF-19C for details of subdrain installation.
- ② 6" Sloped Curb (PV-102)
- ③ \*KT-2" or \*L-2" Joint

**TYPICAL CROSS SECTION  
4-LANE DIVIDED ROADWAY PAVING  
WITH PARTIALLY PAVED SHOULDERS  
AND 3.0' CURB SECTION**

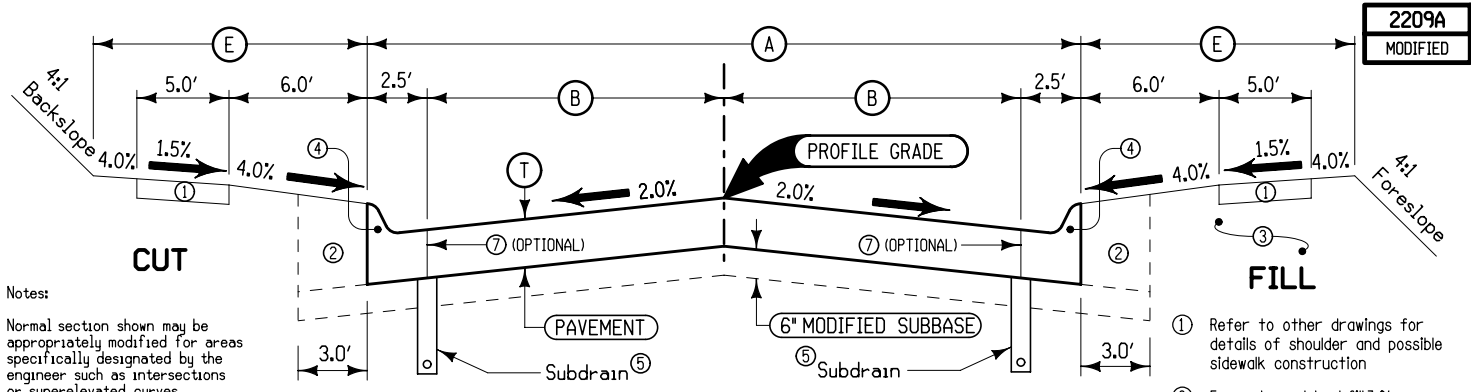
Location		T	S	D	MW	M
Road Identification	Station To Station	Inches	Inches	Inches	Feet	Feet
KIMBERLY RD.	4625+40.00 - 4627+00.00	8	6	6	28.2	1.6



**HMA PAVED SHOULDER ADJACENT TO EXISTING PAVEMENT**

Location			P	G	Quantities ②			
Road Identification	Station To Station	Side			Special Backfill ① Tons	Earth Shoulder Construction Sta.	Paved Shoulder ① Sq. Yds.	Granular Shoulder ① Tons
KIMBERLY ROAD (SBL)	4625+40.00 - 4627+00.00	MEDIAN	6		46.1	1.0	66.7	0.0

2209A  
MODIFIED



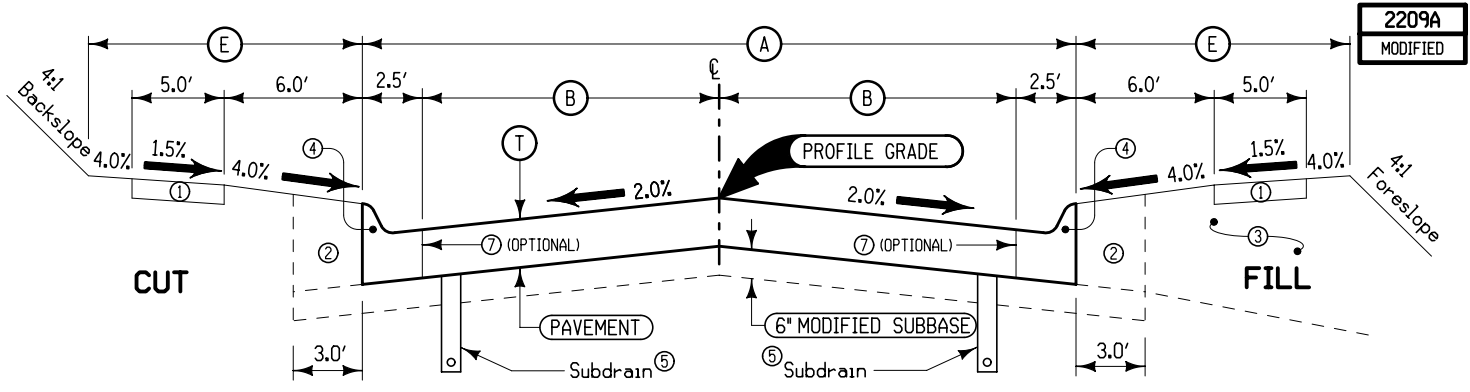
Notes:  
Normal section shown may be appropriately modified for areas specifically designated by the engineer such as intersections or superelevated curves.  
Refer to other drawings for details of shoulder design and construction.

**TYPICAL CROSS SECTION  
2 LANE ROADWAY WITH 2.5' CURB SECTION**

LOCATION ⑥		A	B	E	T
ROAD IDENTIFICATION	STATION TO STATION	Feet	Feet	Feet	Inches
12TH ST.	4603+14.16 - 4604+10.00	40	17.5	13	8
12TH ST.	4604+10.00 - 4605+07.08	40-62.5	17.5-28.75	13	8

- ① Refer to other drawings for details of shoulder and possible sidewalk construction
- ② Excavate and backfill 3.0'
- ③ Backfill
- ④ 6" Standard Curb
- ⑤ Refer to Standard Road Plan RF-19C
- ⑥ Refer to other drawings for limits of construction for this project.
- ⑦ \*KT-2" or "L-2" Joint

2209A  
MODIFIED



Notes:  
Normal section shown may be appropriately modified for areas specifically designated by the engineer such as intersections or superelevated curves.  
Refer to other drawings for details of shoulder design and construction.

**TYPICAL CROSS SECTION  
2 LANE ROADWAY WITH 2.5' CURB SECTION**

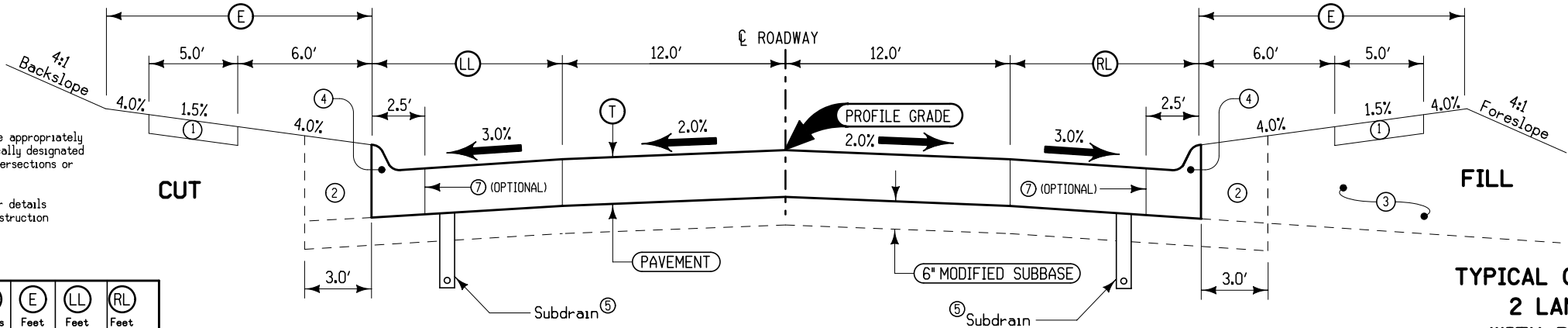
LOCATION ⑥		A	B	E	T
ROAD IDENTIFICATION	STATION TO STATION	Feet	Feet	Feet	Inches
BROWN ST.	9+62.00 - 11+60.42	29	12	13	8
MISSISSIPPI BLVD	8+60.00 - 11+70.00	29	12	13	8
MISSISSIPPI BLVD	12+10.01 - 16+06.69	29	12	13	8
10TH STREET	10100+00.00 - 10100+47.00	35.6	15.1L+	8	8
			15.5R+		
11TH STREET	11100+00.00 - 11100+85.05	34	14.5	13	8
13TH STREET	13100+46.50 - 13103+22.14	36.7	15.9	11	8
15TH STREET	105+40.00 - 106+05.50	36.6	15.7L+	11	8
			15.9R+		
16TH STREET	100+45.00 - 101+72.59	31.8	13.5L+	13	8
			13.3R+		
16TH STREET	102+33.40 - 103+53.87	29	12	13	8
16TH STREET	103+92.37 - 104+85.00	25.6	10.3	15	8

- ① Refer to other drawings for details of shoulder and possible sidewalk construction
- ② Excavate and backfill 3.0'
- ③ Backfill
- ④ 6" Standard Curb
- ⑤ Refer to Standard Road Plan RF-19C
- ⑥ Refer to other drawings for limits of construction for this project.
- ⑦ \*KT-2" or "L-2" Joint

3222B  
MODIFIED

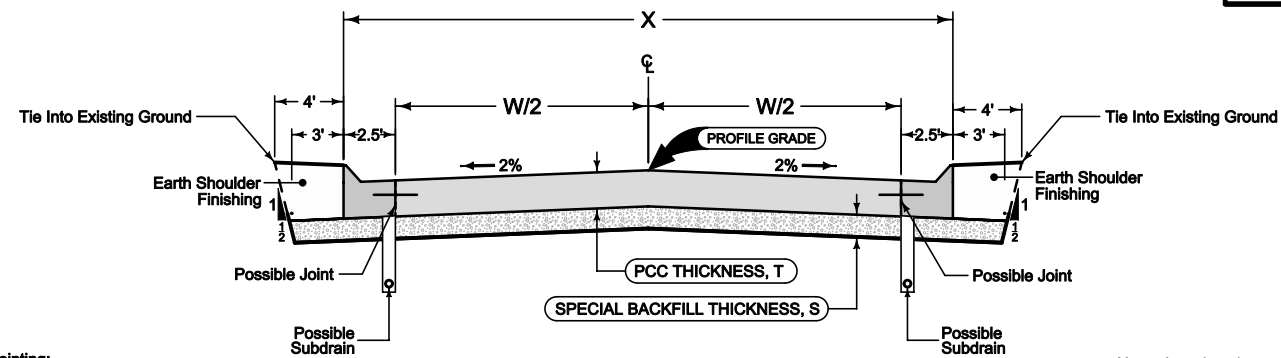
- ① Refer to other drawings for details of shoulder and possible sidewalk construction
- ② Excavate and backfill 3.0'
- ③ Backfill
- ④ 6" Standard Curb (PV-102)
- ⑤ Refer to Standard Road Plan RF-19C
- ⑥ Refer to other drawings for limits of construction for this project.
- ⑦ \*KT-2" or "L-2" Joint

Notes:  
Normal section shown may be appropriately modified for areas specifically designated by the engineer such as intersections or superelevated curves.  
Refer to other drawings for details of shoulder design and construction.



**TYPICAL CROSS SECTION  
2 LANE 43' B-B  
WITH PARKING AND  
2.5' CURB SECTION**

LOCATION ⑥		T	E	LL	RL
ROAD IDENTIFICATION	STATION TO STATION	Inches	Feet	Feet	Feet
15TH STREET	107+12.88 - 108+05.72	8	13	10	10
15TH STREET	108+05.72 - 108+50.00	8	13	VARIABLES	VARIABLES



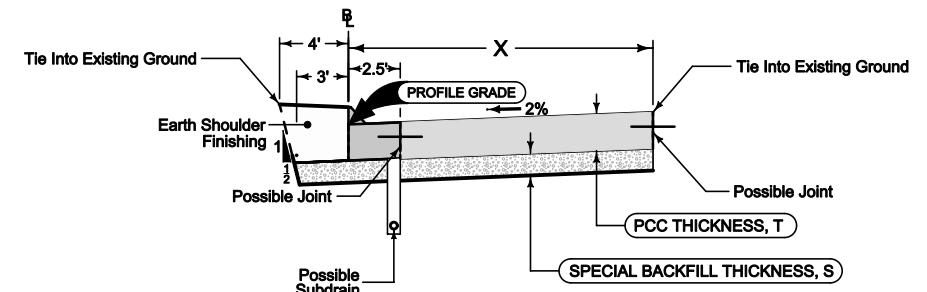
Shoulder Jointing:  
Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
Staged: KT-2  
Transverse: C at 20' spacing

### 2 LANE DETOUR PAVING

Normal section shown may be modified appropriately in areas of superelevated curves or other locations specifically designated by the Engineer.

LOCATION		DIMENSIONS				Curb Type See PV-102
ROAD IDENTIFICATION	STATION TO STATION	T Inches	S Inches	W Feet	X Feet	
TEMPORARY CONNECTION 10	41209+58.33 41211+92.52	8.5	12	11	27	6" Standard Curb and Gutter
TEMPORARY CONNECTION 11	12591+08.56 12593+58.81	8.5	12	VARIABLES	VARIABLES	6" Standard Curb and Gutter



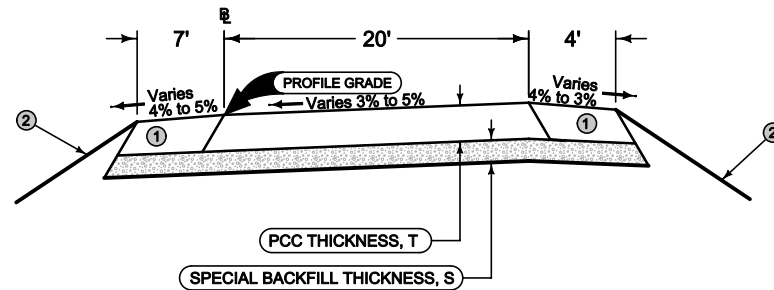
Shoulder Jointing:  
Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
Staged: KT-2  
Transverse: C at 20' spacing

### DETOUR PAVING - RUN AROUND

Normal section shown may be modified appropriately in areas of superelevated curves or other locations specifically designated by the Engineer.

LOCATION		DIMENSIONS			Curb Type See PV-102
ROAD IDENTIFICATION	STATION TO STATION	T Inches	S Inches	X Feet	
TEMPORARY CONNECTION 1	196+74.58 201+51.07	8.5	12	VARIABLES	6" Standard Curb and Gutter
TEMPORARY CONNECTION 2	212+56.30 218+18.49	8.5	12	VARIABLES	6" Standard Curb and Gutter
TEMPORARY CONNECTION 4		8.5	6	VARIABLES	6" Standard Curb and Gutter

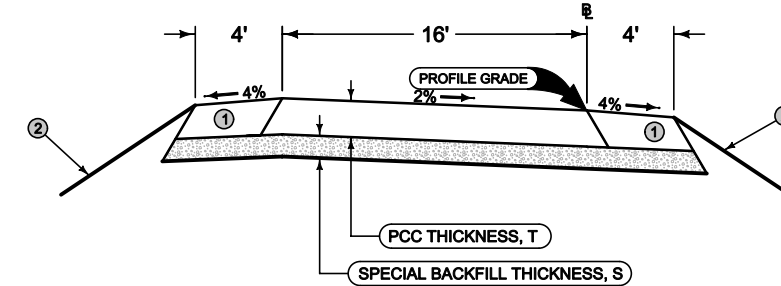


Quantity calculations based on vertical pavement edges.  
Normal section shown may be modified appropriately in areas of superelevated curves or other locations specifically designated by the Engineer.

- ① Full Depth HMA Shoulder
- ② Tie slope into existing ground with at least a 3:1 Foreslope

### TEMPORARY LOOP DETOUR PAVING

LOCATION		DIMENSIONS		HMA Full Depth Shoulder SY
ROAD IDENTIFICATION	STATION TO STATION	T Inches	S Inches	
TEMPORARY CONNECTION 9	21000+00.00 21002+48.53	8.5	12	253.51

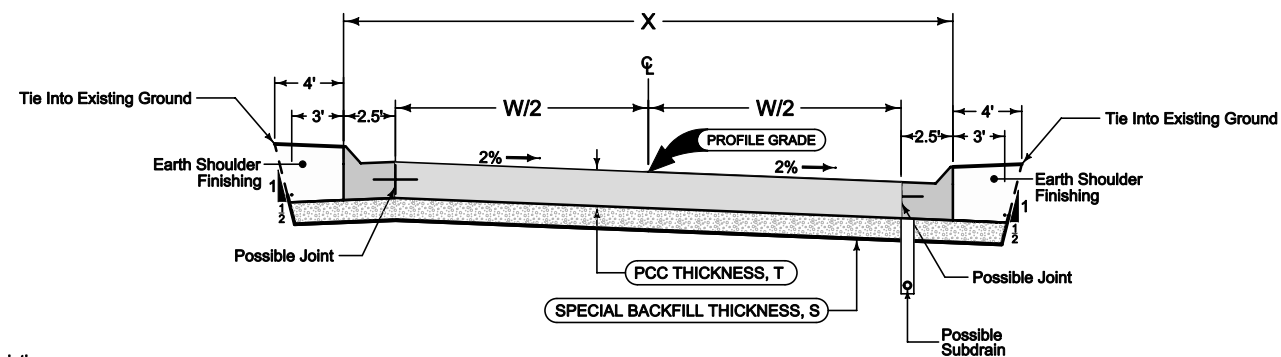


Quantity calculations based on vertical pavement edges.  
Normal section shown may be modified appropriately in areas of superelevated curves or other locations specifically designated by the Engineer.

- ① Full Depth HMA Shoulder
- ② Tie slope into existing ground with at least a 3:1 Foreslope

### TEMPORARY LOOP DETOUR PAVING

LOCATION		DIMENSIONS		HMA Full Depth Shoulder SY
ROAD IDENTIFICATION	STATION TO STATION	T Inches	S Inches	
TEMPORARY CONNECTION 3	428+75.87 430+19.46	8.5	12	55.46



Shoulder Jointing:  
Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

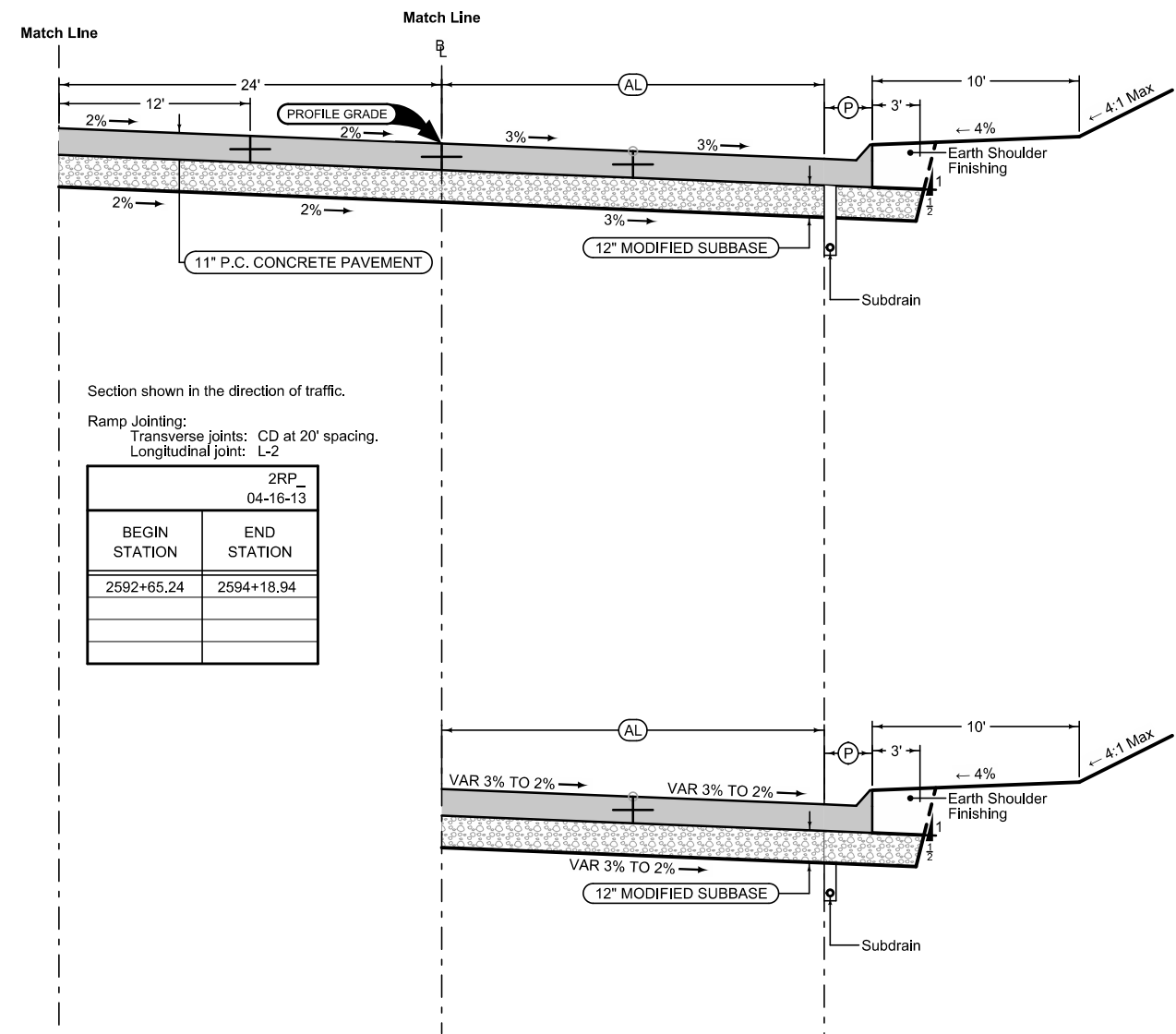
Single pour: L-2  
Staged: KT-2  
Transverse: C at 20' spacing

### 2 LANE DETOUR PAVING

Normal section shown may be modified appropriately in areas of superelevated curves or other locations specifically designated by the Engineer.

LOCATION		DIMENSIONS				Curb Type See PV-102
ROAD IDENTIFICATION	STATION TO STATION	T Inches	S Inches	W Feet	X Feet	
TEMPORARY CONNECTION 5	72000+12.88 72001+53.27	8.5	6	5.5	16	6" Standard Curb and Gutter





Section shown in the direction of traffic.  
 Ramp Jointing:  
 Transverse joints: CD at 20' spacing.  
 Longitudinal joint: L-2

2RP_04-16-13	
BEGIN STATION	END STATION
2592+65.24	2594+18.94

### Auxiliary Lane

Longitudinal joint: L or KT  
 Transverse joint: Match Mainline

2_AuxLane_PCC_10-19-10		(AL) Feet
STATION TO STATION		
2592+65.24	2593+60.00	24

### Curbed Shoulder

Shoulder jointing:  
 Longitudinal joint not required when distance from back of curb to nearest joint is less than 16':

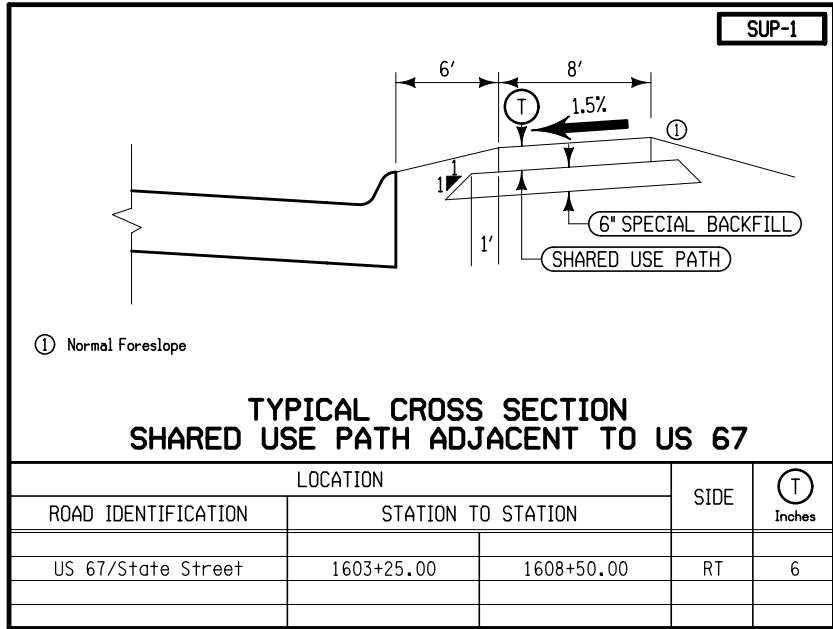
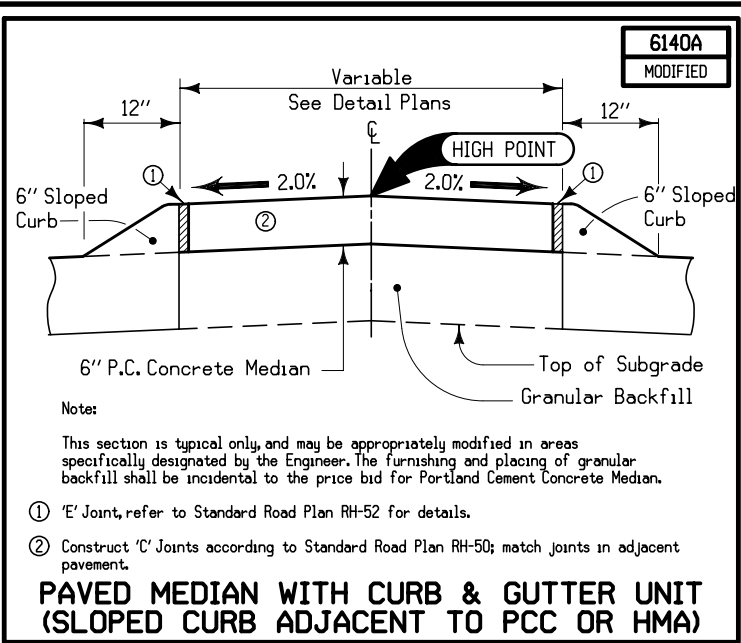
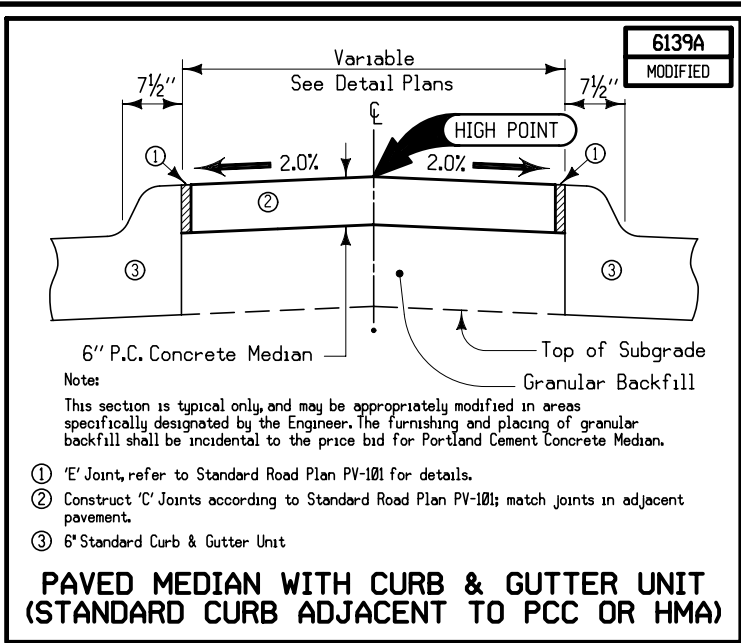
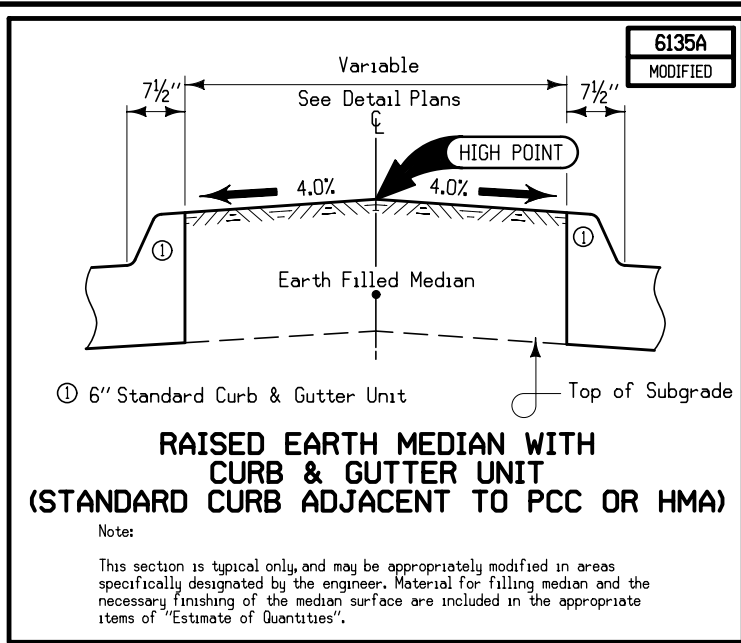
Transverse: C at 20' spacing  
 Single pour: L-2  
 Staged: KT-2

1R_Curb MODIFIED		(P) Feet	Curb Type See PV-102
BEGIN STATION	END STATION		
2592+65.24	2594+18.94	3'	6" Std

### Auxiliary Lane

Longitudinal joint: L or KT  
 Transverse joint: Match Mainline

2_AuxLane_PCC_MODIFIED		(AL) Feet
STATION TO STATION		
2593+60.00	2594+18.94	24



LOCATION		SIDE	T Inches
ROAD IDENTIFICATION	STATION TO STATION		
US 67/State Street	1603+25.00 1608+50.00	RT	6

### SURVEY SYMBOLS

### UTILITY LEGEND

### PLAN VIEW COLOR LEGEND OF PLAN AND PROFILE SHEETS (ROAD)

	Interstate Highway Symbol		Septic Tank
	U.S. Highway Symbol		Cistern
	Iowa Highway Symbol		L.P. Gas Tank (No Footing)
	County Road Highway Symbol		Underground Storage Tank
	Evergreen Tree		Latrine
	Deciduous Tree		Luminaire
	Fruit Tree		Traffic Signal
	Shrub (Bushes)		Traffic Signal with Luminaire
	Timber		Telephone Pedestal
	Hedge		Television Pedestal
	Stump		Telephone Pole
	Swamp		Telephone Pole (Second Company)
	Rock Outcrop		Telephone Pole (Third Company)
	Broken Concrete		Telephone Pole (Fourth Company)
	Revetment (Rip Rap)		Telephone Pole (Fifth Company)
	Cemetery		Power Pole
	Grave		Power Pole (Second Company)
	Cave		Power Pole (Third Company)
	Sink Hole		Power Pole (Fourth Company)
	Board Fence		Power Pole (Fifth Company)
	Chain Link or Security Fence		Electrical Highline Tower (Metal or Concrete)
	Wire Fence		Telephone Riser Pole
	Terrace		Power Riser Pole
	Earth Dam or Dike (Existing)		Telegraph Pole
	Earth Dam or Dike (Proposed)		Satellite TV Dish
	Tile Outlet		Guardrail (Beam or Cable)
	Edge of Water		Guard Post (one or two)
	Existing Drainage		Guard Post (over two)
	Proposed Drainage		Filler Pipe
	Right of Way Rail or Lot Corner		Gas Valve
	Concrete Monument		Water Valve
	Well		Speed Limit Sign
	Windmill		Mile Marker Post
	Beehive Intake		Sign
	Existing Intake		Water Hook Up
	Proposed Intake		Radio Tower
	Existing Utility Access (Manhole)		Tower Anchor
	Proposed Utility Access (Manhole)		Electric Box
	Fire Hydrant		Traffic Signal Control Box
	Water Hydrant (Rural)		Rail Road Signal Control Box
			Telephone Switch Box

	Existing Fiber Optics (Central Scott)
	Existing Fiber Optics (McLeod USA)
	Existing Fiber Optics (Qwest)
	Existing Fiber Optics (ATT)
	Existing Fiber Optics (MediaCom)
	Existing Fiber Optics (Bettendorf)
	Existing Fiber Optics (IowaDOT)
	Existing Power Line (MidAmerican)
	Existing Power Line (MidAmerican)
	Existing Power Line (MidAmerican)
	Existing Power Line (MidAmerican)
	Existing Power Line (IowaDOT)
	Existing Gas Line (MidAmerican)
	Existing High Pressure Gas Line (MidAmerican)
	Existing Sanitary Sewer Line (Bettendorf)
	Existing Sanitary Sewer Line (Davenport)
	Existing Telephone Line (Qwest)
	Existing Cable Television Line (MediaCom)
	Existing Cable Television Line (MediaCom)
	Existing Water Line (IA American)

LINE WORK	Design Color No.		
Green	(2)		Existing Topographic Features and Labels
Blue	(1)		Proposed Alignment, Stationing, Tic Marks, and Alignment Annotation
Magenta	(5)		Existing Utilities
Blue, Light	(230)		Previously Constructed Storm Sewer*
SHADING		Design Color No.	
Yellow	(4)		Highlight for Critical Notes or Features
Red	(3)		Delineates Restricted Areas
Lavender	(9)		Temporary Pavement Shading
Gray, Light	(48)		Proposed Bridge Shading
Gray, Med	(80)		Proposed Granular Shading
Gray, Dark	(112)		Proposed Pavement Shading
Brown, Light	(236)		Grading Shading
Brown, Dark	(237)		Previously Constructed Pavement*
Tan	(8)		Proposed Sidewalk Shading
Pink	(11)		Proposed Sidewalk Ramp Shading

LINE WORK	Design Color No.		
Green	(2)		Existing Ground Line Profile
Blue	(1)		Proposed Profile and Annotation
Magenta	(5)		Existing Utilities
Blue, Light	(230)		Proposed Ditch Grades, Left
Black	(0)		Proposed Ditch Grades, Median
Rust	(14)		Proposed Ditch Grades, Right

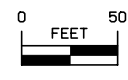
TABULATION OF UTILITIES	102-13A 10-29-02
CENTRAL SCOTT TELEPHONE: Fiber Optics	
McLEOD USA: Fiber Optics	
QWEST COMMUNICATIONS: Fiber Optics, Telephone Lines	
AT&T: Fiber Optics	
MEDIACOM: Fiber Optics, Television	
IOWA DOT: Fiber Optics, Power Lines	
MIDAMERICAN ENERGY - Power Lines, Gas	
BETTENDORF: Sanitary Sewer Line	
DAVENPORT: Sanitary Sewer Line	
IA-AMERICAN: Water Line	

CONVENTIONAL SIGNS	
	Survey Line
	Section Corner
	Ground Line Intercept
	Saw Cut
	Guardrail
	Properties Assumed Cleared (By Others) Prior to Construction

RIGHT OF WAY LEGEND	
	Proposed Right of Way
	Existing and Proposed Right of Way
	Easement and Existing Right of Way
	Borrow
	Easement (Temporary)
	Easement
	Excess
	Access Control

### Legend And Symbol Information Sheet D, E, F, AND K SHEETS (Symbols are Typical Only)

DAVENPORT TWP.  
T-78 N R-4 E  
SEC. 29



For Profile Details  
Refer to Sheet No. D.3

For Side Road Details  
Refer to E Sheets

For Storm Sewer Details  
Refer to M Sheets

Curve Data

PI Sta 198+57.61  
 $\Delta = 09^{\circ}13'58.61''$  LT  
 $D = 5^{\circ}43'46.48''$   
R = 1000.00  
T = 80.75  
L = 161.15  
E = 3.25  
e = N.C.  
L = NA  
X = NA  
m = NA

Curve Data

PI Sta 203+39.69  
 $\Delta = 06^{\circ}55'20.12''$  RT  
 $D = 2^{\circ}51'53.24''$   
R = 2000.00  
T = 120.96  
L = 241.63  
E = 3.65  
e = N.C.  
L = NA  
X = NA  
m = NA

STA. 197+00.00  
BEGIN CONSTRUCTION

POT STA. 199+65.00 (US 67 SB)  
=PI STA. 4605+66.55 (KIMBERLY RD)

STA. 203+20.00, 0.00  
END 10:1 TAPER

64°00'00.02"  
STA. 202+00.00, 12.00 RT  
BEGIN 10:1 TAPER

PI Sta 203+39.69

PI Sta 198+57.61

PC Sta 202+18.73

PT Sta 204+60.36

EX. STATE ST

US 67 SB

ENGLISH

IOWA DOT

DESIGN TEAM **WHKS & Co.**

SCOTT COUNTY

PROJECT NUMBER **IM-74-1(226)4--13-82**

SHEET NUMBER **D.2**



DAVENPORT TWP.  
T-78 N R-4 E  
SEC. 28

Curve Data

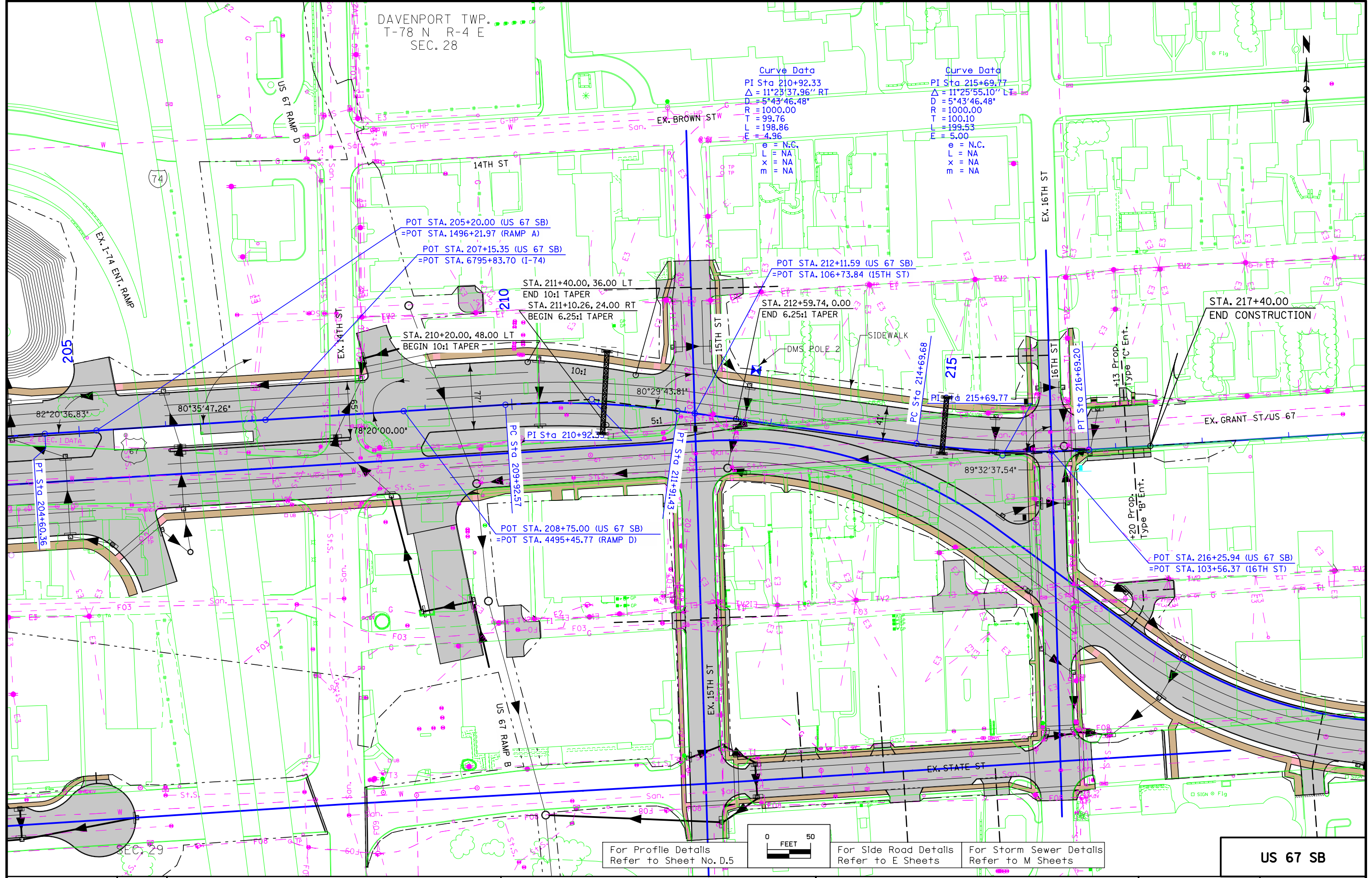
PI Sta 210+92.33  
 $\Delta = 11^{\circ}23'37.96''$  RT  
 $D = 5^{\circ}43'46.48''$   
R = 1000.00  
T = 99.76  
L = 198.86  
E = -4.96

Curve Data

PI Sta 215+69.77  
 $\Delta = 11^{\circ}25'55.10''$  LT  
 $D = 5^{\circ}43'46.48''$   
R = 1000.00  
T = 100.10  
L = 199.53  
E = 5.00

e = N.C.  
L = NA  
X = NA  
m = NA

e = N.C.  
L = NA  
X = NA  
m = NA



POT STA. 205+20.00 (US 67 SB)  
=POT STA. 1496+21.97 (RAMP A)  
POT STA. 207+15.35 (US 67 SB)  
=POT STA. 6795+83.70 (I-74)

STA. 211+40.00, 36.00 LT  
END 10:1 TAPER  
STA. 211+10.26, 24.00 RT  
BEGIN 6.25:1 TAPER

STA. 210+20.00, 48.00 LT  
BEGIN 10:1 TAPER

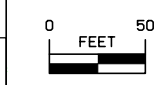
POT STA. 212+11.59 (US 67 SB)  
=POT STA. 106+73.84 (15TH ST)

STA. 212+59.74, 0.00  
END 6.25:1 TAPER

STA. 217+40.00  
END CONSTRUCTION

POT STA. 208+75.00 (US 67 SB)  
=POT STA. 4495+45.77 (RAMP D)

POT STA. 216+25.94 (US 67 SB)  
=POT STA. 103+56.37 (16TH ST)



For Profile Details  
Refer to Sheet No. D.5

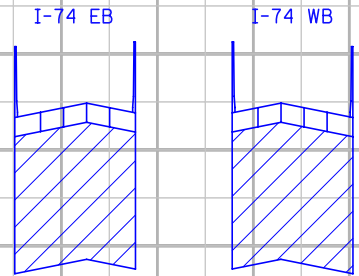
For Side Road Details  
Refer to E Sheets

For Storm Sewer Details  
Refer to M Sheets

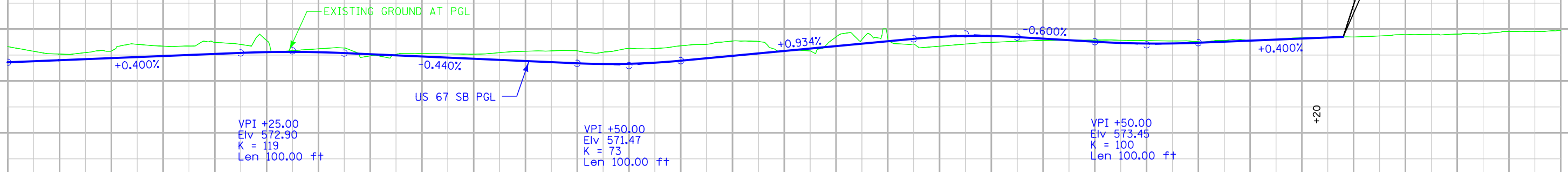
US 67 SB

Fill+30% = 6 CY  
 Waste = 7,898 CY  
 7,904 CY

Cut = 7,904 CY



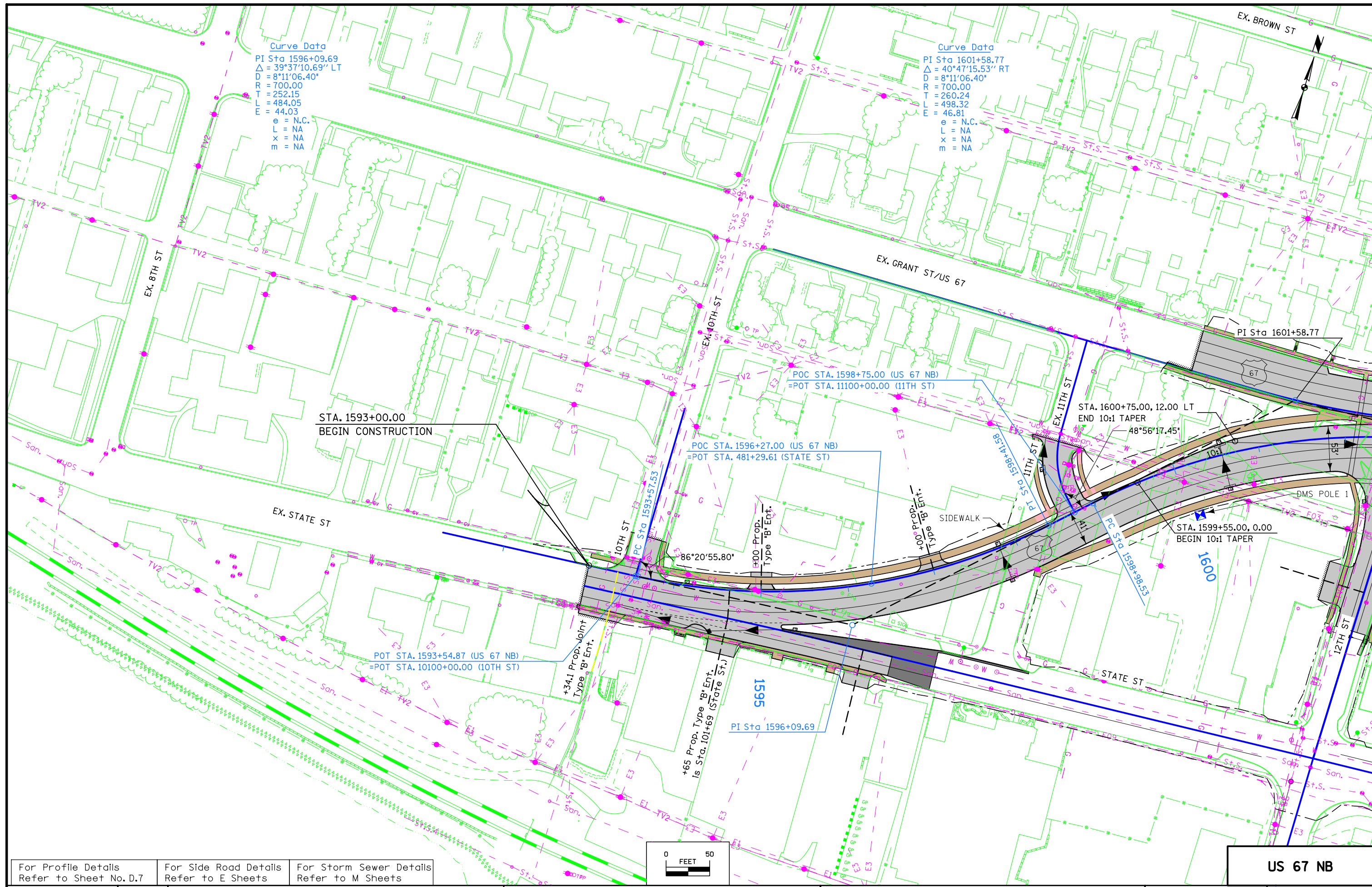
I-74 EBL VERTICAL CLEARANCE = 17.25'  
 I-74 WBL VERTICAL CLEARANCE = 17.01'



STA. 217+40.00  
 END CONSTRUCTION  
 ELEV. 574.21

**US 67 SB**  
 For Plan Details  
 Refer to Sheet No.D.4

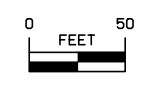
Lt	Ramp A	Curb & Gutter	Ramp D	Curb & Gutter	15th St.	Curb & Gutter	16th St.	C&G Ent.C&G	Lt																																										
Rt	Intersection	Curb & Gutter	Intersection	Curb & Gutter	Curb & Gutter	Curb & Gutter	16th St.	C&G Ent.C&G	Rt																																										
571.80	571.90	572.00	572.10	572.20	572.30	572.40	572.50	572.60	572.70	572.77	572.80	572.76	572.68	572.57	572.46	572.35	572.24	572.13	572.02	571.91	571.80	571.69	571.62	571.64	571.75	571.94	572.17	572.40	572.64	572.87	573.10	573.34	573.57	573.80	574.04	574.22	574.31	574.31	574.20	574.05	573.90	573.75	573.64	573.58	573.59	573.65	573.75	573.85	573.95	574.05	574.15



Curve Data  
 PI Sta 1596+09.69  
 $\Delta = 39^\circ 37' 10.69''$  LT  
 D = 8'11'06.40"  
 R = 700.00  
 T = 252.15  
 L = 484.05  
 E = 44.03  
 e = N.C.  
 L = NA  
 x = NA  
 m = NA

Curve Data  
 PI Sta 1601+58.77  
 $\Delta = 40^\circ 47' 15.53''$  RT  
 D = 8'11'06.40"  
 R = 700.00  
 T = 260.24  
 L = 498.32  
 E = 46.81  
 e = N.C.  
 L = NA  
 x = NA  
 m = NA

For Profile Details Refer to Sheet No. D.7  
 For Side Road Details Refer to E Sheets  
 For Storm Sewer Details Refer to M Sheets



US 67 NB



Fill+30% = 588 CY  
 Waste = 2,699 CY  
 3,287 CY

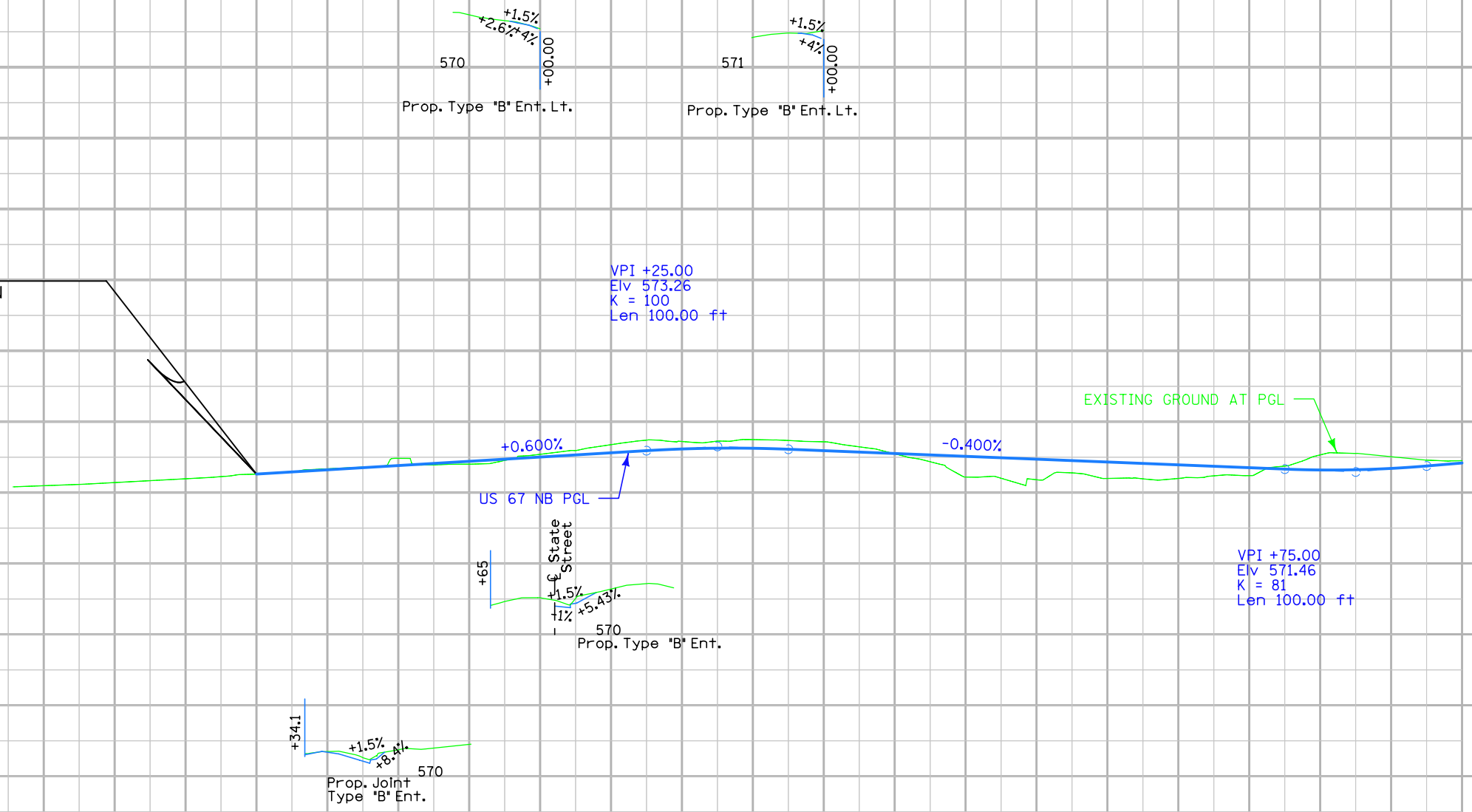
Cut = 3,287 CY



STA. 1593+00.00  
 BEGIN CONSTRUCTION  
 ELEV. 571.31

VPI +25.00  
 Elev 573.26  
 K = 100  
 Len 100.00 ft

VPI +75.00  
 Elev 571.46  
 K = 81  
 Len 100.00 ft



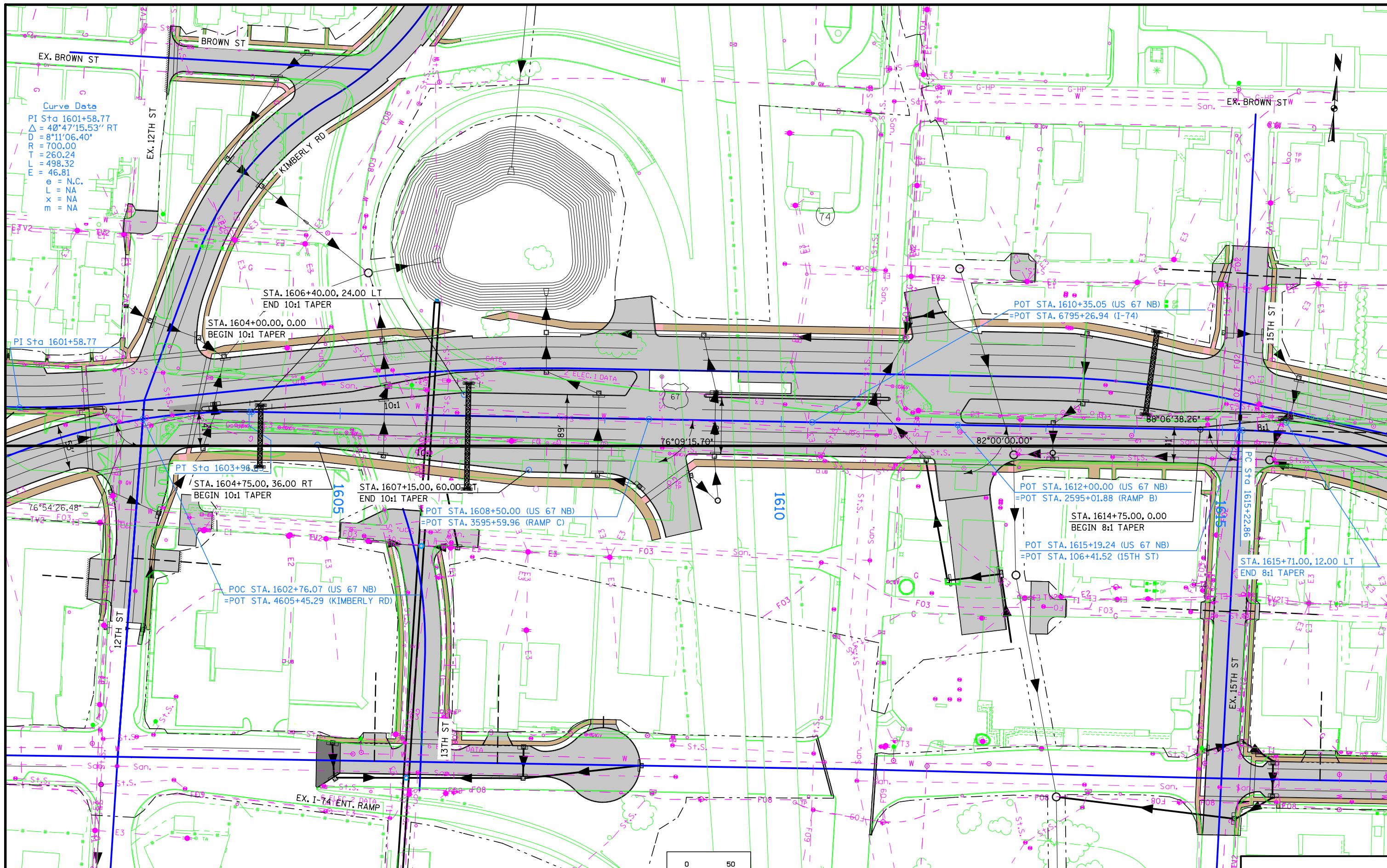
EXISTING GROUND AT PGL

US 67 NB PGL

+65  
 State Street

**US 67 NB**  
 For Plan Details  
 Refer to Sheet No.D.6

Lt	C&G	10th St.	Curb & Gutter	Ent.	Curb & Gutter	Ent.	C&G	11th St.	Curb & Gutter	Lt																									
Rt	Ent.	Curb & Gutter	11th St.	Ent.	Curb & Gutter					Rt																									
	571.31	571.46	571.61	571.76	571.91	572.06	572.21	572.36	572.51	572.66	572.81	572.96	573.08	573.13	573.13	573.06	572.96	572.86	572.76	572.66	572.56	572.46	572.36	572.26	572.16	572.06	571.96	571.86	571.76	571.66	571.60	571.61	571.71	571.88	572.09



Curve Data  
 PI Sta 1601+58.77  
 $\Delta = 40^\circ 47' 15.53''$  RT  
 $D = 8^\circ 11' 06.40''$   
 $R = 700.00$   
 $T = 260.24$   
 $L = 498.32$   
 $E = 46.81$   
 $e =$  N.C.  
 $L =$  NA  
 $X =$  NA  
 $m =$  NA

PI Sta 1601+58.77

STA. 1606+40.00, 24.00 LT  
 END 10:1 TAPER  
 STA. 1604+00.00, 0.00  
 BEGIN 10:1 TAPER

PT Sta 1603+96.00  
 STA. 1604+75.00, 36.00 RT  
 BEGIN 10:1 TAPER

STA. 1607+15.00, 60.00 LT  
 END 10:1 TAPER

POT STA. 1608+50.00 (US 67 NB)  
 =POT STA. 3595+59.96 (RAMP C)

POC STA. 1602+76.07 (US 67 NB)  
 =POT STA. 4605+45.29 (KIMBERLY RD)

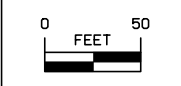
POT STA. 1612+00.00 (US 67 NB)  
 =POT STA. 2595+01.88 (RAMP B)

STA. 1614+75.00, 0.00  
 BEGIN 8:1 TAPER

POT STA. 1615+19.24 (US 67 NB)  
 =POT STA. 106+41.52 (15TH ST)

STA. 1615+71.00, 12.00 LT  
 END 8:1 TAPER

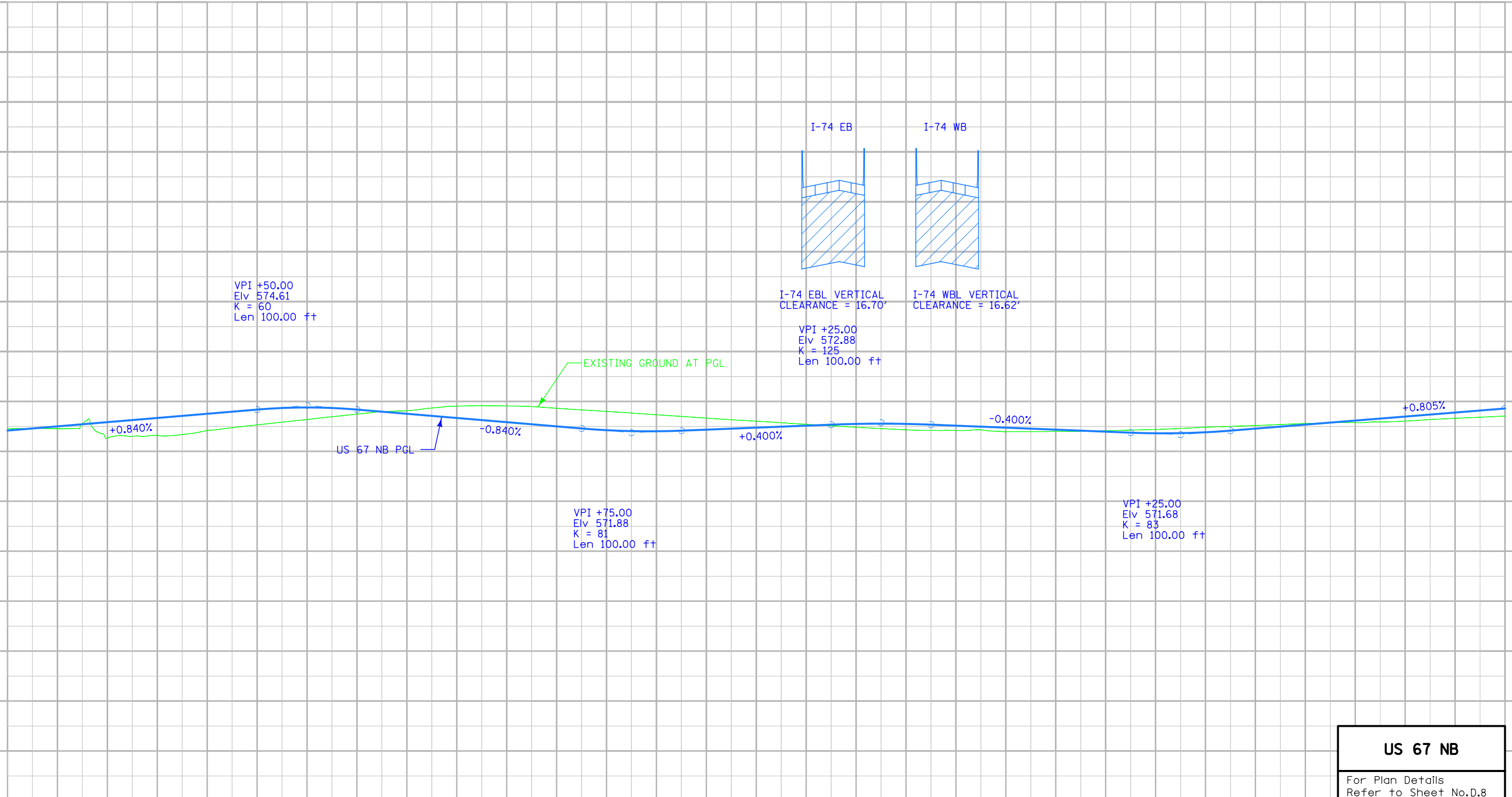
For Profile Details Refer to Sheet No. D.9  
 For Side Road Details Refer to E Sheets  
 Refer to M Sheets for Storm Sewer Details



US 67 NB

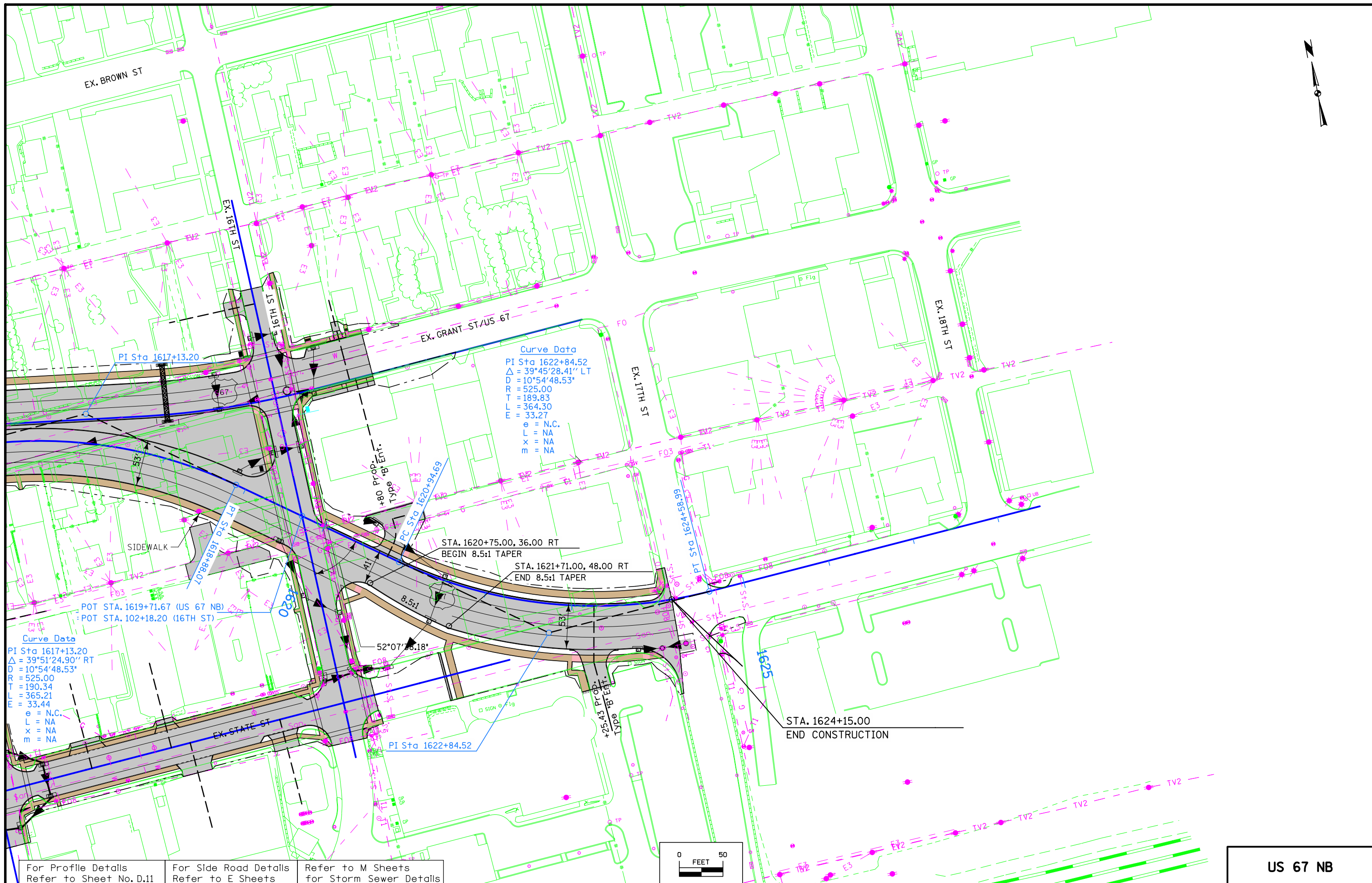
Fill+30% = 322 CY  
 Waste = 7,456 CY  
 7,778 CY

Cut = 7,778 CY



**US 67 NB**  
 For Plan Details  
 Refer to Sheet No.D.8

Lt	Kimberly Rd.	Curb & Gutter	Intersection	Curb & Gutter	Intersection	Curb & Gutter	15th St.	Lt																																																				
Rt	12th St.	Curb & Gutter	Ramp C	Curb & Gutter	Ramp B	Curb & Gutter		Rt																																																				
	572.30	572.51	572.72	572.93	573.14	573.35	573.56	573.77	573.98	574.19	574.35	574.40	574.35	574.19	573.98	573.77	573.56	573.35	573.14	572.93	572.72	572.51	572.30	572.13	572.04	572.02	572.08	572.18	572.28	572.38	572.48	572.58	572.68	572.76	572.78	572.76	572.68	572.58	572.48	572.38	572.28	572.18	572.08	571.98	571.88	571.82	571.83	571.92	572.08	572.28	572.49	572.69	572.89	573.09	573.29	573.49	573.69	573.89	574.10	574.30

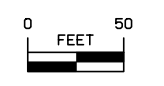


Curve Data  
 PI Sta 1617+13.20  
 $\Delta = 39^\circ 51' 24.90''$  RT  
 $D = 10^\circ 54' 48.53''$   
 $R = 525.00$   
 $T = 190.34$   
 $L = 365.21$   
 $E = 33.44$   
 $e =$  N.C.  
 $L =$  NA  
 $x =$  NA  
 $m =$  NA

Curve Data  
 PI Sta 1622+84.52  
 $\Delta = 39^\circ 45' 28.41''$  LT  
 $D = 10^\circ 54' 48.53''$   
 $R = 525.00$   
 $T = 189.83$   
 $L = 364.30$   
 $E = 33.27$   
 $e =$  N.C.  
 $L =$  NA  
 $x =$  NA  
 $m =$  NA

STA. 1620+75.00, 36.00 RT  
 BEGIN 8.5:1 TAPER  
 STA. 1621+71.00, 48.00 RT  
 END 8.5:1 TAPER

STA. 1624+15.00  
 END CONSTRUCTION



For Profile Details Refer to Sheet No. D.11  
 For Side Road Details Refer to E Sheets  
 Refer to M Sheets for Storm Sewer Details

**US 67 NB**

Fill+30% = 252 CY  
 Waste = 4,088 CY  
 4,340 CY

Cut = 4,340 CY



+0.2% +2%  
 571  
 Prop. Type 'B' Ent. Lt.

VPI +00.00  
 Elv 574.70  
 K = 83  
 Len 100.00 ft

STA. 1624+15.00  
 END CONSTRUCTION  
 ELEV. 573.96

EXISTING GROUND AT PGL

-0.400%

+0.400%

US 67 NB PGL

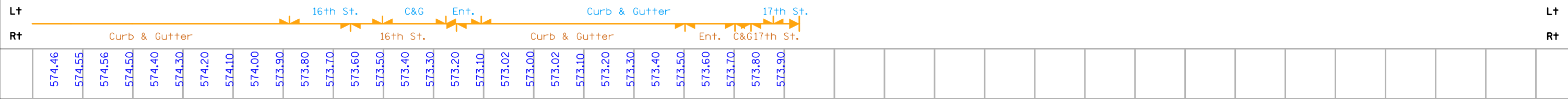
VPI +50.00  
 Elv 572.90  
 K = 125  
 Len 100.00 ft

+25.43 Prop. Type 'B' Ent.

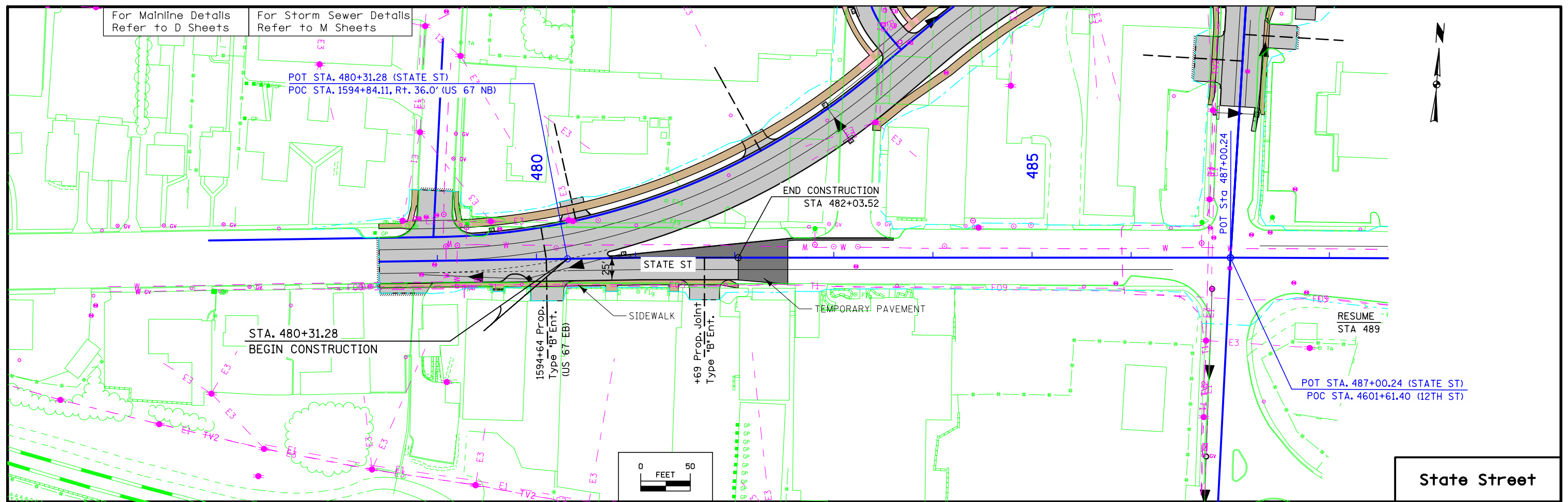
570

**US 67 NB**

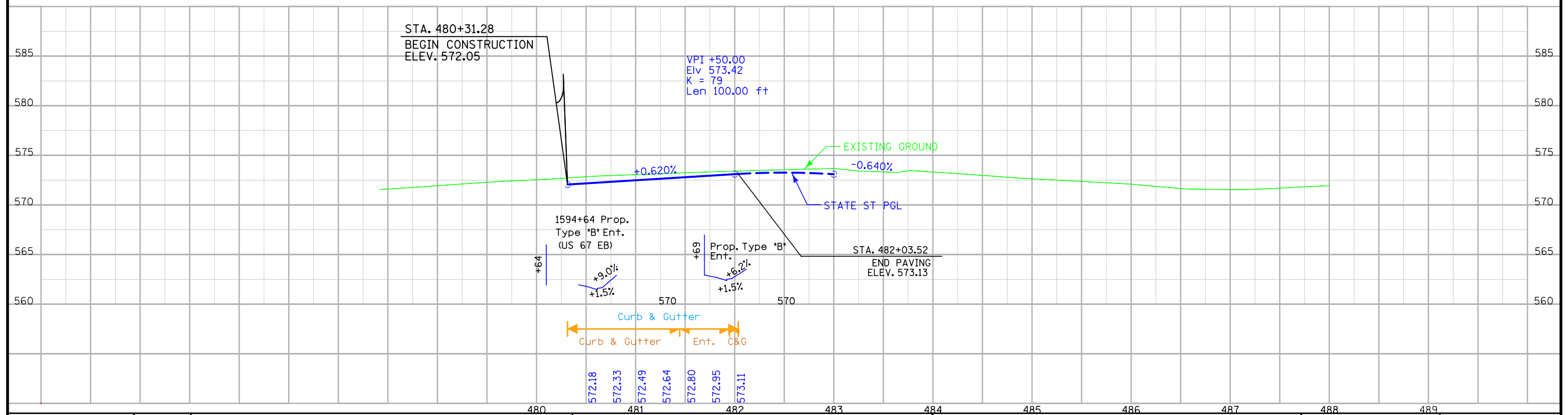
For Plan Details  
 Refer to Sheet No.D.10



For Mainline Details Refer to D Sheets  
 For Storm Sewer Details Refer to M Sheets

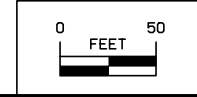
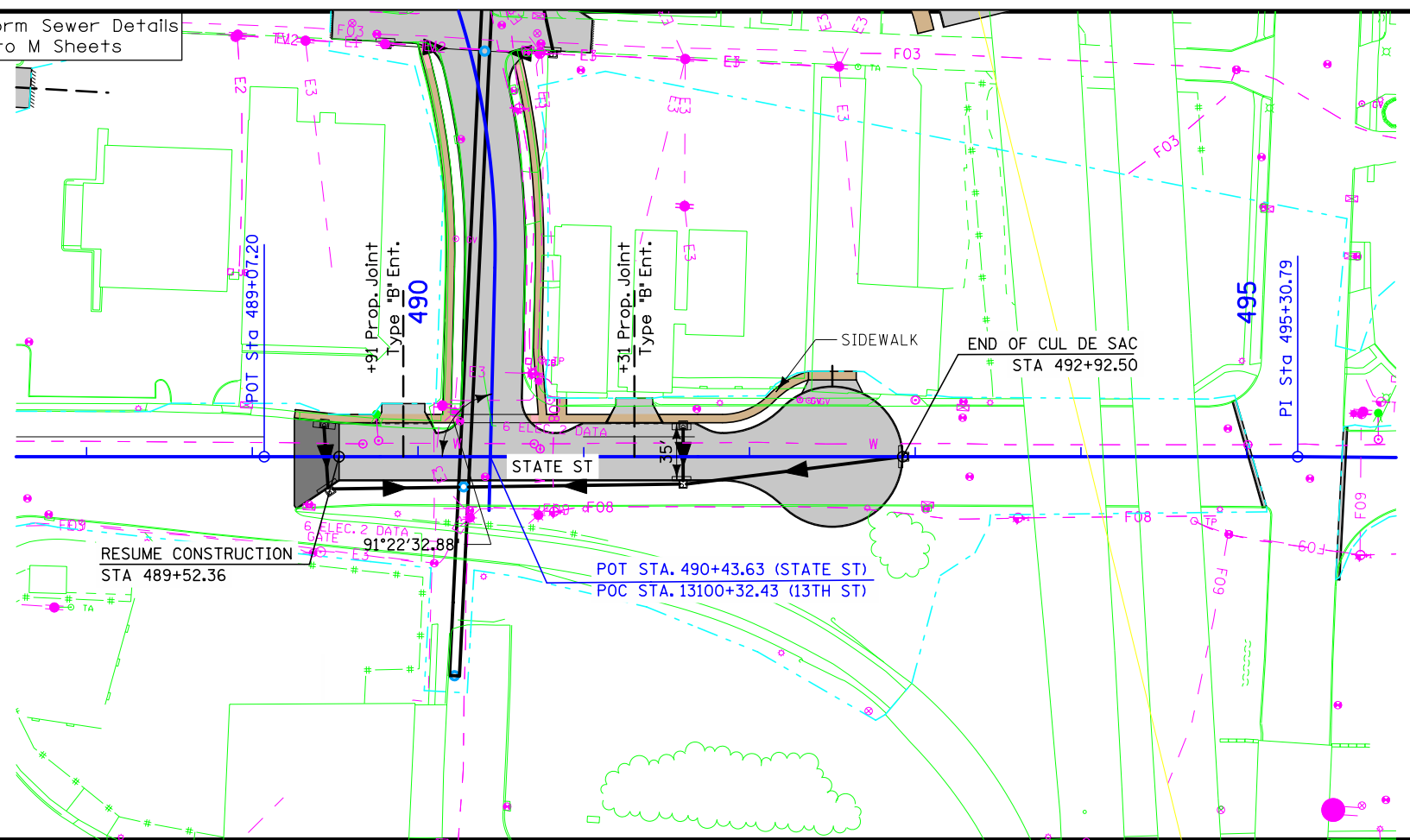


Fill+30% = CY    Cut =    CY  
 Waste =    CY  
 ←    →



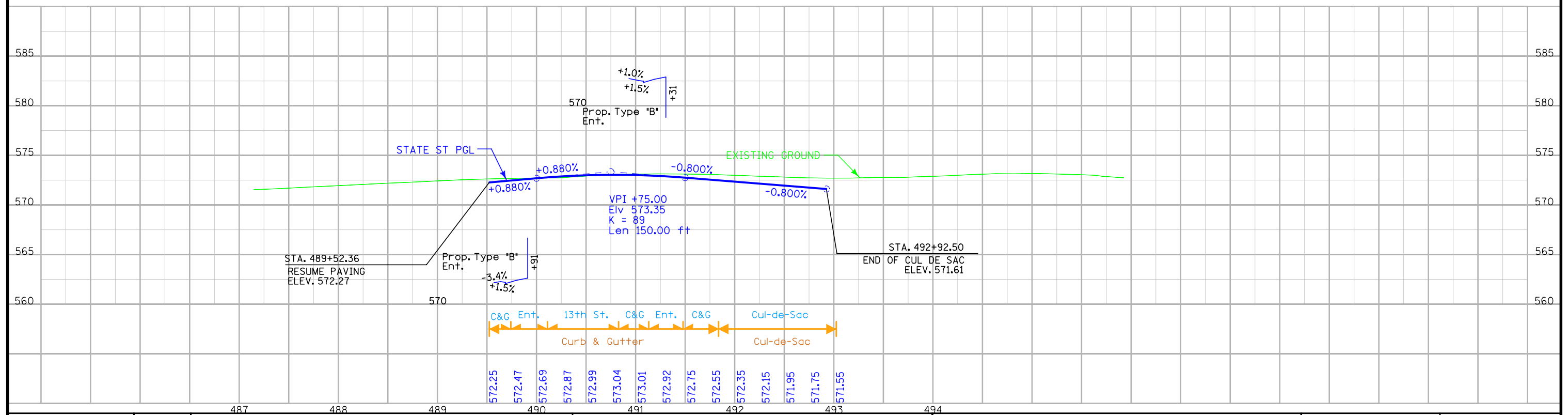
For Mainline Details  
Refer to D Sheets

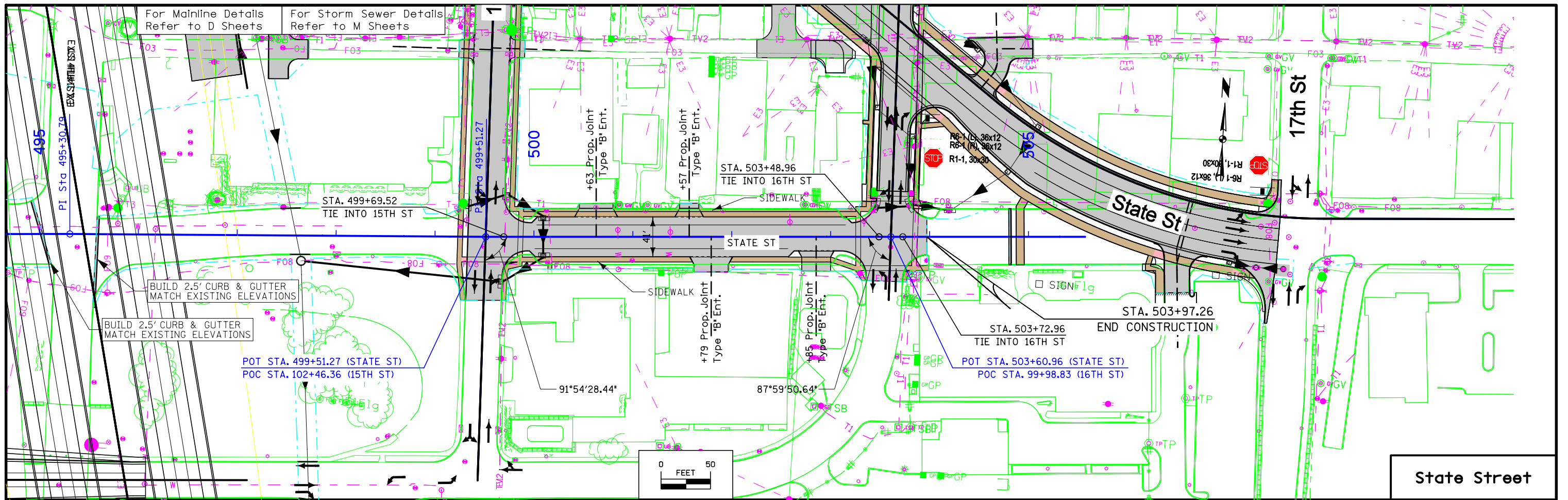
For Storm Sewer Details  
Refer to M Sheets



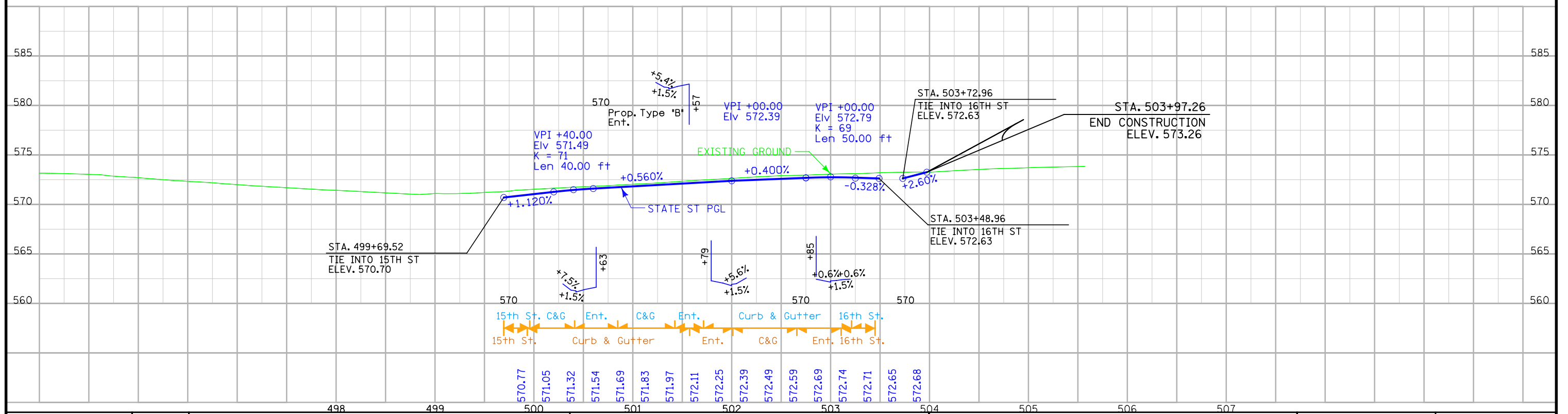
State Street

Fill+30% = CY    Cut = CY  
Waste = CY





Fill+30% = CY    Cut = CY  
Waste = CY    CY





For Profile Details Refer to Sheet No. E.5  
 For Mainline Details Refer to D Sheets  
 For Storm Sewer Details Refer to M Sheets

2035 ADT  
 = 1600 VPD

DAVENPORT TWP.  
 T-78 N R-4 E  
 SEC. 29

Curve Data

$\Delta = 39^{\circ}21'04.13''$  RT  
 $D = 16^{\circ}22'12.80''$   
 $R = 350.00$   
 $T = 125.15$   
 $L = 248.38$   
 $E = 21.70$   
 $e = N.C.$

Curve Data

$\Delta = 57^{\circ}17'23.73''$  LT  
 $D = 16^{\circ}22'12.80''$   
 $R = 350.00$   
 $T = 191.18$   
 $L = 349.96$   
 $E = 48.81$   
 $e = N.C.$

PI STA. 4605+66.55 (KIMBERLY RD)  
 =POT STA. 199+65.00 (US 67 SB)

STA. 4606+06.61  
 TIE INTO US 67 SB

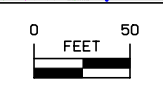
PI Sta 4608+63.42

PC Sta 4607+38.27

POT STA. 4614+43.90 (KIMBERLY RD)  
 =PI STA. 11+90.00 (MISSISSIPPI BLVD)

POT STA. 4610+35.05 (KIMBERLY RD)  
 =POT STA. 11+90.80 (BROWN ST)

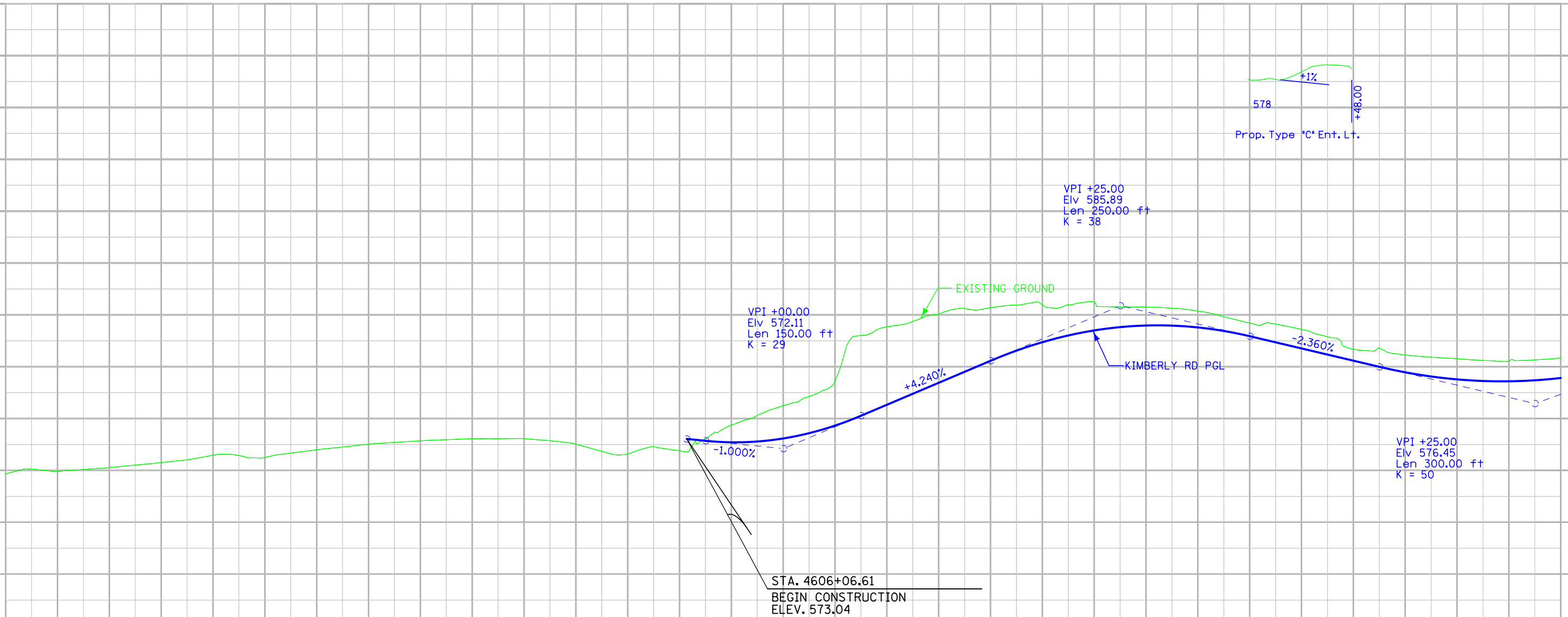
PI Sta 4611+69.83



KIMBERLY ROAD

Fill+30% = 255 CY  
 Waste = 9,871 CY  
 10,126 CY

Cut = 10,126 CY



**KIMBERLY ROAD**  
 For Plan Details  
 Refer to Sheet No.E.4

Lt	Curb & Gutter																						Brown St.		Curb & Gutter		Alley Curb & Gutter		Mississippi Blvd.		Lt			
Rt	Curb & Gutter																						Temp. Kimberly Road		Curb & Gutter		Mississippi Blvd.		Rt					
	572.86	572.72	572.79	573.09	573.60	574.34	575.29	576.35	577.41	578.47	579.53	580.59	581.56	582.38	583.02	583.51	583.82	583.98	583.96	583.79	583.44	582.94	582.35	581.76	581.17	580.58	579.99	579.46	579.06	578.78	578.63	578.60	578.70	578.92

For Profile Details Refer to Sheet No. E.7 | For Mainline Details Refer to D Sheets | For Storm Sewer Details Refer to M Sheets

DAVENPORT TWP.  
T-78 N R-4 E  
SEC. 29

Curve Data  
 $\Delta = 52^{\circ}17'21.44''$  LT  
 $D = 6^{\circ}21'58.31''$   
 $R = 900.00$   
 $T = 441.78$   
 $L = 821.36$   
 $E = 102.58$   
 $e = 3.60$

POT STA. 4614+43.09 (KIMBERLY RD)  
 =PI STA. 11+90.00 (MISSISSIPPI BLVD)

STA. 4627+00.00  
 END CONSTRUCTION

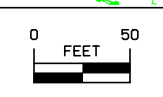
4620

Pc Sta 4616+65.09

PI Sta 4624+86.45

PI Sta 4621+06.86

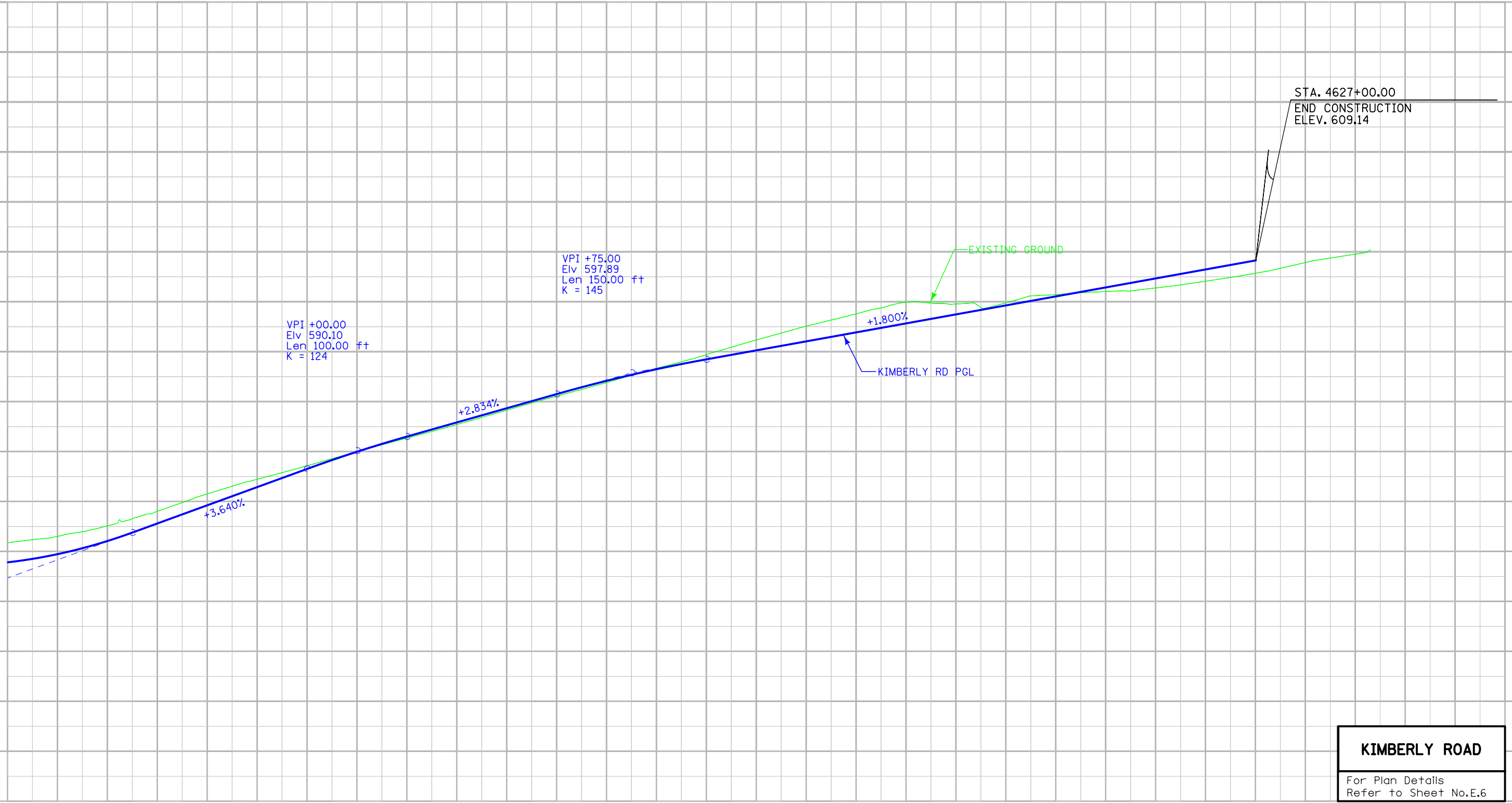
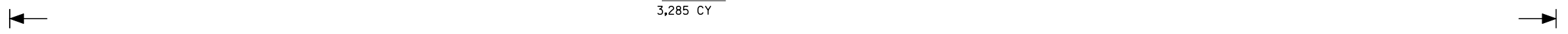
74



**KIMBERLY ROAD**

Fill+30% = 2,655 CY  
 Waste = 630 CY  
 3,285 CY

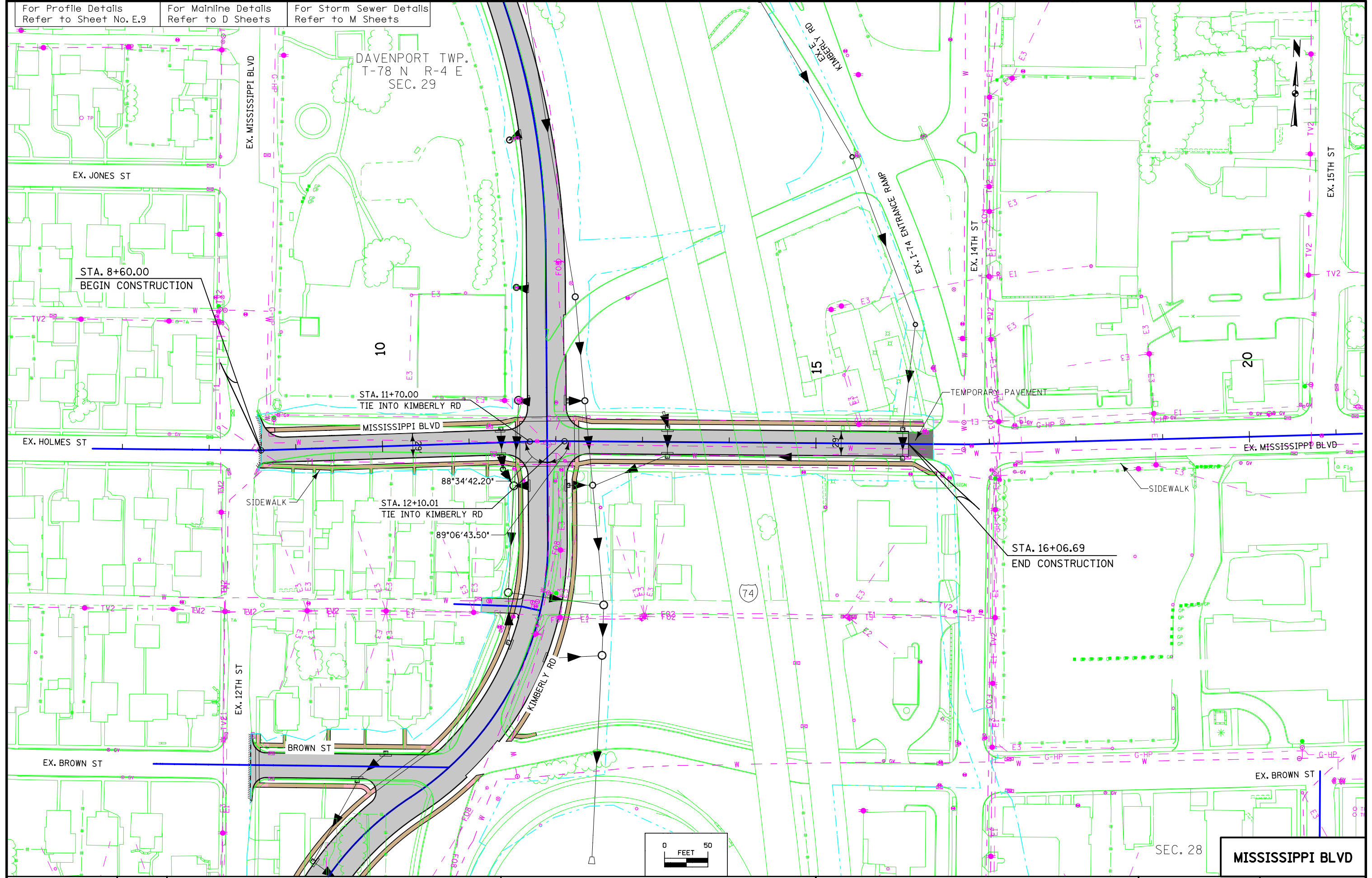
Cut = 3,285 CY



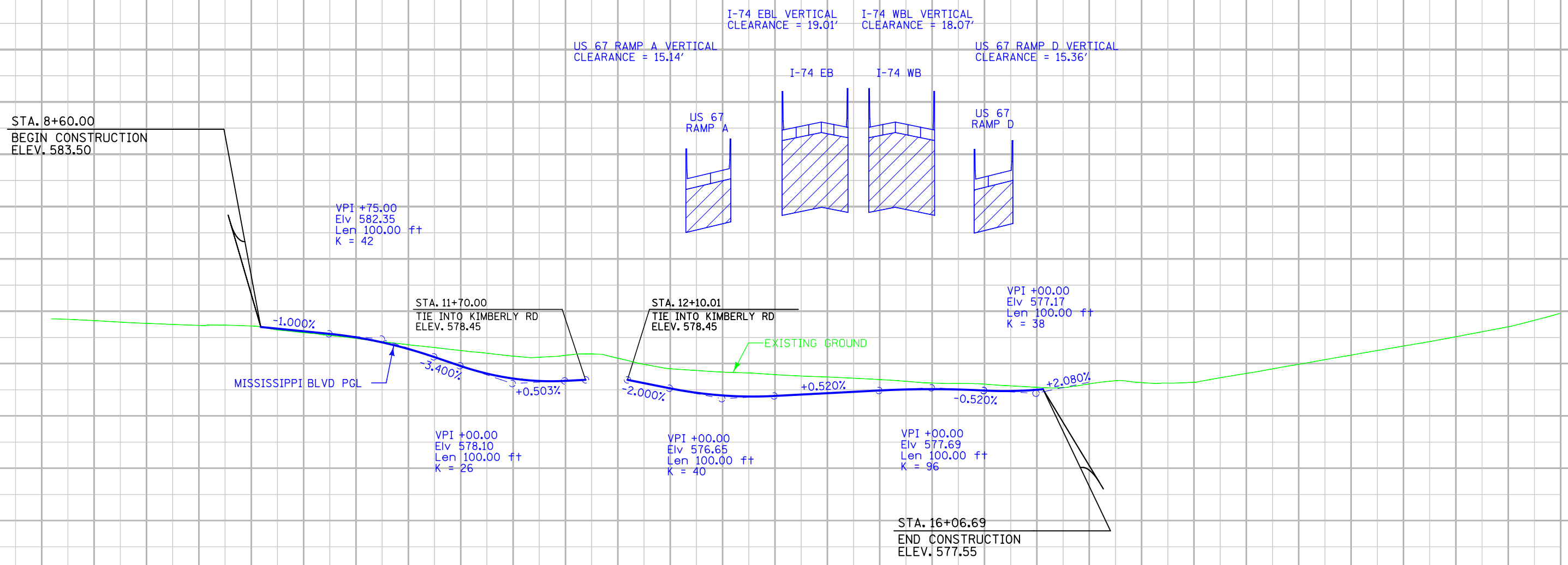
**KIMBERLY ROAD**  
 For Plan Details  
 Refer to Sheet No.E.6

Lt	Mississippi Blvd.	Curb & Gutter	Lt																																															
Rt	Mississippi Blvd.	Curb & Gutter	Rt																																															
578.92	579.27	579.74	580.34	581.06	581.91	582.82	583.73	584.64	585.55	586.46	587.37	588.28	589.16	590.00	590.78	591.51	592.22	592.93	593.64	594.35	595.06	595.76	596.45	597.10	597.70	598.25	598.77	599.24	599.69	600.14	600.59	601.04	601.49	601.94	602.39	602.84	603.29	603.74	604.19	604.64	605.09	605.54	605.99	606.44	606.89	607.34	607.79	608.24	608.69	609.14

For Profile Details Refer to Sheet No. E.9  
 For Mainline Details Refer to D Sheets  
 For Storm Sewer Details Refer to M Sheets



Fill+30% = 231 CY  
 Waste = 2,592 CY  
 2,823 CY  
 Cut = 2,823 CY



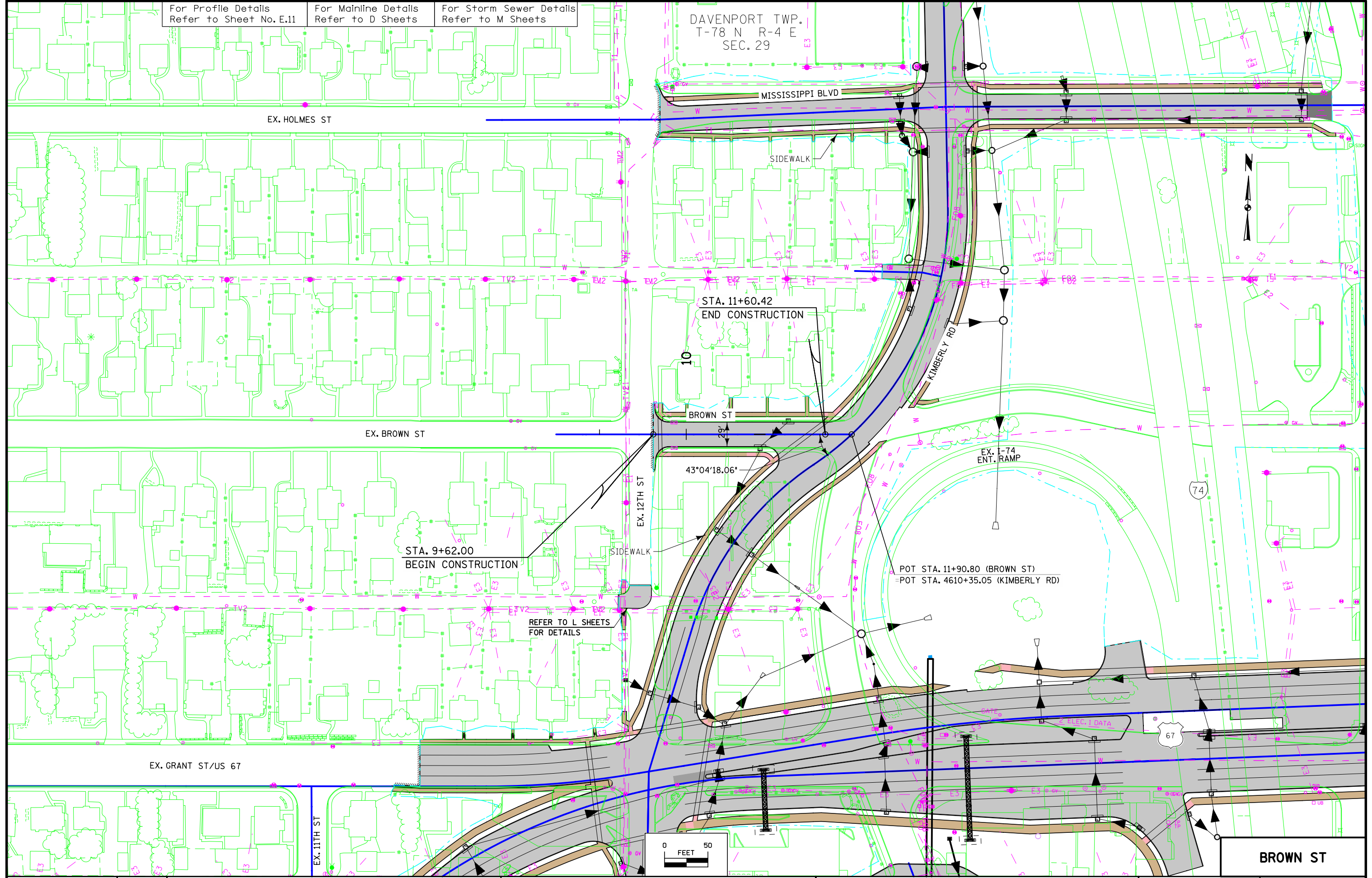
**MISSISSIPPI BLVD**  
 For Plan Details  
 Refer to Sheet No.E.8



583.35	583.10	582.85	582.52	582.05	581.42	580.65	579.80	579.07	578.59	578.35	578.35	578.45	578.45	578.15	577.65	577.23	576.97	576.86	576.91	577.04	577.17	577.30	577.43	577.53	577.56	577.53	577.43	577.38	577.50
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For Profile Details Refer to Sheet No. E.11  
 For Mainline Details Refer to D Sheets  
 For Storm Sewer Details Refer to M Sheets

DAVENPORT TWP.  
 T-78 N R-4 E  
 SEC. 29



Fill+30% = 36 CY  
 Waste = 293 CY  
 329 CY



STA. 9+62.00  
 BEGIN CONSTRUCTION  
 ELEV. 585.49

VPI +00.00  
 Elev 585.42  
 Len 75.00 ft  
 K = 42

-0.200%  
 -2.002%  
 +1.000%  
 BROWN ST PGL

VPI +25.00  
 Elev 582.92  
 Len 60.00  
 K = 20

EXISTING GROUND

STA. 11+60.42  
 END CONSTRUCTION  
 ELEV. 583.27

**BROWN ST**  
 For Plan Details  
 Refer to Sheet No.E.10

Lt

Rt

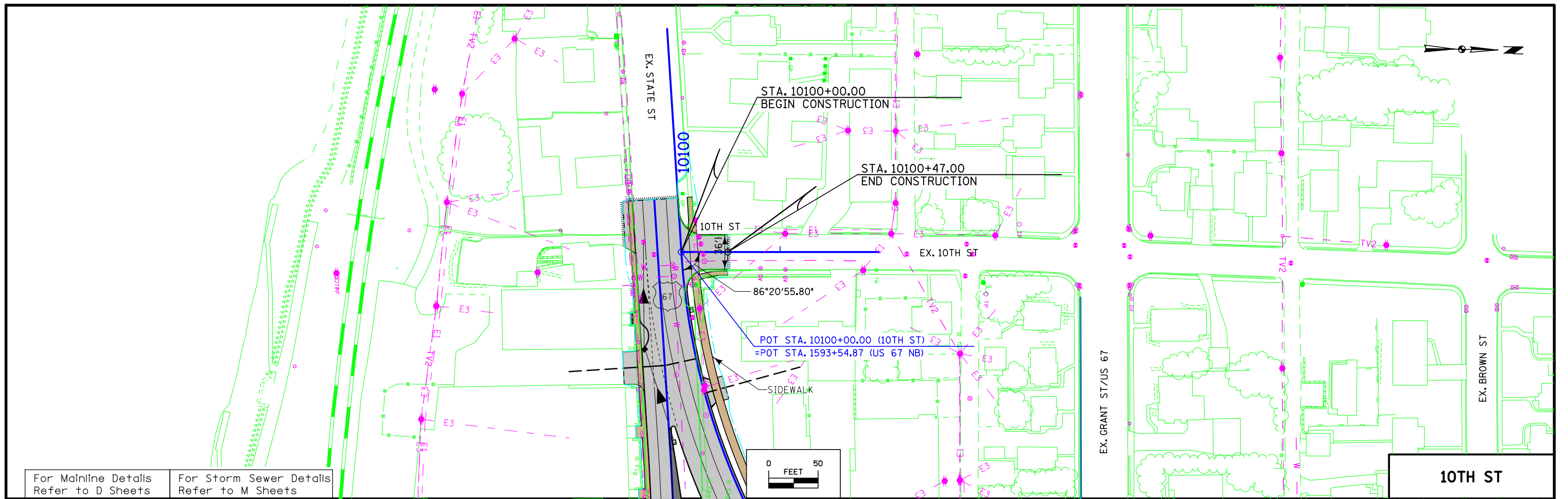


Lt

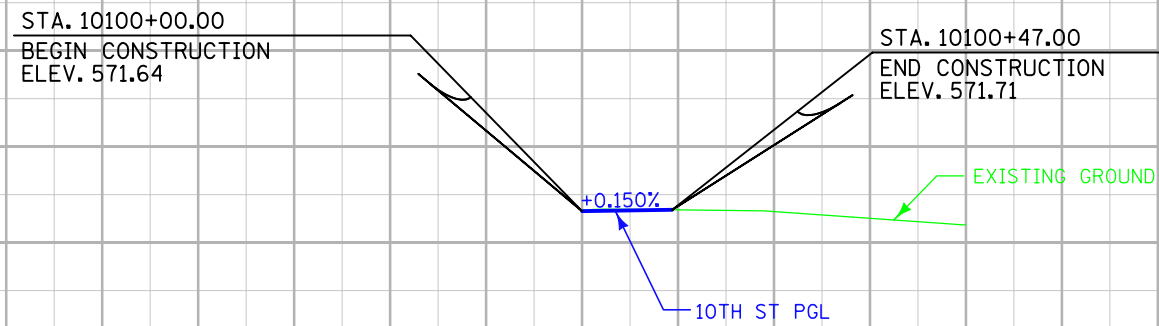
Rt

585.45  
 585.25  
 584.90  
 584.42  
 583.92  
 583.42  
 583.14  
 583.17

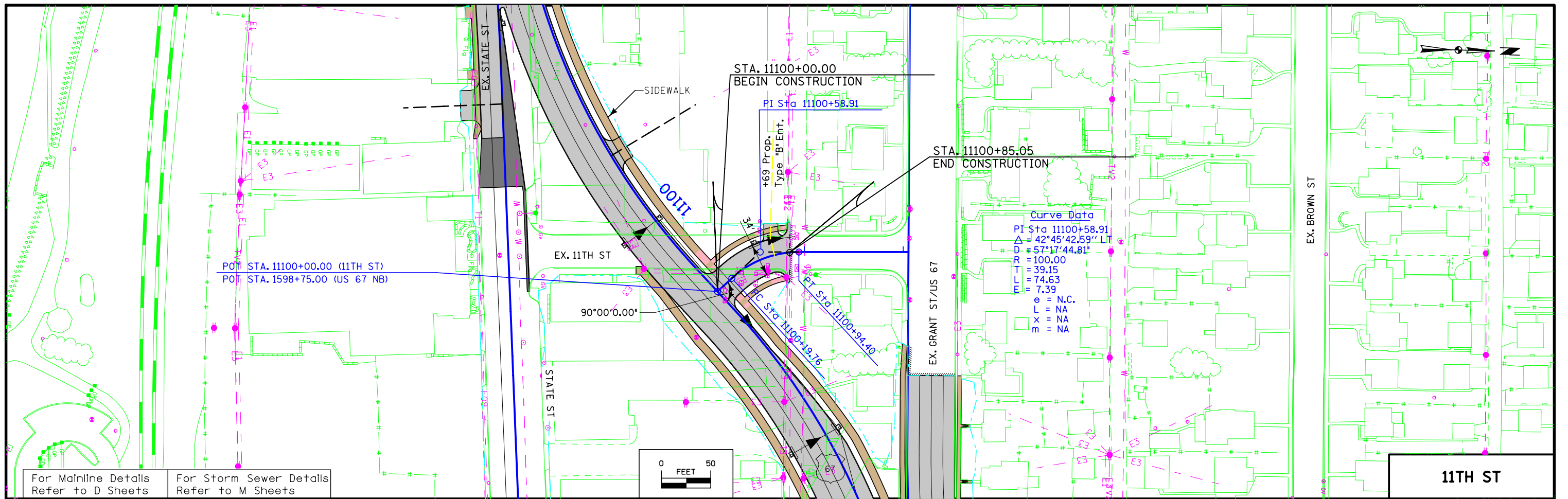




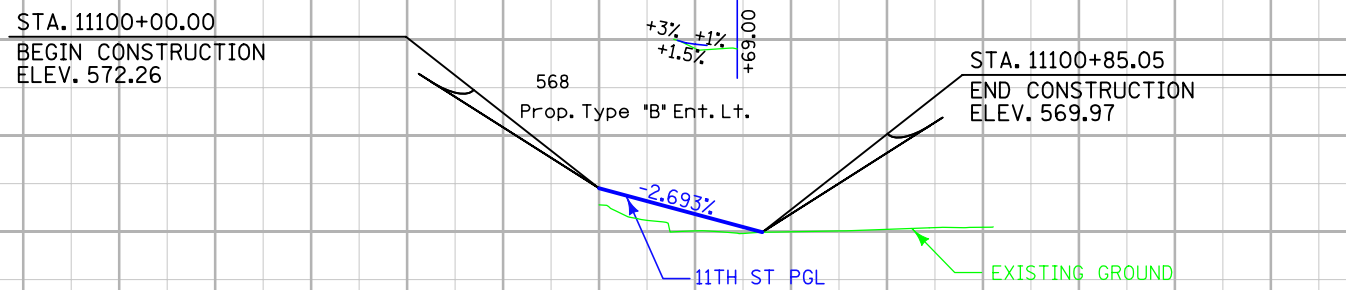
Fill+30% = 3 CY  
 Waste = 27 CY  
 30 CY  
 Cut = 30 CY



Lt  
 Rt  
 US 67 NB C&G  
 US 67C&G NB  
 571.64  
 571.68



Fill+30% = 60 CY      Cut = 21 CY  
 From 12th St = 39 CY  
 60 CY

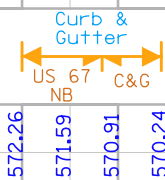


Lt

Rt

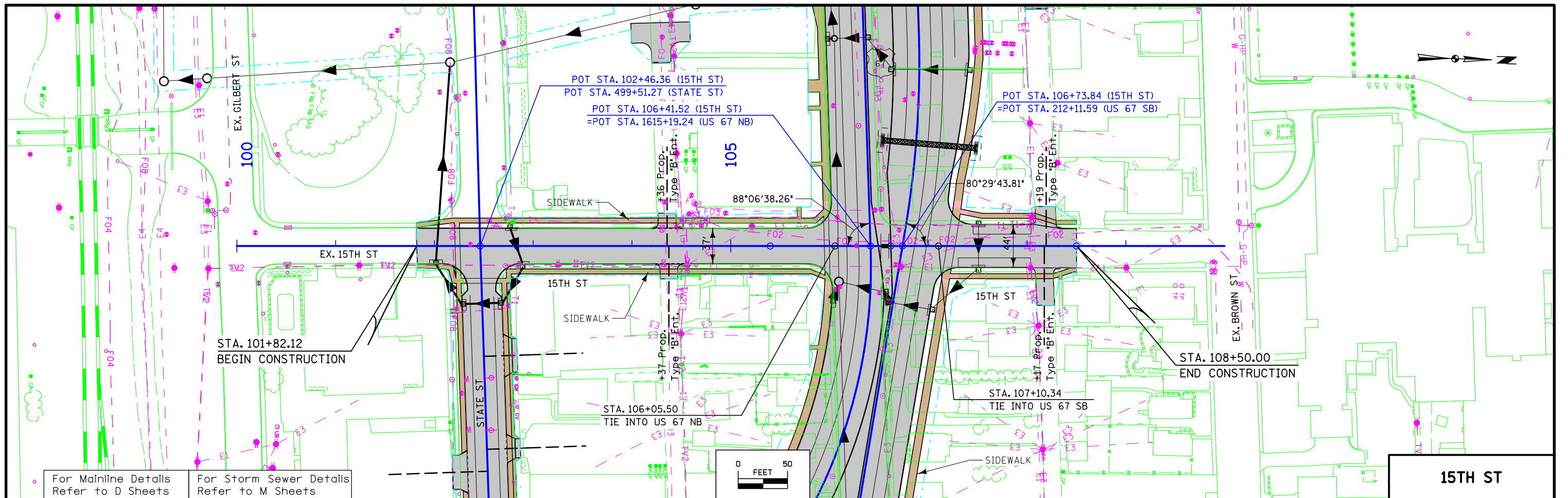
Lt

Rt

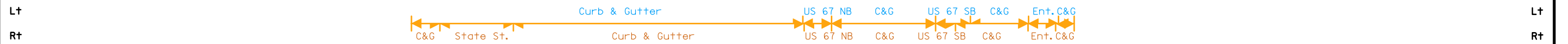
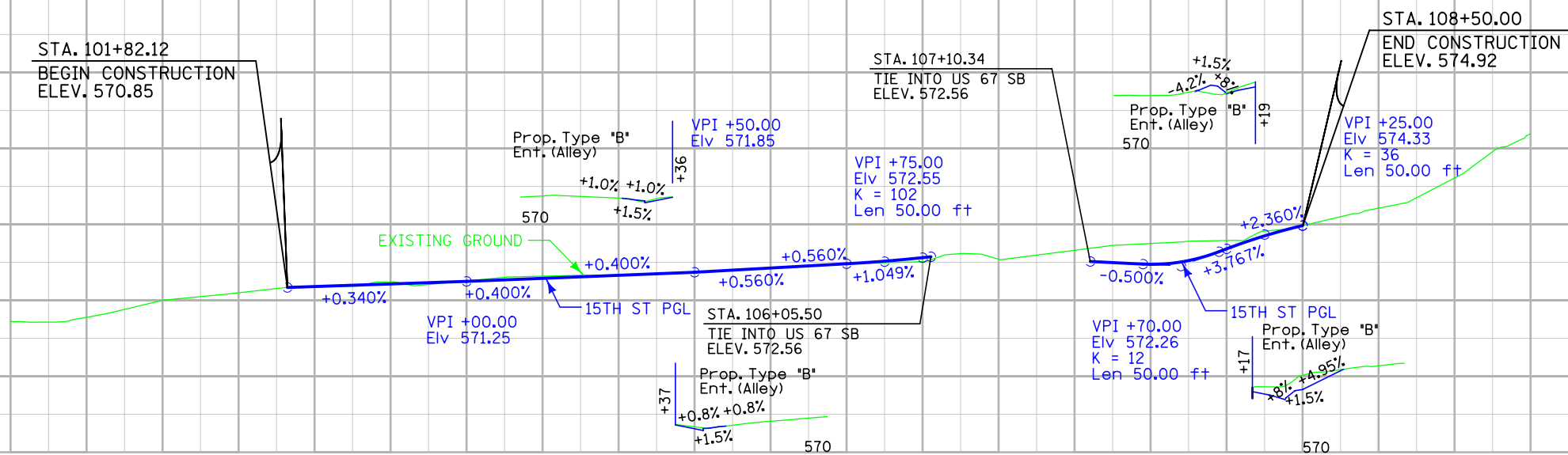




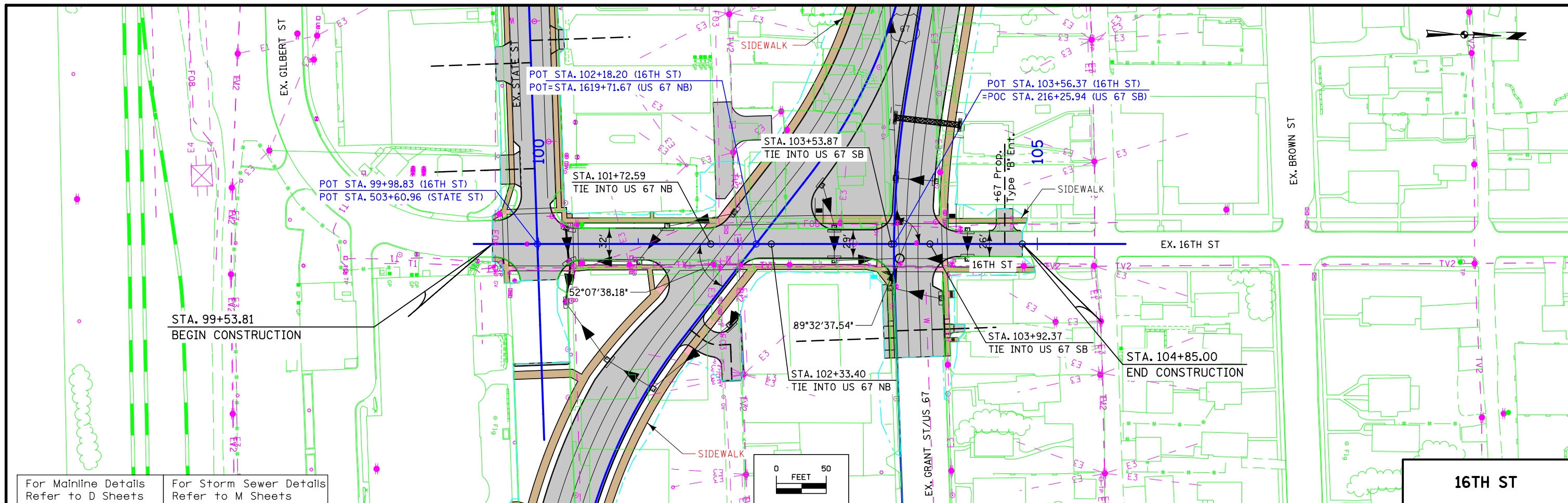




Fill+30% = 22 CY  
 Waste = 339 CY  
 361 CY  
 Cut = 361 CY



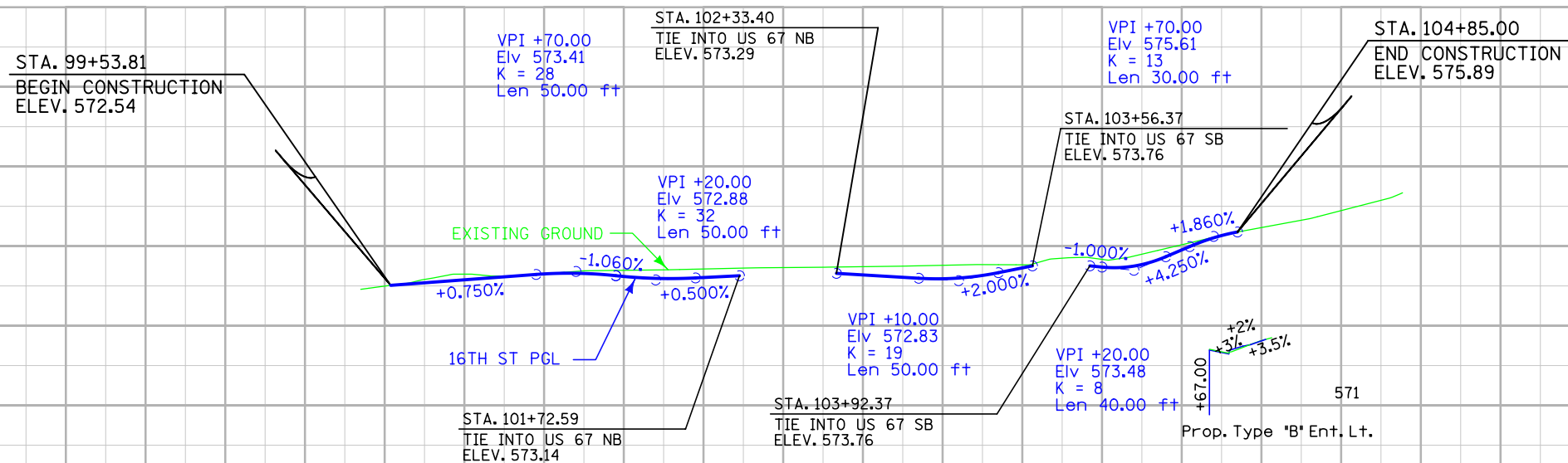
570.91	570.99	571.08	571.16	571.25	571.35	571.45	571.55	571.65	571.75	571.86	571.99	572.13	572.27	572.41	572.58	572.81	573.39	573.01	573.00	572.88	572.49	572.37	572.62	573.39	574.25	574.92
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For Mainline Details  
Refer to D Sheets

For Storm Sewer Details  
Refer to M Sheets

Fill+30% = 10 CY      Cut = 1,093 CY  
Waste = 1,083 CY  
1,093 CY

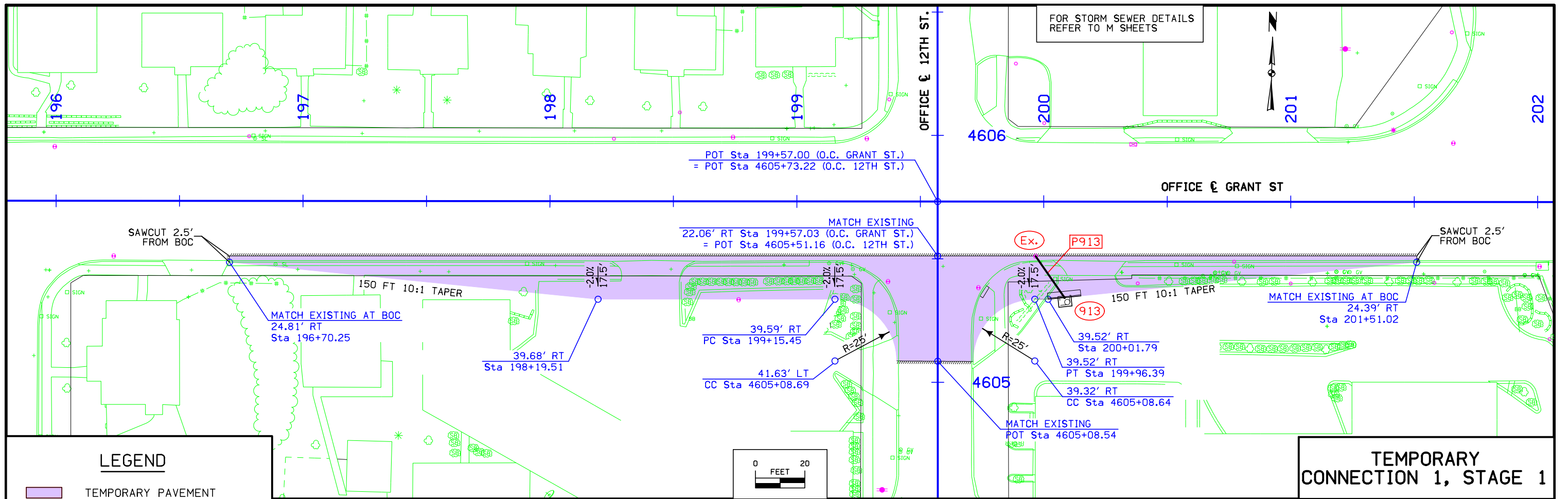


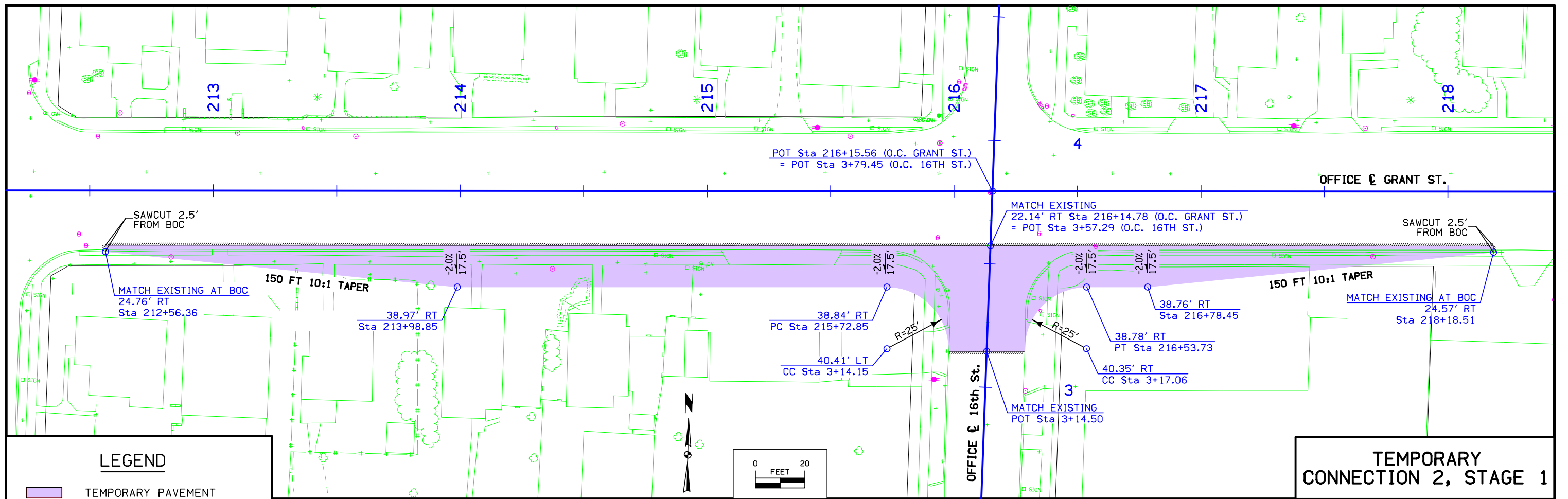
Lt      Lt  
Rt      Rt

Curb & Gutter    US 67 NB    C&G    US 67 SB    C&G Ent. C&G

C&G    US 67 NB    Curb & Gutter    US 67 SB    C&G

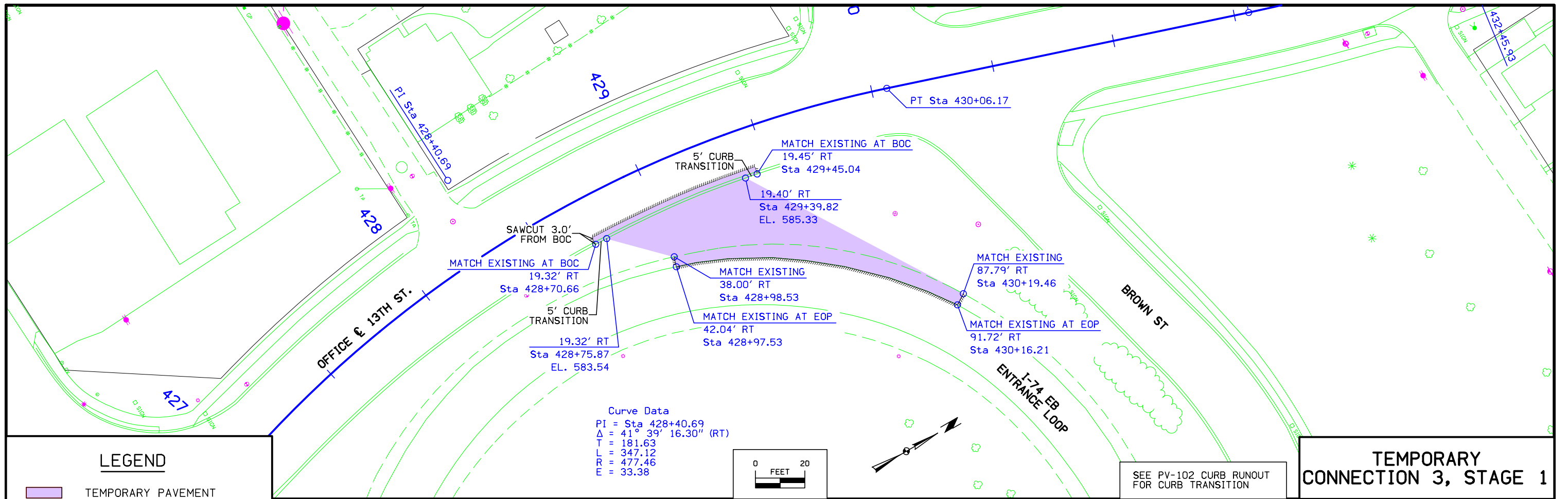
572.69	572.88	573.07	573.26	573.35	573.09	572.96	573.03	573.20	573.76	573.47	573.19	573.04	572.95	573.16	573.63	573.93	573.68	573.84	574.76	575.66
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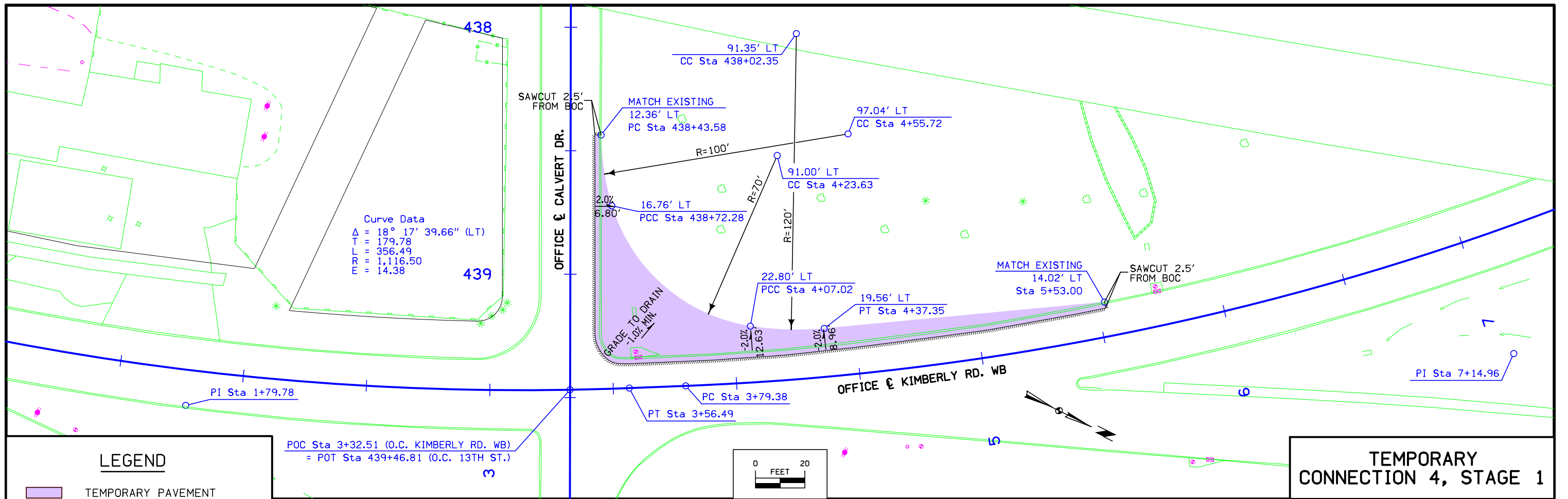






**TEMPORARY  
CONNECTION 2, STAGE 1**



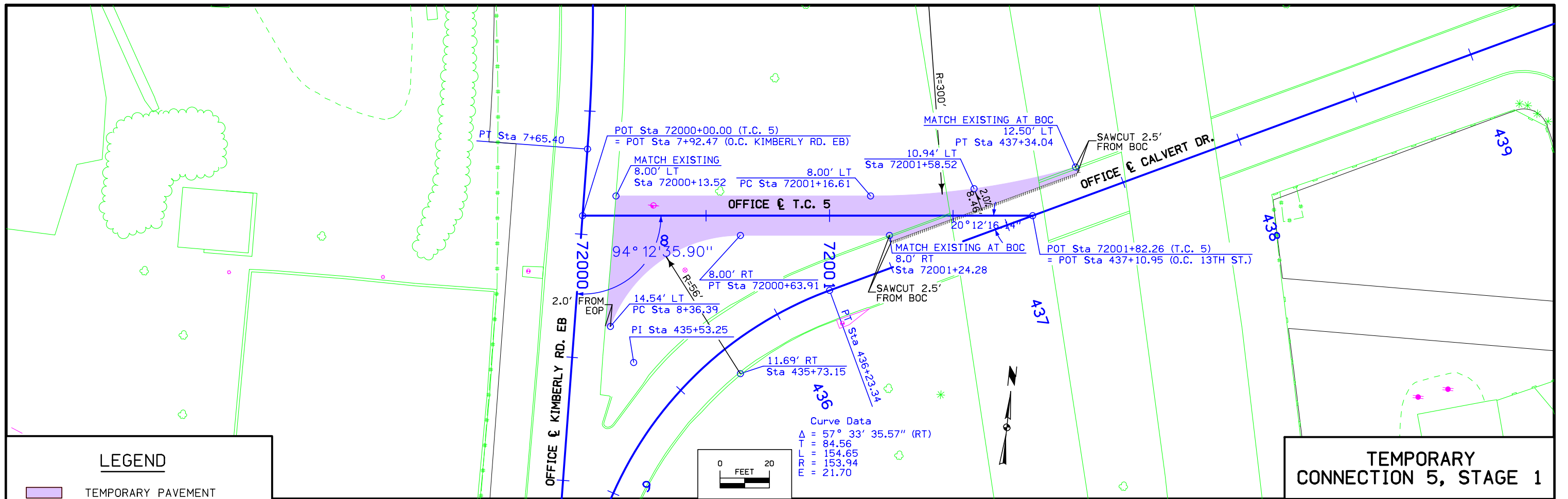






**LEGEND**

 TEMPORARY PAVEMENT  
 SAW CUT

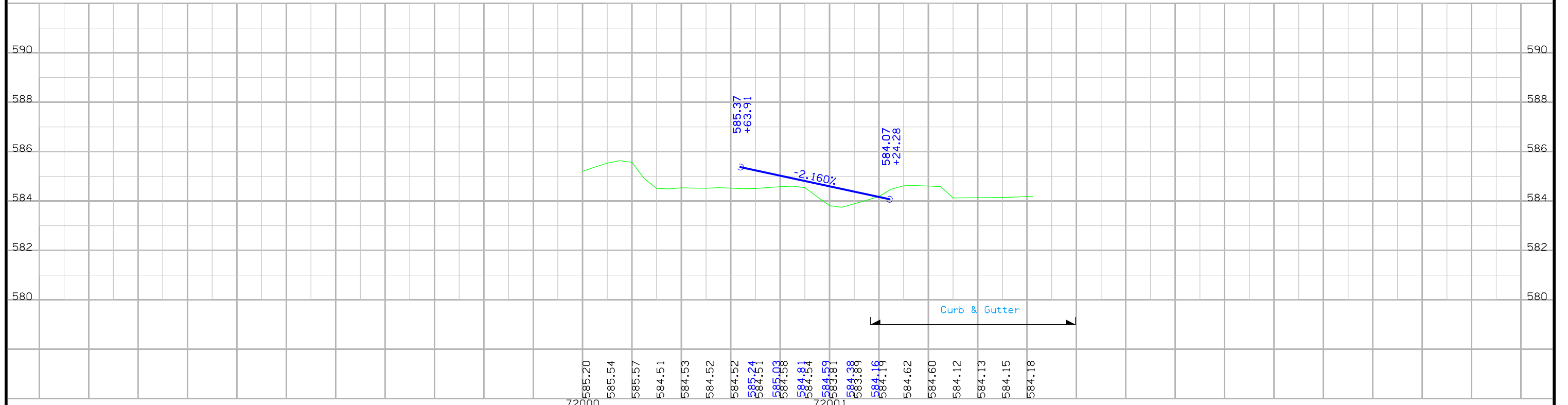
**TEMPORARY CONNECTION 4, STAGE 1**

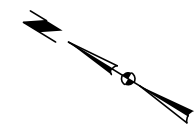


**LEGEND**

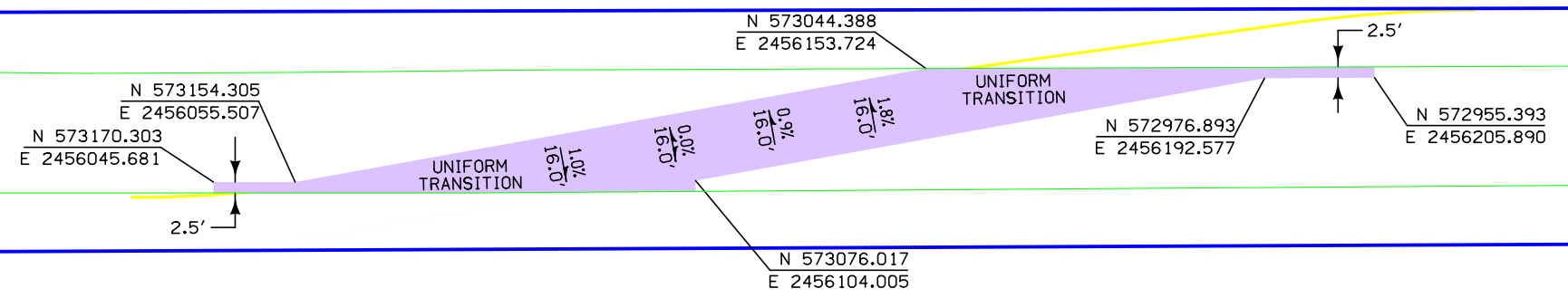
 TEMPORARY PAVEMENT  
 SAW CUT

**TEMPORARY CONNECTION 5, STAGE 1**

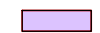
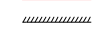


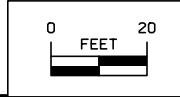


OFFICE  $\odot$  EX. KIMBERLY RD. WB

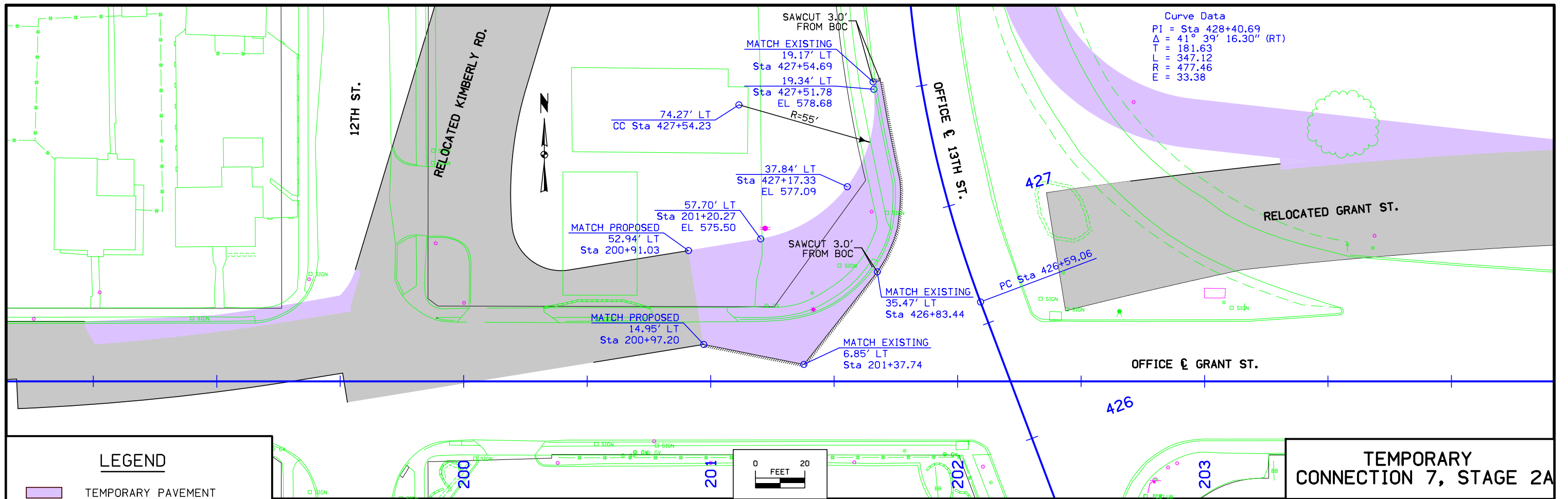


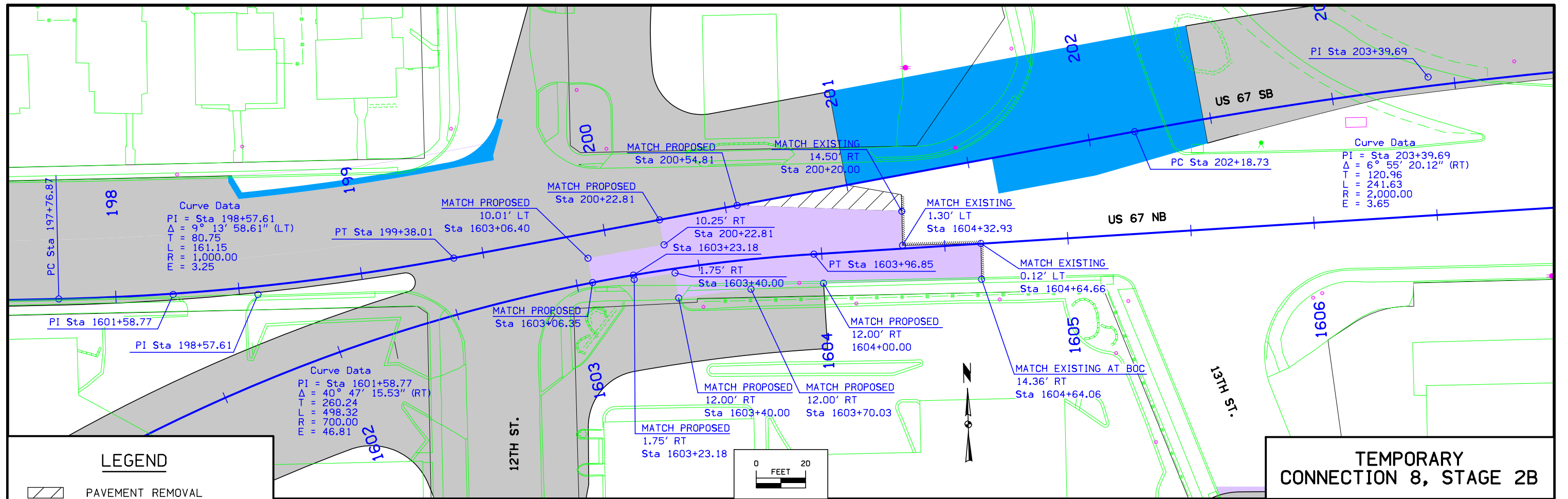
**LEGEND**

-  TEMPORARY PAVEMENT
-  SAW CUT



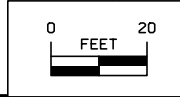
**TEMPORARY  
CONNECTION 6, STAGE 1**





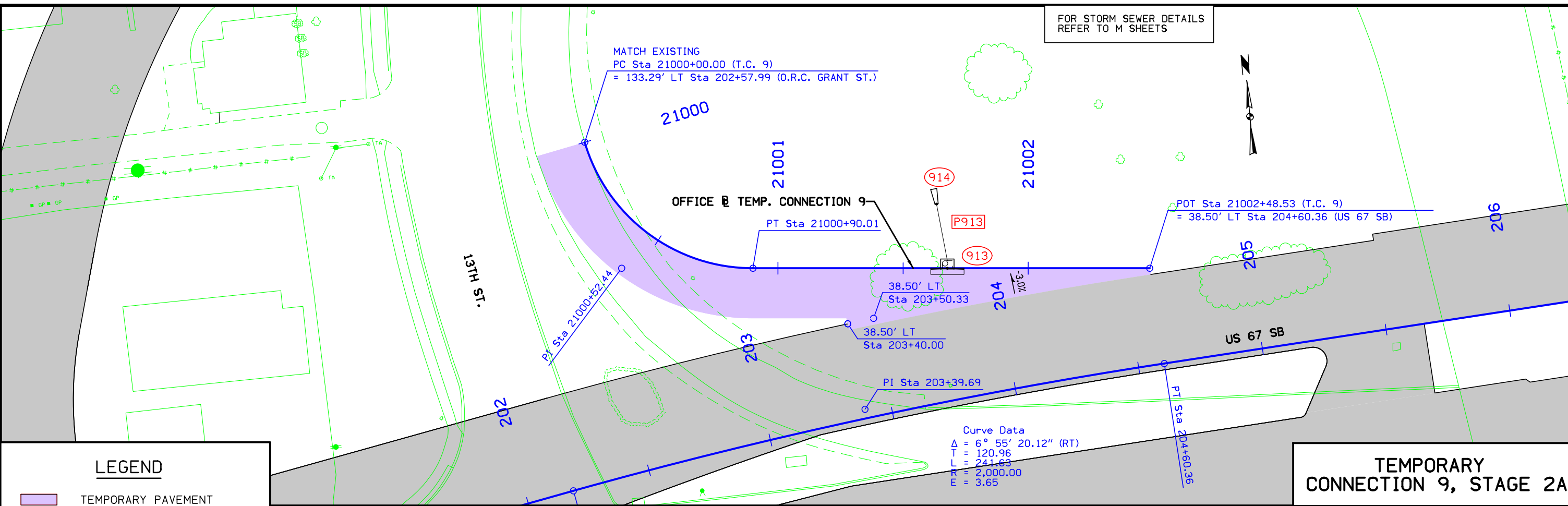
**LEGEND**

-  PAVEMENT REMOVAL
-  TEMPORARY PAVEMENT
-  SAW CUT



**TEMPORARY CONNECTION 8, STAGE 2B**

FOR STORM SEWER DETAILS  
REFER TO M SHEETS



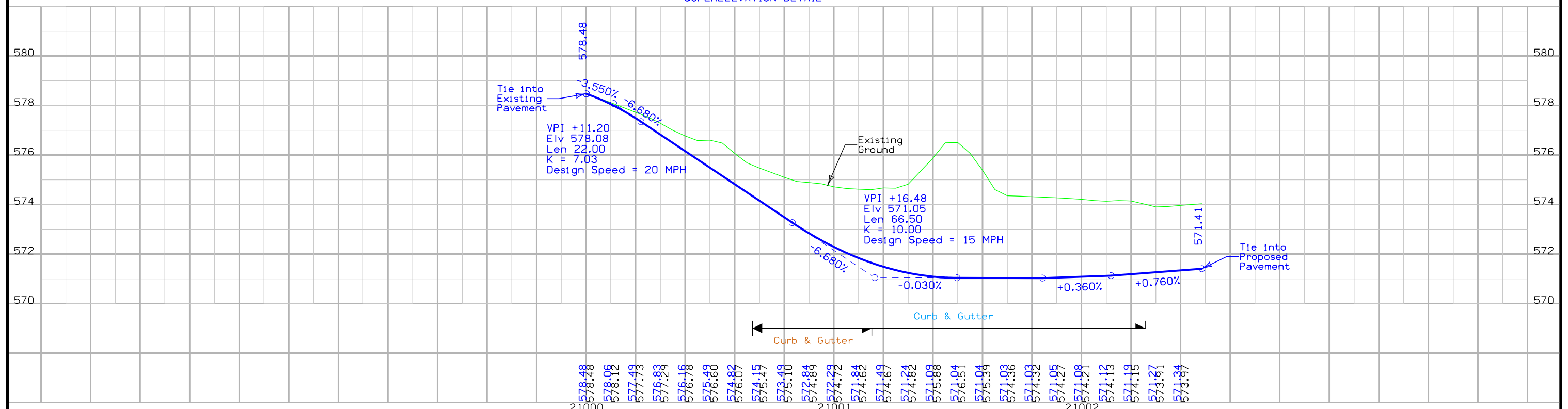
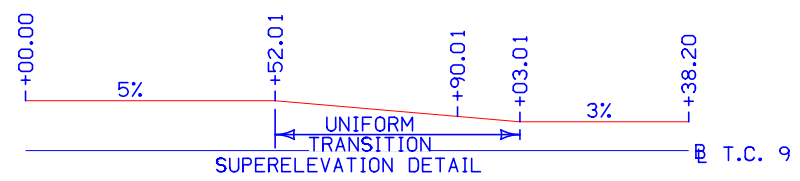
Curve Data  
 $\Delta = 6^\circ 55' 20.12''$  (RT)  
 $T = 120.96$   
 $L = 241.63$   
 $R = 2,000.00$   
 $E = 3.65$

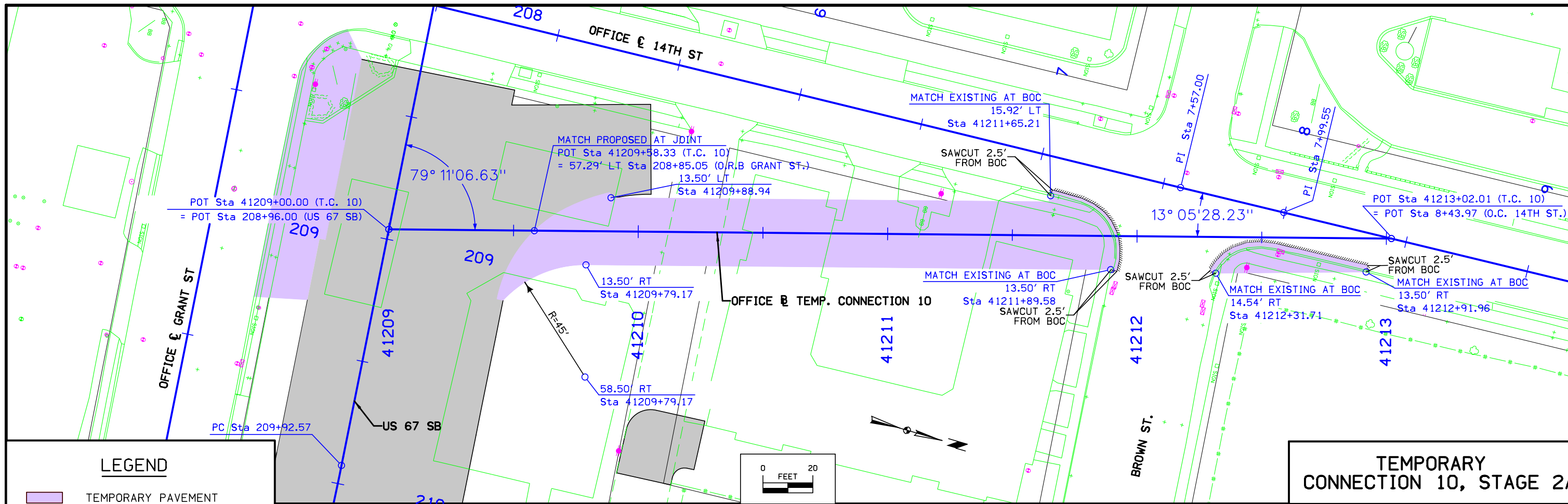
**LEGEND**

TEMPORARY PAVEMENT

SAW CUT

**TEMPORARY  
CONNECTION 9, STAGE 2A**





LEGEND

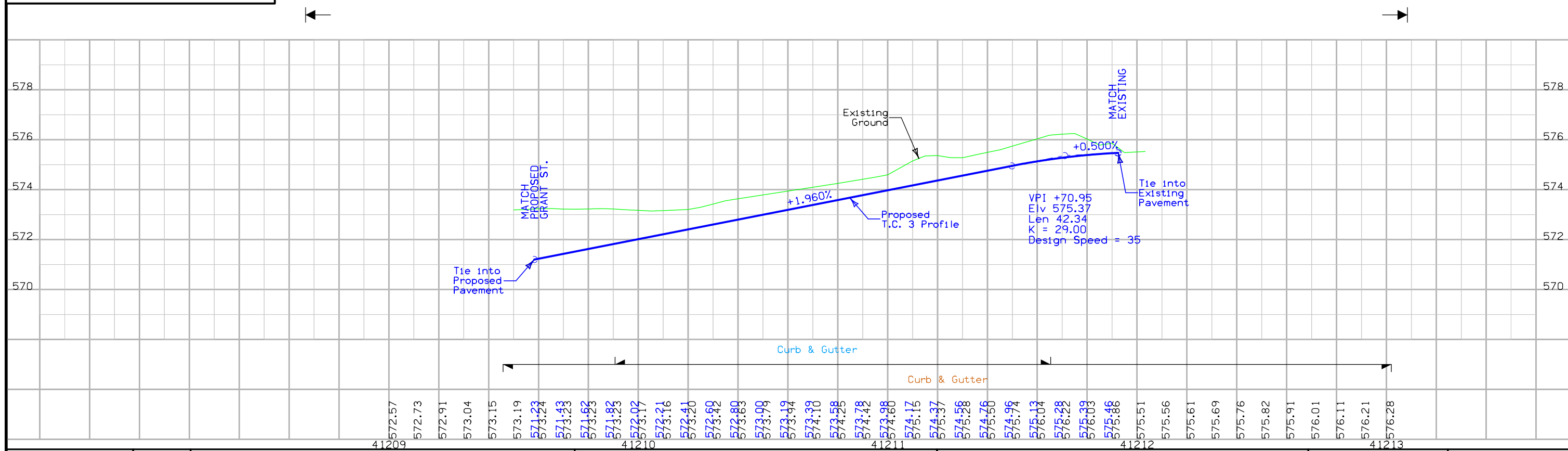
TEMPORARY PAVEMENT

SAW CUT

0 FEET 20

Cut/Fill = xx

TEMPORARY CONNECTION 10, STAGE 2A



ENGLISH	IOWA DOT	DESIGN TEAM	WHKS & CO.	SCOTT	COUNTY	PROJECT NUMBER	IM-074-1(226)5--13-82	SHEET NUMBER	F.10
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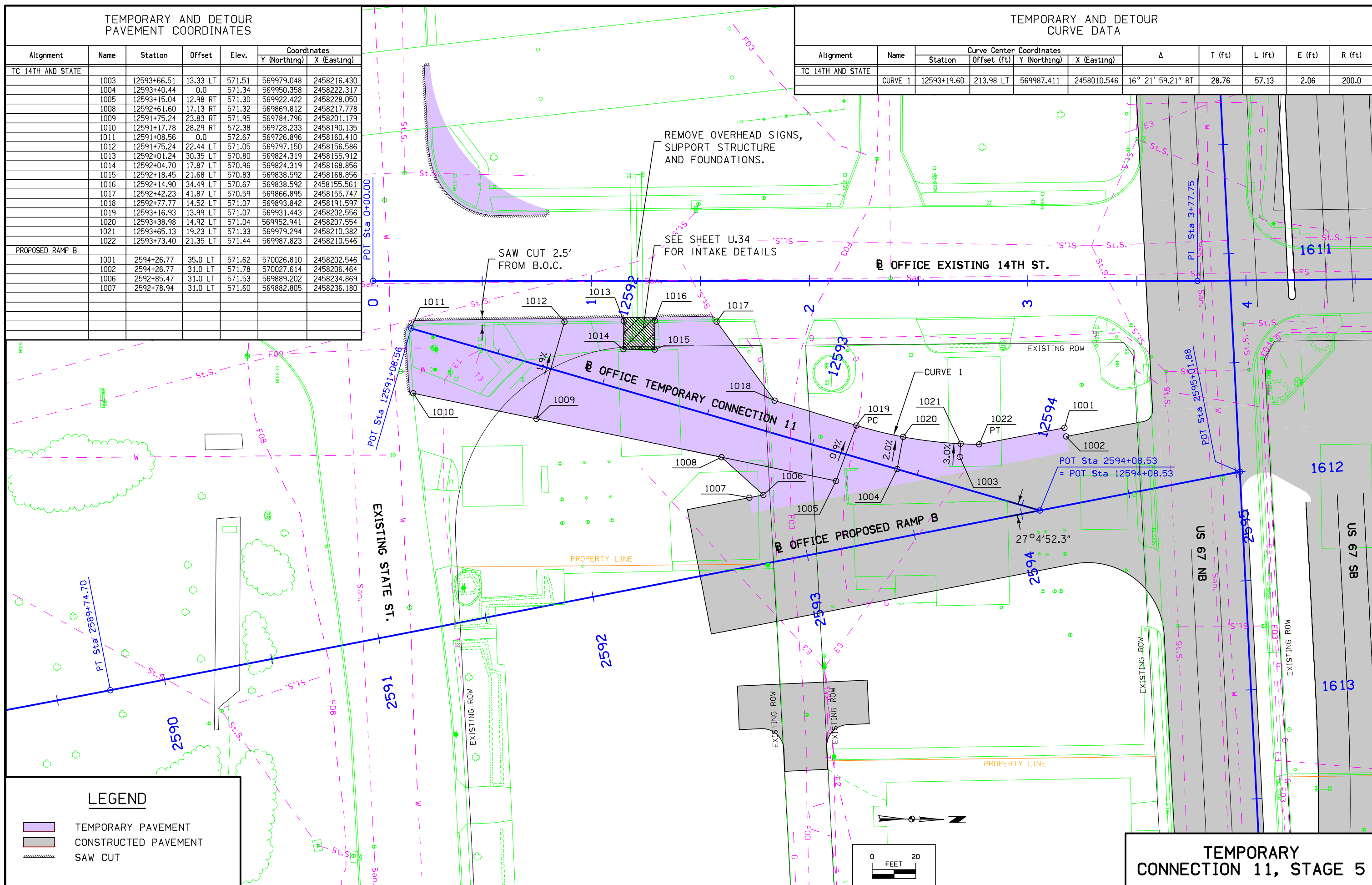


TEMPORARY AND DETOUR PAVEMENT COORDINATES

Alignment	Name	Station	Offset	Elev.	Coordinates	
					Y (Northing)	X (Easting)
TC 14TH AND STATE	1003	12593+66.51	13.33 LT	571.51	569979.048	2458216.430
	1004	12593+40.44	0.0	571.34	569950.358	2458222.317
	1005	12593+15.04	12.98 RT	571.30	569922.422	2458228.050
	1008	12592+61.60	17.13 RT	571.32	569869.812	2458217.778
	1009	12591+75.24	23.83 RT	571.95	569784.796	2458201.179
	1010	12591+17.78	28.29 RT	572.38	569728.233	2458190.135
	1011	12591+08.56	0.0	572.67	569726.896	2458160.410
	1012	12591+75.24	22.44 LT	571.05	569797.150	2458156.586
	1013	12592+01.24	30.35 LT	570.80	569824.319	2458155.912
	1014	12592+04.70	17.87 LT	570.96	569824.319	2458168.856
	1015	12592+18.45	21.68 LT	570.83	569838.592	2458168.856
	1016	12592+14.90	34.49 LT	570.67	569838.592	2458155.561
	1017	12592+42.23	41.87 LT	570.59	569866.895	2458155.747
1018	12592+77.77	14.52 LT	571.07	569893.842	2458191.597	
1019	12593+16.93	13.99 LT	571.07	569931.443	2458202.556	
1020	12593+38.98	14.92 LT	571.04	569952.941	2458207.554	
1021	12593+65.13	19.23 LT	571.33	569979.294	2458210.382	
1022	12593+73.40	21.35 LT	571.44	569987.823	2458210.546	
PROPOSED RAMP B	1001	2594+26.77	35.0 LT	571.62	570026.810	2458202.546
	1002	2594+26.77	31.0 LT	571.78	570027.614	2458206.464
	1006	2592+85.47	31.0 LT	571.53	569889.202	2458234.869
	1007	2592+78.94	31.0 LT	571.60	569882.805	2458236.180

TEMPORARY AND DETOUR CURVE DATA

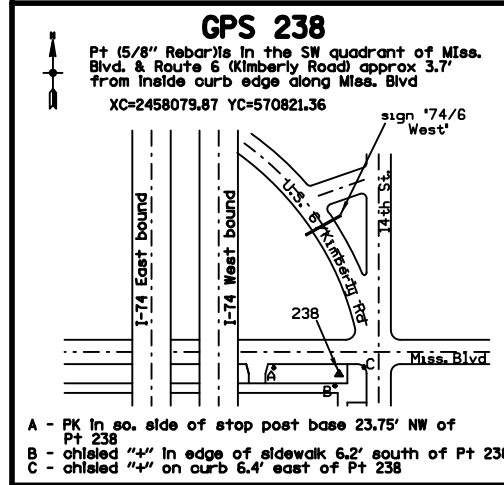
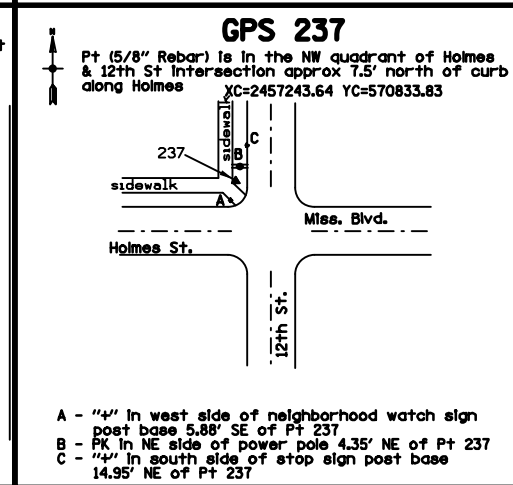
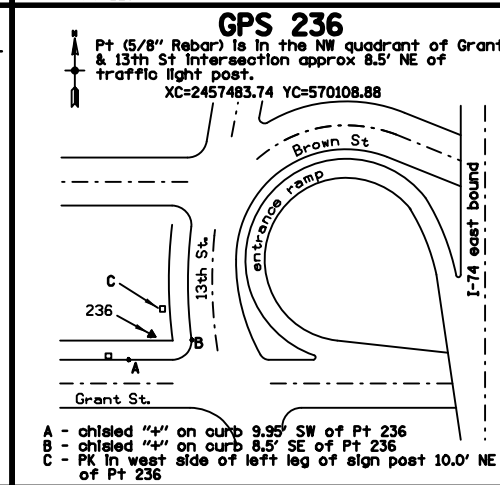
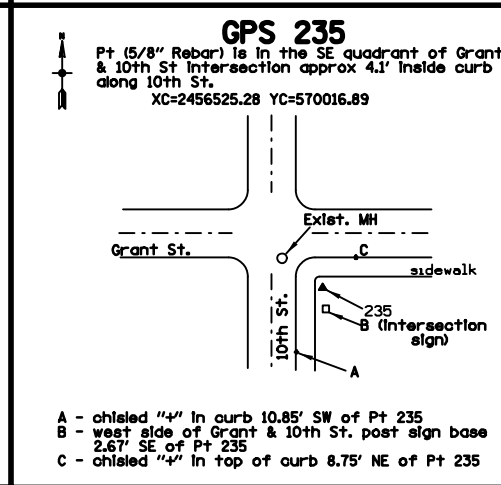
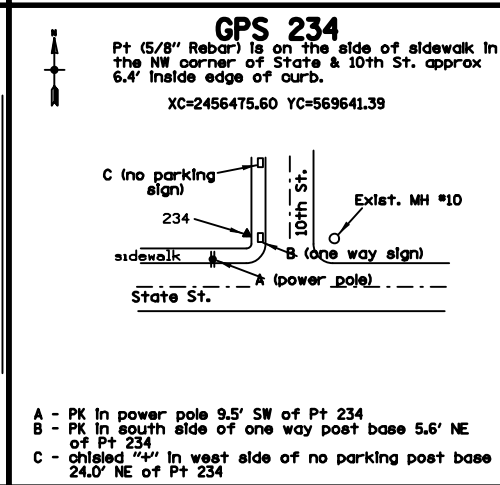
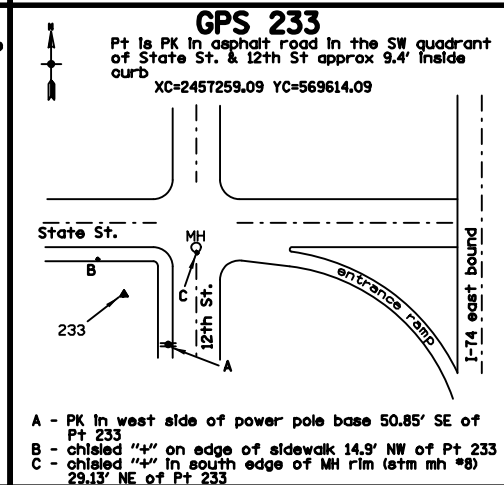
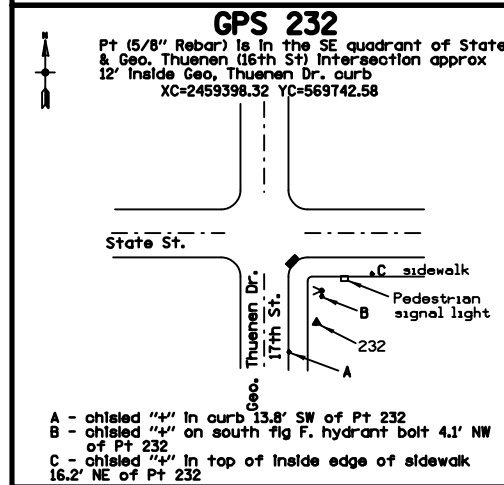
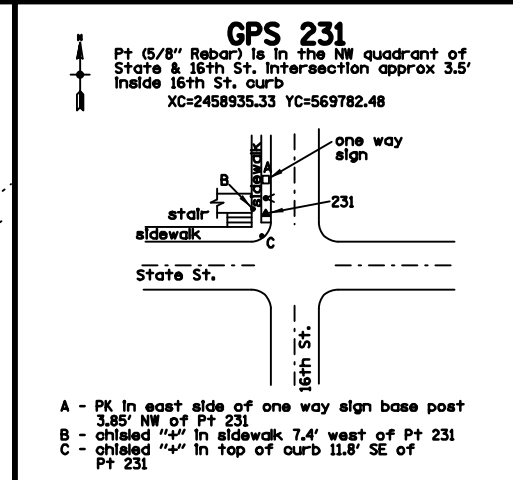
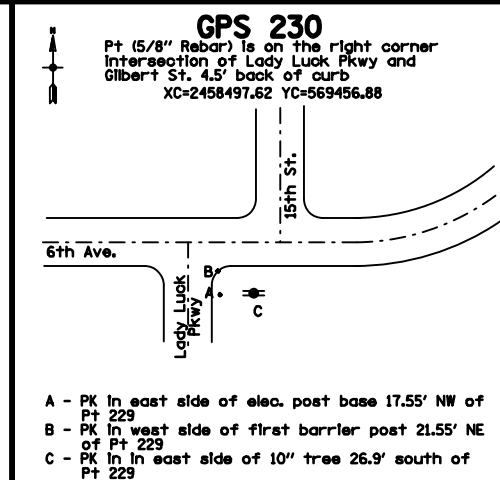
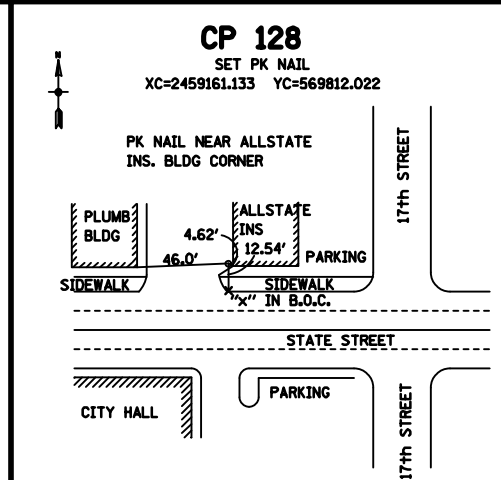
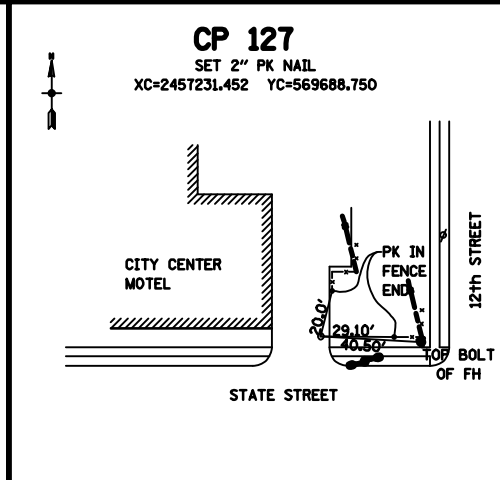
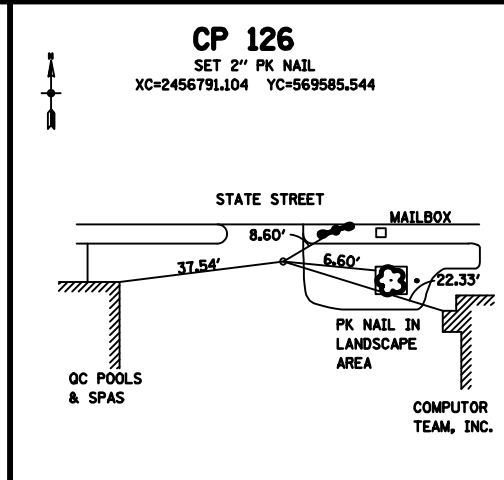
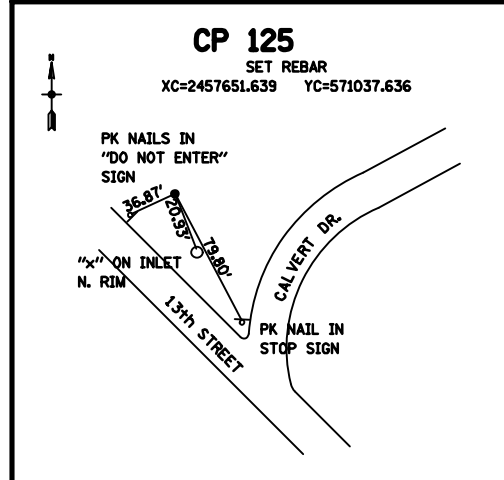
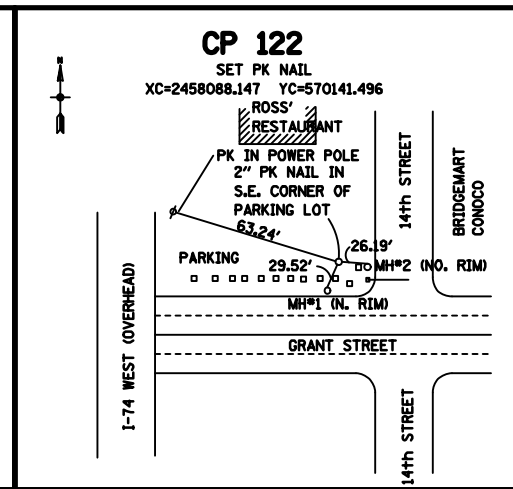
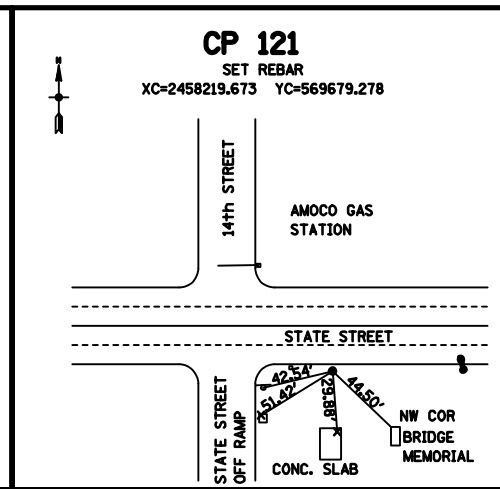
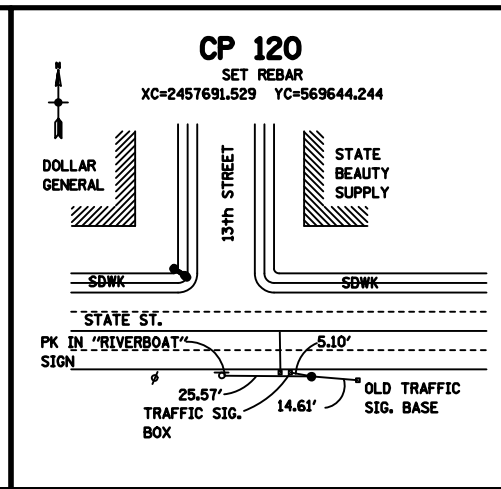
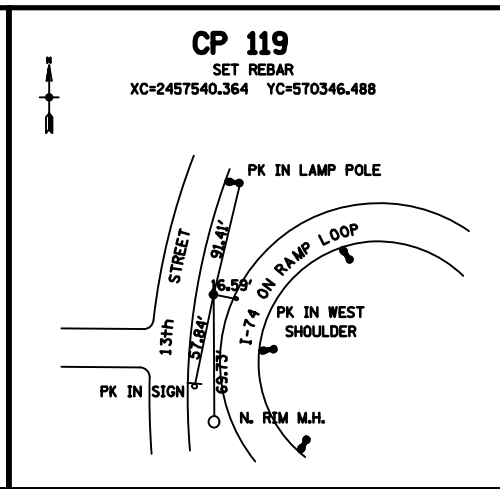
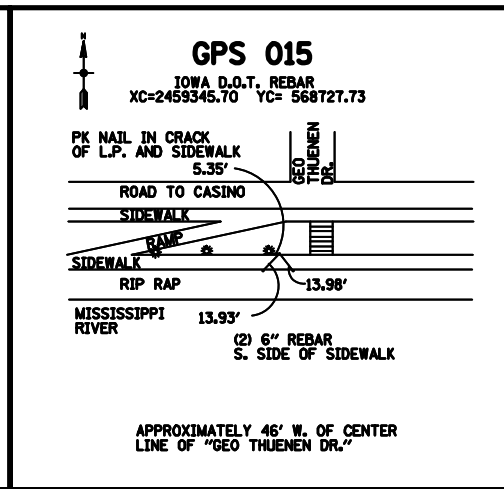
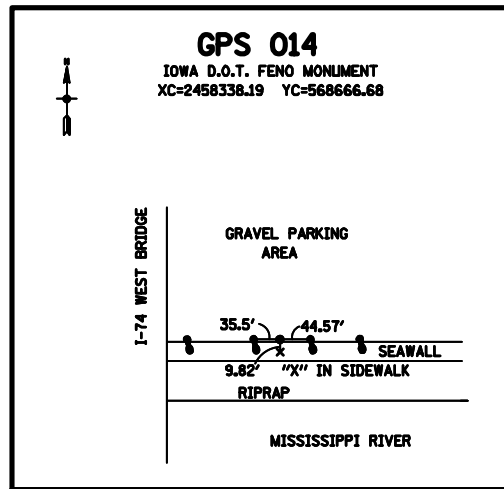
Alignment	Name	Curve Center Coordinates				Δ	T (ft)	L (ft)	E (ft)	R (ft)
		Station	Offset (ft)	Y (Northing)	X (Easting)					
TC 14TH AND STATE	CURVE 1	12593+19.60	213.98 LT	569987.411	2458010.546	16° 21' 59.21" RT	28.76	57.13	2.06	200.0



LEGEND

- TEMPORARY PAVEMENT
- CONSTRUCTED PAVEMENT
- SAW CUT

TEMPORARY CONNECTION 11, STAGE 5



#### DATUM INFORMATION

THE DATUM PLANE FOR THIS SURVEY IS RELATIVE TO N.A.V.D. 88 DATUM. IN IOWA BENCHES WERE RUN FROM NGS BENCHMARK "DAVENPORT" TO NGS BENCHMARK "F 70 RESET". IN ILLINOIS A BENCH CHECK WAS RUN FROM NGS BENCHMARK "W 52" TO NGS BENCHMARK "Z 52", THEN DATUM WAS CARRIED SOUTH TO THE END OF PROJECT.

ALL CONTROL POINT COORDINATES SHOWN ARE LOCAL PROJECT PLANE (GROUND) COORDINATES.  
CONVERSION EQUATION GRID TO GROUND: GROUND COORD = (STATE PLANE - HOLD POINT) 1/GRID FACTOR + HOLD POINT  
CONVERSION EQUATION GROUND TO GRID: GRID COORD = (GROUND - HOLD POINT) GRID FACTOR + HOLD POINT

HOLD POINT = G021	NORTH	EAST	GRID FACTOR	1/GRID FACTOR
	580322.54	2455353.37	0.999936506	1.000063498

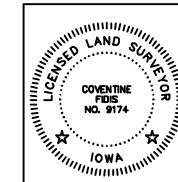
#### BENCH MARKS

##### IOWA BENCHMARKS:

No.	Sta.	Description	ELEVATION
No. 500	Sta.	CHISELED "X" IN BOLT E. SIDE CONC. STRUCTURE	575.797
No. 501	Sta.	CHISELED "X" IN S.W. FLANGE BOLT IN FHYD	568.923
No. 502	Sta.	CHISELED "X" IN FLANGE BOLT IN WORD "MUELLER" FHYD	575.247
No. 503	Sta.	CHISELED "X" IN FLANGE BOLT IN WORD "MUELLER" FHYD	580.282

##### ELEVATION

NORTHING	EASTING	STATION	OFFSET
N 568688.8797	E 2458216.7809	6781+18.92	161.19' LT.
N 569456.8395	E 2458524.4416	6787+97.99	311.34' RT.
N 569737.4808	E 2458179.1280	6791+49.11	38.00' RT.
N 570811.0288	E 2458144.2367	6801+93.58	255.44' RT.



I hereby certify that this plan was prepared by me or under my direct personal supervision and that I am a duly licensed Land Surveyor under the laws of the State of Iowa.

COVENTINE FIDIS \_\_\_\_\_ DATE: \_\_\_\_\_  
License number 09174

My license renewal date is December, 2014  
Pages or sheets covered by this seal: G.1

Scott County  
 IM-074-1(162)2--13-82  
 I-74 From South of Lincoln Rd to North of 67th Street  
 Pin 03-82-074-010-04  
 SAP 0337.4

## Survey Information

GENERAL INFORMATION FOR GPS PROJECT : Sap 0337 IMN-74-1(123)0-0e-82

STATE PLANE COORDINATE ZONE 1402 ( IOWA SOUTH LAMBERT )

STATE PLANE COORDINATES HELD AT POINT G021

AVERAGE PROJECT LATITUDE = 41 33 2.13112

RESULTING RADIUS = 6363530.832 (METERS)

MEAN PROJECT ELEVATION = 195.000 (METERS)

SEA LEVEL FACTOR = 0.999969358

AVERAGE PROJECT SCALE FACTOR = 0.999967147

COMBINED FACTOR (GRID) = 0.999936506

1 / GRID = 1.000063498

VERTICAL DATUM = NAVD 88 <-> HORIZONTAL DATUM = NAD 83 (1996)

Local Project Plane Coordinate Conversion Equation:

- a. Local Project Coord y = [(State Plane y - hold point y) 1/grid factor] + hold point y
- b. Local Project Coord x = [(State Plane x - hold point x) 1/grid factor] + hold point x

ALL COORDINATES CONVERTED TO ENGLISH UNITS

POINT	STATE PLANE COORD(Y)	STATE PLANE COORD(X)	POINT SCALE FACTOR	LOCAL PROJECT PLANE COORD(Y)	LOCAL PROJECT PLANE COORD(X)	Leveled Elevation
2	575101.117	2456247.982	0.99996155	575100.785	2456248.039	671.710
3	575305.685	2456670.142	0.99995733	575305.366	2456670.226	677.231
4	587761.953	2454890.061	0.99995075	587762.425	2454890.032	699.736
5	579548.598	2465909.733	0.99996337	579548.549	2465910.403	-----
6	592312.279	2465381.409	0.99996316	592313.040	2465382.046	-----
7	570852.050	2449647.188	0.99996322	570851.449	2449646.826	-----
10	566360.220	2467519.750	0.99996337	566359.333	2467520.523	-----
11	565294.190	2460631.000	0.99996316	565293.236	2460631.335	-----
12	564685.560	2459258.240	0.99996301	564684.567	2459258.488	-----
13	565480.250	2459044.560	0.99996322	565479.308	2459044.794	-----
14	568667.420	2458338.000	0.99996407	568666.680	2458338.190	575.724
15	568728.460	2459345.450	0.99996408	568727.724	2459345.703	576.878
16	571682.620	2457637.330	0.99996489	571682.071	2457637.475	625.186
17	572958.520	2457220.200	0.99996525	572958.052	2457220.319	658.505
Original Cp 18 has been destroyed						
19	576053.260	2456566.470	0.99996613	576052.989	2456566.547	659.900
20	577880.740	2455743.660	0.99996666	577880.585	2455743.685	626.865
21	580322.540	2455353.370	0.99996738	580322.540	2455353.370	677.568
22	582952.070	2455193.020	0.99996817	582952.237	2455193.010	688.928
Original Cp 23 has been destroyed						
24	590858.430	2455023.020	0.99997062	590859.099	2455022.999	727.114
25	593042.580	2454447.940	0.99997133	593043.388	2454447.883	748.901
26	595178.440	2454058.310	0.99997203	595179.383	2454058.228	732.221

### General Information

This survey is in English Units. The purpose of this survey was to re-observe and re-level all control. All Bench Marks on the Iowa side of the Mississippi River were re-leveled. All control on the Iowa side of the Mississippi River North of CP123 was re-observed. One new GPS calibration was generated using re-observed control, new supplemental project control, and re-leveled benchmarks along existing I-74. The purpose of this survey was to update old information, confirm the accuracy of old information then collect and verify information along I-74.

The IDOT GPS network control along I-74 was held fixed in the RTK calibration.  
 Calibration points held fixed Horizontally and Vertically 5,6,10,17,20,25,100,101,102,103,111,153,243,560,612,  
 Calibration points held fixed Vertically 7,21,106,108,109,110,116,157,250,252,254,255,556,563  
 Calibration points held fixed Horizontally 12, 13,15.

The project coordinates are projected to the ground.

### Vertical Datum

This survey is relative to NAVD 88 vertical datum. A new level loop originated and terminated on the original I-74 mark BM 503. The new level loop ran north to I-80 then returned to BM 503. Loops were also run on Lincoln, Middle road, Spruce Hills & 53rd Street.

Benchmarks were re-established on the new Lincoln Rd overpass bridge. Vertical differences between the 2003 CH2MHill marks and the newly established elevations are as follows, At the Middle Road intersection a vertical difference of 0.062 was found (SEE BM509 equation below). At the Spruce Hills intersection a vertical difference of 0.078 was found (SEE BM514 equation below). All elevations along 53rd street were originally leveled from BM 522, BM 522 has been destroyed in recent construction. Just West of the 53rd Street intersection a vertical difference of 0.066 was found (SEE BM560 equation below).

### Vertical Equations;

BM # 503 this survey =BM # 503	Elev. = 580.282 (NAVD 88 English) 2011 Datum BM Elev. = 580.282 (NAVD 88 English) I-74 Consultant survey
BM # 509 this survey =BM # 509	Elev. = 677.516 (NAVD 88 English) 2011 Re-Leveled Elev. = 677.578 (NAVD 88 English) I-74 Consultant survey
BM # 514 this survey =BM # 514 =BM # 514	Elev. = 680.944 (NAVD 88 English) 2011 Re-Leveled Elev. = 681.022 (NAVD 88 English) I-74 Consultant survey Elev. = 681.022 (NAVD 88 English) 2009 WHKS Kimberly Road Survey
BM # 560 this survey =BM # 560	Elev. = 686.934 (NAVD 88 English) 2011 Re-Leveled Elev. = 687.000 (NAVD 88 English) 2008 WHKS 53rd St Survey
BM # 564 this survey =BM # 564	Elev. = 710.632 (NAVD 88 English) 2011 Re-Leveled Elev. = 710.688 (NAVD 88 English) 2008 WHKS 53rd St Survey
BM # 556 this survey =BM # 556	Elev. = 707.691 (NAVD 88 English) 2011 Re-Leveled Elev. = 707.745 (NAVD 88 English) 2008 WHKS 53rd St Survey
BM # 573 this survey =BM # 317 Scott Co Plan (I-74-1(5)2**01--82	Elev. = 624.321 (NAVD 88 English) 2011 Re-Leveled Elev. = 625.21 Datum Unknown
BM # 574 this survey =BM 320"A" Scott Co Plan (I-74-1(5)2--01--82	Elev. = 649.518 (NAVD 88 English) 2011 Re-Leveled Elev. = 649.36 Datum Unknown
BM # 575 this survey =BM 423"A" Scott Co Plan (I-74-1(5)2--01--82	Elev. = 653.550 (NAVD 88 English) 2011 Re-Leveled Elev. = 654.42 Datum Unknown
BM # 576 this survey =BM 328"A" Scott Co Plan (I-74-1(6)3**01-82	Elev. = 646.600 (NAVD 88 English) 2011 Re-Leveled Elev. = 647.61 Datum Unknown

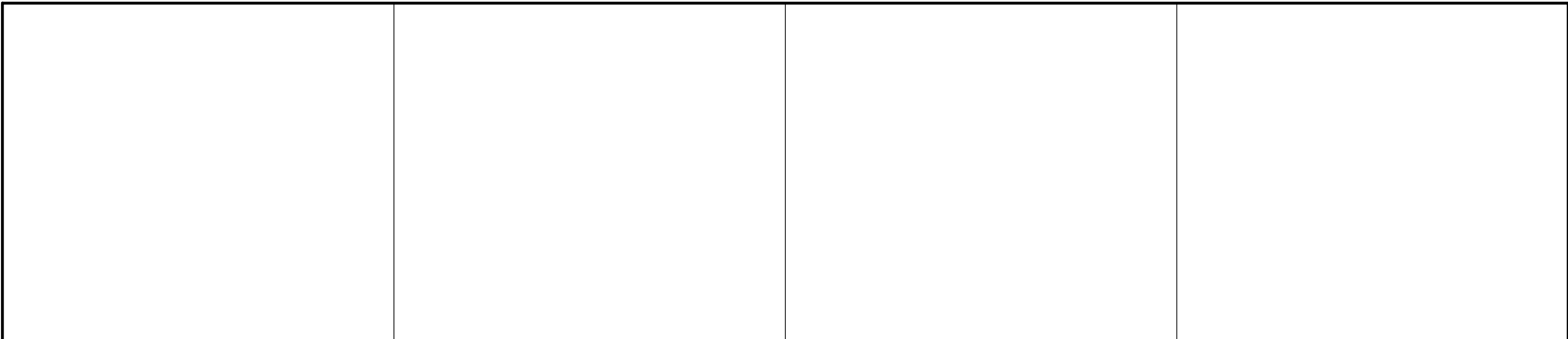
### Alignments

Alignments were provided from previous surveys.

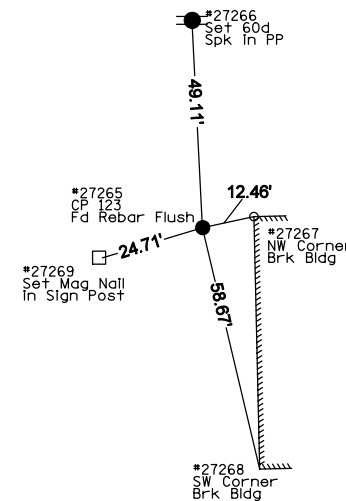
### Horizontal Datum & Project Coordinate Transformation

The IDOT observed a GPS Static control network for the I-74 Mississippi River Bridge.

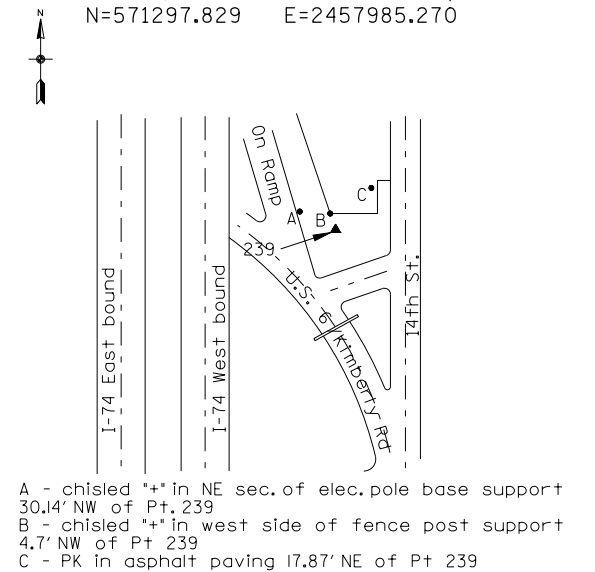
BENCHMARKS		ELEVATION							
No. 500 Sta.	6781+18.803	161.128 Lt. Y: 568688.781 X:2458216.869	Fd "X" on bolt East side conc structure =CH2MHill BM500 EL=575.797-----	575.796	No. 570 Sta.	6975+27.062	133.539 Rt. Y: 587684.130 X:2455161.067	Fd IDOT Brass Button on SE BRG/SWK Barrier Rail (New BM on new Brg Addition)-----	708.439
No. 501 Sta.	6787+97.906	311.471 Rt. Y: 569456.791 X:2458524.591	Fd CHISELED "X" IN S.W. FLANGE BOLT IN Fire Hyd (FD Good Cond) =CH2MHill BM501 EL 568.923-----	568.926	No. 571 Sta.	6848+78.456	59.872 Lt. Y: 575251.235 X:2456614.586	BM Established in 2011 DOT BM Button on SW Barrier Rail of SBL Brg over Middle Rd-----	683.959
No. 502 Sta.	6791+49.022	38.344 Rt. Y: 569737.468 X:2458179.480	FD CHISELED "X" IN FLANGE BOLT IN WORD "MUELLER" FHYD (Leaning) =CH2MHill BM502 EL 575.247-----	575.247	No. 572 Sta.	6865+94.623	31.589 Lt. Y: 576920.853 X:2456245.148	BM Established in 2011 DOT BM Button on SW Barrier Rail of SBL Brg over Duck Creek-----	638.686
No. 503 Sta.	6801+93.632	255.412 Rt. Y: 570811.071 X:2458144.197	FD CHISELED "X" IN FLANGE BOLT IN WORD "MUELLER" FHYD (Leaning) =CH2MHill BM503 EL 580.282-----	580.282	No. 573 Sta.	6875+20.239	245.376 Rt. Y: 577875.339 X:2456080.499	BM Established in 2011 (X) on SW Bolt fire hyd Jct Hawthorne Dr & Cypress Drive =Plan BM 317 Plan Elev=625.21 (I-74-1(5)2**01--82 Fd on sheet 4 of 8-----	624.321
No. 504 Sta.	6809+00.595	189.731 Rt. Y: 571477.720 X:2457880.301	Fd "X" IN West side conc Luminaire Base =CH2MHill BM504 EL 602.945-----	602.920	No. 574 Sta.	6893+06.470	271.761 Rt. Y: 579499.349 X:2455688.689	BM Established in 2011 (x) on NW bolt of Fire Hyd Plan Elev =649.36 =Plan BM 320"A" (I-74-1(5)2--01--82 Fd on sheet 47A-----	649.518
No. 505 Sta.	6810+90.128	75.269 Rt. Y: 571625.939 X:2457715.814	Fd Square on NE Barrier Rail of NBL I-74 Bridge over Kimberly Rd =CH2MHill BM505 EL 621.93-----	621.906	No. 575 Sta.	6904+13.118	741.405 Lt. Y: 580507.579 X:2454577.542	BM Established in 2011 IHC BM on inlet Hdwl 4x4 RCB Plan Elev =654.42 =Plan BM 423"A" (I-74-1(5)2--01--82 Fd on sheet 47A-----	653.550
No. 506 Sta.	6822+67.473	84.778 Rt. Y: 572755.496 X:2457383.651	Fd "X" on West side Cir conc Lum Pole Base =CH2MHill BM506 EL 655.749-----	655.710	No. 576 Sta.	6955+96.514	74.251 Rt. Y: 585752.751 X:2455143.593	BM Established in 2011 =Plan BM 328"A" Plan calls for IHC This Survey Fd "X" on Inlet end 6x6 RCB Plan Plan Elev=647.61 (I-74-1(6)3**01-82 Fd on sheet 30-----	646.600
No. 507 Sta.	6834+05.076	68.762 Rt. Y: 573843.513 X:2457063.995	Not Found at given coordinates presumed destroyed CH2M Hill BM-----	668.133	No. 577 Sta.	6833+94.859	132.343 Rt. Y: 573849.206 X:2457128.132	BM Established in 2011 Fd PK nail in SE Barrier Rail of New bridge on Lincoln-----	688.859
No. 508 Sta.	6842+55.549	62.831 Rt. Y: 574668.276 X:2456866.038	Fd "X" on East side conc Lum Pole Base =CH2MHill BM508 EL 671.518-----	671.448	No. 578 Sta.	6835+04.164	126.319 Lt. Y: 573892.486 X:2456850.685	BM Established in 2011 Fd PK nail in NW Barrier Rail of New bridge on Lincoln-----	689.703
No. 509 Sta.	6851+85.690	59.105 Rt. Y: 575576.663 X:2456666.015	Fd IDOT BM Button on NE Barrier Rail of NBL Brg over Middle Rd =CH2MHill BM509 EL 677.578-----	677.516	No. 600 Sta.	6904+02.998	796.519 Lt. Y: 580492.406 X:2454523.600	Fd x on NE Bolt of lum pole base (Good Cond) equals SAP 0576 BM601 ZC=657.159--	657.067
No. 510 Sta.	6861+43.895	80.14 Rt. Y: 576518.994 X:2456482.513	Fd "X" on E Side conc Luminaire Base at AB Sta 2170+00+/- =CH2MHill BM510 EL 645.087-----	645.044	MISCELLANEOUS LOCATIONS				
No. 511 Sta.	6868+67.531	100.64 Rt. Y: 577225.907 X:2456261.874	Fd DOT BM Button on NE Barrier Rail of NBL Brg over Duck Creek =CH2MHill BM511 EL 638.647-----	638.614	BENCHMARKS along Kimberly Rd				
No. 512 Sta.	6882+39.229	73.974 Rt. Y: 578444.501 X:2455659.981	Fd "X" on Conc Base of W post of sign "exit 2" =CH2MHill BM512 EL 631.703-----	631.676	ELEVATION				
No. 513 Sta.	6892+34.511	84.403 Rt. Y: 579410.375 X:2455508.787	Fd "X" on top east end 48" RCP =CH2MHill BM513 EL 649.572 125' +/- N. of MM 3.1-----	649.537	No. 566 Sta.	7293+72.752	85.356 Lt. Y: 580808.174 X:2452917.397	IHC BM on Inlet Hdwl of Twin 14 x 14 RCB equals SAP 0576 BM505 ZC=632.189-----	632.115
No. 514 Sta.	6901+57.825	60.026 Rt. Y: 580327.480 X:2455399.144	Fd DOT BM Button on SE Barrier Rail of NBL Brg over Spruce Hills Rd =CH2MHill BM514 EL 781.022-----	680.944	No. 567 Sta.	7294+25.010	66.073 Rt. Y: 580650.413 X:2452889.591	"X" on Outlet Hdwl 30 x 15 Conc Arch equals SAP 0576 BM506 equals ZC=634.892--	634.811
No. 515 Sta.	6912+23.757	330.731 Rt. Y: 581413.875 X:2455570.133	Fd CHISELED "X" IN FLANGE BOLT IN WORD "MUELLER" =CH2MHill BM515 EL 683.991-----	683.924	No. 601 Sta.	7296+53.544	145.968 Lt. Y: 580724.974 X:2453192.177	Fd x on East bolt of lum base (Good Cond) equals SAP 0576 BM601 ZC=637.567--	637.475
No. 516 Sta.	6923+17.968	89.277 Lt. Y: 582468.666 X:2455061.538	Fd "X" on E side conc Lum Pole Base =CH2MHill BM516 EL 686.241-----	686.161	BENCHMARKS along Spruce Hills				
No. 518 Sta.	6934+40.127	101.985 Rt. Y: 583597.470 X:2455218.010	Fd "T" IN HDWL R.C.B. 2' from WHKS BM (#16589 Fd IHC inlet hdwl 4x5x175.9RCB) Not Found at given coordinates presumed destroyed CH2M Hill BM-----	667.743	No. 600 Sta.	7310+89.997	68.872 Lt. Y: 580492.406 X:2454523.600	Fd x on NE Bolt of lum pole base (Good Cond) equals SAP 0576 BM601 ZC=657.159--	657.067
No. 519 Sta.	6946+33.160	100.75 Lt. Y: 584785.834 X:2454989.491	Fd "X" in Sign Base =CH2MHill BM520 EL 746.765-----	646.698	No. 575 Sta.	7311+44.092	83.488 Lt. Y: 580507.579 X:2454577.542	BM Established in 2011 IHC BM on inlet Hdwl 4x4 RCB Plan Elev =654.42 =Plan BM 423"A" (I-74-1(5)2--01--82 Fd on sheet 47A-----	653.550
No. 520 Sta.	6956+41.991	87.183 Rt. Y: 585798.497 X:2455155.537	Fd "X" on Lum Base =CH2MHill BM521 EL 660.130-----	660.059	BENCHMARKS along 53rd Street				
No. 521 Sta.	6963+09.707	63.203 Rt. Y: 586465.537 X:2455117.105	Not Found at given coordinates presumed destroyed CH2M Hill BM-----	679.620	No. 556 Sta.	5101+39.040	46.77 Lt. Y: 587664.694 X:2452092.820	Fd City of Davenport conc pad with disk (Good Cond)-Previous El=707.745 -----	707.691
No. 522 Sta.	6974+87.011	80.483 Rt. Y: 587642.940 X:2455108.891	Fd "X" on Lum Base =CH2MHill BM523 EL 700.669-----	700.594	No. 557 Sta.	5109+03.090	61.092 Rt. Y: 587591.784 X:2452860.855	Fd RR spk N side PP 53rd & Ent to Wendys -Previous El=700.401 -----	700.342
No. 523 Sta.	6988+24.965	63.13 Rt. Y: 588980.204 X:2455062.572	Fd "X" on Lum Base =CH2MHill BM524 EL 719.358-----	719.265	No. 559 Sta.	5119+68.333	62.324 Lt. Y: 587757.826 X:2453921.509	Fd "X" SW Bolt F Hyd-Previous El=697.692----	697.643
No. 524 Sta.	7000+10.296	63.463 Rt. Y: 590165.265 X:2455037.241	Fd "X" in Sign Base =CH2MHill BM525 EL 727.605-----	727.494	No. 560 Sta.	5125+32.940	112.763 Rt. Y: 587587.849 X:2454490.316	Fd "X" on E side Lum Pole Base @ SW Quad 53rd & SBL on Ramp to I-74 Previous El=687.000 -----	686.934
No. 525 Sta.	7010+07.602	91.313 Lt. Y: 591158.986 X:2454860.908	FD "X" on east side sign base =CH2MHill BM526 EL 731.873-----	731.766	No. 564 Sta.	5135+05.67	47.436 Lt. Y: 587788.155 X:2455457.106	Fd "X" on S side Lum Pole Base Previous El=710.688 -----	710.632
No. 526 Sta.	7019+95.564	98.784 Lt. Y: 592123.621 X:2454768.643	Fd RR Spk in w. side fence post =CH2MHill BM527 EL 738.163-----	738.026	No. 563 Sta.	5141+77.716	50.554 Rt. Y: 587718.639 X:2456132.692	Fd RR spk S side PP-Previous El=714.911-----	714.865
No. 527 Sta.	7033+00.191	150.317 Rt. Y: 593441.396 X:2454656.773	Fd "X" in Sign Base =CH2MHill BM528 EL 733.087-----	732.973	No. 561 Sta.	5144+84.792	67.902 Lt. Y: 587849.961 X:2456434.490	Fd "X" on S Traffic Signal Base Previous El=710.934 -----	710.882
No. 528 Sta.	7039+00.326	82.286 Rt. Y: 593998.080 X:2454422.481	Fd IHC BM on SE Cor 67th St. Bridge =CH2MHill BM529 EL 751.48-----	751.346	No. 562 Sta.	5147+11.075	49.497 Rt. Y: 587742.225 X:2456665.530	Fd RR spk N side PP-Previous El=707.554-----	707.513
No. 529 Sta.	7028+71.801	105.188 Rt. Y: 593017.636 X:2454734.117	Not Found at given coordinates presumed destroyed CH2M Hill BM City of Davenport B.M. BRASS Monu-----	711.250	BENCHMARKS Outside Project Limits				
No. 533 Sta.	6975+42.730	2347.104 Rt. Y: 587747.722 X:2457373.774	Fd City of Davenport conc pad with disk (Good Cond)-Previous El=707.745 -----	707.691	No. 612 Sta.	6923+98.048	12492.508 Lt. Y: 581906.735 X:2442670.591	Fd "X" NE Cor Coc Base(=BM612 J Krieger 2007 Rd Survey ZC=680.814)=(BM612 2009 Kimberly Rd Survey ZC=680.884)-----	680.800
No. 556 Sta.	6975+74.064	2934.409 Lt. Y: 587664.694 X:2452092.820	Not Found at given coordinates presumed destroyed BM from 53rd St Survey-----	709.391	No. 999 Sta.	6826+44.943	15993.986 Lt. Y: 568672.694 X:2441824.208	Fd 60 spk in SWK crack near electrical access lid approx 500' NE of station "Davenport" the swk is along west side of Perry Street just north of E 4th St.-----	584.737
No. 558 Sta.	6976+23.550	1683.44 Lt. Y: 587741.254 X:2453342.425	Fd "X" on S side Lum Pole Base-----	710.632	ELEVATION				
No. 564 Sta.	6976+24.653	431.761 Rt. Y: 587788.155 X:2455457.106	Fd IHC BM on inlet hdwl 4x5x175.9 RCB Note:This BM is 2' from CH2MHill #518-----	668.319					
No. 565 Sta.	6934+41.901	101.993 Rt. Y: 583599.244 X:2455217.979	Fd IDOT Brass Button on NE BRG/SWK Barrier Rail-----	708.326					
No. 568 Sta.	6976+08.417	131.511 Rt. Y: 587765.422 X:2455157.278	Fd IDOT Brass Button on NW BRG/SWK Barrier Rail-----	703.120					
No. 569 Sta.	6976+03.140	118.937 Lt. Y: 587754.724 X:2454907.003							



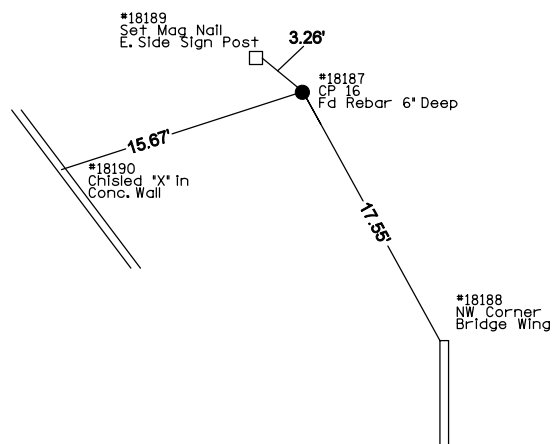
C.P. STA 6805+63.73 RIGHT 341.97  
 C.P. 123, Fd 5/8" Rebar Flush  
 N=571198.304 E=2458123.906



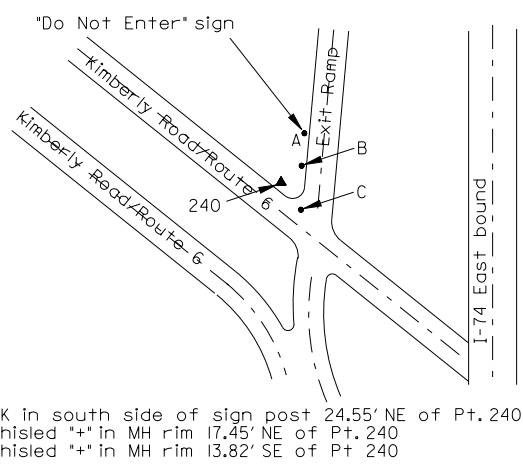
C.P. STA 6806+98.00 RIGHT 238.05  
 C.P. 239, Fd 5/8" Rebar 4" Deep  
 N=571297.829 E=2457985.270



C.P. STA 6811+66.56 RIGHT 16.56  
 C.P. 16, Fd 5/8" Rebar 6" Deep  
 N=571682.071 E=2457637.475

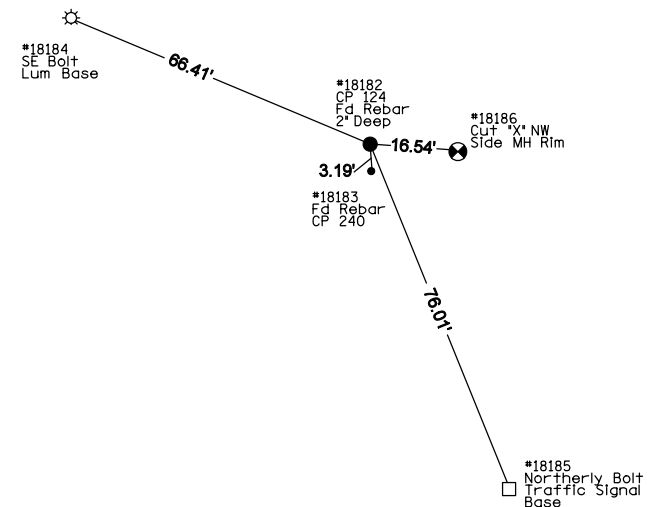


C.P. STA 6813+84.75 LEFT 284.22  
 C.P. 240, Fd 5/8" Rebar 2" Deep  
 N=571803.709 E=2457286.364

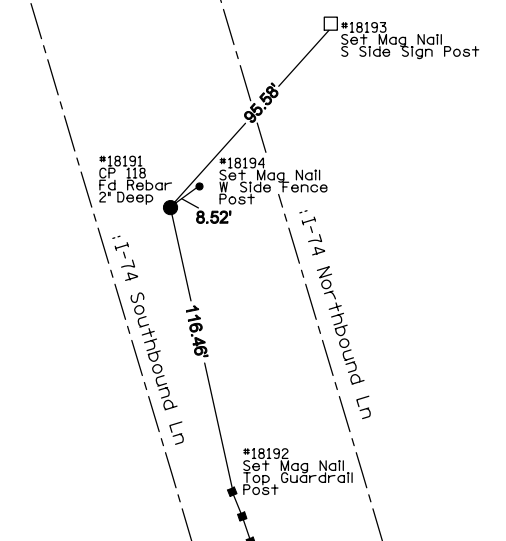


A - PK in south side of sign post 24.55' NE of Pt. 240  
 B - chisled '+' in MH rim 17.45' NE of Pt. 240  
 C - chisled '+' in MH rim 13.82' SE of Pt. 240

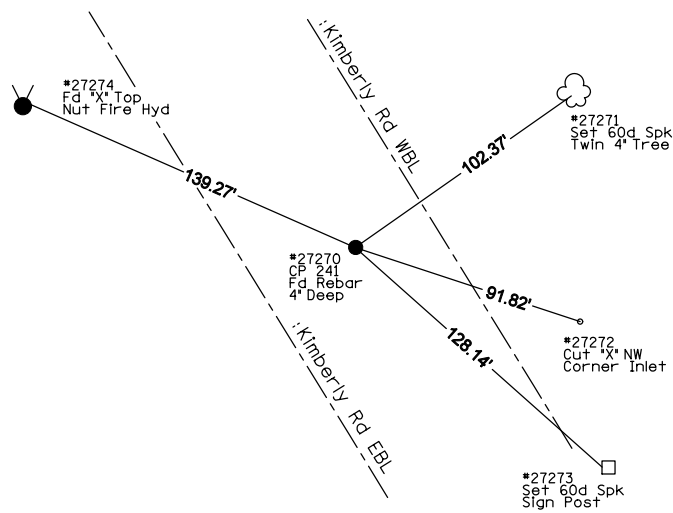
C.P. STA 6813+87.86 LEFT 283.53  
 C.P. 124, Fd 5/8" Rebar 2" Deep  
 N=571806.885 E=2457286.125



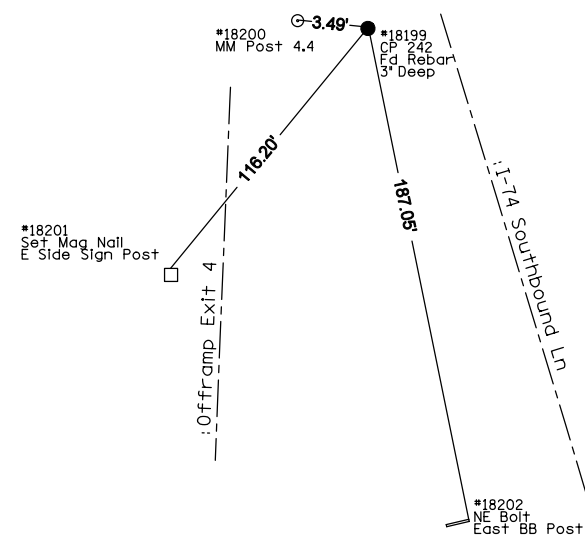
C.P. STA 6815+94.35 LEFT 6.56  
 C.P. 118, Fd 5/8" Rebar 2" Deep  
 N=572084.791 E=2457491.344



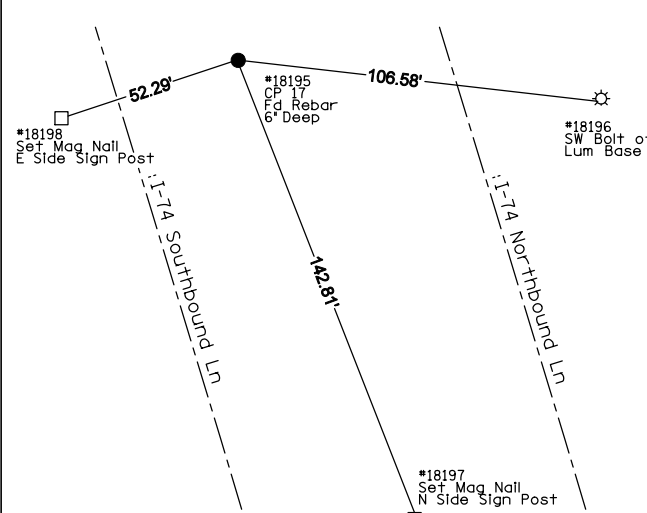
C.P. STA 6817+24.44 LEFT 676.03  
 C.P. 241, Fd 5/8" Rebar 4" Deep  
 N=572015.245 E=2456812.909



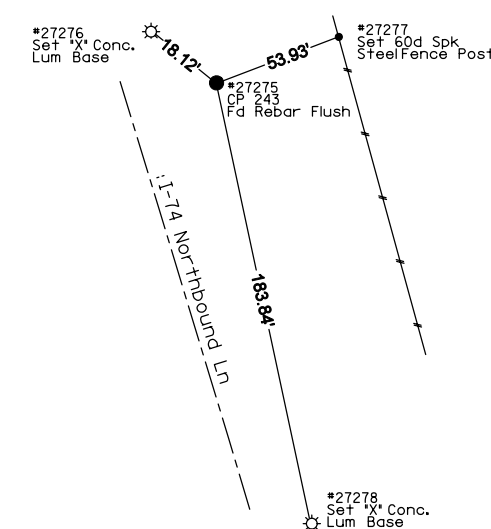
C.P. STA 6822+23.18 LEFT 63.92  
 C.P. 242, Fd 5/8" Rebar 3" Deep  
 N=572670.004 E=2457254.178



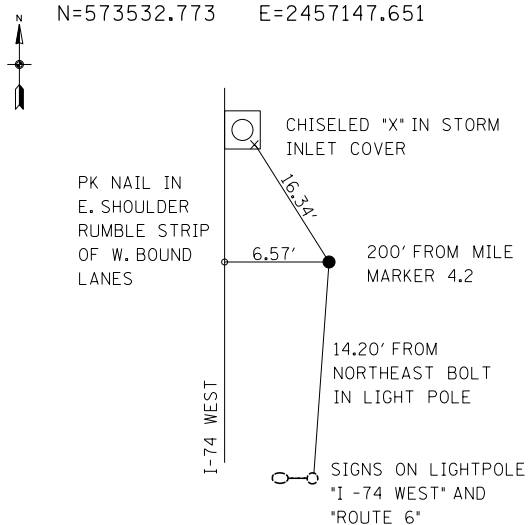
C.P. STA 6825+08.57 LEFT 13.59  
 C.P. 17, Fd 5/8" Rebar 6" Deep  
 N=572958.052 E=2457220.319



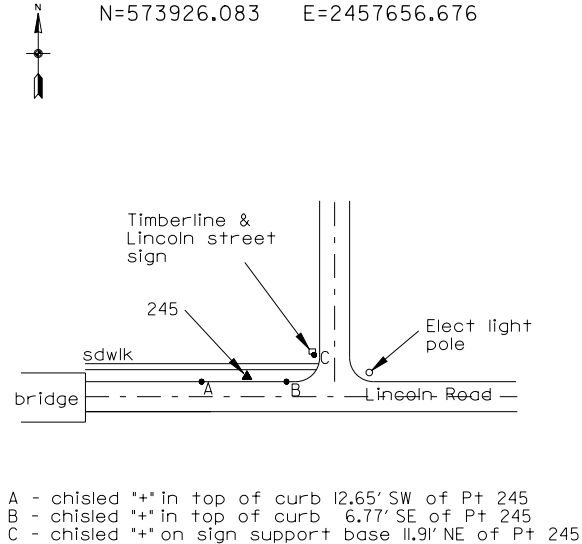
C.P. STA 6830+52.74 RIGHT 84.80  
 C.P. 243, Fd 5/8" Rebar Flush  
 N=573507.713 E=2457167.961



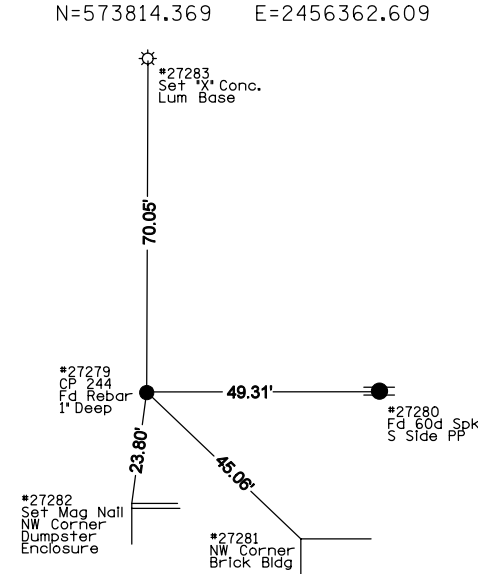
C.P. STA 6830+82.30 RIGHT 71.66  
 C.P. 117, Fd 5/8" Rebar Flush  
 N=573532.773 E=2457147.651



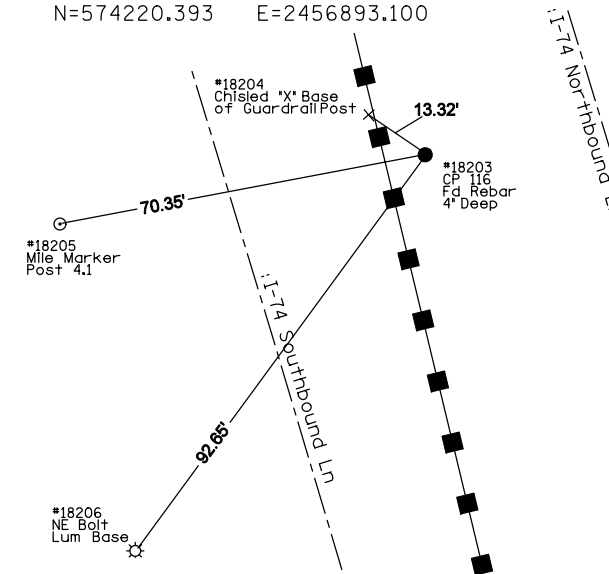
C.P. STA 6833+38.33 RIGHT 663.55  
 C.P. 245, Fd 5/8" Rebar Flush  
 N=573926.083 E=2457656.676



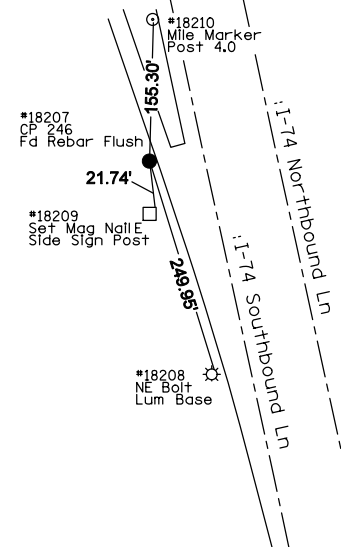
C.P. STA 6835+44.52 LEFT 618.90  
 C.P. 244, Fd 5/8" Rebar 1" Deep  
 N=573814.369 E=2456362.609



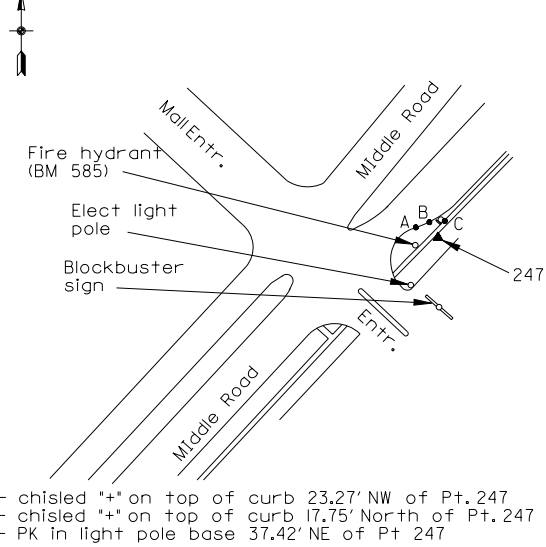
C.P. STA 6838+12.17 LEFT 8.42  
 C.P. 116, Fd 5/8" Rebar 4" Deep  
 N=574220.393 E=2456893.100



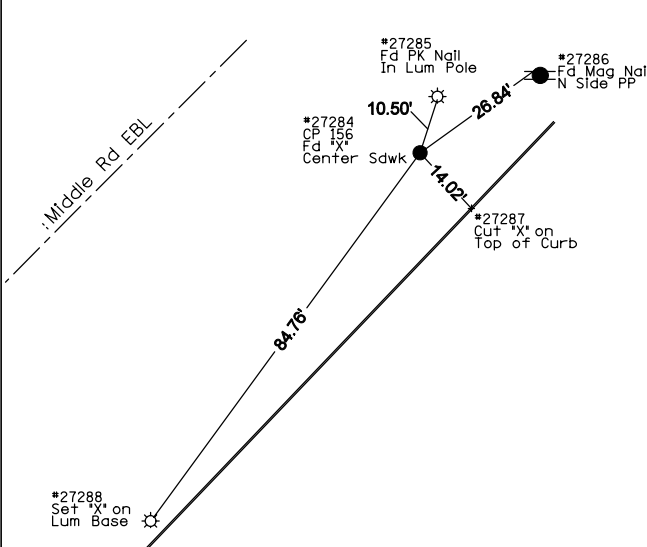
C.P. STA 6841+97.00 LEFT 102.14  
 C.P. 246, Fd 5/8" Rebar Flush  
 N=574576.214 E=2456717.144



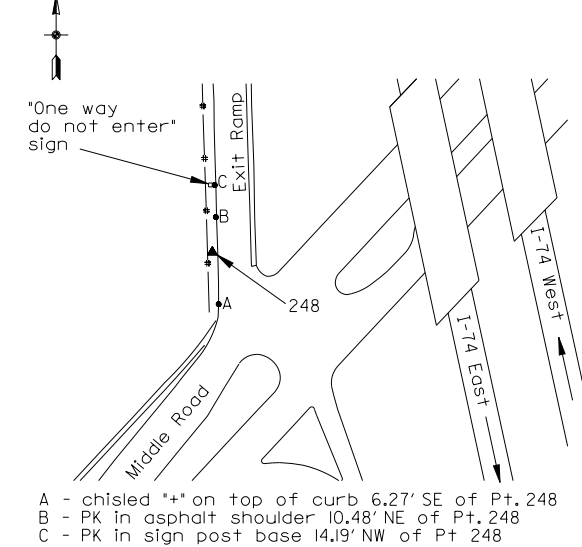
C.P. STA 6843+81.85 LEFT 829.78  
 C.P. 247, Fd 5/8" Rebar Flush  
 N=574603.267 E=2455966.880

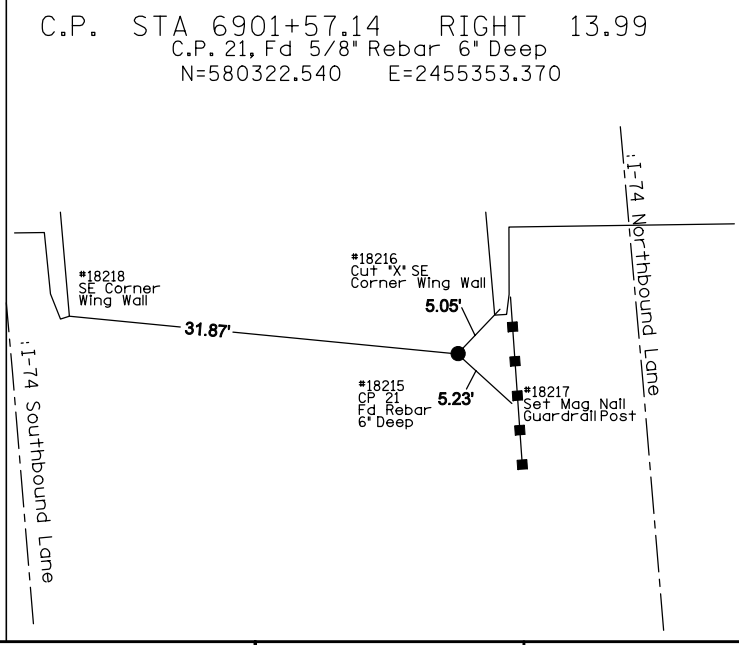
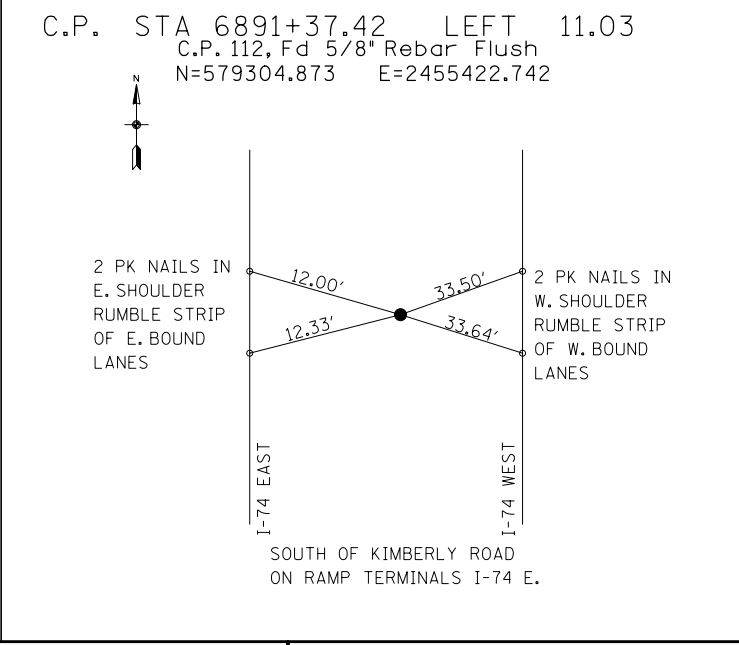
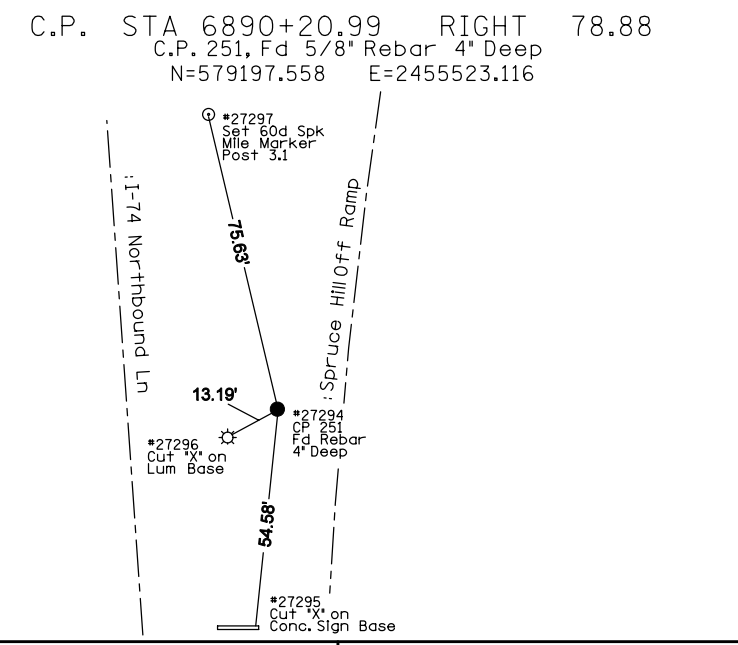
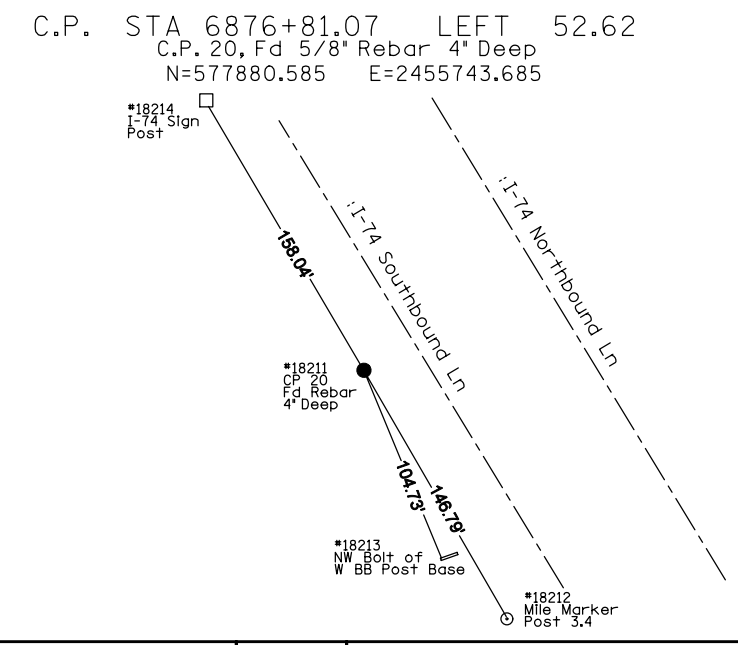
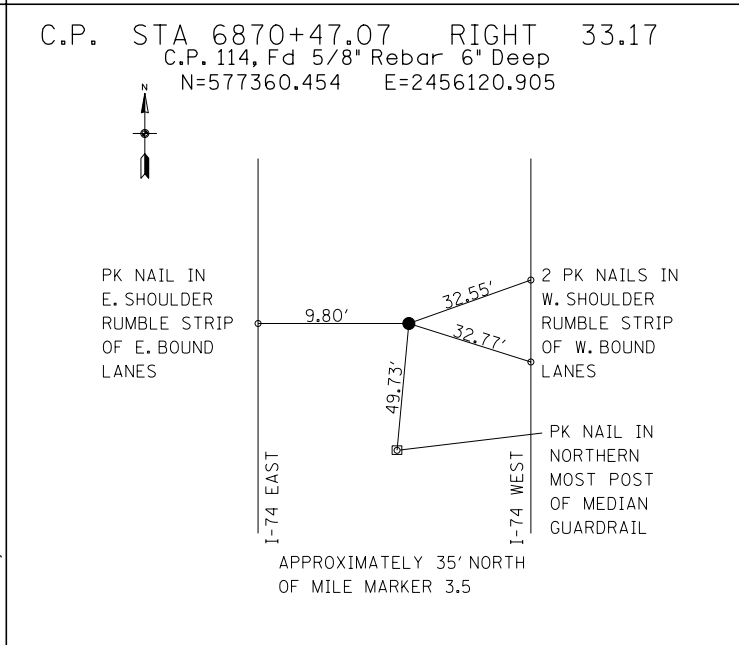
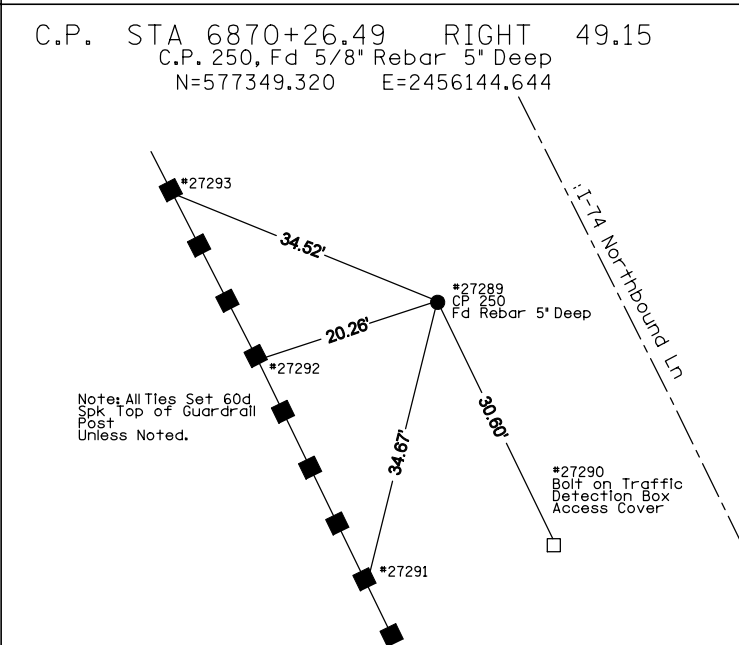
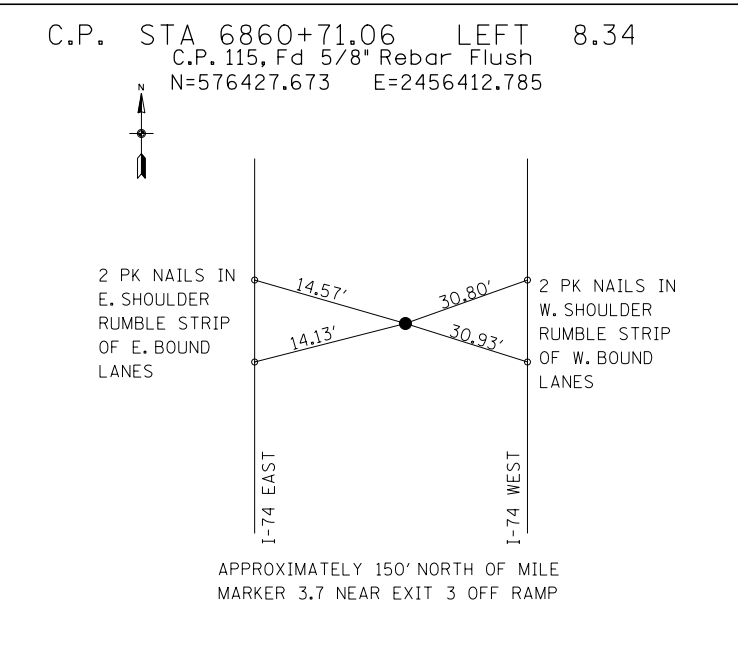
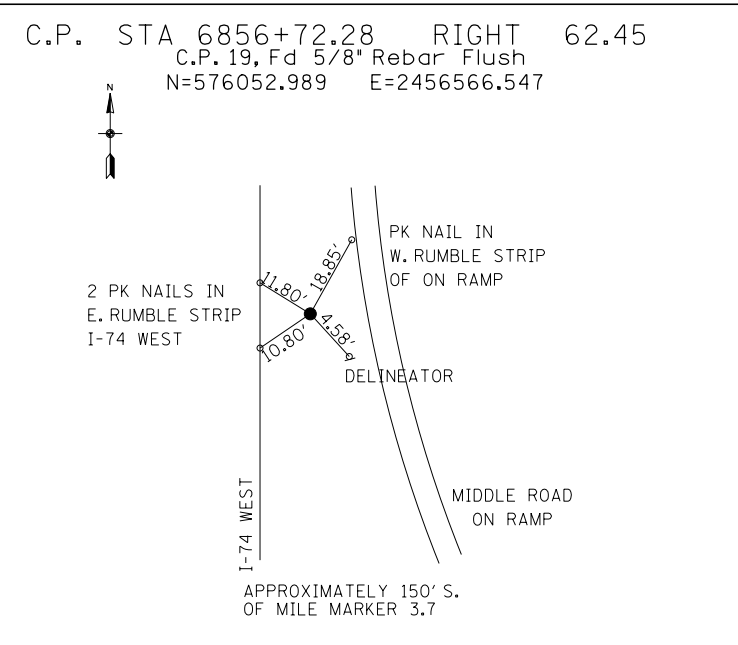
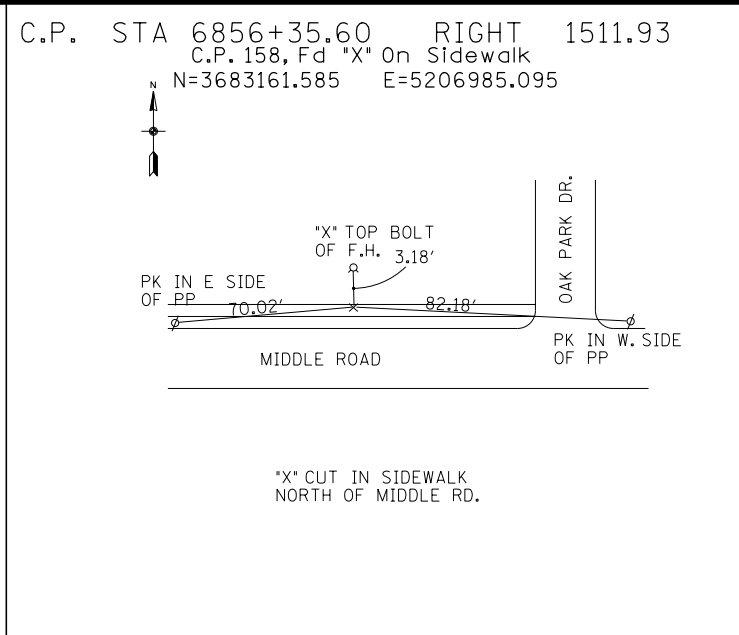
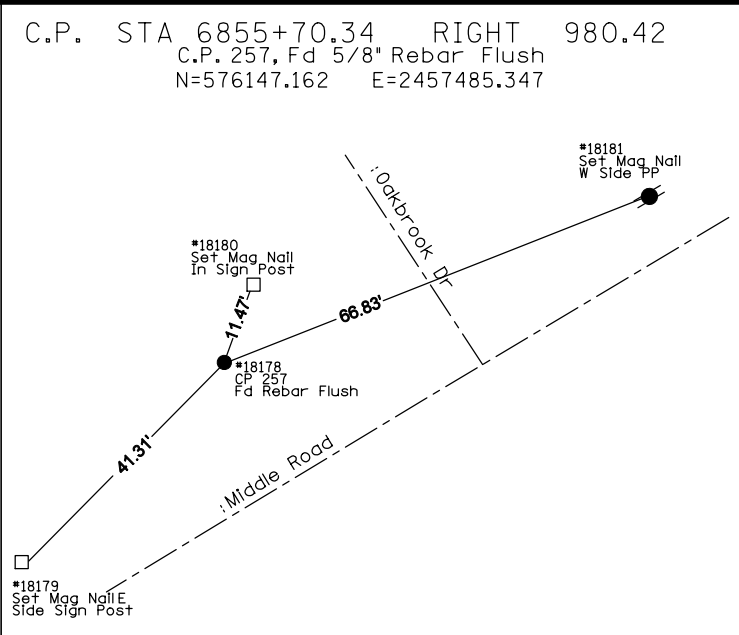
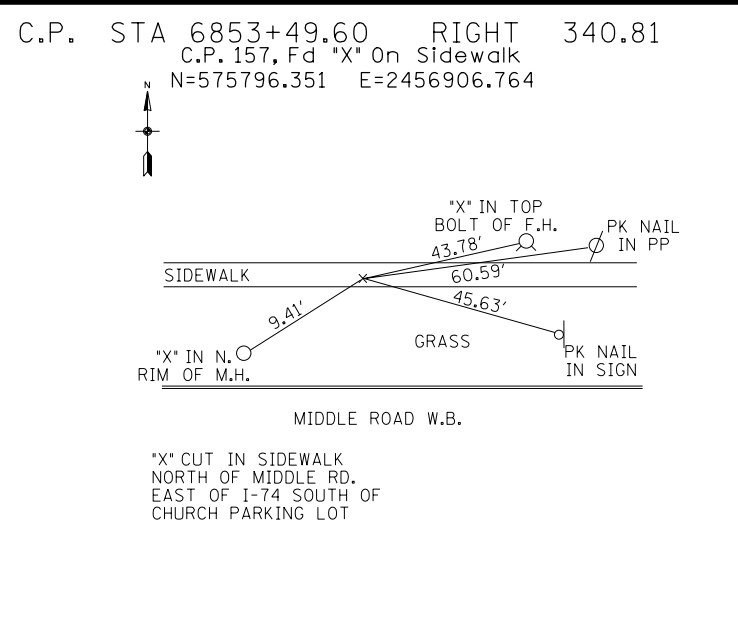
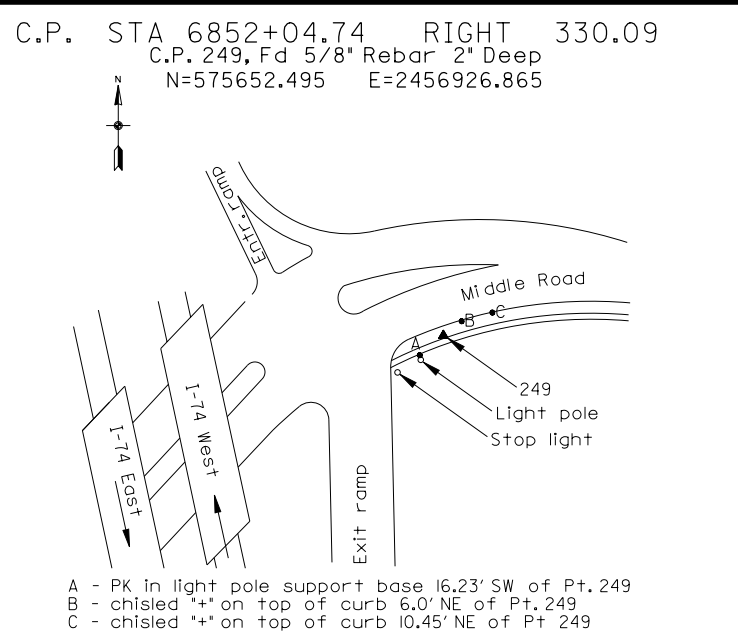


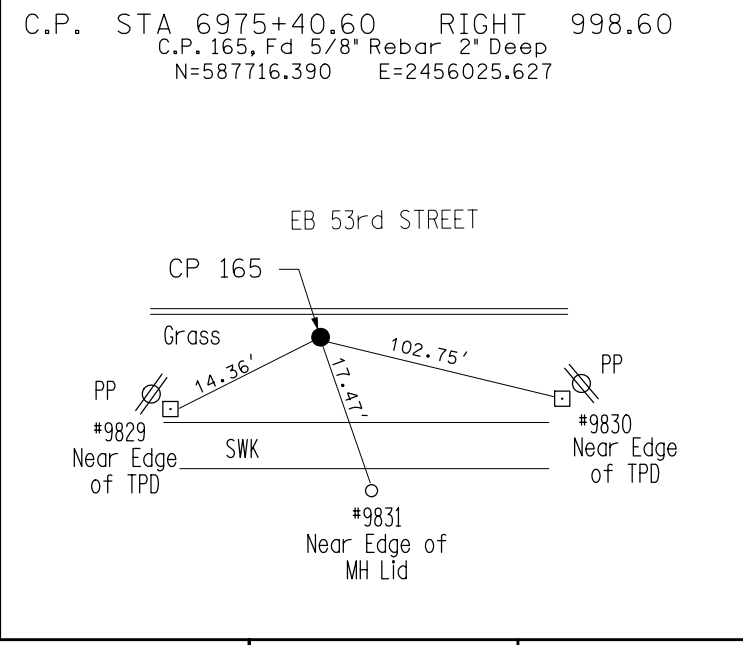
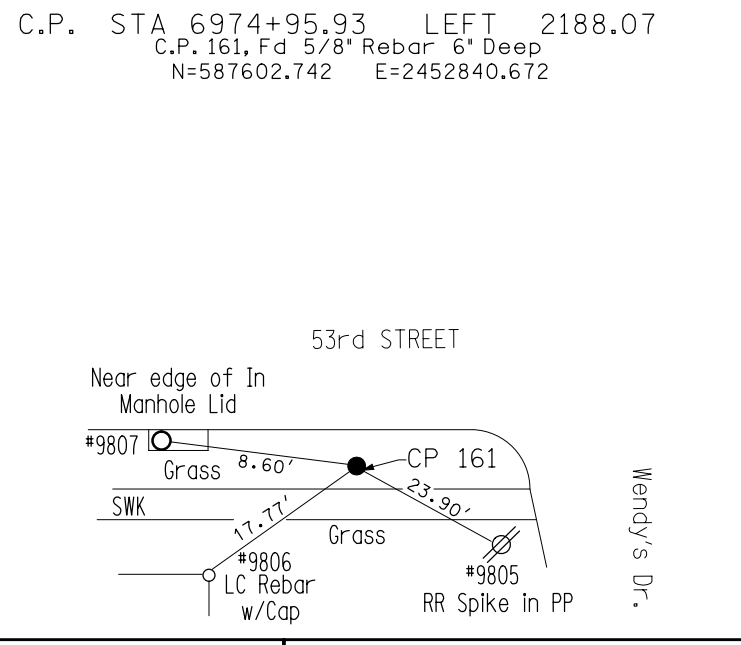
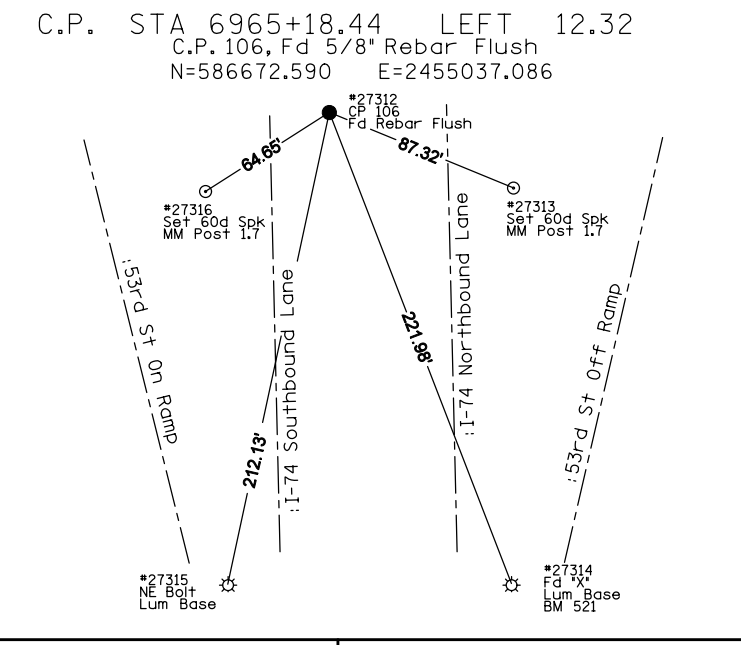
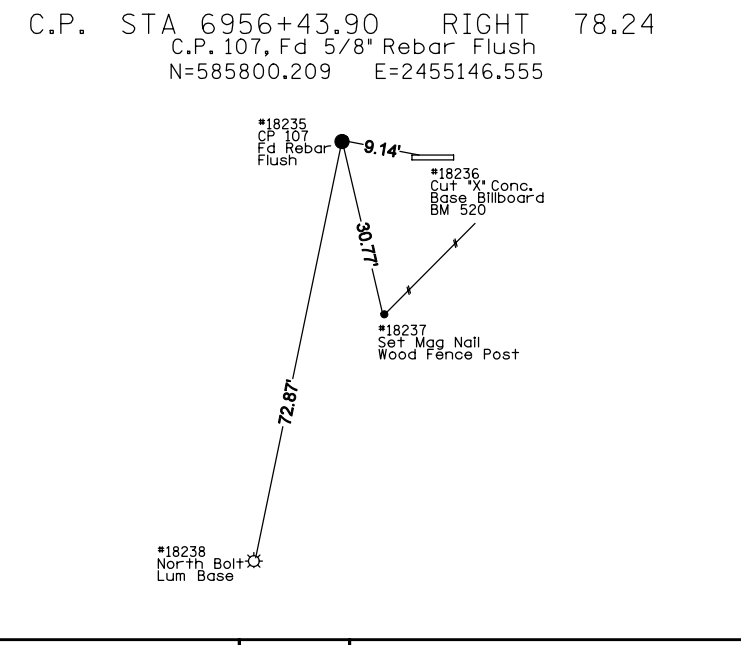
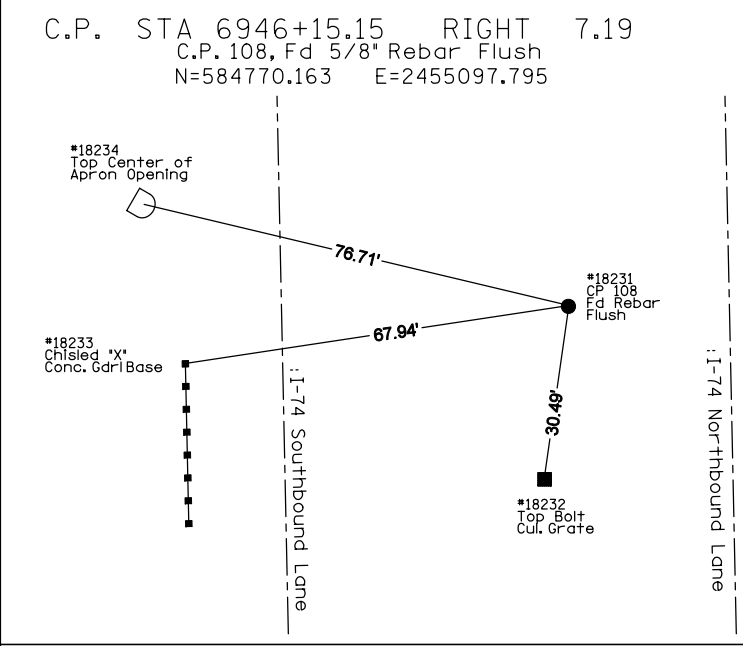
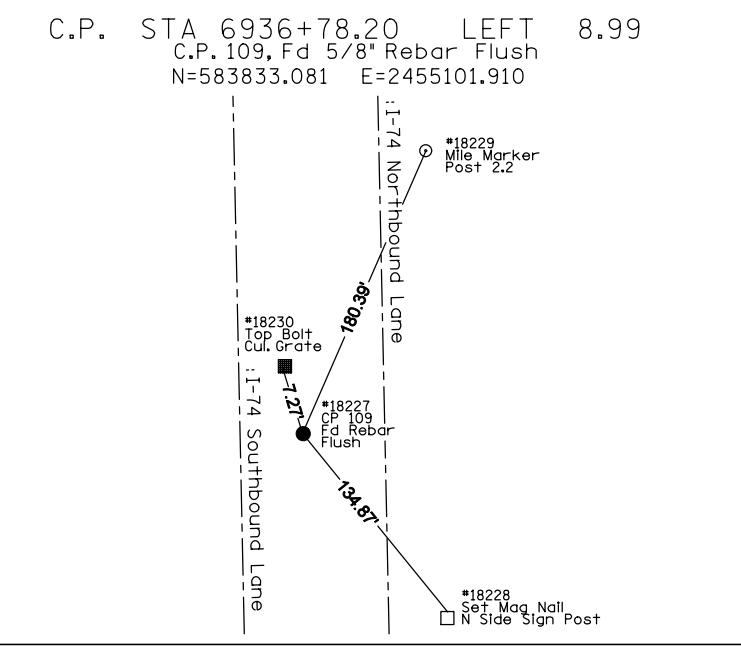
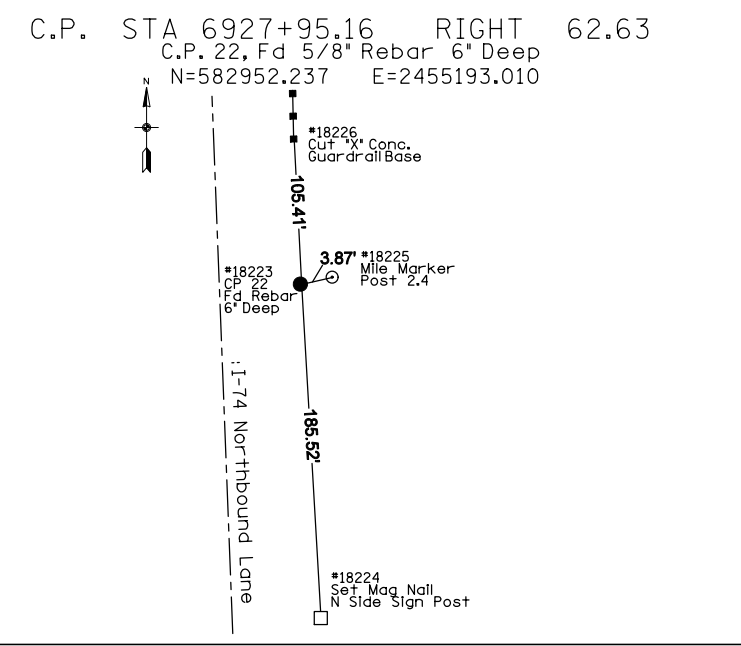
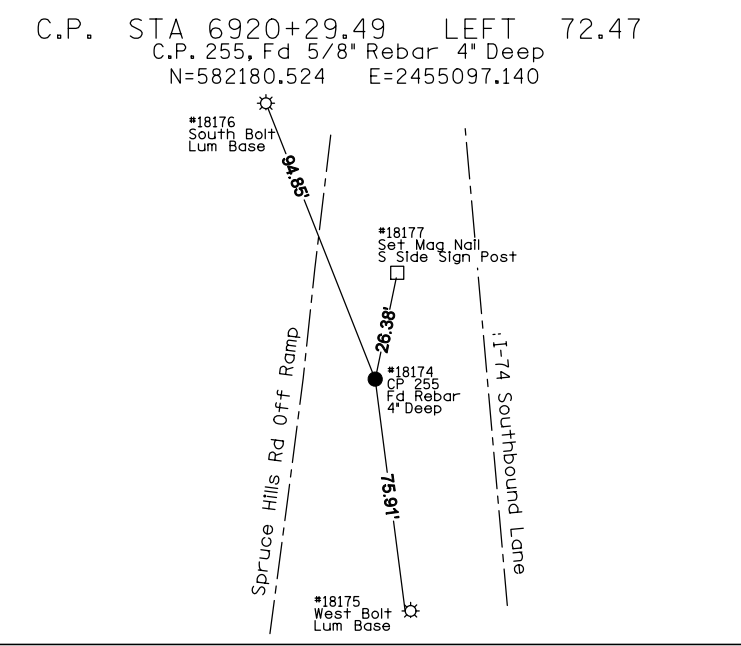
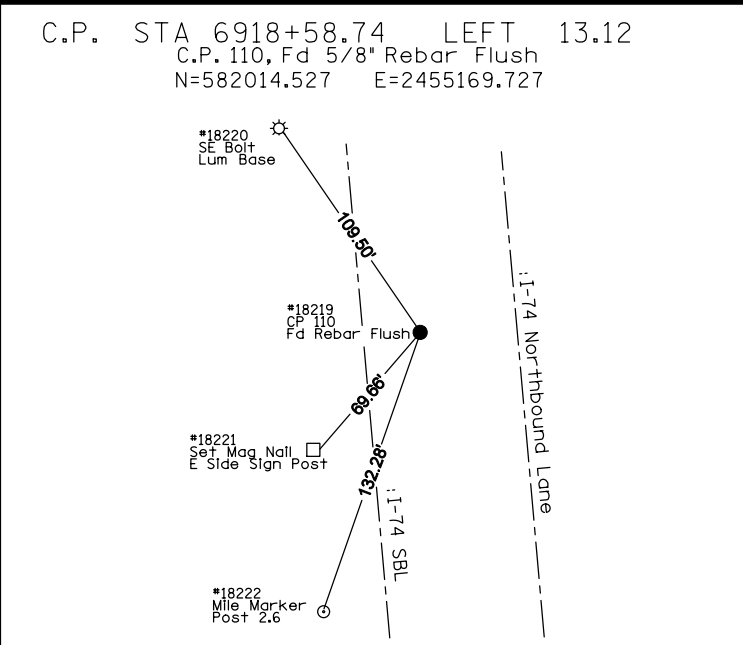
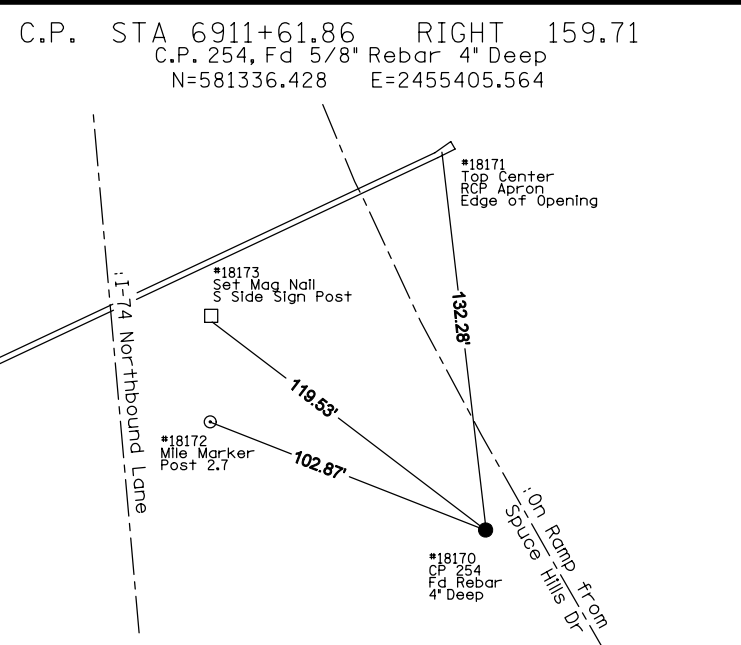
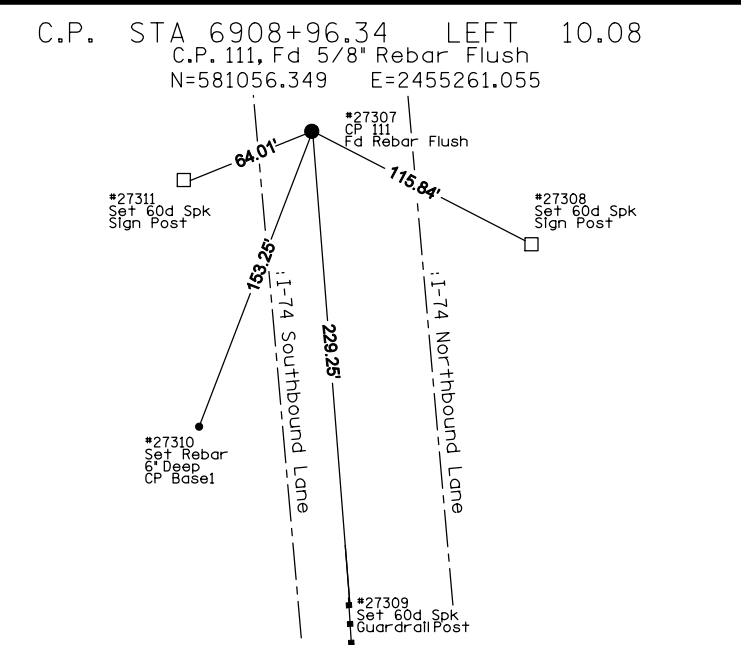
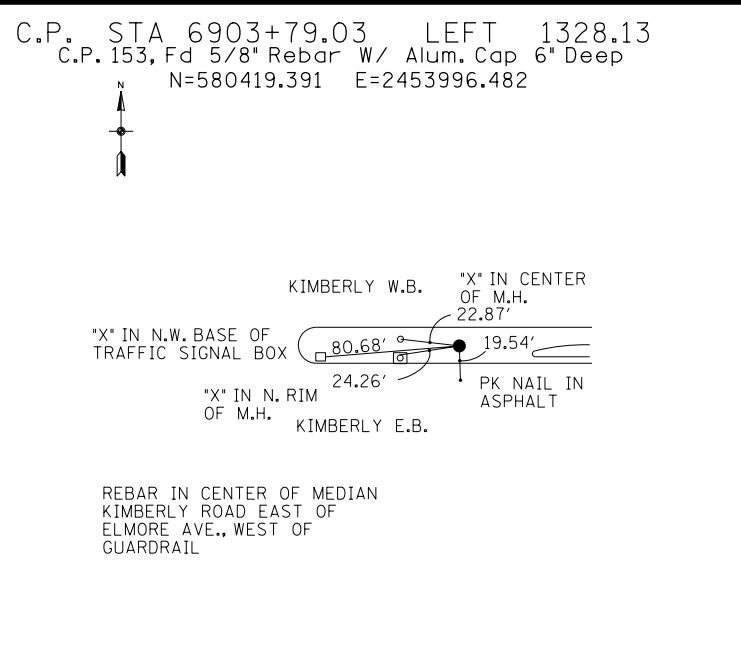
C.P. STA 6845+55.04 LEFT 586.64  
 C.P. 156, Fd 'X' On Sidewalk  
 N=574823.895 E=2456167.973



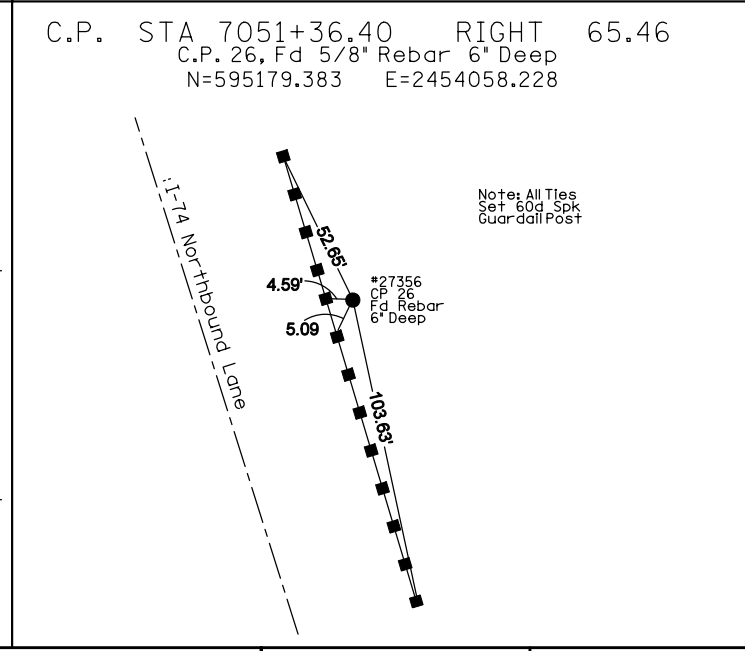
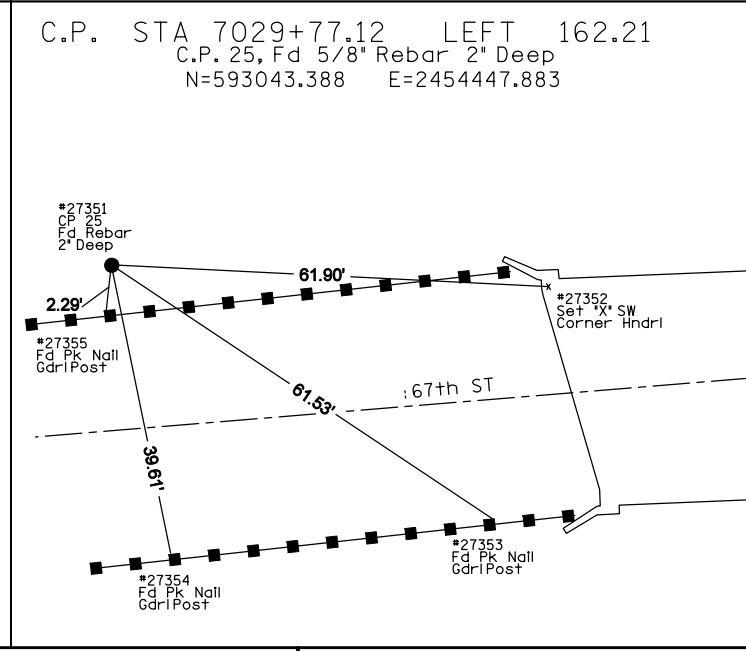
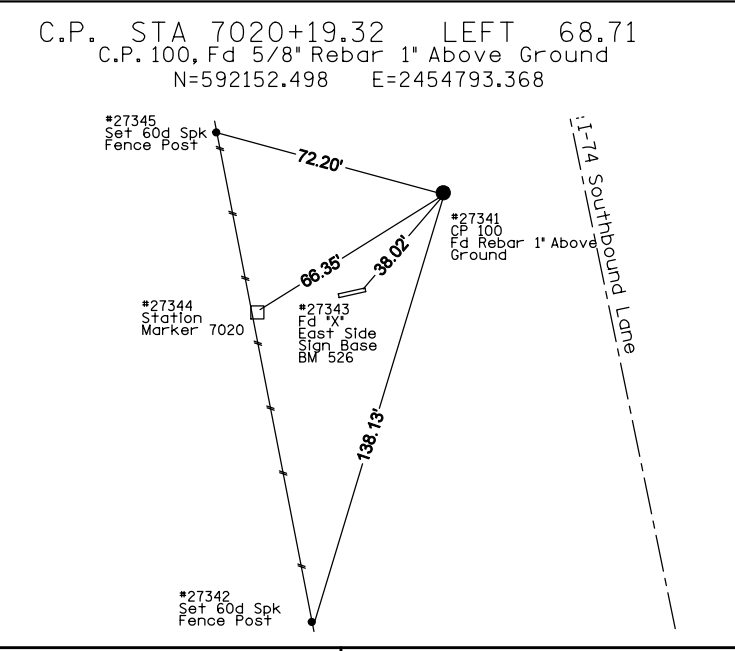
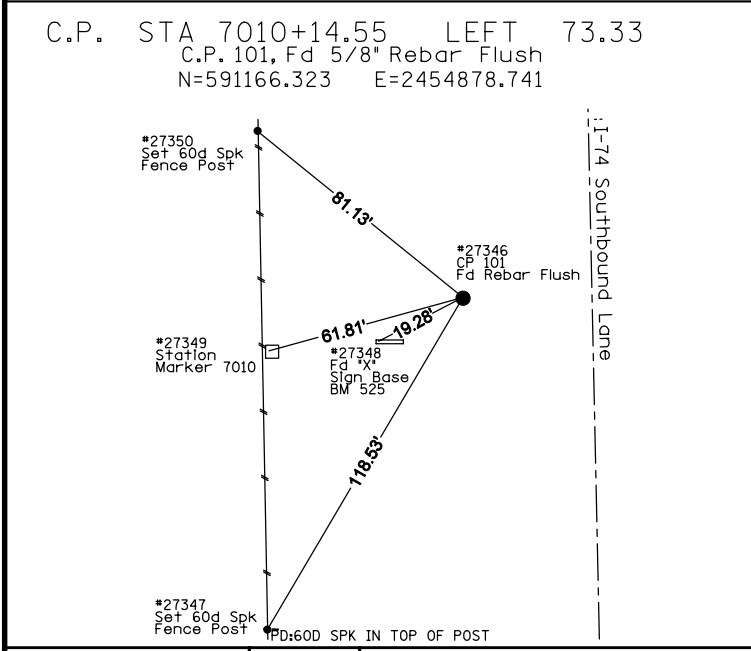
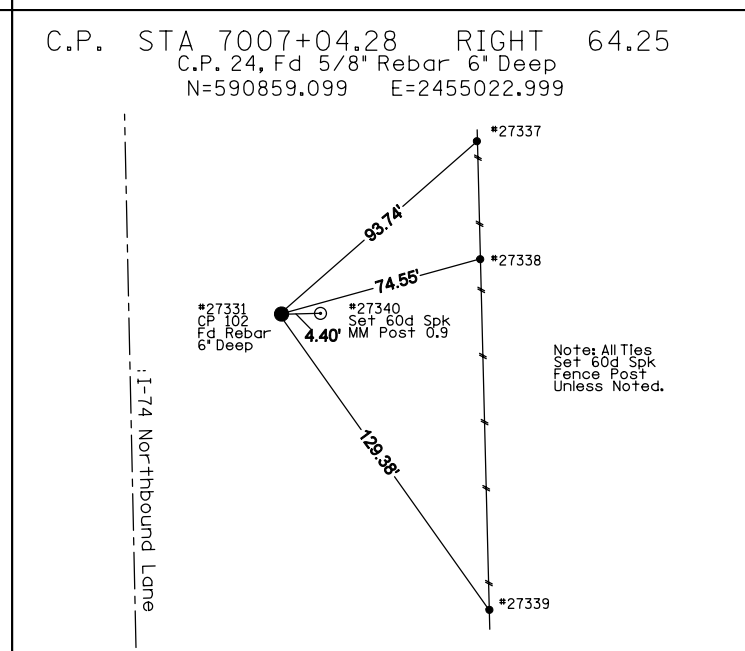
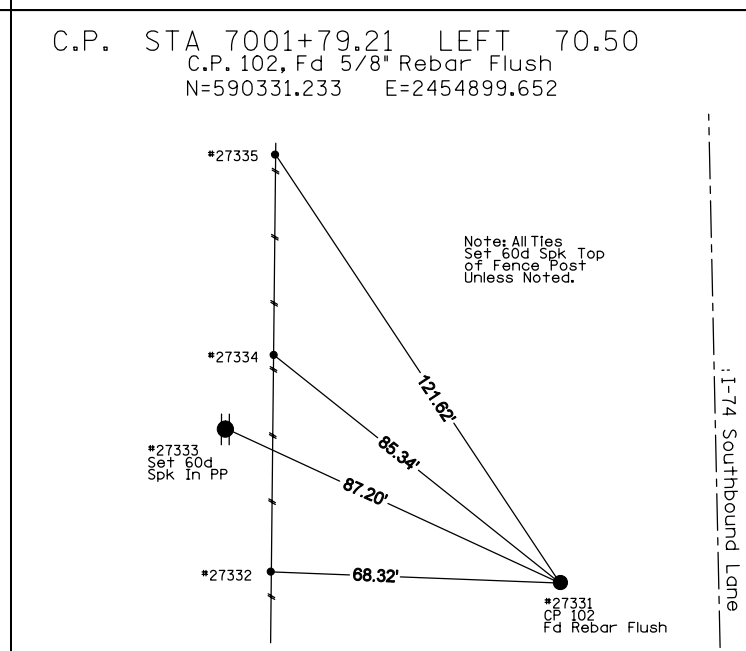
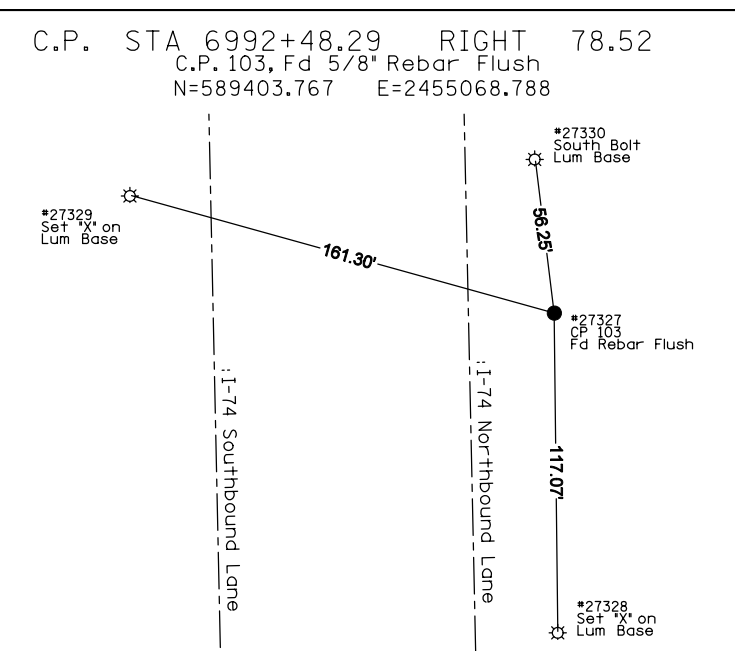
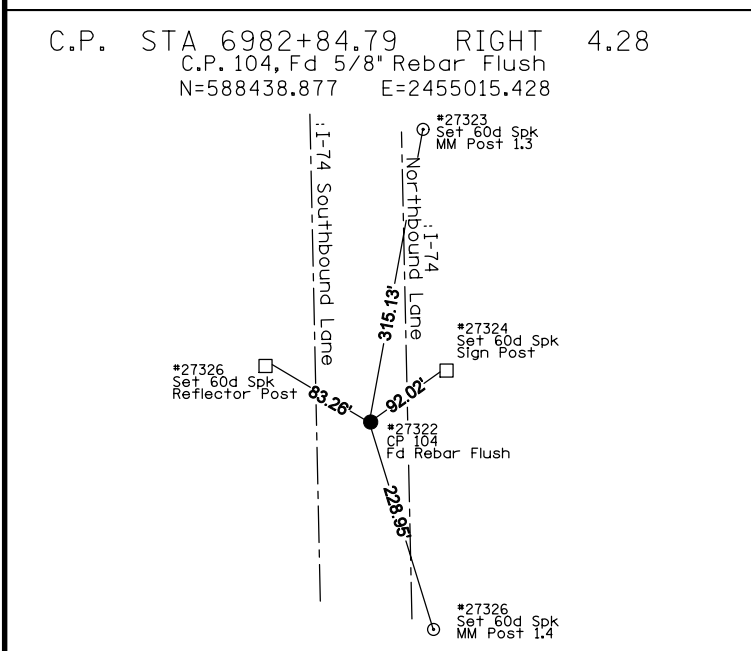
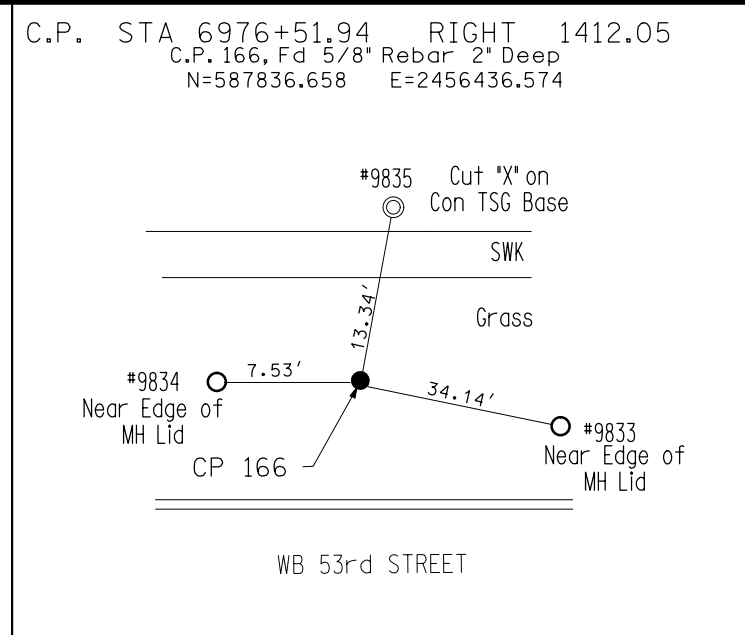
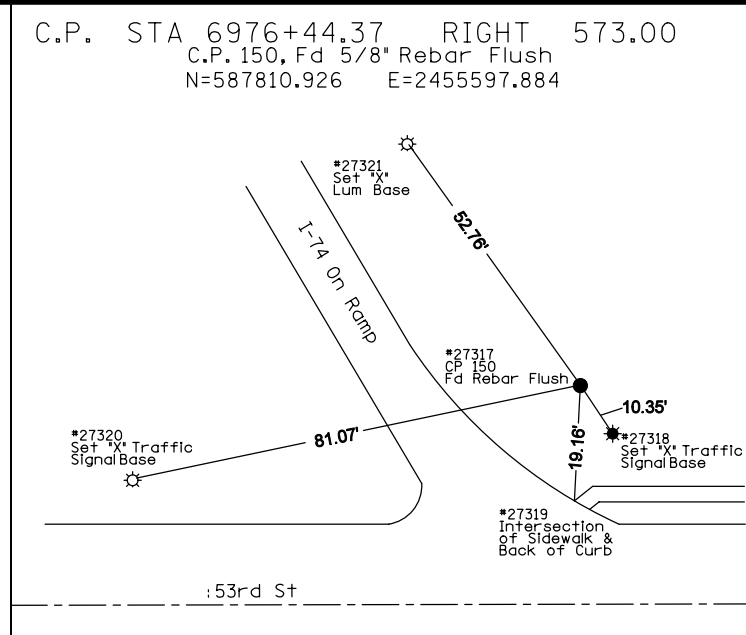
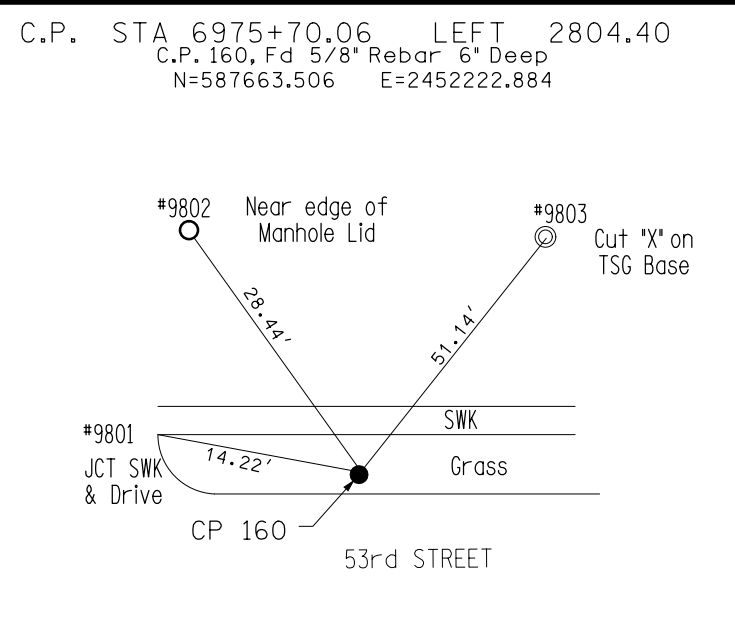
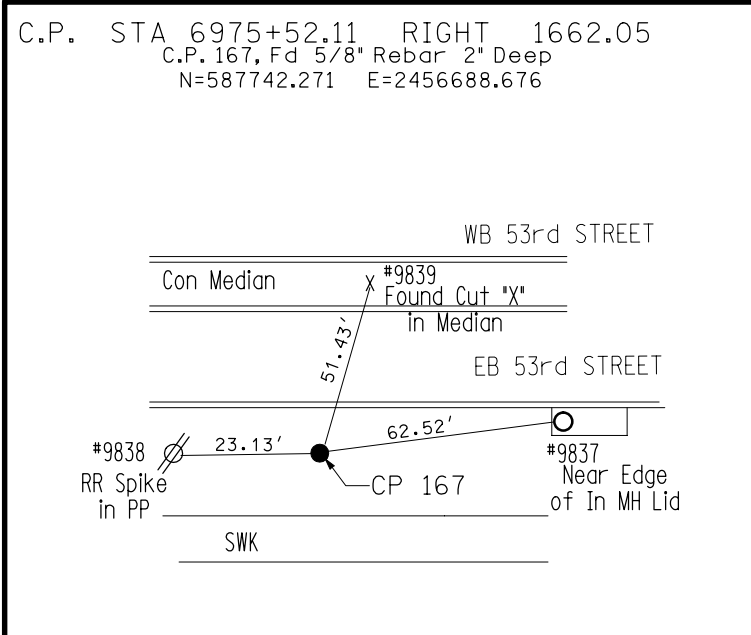
C.P. STA 6849+55.35 LEFT 237.03  
 C.P. 248, Fd 5/8" Rebar 6" Deep  
 N=575288.988 E=2456425.184



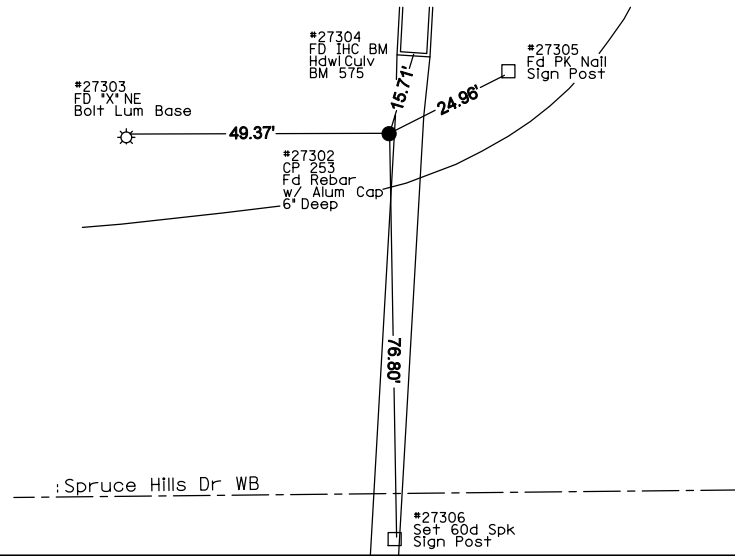




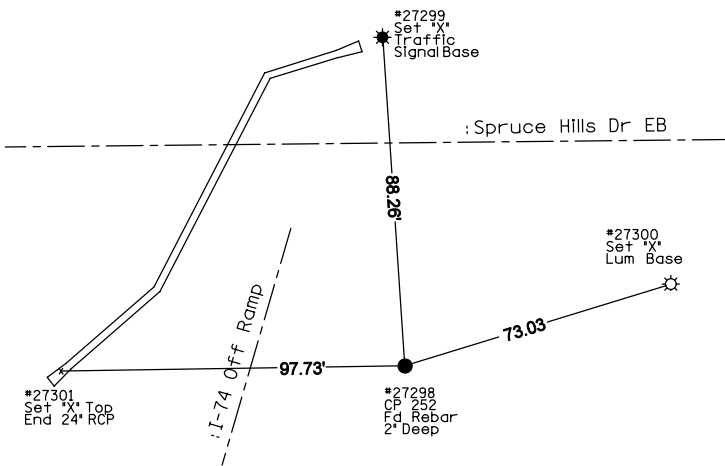




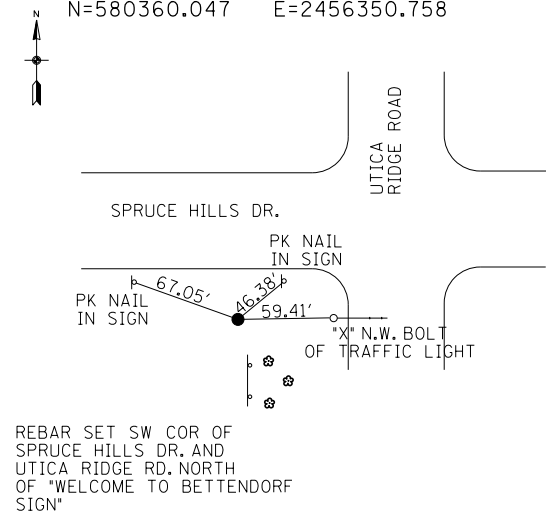
C.P. STA 7311+39.37 LEFT 68.51  
 C.P. 253, Fd 5/8" Rebar W/ Alum Cap 6" Deep  
 N=580492.550 E=2454572.971

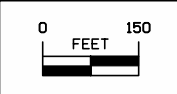
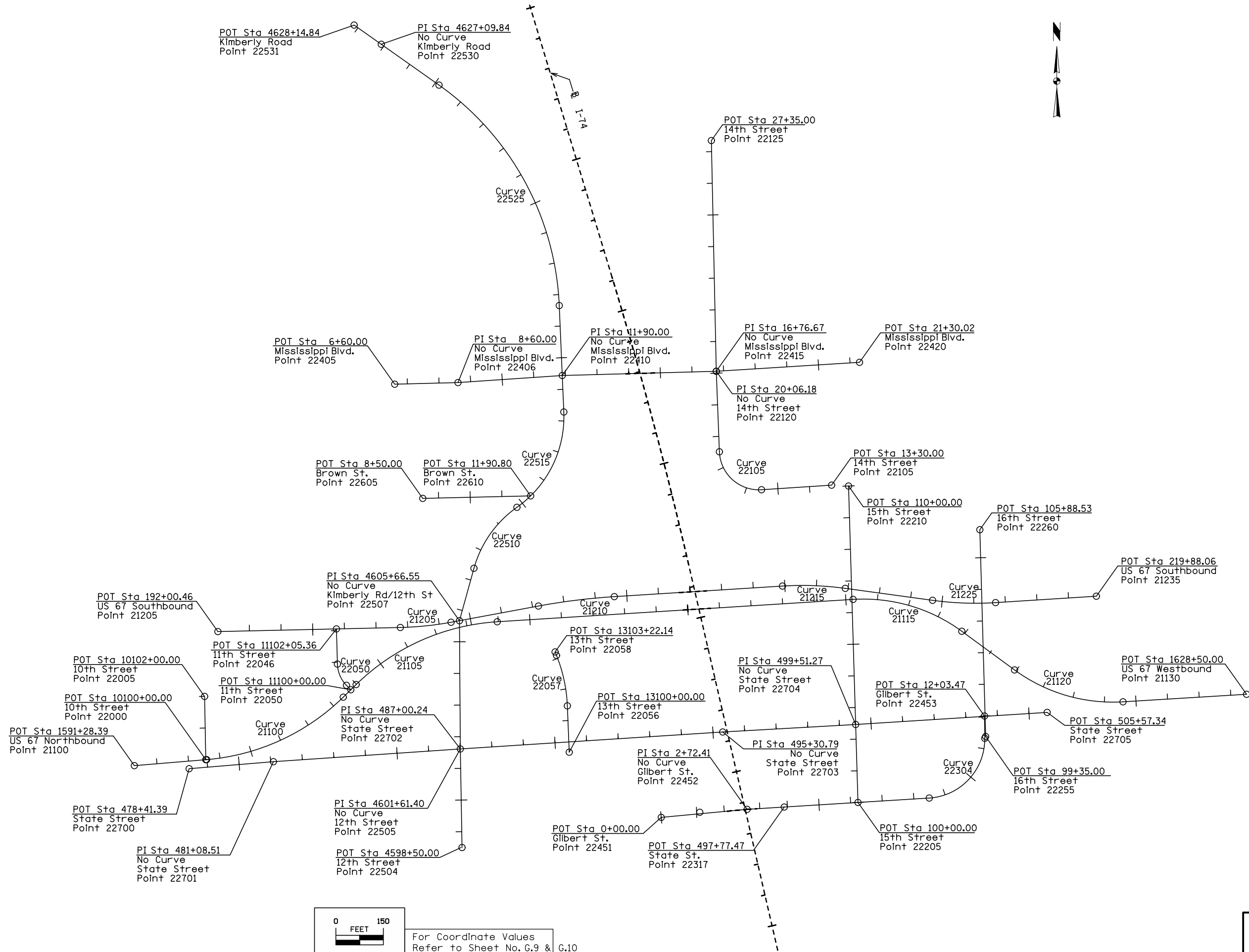


C.P. STA 7324+99.56 RIGHT 87.97  
 C.P. 252, Fd 5/8" Rebar 2" Deep  
 N=580350.094 E=2455934.705



C.P. STA 7329+18.19 RIGHT 82.06  
 C.P. 154, Fd 5/8" Rebar Flush  
 N=580360.047 E=2456350.758





For Coordinate Values  
Refer to Sheet No. G.9 & G.10

**ALIGNMENTS  
LOCAL ROADS**

Curve 6RD\_IL-1 (6TH-D)

PISTA. = 421+45.59  
 $\Delta$  = 4° 18' 49" (LT)  
 D = 4° 46' 29"  
 R = 1,200.00'  
 T = 45.19'  
 L = 90.35'  
 E = 0.85'  
 e = R.C.  
 L = NA  
 x = NA  
 m = NA

Curve 6RD\_IL-2 (6TH-D)

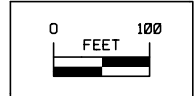
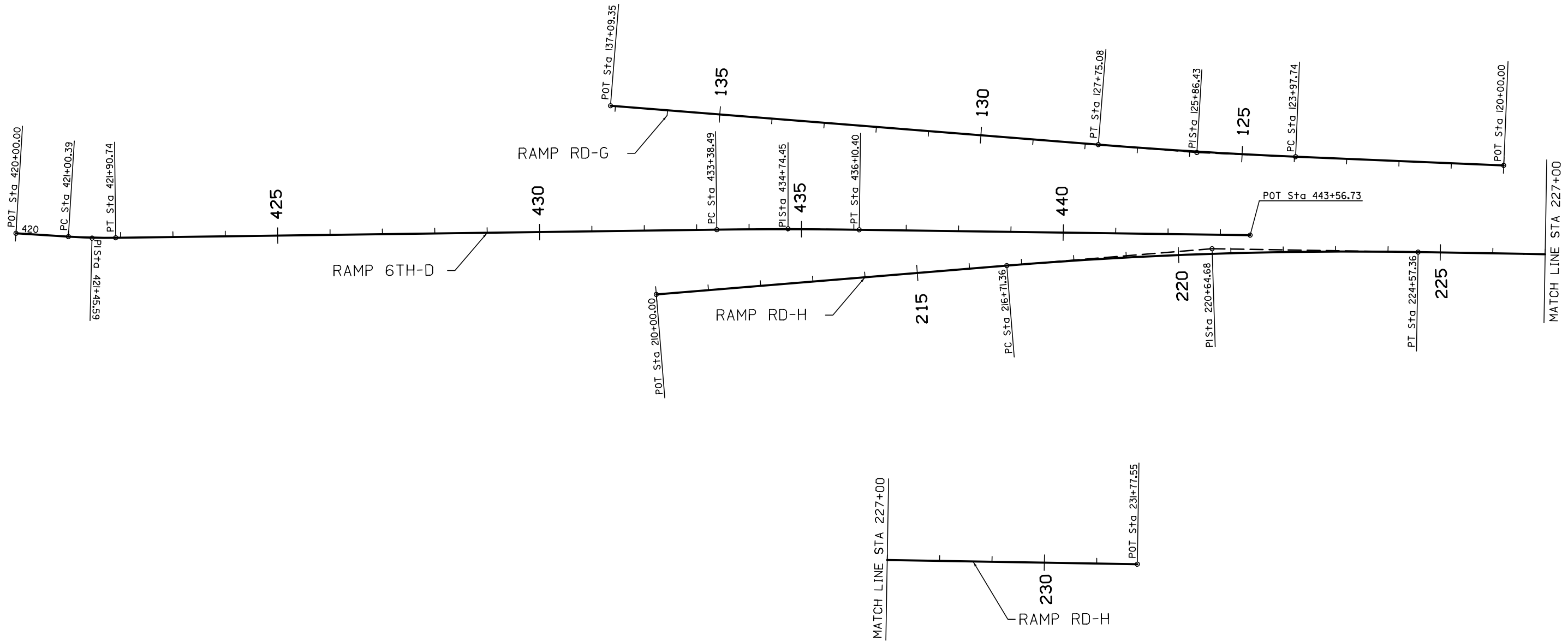
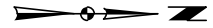
PISTA. = 434+74.45  
 $\Delta$  = 1° 33' 29" (RT)  
 D = 0° 34' 23"  
 R = 10,000.00'  
 T = 135.97'  
 L = 271.92'  
 E = 0.92'  
 e = R.C.  
 L = NA  
 x = NA  
 m = NA

Curve RRD-G-1 (RD-G)

PISTA. = 125+86.43  
 $\Delta$  = 2° 09' 43" (RT)  
 D = 0° 34' 23"  
 R = 10,000.00'  
 T = 188.69'  
 L = 377.33'  
 E = 1.78'  
 e = N.C.  
 L = NA  
 x = NA  
 m = NA

Curve 3RH\_IL-1 (RD-H)

PISTA. = 220+64.68  
 $\Delta$  = 5° 37' 46" (RT)  
 D = 0° 42' 58"  
 R = 8,000.00'  
 T = 393.32'  
 L = 786.00'  
 E = 9.66'  
 e = R.C.  
 L = NA  
 x = NA  
 m = NA



**ALIGNMENTS**  
**Illinois Ramps**

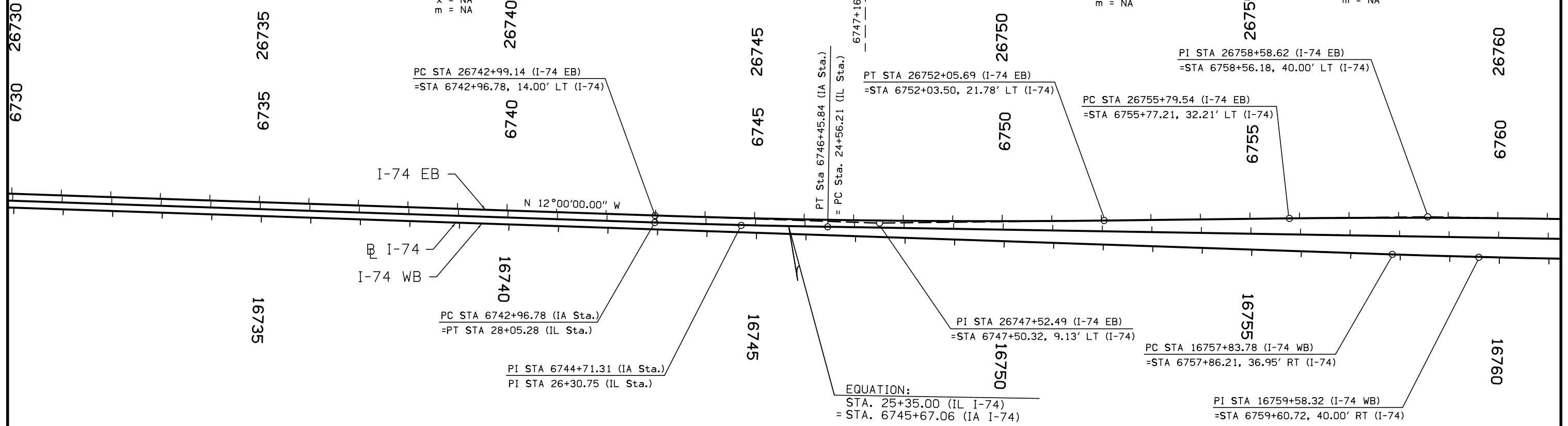


Curve 21010 (I-74)  
 PI Sta 6744+71.31 (IA Sta.)  
 PI Sta 26+30.75 (IL Sta.)  
 $\Delta = 01^{\circ}00'00.00''$  LT  
 $D = 0^{\circ}17'11.32''$   
 $R = 20000.00'$   
 $T = 174.54'$   
 $L = 349.07'$   
 $E = 0.76'$   
 $e = \text{N.C.}$   
 $L = \text{NA}$   
 $x = \text{NA}$   
 $m = \text{NA}$

Curve 21003 (I-74 EB)  
 PI Sta 26747+52.49  
 $\Delta = 2^{\circ}35'56.05''$  (LT)  
 $D = 0^{\circ}17'12.05''$   
 $R = 19,986.00'$   
 $T = 453.35'$   
 $L = 906.55'$   
 $E = 5.14'$   
 $e = \text{N.C.}$   
 $L = \text{NA}$   
 $x = \text{NA}$   
 $m = \text{NA}$

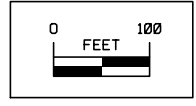
Curve 21005 (I-74 EB)  
 PI Sta 26758+58.62  
 $\Delta = 1^{\circ}35'56.05''$  (RT)  
 $D = 0^{\circ}17'11.32''$   
 $R = 20,000.00'$   
 $T = 279.08'$   
 $L = 558.12'$   
 $E = 1.95'$   
 $e = \text{N.C.}$   
 $L = \text{NA}$   
 $x = \text{NA}$   
 $m = \text{NA}$

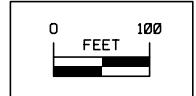
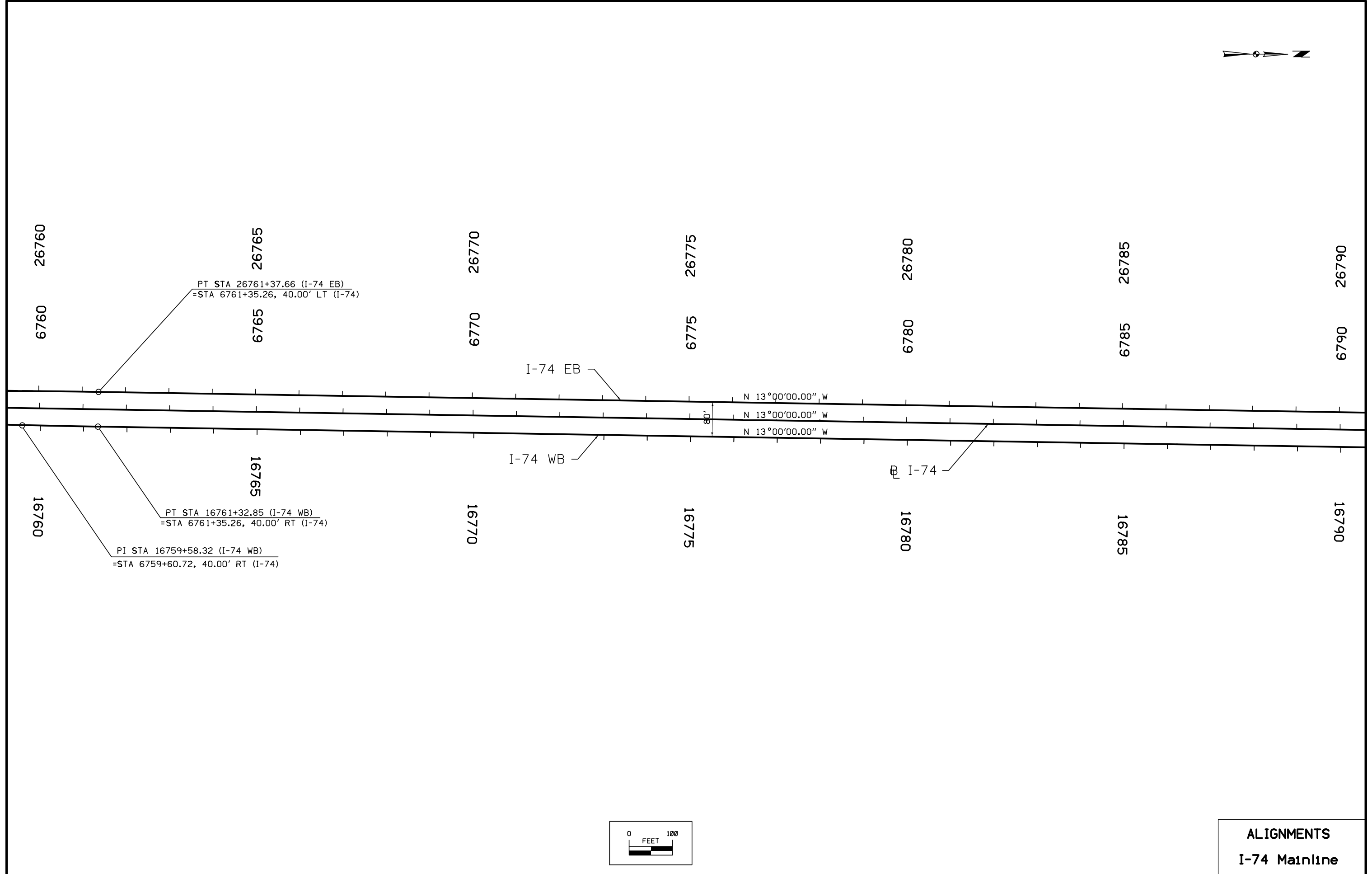
ILLINOIS JURISDICTION | IOWA JURISDICTION



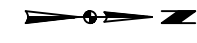
FOR RAMP RD-H, RD-G AND 6TH-D, REFER TO ILLINOIS PACKAGE A, FOR RAMP 6TH-C, REFER TO ILLINOIS PACKAGE B

ALIGNMENTS  
I-74 Mainline





**ALIGNMENTS**  
I-74 Mainline



Curve 21015 (I-74)  
 PI Sta 6801+41.31  
 $\Delta = 03^\circ 50' 58.23''$  LT  
 $D = 0^\circ 24' 33.32''$   
 $R = 14000.00$   
 $T = 470.48$   
 $L = 940.61$   
 $E = 7.90$   
 $e = \text{N.C.}$   
 $L = \text{NA}$   
 $x = \text{NA}$   
 $m = \text{NA}$

Curve 21017 (I-74 EB)  
 PI Sta 26805+14.75  
 $\Delta = 03^\circ 50' 58.22''$  LT  
 $D = 0^\circ 24' 33.32''$   
 $R = 14000.00$   
 $T = 470.48$   
 $L = 940.61$   
 $E = 7.90$   
 $e = \text{N.C.}$   
 $L = \text{NA}$   
 $x = \text{NA}$   
 $m = \text{NA}$

PC STA. 26800+44.27 (I-74 EB)  
 =POC STA. 6800+42.84, 35.07 LT (I-74)

PT STA. 16802+38.00 (I-74 WB)  
 =POC STA. 6802+39.05, 19.96 RT (I-74)

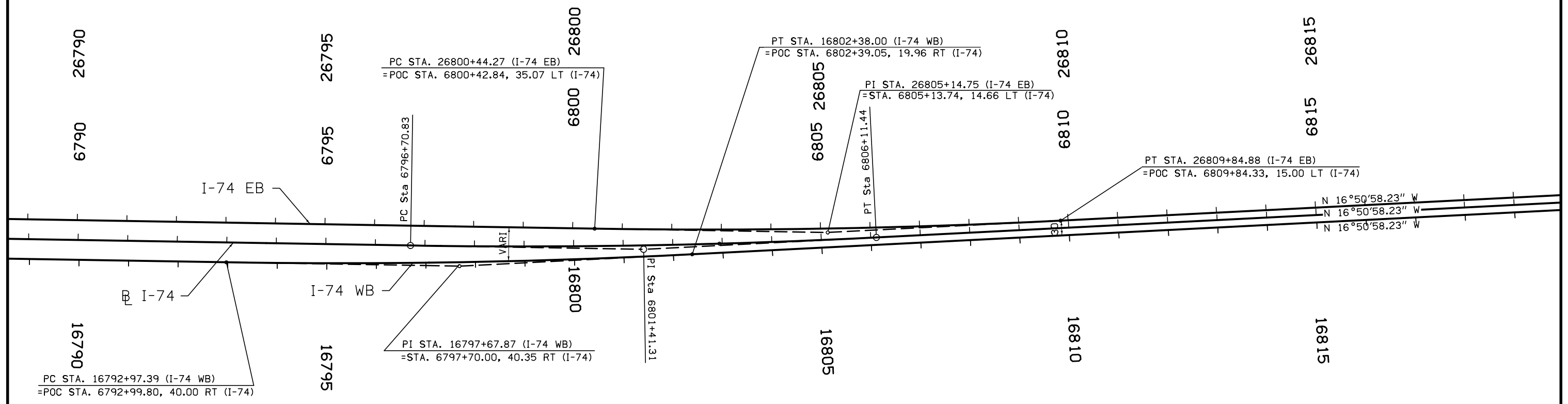
PI STA. 26805+14.75 (I-74 EB)  
 =STA. 6805+13.74, 14.66 LT (I-74)

PT STA. 26809+84.88 (I-74 EB)  
 =POC STA. 6809+84.33, 15.00 LT (I-74)

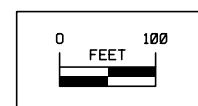
PI STA. 16797+67.87 (I-74 WB)  
 =STA. 6797+70.00, 40.35 RT (I-74)

PC STA. 16792+97.39 (I-74 WB)  
 =POC STA. 6792+99.80, 40.00 RT (I-74)

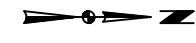
Curve 21016 (I-74 WB)  
 PI Sta 16797+67.87  
 $\Delta = 03^\circ 50' 58.24''$  LT  
 $D = 0^\circ 24' 33.32''$   
 $R = 14000.00$   
 $T = 470.48$   
 $L = 940.61$   
 $E = 7.90$   
 $e = \text{N.C.}$   
 $L = \text{NA}$   
 $x = \text{NA}$   
 $m = \text{NA}$



N 16°50'58.23" W  
 N 16°50'58.23" W  
 N 16°50'58.23" W

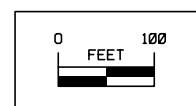
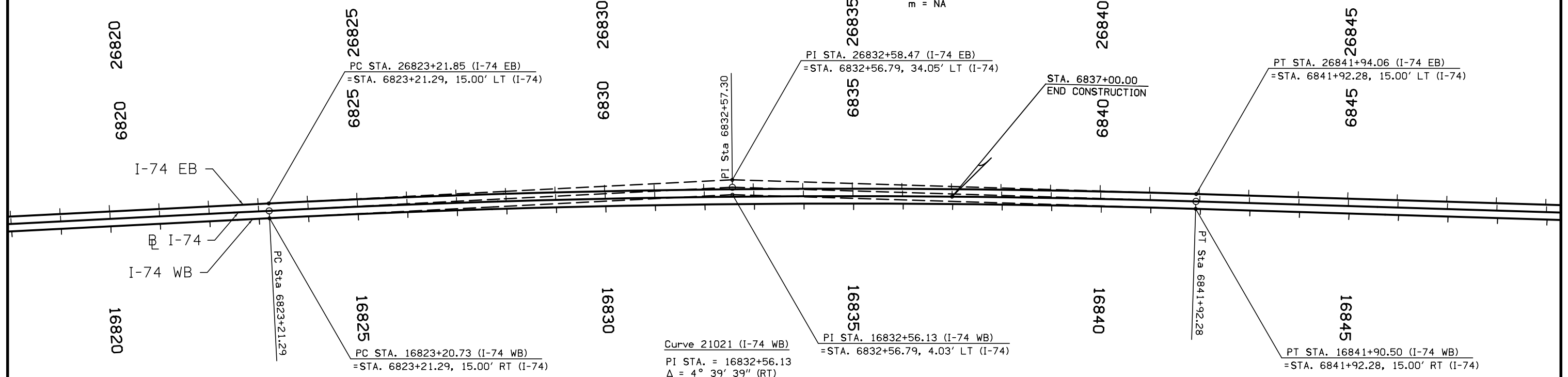


**ALIGNMENTS**  
**I-74 Mainline**



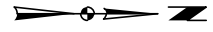
Curve 21020 (I-74)  
 PI Sta 6832+57.30  
 $\Delta = 04^{\circ}39'39.15''$  RT  
 $D = 0^{\circ}14'56.80''$   
 $R = 23000.00$   
 $T = 936.01$   
 $L = 1871.00$   
 $E = 19.04$   
 $e = \text{N.C.}$   
 $L = \text{NA}$   
 $x = \text{NA}$   
 $m = \text{NA}$

Curve 21022 (I-74 EB)  
 PI STA. = 26832+58.47  
 $\Delta = 4^{\circ}39'39''$  (RT)  
 $D = 0^{\circ}14'56''$   
 $R = 23,015.00'$   
 $T = 936.62'$   
 $L = 1,872.22'$   
 $E = 19.05'$   
 $e = \text{N.C.}$   
 $L = \text{NA}$   
 $x = \text{NA}$   
 $m = \text{NA}$



**ALIGNMENTS**  
**I-74 Mainline**





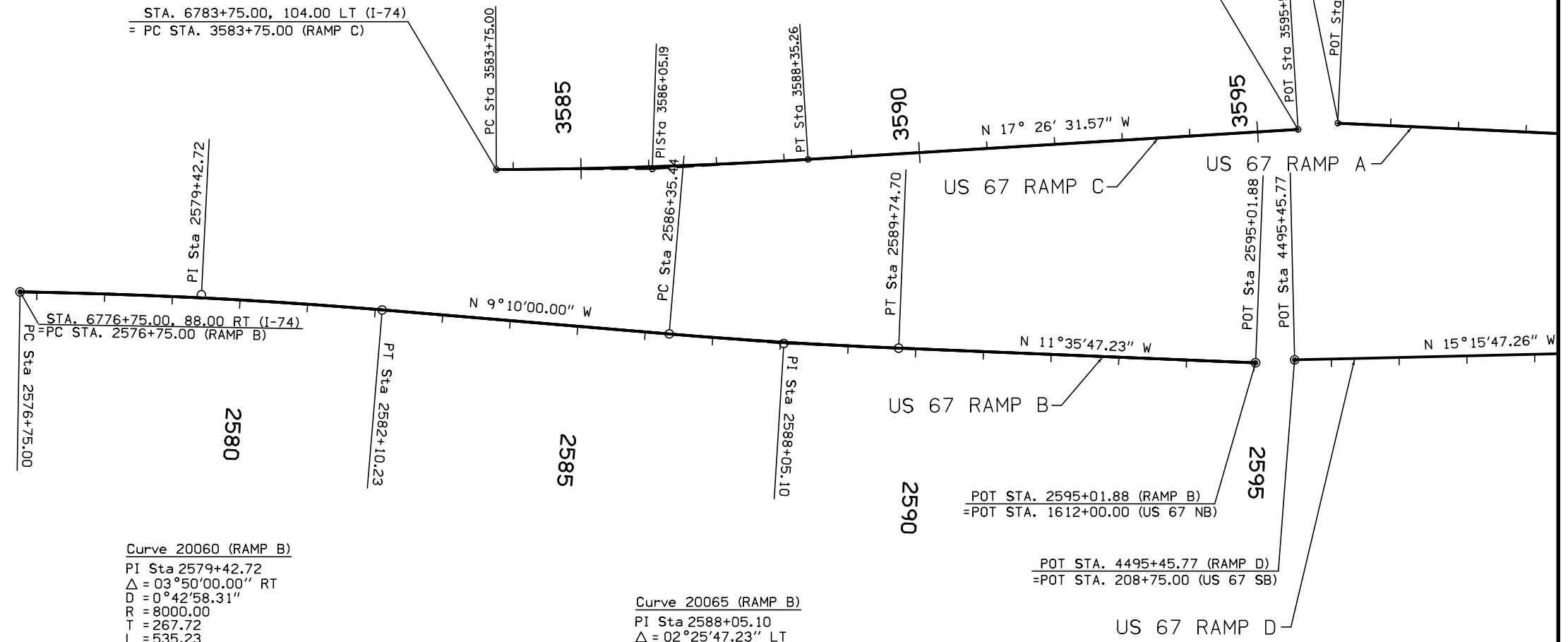
Curve 20110 (RAMP C)

PI Sta 3586+05.19  
Δ = 3° 17' 47" (LT)  
D = 0° 42' 58"  
R = 8,000.00'  
T = 230.19'  
L = 460.26'  
E = 3.31'  
e = N.C.  
L = NA  
x = NA  
m = NA

POT STA. 1496+21.97 (RAMP A)  
= POT STA. 205+20.00 (US 67 SB)

POT STA. 3595+59.96 (RAMP C)  
= POT STA. 1608+50.00 (US 67 NB)

STA. 6783+75.00, 104.00 LT (I-74)  
= PC STA. 3583+75.00 (RAMP C)



Curve 20060 (RAMP B)

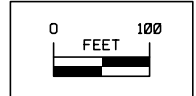
PI Sta 2579+42.72  
Δ = 03° 50' 00.00" RT  
D = 0° 42' 58.31"  
R = 8000.00  
T = 267.72  
L = 535.23  
E = 4.48  
e = N.C.  
L = NA  
x = NA  
m = NA

Curve 20065 (RAMP B)

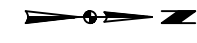
PI Sta 2588+05.10  
Δ = 02° 25' 47.23" LT  
D = 0° 42' 58.31"  
R = 8000.00  
T = 169.66  
L = 339.26  
E = 1.80  
e = N.C.  
L = NA  
x = NA  
m = NA

POT STA. 2595+01.88 (RAMP B)  
= POT STA. 1612+00.00 (US 67 NB)

POT STA. 4495+45.77 (RAMP D)  
= POT STA. 208+75.00 (US 67 SB)

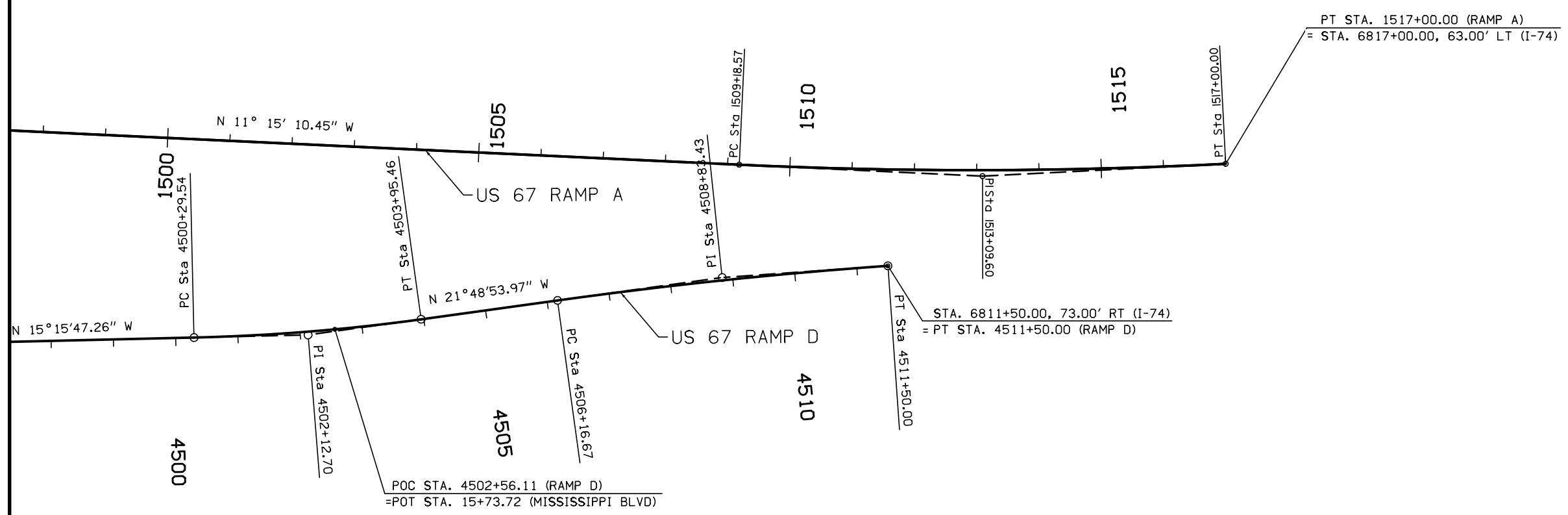


ALIGNMENTS  
US 67 Ramps



Curve 20010 (RAMP A)

PI Sta 1513+09.60  
Δ = 5° 35' 48" (LT)  
D = 0° 42' 58"  
R = 8,000.00'  
T = 391.03'  
L = 781.43'  
E = 9.55'  
e = N.C.  
L = NA  
x = NA  
m = NA

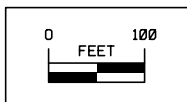


Curve 20155 (RAMP D)

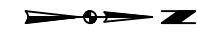
PI Sta 4502+12.70  
Δ = 06° 33' 06.71" LT  
D = 1° 47' 25.78"  
R = 3200.00  
T = 183.16  
L = 365.93  
E = 5.24  
e = 2.80  
L = 71.00  
x = 52.00  
m = 21.30

Curve 20160 (RAMP D)

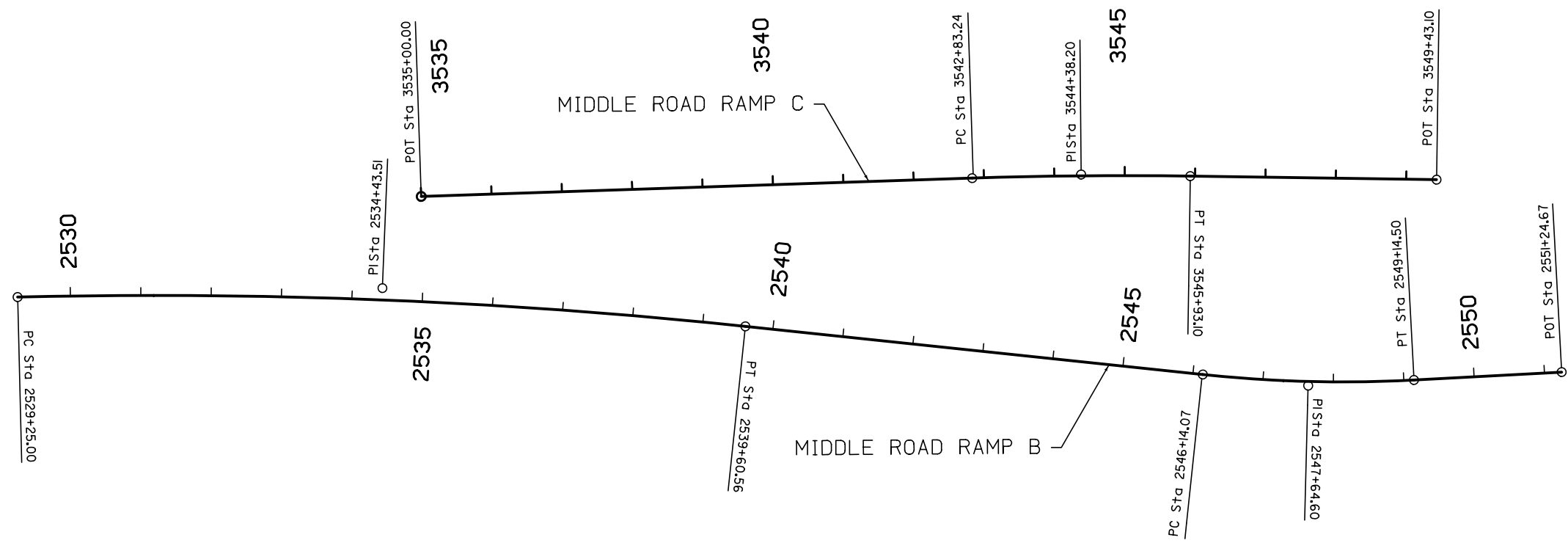
PI Sta 4508+83.43  
Δ = 03° 49' 10.99" RT  
D = 0° 42' 58.31"  
R = 8000.00  
T = 266.77  
L = 533.33  
E = 4.45  
e = N.C.  
L = NA  
x = NA  
m = NA



ALIGNMENTS  
US 67 Ramps

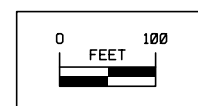


Curve 20315 (RAMP C)  
 PI Sta 3544+38.20  
 $\Delta = 2^\circ 43' 52.68''$  (RT)  
 $D = 0^\circ 52' 53.30''$   
 $R = 6,500.00$   
 $T = 154.96$   
 $L = 309.86$   
 $E = 1.85$   
 $e = \text{N.C.}$   
 $L = \text{NA}$   
 $x = \text{NA}$   
 $m = \text{NA}$



Curve 20265 (RAMP B)  
 PI Sta 2534+43.51  
 $\Delta = 7^\circ 25' 00.00''$  (RT)  
 $D = 0^\circ 42' 58.31''$   
 $R = 8,000.00$   
 $T = 518.51$   
 $L = 1,035.56$   
 $E = 16.79$   
 $e = \text{N.C.}$   
 $L = \text{NA}$   
 $x = \text{NA}$   
 $m = \text{NA}$

Curve 20275 (RAMP B)  
 PI Sta 2547+64.60  
 $\Delta = 9^\circ 03' 34.99''$  (LT)  
 $D = 3^\circ 00' 56.04''$   
 $R = 1,900.00$   
 $T = 150.53$   
 $L = 300.43$   
 $E = 5.95$   
 $e = \text{N.C.}$   
 $L = \text{NA}$   
 $x = \text{NA}$   
 $m = \text{NA}$



**ALIGNMENTS**  
 Middle Rd Ramps

**ALIGNMENT COORDINATES**

101-16  
10-20-09

Name	Location	Point on Tangent			Begin Spiral			Begin Curve			Simple Curve PI or Master PI of SCS			End Curve			End Spiral		
		Station	Coordinates		Station	Coordinates		Station	Coordinates		Station	Coordinates		Station	Coordinates		Station	Coordinates	
			Y (Northing)	X (Easting)		Y (Northing)	X (Easting)		Y (Northing)	X (Easting)		Y (Northing)	X (Easting)		Y (Northing)	X (Easting)		Y (Northing)	X (Easting)
21205	US 67 SOUTHBOUND	192+00.46	570,025.981	2,456,537.815															
21205							197+76.87	570,038.915	2,457,114.077	198+57.61	570,040.727	2,457,194.804	199+38.01	570,055.468	2,457,274.194				
21210							202+18.73	570,106.715	2,457,550.194	203+39.69	570,128.797	2,457,669.124	204+60.36	570,136.385	2,457,789.849				
21215							209+92.57	570,169.770	2,458,321.011	210+92.33	570,176.028	2,458,420.574	211+91.43	570,162.494	2,458,519.411				
21225							214+69.68	570,124.744	2,458,795.083	215+69.77	570,111.164	2,458,894.252	216+69.20	570,117.509	2,458,994.146				
21235		219+88.06	570,137.723	2,459,312.364															
	US 67 NORTHBOUND																		
21100		1591+28.39	569,602.502	2,456,273.908															
21100							1593+57.53	569,621.535	2,456,502.260	1596+09.69	569,642.479	2,456,753.540	1598+41.58	569,818.849	2,456,933.744				
21105							1598+98.53	569,858.684	2,456,974.445	1601+58.77	570,040.714	2,457,160.431	1603+96.85	570,057.039	2,457,420.160				
21115							1615+22.86	570,127.672	2,458,543.955	1617+13.20	570,139.612	2,458,733.922	1618+88.07	570,027.033	2,458,887.402				
21120							1620+94.69	569,904.824	2,459,054.011	1622+84.52	569,792.548	2,459,207.077	1624+58.99	569,804.128	2,459,396.552				
21130		1628+50.00	569,827.981	2,459,786.835															
	STATE STREET																		
22700		478+41.39	569,592.839	2,456,446.919															
22701		481+08.51	569,615.027	2,456,713.123															
22702		487+00.24	569,655.251	2,457,303.482															
22703		495+30.79	569,708.817	2,458,132.302															
22704		499+51.27	569,732.469	2,458,552.118															
22705		505+57.34	569,770.682	2,459,156.979															
	KIMBERLY ROAD																		
22507		4605+66.55	570,060.395	2,457,300.729															
22510							4607+38.27	570,225.879	2,457,346.563	4608+63.42	570,346.488	2,457,379.969	4609+78.65	570,418.570	2,457,482.275				
22515							4609+78.65	570,418.570	2,457,482.275	4611+69.83	570,528.686	2,457,638.562	4613+28.61	570,719.693	2,457,630.364				
22525							4616+65.09	571,055.857	2,457,615.937	4621+06.86	571,497.226	2,457,596.994	4624+86.45	571,752.215	2,457,236.236				
22530		4627+09.84	571,881.154	2,457,053.814															
22531		4628+14.84	571,941.759	2,456,968.070															
	MISSISSIPPI BOULEVARD																		
22405		6+60.00	570,807.419	2,457,096.238															
22406		8+60.00	570,812.549	2,457,296.172															
22410		11+90.00	570,834.875	2,457,625.421															
22415		16+76.67	570,848.205	2,458,111.902															
22420		21+30.02	570,876.535	2,458,564.364															
	BROWN STREET																		
22605		8+50.00	570,446.718	2,457,184.866															
22610		11+90.80	570,454.623	2,457,525.571															
	10TH STREET																		
22000		10100+00.00	569,621.314	2,456,499.608															
22005		10102+00.00	569,821.276	2,456,495.722															
	11TH STREET																		
22050		11100+00.00	569,842.226	2,456,957.629															
22050							11100+19.76	569,856.349	2,456,943.806	11100+58.91	569,884.329	2,456,916.421	11100+94.40	569,923.465	2,456,915.312				
22046		11102+05.36	570,034.383	2,456,912.171															
	12TH STREET																		
22504		4598+50.00	569,343.898	2,457,308.898															
22505		4601+61.40	569,655.251	2,457,303.482															
	13TH STREET																		
22056		13100+00.00	569,644.992	2,457,647.476															
22057							13101+46.55	569,791.420	2,457,641.538	13102+29.20	569,874.001	2,457,638.190	13103+10.22	569,950.650	2,457,607.278				
22058		13103+22.14	569,961.702	2,457,602.822															
	15TH STREET																		
22205		100+00.00	569,486.215	2,458,559.458															
22210		110+00.00	570,485.771	2,458,529.668															
	16TH STREET																		
22255		99+35.00	569,694.498	2,458,962.784															
22260		105+88.53	570,347.770	2,458,944.397															
	US 67 RAMP B																		
20060		2576+75.00	568,312.394	2,458,559.446															
20060							2576+75.00	568,312.394	2,458,559.446	2579+42.72	568,573.250	2,458,499.223	2582+10.23	568,837.548	2,458,456.574				
20065							2586+35.44	569,257.325	2,458,388.835	2588+05.10	569,424.815	2,458,361.808	2589+74.70	569,591.009	2,458,327.704				
20070		2595+01.88	570,107.420	2,458,221.733															

**ALIGNMENT COORDINATES**

101-16  
10-20-09

Name	Location	Point on Tangent			Begin Spiral			Begin Curve			Simple Curve PI or Master PI of SCS			End Curve			End Spiral		
		Station	Coordinates		Station	Coordinates		Station	Coordinates		Station	Coordinates		Station	Coordinates		Station	Coordinates	
			Y (Northing)	X (Easting)		Y (Northing)	X (Easting)		Y (Northing)	X (Easting)		Y (Northing)	X (Easting)		Y (Northing)	X (Easting)		Y (Northing)	X (Easting)
25091	CL EXISTING GRANT ST.	192+00.46	570,049.975	2,456,537.277															
25092		207+70.61	570,085.208	2,458,107.029															
25093		219+76.85	570,161.674	2,459,310.842															
25030	CL EXISTING 12TH ST.	4601+61.40	569,655.251	2,457,303.482															
25031		4616+29.08	571,122.513	2,457,268.347															
25035	CL EXISTING 16TH ST.	0+00.00	569,759.472	2,458,960.955															
25036		5+88.53	570,347.770	2,458,944.397															
25012	CL EXISTING 13TH ST.	422+15.09	569,677.428	2,457,646.621															
25015								423+29.77	569,792.003	2,457,641.707	424+11.84	569,874.001	2,457,638.190	424+92.33	569,950.117	2,457,607.493			
25016								426+59.06	570,104.746	2,457,545.135	428+40.69	570,273.194	2,457,477.204	430+06.17	570,444.203	2,457,538.405			
25017								431+55.95	570,585.219	2,457,588.872	432+45.93	570,669.940	2,457,619.192	433+33.83	570,759.885	2,457,616.594			
25013		434+08.61	570,834.632	2,457,614.435															
25018	CL EXISTING CALVERT DR.																		
25014		440+70.13	571,215.156	2,458,109.560				434+68.69	570,894.441	2,457,620.893	435+53.25	570,978.441	2,457,629.981	436+23.34	571,015.867	2,457,705.806			
25040	CL EXISTING KIMBERLY ROAD WB							0+00.00	570,845.530	2,458,102.914	1+79.78	571,022.398	2,458,070.703	3+56.49	571,180.216	2,457,984.602			
25041								3+79.38	571,200.305	2,457,973.642	7+14.96	571,494.899	2,457,812.919	10+31.37	571,652.064	2,457,516.412			
25042		13+38.08	571,795.707	2,457,245.414															
25045	CL EXISTING KIMBERLY ROAD EB							0+00.00	571,709.971	2,457,248.582	4+13.26	571,475.474	2,457,588.863	7+65.40	571,062.436	2,457,602.248			
25046		9+93.43	570,834.525	2,457,609.633															
25050	TEMPORARY CONNECTION 5	72000+00.00	571,035.381	2,457,603.124															
25051		72001+82.26	571,054.641	2,457,784.360															
25045	TEMPORARY CONNECTION 9							21000+00.00	570,245.004	2,457,567.103	21000+52.44	570,193.568	2,457,577.308	21000+90.01	570,188.905	2,457,629.540			
25046		21002+48.53	570,174.810	2,457,787.434															
25050	TEMPORARY CONNECTION 10	41209+00.00	570,163.712	2,458,224.627															
25051		41213+02.01	570,553.068	2,458,124.576															
25022	CL EXISTING 14TH ST.	0+00.00	569,709.230	2,458,138.897															
25023		3+77.75	570,086.957	2,458,134.570															
25024		7+57.00	570,466.117	2,458,126.618															
25025		7+99.55	570,508.663	2,458,125.599															
25026		12+21.53	570,930.528	2,458,115.878															
25027		15+11.10	571,220.022	2,458,109.452															
25060	TEMPORARY CONNECTION 11	12591+08.56	569,726.896	2,458,160.410															
25061		12594+08.53	570,015.980	2,458,240.497															

**ALIGNMENT COORDINATES**

101-16  
10-20-09

Name	Location	Point on Tangent			Begin Spiral			Begin Curve			Simple Curve PI or Master PI of SCS			End Curve			End Spiral		
		Station	Coordinates		Station	Coordinates		Station	Coordinates		Station	Coordinates		Station	Coordinates		Station	Coordinates	
			Y (Northing)	X (Easting)		Y (Northing)	X (Easting)		Y (Northing)	X (Easting)		Y (Northing)	X (Easting)		Y (Northing)	X (Easting)		Y (Northing)	X (Easting)
POT 21009	I-74 MAINLINE	6719+17.38	562672.89	2459725.37															
CURVE 21010	I-74 MAINLINE						6742+96.78	565000.29	2459230.66	6744+71.31	565171.01	2459194.38	6746+45.84	565341.08	2459155.11				
CURVE 21015	I-74 MAINLINE						6796+70.83	570237.28	2458024.74	6801+41.31	570695.70	2457918.90	6806+11.44	571145.99	2457782.53				
CURVE 21020	I-74 MAINLINE						6823+21.29	572782.43	2457286.91	6832+57.30	573678.26	2457015.60	6841+92.28	574593.17	2456817.98				
POT 21001	I-74 EB	26722+61.55	563004.32	2459640.61															
CURVE 21003	I-74 EB						26742+99.14	564997.38	2459216.97	26747+52.49	565440.83	2459122.71	26752+05.69	565879.54	2459008.44				
CURVE 21005	I-74 EB						26755+79.54	566241.33	2458914.21	26758+58.62	566511.40	2458843.87	26761+37.66	566783.32	2458781.09				
CURVE 21017	I-74 EB						26800+44.27	570589.80	2457902.30	26805+14.75	571048.23	2457796.46	26809+84.88	571498.51	2457660.09				
CURVE 21022	I-74 EB						26823+21.85	572778.08	2457272.56	26832+58.47	573674.49	2457001.07	26841+94.06	574590.00	2456803.32				
POT 21000	I-74 WB	16722+56.30	563010.14	2459668.00															
CURVE 21001	I-74 WB						16757+83.78	566460.53	2458934.59	16759+58.32	566631.26	2458898.31	16761+32.85	566801.32	2458859.04				
CURVE 21016	I-74 WB						16792+97.39	569884.75	2458147.18	16797+67.87	570343.18	2458041.34	16802+38.00	570793.46	2457904.97				
CURVE 21021	I-74 WB						16823+20.73	572786.78	2457301.27	16832+56.13	573682.02	2457030.14	16841+90.50	574596.34	2456832.64				
POT 20005	RAMP A	1496+21.97	570140.13	2457849.37															
CURVE 20010	RAMP A						1509+18.57	571411.80	2457596.35	1513+09.60	571795.31	2457520.05	1517+00.00	572169.55	2457406.70				
POT 20010	RAMP A	1517+00.00	572169.55	2457406.70															
POT 20060	RAMP B	2576+75.00	568312.39	2458559.45															
CURVE 20060	RAMP B						2576+75.00	568312.39	2458559.45	2579+42.72	568573.25	2458499.22	2582+10.23	568837.55	2458456.57				
CURVE 20065	RAMP B						2586+35.44	569257.33	2458388.84	2588+05.10	569424.82	2458361.81	2589+74.70	569591.01	2458327.70				
POT 20070	RAMP B	2595+01.88	570107.42	2458221.73															
POT 20110	RAMP C	3583+75.00	568951.26	2458214.90															
CURVE 20110	RAMP C						3583+75.00	568951.26	2458214.90	3586+05.19	569174.47	2458158.65	3588+35.26	569394.08	2458089.65				
POT 20115	RAMP C	3595+59.96	570085.46	2457872.42															
POT 20150	RAMP D	4495+45.77	570162.40	2458203.67															
CURVE 20155	RAMP D						4500+29.54	570629.09	2458076.32	4502+12.70	570805.80	2458028.10	4503+95.46	570975.84	2457960.04				
CURVE 20160	RAMP D						4506+16.67	571181.20	2457877.84	4508+83.43	571428.87	2457778.70	4511+50.00	571682.58	2457696.29				
POT 20160	RAMP D	4511+50.00	571682.58	2457696.29															
POT RRDG1	RAMP RD-G	120+00.00	566229.23	2458867.76															
CURVE RRD-G-1	RAMP RD-G						123+97.74	565839.53	2458947.34	125+86.43	565654.65	2458985.09	127+75.08	565468.49	2459015.84				
POT RRDG2	RAMP RD-G	137+09.35	564546.70	2459168.10															
POT RRDH1	RAMP RD-H	210+00.00	564718.25	2459496.40															
CURVE 3RH IL-1	RAMP RD-H						216+71.36	565354.44	2459281.94	220+64.68	565727.15	2459156.30	224+57.36	566110.38	2459067.82				
POT RRDH2	RAMP RD-H	231+77.55	566812.12	2458905.81															
POT RSIXD1	RAMP 6TH-D	420+00.00	563503.63	2459677.85															
CURVE 6RD IL-1	RAMP 6TH-D						421+00.39	563602.38	2459659.74	421+45.59	563646.83	2459651.59	421+90.74	563690.55	2459640.12				
CURVE 6RD IL-2	RAMP 6TH-D						433+38.49	564800.71	2459348.80	434+74.45	564932.22	2459314.29	436+10.40	565064.62	2459283.36				
POT RSIXD2	RAMP 6TH-D	443+56.73	565791.39	2459113.63															
POT RW200	RET WALL 165	11503+85.96	570886.40	2457684.57															
POT RW201	RET WALL 165	11504+40.00	570939.40	2457674.02															
POT RW202	RET WALL 165	11504+60.00	570959.31	2457672.10															
CURVE RW165 1	RET WALL 165						11509+18.57	571409.06	2457582.62	11509+95.78	571484.79	2457567.55	11510+72.99	571560.22	2457551.02				

**SPIRAL OR CIRCULAR CURVE DATA**

101-17  
04-19-11

Name	Location	$\Delta_{scs}$	Horizontal Alignment Data												Remarks		
			Spiral Data						Curve Data								
			$\theta_s$	Ls	Ts	Es	Xc	Yc	L.T.	S.T.	$\Delta_c$	T	L	R		E	
CURVE 21010	I-74 MAINLINE												1°00'00.00" LT	174.54'	349.07'	20,000.00'	0.76'
CURVE 21015	I-74 MAINLINE												3°50'58.23" LT	470.48'	940.61'	14,000.00'	7.90'
CURVE 21020	I-74 MAINLINE												4°39'39.15" RT	936.01'	1,871.00'	23,000.00'	19.04'
CURVE 21003	I-74 EB												2°35'56.05" LT	453.35'	906.55'	19,986.00'	5.14'
CURVE 21005	I-74 EB												1°35'56.05" RT	279.08'	558.12'	20,000.00'	1.95'
CURVE 21017	I-74 EB												3°50'58.22" LT	470.48'	940.61'	14,000.00'	7.90'
CURVE 21022	I-74 EB												4°39'39.15" RT	936.62'	1,872.22'	23,015.00'	19.05'
CURVE 21001	I-74 WB												1°00'00.00" LT	174.54'	349.07'	20,000.00'	0.76'
CURVE 21016	I-74 WB												3°50'58.24" LT	470.48'	940.61'	14,000.00'	7.90'
CURVE 21021	I-74 WB												4°39'39.15" RT	935.40'	1,869.78'	22,985.00'	19.03'
CURVE 20010	RAMP A												5°35'47.78" LT	391.03'	781.43'	8,000.00'	9.55'
CURVE 20060	RAMP B												3°50'00.00" RT	267.72'	535.23'	8,000.00'	4.48'
CURVE 20065	RAMP B												2°25'47.23" LT	169.66'	339.26'	8,000.00'	1.80'
CURVE 20110	RAMP C												3°17'46.82" LT	230.19'	460.26'	8,000.00'	3.31'
CURVE 20155	RAMP D												6°33'06.71" LT	183.16'	365.93'	3,200.00'	5.24'
CURVE 20160	RAMP D												3°49'10.99" RT	266.77'	533.33'	8,000.00'	4.45'
CURVE RRD-G-1	RAMP RD-G												2°09'43.09" RT	188.69'	377.33'	10,000.00'	1.78'
CURVE 3RHIL-1	RAMP RD-H												5°37'45.59" RT	393.32'	786.00'	8,000.00'	9.66'
CURVE 6RDIL-1	RAMP 6TH-D												4°18'49.27" LT	45.19'	90.35'	1,200.00'	0.85'
CURVE 6RDIL-2	RAMP 6TH-D												1°33'28.68" RT	135.97'	271.91'	10,000.00'	0.92'
CURVE RW165 1	RET WALL 165												1°06'28.57" LT	77.21'	154.42'	7,986.00'	0.37'

**TRAFFIC CONTROL PLAN**

Grant St. Stage 1 - See Sheet Nos. J.3 - J.5 for details.  
 Kimberly Road SB & 13th St. Stage 1 - See Sheet Nos. J.6 - J.7 for details.  
 Kimberly Road Stage 1 - See Sheet No. J.8 for details.

Grant St. Stage 2A - See Sheet Nos. J.10 - J.12 for details.  
 Kimberly Road & 13th St. Stage 2A - See Sheet No. J.13 for details.  
 Brown St. Stage 2A - See Sheet No. J.14 for details.  
 Kimberly Road Stage 2A - See Sheet Nos. J.15 - J.17 for details.

Grant St. Stage 2B - See Sheet Nos. J.19 - J.21 for details.  
 Mississippi Blvd. Stage 2B - See Sheet No. J.22 for details.  
 Brown St. Stage 2B - See Sheet No. J.23 for details.

Kimberly Road Stage 2C - See Sheet Nos. J.25 - J.26 for details.  
 Grant St. Stage 2C - See Sheet No. J.27 for details.

Grant St. Stage 3 - See Sheet Nos. J.29 - J.31 for details.

Grant St. Stage 4 - See Sheet Nos. J.33 - J.35.

State St. Stage 5 - See Sheet Nos. J.37 - J.39

14th St. Stage 6 - See Sheet No. J.41

State St. Stage 7 - See Sheet Nos. J.43 - J.45

State St. Stage 8 - See Sheet Nos. J.47 - J.49

**General Staging Notes:**  
 Contractor must maintain two lanes of traffic on US 67, in both directions, at all times.  
 Contact the Bettendorf City Administrator 48 hours prior to any road or sidewalk closure.  
 Pedestrian access will be maintained as shown on the J sheets.  
 Pedestrian access on all routes will be maintained to full ADA standards.

In the event of extreme high water, the City of Bettendorf may temporarily place sandbag dikes along 10th Street between Grant Street and State Street. Grant Street and State Street would have to be closed to traffic during extreme high waters. The Contractor will fully cooperate with the City including suspension of work and the removal or relocation of all lane closures, construction equipment, and traffic control devices as requested by the City.

**STAGING NOTES**

For complete Staging Notes, refer to sheets showing individual stages.

**TABULATION OF SPECIAL EVENTS**

Event	Location	Date
Ride the River Bicycle Ride	Quad Cities Riverfront	June 14, 2015
Independence Day Events	Bettendorf	July 3 to 5, 2015
Deere Classic PGA Tournament	Quad City Area	July 2015
Quad City Marathon	Quad City Riverfront	September 2015



**LEGEND**

- PROPOSED PAVEMENT HMA/PCC
- PREVIOUSLY CONSTRUCTED PAVEMENT
- PROPOSED TEMPORARY PAVEMENT
- TEMPORARY PAVEMENT
- PAVEMENT REMOVAL
- TEMPORARY ROAD CLOSURE
- ROAD CLOSURE
- DIRECTION OF TRAFFIC PER LANE

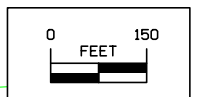
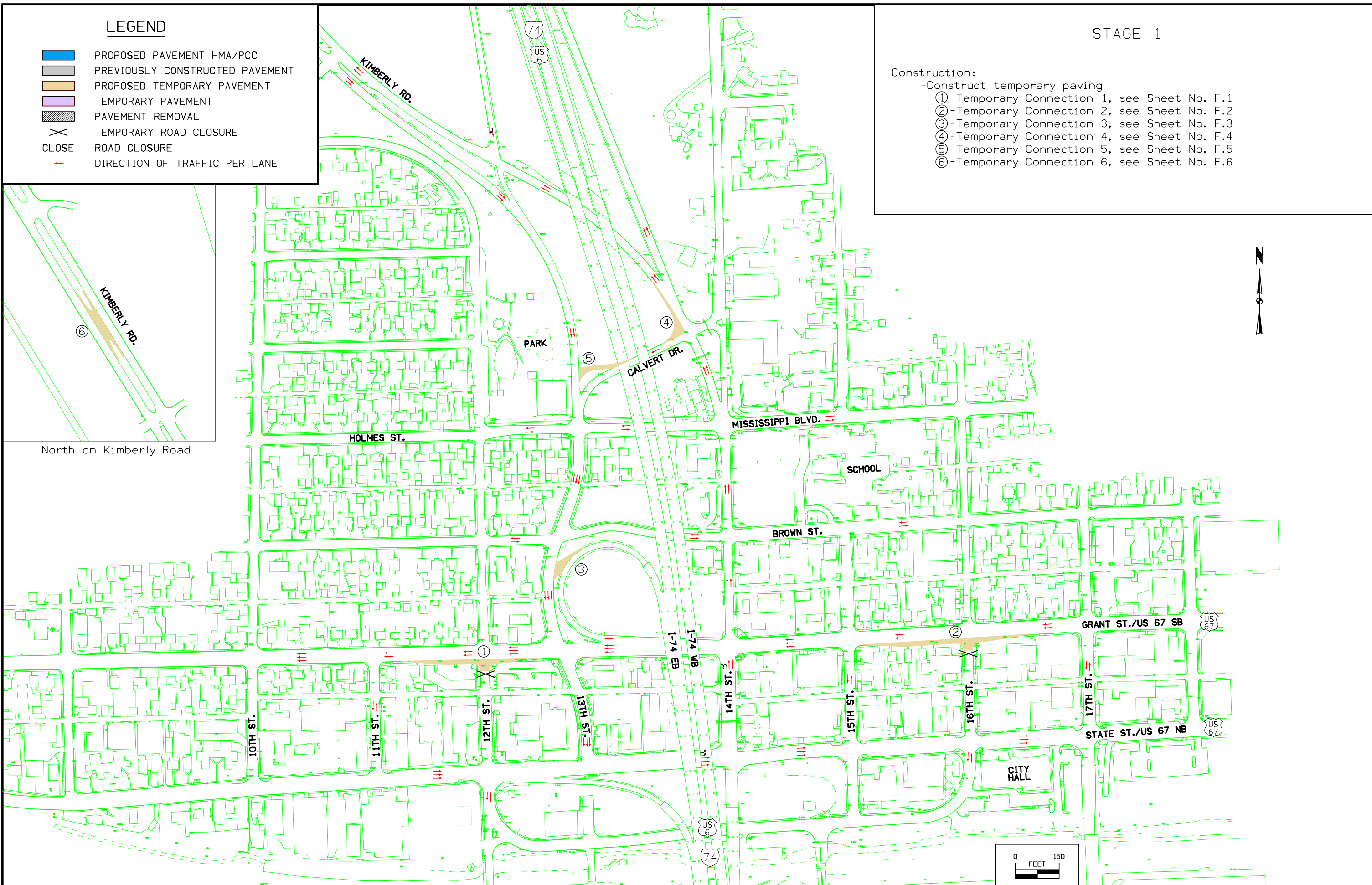
STAGE 1

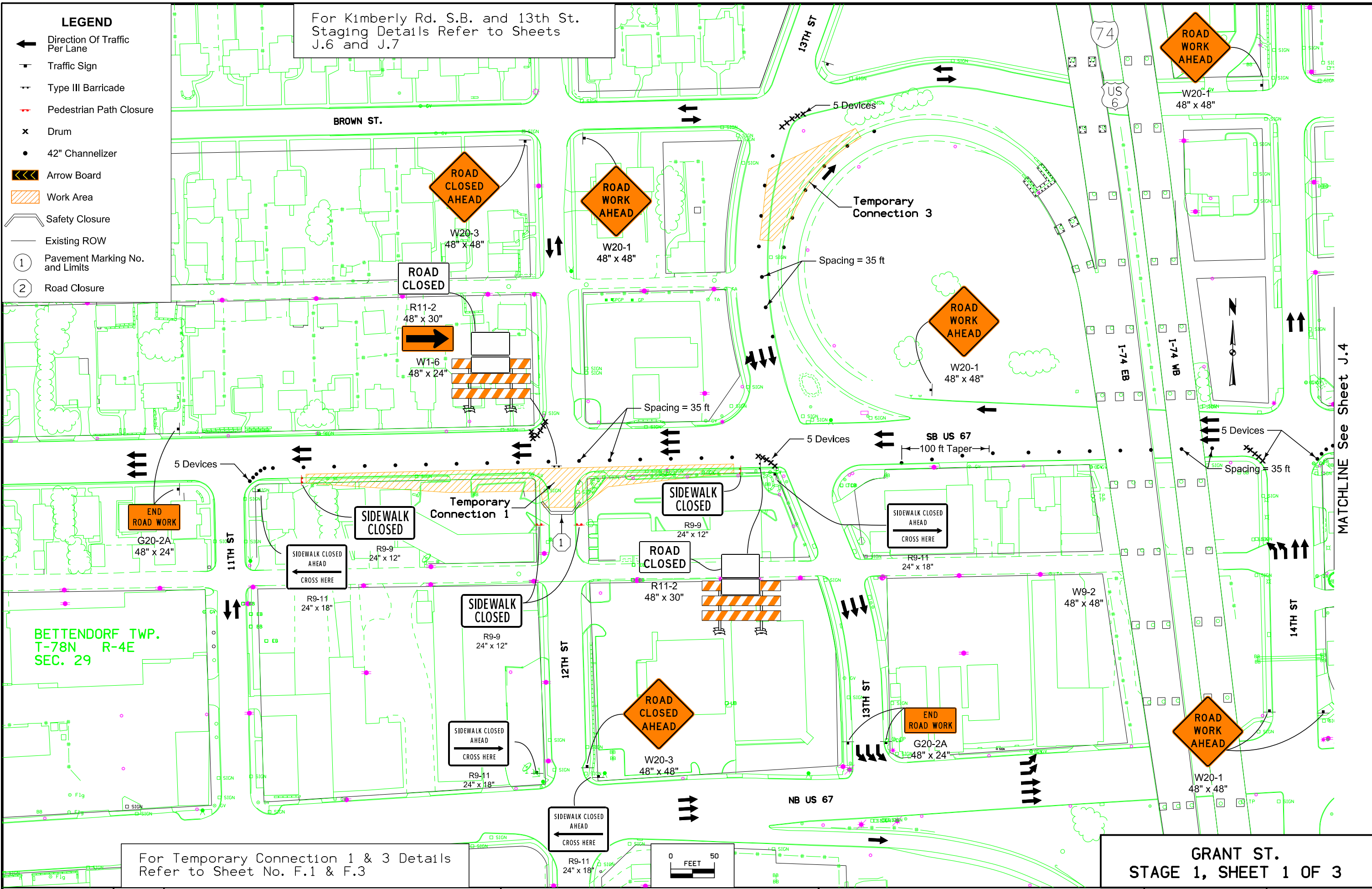
Construction:

- Construct temporary paving
- ①-Temporary Connection 1, see Sheet No. F.1
- ②-Temporary Connection 2, see Sheet No. F.2
- ③-Temporary Connection 3, see Sheet No. F.3
- ④-Temporary Connection 4, see Sheet No. F.4
- ⑤-Temporary Connection 5, see Sheet No. F.5
- ⑥-Temporary Connection 6, see Sheet No. F.6



North on Kimberly Road

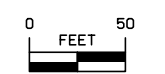




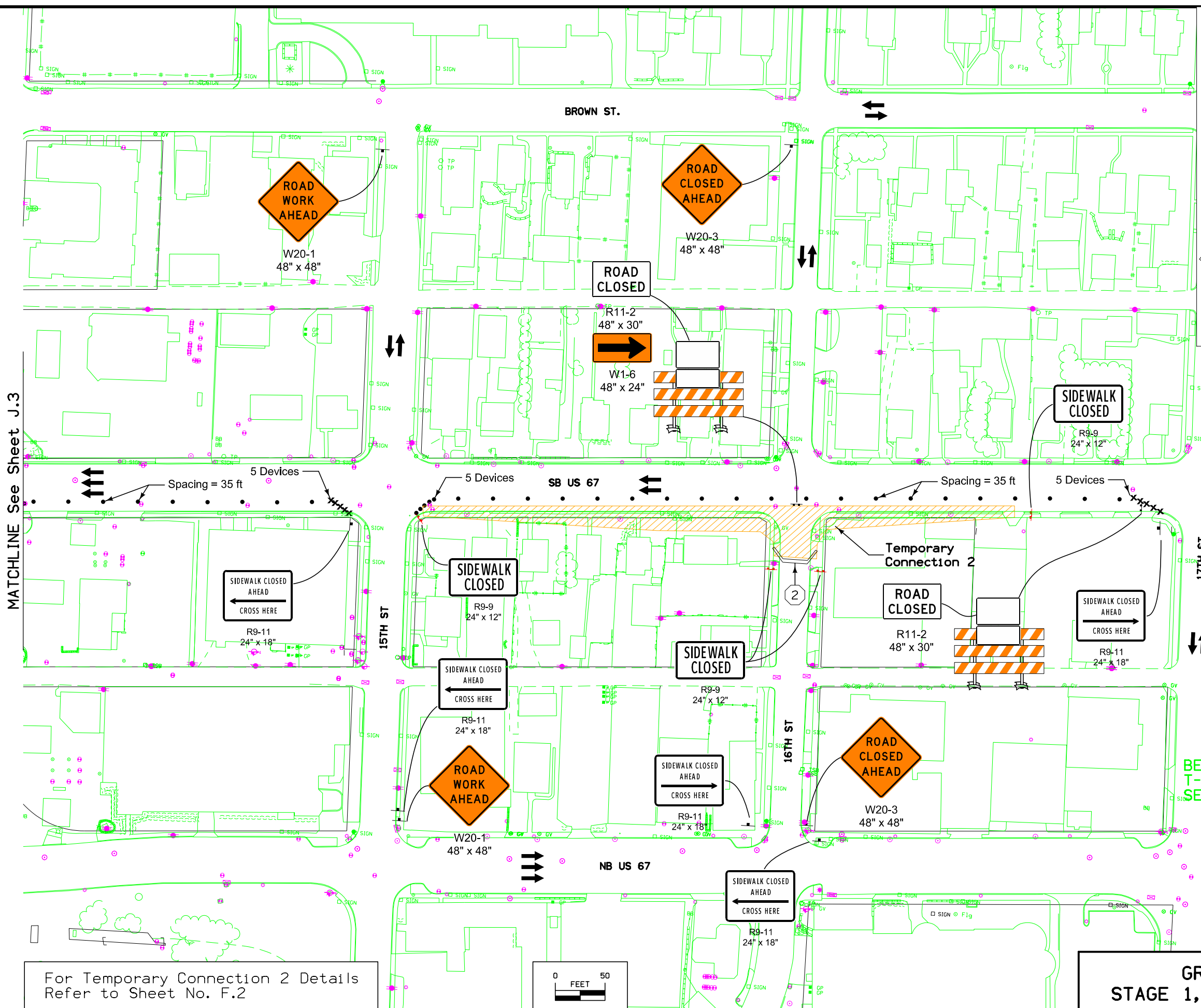
For Kimberly Rd. S.B. and 13th St.  
Staging Details Refer to Sheets  
J.6 and J.7

- LEGEND**
- ← Direction Of Traffic Per Lane
  - ⊣ Traffic Sign
  - Type III Barricade
  - Pedestrian Path Closure
  - x Drum
  - 42" Channelizer
  - ▶▶▶ Arrow Board
  - ▨ Work Area
  - Safety Closure
  - Existing ROW
  - ① Pavement Marking No. and Limits
  - ② Road Closure

For Temporary Connection 1 & 3 Details  
Refer to Sheet No. F.1 & F.3



**GRANT ST.**  
**STAGE 1, SHEET 1 OF 3**

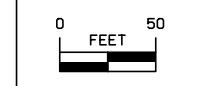


- LEGEND**
- ← Direction Of Traffic Per Lane
  - ⊥ Traffic Sign
  - Type III Barricade
  - Pedestrian Path Closure
  - x Drum
  - 42" Channelizer
  - ▬▬▬ Arrow Board
  - ▨ Work Area
  - ⌒ Safety Closure
  - Existing ROW
  - ① Pavement Marking No. and Limits
  - ② Road Closure

MATCHLINE See Sheet J.3

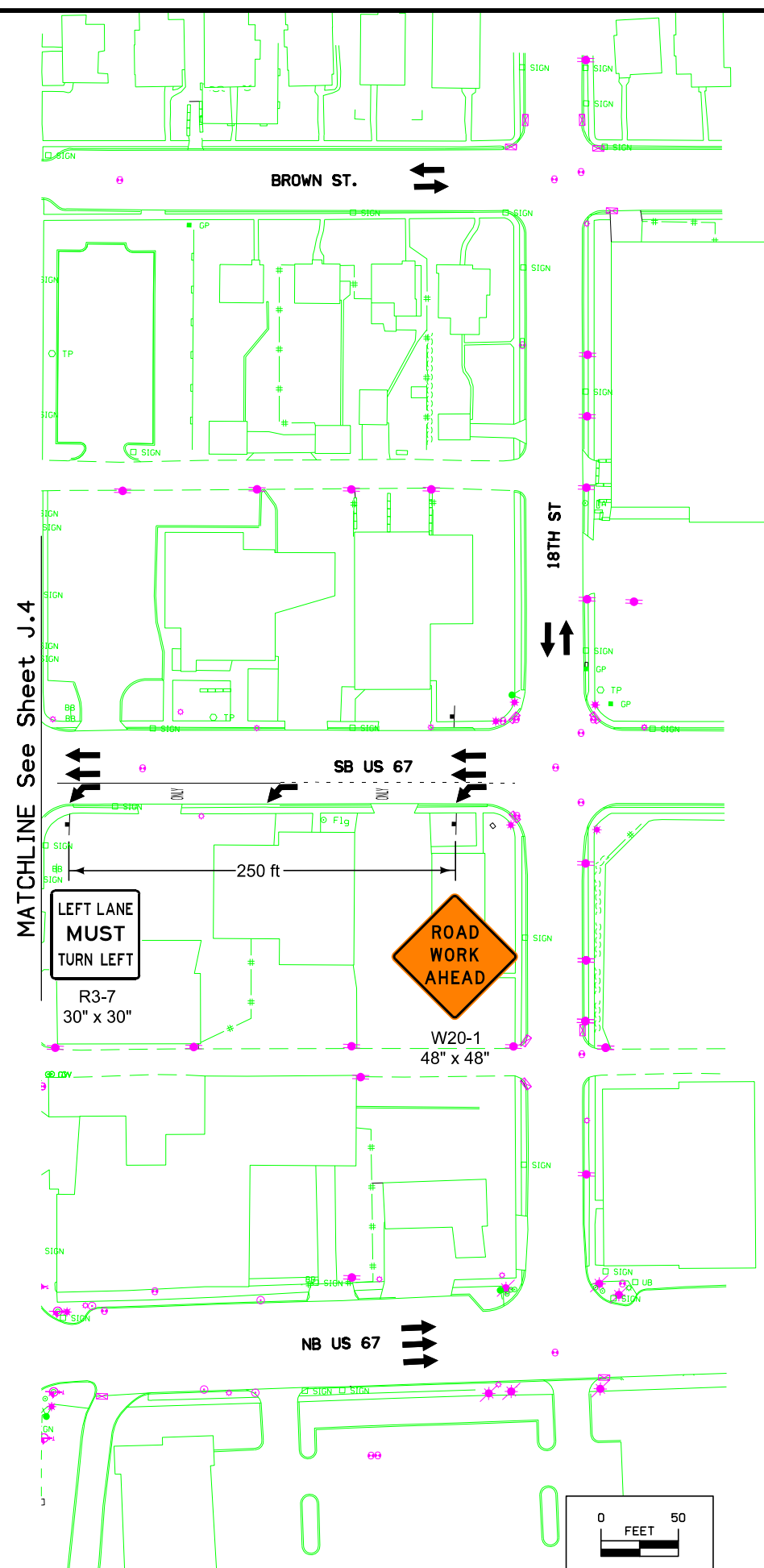
MATCHLINE See Sheet J.5

For Temporary Connection 2 Details Refer to Sheet No. F.2



**GRANT ST.  
STAGE 1, SHEET 2 OF 3**

BETTENDORF TWP.  
T-78N R-4E  
SEC. 28



**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊣ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ◀◀◀ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure



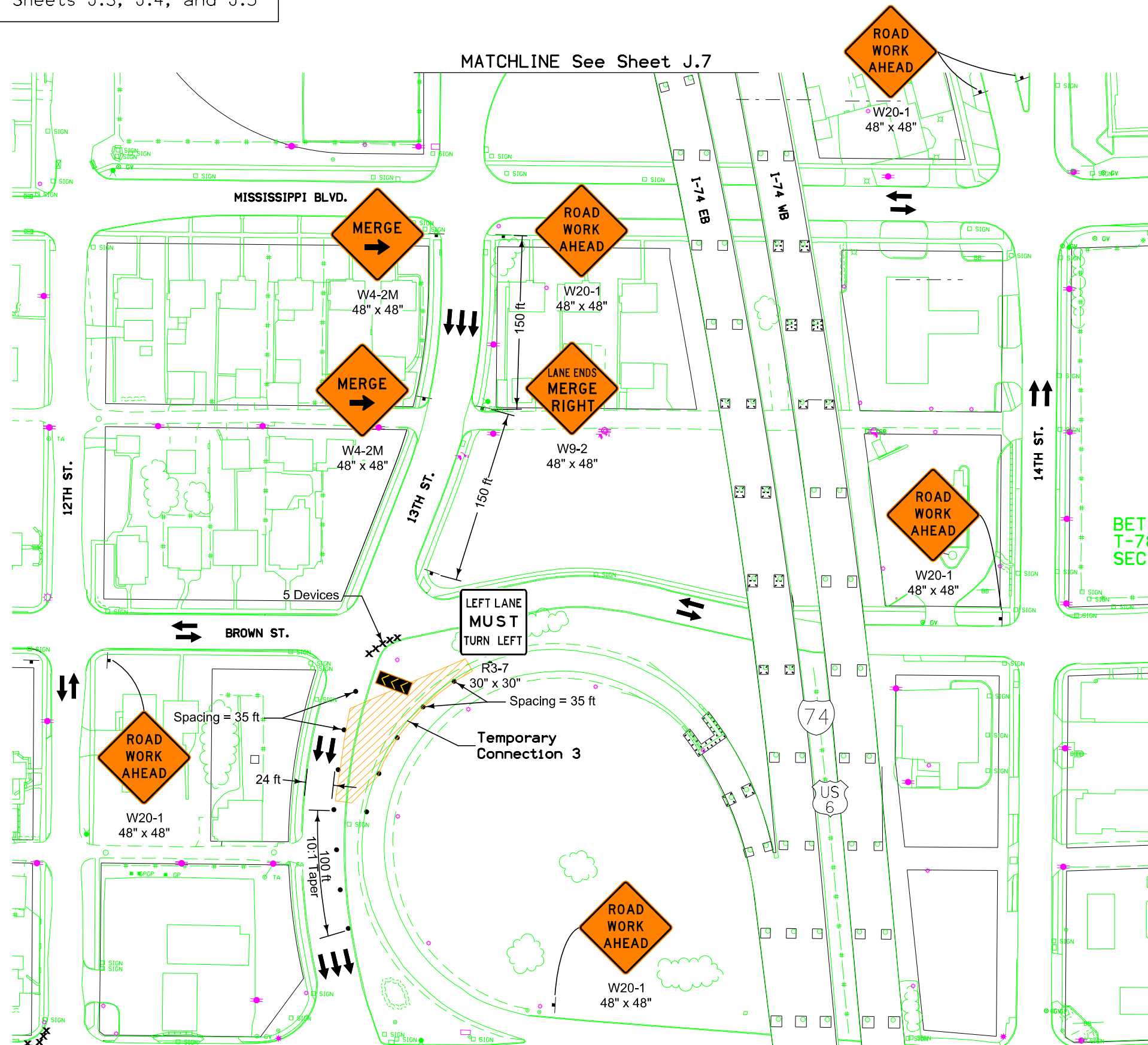
BETTENDORF TWP.  
T-78N R-4E  
SEC. 28

**GRANT ST.  
STAGE 1, SHEET 3 OF 3**

For Grant St. Staging Details  
Refer to Sheets J.3, J.4, and J.5

**LEGEND**

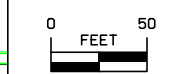
- ← Direction Of Traffic Per Lane
- ⊣ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ➡➡➡ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure



BETTENDORF TWP.  
T-78N R-4E  
SEC. 28

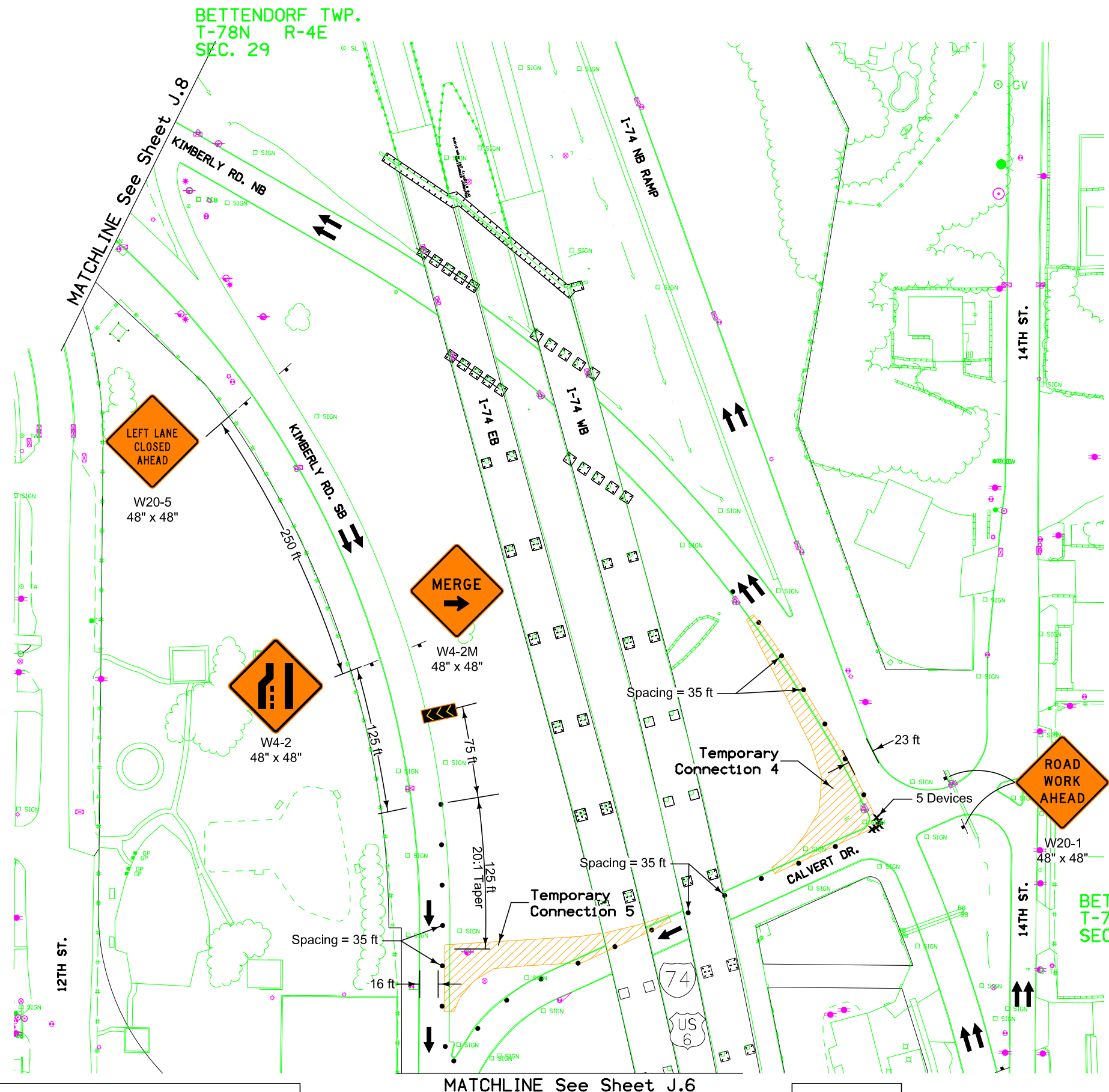
BETTENDORF TWP.  
T-78N R-4E  
SEC. 29

For Temporary Connection 3 Details  
Refer to Sheet No. F.3

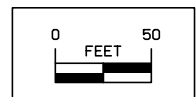


KIMBERLY RD. & 13TH ST.  
STAGE 1, SHEET 1 OF 3

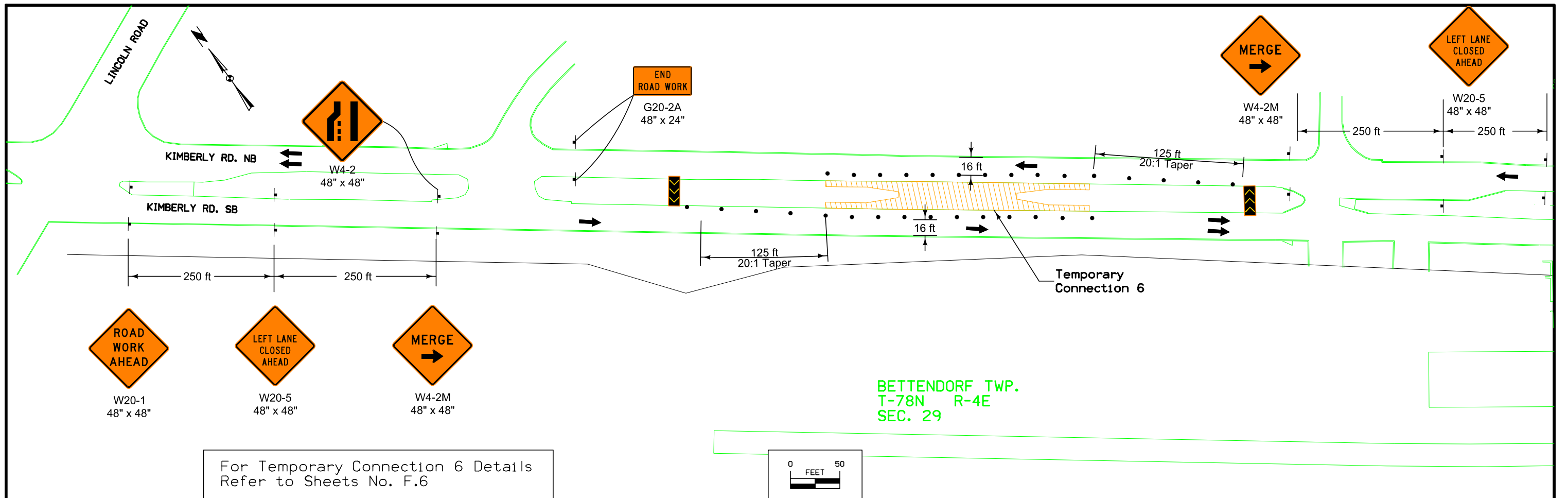
LEGEND	
	Direction Of Traffic Per Lane
	Traffic Sign
	Type III Barricade
	Pedestrian Path Closure
	Drum
	42" Channelizer
	Arrow Board
	Work Area
	Safety Closure
	Existing ROW
	Pavement Marking No. and Limits
	Road Closure



For Temporary Connections 4 & 5 Details Refer to Sheets No. F.4 & F.5



KIMBERLY RD. & 13TH ST.  
STAGE 1, SHEET 2 OF 3



LEGEND	
←	Direction Of Traffic Per Lane
⊣	Traffic Sign
⚡	Type III Barricade
⚡	Pedestrian Path Closure
x	Drum
•	42" Channelizer
↔	Arrow Board
▨	Work Area
⌒	Safety Closure
—	Existing ROW
①	Pavement Marking No. and Limits
②	Road Closure

KIMBERLY RD.  
STAGE 1, SHEET 1 OF 1

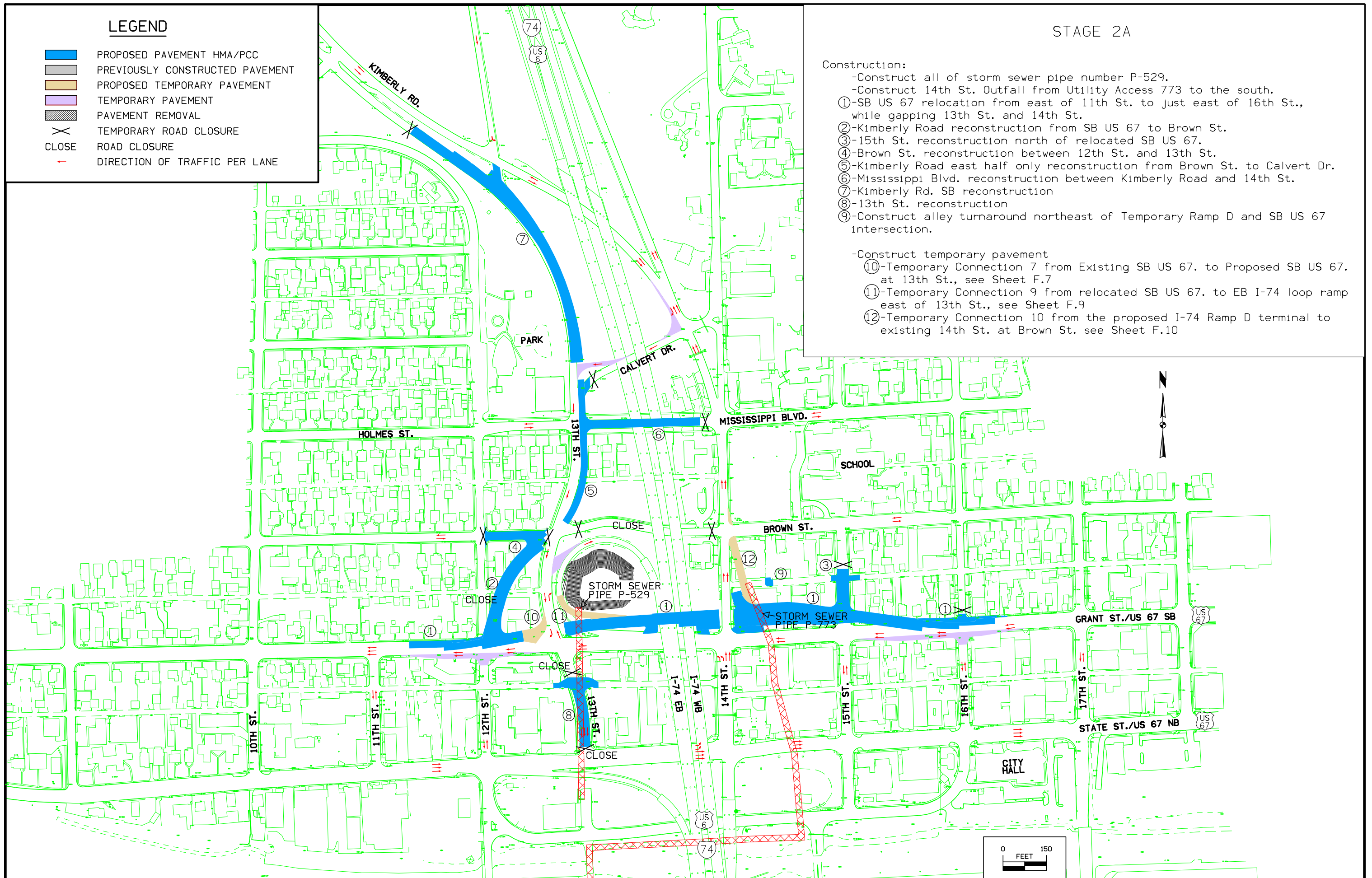
**LEGEND**

- PROPOSED PAVEMENT HMA/PCC
- PREVIOUSLY CONSTRUCTED PAVEMENT
- PROPOSED TEMPORARY PAVEMENT
- TEMPORARY PAVEMENT
- PAVEMENT REMOVAL
- TEMPORARY ROAD CLOSURE
- ROAD CLOSURE
- DIRECTION OF TRAFFIC PER LANE

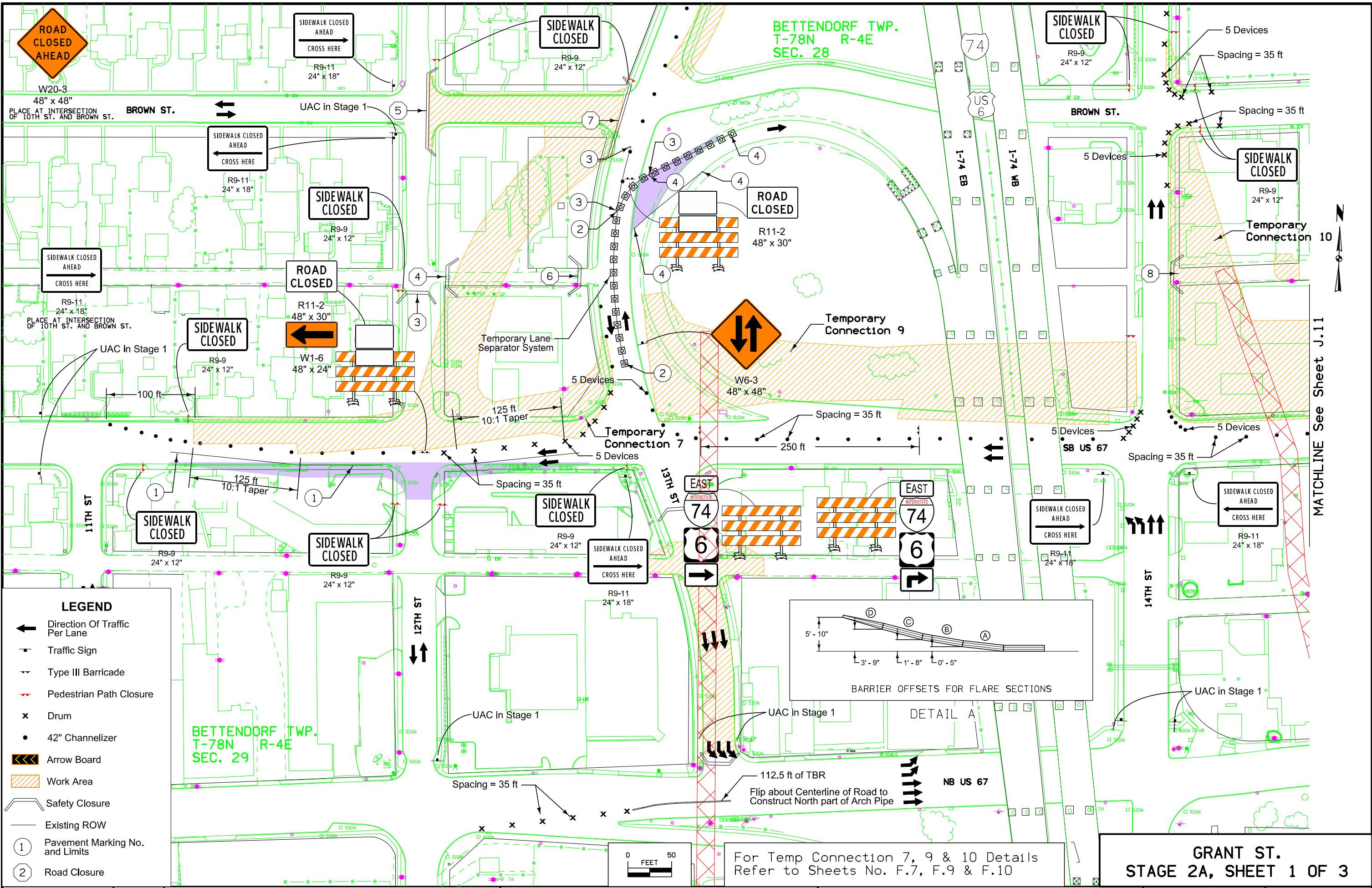
STAGE 2A

Construction:

- Construct all of storm sewer pipe number P-529.
- Construct 14th St. Outfall from Utility Access 773 to the south.
- ①-SB US 67 relocation from east of 11th St. to just east of 16th St., while gapping 13th St. and 14th St.
- ②-Kimberly Road reconstruction from SB US 67 to Brown St.
- ③-15th St. reconstruction north of relocated SB US 67.
- ④-Brown St. reconstruction between 12th St. and 13th St.
- ⑤-Kimberly Road east half only reconstruction from Brown St. to Calvert Dr.
- ⑥-Mississippi Blvd. reconstruction between Kimberly Road and 14th St.
- ⑦-Kimberly Rd. SB reconstruction
- ⑧-13th St. reconstruction
- ⑨-Construct alley turnaround northeast of Temporary Ramp D and SB US 67 intersection.
- Construct temporary pavement
- ⑩-Temporary Connection 7 from Existing SB US 67. to Proposed SB US 67. at 13th St., see Sheet F.7
- ⑪-Temporary Connection 9 from relocated SB US 67. to EB I-74 loop ramp east of 13th St., see Sheet F.9
- ⑫-Temporary Connection 10 from the proposed I-74 Ramp D terminal to existing 14th St. at Brown St. see Sheet F.10

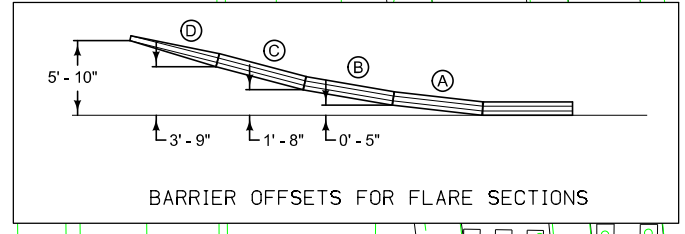






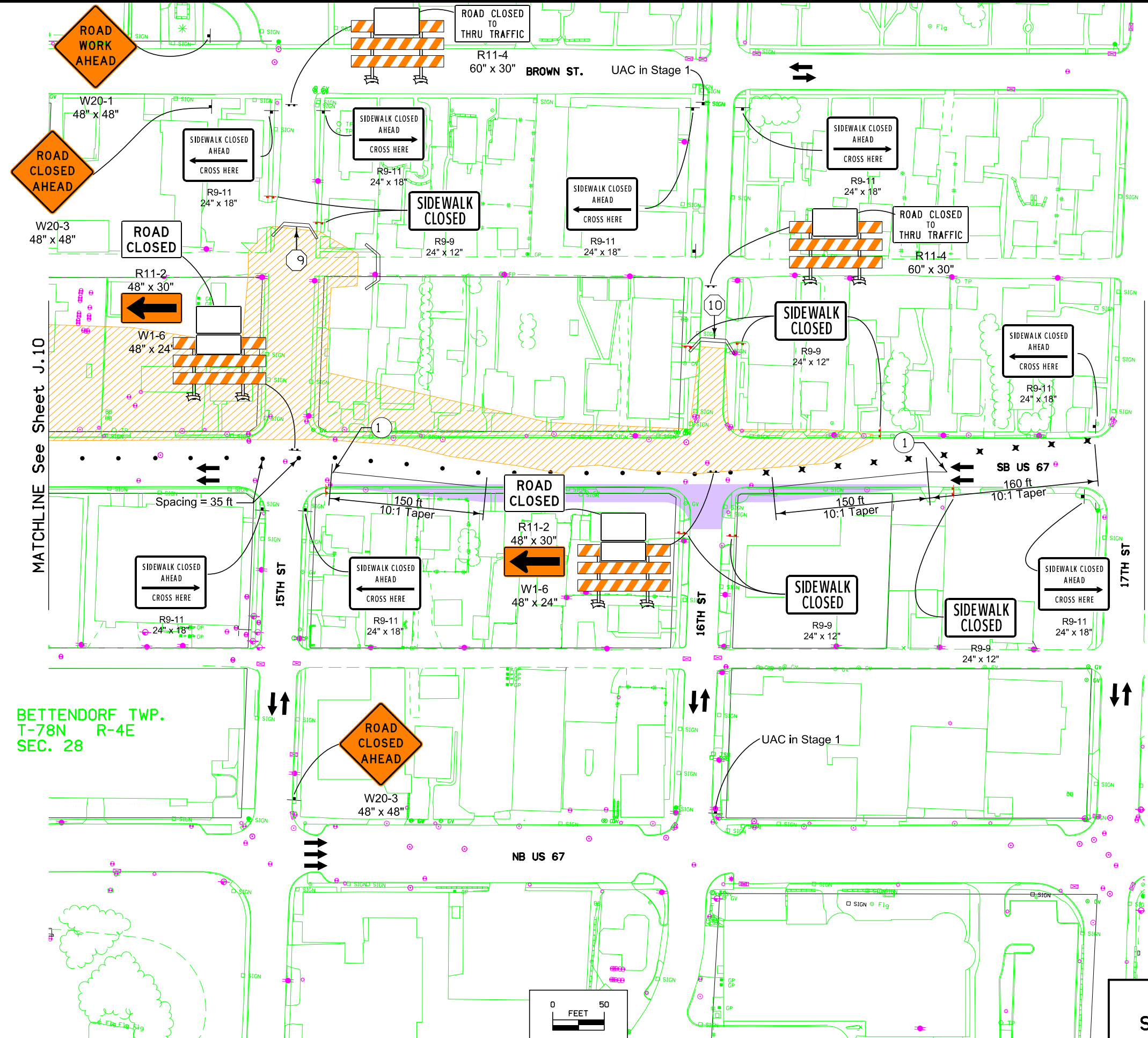
**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊠ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ▶▶▶ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure



For Temp Connection 7, 9 & 10 Details Refer to Sheets No. F.7, F.9 & F.10

**GRANT ST.  
STAGE 2A, SHEET 1 OF 3**



**LEGEND**

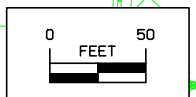
- ← Direction Of Traffic Per Lane
- ⊣ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- × Drum
- 42" Channelizer
- ◀▶▶▶ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure



MATCHLINE See Sheet J.10

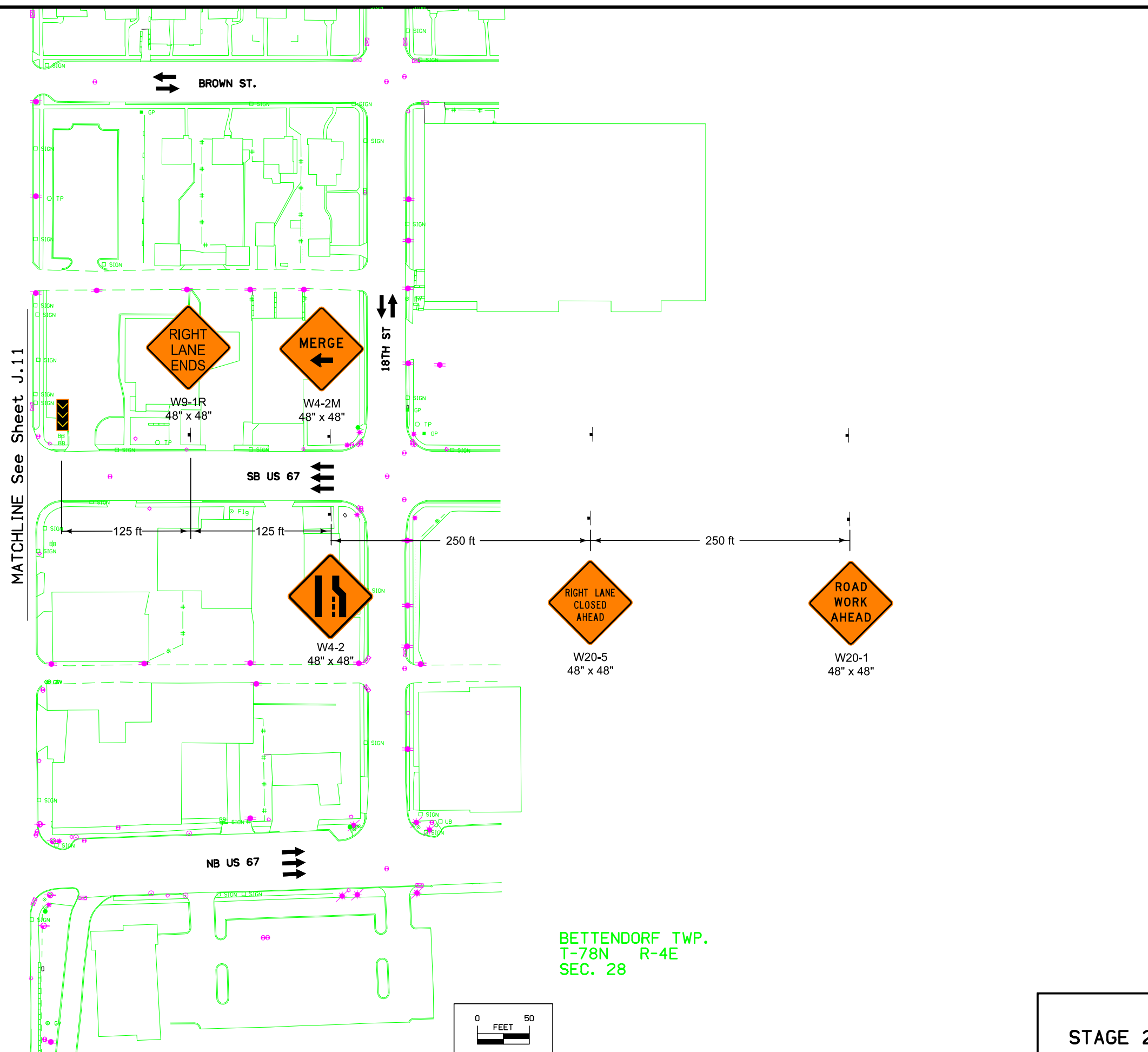
MATCHLINE See Sheet J.12

BETTENDORF TWP.  
T-78N R-4E  
SEC. 28



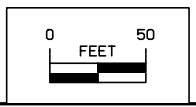
**GRANT ST.  
STAGE 2A, SHEET 2 OF 3**

LEGEND	
	Direction Of Traffic Per Lane
	Traffic Sign
	Type III Barricade
	Pedestrian Path Closure
	Drum
	42" Channelizer
	Arrow Board
	Work Area
	Safety Closure
	Existing ROW
	Pavement Marking No. and Limits
	Road Closure

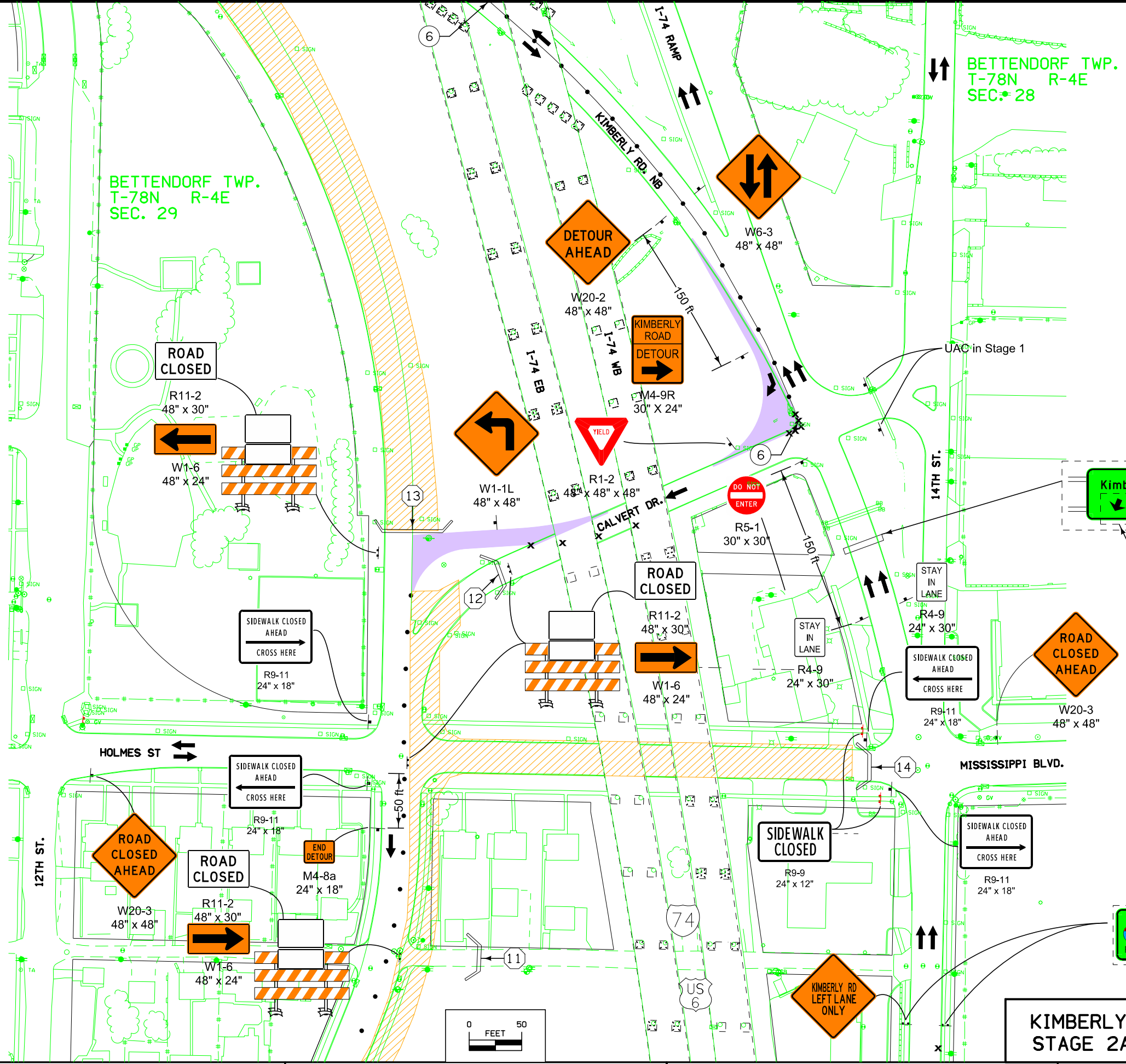


MATCHLINE See Sheet J.11

BETTENDORF TWP.  
T-78N R-4E  
SEC. 28



**GRANT ST.**  
**STAGE 2A, SHEET 3 OF 3**



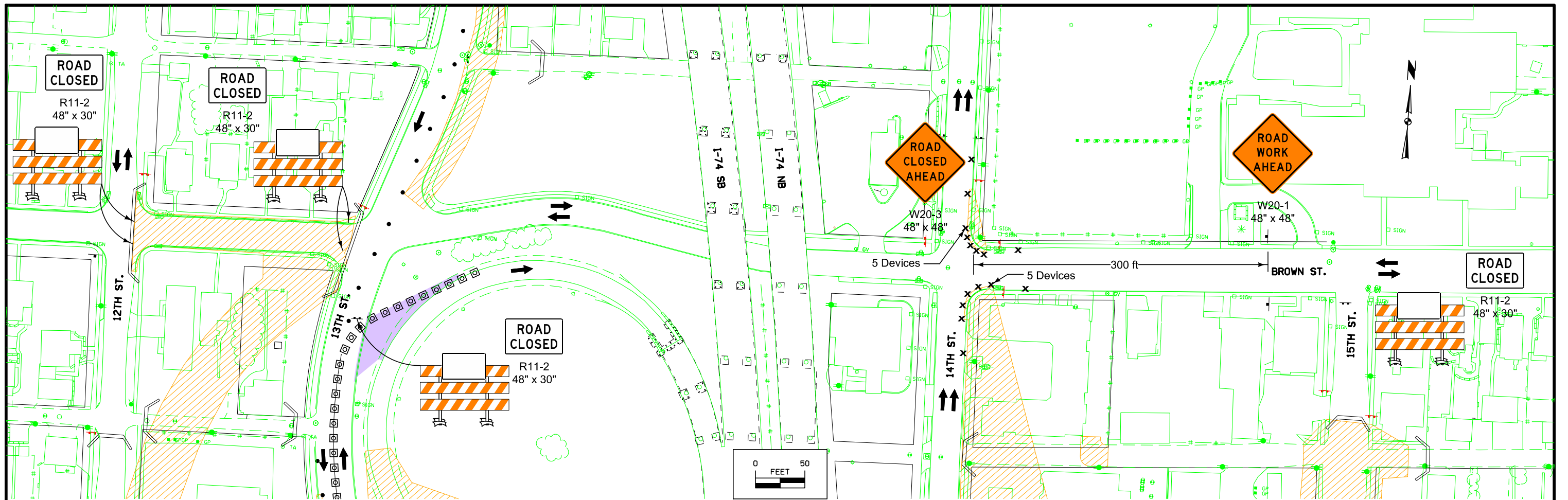
**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊠ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ◀▶▶▶ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure

CONTRACTOR TO COVER LEFT ARROW ONLY

CONTRACTOR TO COVER

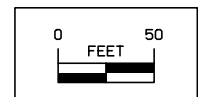
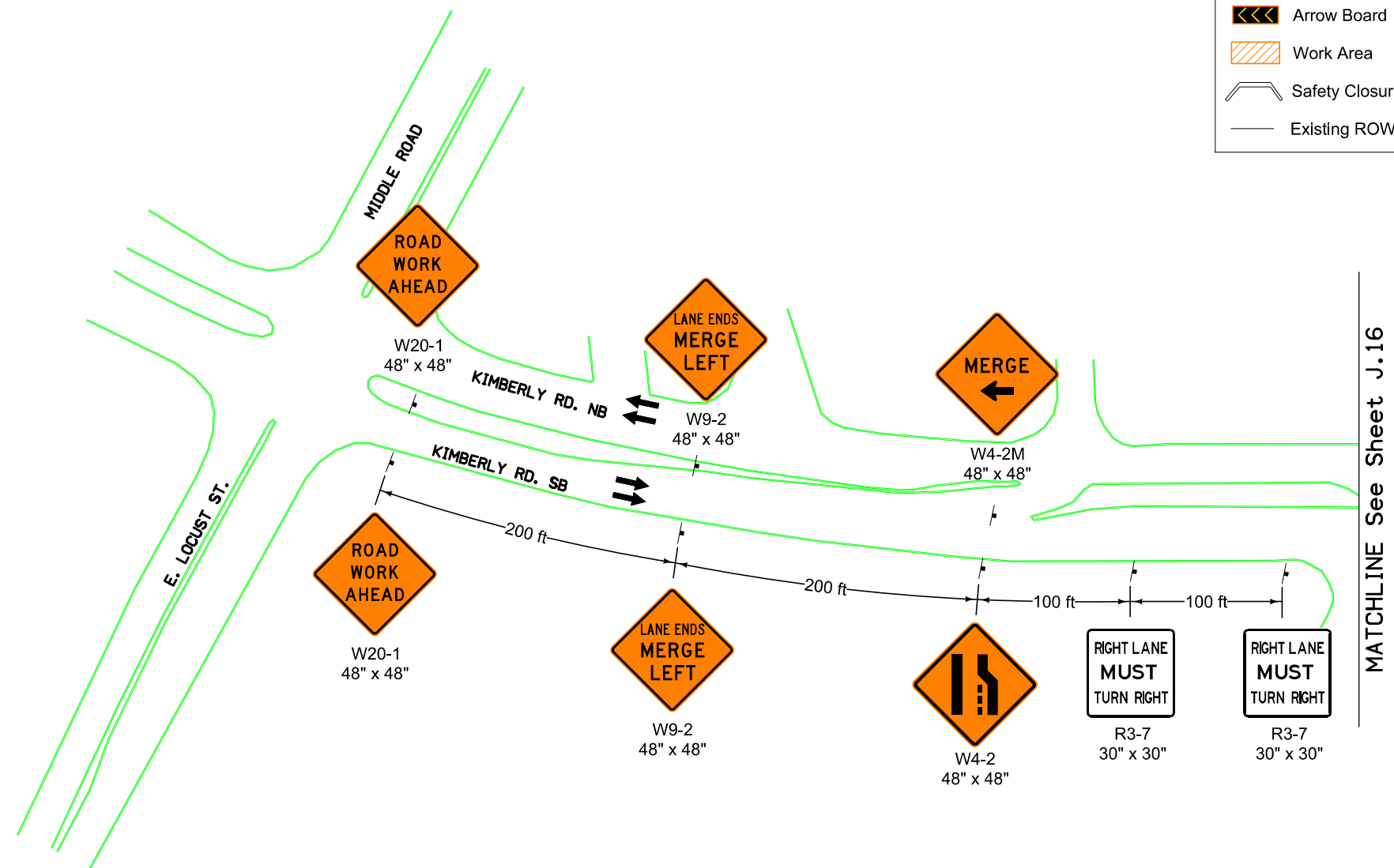
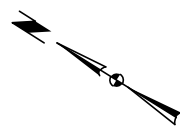
**KIMBERLY RD. & 13TH ST.  
STAGE 2A, SHEET 1 OF 1**



LEGEND	
	Direction Of Traffic Per Lane
	Traffic Sign
	Type III Barricade
	Pedestrian Path Closure
	Drum
	42\" Channelizer
	Arrow Board
	Work Area
	Safety Closure
	Existing ROW
	Pavement Marking No. and Limits
	Road Closure

**BROWN ST.  
STAGE 2A, SHEET 1 OF 1**

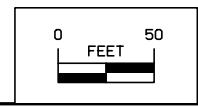
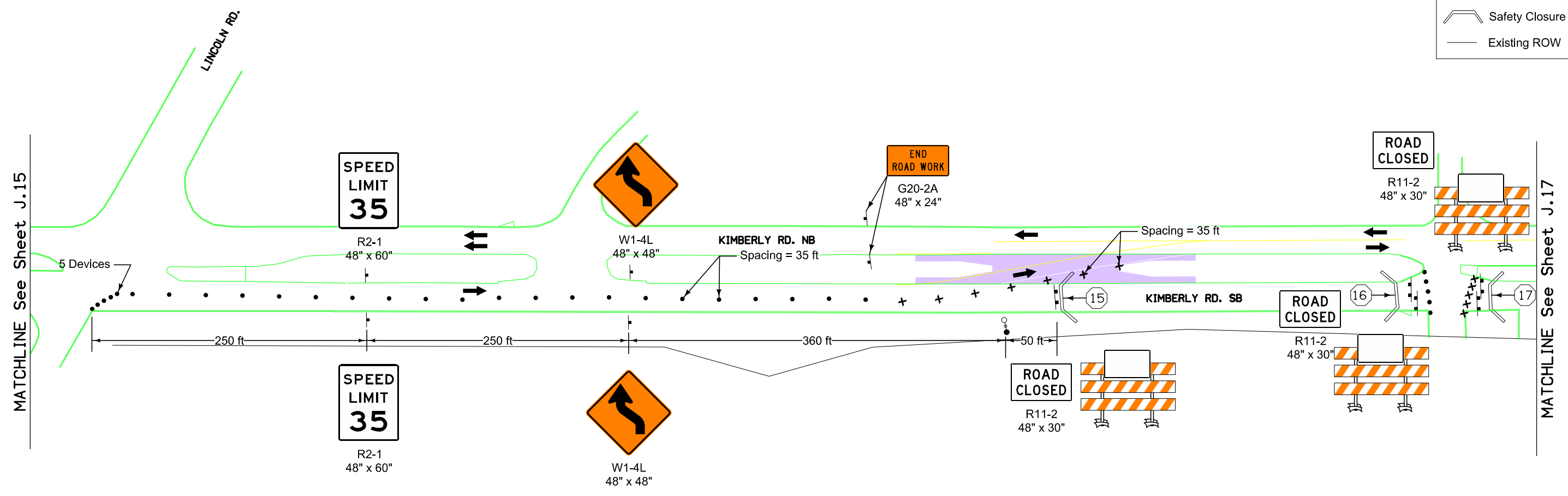
LEGEND	
	Direction Of Traffic Per Lane
	Traffic Sign
	Type III Barricade
	Drum
	42" Channelizer
	Arrow Board
	Work Area
	Safety Closure
	Existing ROW



**KIMBERLY RD.  
STAGE 2A, SHEET 1 OF 3**

**LEGEND**

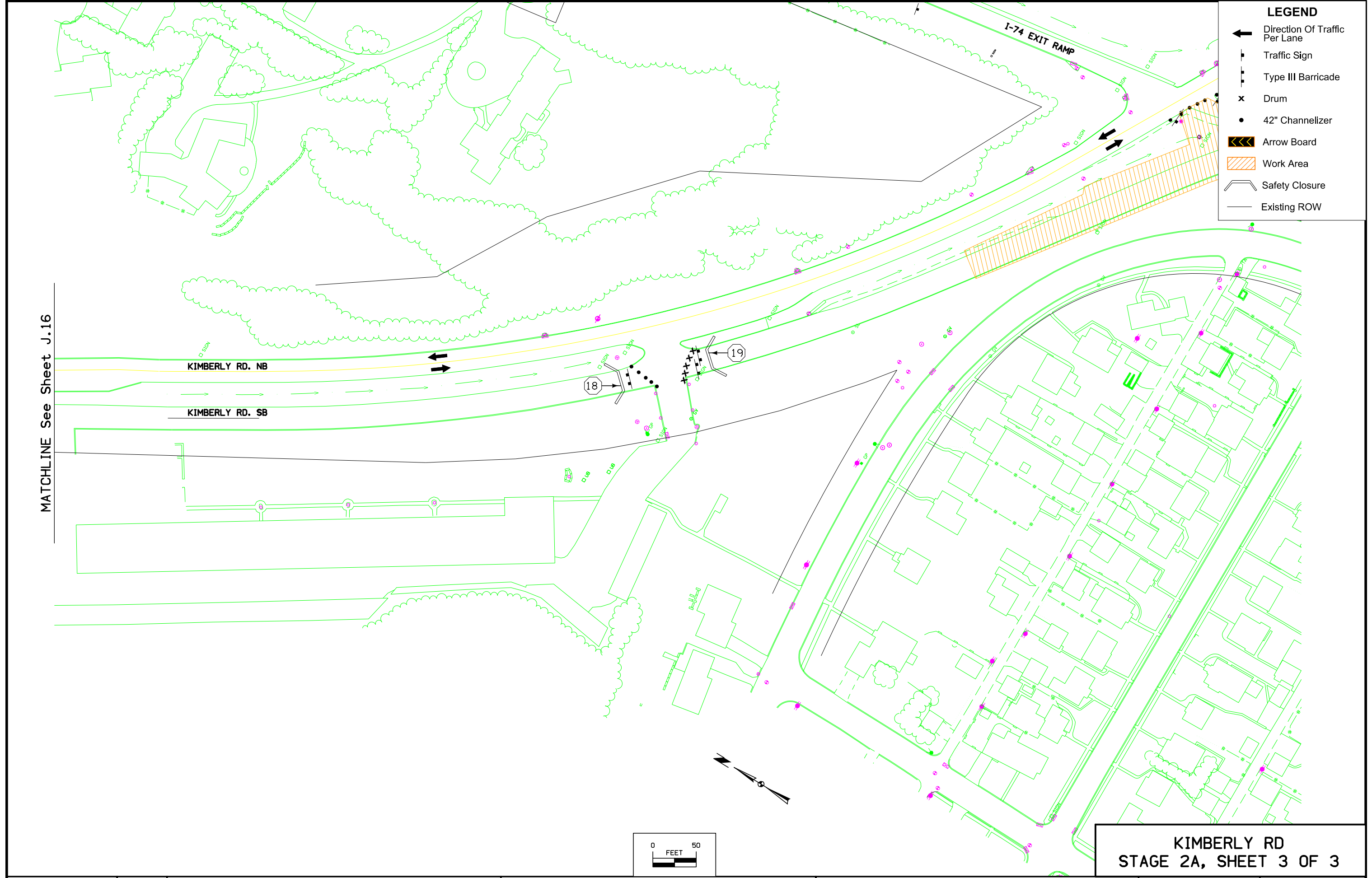
- ← Direction Of Traffic Per Lane
- ⊥ Traffic Sign
- ⊥ Type III Barricade
- x Drum
- 42" Channelizer
- ◀▶▶ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW



**KIMBERLY RD  
STAGE 2A, SHEET 2 OF 3**

**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊥ Traffic Sign
- ⊥ Type III Barricade
- x Drum
- 42" Channelizer
- ▬ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW



MATCHLINE See Sheet J.16

KIMBERLY RD. NB  
KIMBERLY RD. SB

**KIMBERLY RD  
STAGE 2A, SHEET 3 OF 3**



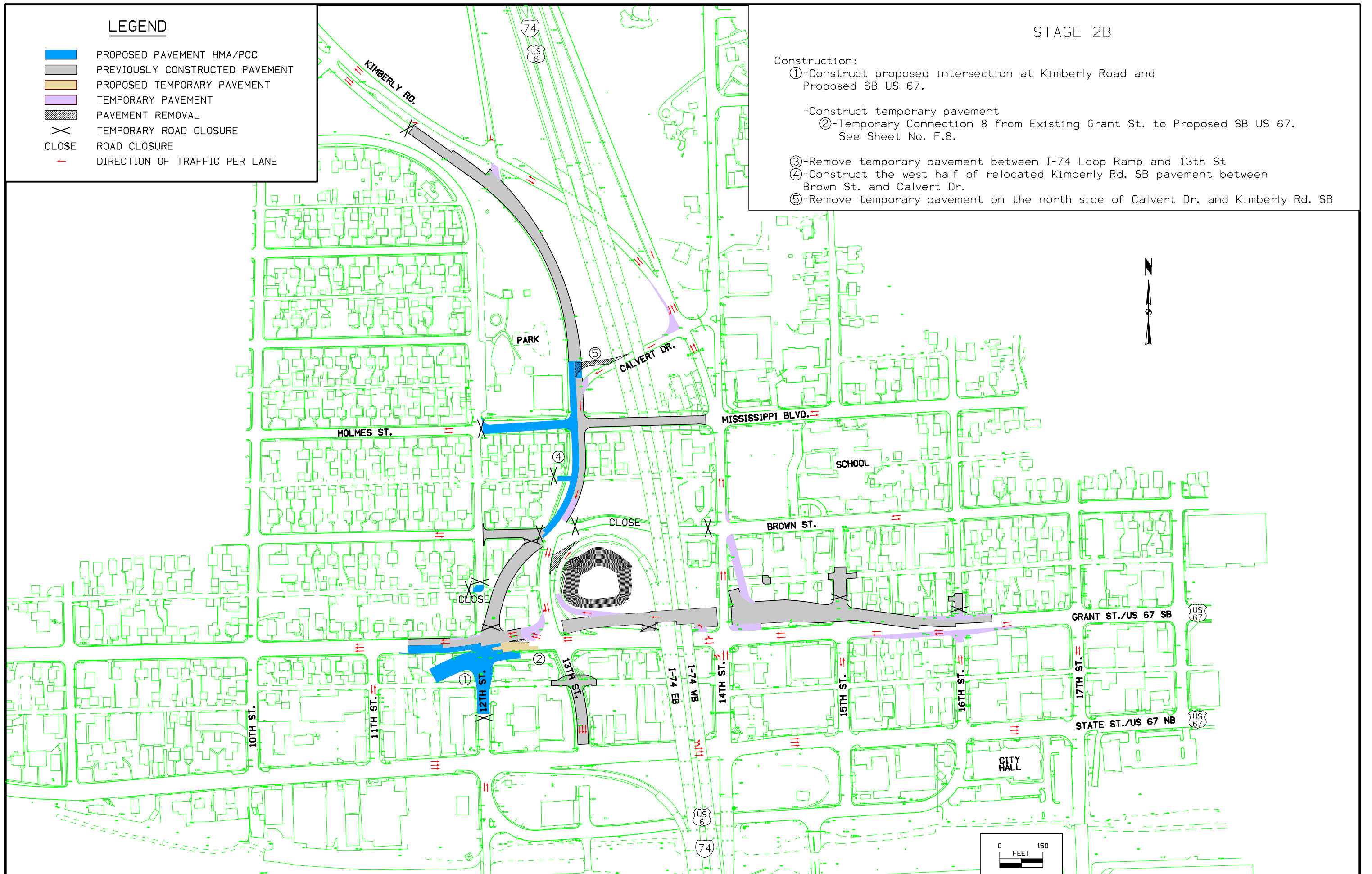
**LEGEND**

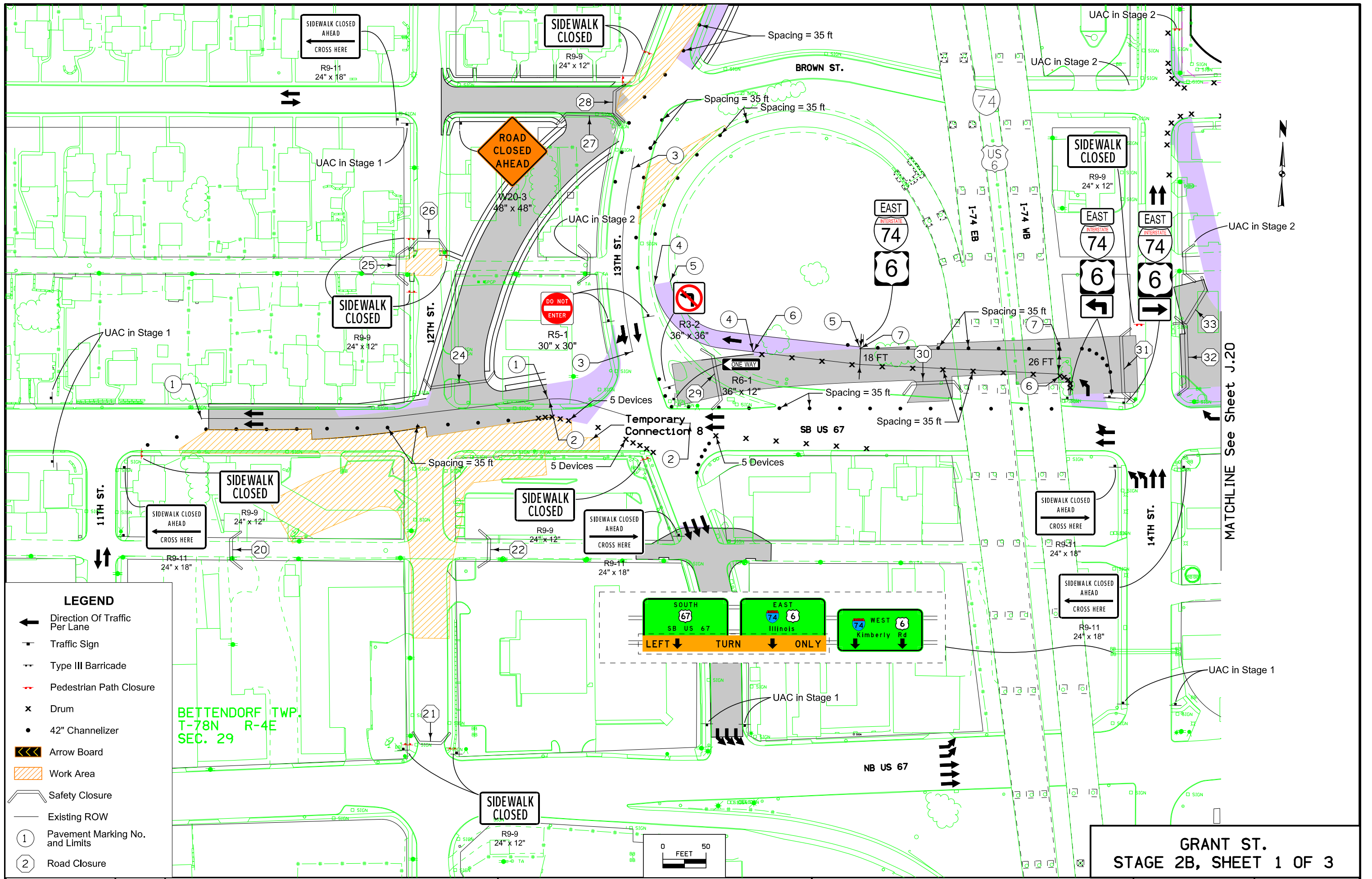
- PROPOSED PAVEMENT HMA/PCC
- PREVIOUSLY CONSTRUCTED PAVEMENT
- PROPOSED TEMPORARY PAVEMENT
- TEMPORARY PAVEMENT
- PAVEMENT REMOVAL
- TEMPORARY ROAD CLOSURE
- ROAD CLOSURE
- DIRECTION OF TRAFFIC PER LANE

STAGE 2B

Construction:

- ①-Construct proposed intersection at Kimberly Road and Proposed SB US 67.
- Construct temporary pavement
- ②-Temporary Connection 8 from Existing Grant St. to Proposed SB US 67. See Sheet No. F.8.
- ③-Remove temporary pavement between I-74 Loop Ramp and 13th St
- ④-Construct the west half of relocated Kimberly Rd. SB pavement between Brown St. and Calvert Dr.
- ⑤-Remove temporary pavement on the north side of Calvert Dr. and Kimberly Rd. SB



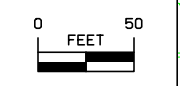
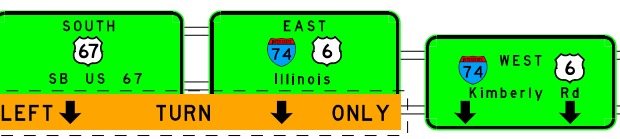


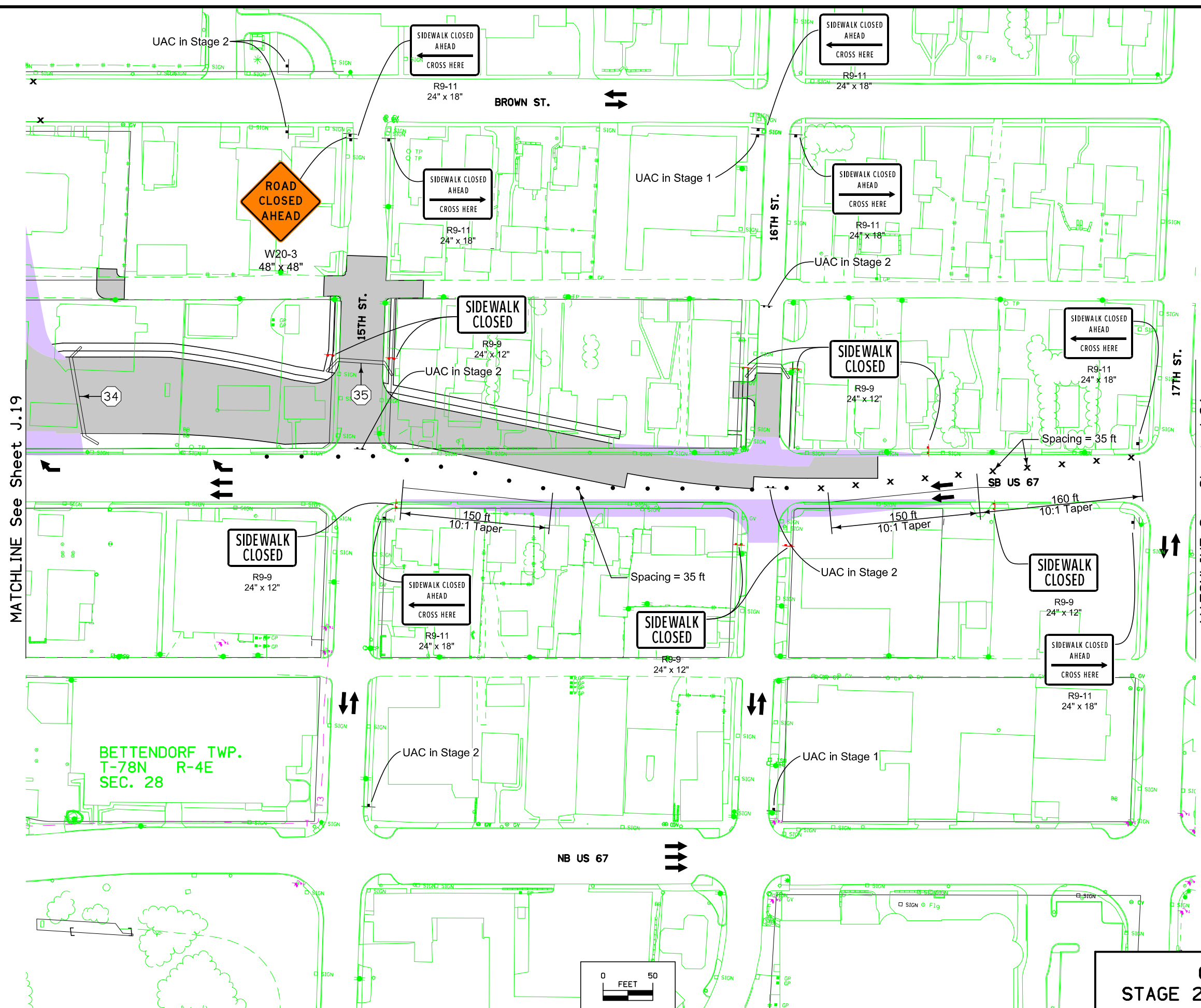
MATCHLINE See Sheet J.20

**GRANT ST.  
STAGE 2B, SHEET 1 OF 3**

- LEGEND**
- ← Direction Of Traffic Per Lane
  - ▬ Traffic Sign
  - Type III Barricade
  - Pedestrian Path Closure
  - x Drum
  - 42" Channelizer
  - ▬▬▬ Arrow Board
  - ▨ Work Area
  - ▭ Safety Closure
  - Existing ROW
  - ① Pavement Marking No. and Limits
  - ② Road Closure

BETTENDORF TWP.  
T-78N R-4E  
SEC. 29





**LEGEND**

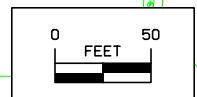
- ← Direction Of Traffic Per Lane
- ⊣ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ◀▶▶▶ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits



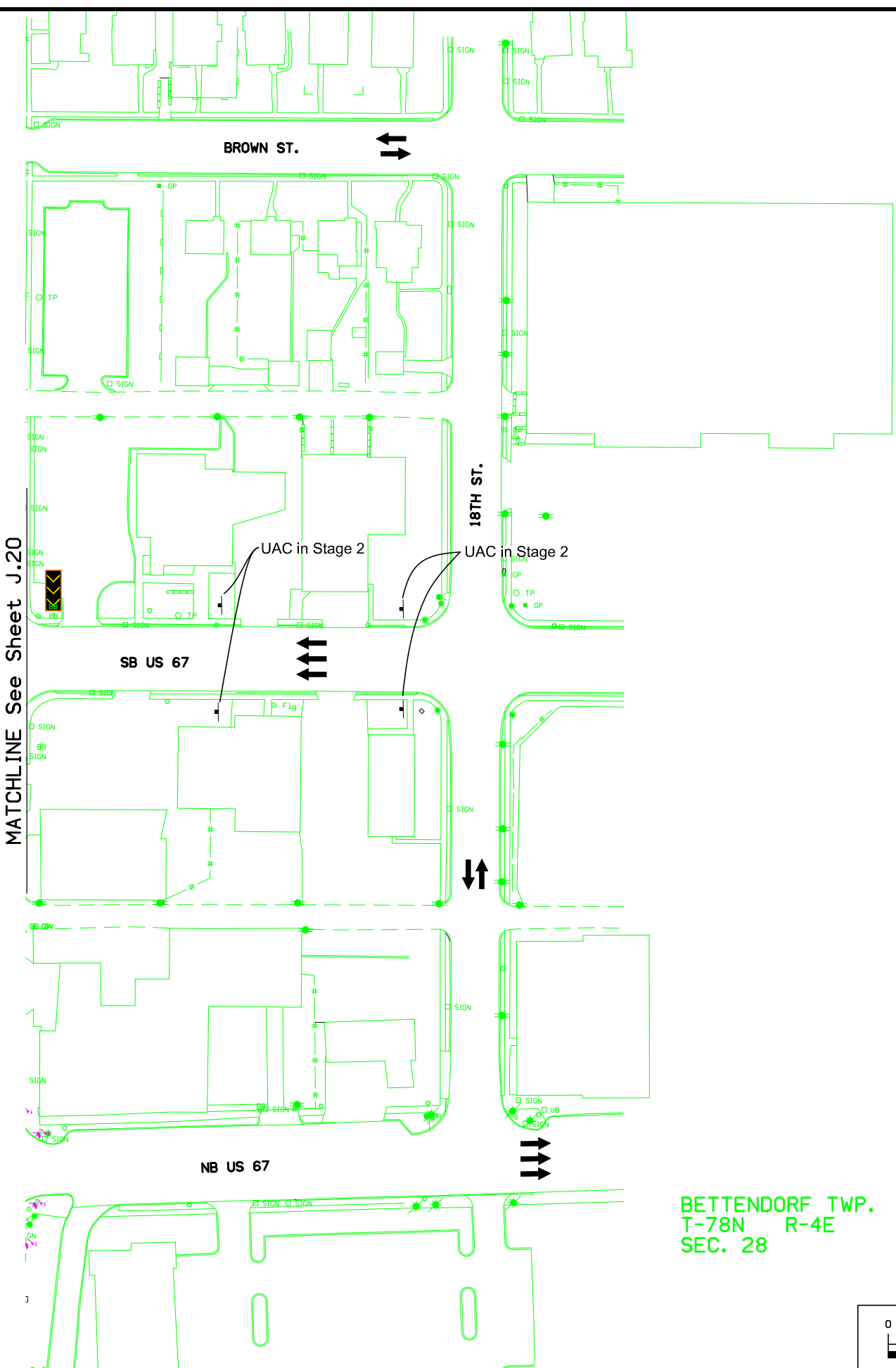
MATCHLINE See Sheet J.19

MATCHLINE See Sheet J.21

BETTENDORF TWP.  
T-78N R-4E  
SEC. 28



**GRANT ST.  
STAGE 2B, SHEET 2 OF 3**

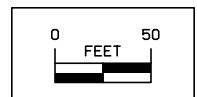


**LEGEND**

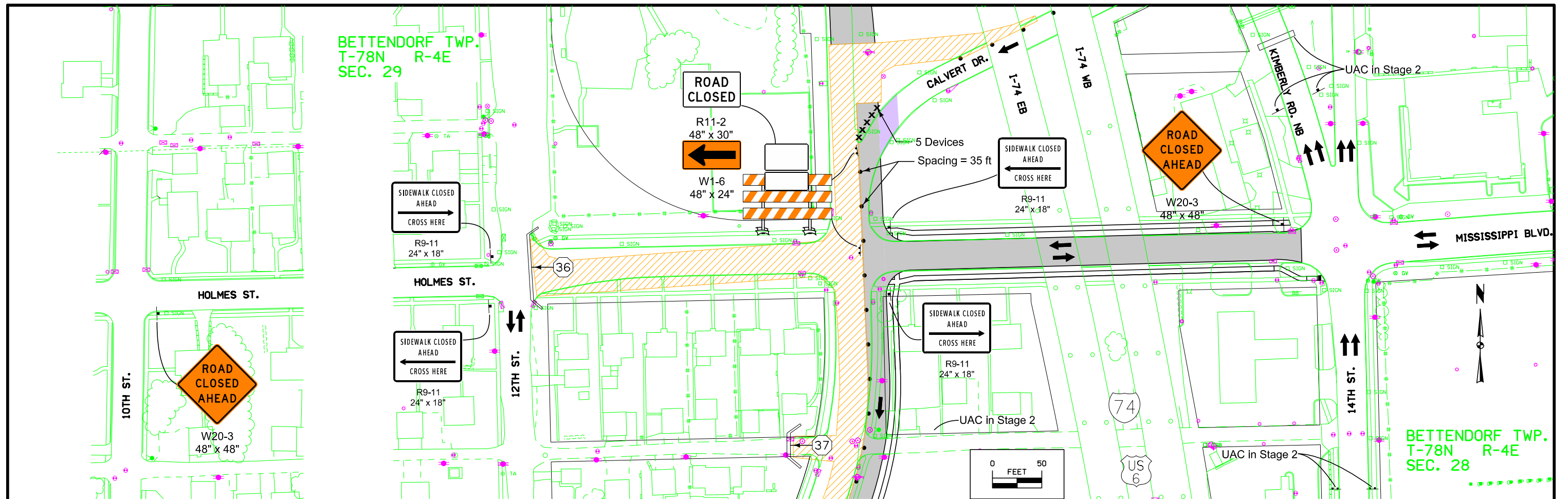
- ← Direction Of Traffic Per Lane
- ⊣ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ◀◀◀ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits



BETTENDORF TWP.  
T-78N R-4E  
SEC. 28

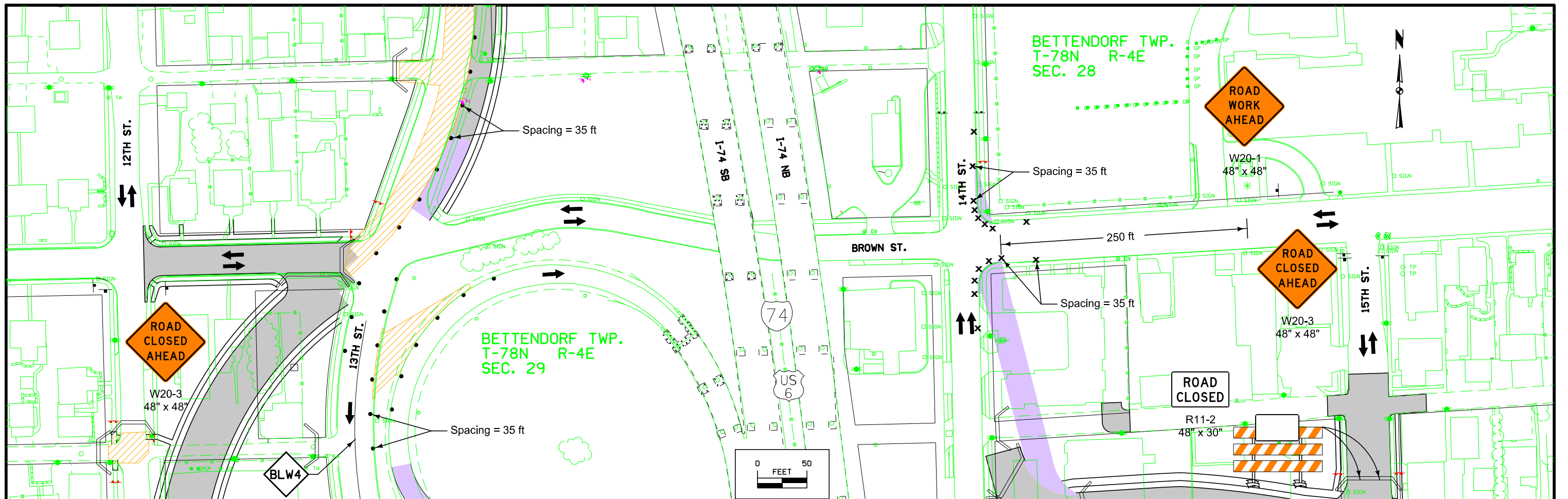


**GRANT ST.  
STAGE 2B, SHEET 3 OF 3**



LEGEND	
	Direction Of Traffic Per Lane
	Traffic Sign
	Type III Barricade
	Pedestrian Path Closure
	Drum
	42" Channelizer
	Arrow Board
	Work Area
	Safety Closure
	Existing ROW
	Pavement Marking No. and Limits
	Road Closure

**MISSISSIPPI BLVD.  
STAGE 2B, SHEET 1 OF 1**



LEGEND	
	Direction Of Traffic Per Lane
	Traffic Sign
	Type III Barricade
	Pedestrian Path Closure
	Drum
	42\" Channelizer
	Arrow Board
	Work Area
	Safety Closure
	Existing ROW
	Pavement Marking No. and Limits
	Road Closure

For Temporary Pavement Markings Refer to Sheet C.XX

**BROWN ST.  
STAGE 2B, SHEET 1 OF 1**

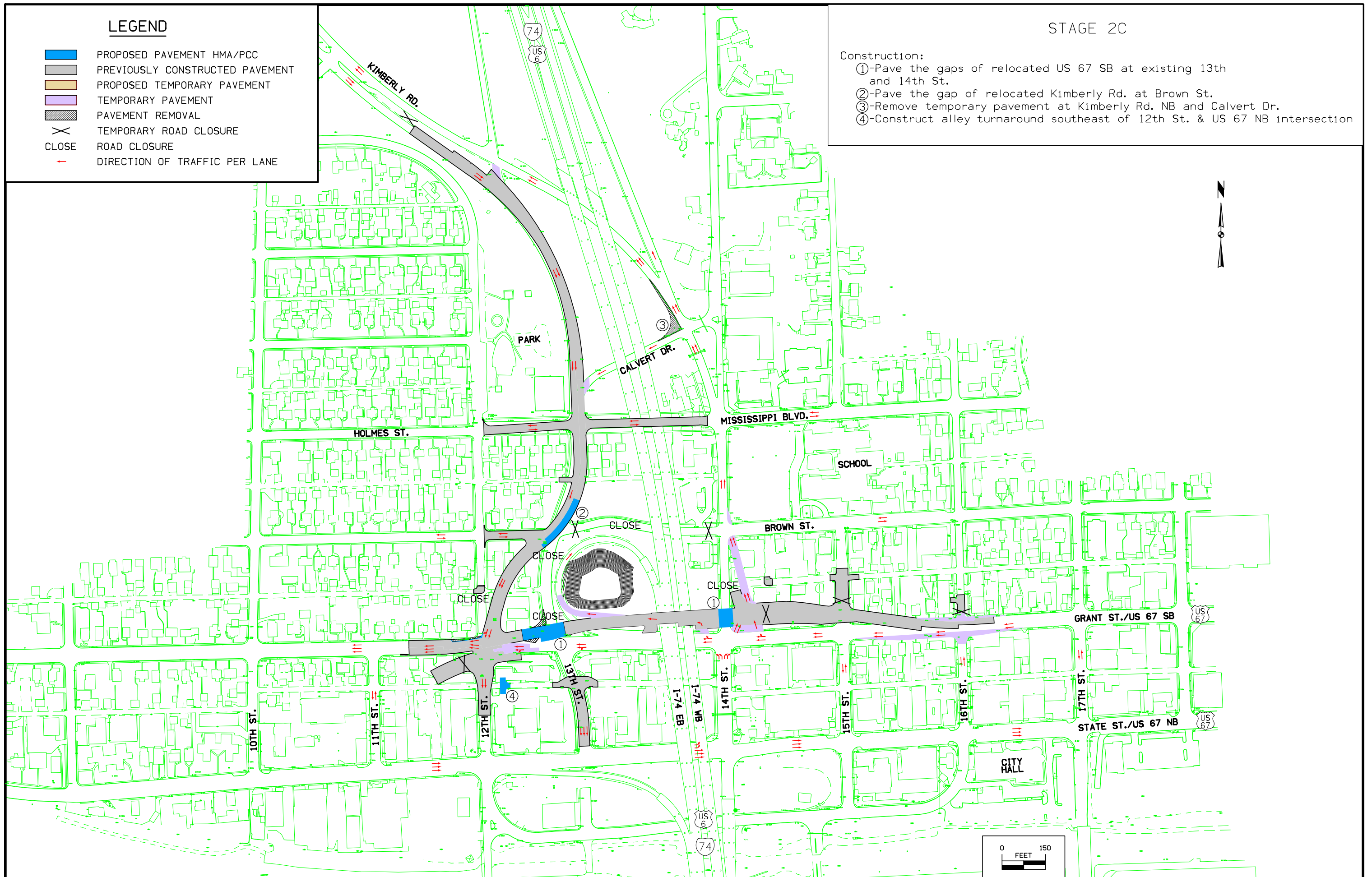
**LEGEND**

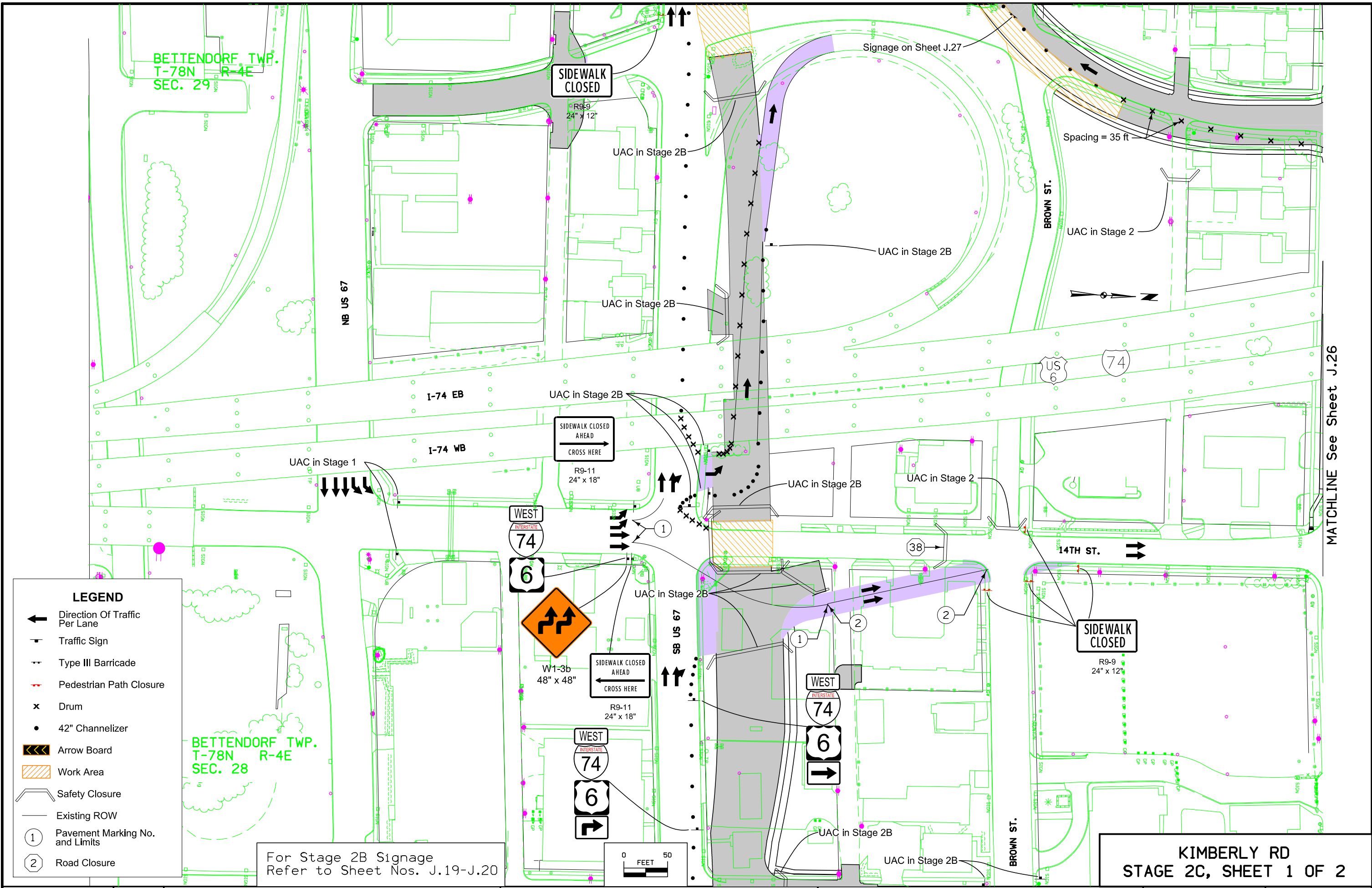
- PROPOSED PAVEMENT HMA/PCC
- PREVIOUSLY CONSTRUCTED PAVEMENT
- PROPOSED TEMPORARY PAVEMENT
- TEMPORARY PAVEMENT
- PAVEMENT REMOVAL
- TEMPORARY ROAD CLOSURE
- ROAD CLOSURE
- DIRECTION OF TRAFFIC PER LANE

STAGE 2C

Construction:

- ①-Pave the gaps of relocated US 67 SB at existing 13th and 14th St.
- ②-Pave the gap of relocated Kimberly Rd. at Brown St.
- ③-Remove temporary pavement at Kimberly Rd. NB and Calvert Dr.
- ④-Construct alley turnaround southeast of 12th St. & US 67 NB intersection

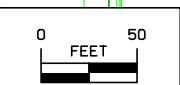




**LEGEND**

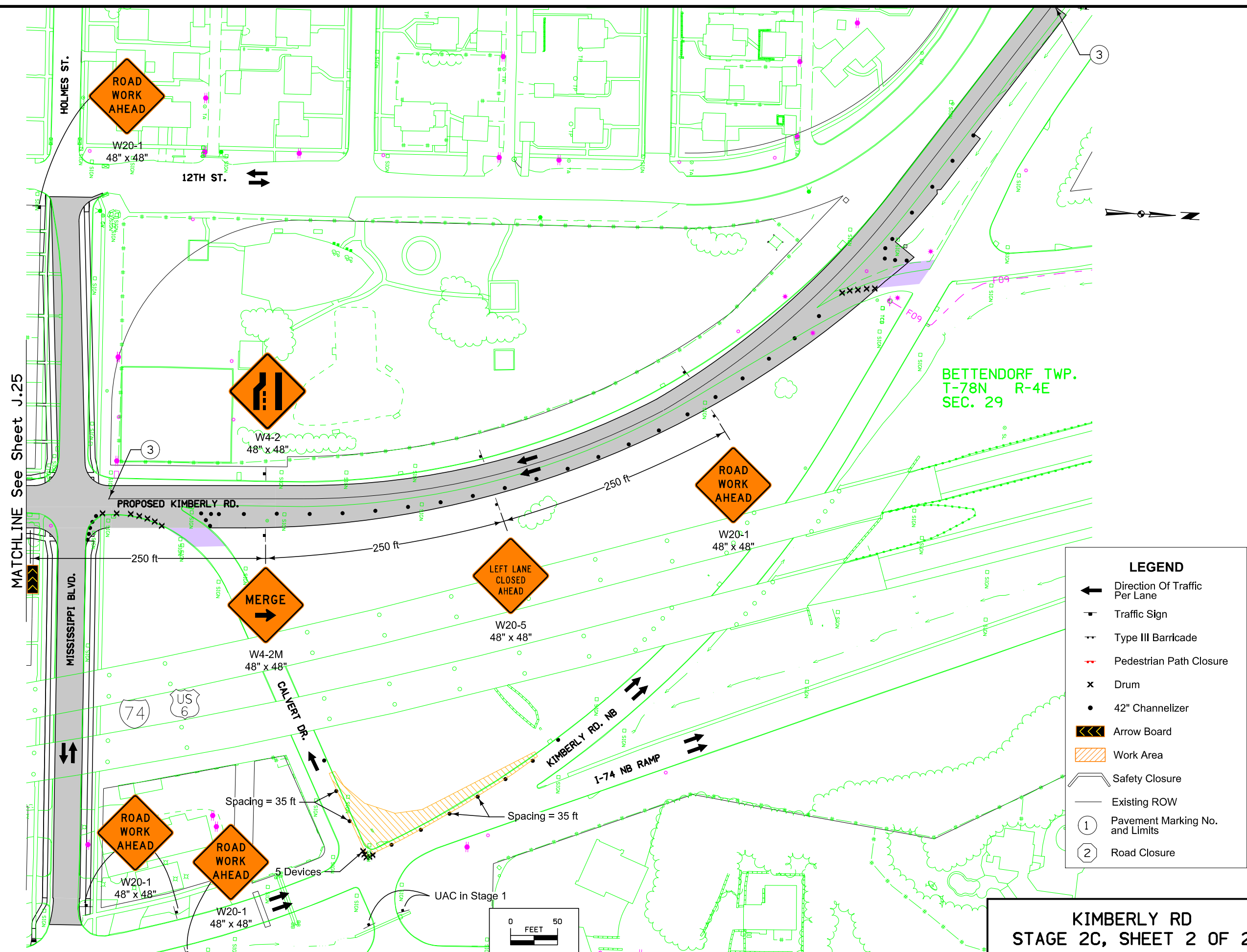
- ← Direction Of Traffic Per Lane
- ⊠ Traffic Sign
- Type III Barricade
- ⊠ Pedestrian Path Closure
- × Drum
- 42" Channelizer
- ▨ Arrow Board
- ▨ Work Area
- ▭ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure

For Stage 2B Signage Refer to Sheet Nos. J.19-J.20



**KIMBERLY RD  
STAGE 2C, SHEET 1 OF 2**





MATCHLINE See Sheet J.25

LEGEND	
	Direction Of Traffic Per Lane
	Traffic Sign
	Type III Barricade
	Pedestrian Path Closure
	Drum
	42" Channelizer
	Arrow Board
	Work Area
	Safety Closure
	Existing ROW
	Pavement Marking No. and Limits
	Road Closure

**KIMBERLY RD  
STAGE 2C, SHEET 2 OF 2**

BETTENDORF TWP.  
T-78N R-4E  
SEC. 29

BROWN ST.

ROAD CLOSED

W1-6  
48" x 24"

R11-2  
48" x 30"

SIDEWALK CLOSED  
AHEAD  
CROSS HERE

Spacing = 35 ft

UAC in Stage 2

SIDEWALK CLOSED  
AHEAD  
CROSS HERE

R9-11  
24" x 18"

UAC in Stage 1

SIDEWALK CLOSED

R9-9  
24" x 12"

5 Devices

DO NOT ENTER

R5-1  
30" x 30"

Spacing = 20 ft

UAC in Stage 2B

Spacing = 35 ft

SB US 67

SIDEWALK CLOSED

R9-9  
24" x 12"

5 Devices

SIDEWALK CLOSED

R9-9  
24" x 12"

**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊠ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ▶▶▶ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure

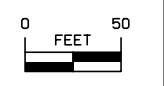
10TH ST.

11TH ST.

12TH ST.

13TH ST.

NB US 67



GRANT ST.  
STAGE 2C, SHEET 1 OF 1

ENGLISH

IOWA DOT

DESIGN TEAM

WHKS & CO.

SCOTT

COUNTY

PROJECT NUMBER

IM-074-1(226)5--13-82

SHEET NUMBER

J.27

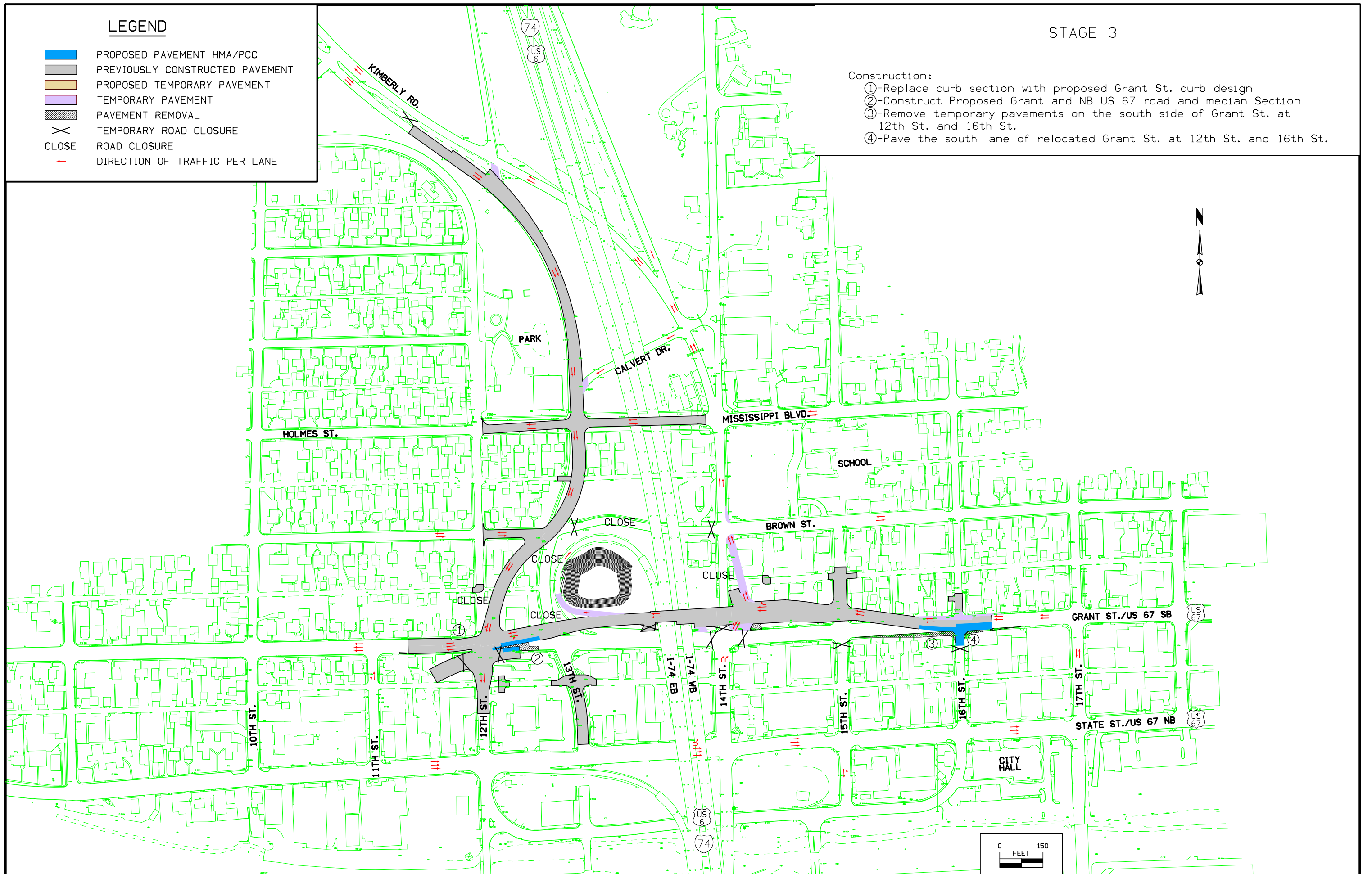
**LEGEND**

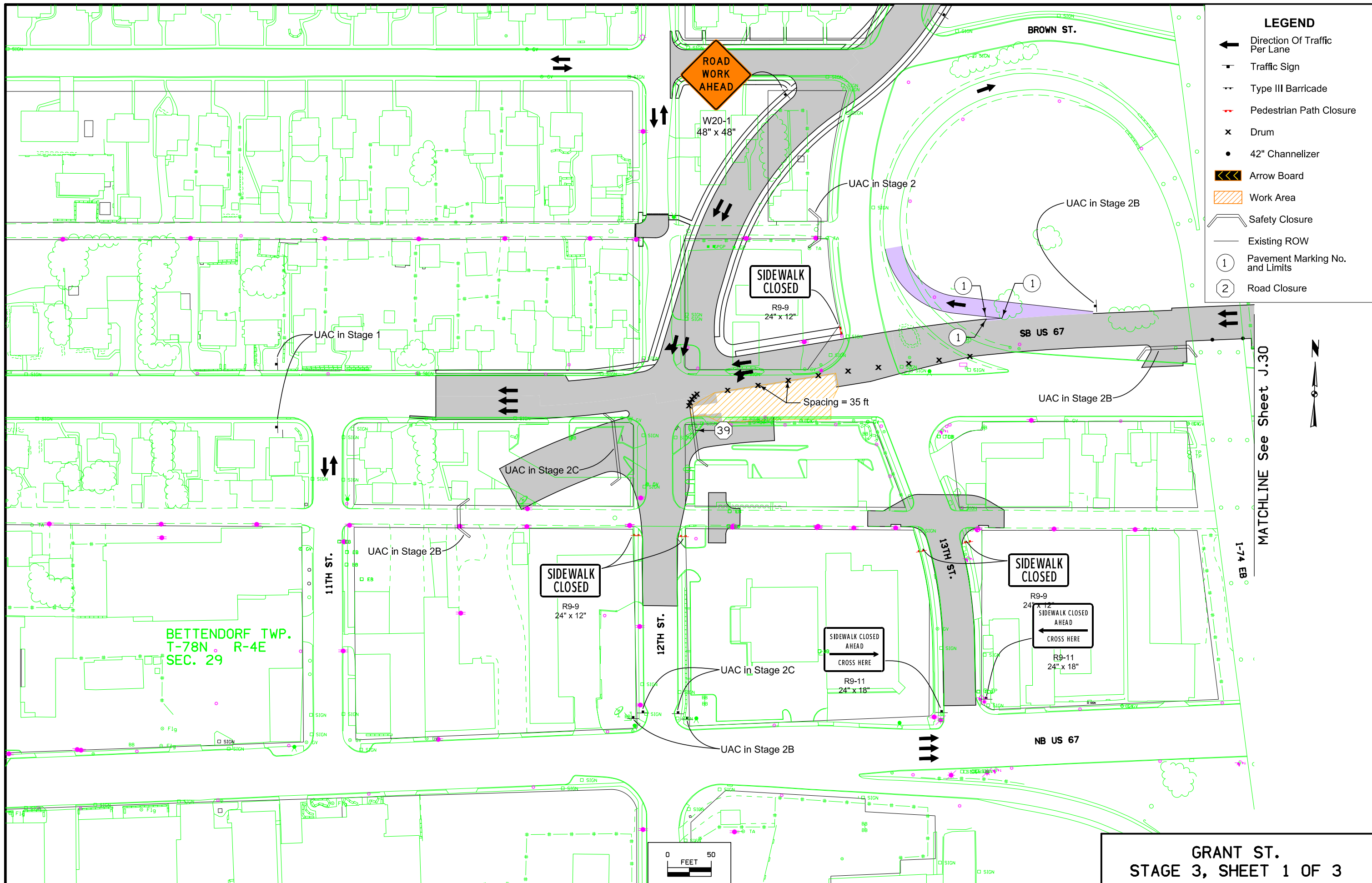
- PROPOSED PAVEMENT HMA/PCC
- PREVIOUSLY CONSTRUCTED PAVEMENT
- PROPOSED TEMPORARY PAVEMENT
- TEMPORARY PAVEMENT
- PAVEMENT REMOVAL
- TEMPORARY ROAD CLOSURE
- ROAD CLOSURE
- DIRECTION OF TRAFFIC PER LANE

STAGE 3

Construction:

- ①-Replace curb section with proposed Grant St. curb design
- ②-Construct Proposed Grant and NB US 67 road and median Section
- ③-Remove temporary pavements on the south side of Grant St. at 12th St. and 16th St.
- ④-Pave the south lane of relocated Grant St. at 12th St. and 16th St.





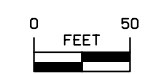
**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊥ Traffic Sign
- Type III Barricade
- - - Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ◀▶▶▶ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure

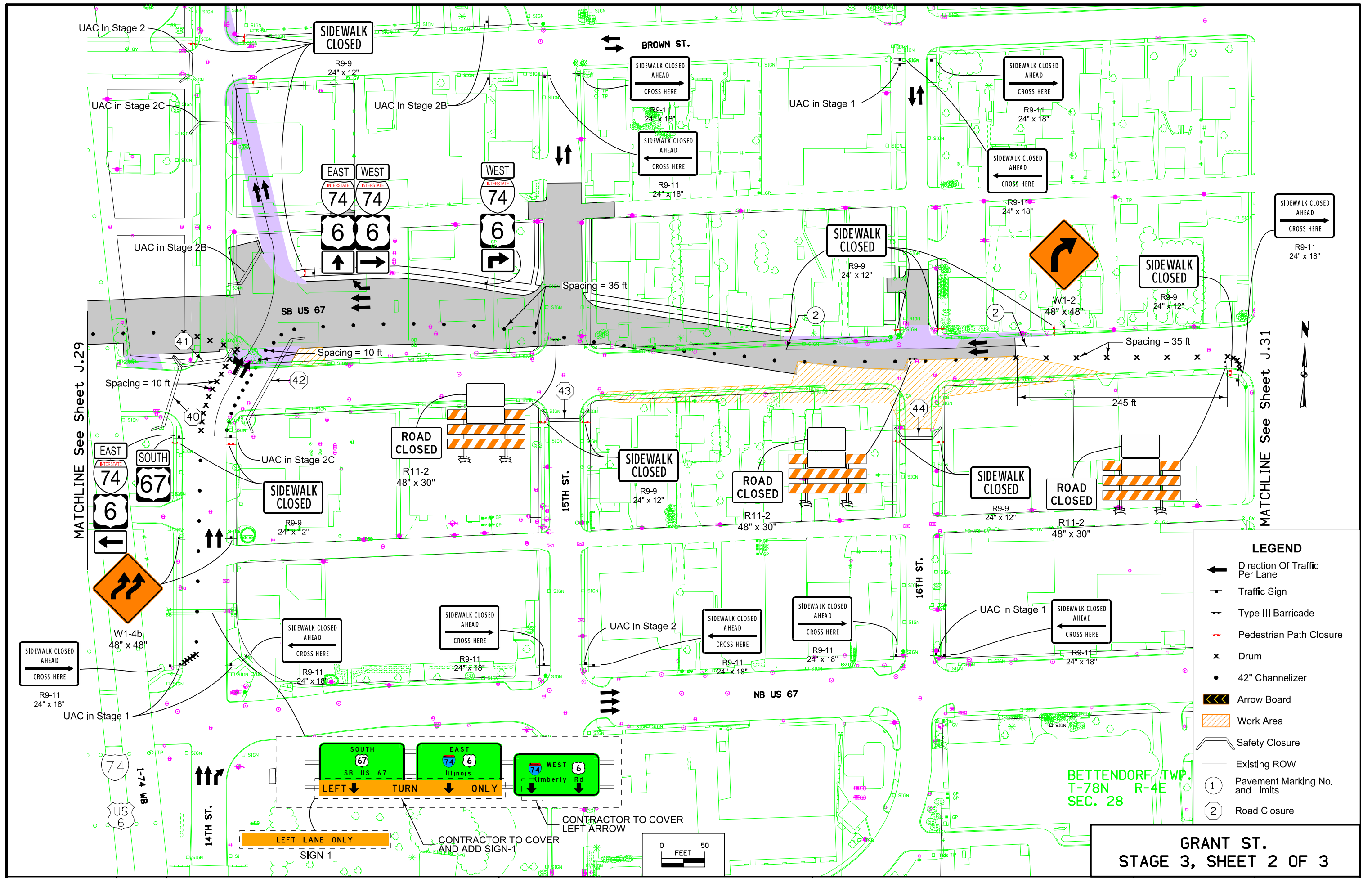


MATCHLINE See Sheet J.30

BETTENDORF TWP.  
T-78N R-4E  
SEC. 29



**GRANT ST.  
STAGE 3, SHEET 1 OF 3**

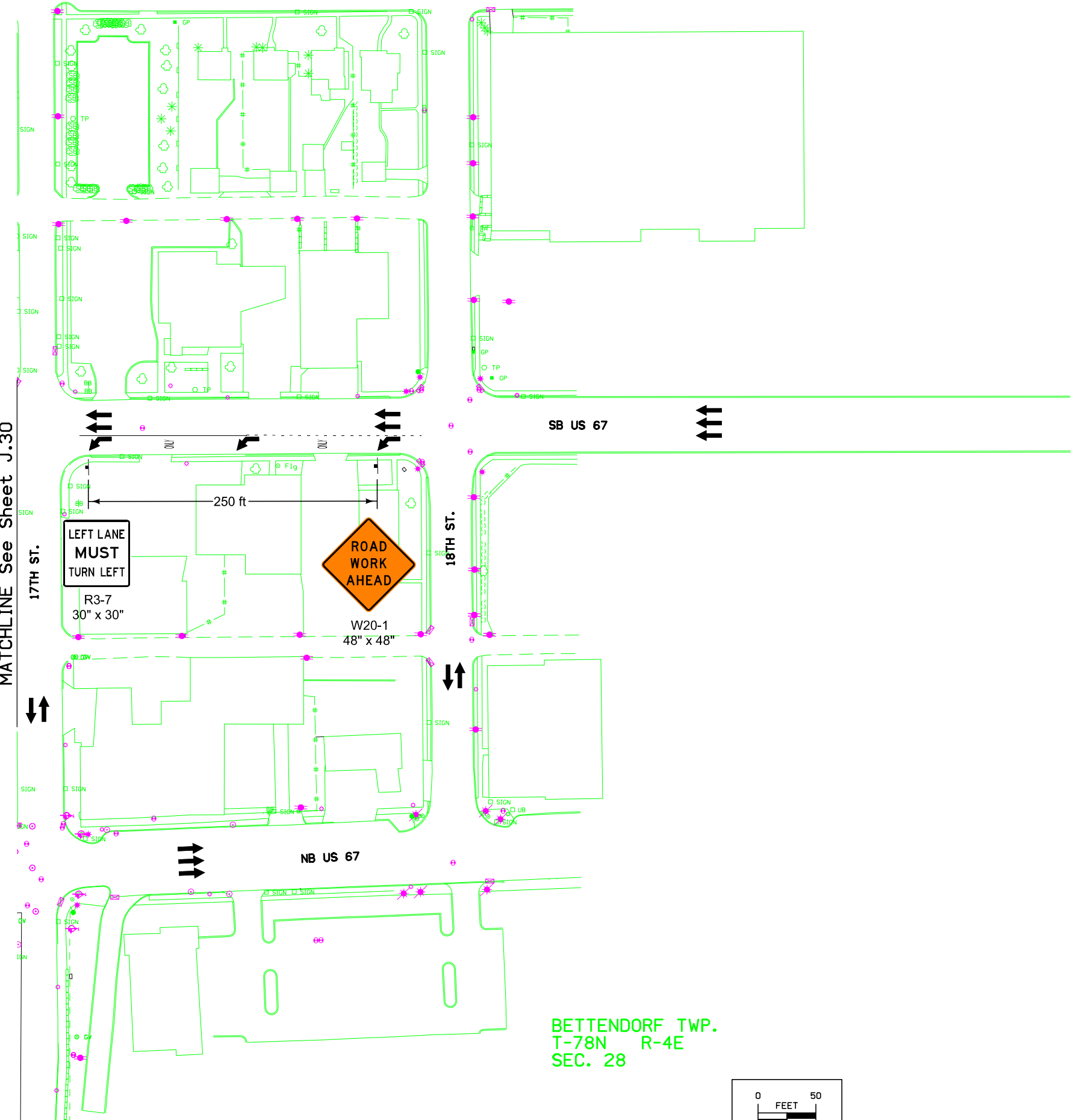


**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊢ Traffic Sign
- Type III Barricade
- - - Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ▶▶▶ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure

**GRANT ST.  
STAGE 3, SHEET 2 OF 3**

BROWN ST. ←

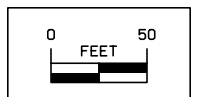


**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊣ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ◀▶▶▶ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure



BETTENDORF TWP.  
T-78N R-4E  
SEC. 28



GRANT ST.  
STAGE 3, SHEET 3 OF 3

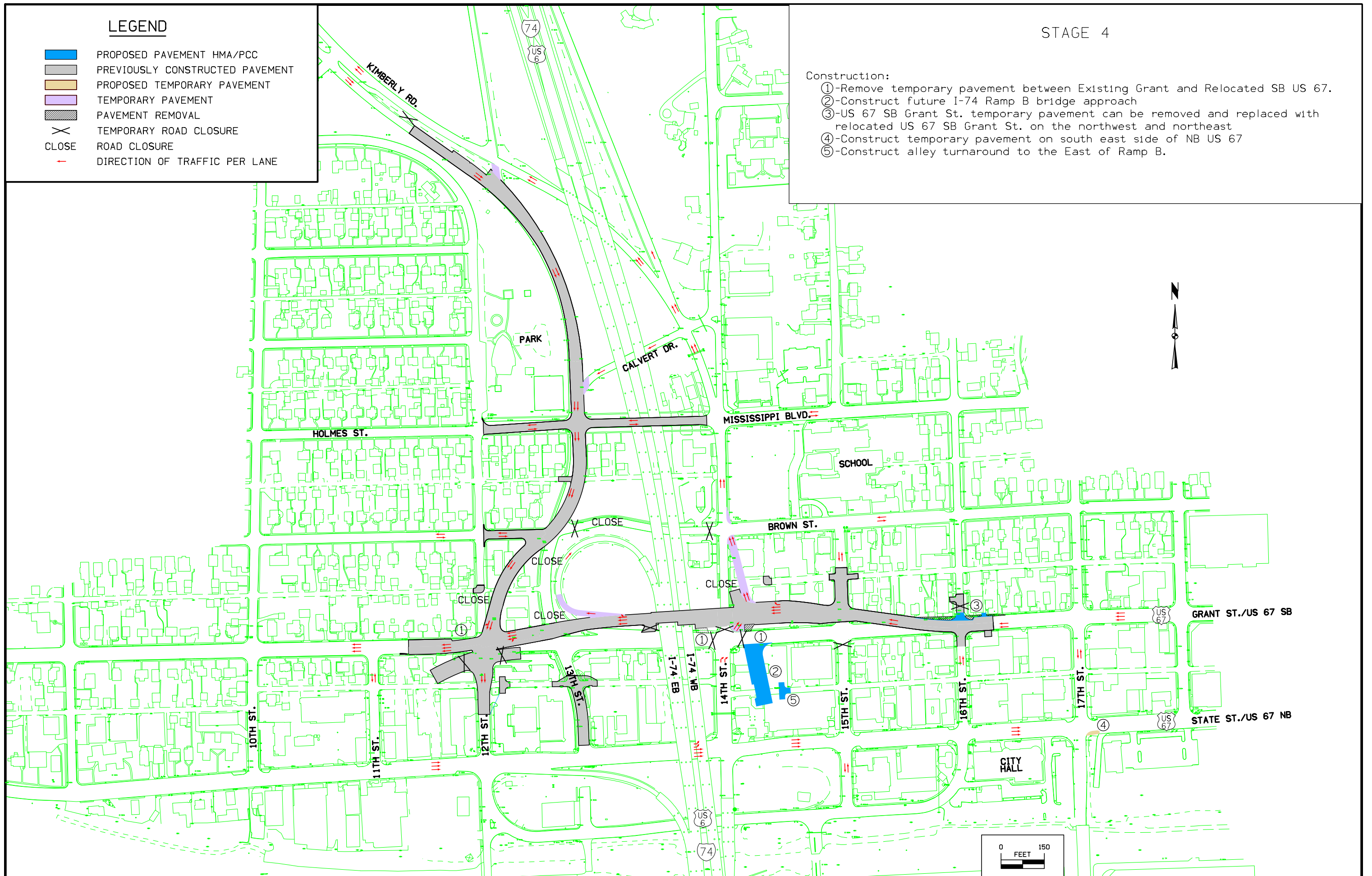
**LEGEND**

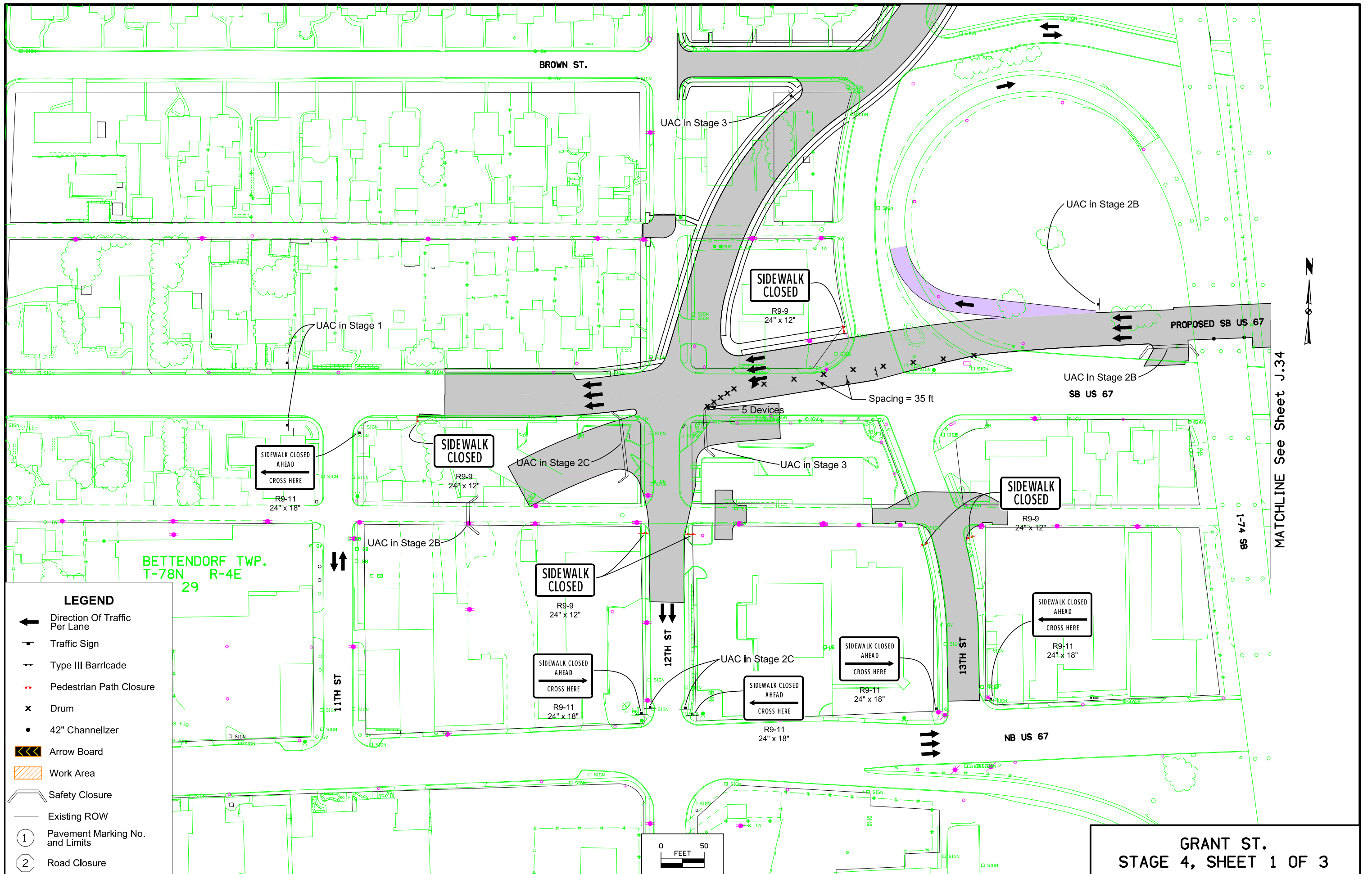
- PROPOSED PAVEMENT HMA/PCC
- PREVIOUSLY CONSTRUCTED PAVEMENT
- PROPOSED TEMPORARY PAVEMENT
- TEMPORARY PAVEMENT
- PAVEMENT REMOVAL
- TEMPORARY ROAD CLOSURE
- ROAD CLOSURE
- DIRECTION OF TRAFFIC PER LANE

STAGE 4

Construction:

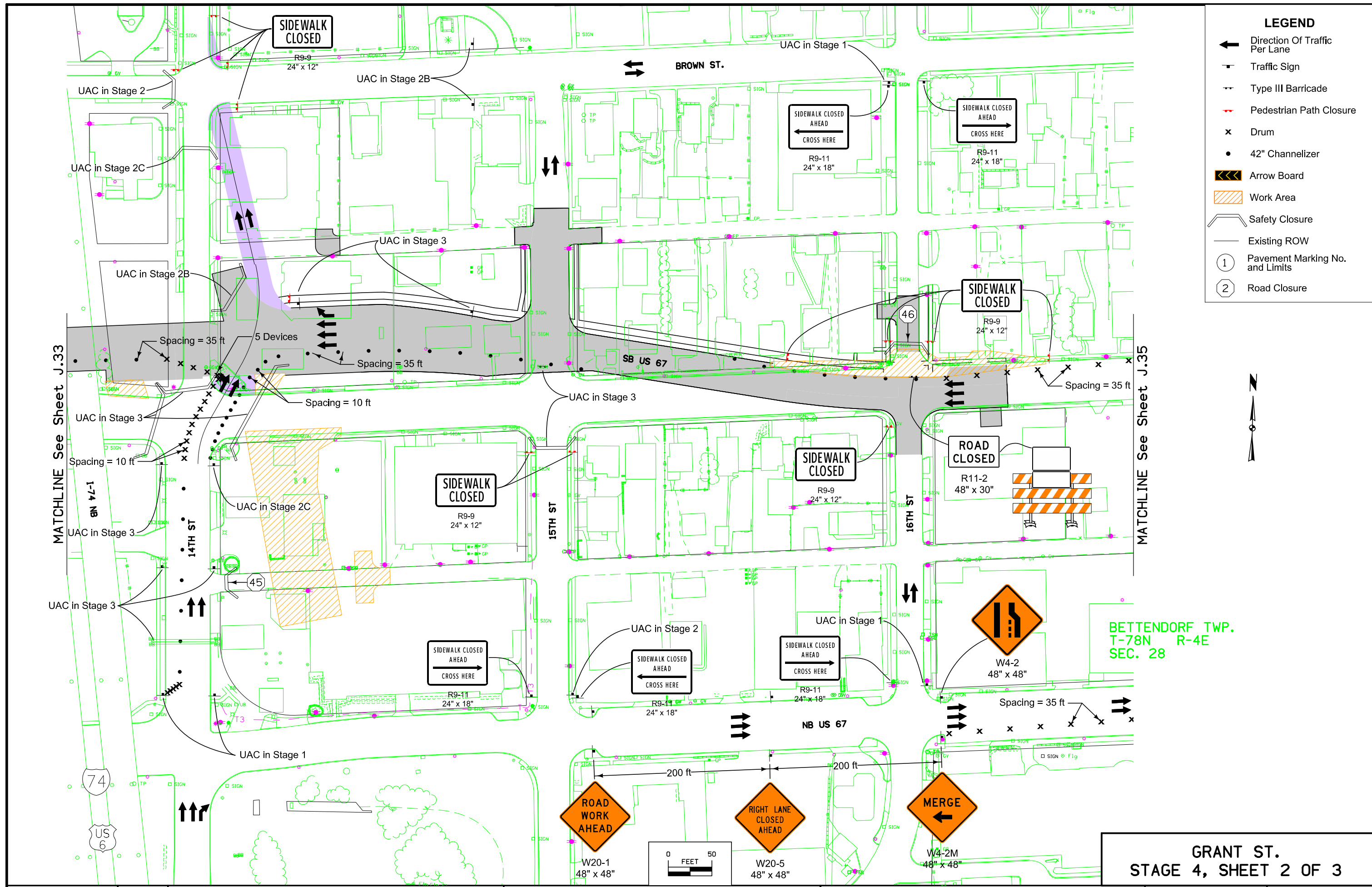
- ①-Remove temporary pavement between Existing Grant and Relocated SB US 67.
- ②-Construct future I-74 Ramp B bridge approach
- ③-US 67 SB Grant St. temporary pavement can be removed and replaced with relocated US 67 SB Grant St. on the northwest and northeast
- ④-Construct temporary pavement on south east side of NB US 67
- ⑤-Construct alley turnaround to the East of Ramp B.





**GRANT ST.  
STAGE 4, SHEET 1 OF 3**





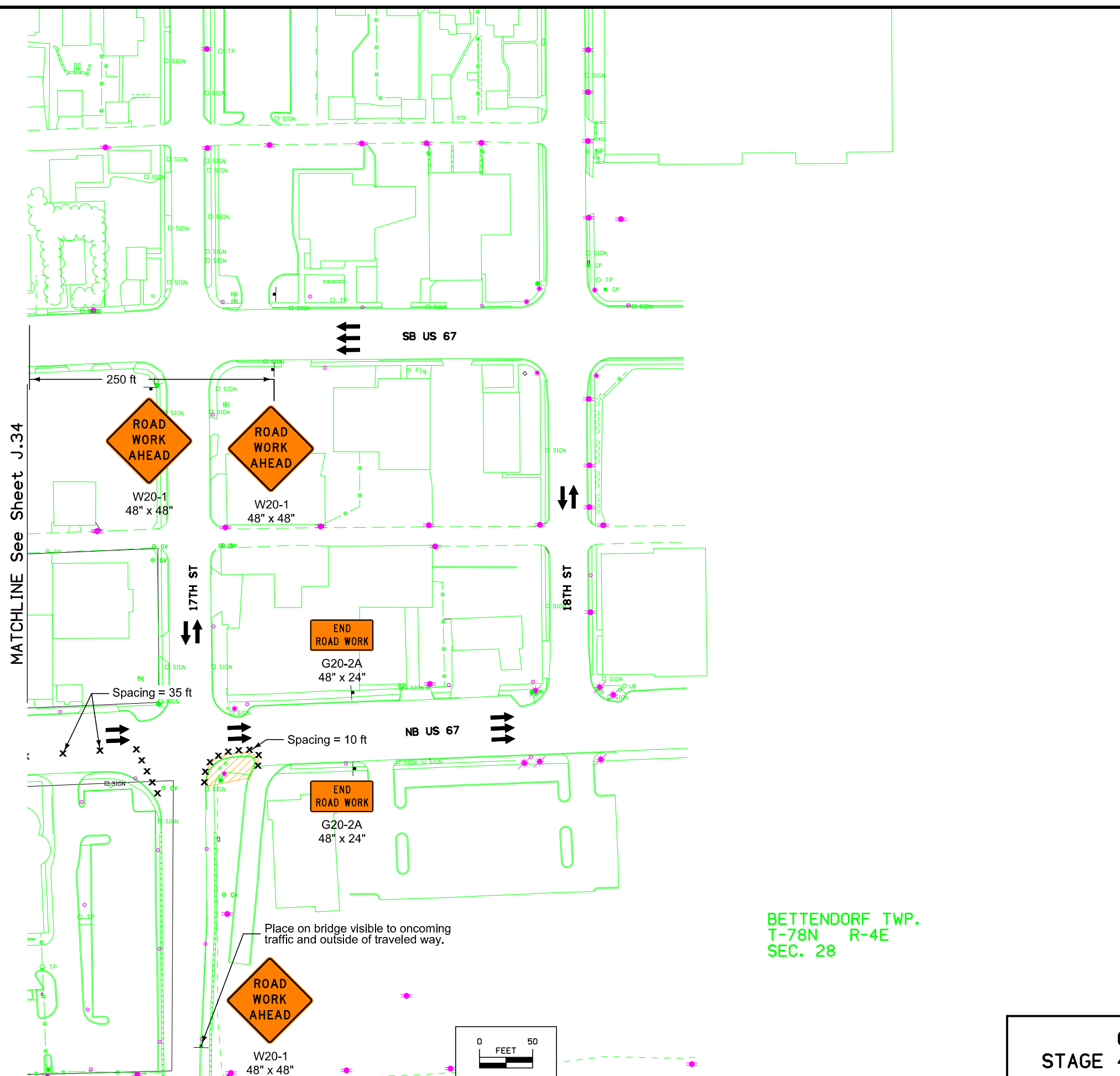
**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊥ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ◀▶▶▶ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure



BETTENDORF TWP.  
T-78N R-4E  
SEC. 28

**GRANT ST.  
STAGE 4, SHEET 2 OF 3**



**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊣ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ◀◀◀ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure



BETTENDORF TWP.  
T-78N R-4E  
SEC. 28

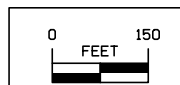
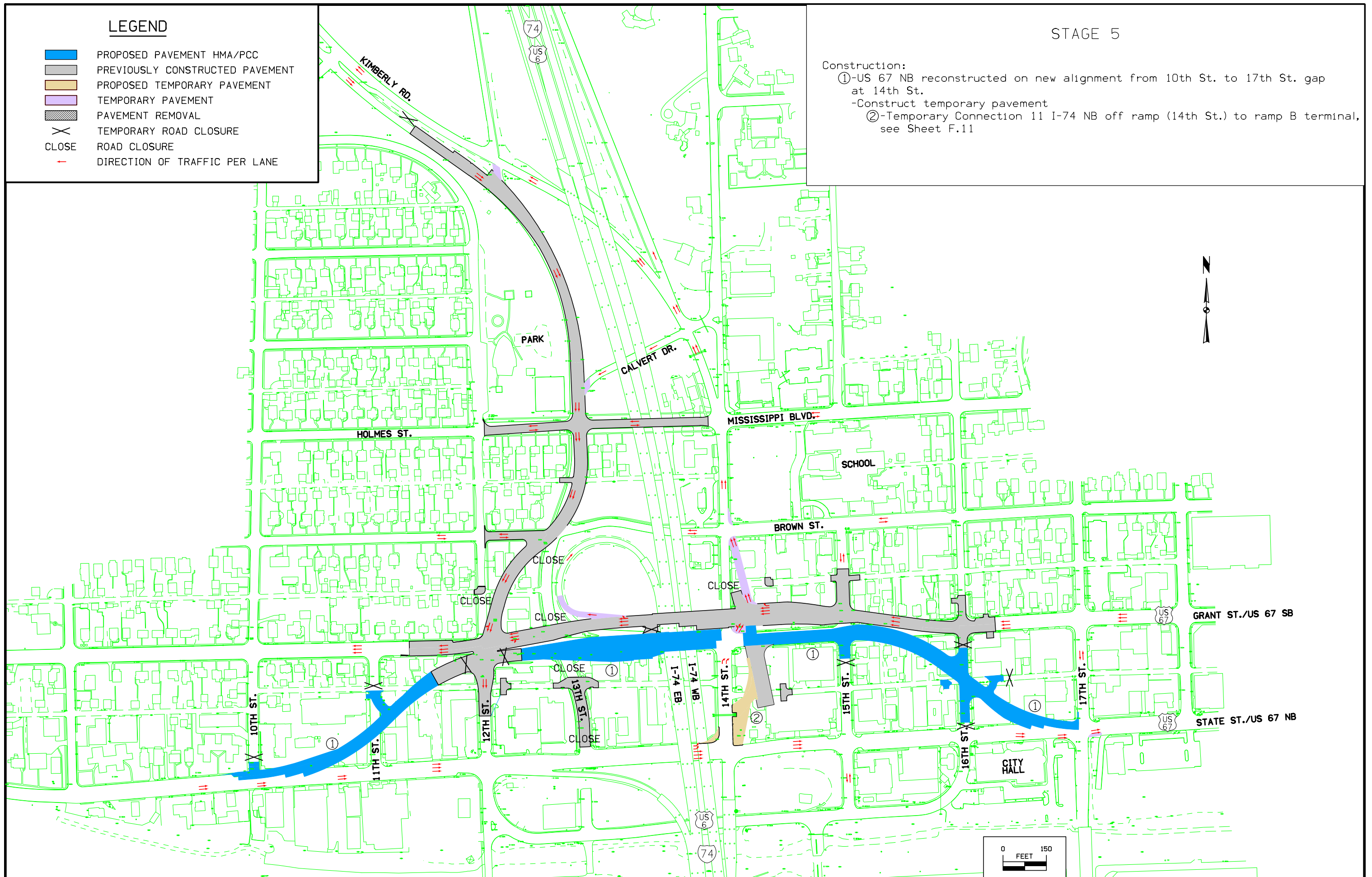
**GRANT ST.  
STAGE 4, SHEET 3 OF 3**

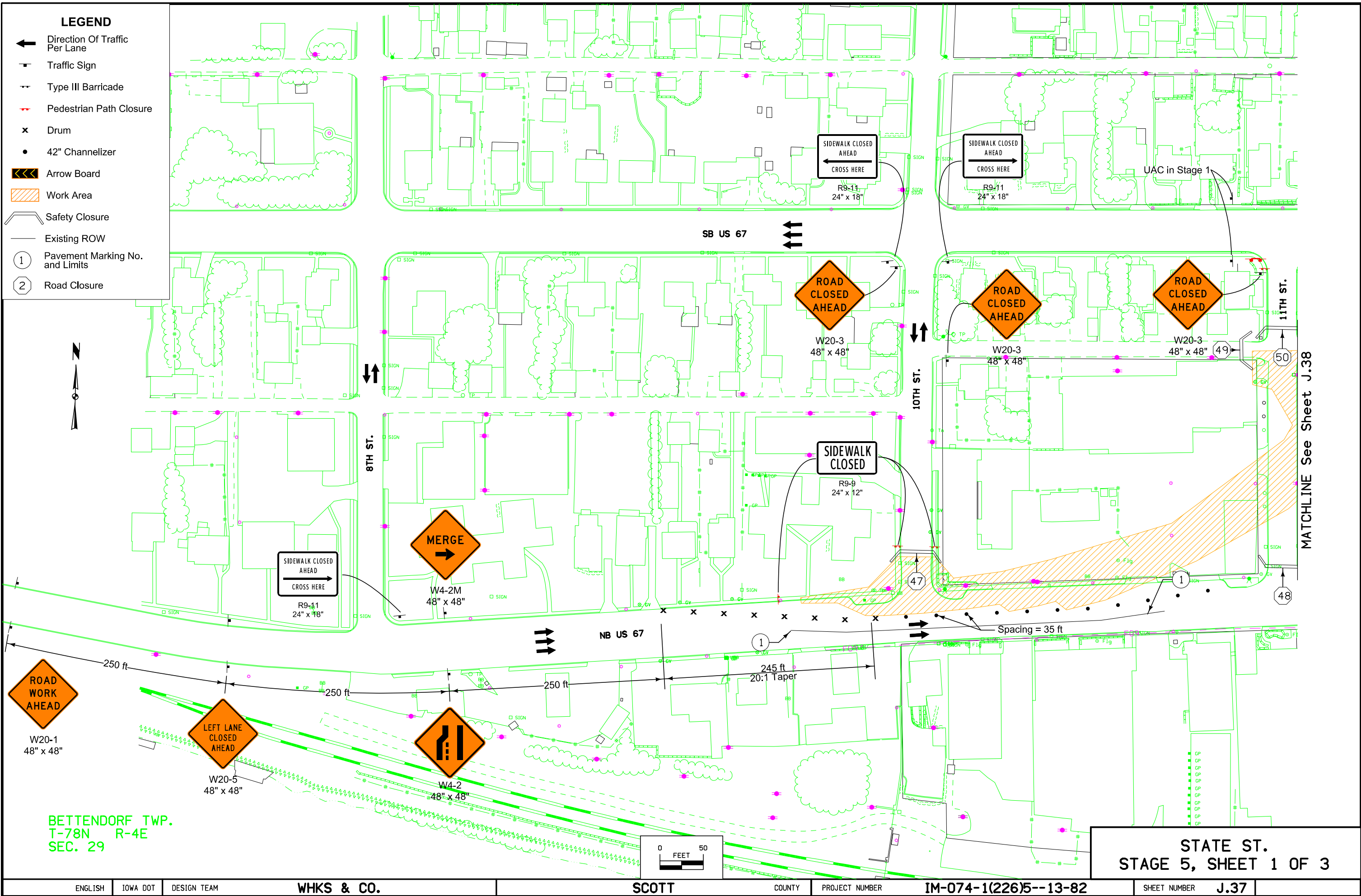
**LEGEND**

- PROPOSED PAVEMENT HMA/PCC
- PREVIOUSLY CONSTRUCTED PAVEMENT
- PROPOSED TEMPORARY PAVEMENT
- TEMPORARY PAVEMENT
- PAVEMENT REMOVAL
- TEMPORARY ROAD CLOSURE
- ROAD CLOSURE
- DIRECTION OF TRAFFIC PER LANE

STAGE 5

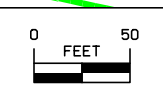
Construction:  
 ①-US 67 NB reconstructed on new alignment from 10th St. to 17th St. gap at 14th St.  
 -Construct temporary pavement  
 ②-Temporary Connection 11 I-74 NB off ramp (14th St.) to ramp B terminal, see Sheet F.11



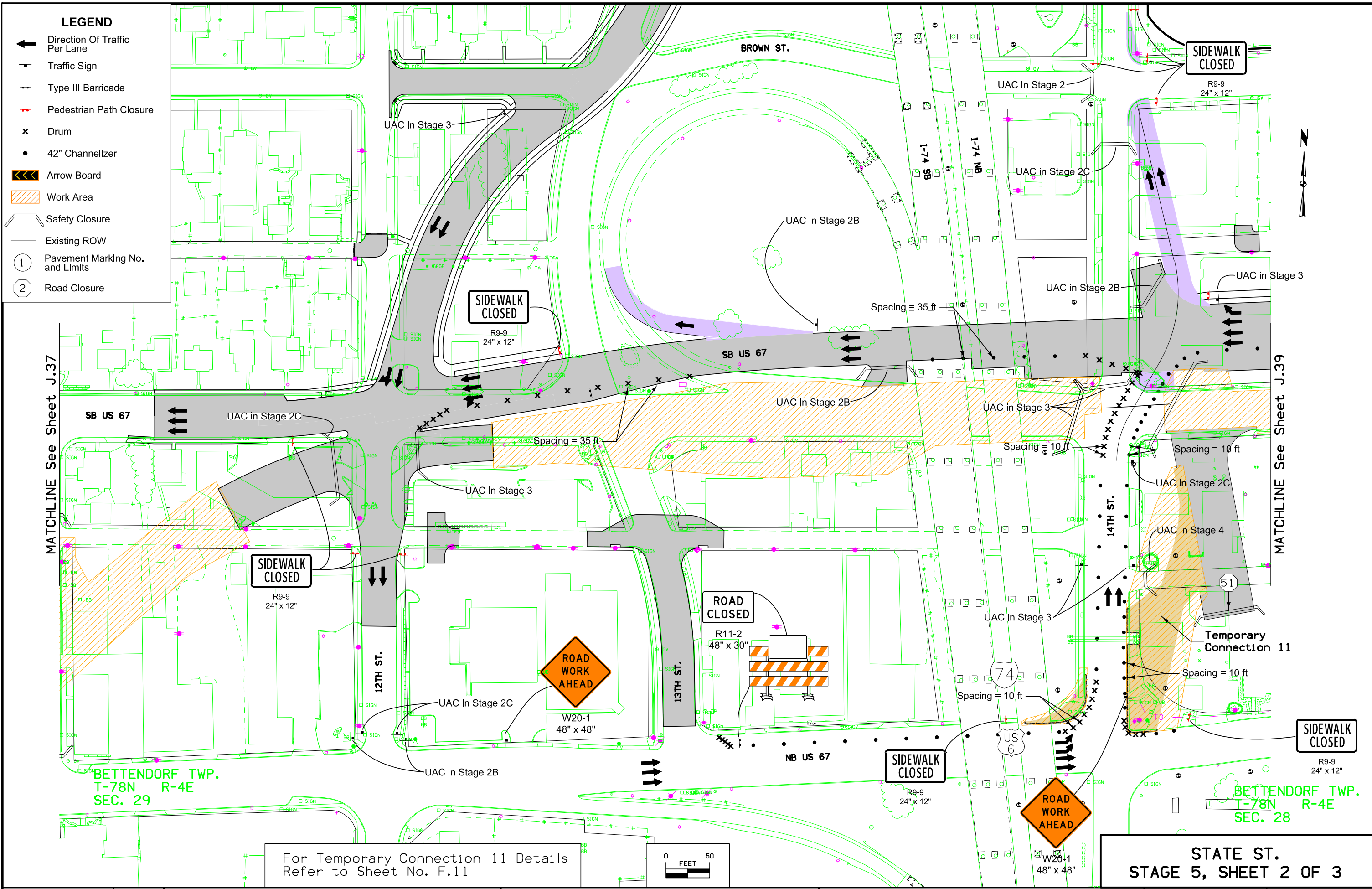


- LEGEND**
- ← Direction Of Traffic Per Lane
  - ⚡ Traffic Sign
  - Type III Barricade
  - Pedestrian Path Closure
  - x Drum
  - 42" Channelizer
  - ▶▶▶ Arrow Board
  - ▨ Work Area
  - ⌒ Safety Closure
  - Existing ROW
  - ① Pavement Marking No. and Limits
  - ② Road Closure

BETTENDORF TWP.  
T-78N R-4E  
SEC. 29



STATE ST.  
STAGE 5, SHEET 1 OF 3



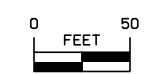
- LEGEND**
- ← Direction Of Traffic Per Lane
  - ⊠ Traffic Sign
  - Type III Barricade
  - Pedestrian Path Closure
  - x Drum
  - 42" Channelizer
  - ▶▶▶ Arrow Board
  - ▨ Work Area
  - ⌒ Safety Closure
  - Existing ROW
  - ① Pavement Marking No. and Limits
  - ② Road Closure

MATCHLINE See Sheet J.37

MATCHLINE See Sheet J.39



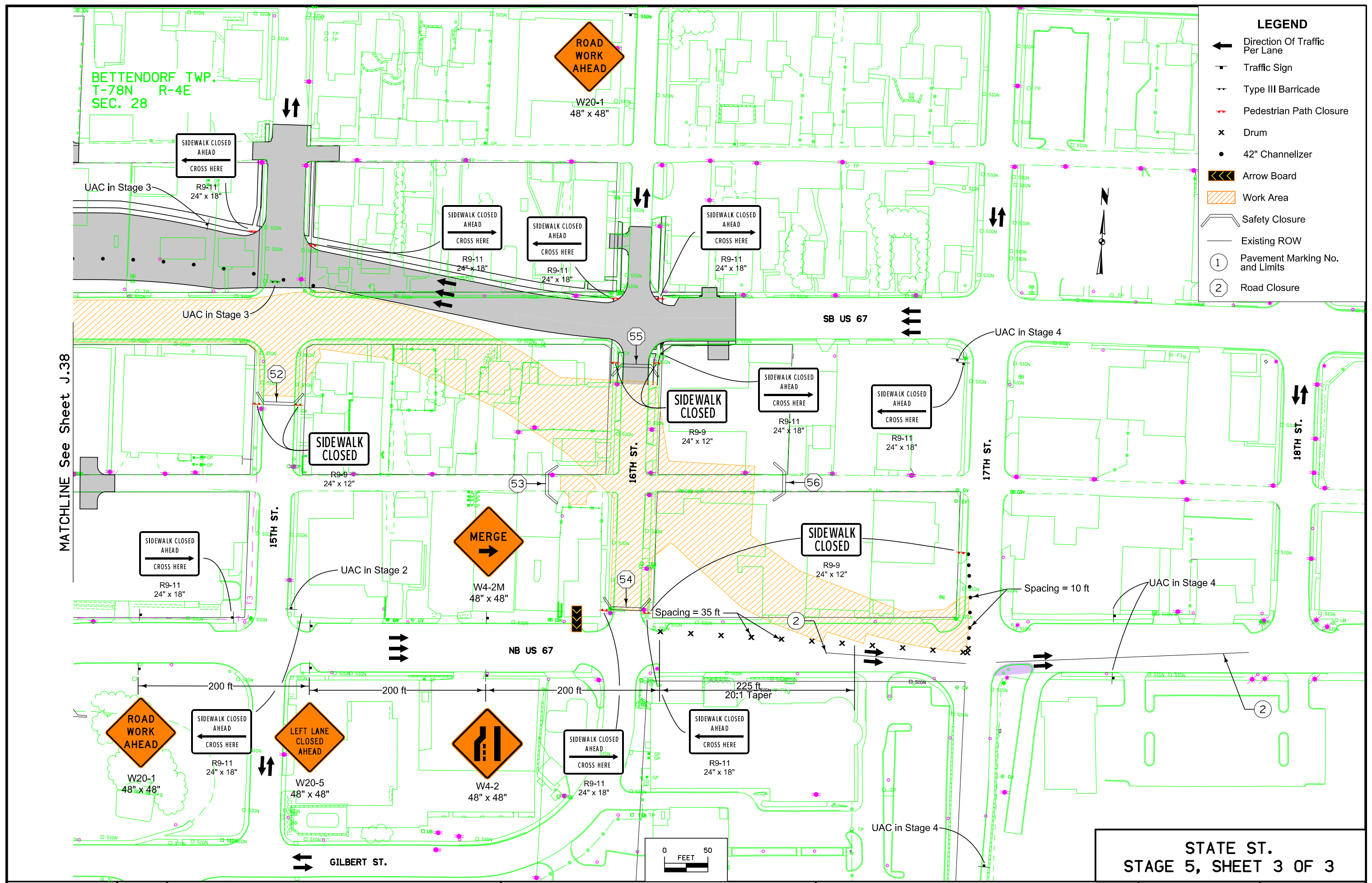
For Temporary Connection 11 Details Refer to Sheet No. F.11



**STATE ST.  
STAGE 5, SHEET 2 OF 3**

**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊥ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ◀▶▶▶ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure



MATCHLINE See Sheet J.38

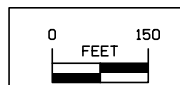
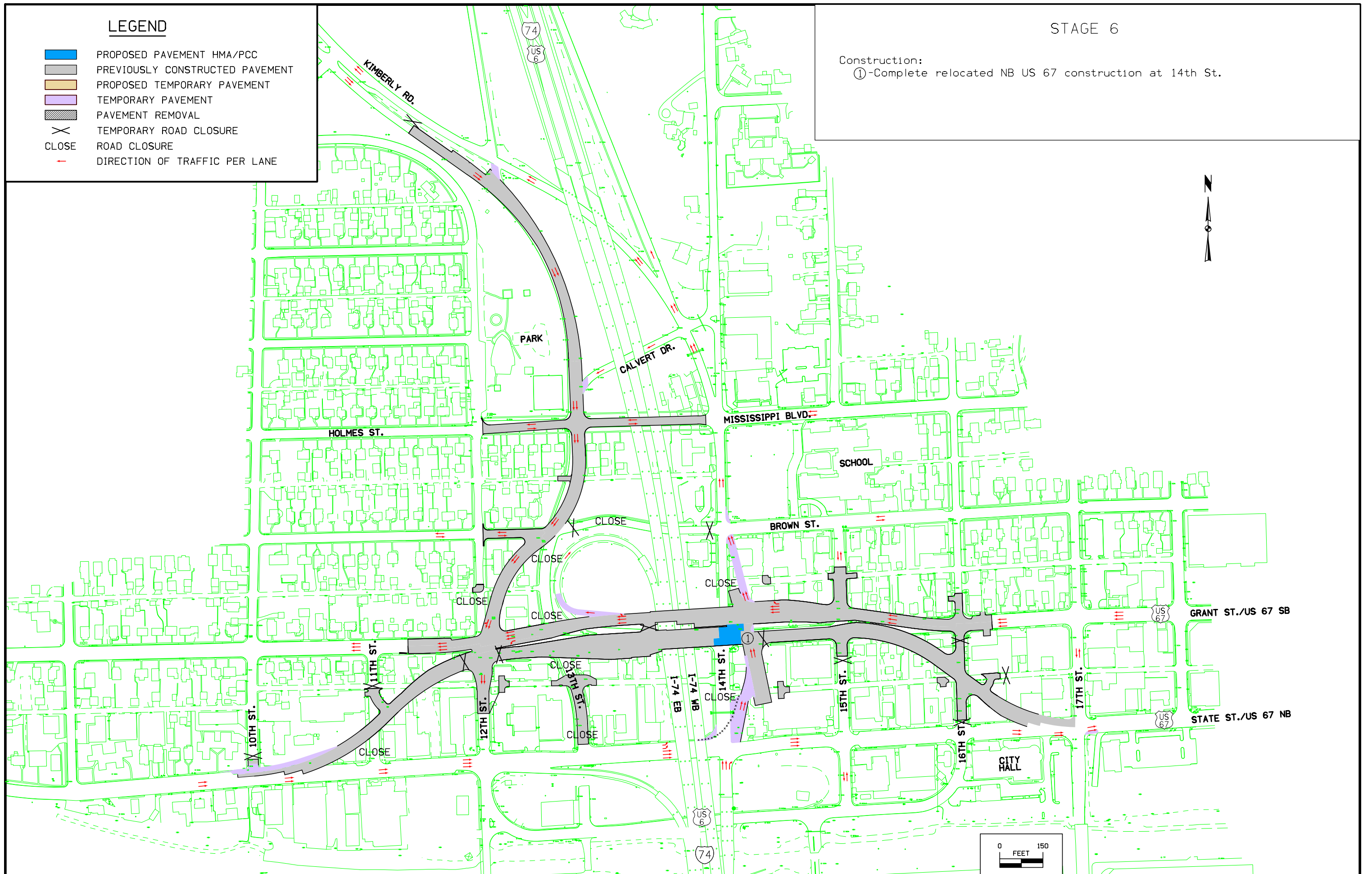
**STATE ST.  
STAGE 5, SHEET 3 OF 3**

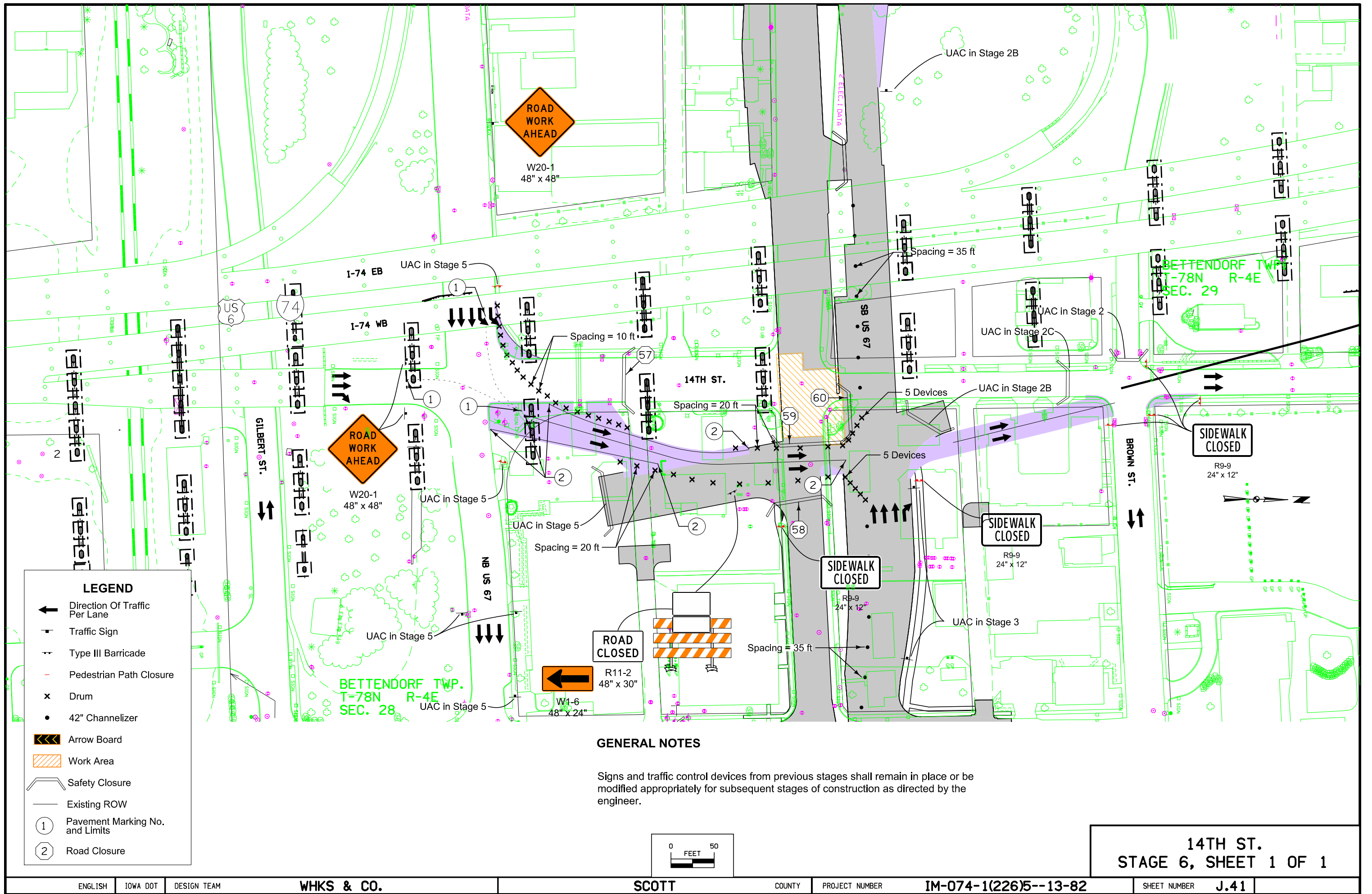
**LEGEND**

- PROPOSED PAVEMENT HMA/PCC
- PREVIOUSLY CONSTRUCTED PAVEMENT
- PROPOSED TEMPORARY PAVEMENT
- TEMPORARY PAVEMENT
- PAVEMENT REMOVAL
- TEMPORARY ROAD CLOSURE
- ROAD CLOSURE
- DIRECTION OF TRAFFIC PER LANE

STAGE 6

Construction:  
 ①-Complete relocated NB US 67 construction at 14th St.





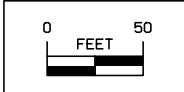
**GENERAL NOTES**

Signs and traffic control devices from previous stages shall remain in place or be modified appropriately for subsequent stages of construction as directed by the engineer.

**14TH ST.  
STAGE 6, SHEET 1 OF 1**

**LEGEND**

- ↑ Direction Of Traffic Per Lane
- ⊥ Traffic Sign
- ⊥ Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure





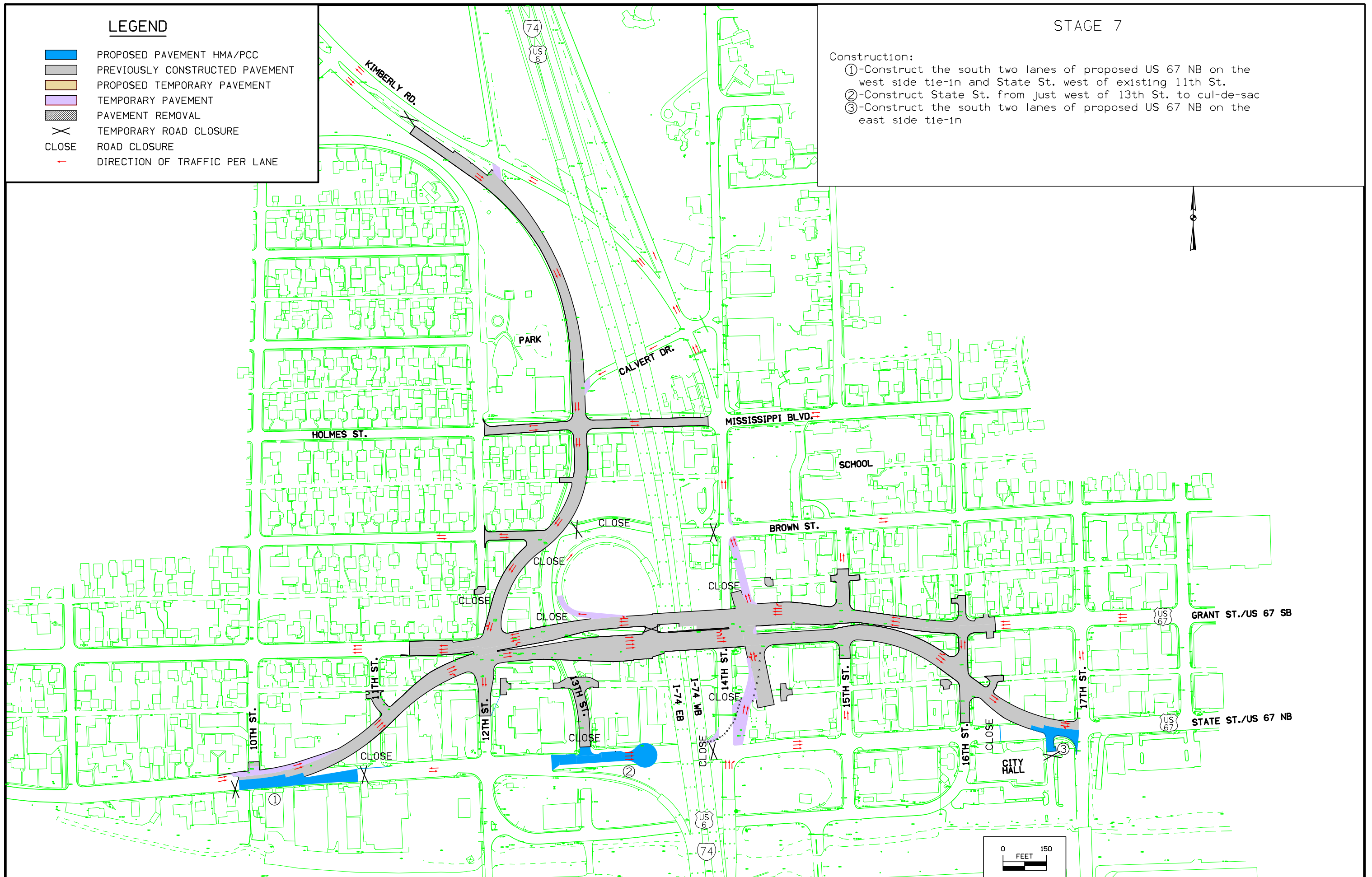
**LEGEND**

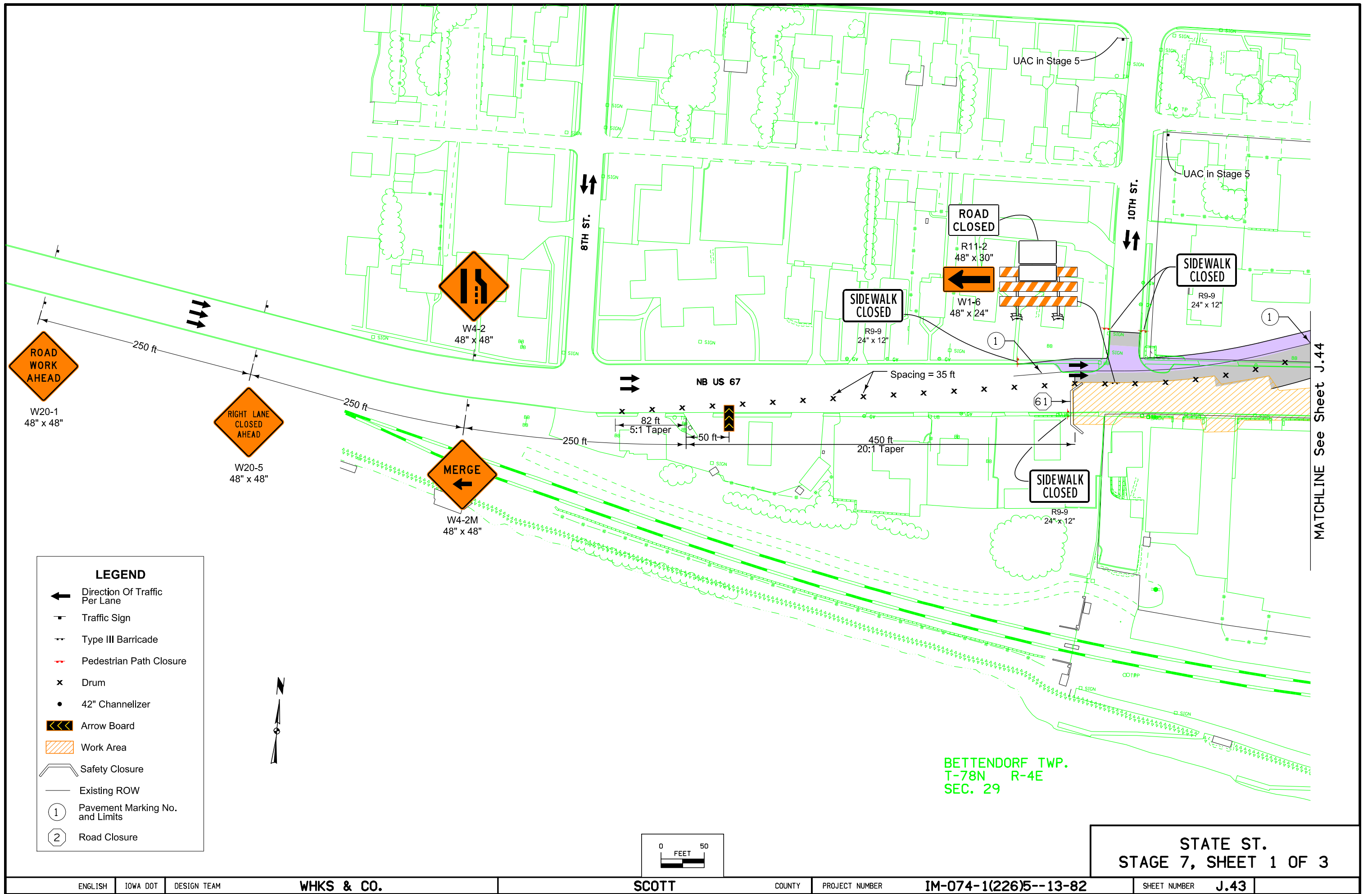
- PROPOSED PAVEMENT HMA/PCC
- PREVIOUSLY CONSTRUCTED PAVEMENT
- PROPOSED TEMPORARY PAVEMENT
- TEMPORARY PAVEMENT
- PAVEMENT REMOVAL
- TEMPORARY ROAD CLOSURE
- ROAD CLOSURE
- DIRECTION OF TRAFFIC PER LANE

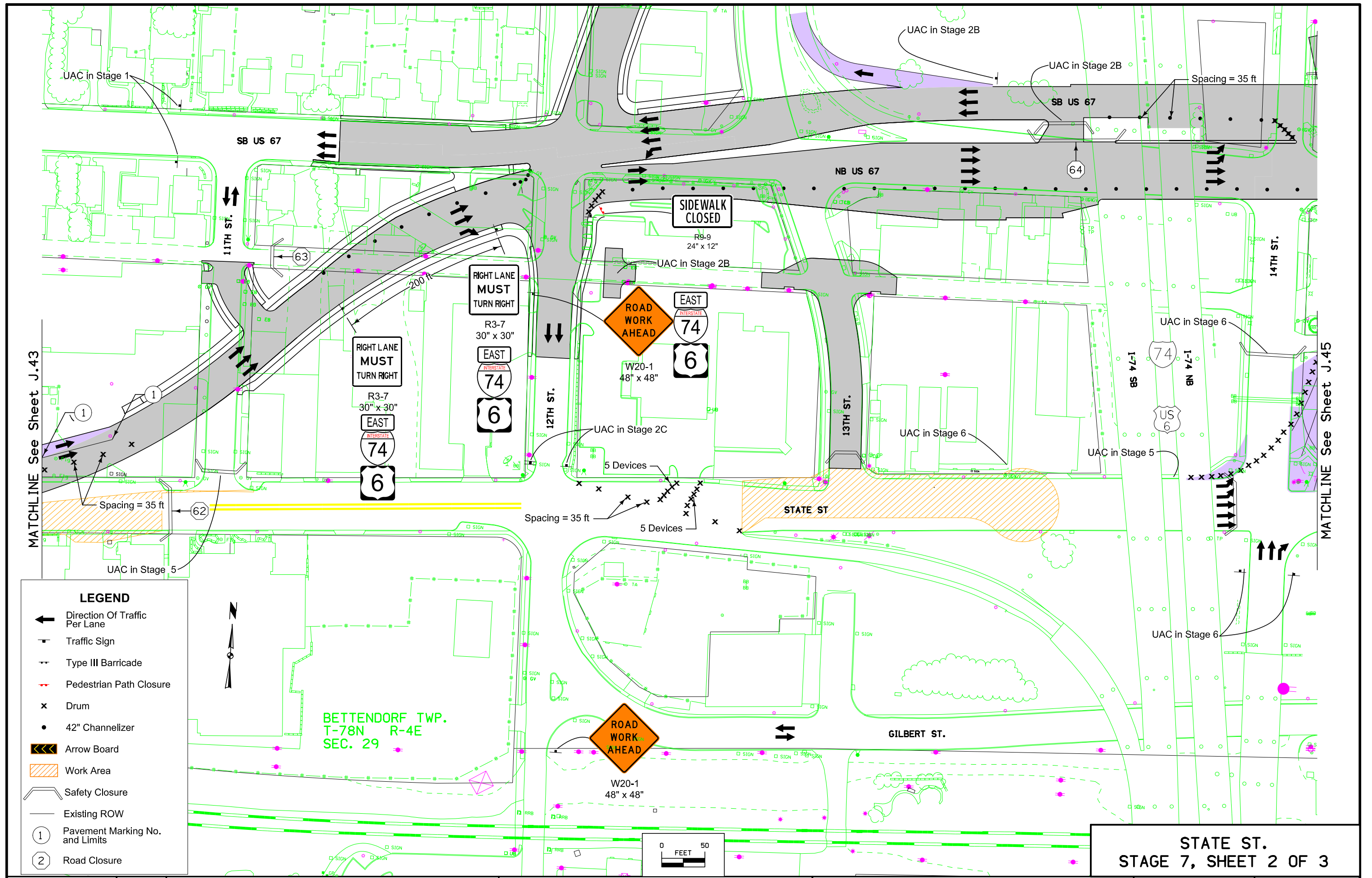
STAGE 7

Construction:

- ①-Construct the south two lanes of proposed US 67 NB on the west side tie-in and State St. west of existing 11th St.
- ②-Construct State St. from just west of 13th St. to cul-de-sac
- ③-Construct the south two lanes of proposed US 67 NB on the east side tie-in







MATCHLINE See Sheet J.43

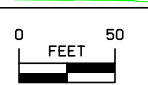
MATCHLINE See Sheet J.45

**LEGEND**

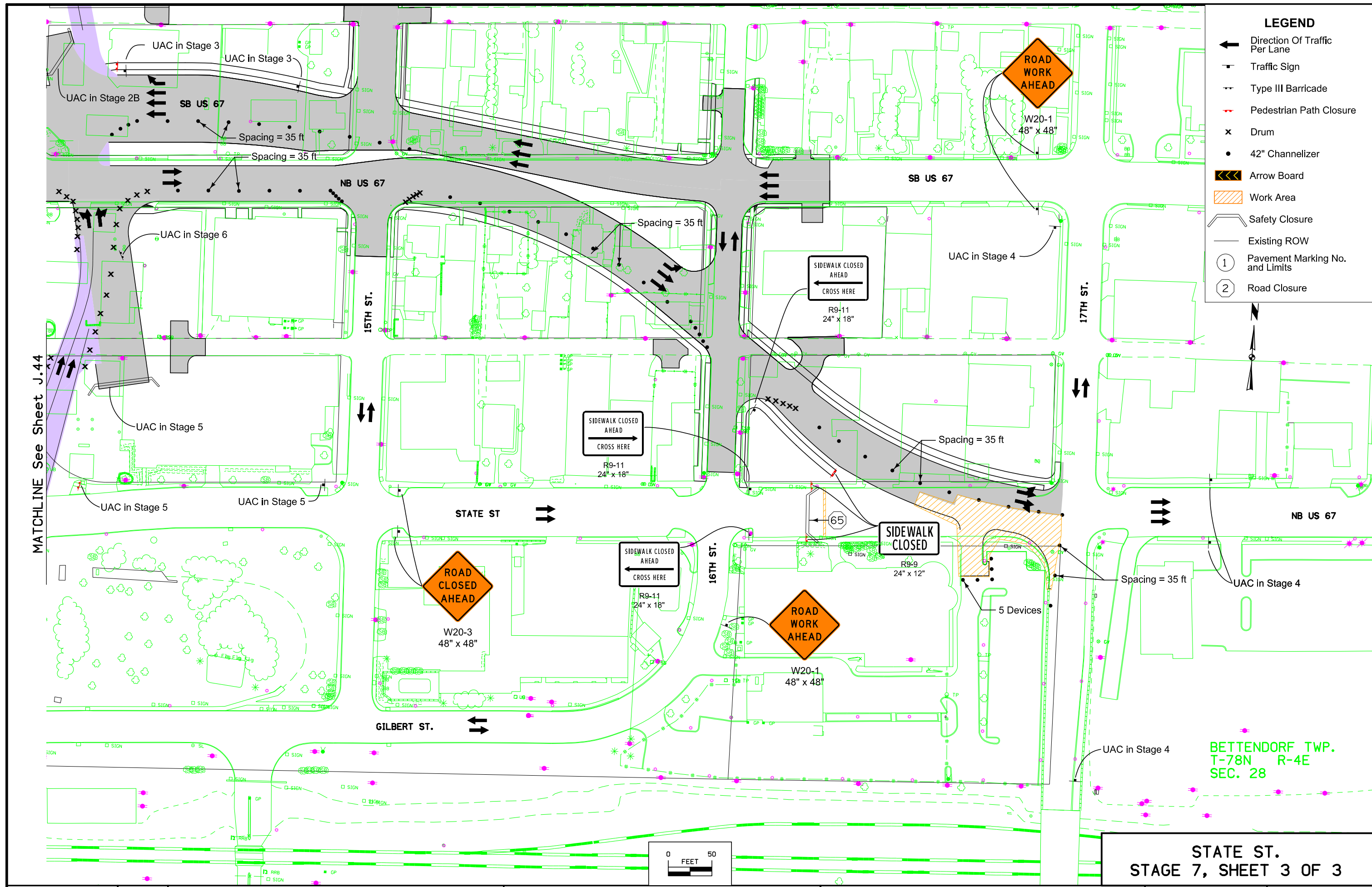
- ← Direction Of Traffic Per Lane
- ⊣ Traffic Sign
- ⋯ Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ▭ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure



BETTENDORF TWP.  
T-78N R-4E  
SEC. 29



**STATE ST.  
STAGE 7, SHEET 2 OF 3**

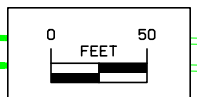


**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊥ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ◀▶▶▶ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure

MATCHLINE See Sheet J.44

BETTENDORF TWP.  
T-78N R-4E  
SEC. 28



**STATE ST.  
STAGE 7, SHEET 3 OF 3**

**LEGEND**

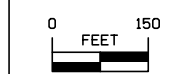
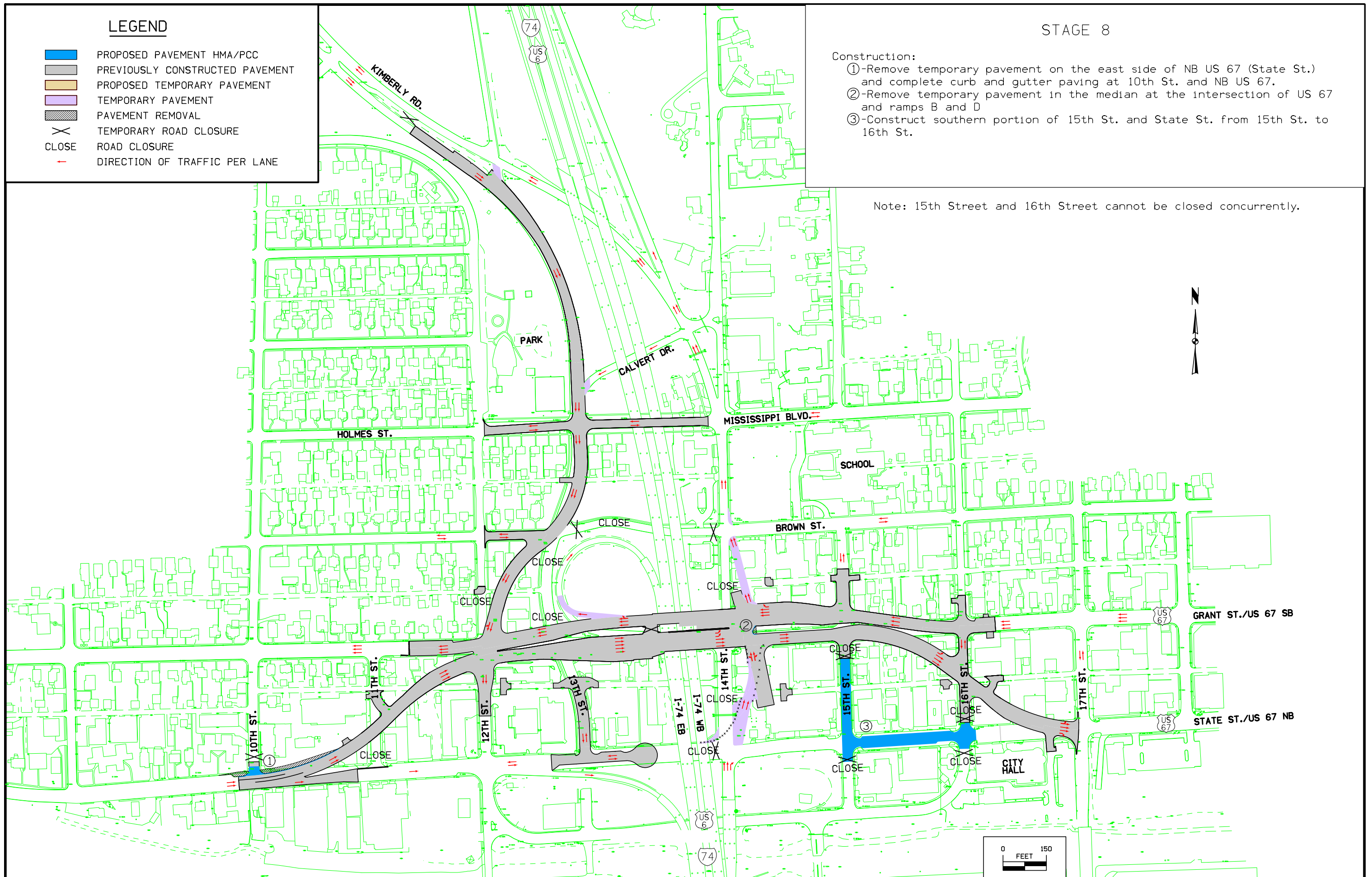
- PROPOSED PAVEMENT HMA/PCC
- PREVIOUSLY CONSTRUCTED PAVEMENT
- PROPOSED TEMPORARY PAVEMENT
- TEMPORARY PAVEMENT
- PAVEMENT REMOVAL
- TEMPORARY ROAD CLOSURE
- ROAD CLOSURE
- DIRECTION OF TRAFFIC PER LANE

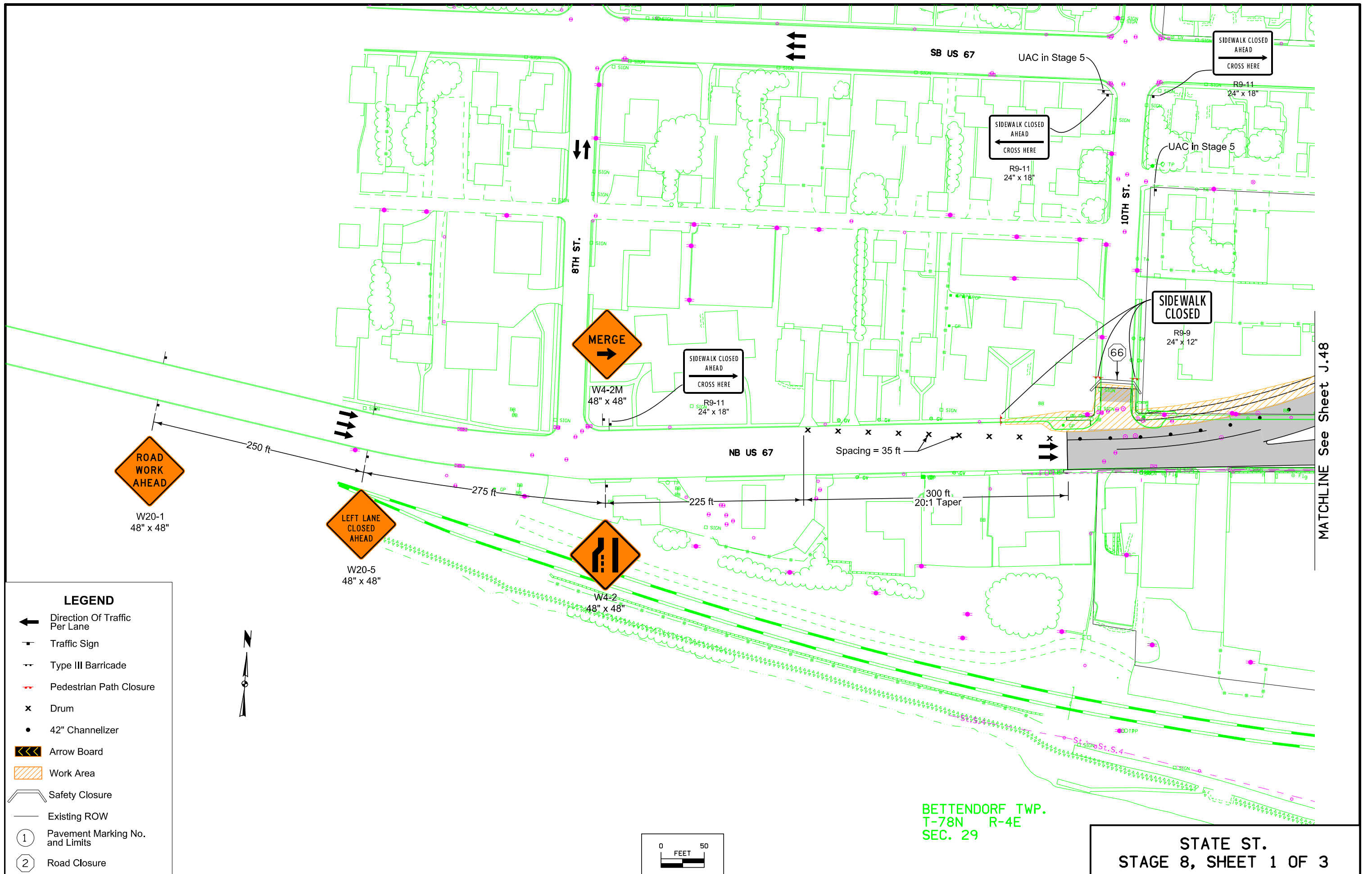
**STAGE 8**

**Construction:**

- ①-Remove temporary pavement on the east side of NB US 67 (State St.) and complete curb and gutter paving at 10th St. and NB US 67.
- ②-Remove temporary pavement in the median at the intersection of US 67 and ramps B and D
- ③-Construct southern portion of 15th St. and State St. from 15th St. to 16th St.

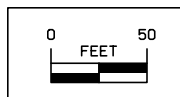
Note: 15th Street and 16th Street cannot be closed concurrently.



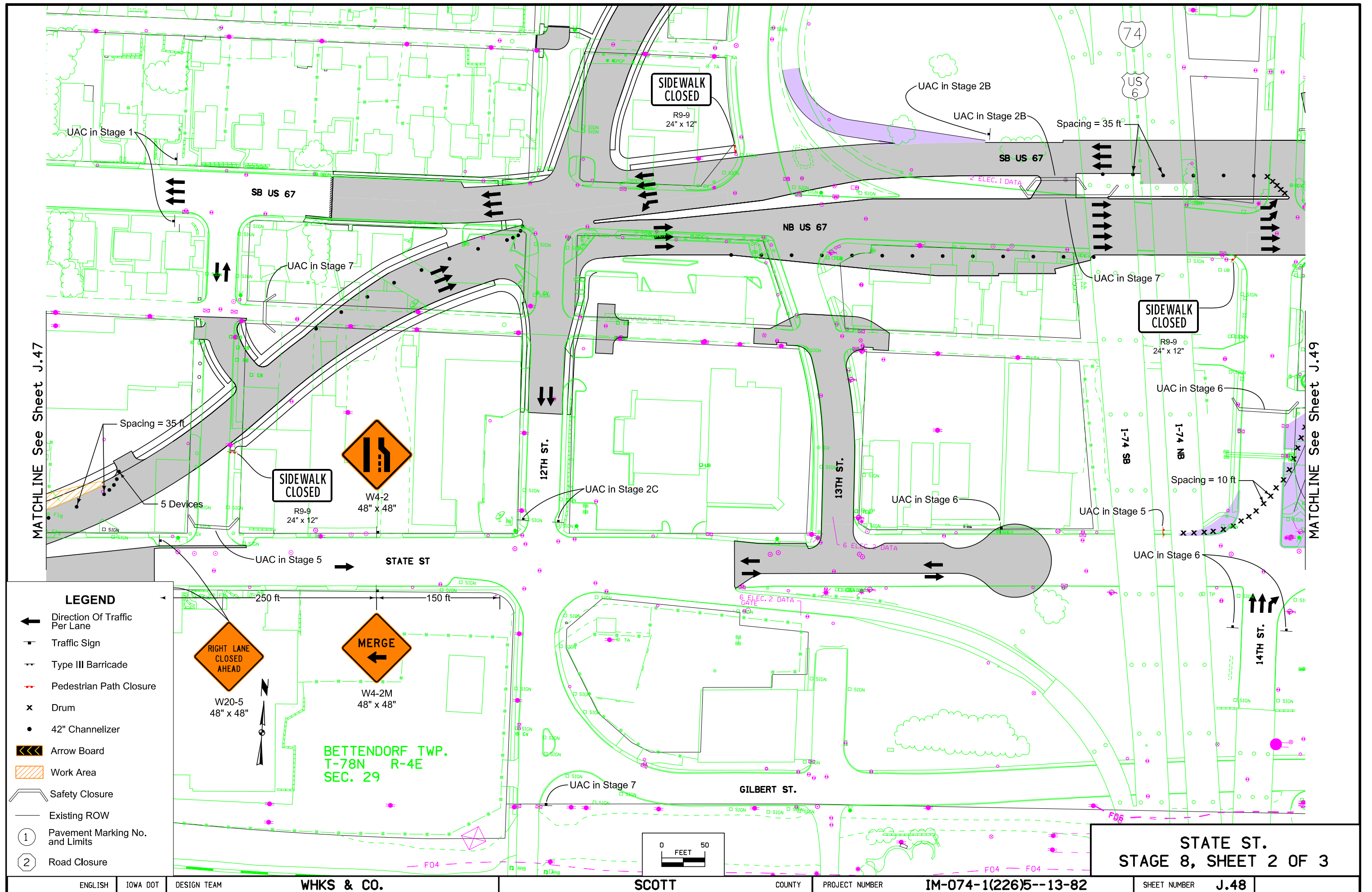


**LEGEND**

- ← Direction Of Traffic Per Lane
- ▬ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ▶▶▶ Arrow Board
- ▨ Work Area
- ▭ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure



**STATE ST.  
STAGE 8, SHEET 1 OF 3**

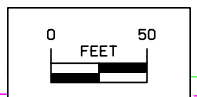
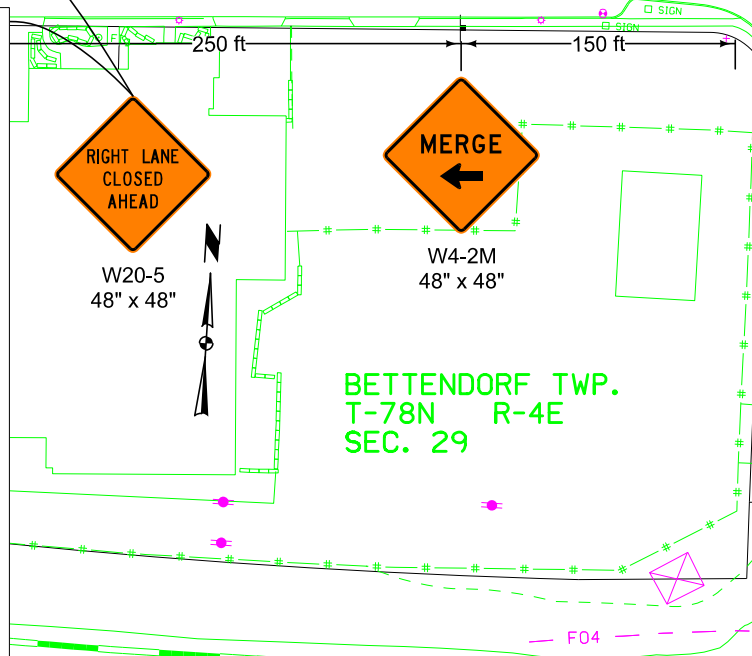


MATCHLINE See Sheet J.47

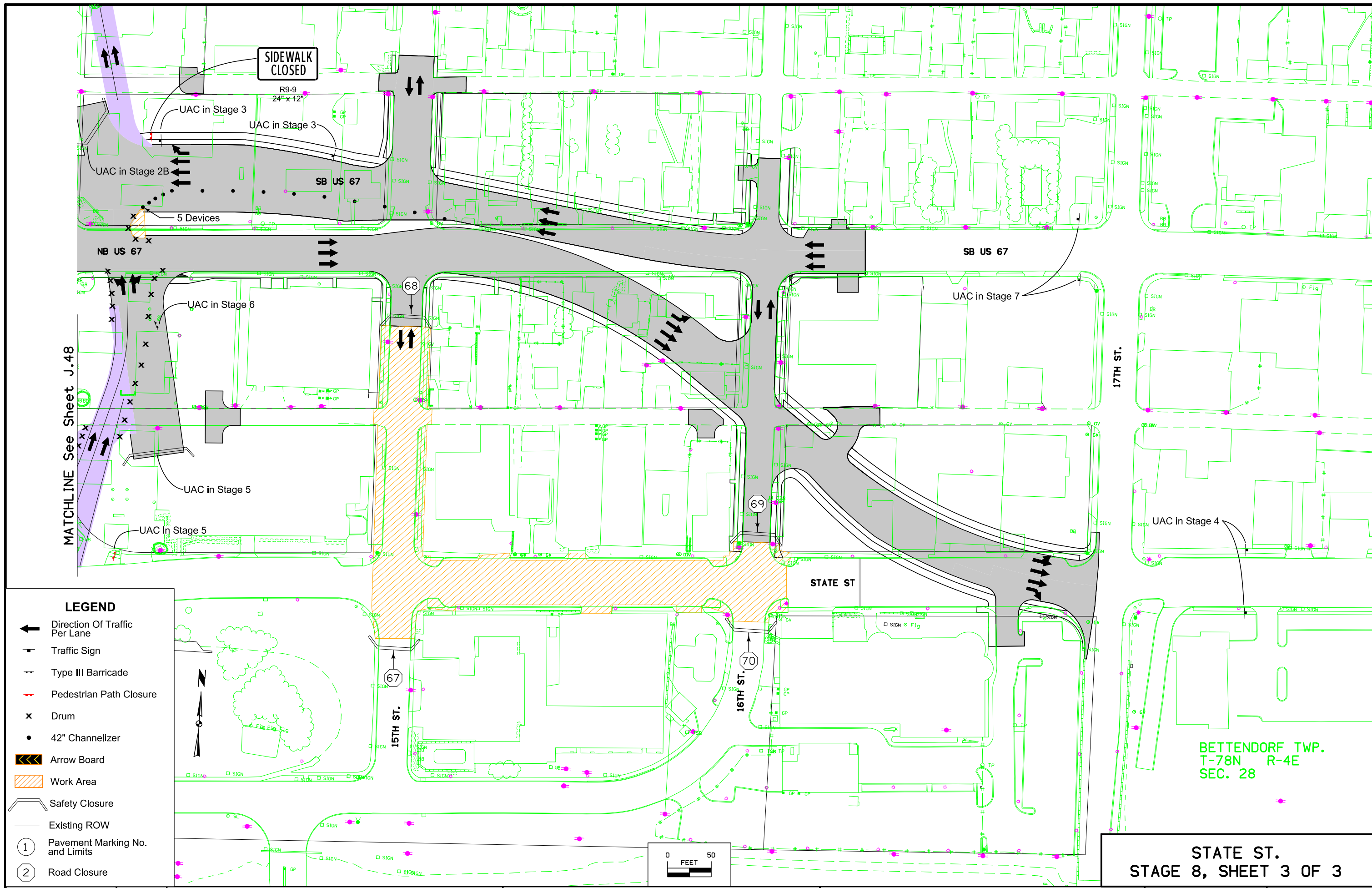
MATCHLINE See Sheet J.49

**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊣ Traffic Sign
- ⊣ Type III Barricade
- ⊣ Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ⏏ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure



**STATE ST.  
STAGE 8, SHEET 2 OF 3**



MATCHLINE See Sheet J.48

**SIDEWALK CLOSED**

R9-9  
24" x 12"

UAC in Stage 3  
UAC in Stage 3

UAC in Stage 2B

5 Devices

SB US 67

NB US 67

UAC in Stage 6

UAC in Stage 5

UAC in Stage 5

SB US 67

UAC in Stage 7

17TH ST.

UAC in Stage 4

STATE ST

15TH ST.

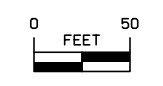
16TH ST.

BETTENDORF TWP.  
T-78N R-4E  
SEC. 28

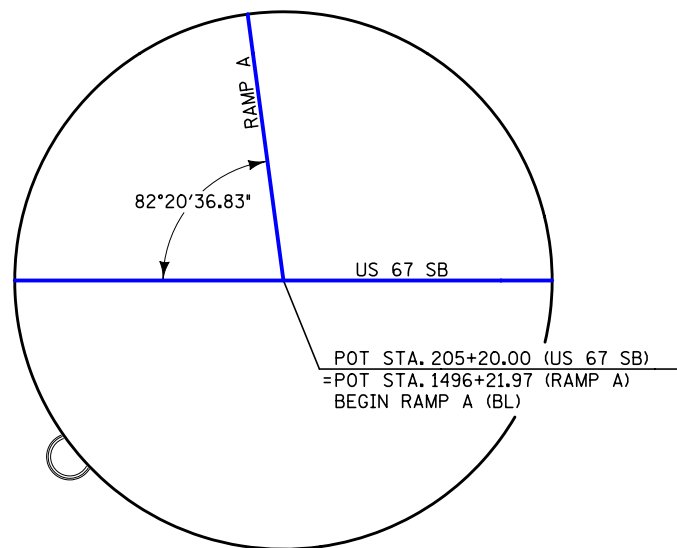
**STATE ST.  
STAGE 8, SHEET 3 OF 3**

**LEGEND**

- ← Direction Of Traffic Per Lane
- ⊠ Traffic Sign
- Type III Barricade
- Pedestrian Path Closure
- x Drum
- 42" Channelizer
- ▶▶▶ Arrow Board
- ▨ Work Area
- ⌒ Safety Closure
- Existing ROW
- ① Pavement Marking No. and Limits
- ② Road Closure



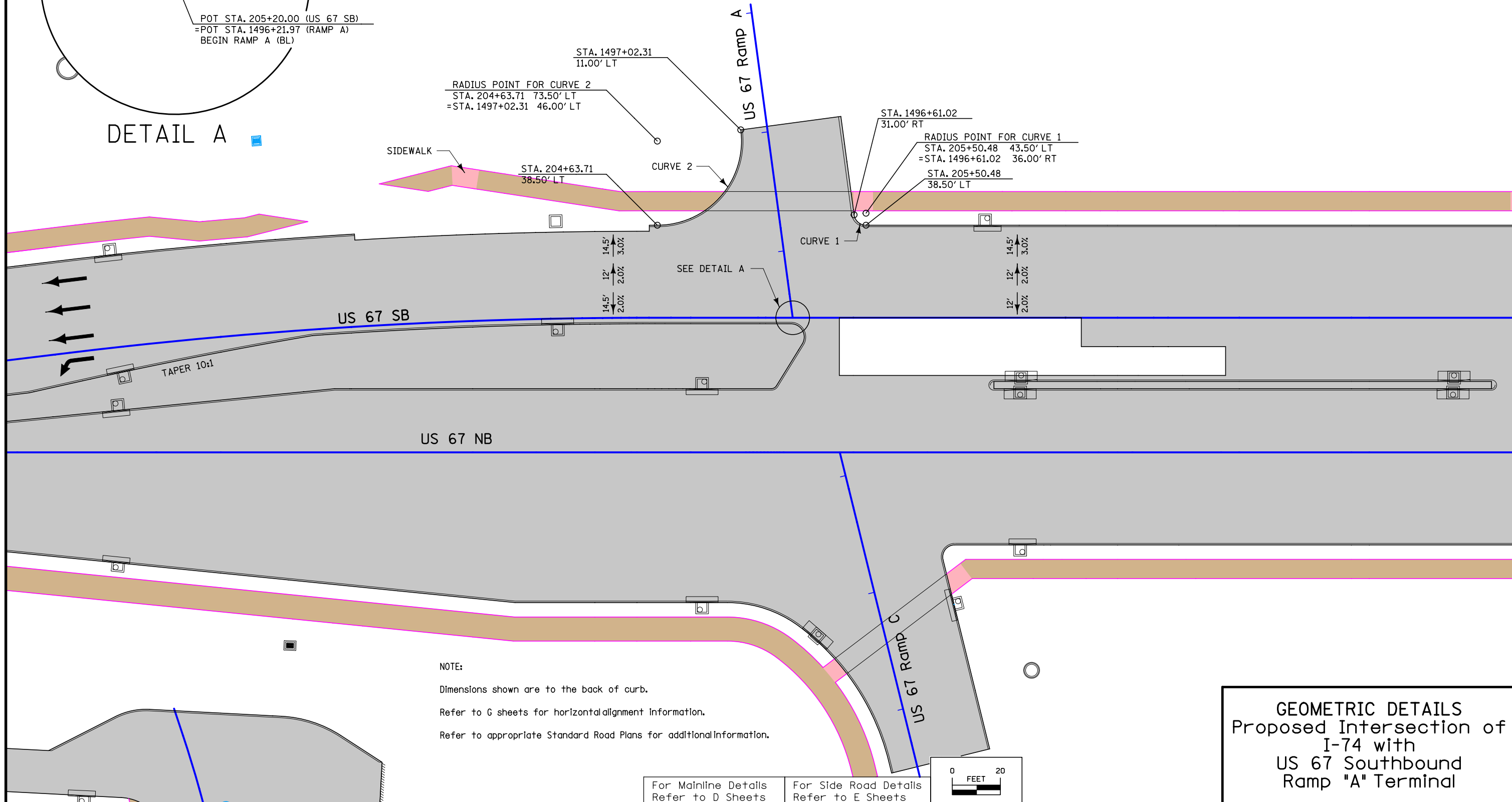




DETAIL A

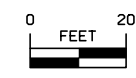
INTERSECTION CIRCULAR CURVE DATA						
Curve Number	$\Delta$	D	T	L	E	R
1	82° 20' 36.84" RT	785' 54" 56.13"	4.38'	7.19'	1.65'	5.00'
2	97° 39' 23.17" RT	163' 42" 8.02"	40.02'	59.66'	18.17'	35.00'

INTERSECTION CURVE COORDINATES						
Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
1	570,184.477	2,457,872.155	570,185.453	2,457,877.058	570,180.462	2,457,877.372
2	570,175.019	2,457,790.772	570,209.950	2,457,788.577	570,216.780	2,457,822.904



NOTE:  
Dimensions shown are to the back of curb.  
Refer to G sheets for horizontal alignment information.  
Refer to appropriate Standard Road Plans for additional information.

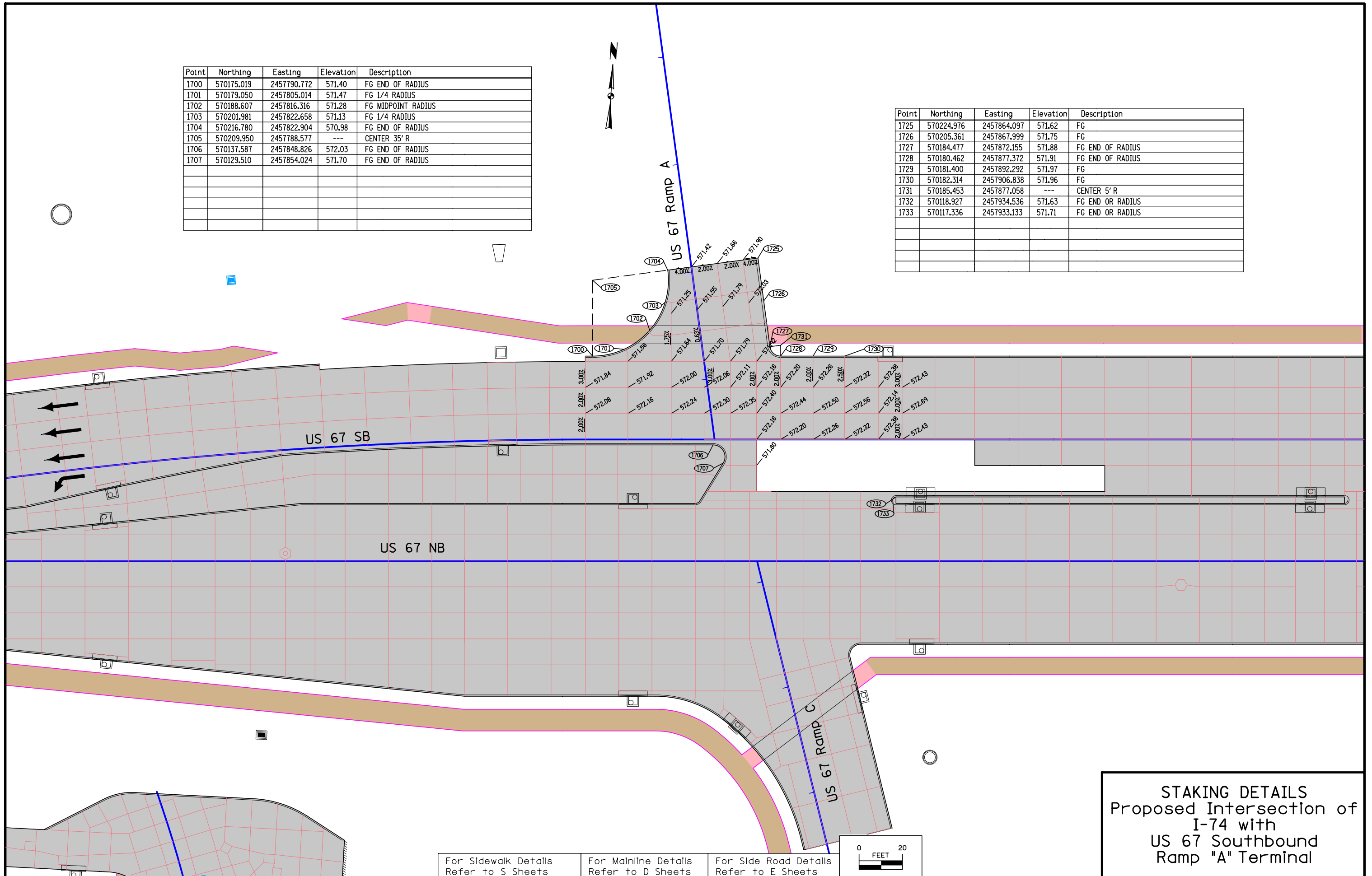
For Mainline Details Refer to D Sheets  
For Side Road Details Refer to E Sheets



GEOMETRIC DETAILS  
Proposed Intersection of  
I-74 with  
US 67 Southbound  
Ramp "A" Terminal

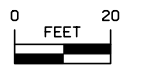
Point	Northing	Easting	Elevation	Description
1700	570175.019	2457790.772	571.40	FG END OF RADIUS
1701	570179.050	2457805.014	571.47	FG 1/4 RADIUS
1702	570188.607	2457816.316	571.28	FG MIDPOINT RADIUS
1703	570201.981	2457822.658	571.13	FG 1/4 RADIUS
1704	570216.780	2457822.904	570.98	FG END OF RADIUS
1705	570209.950	2457788.577	---	CENTER 35' R
1706	570137.587	2457848.826	572.03	FG END OF RADIUS
1707	570129.510	2457854.024	571.70	FG END OF RADIUS

Point	Northing	Easting	Elevation	Description
1725	570224.976	2457864.097	571.62	FG
1726	570205.361	2457867.999	571.75	FG
1727	570184.477	2457872.155	571.88	FG END OF RADIUS
1728	570180.462	2457877.372	571.91	FG END OF RADIUS
1729	570181.400	2457892.292	571.97	FG
1730	570182.314	2457906.838	571.96	FG
1731	570185.453	2457877.058	---	CENTER 5' R
1732	570118.927	2457934.536	571.63	FG END OR RADIUS
1733	570117.336	2457933.133	571.71	FG END OR RADIUS



STAKING DETAILS  
Proposed Intersection of  
I-74 with  
US 67 Southbound  
Ramp "A" Terminal

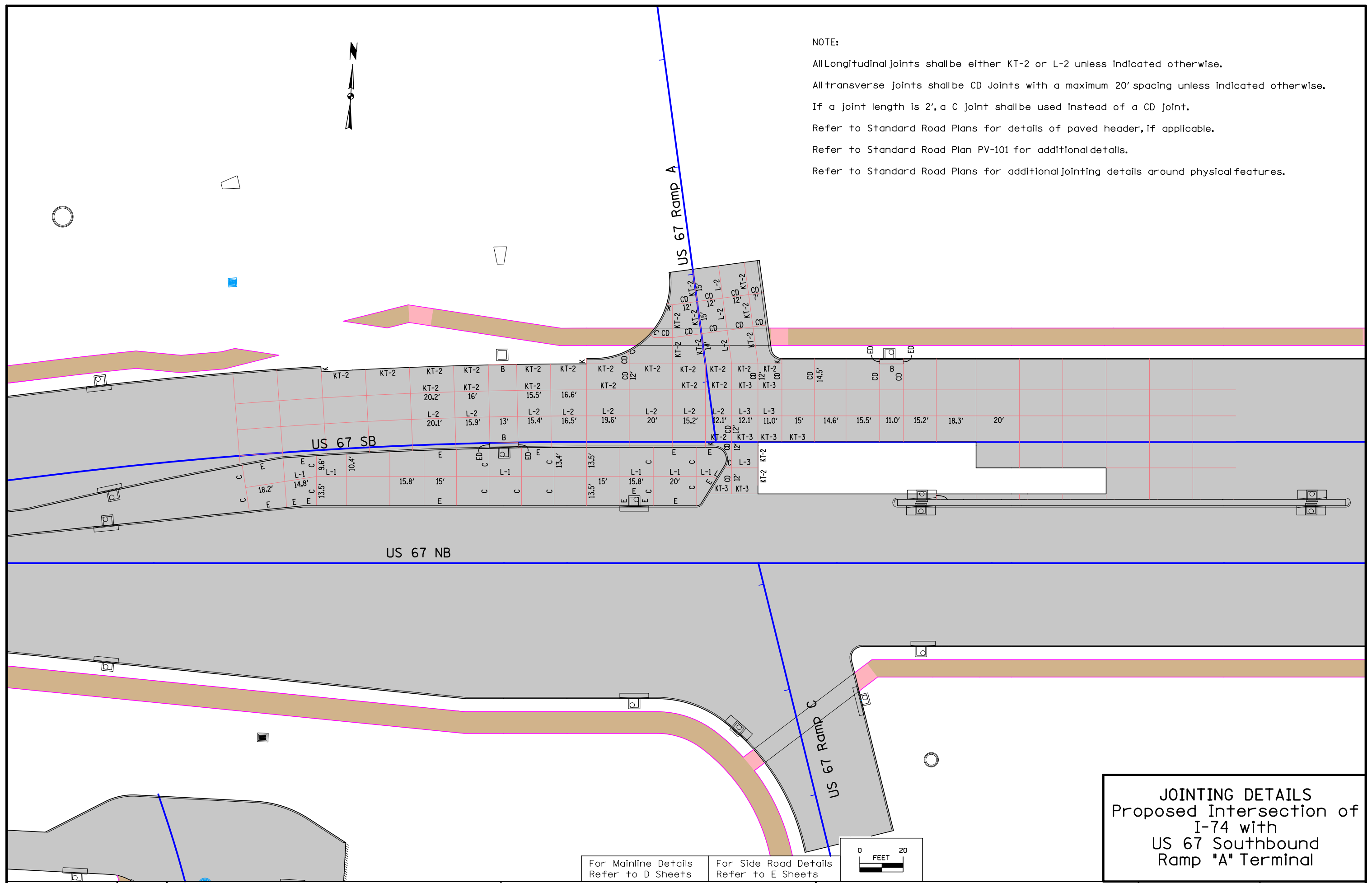
For Sidewalk Details Refer to S Sheets  
For Mainline Details Refer to D Sheets  
For Side Road Details Refer to E Sheets





NOTE:

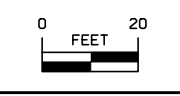
- All Longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.
- All transverse joints shall be CD Joints with a maximum 20' spacing unless indicated otherwise.
- If a joint length is 2', a C joint shall be used instead of a CD joint.
- Refer to Standard Road Plans for details of paved header, if applicable.
- Refer to Standard Road Plan PV-101 for additional details.
- Refer to Standard Road Plans for additional jointing details around physical features.



JOINTING DETAILS  
 Proposed Intersection of  
 I-74 with  
 US 67 Southbound  
 Ramp "A" Terminal

For Mainline Details  
 Refer to D Sheets

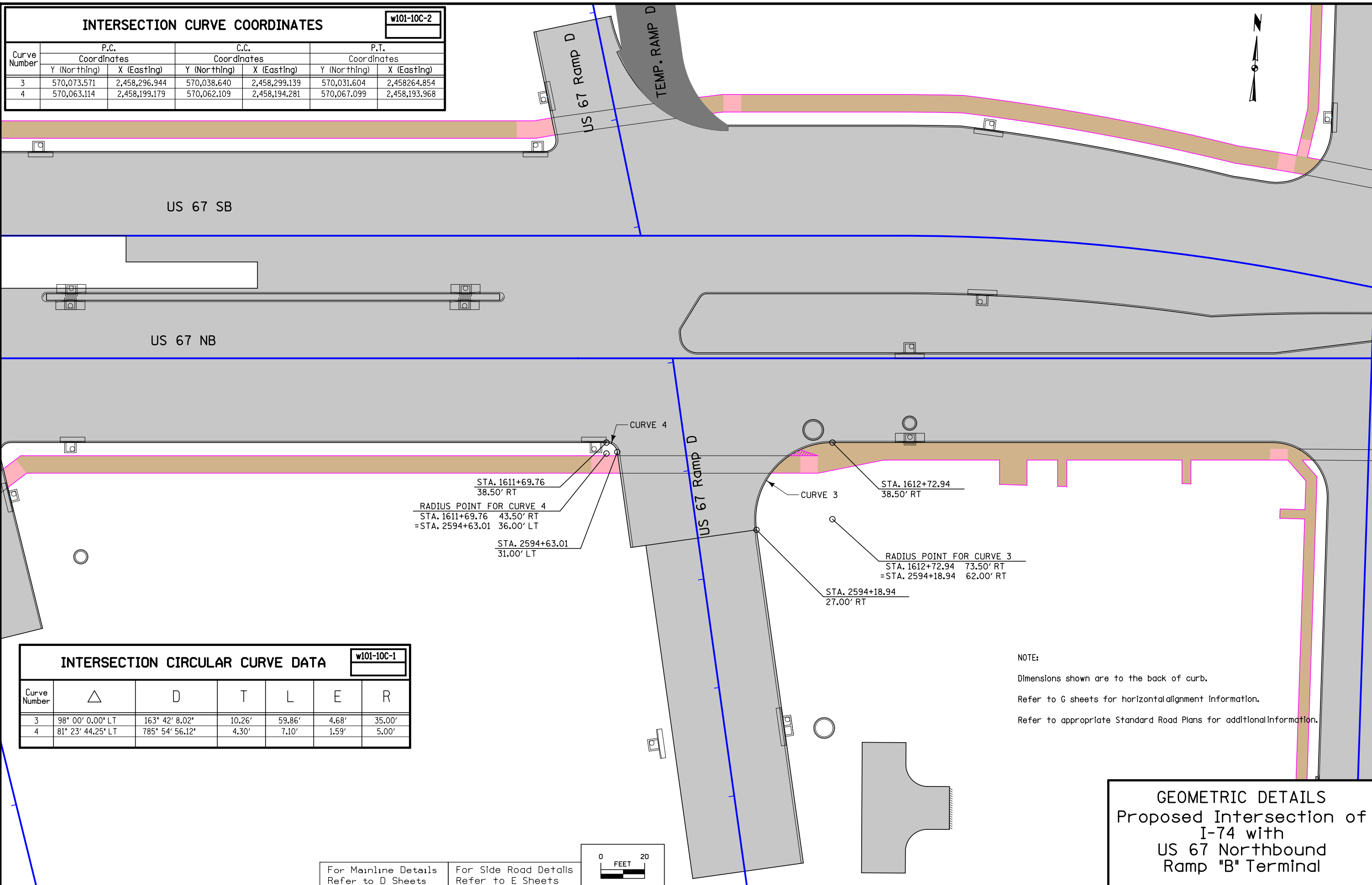
For Side Road Details  
 Refer to E Sheets



**INTERSECTION CURVE COORDINATES**

w101-10C-2

Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
	3	570,073.571	2,458,296.944	570,038.640	2,458,299.139	570,031.604
4	570,063.114	2,458,199.179	570,062.109	2,458,194.281	570,067.099	2,458,193.968



**INTERSECTION CIRCULAR CURVE DATA**

w101-10C-1

Curve Number	Δ	D	T	L	E	R
3	98° 00' 0.00" LT	163° 42' 8.02"	10.26'	59.86'	4.68'	35.00'
4	81° 23' 44.25" LT	785° 54' 56.12"	4.30'	7.10'	1.59'	5.00'

STA. 1611+69.76  
38.50' RT  
RADIUS POINT FOR CURVE 4  
STA. 1611+69.76 43.50' RT  
= STA. 2594+63.01 36.00' LT  
STA. 2594+63.01  
31.00' LT

STA. 1612+72.94  
38.50' RT  
RADIUS POINT FOR CURVE 3  
STA. 1612+72.94 73.50' RT  
= STA. 2594+18.94 62.00' RT  
STA. 2594+18.94  
27.00' RT

**NOTE:**

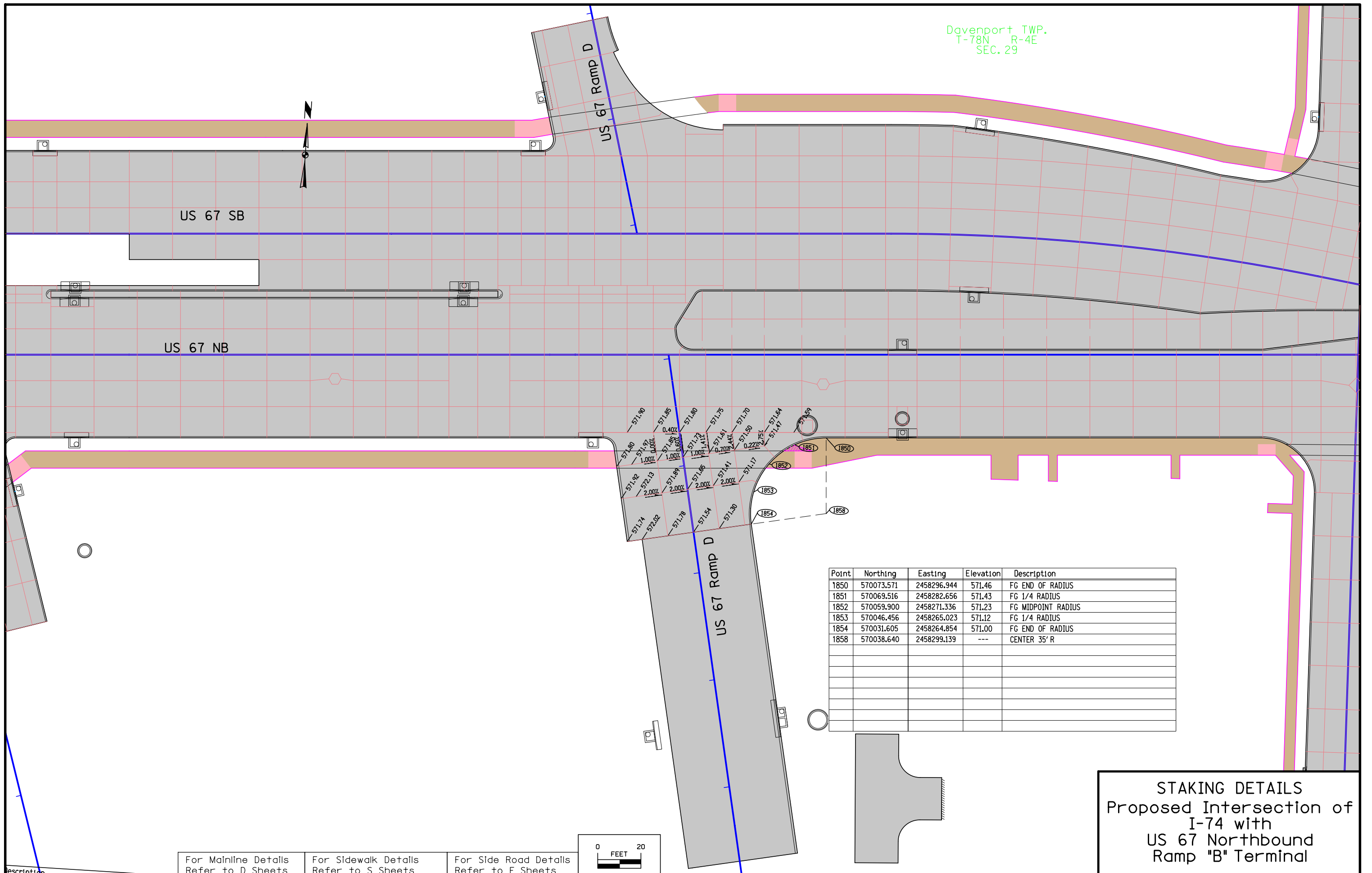
Dimensions shown are to the back of curb.  
Refer to G sheets for horizontal alignment information.  
Refer to appropriate Standard Road Plans for additional information.

**GEOMETRIC DETAILS**  
Proposed Intersection of  
I-74 with  
US 67 Northbound  
Ramp "B" Terminal

For Mainline Details Refer to D Sheets  
For Side Road Details Refer to E Sheets

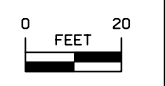


Davenport TWP.  
T-78N R-4E  
SEC. 29

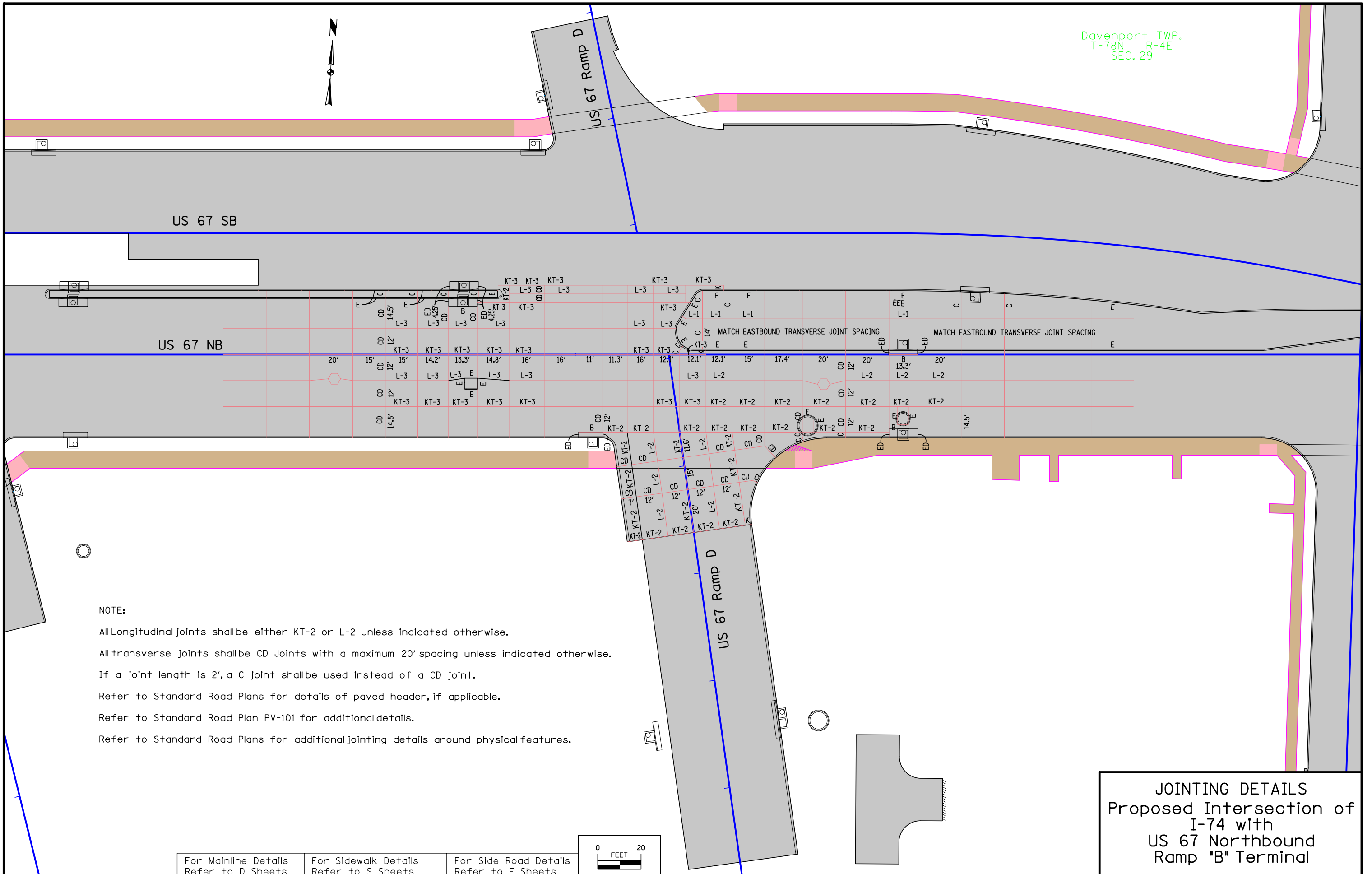


**STAKING DETAILS**  
Proposed Intersection of  
I-74 with  
US 67 Northbound  
Ramp "B" Terminal

For Mainline Details Refer to D Sheets  
For Sidewalk Details Refer to S Sheets  
For Side Road Details Refer to E Sheets



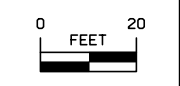
Davenport TWP.  
T-78N R-4E  
SEC. 29



**NOTE:**  
 All Longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.  
 All transverse joints shall be CD joints with a maximum 20' spacing unless indicated otherwise.  
 If a joint length is 2', a C joint shall be used instead of a CD joint.  
 Refer to Standard Road Plans for details of paved header, if applicable.  
 Refer to Standard Road Plan PV-101 for additional details.  
 Refer to Standard Road Plans for additional jointing details around physical features.

**JOINTING DETAILS**  
 Proposed Intersection of  
 I-74 with  
 US 67 Northbound  
 Ramp "B" Terminal

For Mainline Details Refer to D Sheets    For Sidewalk Details Refer to S Sheets    For Side Road Details Refer to E Sheets



**INTERSECTION CIRCULAR CURVE DATA**

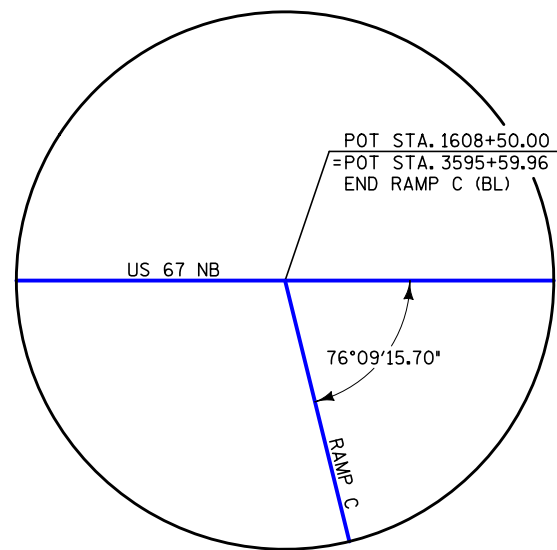
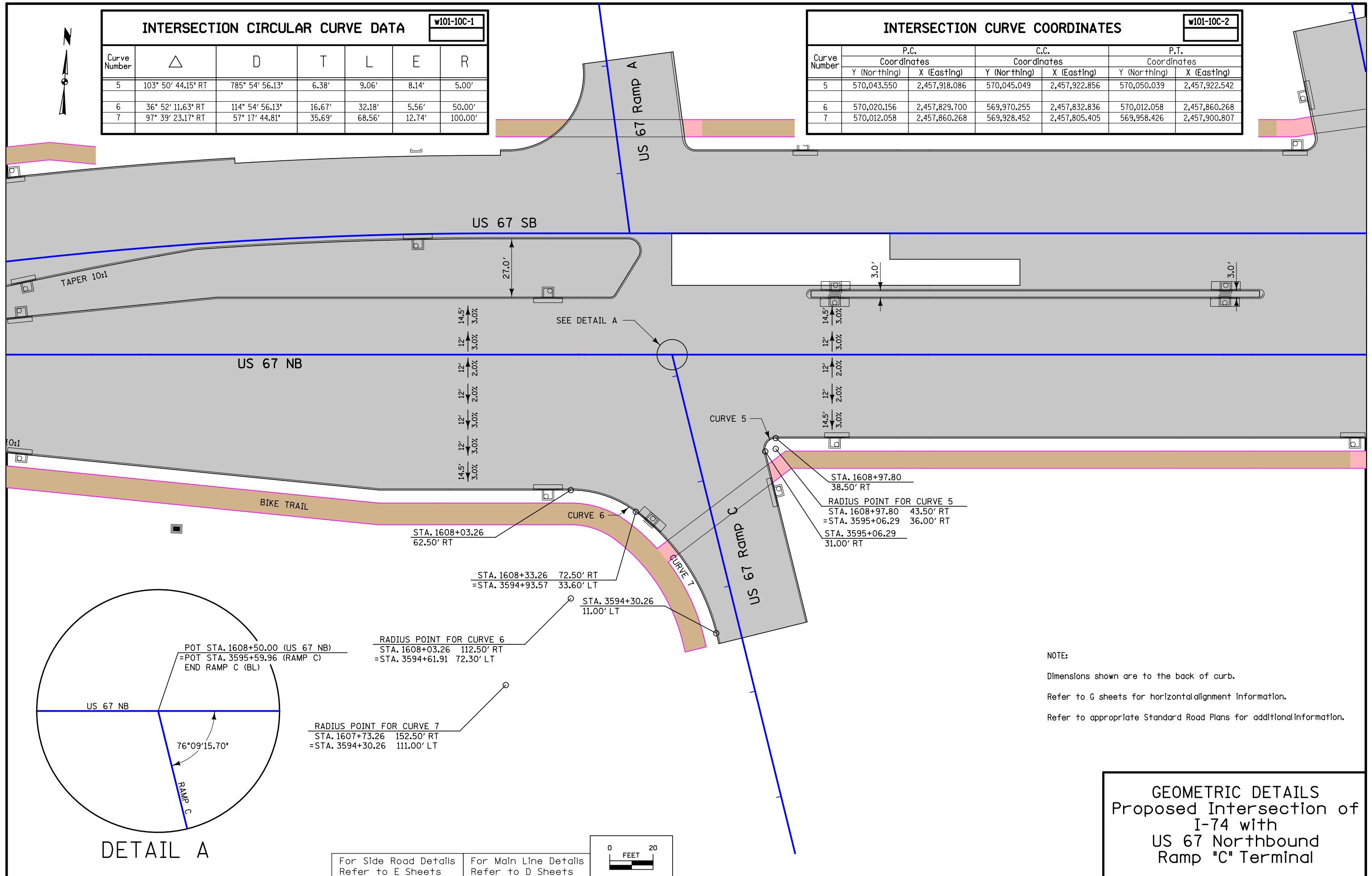
w101-10C-1

Curve Number	Δ	D	T	L	E	R
5	103° 50' 44.15" RT	785° 54' 56.13"	6.38'	9.06'	8.14'	5.00'
6	36° 52' 11.63" RT	114° 54' 56.13"	16.67'	32.18'	5.56'	50.00'
7	97° 39' 23.17" RT	57° 17' 44.81"	35.69'	68.56'	12.74'	100.00'

**INTERSECTION CURVE COORDINATES**

w101-10C-2

Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
5	570,043.550	2,457,918.086	570,045.049	2,457,922.856	570,050.039	2,457,922.542
6	570,020.156	2,457,829.700	569,970.255	2,457,832.836	570,012.058	2,457,860.268
7	570,012.058	2,457,860.268	569,928.452	2,457,805.405	569,958.426	2,457,900.807



RADIUS POINT FOR CURVE 6  
 STA. 1608+03.26 112.50' RT  
 =STA. 3594+61.91 72.30' LT

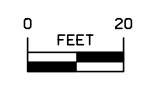
RADIUS POINT FOR CURVE 7  
 STA. 1607+73.26 152.50' RT  
 =STA. 3594+30.26 111.00' LT

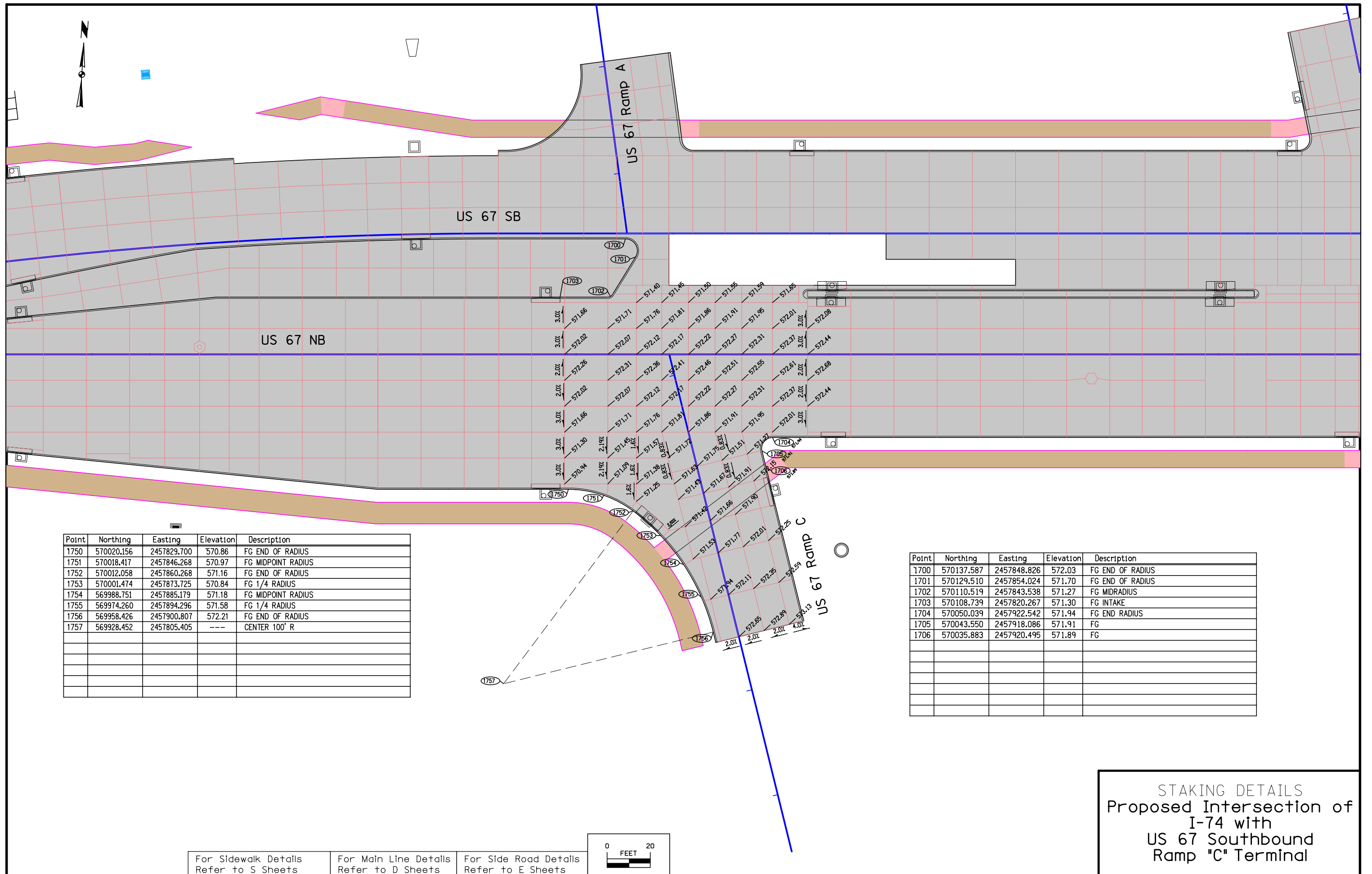
STA. 1608+97.80  
 38.50' RT  
 RADIUS POINT FOR CURVE 5  
 STA. 1608+97.80 43.50' RT  
 =STA. 3595+06.29 36.00' RT  
 STA. 3595+06.29  
 31.00' RT

NOTE:  
 Dimensions shown are to the back of curb.  
 Refer to G sheets for horizontal alignment information.  
 Refer to appropriate Standard Road Plans for additional information.

**GEOMETRIC DETAILS**  
 Proposed Intersection of  
 I-74 with  
 US 67 Northbound  
 Ramp "C" Terminal

For Side Road Details Refer to E Sheets  
 For Main Line Details Refer to D Sheets



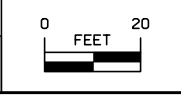


Point	Northing	Easting	Elevation	Description
1750	570020.156	2457829.700	570.86	FG END OF RADIUS
1751	570018.417	2457846.268	570.97	FG MIDPOINT RADIUS
1752	570012.058	2457860.268	571.16	FG END OF RADIUS
1753	570001.474	2457873.725	570.84	FG 1/4 RADIUS
1754	569988.751	2457885.179	571.18	FG MIDPOINT RADIUS
1755	569974.260	2457894.296	571.58	FG 1/4 RADIUS
1756	569958.426	2457900.807	572.21	FG END OF RADIUS
1757	569928.452	2457805.405	---	CENTER 100' R

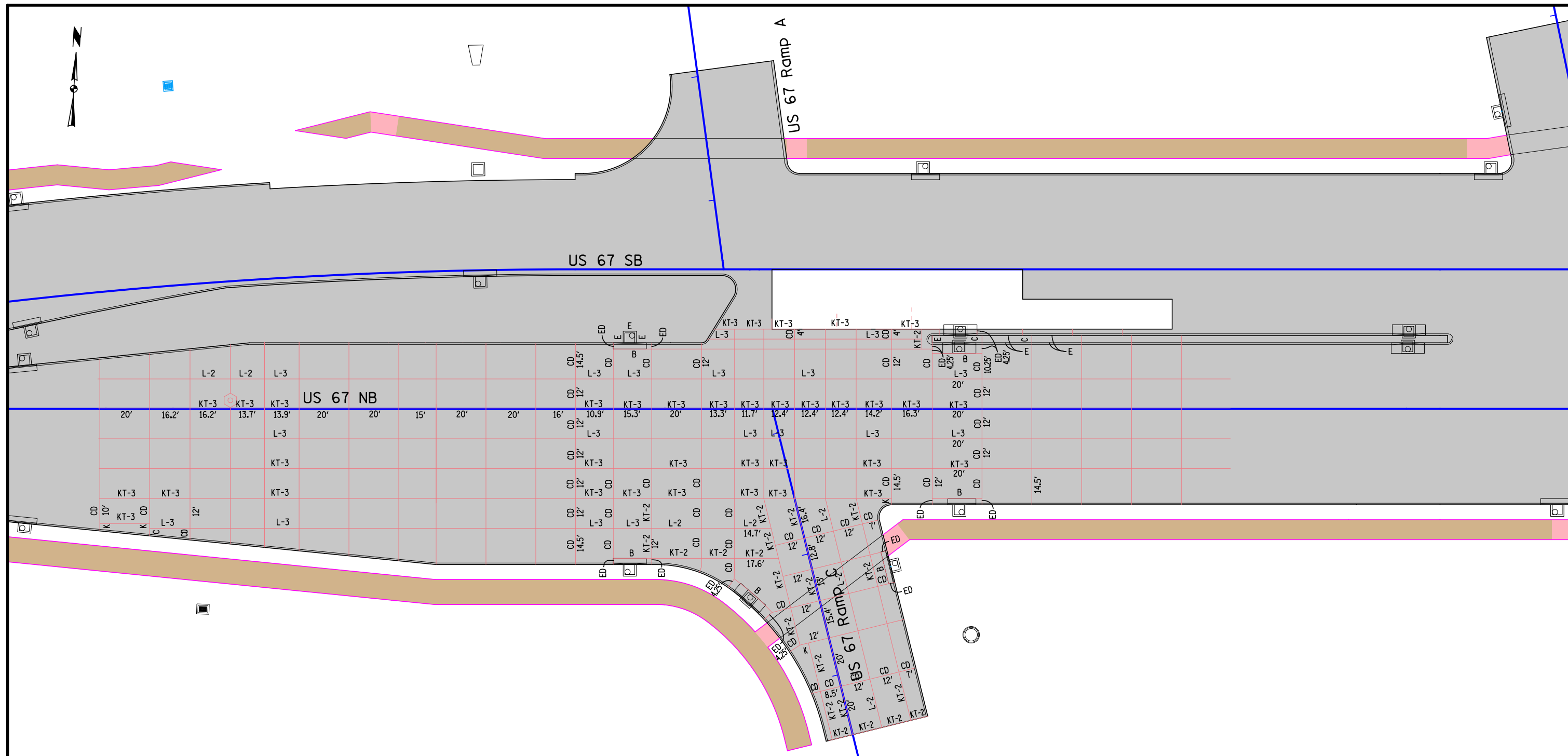
Point	Northing	Easting	Elevation	Description
1700	570137.587	2457848.826	572.03	FG END OF RADIUS
1701	570129.510	2457854.024	571.70	FG END OF RADIUS
1702	570110.519	2457843.538	571.27	FG MIDRADIUS
1703	570108.739	2457820.267	571.30	FG INTAKE
1704	570050.039	2457922.542	571.94	FG END RADIUS
1705	570043.550	2457918.086	571.91	FG
1706	570035.883	2457920.495	571.89	FG

STAKING DETAILS  
Proposed Intersection of  
I-74 with  
US 67 Southbound  
Ramp "C" Terminal

For Sidewalk Details Refer to S Sheets  
For Main Line Details Refer to D Sheets  
For Side Road Details Refer to E Sheets







NOTE:

All Longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.

All transverse joints shall be CD joints with a maximum 20' spacing unless indicated otherwise.

If a joint length is 2', a C joint shall be used instead of a CD joint.

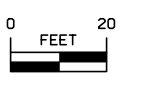
Refer to Standard Road Plans for details of paved header, if applicable.

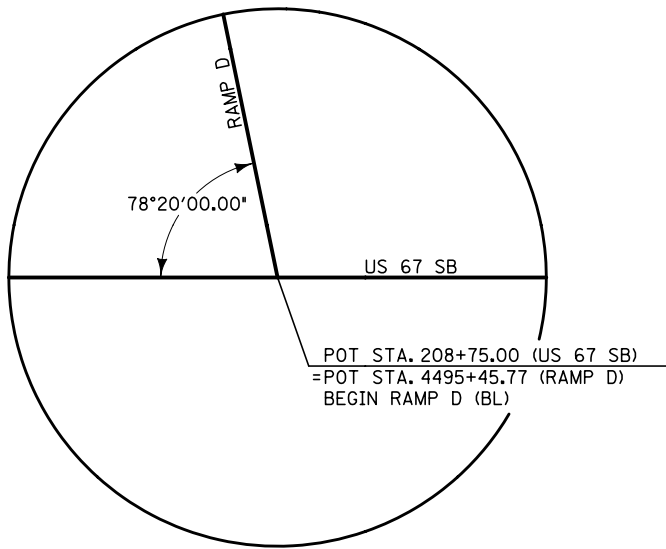
Refer to Standard Road Plan PV-101 for additional details.

Refer to Standard Road Plans for additional jointing details around physical features.

JOINTING DETAILS  
Proposed Intersection of  
I-74 with  
US 67 Southbound  
Ramp "C" Terminal

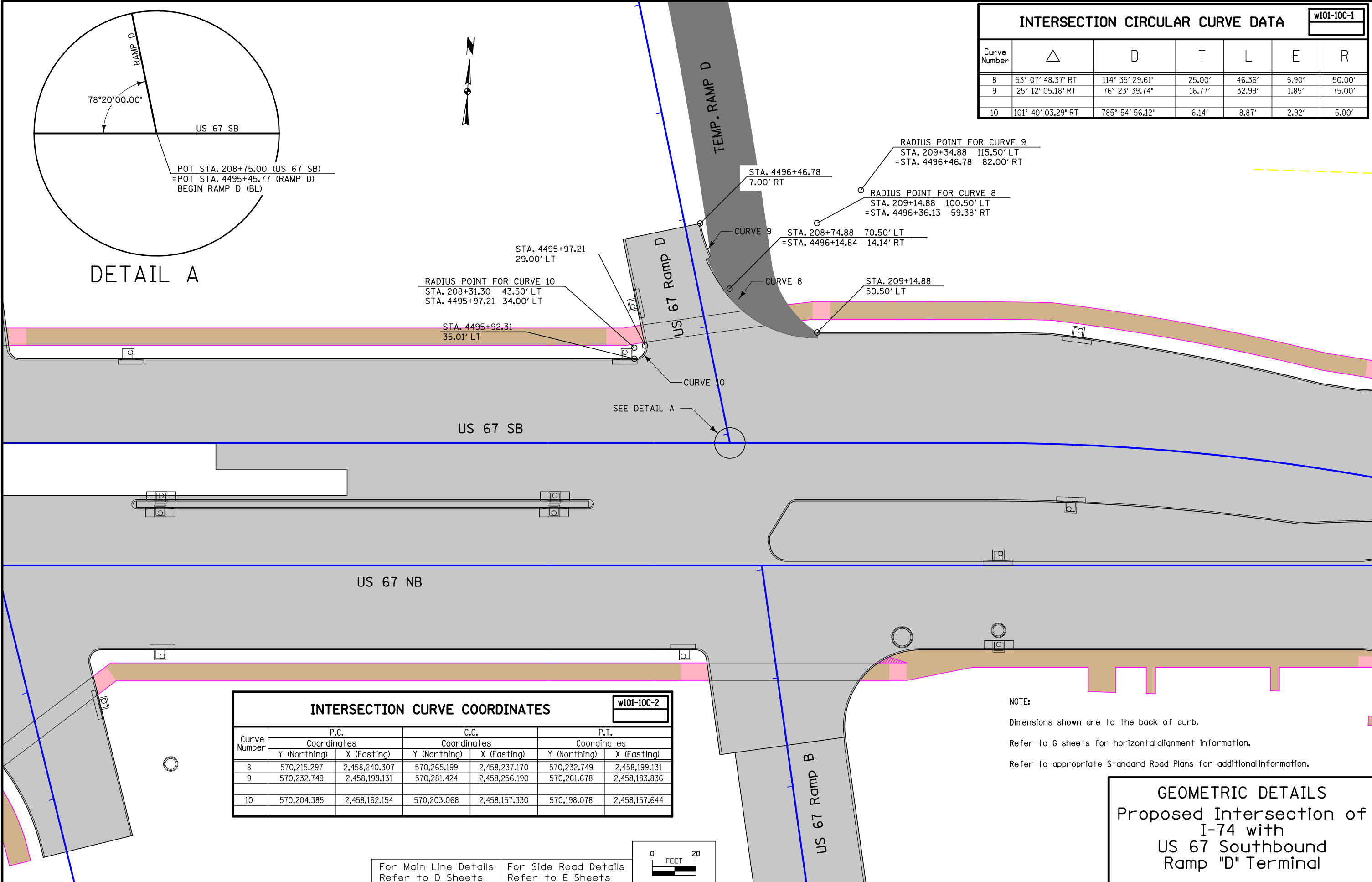
For Main Line Details Refer to D Sheets  
For Side Road Details Refer to E Sheets





DETAIL A

INTERSECTION CIRCULAR CURVE DATA						
Curve Number	$\Delta$	D	T	L	E	R
8	53° 07' 48.37" RT	114° 35' 29.61"	25.00'	46.36'	5.90'	50.00'
9	25° 12' 05.18" RT	76° 23' 39.74"	16.77'	32.99'	1.85'	75.00'
10	101° 40' 03.29" RT	785° 54' 56.12"	6.14'	8.87'	2.92'	5.00'



RADIUS POINT FOR CURVE 10  
 STA. 208+31.30 43.50' LT  
 STA. 4495+97.21 34.00' LT

STA. 4495+92.31  
 35.01' LT

STA. 4495+97.21  
 29.00' LT

STA. 4496+46.78  
 7.00' RT

RADIUS POINT FOR CURVE 8  
 STA. 209+14.88 100.50' LT  
 =STA. 4496+36.13 59.38' RT

RADIUS POINT FOR CURVE 9  
 STA. 209+34.88 115.50' LT  
 =STA. 4496+46.78 82.00' RT

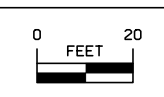
STA. 209+14.88  
 50.50' LT

INTERSECTION CURVE COORDINATES						
Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
8	570,215.297	2,458,240.307	570,265.199	2,458,237.170	570,232.749	2,458,199.131
9	570,232.749	2,458,199.131	570,281.424	2,458,256.190	570,261.678	2,458,183.836
10	570,204.385	2,458,162.154	570,203.068	2,458,157.330	570,198.078	2,458,157.644

NOTE:  
 Dimensions shown are to the back of curb.  
 Refer to G sheets for horizontal alignment information.  
 Refer to appropriate Standard Road Plans for additional information.

GEOMETRIC DETAILS  
 Proposed Intersection of  
 I-74 with  
 US 67 Southbound  
 Ramp "D" Terminal

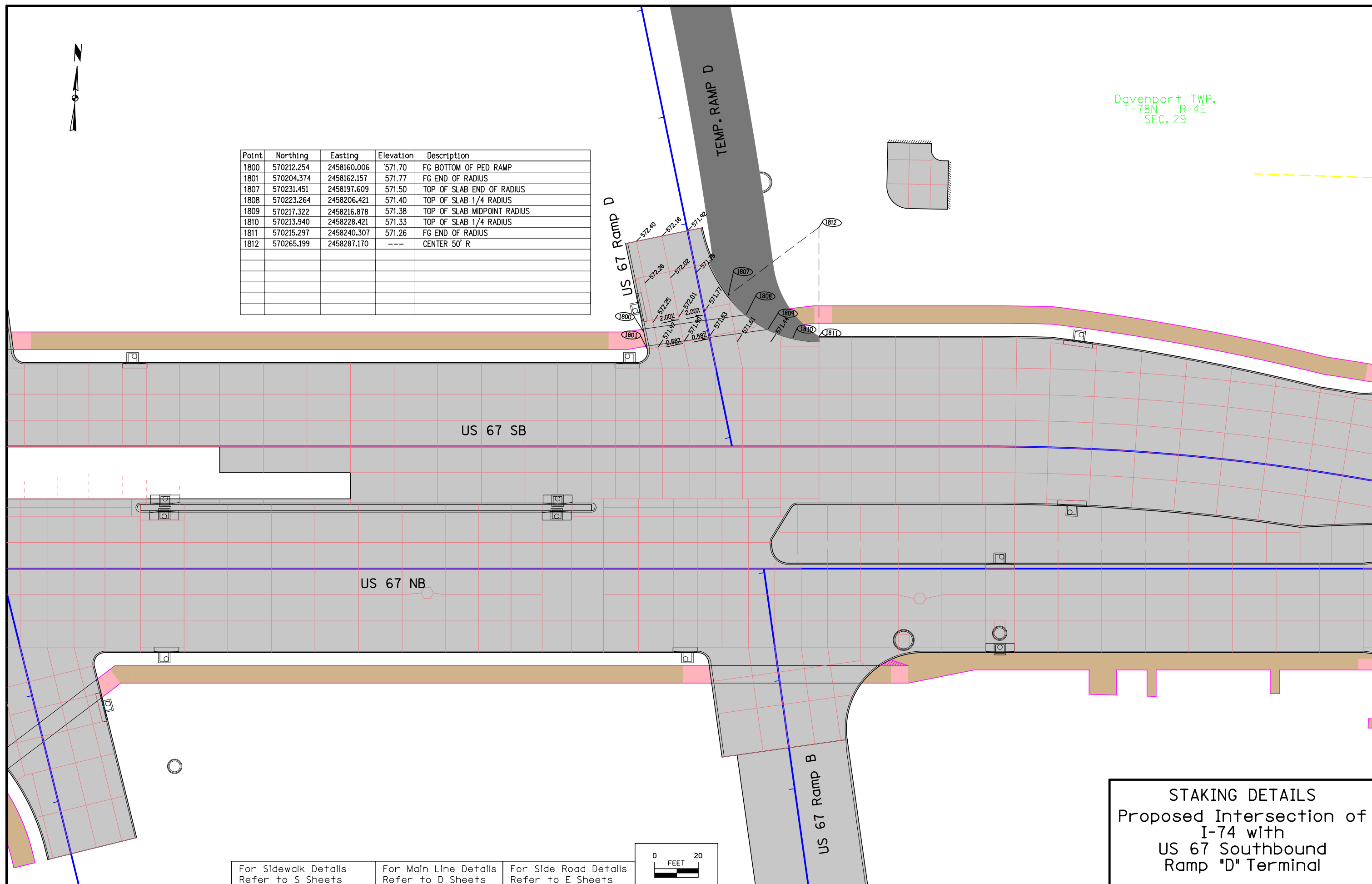
For Main Line Details Refer to D Sheets  
 For Side Road Details Refer to E Sheets



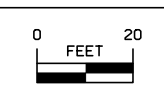


Davenport TWP.  
T-78N R-4E  
SEC. 29

Point	Northing	Easting	Elevation	Description
1800	570212.254	2458160.006	571.70	FG BOTTOM OF PED RAMP
1801	570204.374	2458162.157	571.77	FG END OF RADIUS
1807	570231.451	2458197.609	571.50	TOP OF SLAB END OF RADIUS
1808	570223.264	2458206.421	571.40	TOP OF SLAB 1/4 RADIUS
1809	570217.322	2458216.878	571.38	TOP OF SLAB MIDPOINT RADIUS
1810	570213.940	2458228.421	571.33	TOP OF SLAB 1/4 RADIUS
1811	570215.297	2458240.307	571.26	FG END OF RADIUS
1812	570265.199	2458287.170	---	CENTER 50' R



For Sidewalk Details Refer to S Sheets    For Main Line Details Refer to D Sheets    For Side Road Details Refer to E Sheets

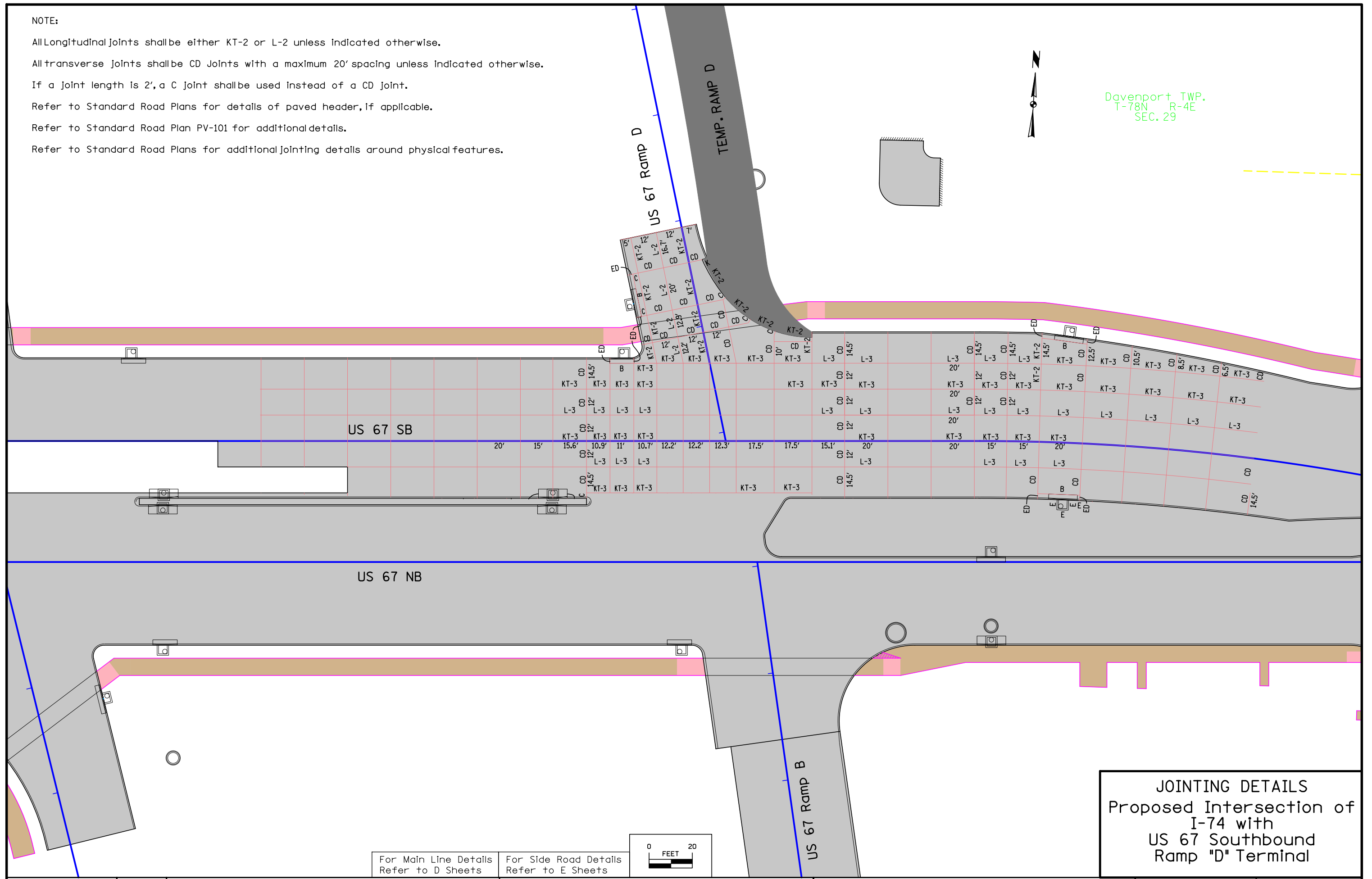


STAKING DETAILS  
Proposed Intersection of  
I-74 with  
US 67 Southbound  
Ramp "D" Terminal

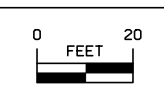
**NOTE:**

- All Longitudinal Joints shall be either KT-2 or L-2 unless indicated otherwise.
- All transverse joints shall be CD Joints with a maximum 20' spacing unless indicated otherwise.
- If a joint length is 2', a C joint shall be used instead of a CD joint.
- Refer to Standard Road Plans for details of paved header, if applicable.
- Refer to Standard Road Plan PV-101 for additional details.
- Refer to Standard Road Plans for additional jointing details around physical features.

Davenport TWP.  
T-78N R-4E  
SEC. 29

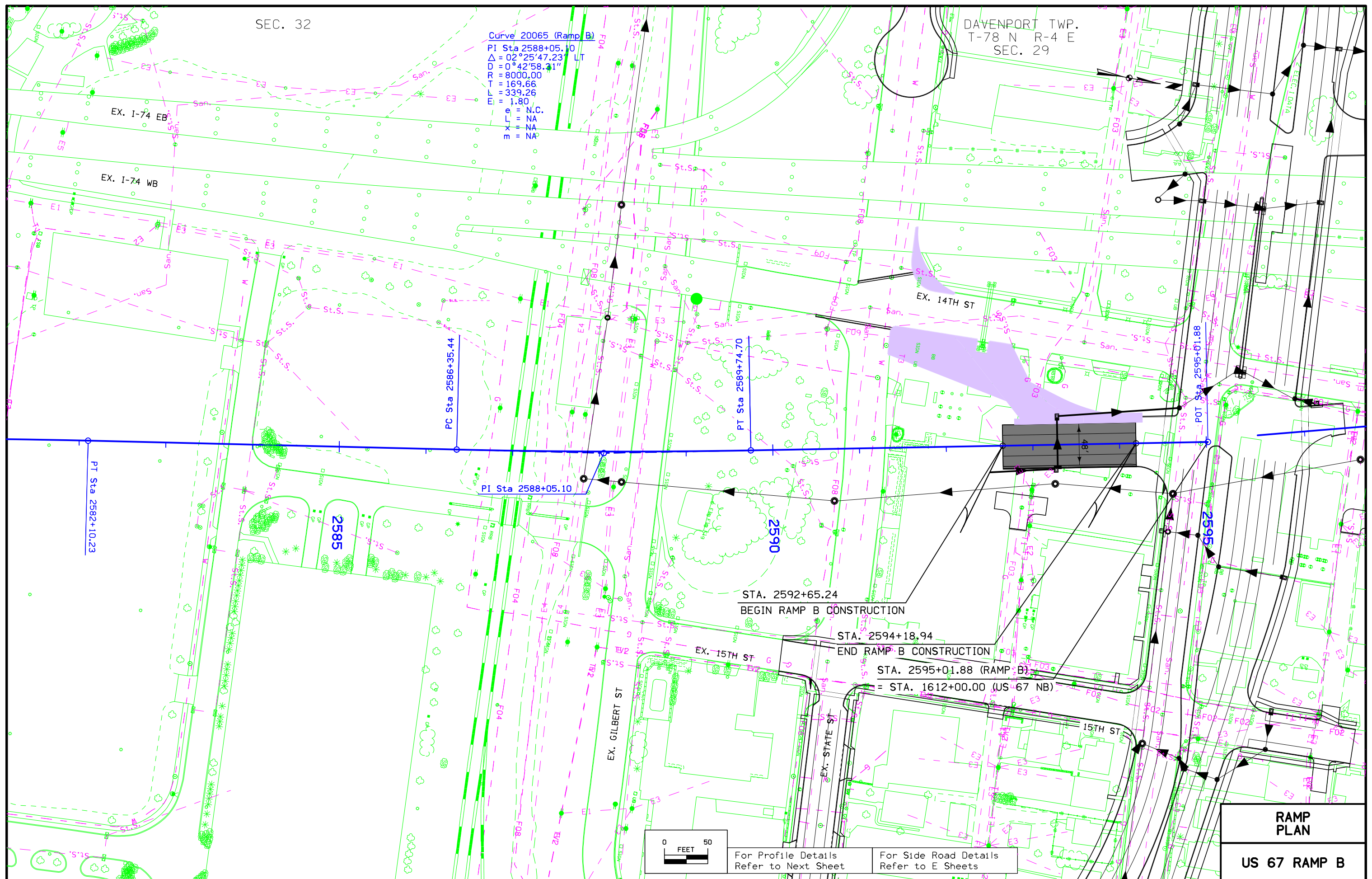


For Main Line Details Refer to D Sheets  
For Side Road Details Refer to E Sheets



**JOINTING DETAILS**  
Proposed Intersection of  
I-74 with  
US 67 Southbound  
Ramp "D" Terminal

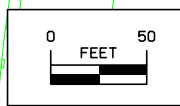
Curve 20065 (Ramp B)  
 PI Sta 2588+05.10  
 $\Delta = 02^\circ 25' 47.23''$  LT  
 $D = 0^\circ 42' 58.31''$   
 $R = 8000.00$   
 $T = 169.66$   
 $L = 339.26$   
 $E = 1.80$   
 $e = N.C.$   
 $L = NA$   
 $X = NA$   
 $M = NA$



STA. 2592+65.24  
 BEGIN RAMP B CONSTRUCTION

STA. 2594+18.94  
 END RAMP B CONSTRUCTION

STA. 2595+01.88 (RAMP B)  
 = STA. 1612+00.00 (US 67 NB)



For Profile Details  
 Refer to Next Sheet

For Side Road Details  
 Refer to E Sheets

**RAMP PLAN**

**US 67 RAMP B**

Ramp A Radius Return

CL 10 Cut = 12 CY  
 12 CY

Fill + 30% = 0 CY  
 Waste = 12 CY  
 12 CY

Ramp C Radius Return

CL 10 Cut = 630 CY  
 630 CY

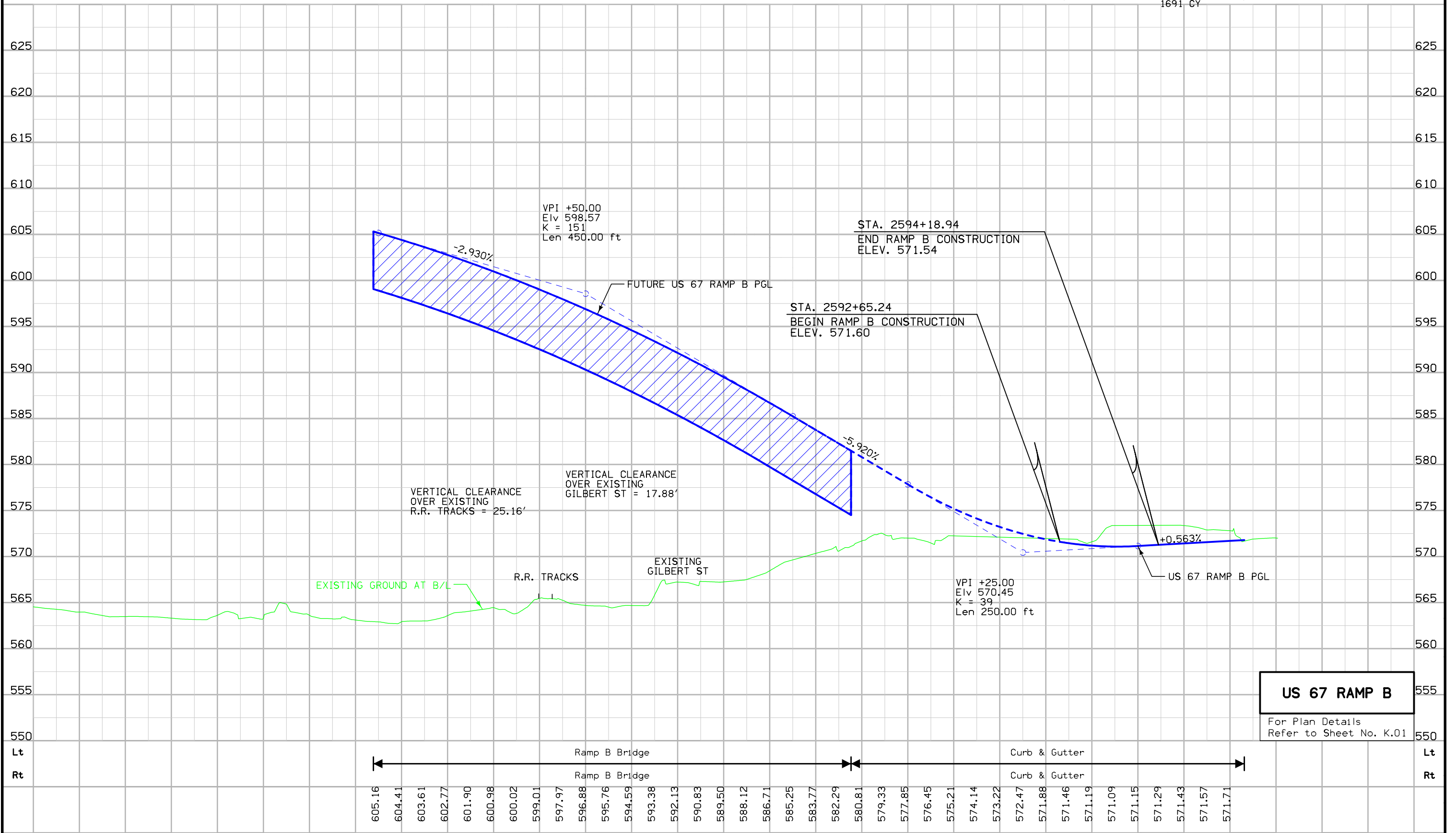
Fill + 30% = 12 CY  
 Waste = 618 CY  
 630 CY

Ramp D Radius Return

CL 10 Cut = 486 CY  
 486 CY

Fill + 30% = 0 CY  
 Waste = 486 CY  
 486 CY

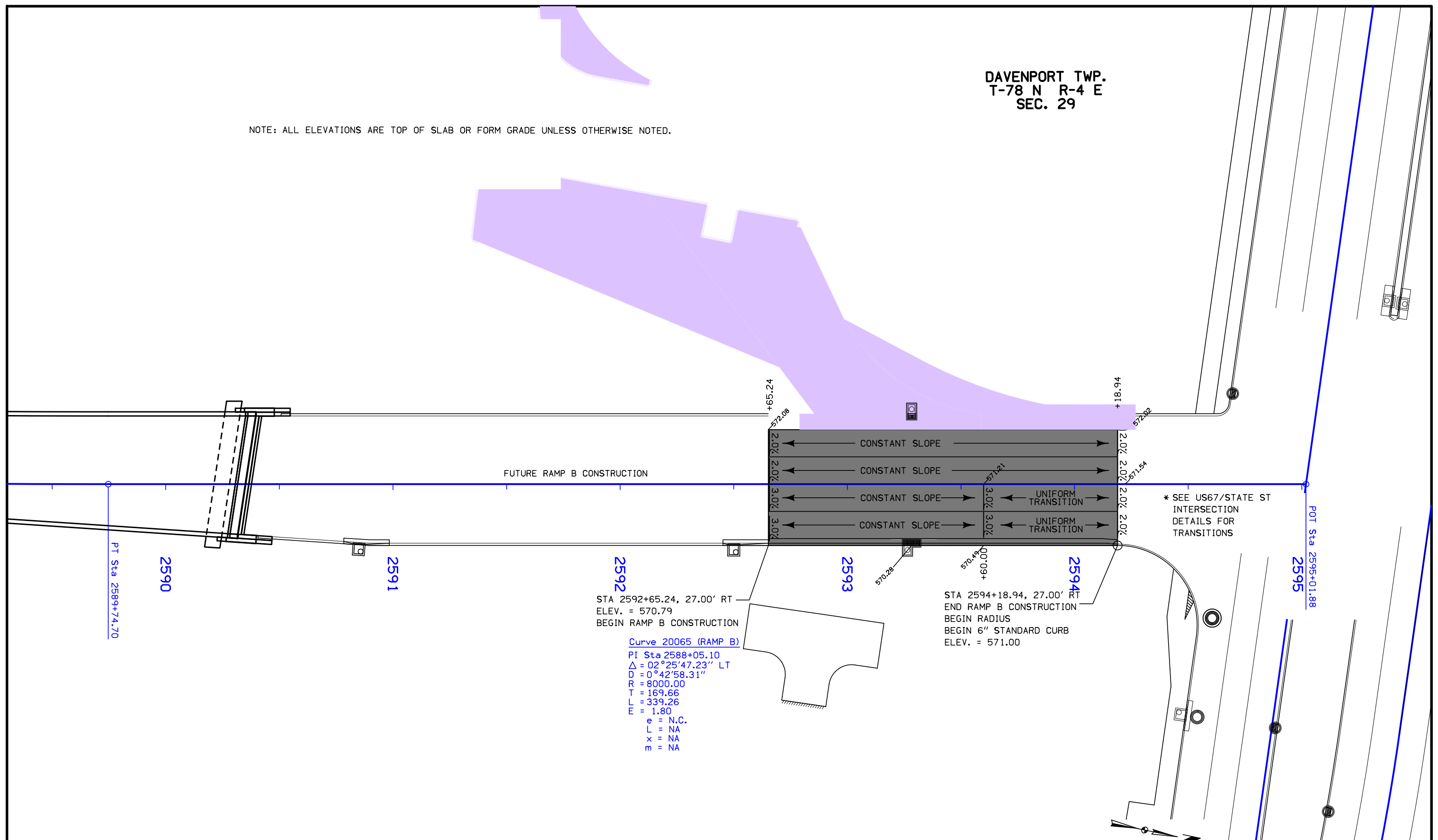
+70.00  
 CL 10 Cut = 1691 CY  
 1691 CY  
 Fill + 30% = 0 CY  
 Waste = 1691 CY  
 1691 CY  
 +65.00



**US 67 RAMP B**  
 For Plan Details  
 Refer to Sheet No. K.01

DAVENPORT TWP.  
T-78 N R-4 E  
SEC. 29

NOTE: ALL ELEVATIONS ARE TOP OF SLAB OR FORM GRADE UNLESS OTHERWISE NOTED.

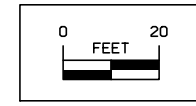


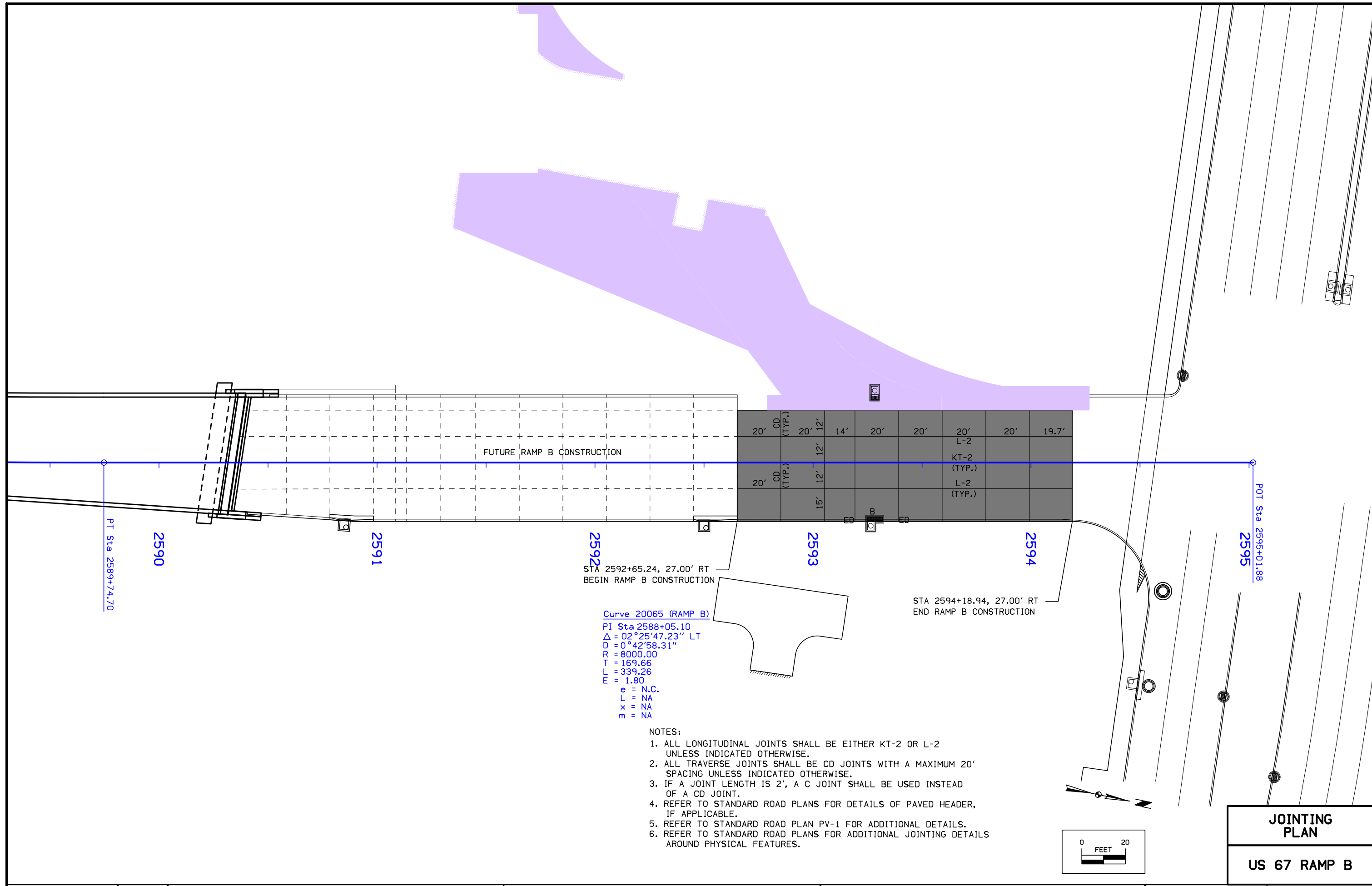
2592  
STA 2592+65.24, 27.00' RT  
ELEV. = 570.79  
BEGIN RAMP B CONSTRUCTION  
Curve 20065 (RAMP B)  
PI Sta 2588+05.10  
 $\Delta = 02^\circ 25' 47.23''$  LT  
D =  $0^\circ 42' 58.31''$   
R = 8000.00  
T = 169.66  
EL = 339.26  
e = 1.80  
L = N.C.  
x = NA  
m = NA

2594  
STA 2594+18.94, 27.00' RT  
END RAMP B CONSTRUCTION  
BEGIN RADIUS  
BEGIN 6" STANDARD CURB  
ELEV. = 571.00

\* SEE US67/STATE ST  
INTERSECTION  
DETAILS FOR  
TRANSITIONS

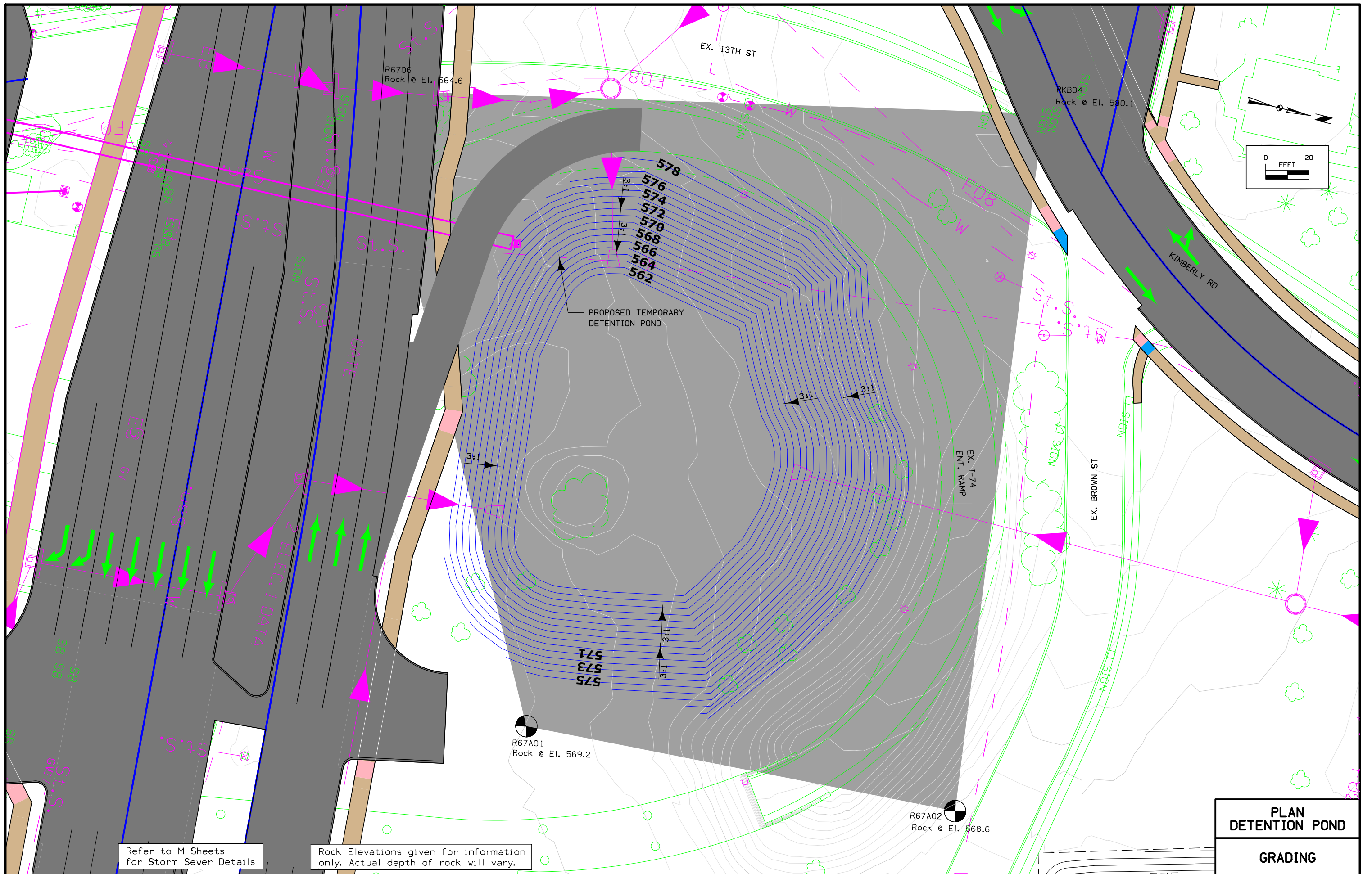
**GEOMETRICS &  
STAKING PLAN**  
**US 67 RAMP B**





**JOINTING PLAN**  
**US 67 RAMP B**





Refer to M Sheets  
for Storm Sewer Details

Rock Elevations given for information  
only. Actual depth of rock will vary.

**PLAN  
DETENTION POND**

**GRADING**

**INTERSECTION CURVE COORDINATES**

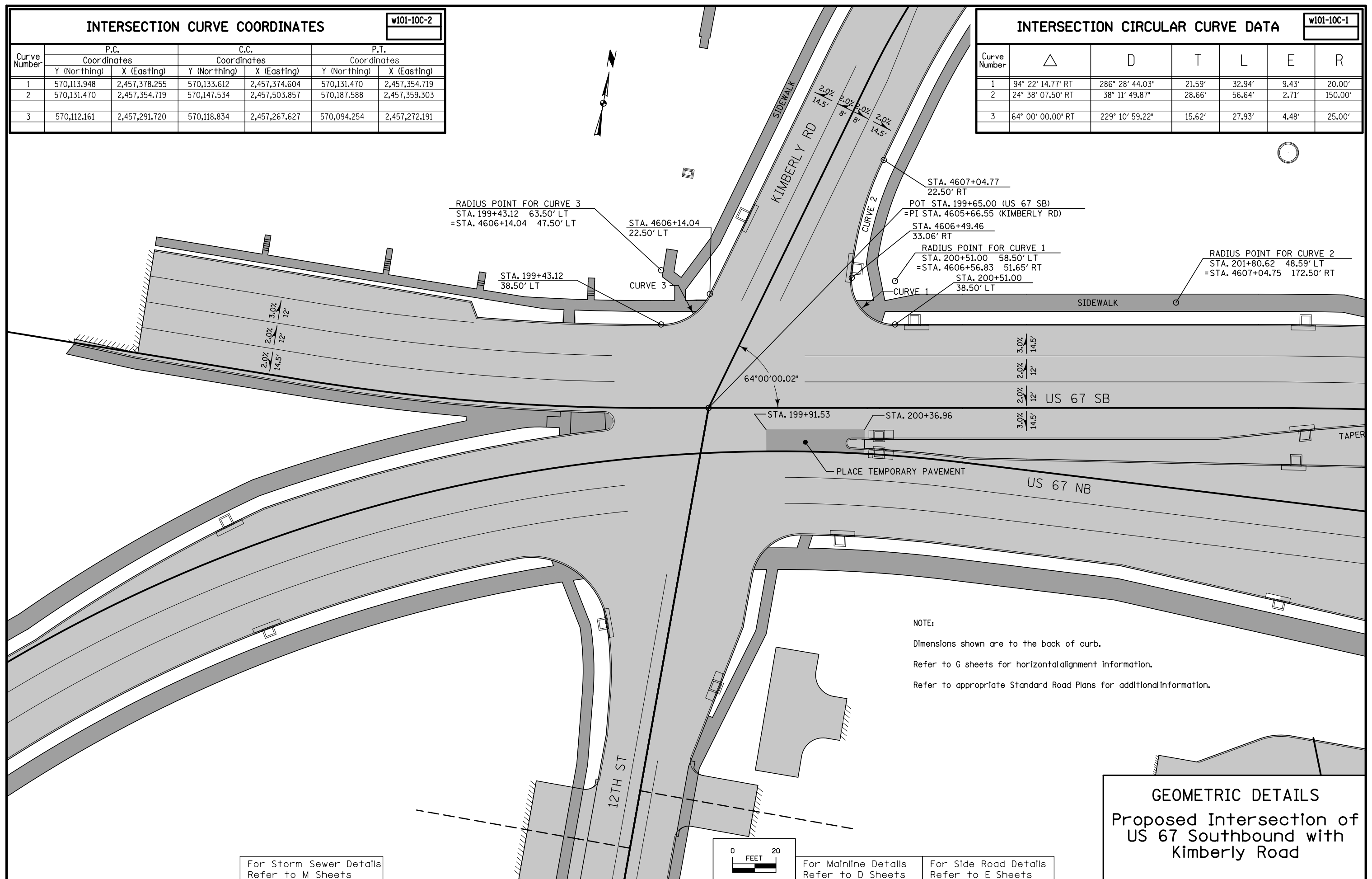
w101-10C-2

Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
1	570,113.948	2,457,378.255	570,133.612	2,457,374.604	570,131.470	2,457,354.719
2	570,131.470	2,457,354.719	570,147.534	2,457,503.857	570,187.588	2,457,359.303
3	570,112.161	2,457,291.720	570,118.834	2,457,267.627	570,094.254	2,457,272.191

**INTERSECTION CIRCULAR CURVE DATA**

w101-10C-1

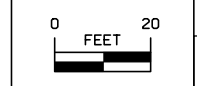
Curve Number	Δ	D	T	L	E	R
1	94° 22' 14.77" RT	286° 28' 44.03'	21.59'	32.94'	9.43'	20.00'
2	24° 38' 07.50" RT	38° 11' 49.87'	28.66'	56.64'	2.71'	150.00'
3	64° 00' 00.00" RT	229° 10' 59.22"	15.62'	27.93'	4.48'	25.00'



**NOTE:**  
 Dimensions shown are to the back of curb.  
 Refer to G sheets for horizontal alignment information.  
 Refer to appropriate Standard Road Plans for additional information.

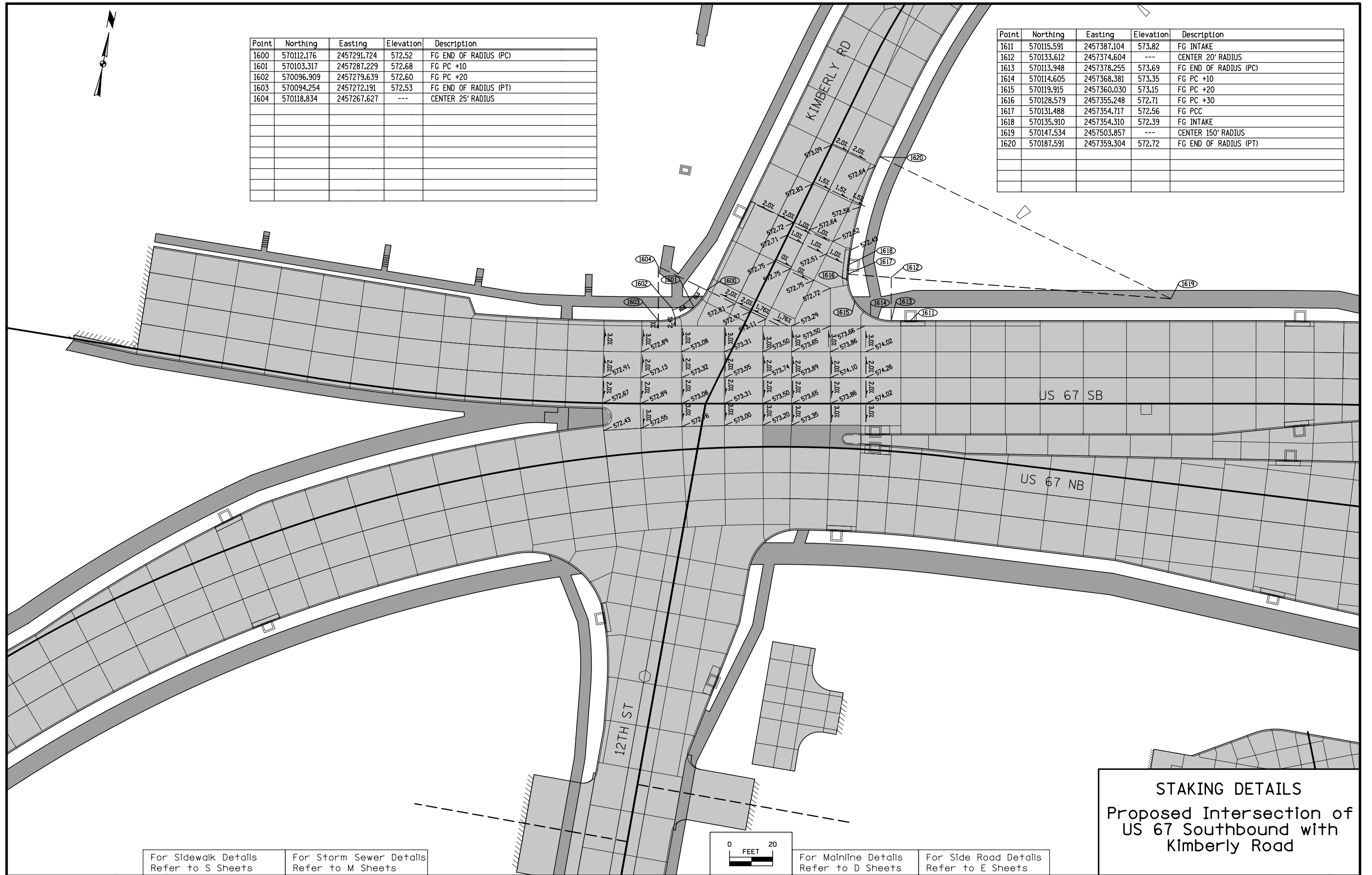
**GEOMETRIC DETAILS**  
 Proposed Intersection of  
 US 67 Southbound with  
 Kimberly Road

For Storm Sewer Details  
 Refer to M Sheets



For Mainline Details  
 Refer to D Sheets

For Side Road Details  
 Refer to E Sheets



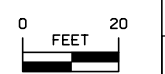
Point	Northing	Easting	Elevation	Description
1600	570112.176	2457291.724	572.52	FG END OF RADIUS (PC)
1601	570103.317	2457287.229	572.68	FG PC +10
1602	570096.909	2457279.639	572.60	FG PC +20
1603	570094.254	2457272.191	572.53	FG END OF RADIUS (PT)
1604	570118.834	2457267.627	---	CENTER 25' RADIUS

Point	Northing	Easting	Elevation	Description
1611	570115.591	2457387.104	573.82	FG INTAKE
1612	570133.612	2457374.604	---	CENTER 20' RADIUS
1613	570113.948	2457378.255	573.69	FG END OF RADIUS (PC)
1614	570114.605	2457368.381	573.35	FG PC +10
1615	570119.915	2457360.030	573.15	FG PC +20
1616	570128.579	2457355.248	572.71	FG PC +30
1617	570131.488	2457354.717	572.56	FG PCC
1618	570135.910	2457354.310	572.39	FG INTAKE
1619	570147.534	2457503.857	---	CENTER 150' RADIUS
1620	570187.591	2457359.304	572.72	FG END OF RADIUS (PT)

**STAKING DETAILS**  
Proposed Intersection of  
US 67 Southbound with  
Kimberly Road

For Sidewalk Details  
Refer to S Sheets

For Storm Sewer Details  
Refer to M Sheets



For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets

NOTE:

All Longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.

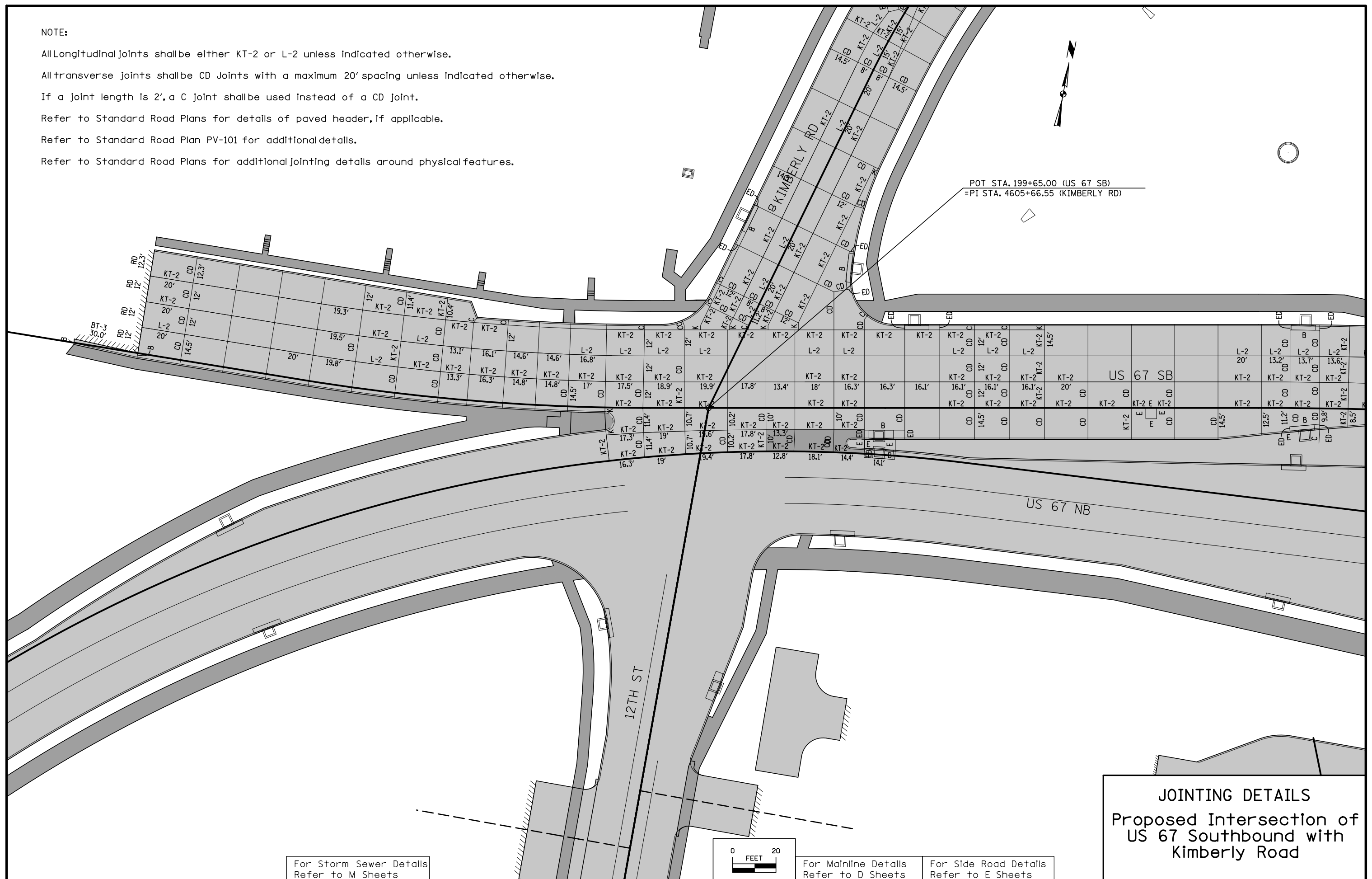
All transverse joints shall be CD Joints with a maximum 20' spacing unless indicated otherwise.

If a joint length is 2', a C Joint shall be used instead of a CD Joint.

Refer to Standard Road Plans for details of paved header, if applicable.

Refer to Standard Road Plan PV-101 for additional details.

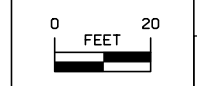
Refer to Standard Road Plans for additional jointing details around physical features.



POT STA. 199+65.00 (US 67 SB)  
=PI STA. 4605+66.55 (KIMBERLY RD)

**JOINTING DETAILS**  
**Proposed Intersection of**  
**US 67 Southbound with**  
**Kimberly Road**

For Storm Sewer Details  
Refer to M Sheets



For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets

**INTERSECTION CIRCULAR CURVE DATA**

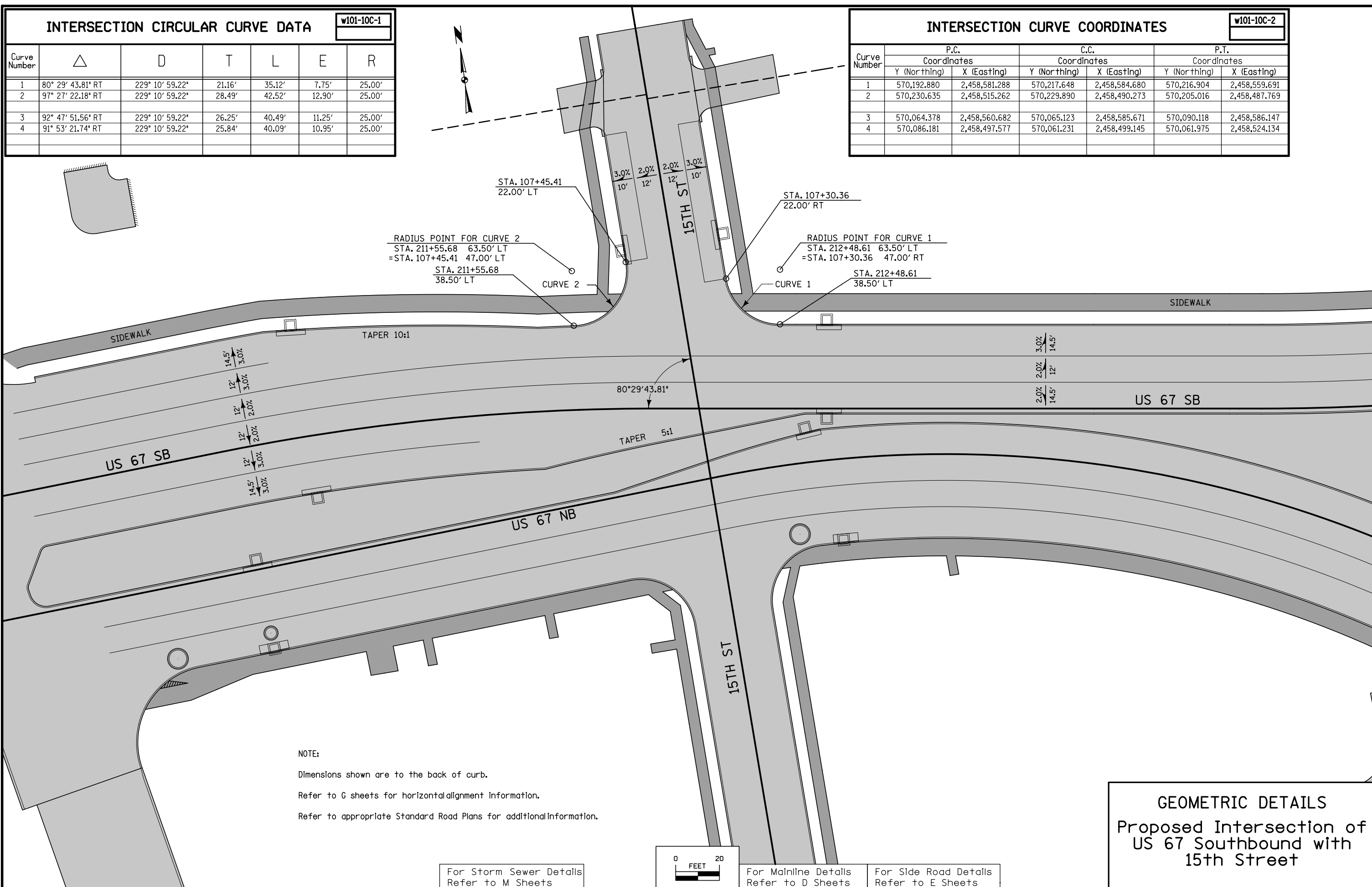
w101-10C-1

Curve Number	△	D	T	L	E	R
1	80° 29' 43.81" RT	229° 10' 59.22"	21.16'	35.12'	7.75'	25.00'
2	97° 27' 22.18" RT	229° 10' 59.22"	28.49'	42.52'	12.90'	25.00'
3	92° 47' 51.56" RT	229° 10' 59.22"	26.25'	40.49'	11.25'	25.00'
4	91° 53' 21.74" RT	229° 10' 59.22"	25.84'	40.09'	10.95'	25.00'

**INTERSECTION CURVE COORDINATES**

w101-10C-2

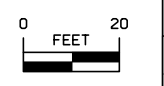
Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
1	570,192.880	2,458,581.288	570,217.648	2,458,584.680	570,216.904	2,458,559.691
2	570,230.635	2,458,515.262	570,229.890	2,458,490.273	570,205.016	2,458,487.769
3	570,064.378	2,458,560.682	570,065.123	2,458,585.671	570,090.118	2,458,586.147
4	570,086.181	2,458,497.577	570,061.231	2,458,499.145	570,061.975	2,458,524.134



NOTE:  
 Dimensions shown are to the back of curb.  
 Refer to C sheets for horizontal alignment information.  
 Refer to appropriate Standard Road Plans for additional information.

**GEOMETRIC DETAILS**  
 Proposed Intersection of  
 US 67 Southbound with  
 15th Street

For Storm Sewer Details  
 Refer to M Sheets

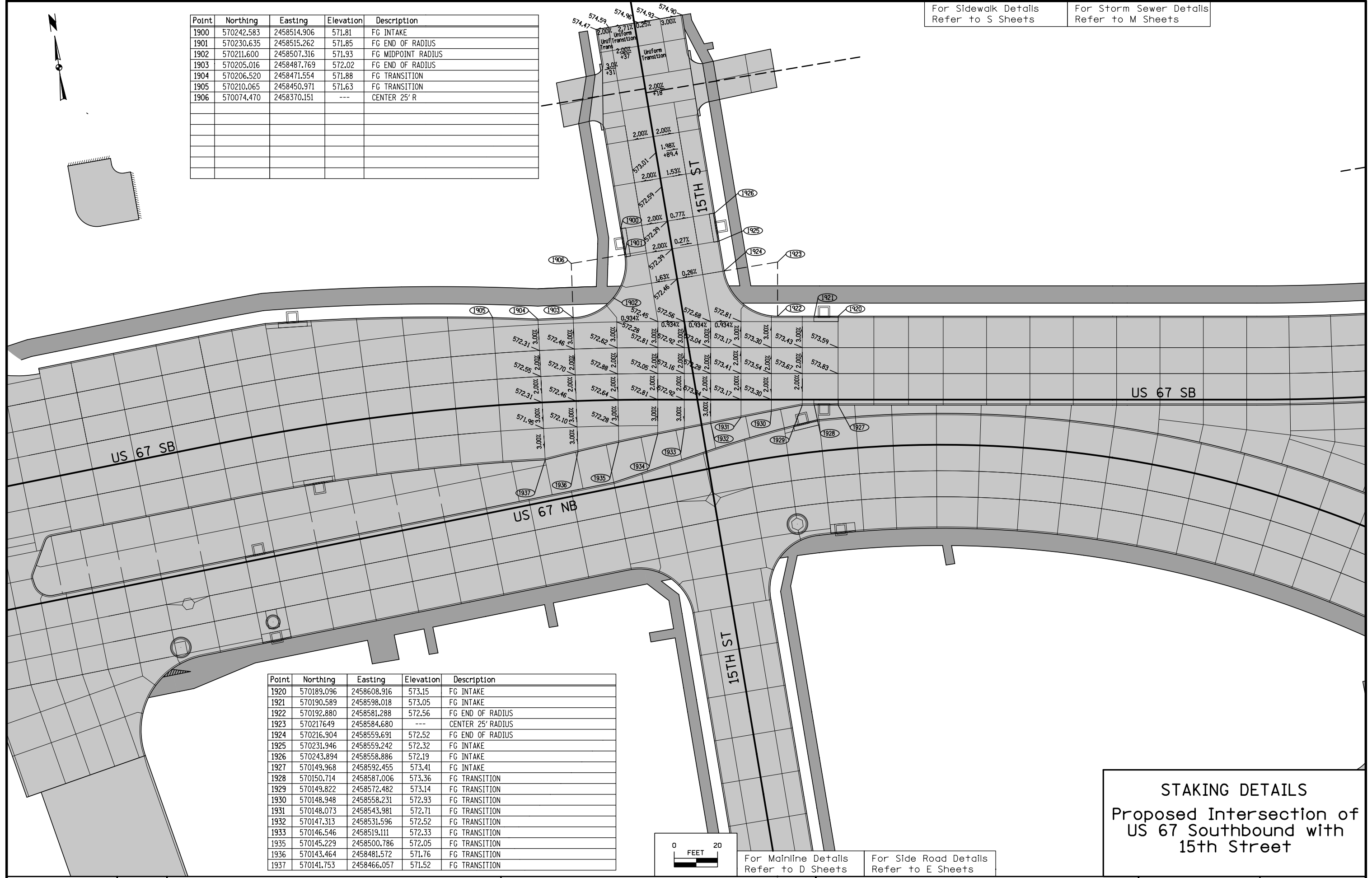


For Mainline Details  
 Refer to D Sheets

For Side Road Details  
 Refer to E Sheets

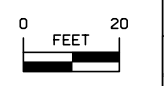
For Sidewalk Details Refer to S Sheets  
For Storm Sewer Details Refer to M Sheets

Point	Northing	Easting	Elevation	Description
1900	570242.583	2458514.906	571.81	FG INTAKE
1901	570230.635	2458515.262	571.85	FG END OF RADIUS
1902	570211.600	2458507.316	571.93	FG MIDPOINT RADIUS
1903	570205.016	2458487.769	572.02	FG END OF RADIUS
1904	570206.520	2458471.554	571.88	FG TRANSITION
1905	570210.065	2458450.971	571.63	FG TRANSITION
1906	570074.470	2458370.151	---	CENTER 25' R



Point	Northing	Easting	Elevation	Description
1920	570189.096	2458608.916	573.15	FG INTAKE
1921	570190.589	2458598.018	573.05	FG INTAKE
1922	570192.880	2458581.288	572.56	FG END OF RADIUS
1923	570217.649	2458584.680	---	CENTER 25' RADIUS
1924	570216.904	2458559.691	572.52	FG END OF RADIUS
1925	570231.946	2458559.242	572.32	FG INTAKE
1926	570243.894	2458558.886	572.19	FG INTAKE
1927	570149.968	2458592.455	573.41	FG INTAKE
1928	570150.714	2458587.006	573.36	FG TRANSITION
1929	570149.822	2458572.482	573.14	FG TRANSITION
1930	570148.948	2458558.231	572.93	FG TRANSITION
1931	570148.073	2458543.981	572.71	FG TRANSITION
1932	570147.313	2458531.596	572.52	FG TRANSITION
1933	570146.546	2458519.111	572.33	FG TRANSITION
1935	570145.229	2458500.786	572.05	FG TRANSITION
1936	570143.464	2458481.572	571.76	FG TRANSITION
1937	570141.753	2458466.057	571.52	FG TRANSITION

**STAKING DETAILS**  
Proposed Intersection of  
US 67 Southbound with  
15th Street



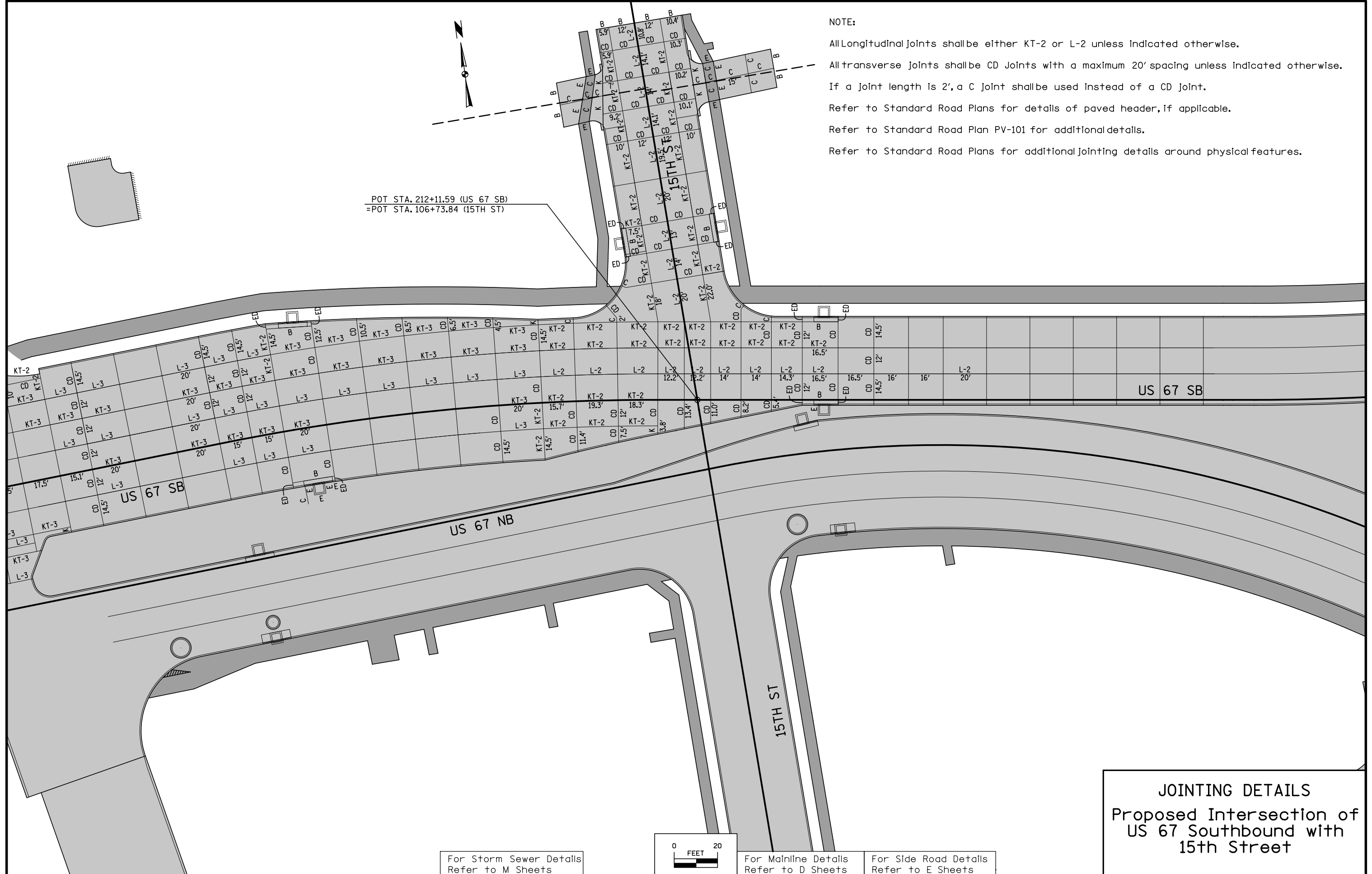
For Mainline Details Refer to D Sheets  
For Side Road Details Refer to E Sheets



POT STA. 212+11.59 (US 67 SB)  
 =POT STA. 106+73.84 (15TH ST)

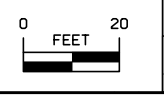
NOTE:

- All Longitudinal Joints shall be either KT-2 or L-2 unless indicated otherwise.
- All transverse joints shall be CD Joints with a maximum 20' spacing unless indicated otherwise.
- If a joint length is 2', a C joint shall be used instead of a CD joint.
- Refer to Standard Road Plans for details of paved header, if applicable.
- Refer to Standard Road Plan PV-101 for additional details.
- Refer to Standard Road Plans for additional jointing details around physical features.



**JOINTING DETAILS**  
 Proposed Intersection of  
 US 67 Southbound with  
 15th Street

For Storm Sewer Details  
 Refer to M Sheets



For Mainline Details  
 Refer to D Sheets

For Side Road Details  
 Refer to E Sheets

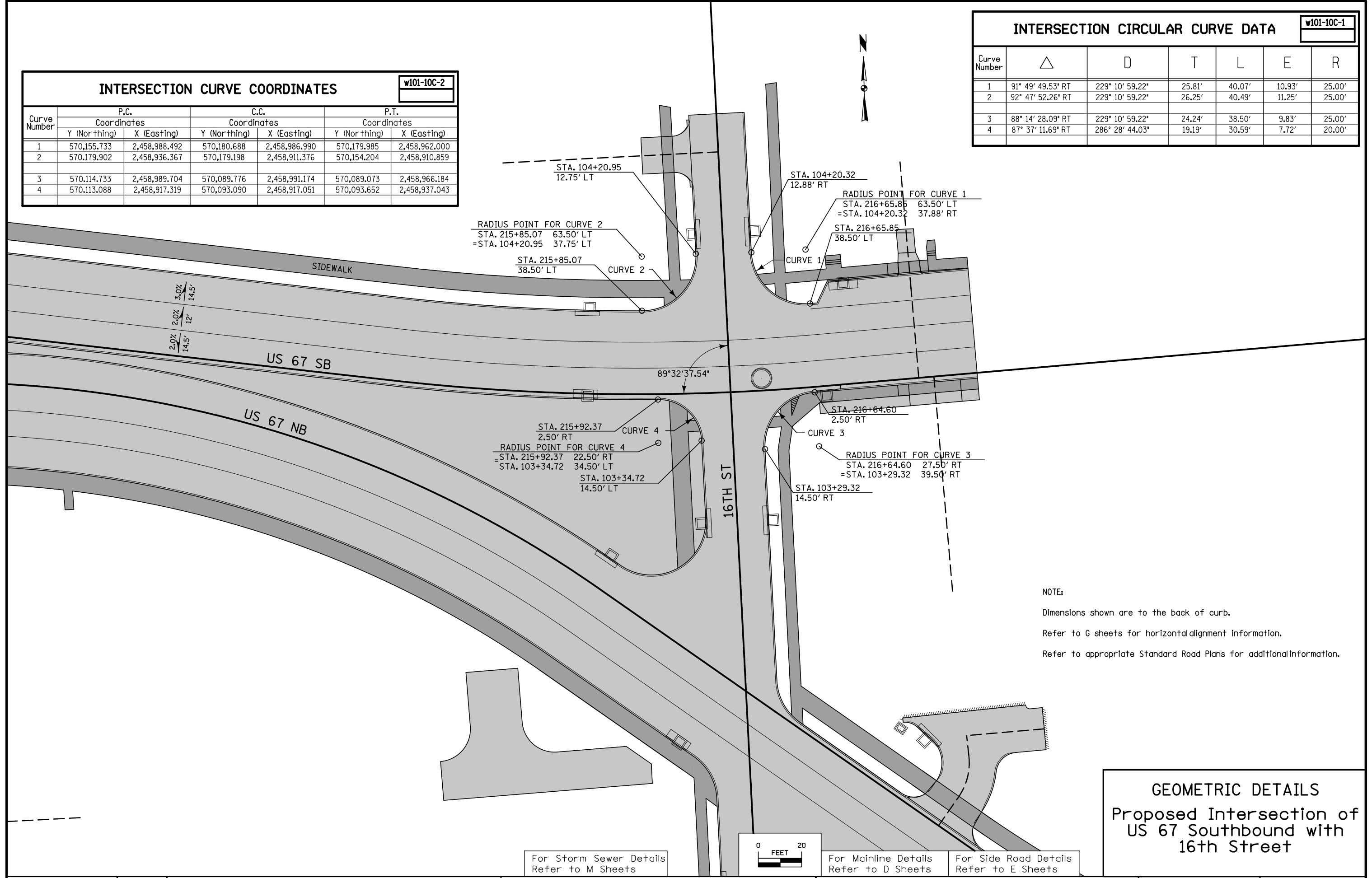
**INTERSECTION CIRCULAR CURVE DATA**

Curve Number	△	D	T	L	E	R
1	91° 49' 49.53" RT	229° 10' 59.22"	25.81'	40.07'	10.93'	25.00'
2	92° 47' 52.26" RT	229° 10' 59.22"	26.25'	40.49'	11.25'	25.00'
3	88° 14' 28.09" RT	229° 10' 59.22"	24.24'	38.50'	9.83'	25.00'
4	87° 37' 11.69" RT	286° 28' 44.03"	19.19'	30.59'	7.72'	20.00'

**INTERSECTION CURVE COORDINATES**

w101-10C-2

Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
	1	570,155.733	2,458,988.492	570,180.688	2,458,986.990	570,179.985
2	570,179.902	2,458,936.367	570,179.198	2,458,911.376	570,154.204	2,458,910.859
3	570,114.733	2,458,989.704	570,089.776	2,458,991.174	570,089.073	2,458,966.184
4	570,113.088	2,458,917.319	570,093.090	2,458,917.051	570,093.652	2,458,937.043



**NOTE:**

Dimensions shown are to the back of curb.  
 Refer to G sheets for horizontal alignment information.  
 Refer to appropriate Standard Road Plans for additional information.

**GEOMETRIC DETAILS**  
 Proposed Intersection of  
 US 67 Southbound with  
 16th Street

For Storm Sewer Details  
 Refer to M Sheets



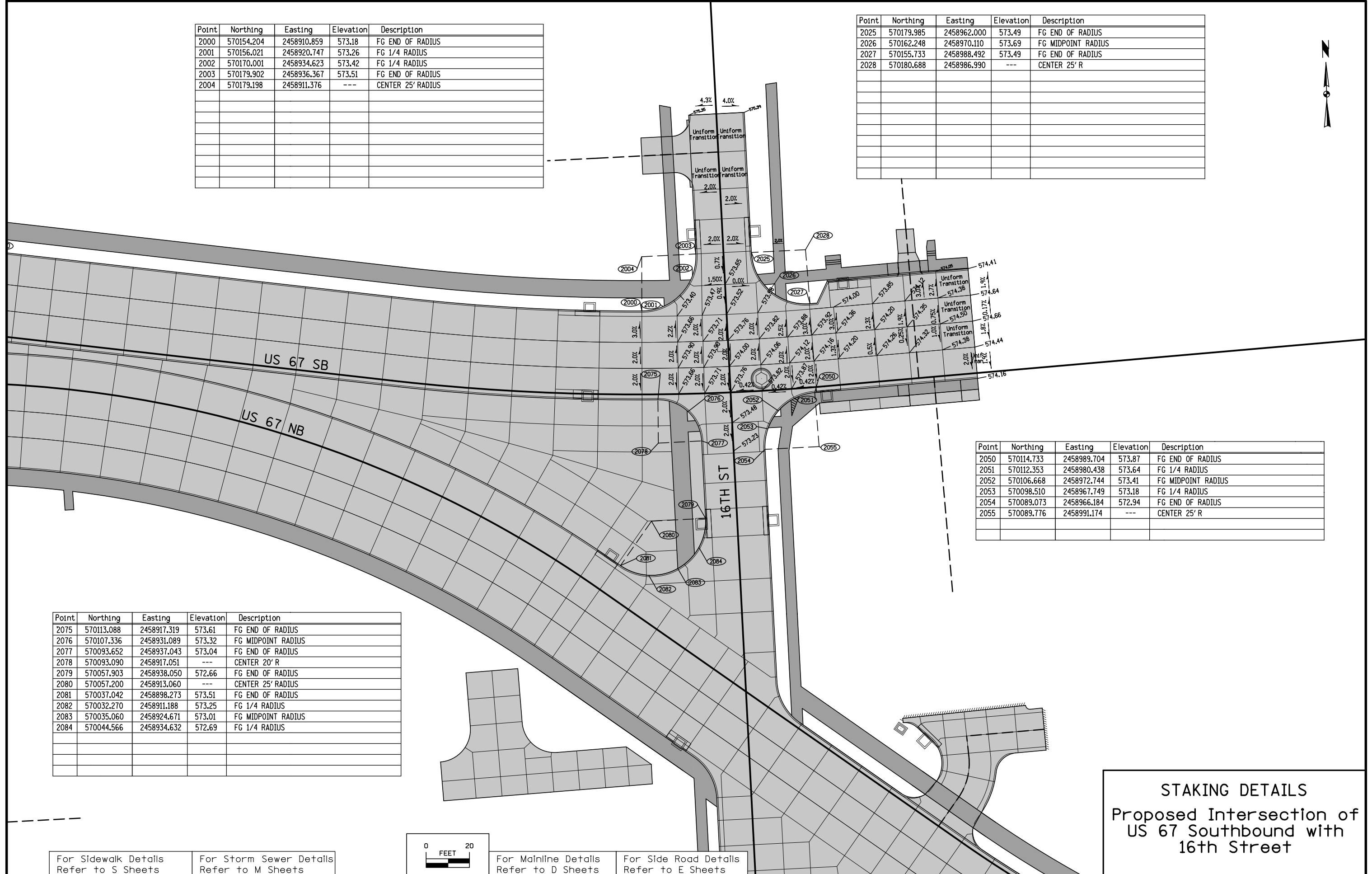
For Mainline Details  
 Refer to D Sheets

For Side Road Details  
 Refer to E Sheets



Point	Northing	Easting	Elevation	Description
2000	570154.204	2458910.859	573.18	FG END OF RADIUS
2001	570156.021	2458920.747	573.26	FG 1/4 RADIUS
2002	570170.001	2458934.623	573.42	FG 1/4 RADIUS
2003	570179.902	2458936.367	573.51	FG END OF RADIUS
2004	570179.198	2458911.376	---	CENTER 25' RADIUS

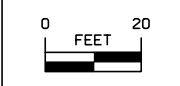
Point	Northing	Easting	Elevation	Description
2025	570179.985	2458962.000	573.49	FG END OF RADIUS
2026	570162.248	2458970.110	573.69	FG MIDPOINT RADIUS
2027	570155.733	2458988.492	573.49	FG END OF RADIUS
2028	570180.688	2458986.990	---	CENTER 25' R



Point	Northing	Easting	Elevation	Description
2050	570114.733	2458989.704	573.87	FG END OF RADIUS
2051	570112.353	2458980.438	573.64	FG 1/4 RADIUS
2052	570106.668	2458972.744	573.41	FG MIDPOINT RADIUS
2053	570098.510	2458967.749	573.18	FG 1/4 RADIUS
2054	570089.073	2458966.184	572.94	FG END OF RADIUS
2055	570089.776	2458991.174	---	CENTER 25' R

Point	Northing	Easting	Elevation	Description
2075	570113.088	2458917.319	573.61	FG END OF RADIUS
2076	570107.336	2458931.089	573.32	FG MIDPOINT RADIUS
2077	570093.652	2458937.043	573.04	FG END OF RADIUS
2078	570093.090	2458917.051	---	CENTER 20' R
2079	570057.903	2458938.050	572.66	FG END OF RADIUS
2080	570057.200	2458913.060	---	CENTER 25' RADIUS
2081	570037.042	2458898.273	573.51	FG END OF RADIUS
2082	570032.270	2458911.188	573.25	FG 1/4 RADIUS
2083	570035.060	2458924.671	573.01	FG MIDPOINT RADIUS
2084	570044.566	2458934.632	572.69	FG 1/4 RADIUS

**STAKING DETAILS**  
Proposed Intersection of  
US 67 Southbound with  
16th Street



For Sidewalk Details Refer to S Sheets | For Storm Sewer Details Refer to M Sheets

For Mainline Details Refer to D Sheets | For Side Road Details Refer to E Sheets



NOTE:

All Longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.

All transverse joints shall be CD joints with a maximum 20' spacing unless indicated otherwise.

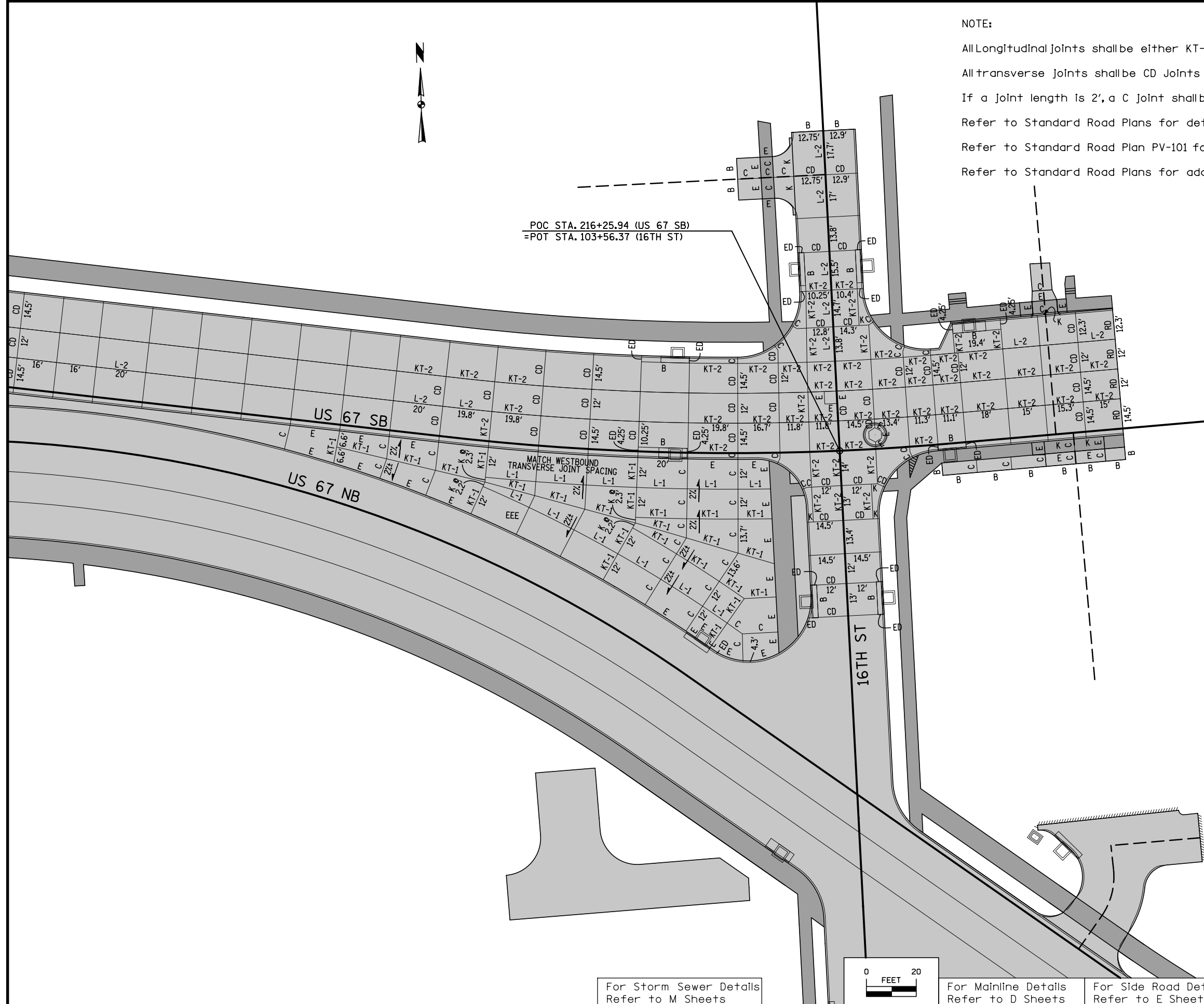
If a joint length is 2', a C joint shall be used instead of a CD joint.

Refer to Standard Road Plans for details of paved header, if applicable.

Refer to Standard Road Plan PV-101 for additional details.

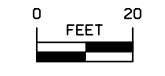
Refer to Standard Road Plans for additional jointing details around physical features.

POC STA. 216+25.94 (US 67 SB)  
=POT STA. 103+56.37 (16TH ST)



JOINTING DETAILS  
Proposed Intersection of  
US 67 Southbound with  
16th Street

For Storm Sewer Details  
Refer to M Sheets

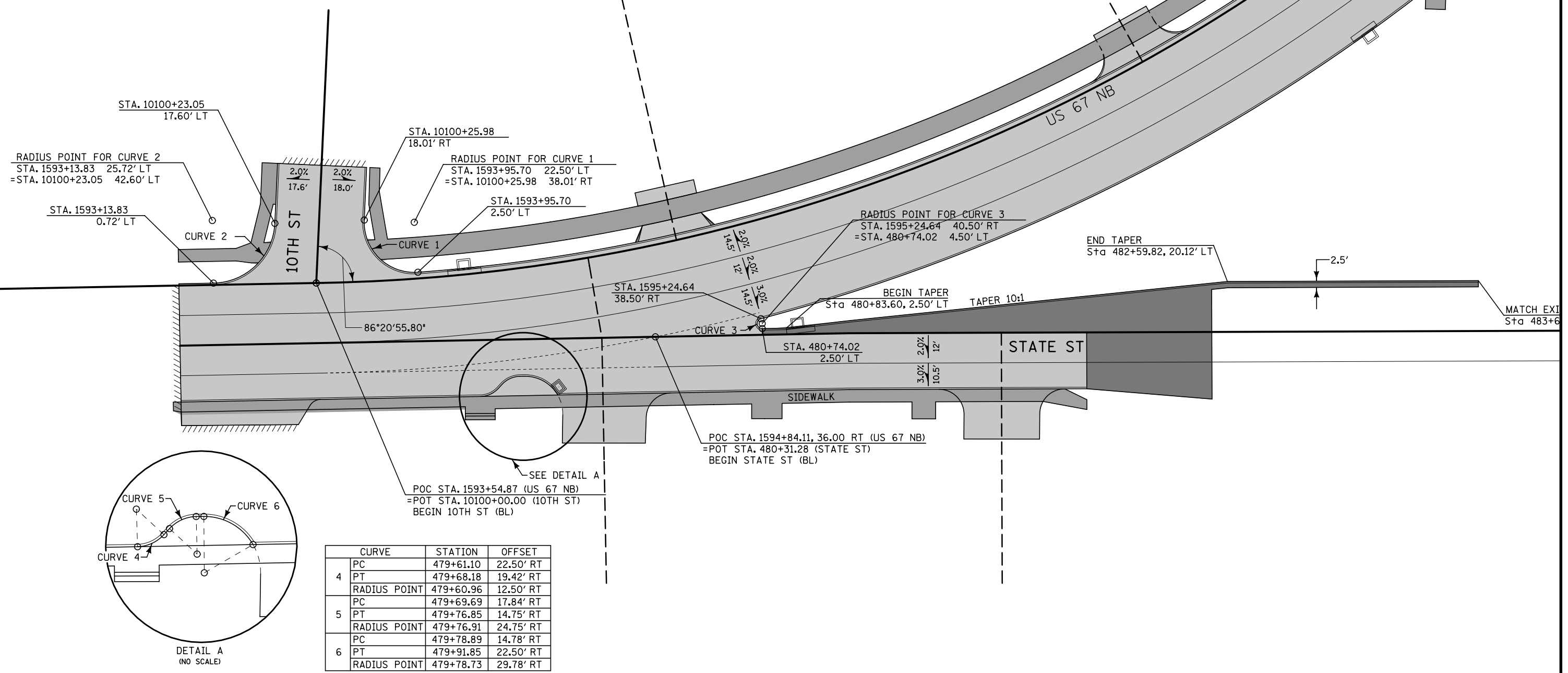


For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets

INTERSECTION CIRCULAR CURVE DATA						
Curve Number	$\Delta$	D	T	L	E	R
1	96° 46' 30.63" RT	286° 28' 44.03"	22.52'	33.78'	10.12'	20.00'
2	86° 20' 55.80" RT	229° 10' 59.22"	23.46'	37.68'	9.28'	25.00'
3	166° 05' 40.45" RT	2864° 47' 23.99"	16.40'	5.80'	14.52'	2.00'

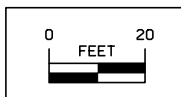
INTERSECTION CURVE COORDINATES						
Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
1	569,628.217	2,456,539.847	569,648.027	2,456,537.102	569,647.639	2,456,517.106
2	569,644.019	2,456,481.564	569,643.533	2,456,456.569	569,618.619	2,456,458.645
3	569,610.972	2,456,667.168	569,612.968	2,456,667.042	569,614.875	2,456,666.440



CURVE	STATION	OFFSET
4	PC	479+61.10 22.50' RT
	PT	479+68.18 19.42' RT
	RADIUS POINT	479+60.96 12.50' RT
5	PC	479+69.69 17.84' RT
	PT	479+76.85 14.75' RT
	RADIUS POINT	479+76.91 24.75' RT
6	PC	479+78.89 14.78' RT
	PT	479+91.85 22.50' RT
	RADIUS POINT	479+78.73 29.78' RT

NOTE:  
 Dimensions shown are to the back of curb.  
 Refer to G sheets for horizontal alignment information.  
 Refer to appropriate Standard Road Plans for additional information.

For Storm Sewer Details  
 Refer to M Sheets



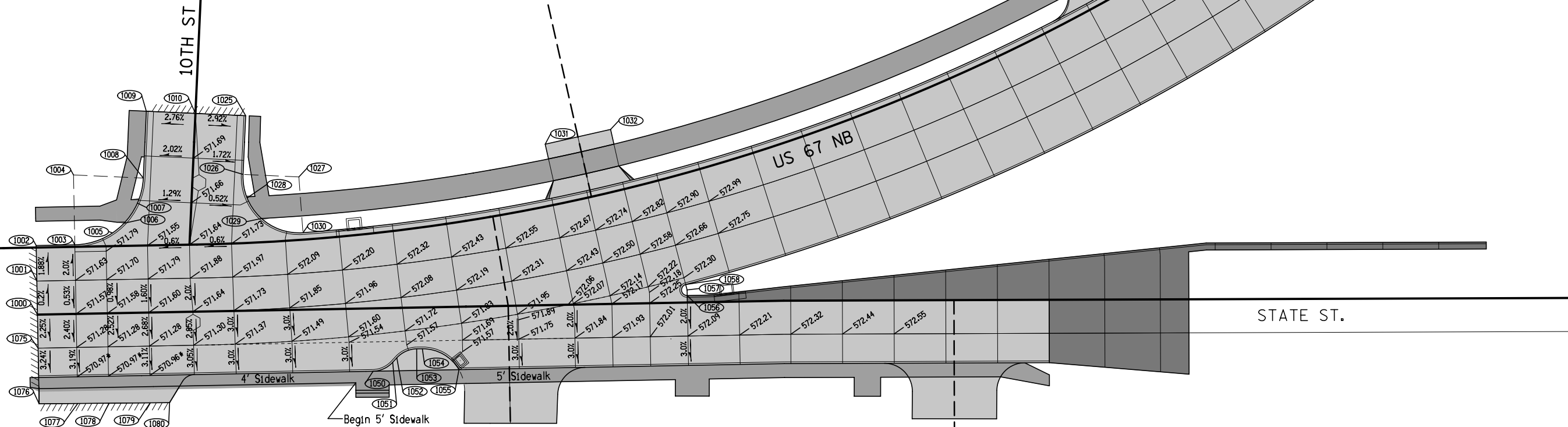
For Mainline Details  
 Refer to D Sheets

For Side Road Details  
 Refer to E Sheets

GEOMETRIC DETAILS  
 Proposed Intersection of  
 State Street with  
 10th Street

Point	Northing	Easting	Elevation	Description
1000	569592.839	2456446.919	571.56	END PAVEMENT/MATCH EXISTING
1001	569604.798	2456445.922	571.53	END PAVEMENT/MATCH EXISTING
1002	569617.471	2456444.865	571.29	FG END PAVEMENT/MATCH EXISTING
1003	569618.619	2456458.645	571.41	FG END OF RADIUS
1004	569643.533	2456456.569	---	CENTER OF 25' R
1005	569623.734	2456471.833	571.43	FG
1006	569627.458	2456475.715	571.43	FG
1007	569634.939	2456480.045	571.41	FG
1008	569644.019	2456481.564	571.38	FG END OF RADIUS
1009	569667.963	2456481.099	571.72	FG END PAVEMENT/MATCH EXISTING
1010	569668.305	2456498.695	571.71	END PAVEMENT/MATCH EXISTING

Point	Northing	Easting	Elevation	Description
1025	569668.655	2456516.698	571.70	FG END PAVEMENT /MATCH EXISTING
1026	569647.639	2456517.106	571.44	FG END OF RADIUS
1027	569648.027	2456537.102	---	CENTER 20' R
1028	569639.315	2456519.099	571.51	FG BOTTOM OF PED RAMP
1029	569632.329	2456524.711	571.65	FG BOTTOM OF PED RAMP
1030	569628.217	2456539.847	571.83	FG END OF RADIUS
1031	569665.393	2456624.325	573.16	END DRIVEWAY/MATCH EXISTING
1032	569672.180	2456647.345	573.41	END DRIVEWAY/MATCH EXISTING

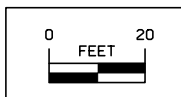


\* - Elevations are Form Grade  
Add 0.25' for Top of 3" Drop Curb

Point	Northing	Easting	Elevation	Description
1075	569580.881	2456447.916	571.29	END PAVEMENT/MATCH EXISTING
1076	569561.032	2456449.571	571.38	END PAVEMENT/MATCH EXISTING
1077	569562.046	2456463.361	571.47	END PAVEMENT/MATCH EXISTING
1078	569562.866	2456474.503	571.41	END PAVEMENT/MATCH EXISTING
1079	569563.947	2456489.207	571.71	END PAVEMENT/MATCH EXISTING
1080	569564.456	2456496.129	571.81	END PAVEMENT/MATCH EXISTING

Point	Northing	Easting	Elevation	Description
1050	569580.361	2456568.092	571.34	FG BEGN RADIUS
1051	569584.021	2456574.889	571.44	FG END RADIUS
1052	569585.719	2456576.263	571.47	FG BEGN RADIUS
1053	569589.387	2456583.145	571.51	FG END RADIUS
1054	569589.535	2456585.179	571.50	FG BEGN RADIUS
1055	569582.914	2456598.731	571.37	FG END RADIUS
1056	569614.653	2456678.541	572.28	FG BEGN RADIUS
1057	569616.646	2456678.375	---	CENTER 2' RADIUS
1058	569618.543	2456677.742	572.25	FG END RADIUS

STAKING DETAILS  
Proposed Intersection of  
State Street with  
10th Street



For Sidewalk Details Refer to S Sheets  
For Storm Sewer Details Refer to M Sheets

For Mainline Details Refer to D Sheets  
For Side Road Details Refer to E Sheets

NOTE:

All Longitudinal Joints shall be either KT-2 or L-2 unless indicated otherwise.

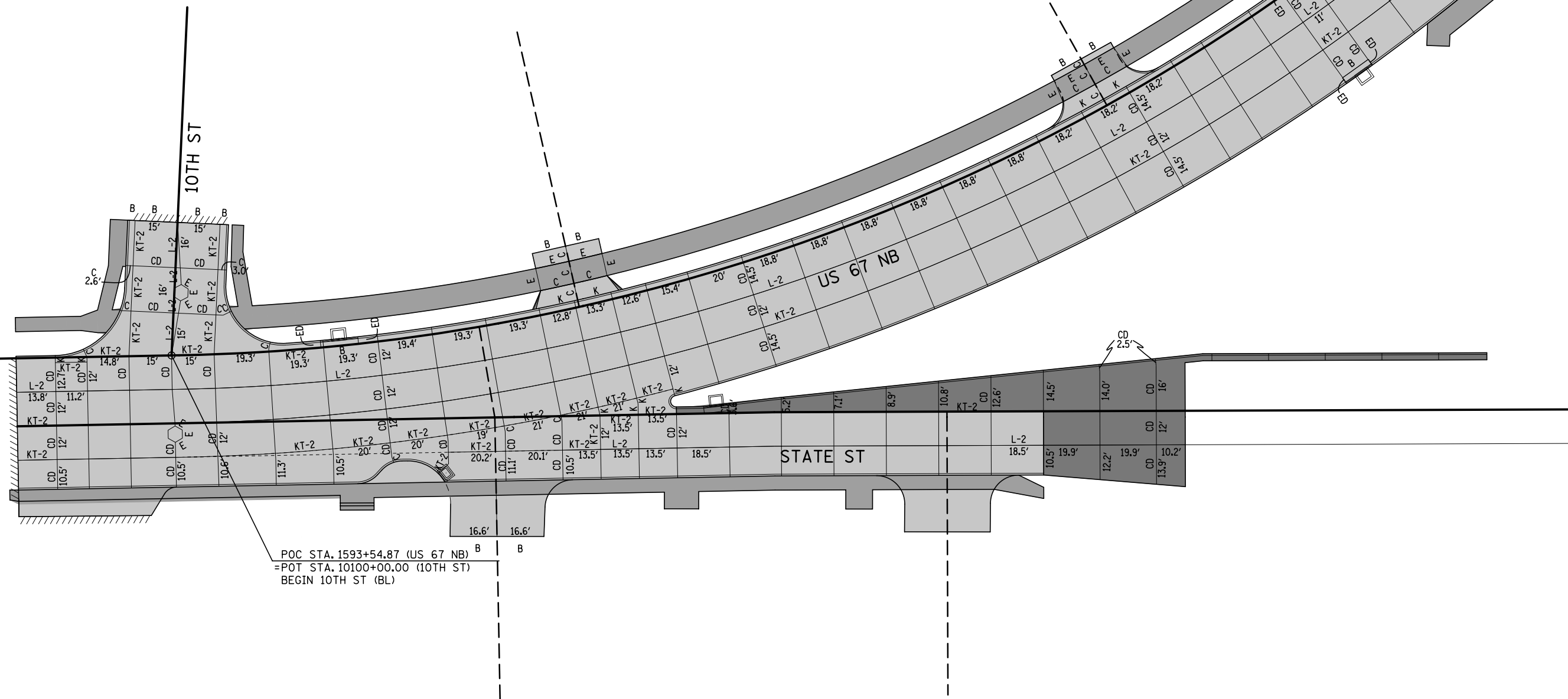
All transverse joints shall be CD Joints with a maximum 20' spacing unless indicated otherwise.

If a joint length is 2', a C Joint shall be used instead of a CD Joint.

Refer to Standard Road Plans for details of paved header, if applicable.

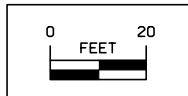
Refer to Standard Road Plan PV-101 for additional details.

Refer to Standard Road Plans for additional jointing details around physical features.



JOINTING DETAILS  
Proposed Intersection of  
State Street with  
10th Street

For Storm Sewer Details  
Refer to M Sheets



For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets

**INTERSECTION CIRCULAR CURVE DATA**

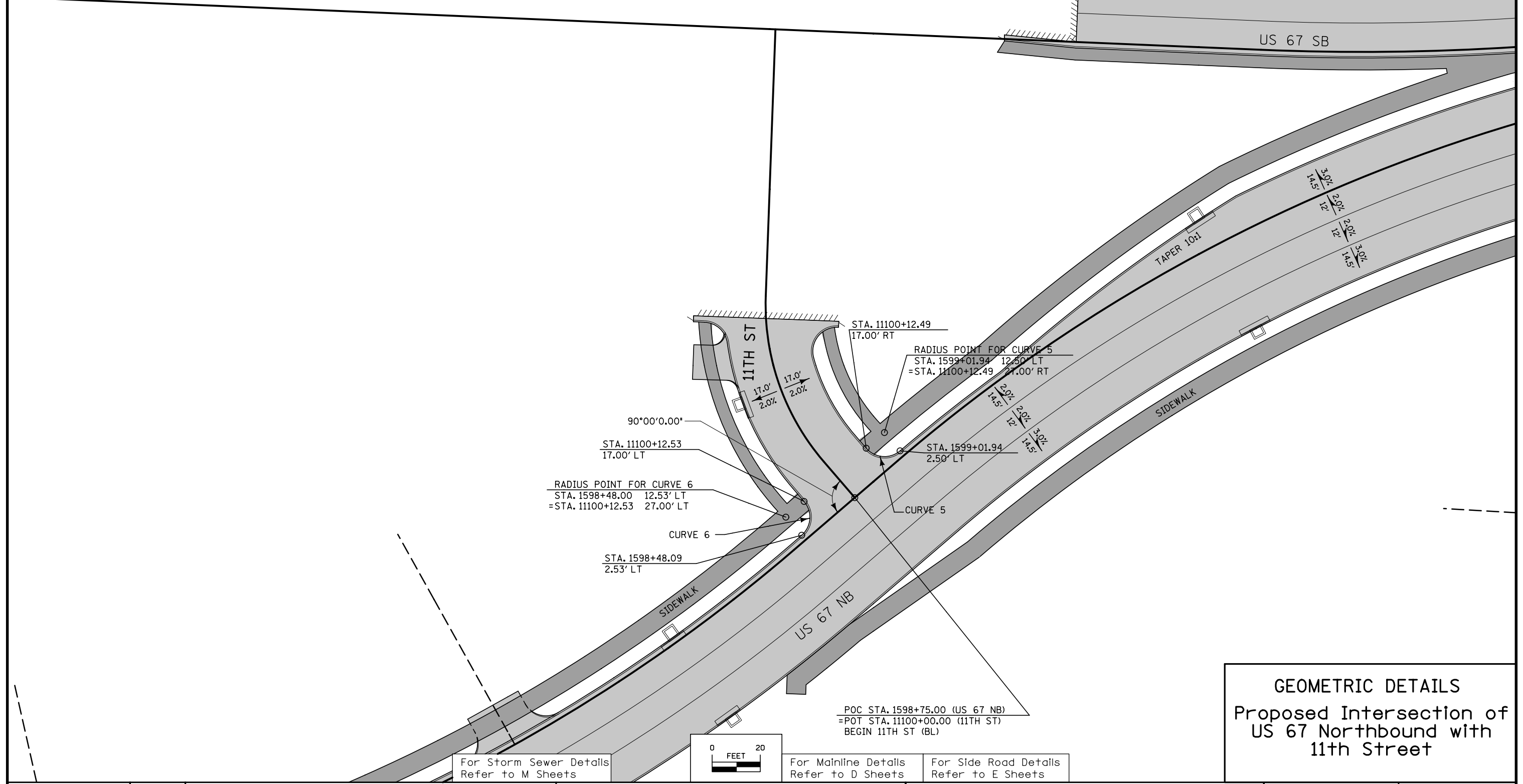
w101-10C-1

Curve Number	$\Delta$	D	T	L	E	R
5	89° 43' 15.46" RT	212° 57' 28.06"	9.95'	15.66'	4.11'	10.00'
6	89° 27' 53.67" RT	212° 57' 28.06"	9.95'	15.61'	4.09'	10.00'

**INTERSECTION CURVE COORDINATES**

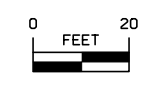
w101-10C-2

Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
5	569,862.858	2,456,975.147	569,870.039	2,456,968.187	569,863.044	2,456,961.041
6	569,839.290	2,456,936.715	569,832.295	2,456,929.568	569,825.214	2,456,936.629



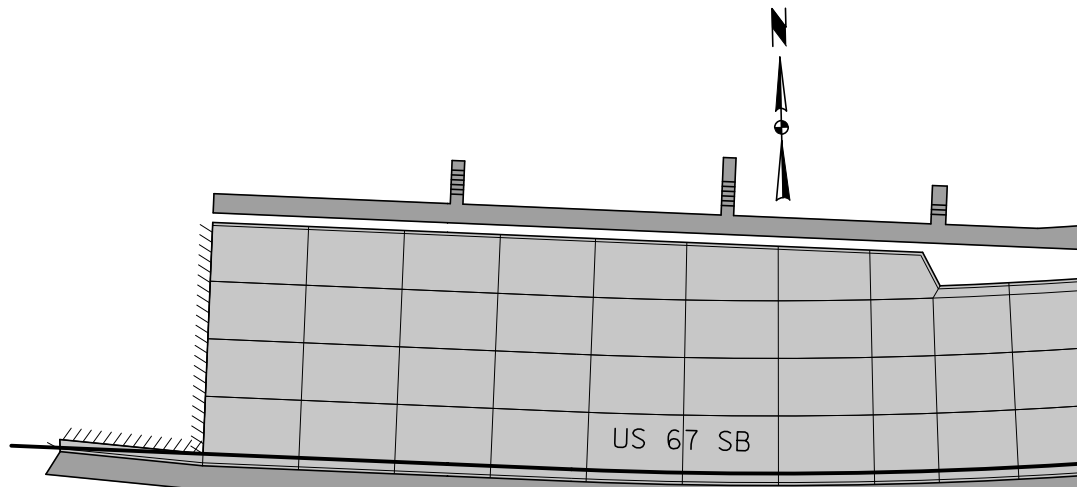
**GEOMETRIC DETAILS**  
Proposed Intersection of  
US 67 Northbound with  
11th Street

For Storm Sewer Details  
Refer to M Sheets



For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets

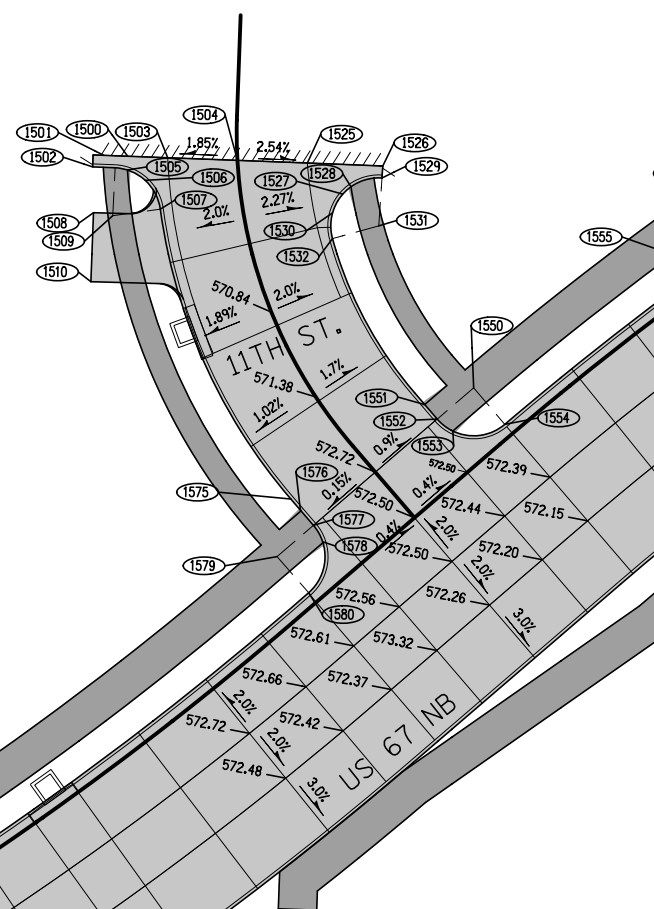


Point	Northing	Easting	Elevation	Description
1525	569914.572	2456930.580	569.60	END PAVEMENT/MATCH EXISTING
1526	569914.948	2456946.304	570.18	END PAVEMENT/MATCH EXISTING
1527	569908.558	2456938.365	570.04	FG
1528	569910.205	2456939.972	570.06	FG
1529	569912.444	2456946.299	570.31	END PAVEMENT/MATCH EXISTING
1530	569904.518	2456936.496	570.04	FG
1531	569902.444	2456946.279	---	CENTER 10' R
1532	569899.177	2456936.828	570.20	FG END OF RADIUS

Point	Northing	Easting	Elevation	Description
1500	569913.588	2456893.099	569.75	END PAVEMENT/MATCH EXISTING
1501	569913.455	2456888.065	569.84	END PAVEMENT/MATCH EXISTING
1502	569910.864	2456885.980	567.79	END PAVEMENT/MATCH EXISTING
1503	569913.807	2456901.455	569.70	END PAVEMENT/MATCH EXISTING
1504	569914.189	2456916.008	569.97	END PAVEMENT/MATCH EXISTING
1505	569910.687	2456893.253	569.70	FG
1506	569908.778	2456897.090	569.60	FG MIDRADIUS
1507	569902.981	2456900.619	569.84	FG END OF RADIUS
1508	569901.351	2456886.774	570.22	END PAVEMENT/MATCH EXISTING
1509	569900.988	2456890.819	---	CENTER 10' R
1510	569886.967	2456887.159	570.46	END PAVEMENT/MATCH EXISTING

Point	Northing	Easting	Elevation	Description
1550	569870.039	2456968.187	---	CENTER 10' RADIUS
1551	569865.888	2456958.258	571.65	FG
1552	569863.044	2456961.041	571.77	FG END OF RADIUS
1553	569860.764	2456964.450	571.84	FG
1554	569862.858	2456975.147	572.10	FG END OF RADIUS
1555	569898.435	2457014.752	571.89	FG BEGIN OF TAPER
1556	569912.624	2457029.127	571.77	FG @ 1599+75.0
1557	569926.431	2457043.948	571.65	FG @ 1599+95.0
1558	569939.842	2457059.203	571.53	FG @ 1600+15.0
1559	569952.844	2457074.883	571.42	FG @ 1600+35.0
1560	569962.862	2457087.619	571.35	FG @ 1600+50.87

Point	Northing	Easting	Elevation	Description
1575	569844.458	2456931.656	571.65	FG
1576	569842.268	2456933.800	571.80	FG
1577	569839.290	2456936.715	571.90	FG END OF RADIUS
1578	569835.996	2456938.858	571.99	FG
1579	569832.295	2456929.568	---	CENTER 15' R
1580	569825.214	2456936.629	572.32	FG END OF RADIUS



**STAKING DETAILS**  
Proposed Intersection of  
US 67 Northbound with  
11th Street

For Sidewalk Details Refer to S Sheets	For Storm Sewer Details Refer to M Sheets	0 FEET 20	For Mainline Details Refer to D Sheets	For Side Road Details Refer to E Sheets
-------------------------------------------	----------------------------------------------	-----------------	-------------------------------------------	--------------------------------------------

NOTE:

All Longitudinal Joints shall be either KT-2 or L-2 unless indicated otherwise.

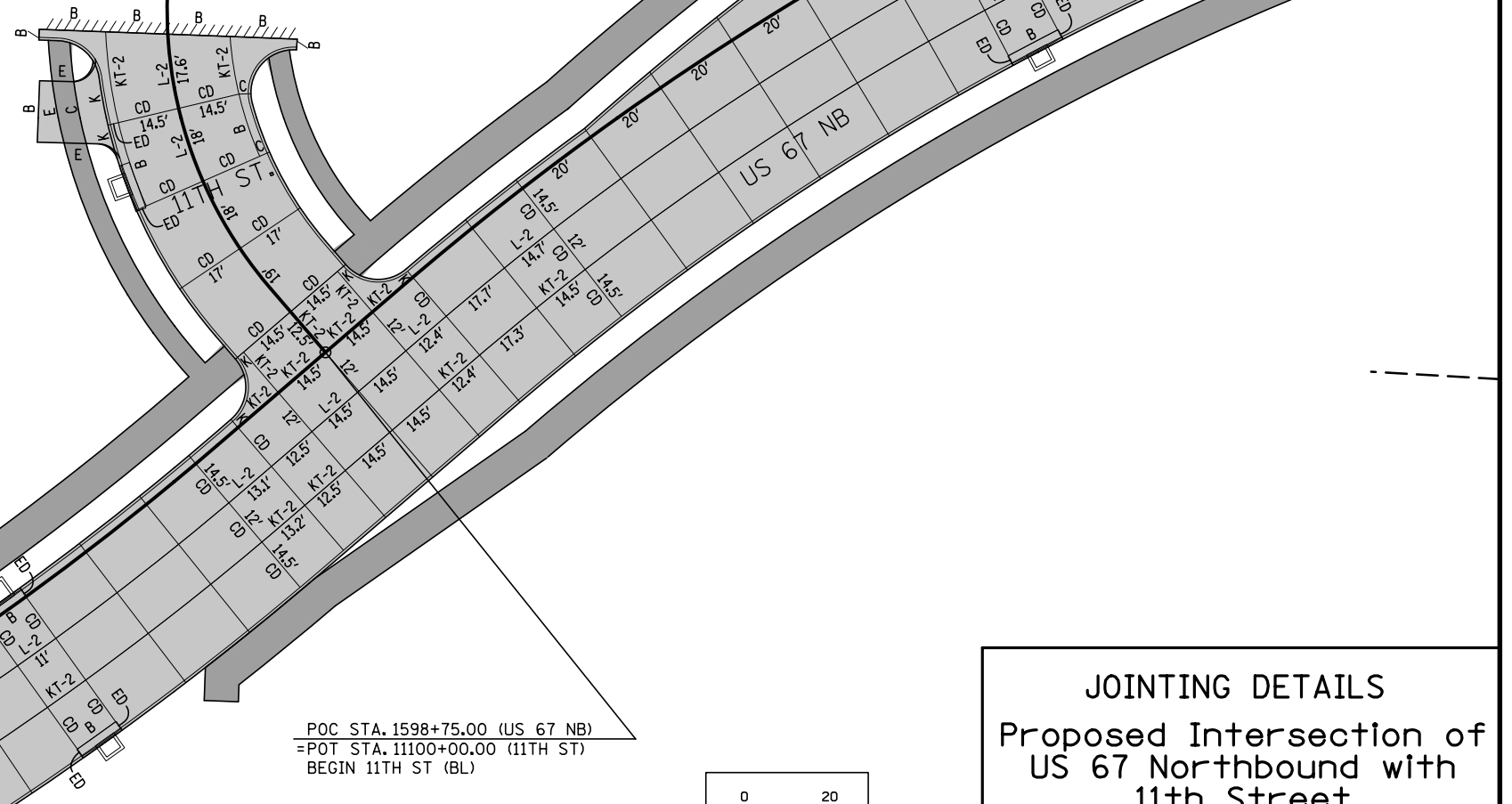
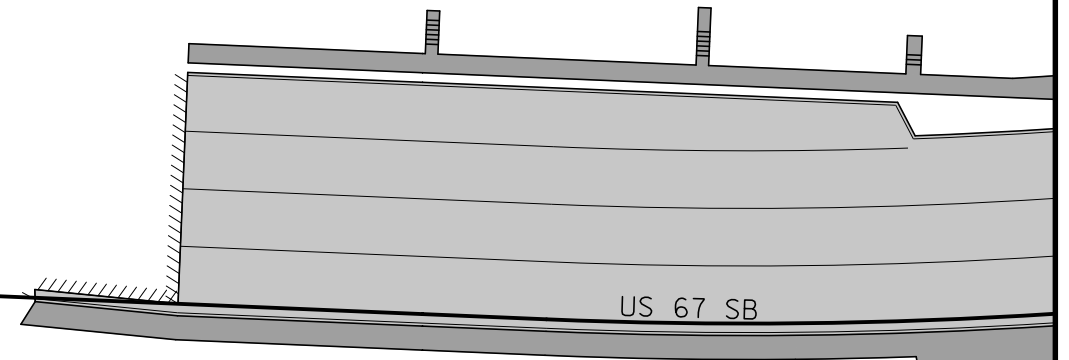
All transverse Joints shall be CD Joints with a maximum 20' spacing unless indicated otherwise.

If a joint length is 2', a C Joint shall be used instead of a CD Joint.

Refer to Standard Road Plans for details of paved header, if applicable.

Refer to Standard Road Plan PV-101 for additional details.

Refer to Standard Road Plans for additional jointing details around physical features.

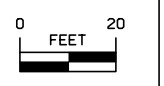


**JOINTING DETAILS**  
 Proposed Intersection of  
 US 67 Northbound with  
 11th Street

For Storm Sewer Details  
 Refer to M Sheets

For Mainline Details  
 Refer to D Sheets

For Side Road Details  
 Refer to E Sheets





**INTERSECTION CIRCULAR CURVE DATA**

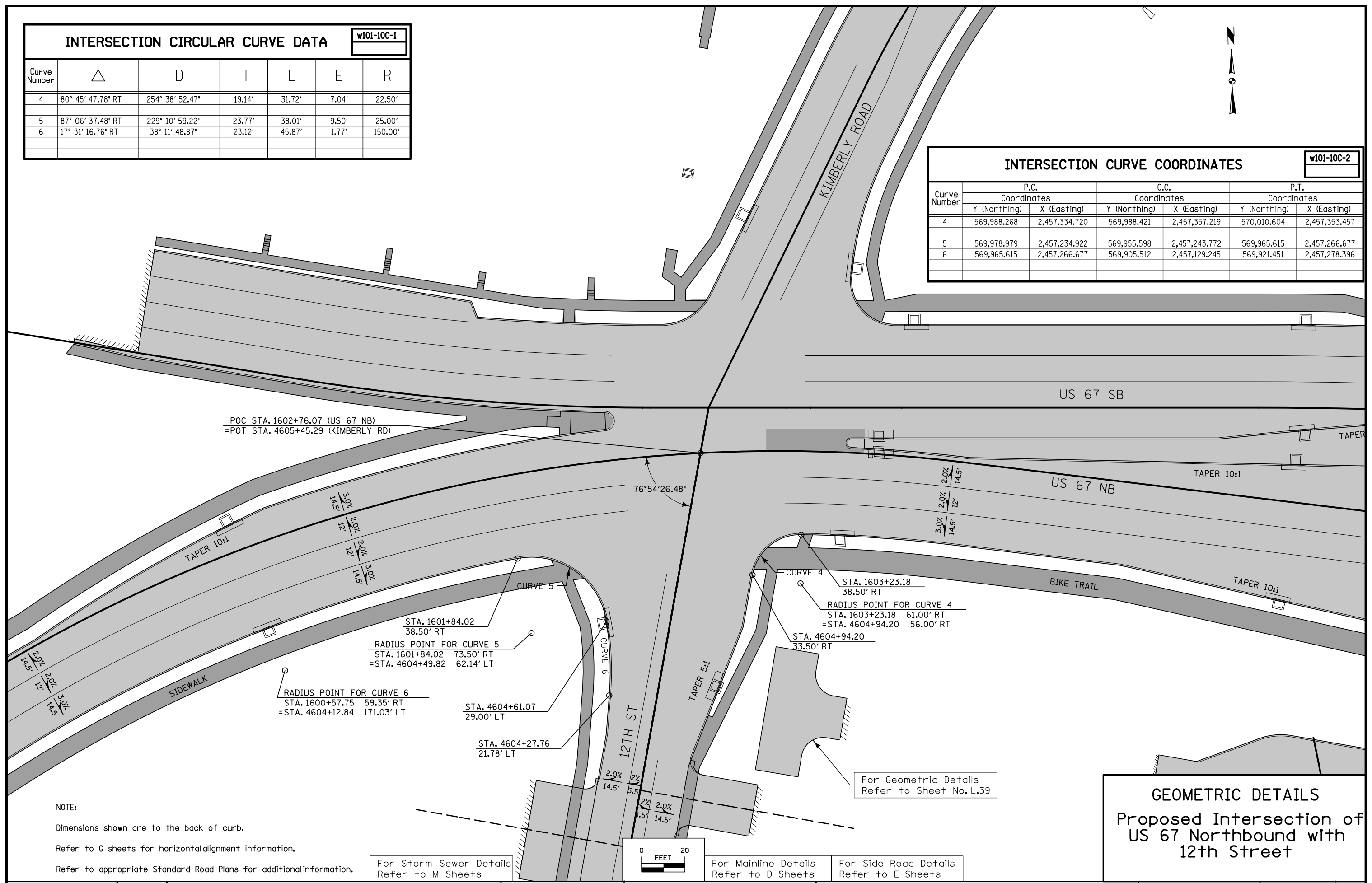
w101-10C-1

Curve Number	△	D	T	L	E	R
4	80° 45' 47.78" RT	254° 38' 52.47"	19.14'	31.72'	7.04'	22.50'
5	87° 06' 37.48" RT	229° 10' 59.22"	23.77'	38.01'	9.50'	25.00'
6	17° 31' 16.76" RT	38° 11' 48.87"	23.12'	45.87'	1.77'	150.00'

**INTERSECTION CURVE COORDINATES**

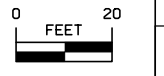
w101-10C-2

Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
4	569,988.268	2,457,334.720	569,988.421	2,457,357.219	570,010.604	2,457,353.457
5	569,978.979	2,457,234.922	569,955.598	2,457,243.772	569,965.615	2,457,266.677
6	569,965.615	2,457,266.677	569,905.512	2,457,129.245	569,921.451	2,457,278.396



NOTE:  
 Dimensions shown are to the back of curb.  
 Refer to G sheets for horizontal alignment information.  
 Refer to appropriate Standard Road Plans for additional information.

For Storm Sewer Details Refer to M Sheets



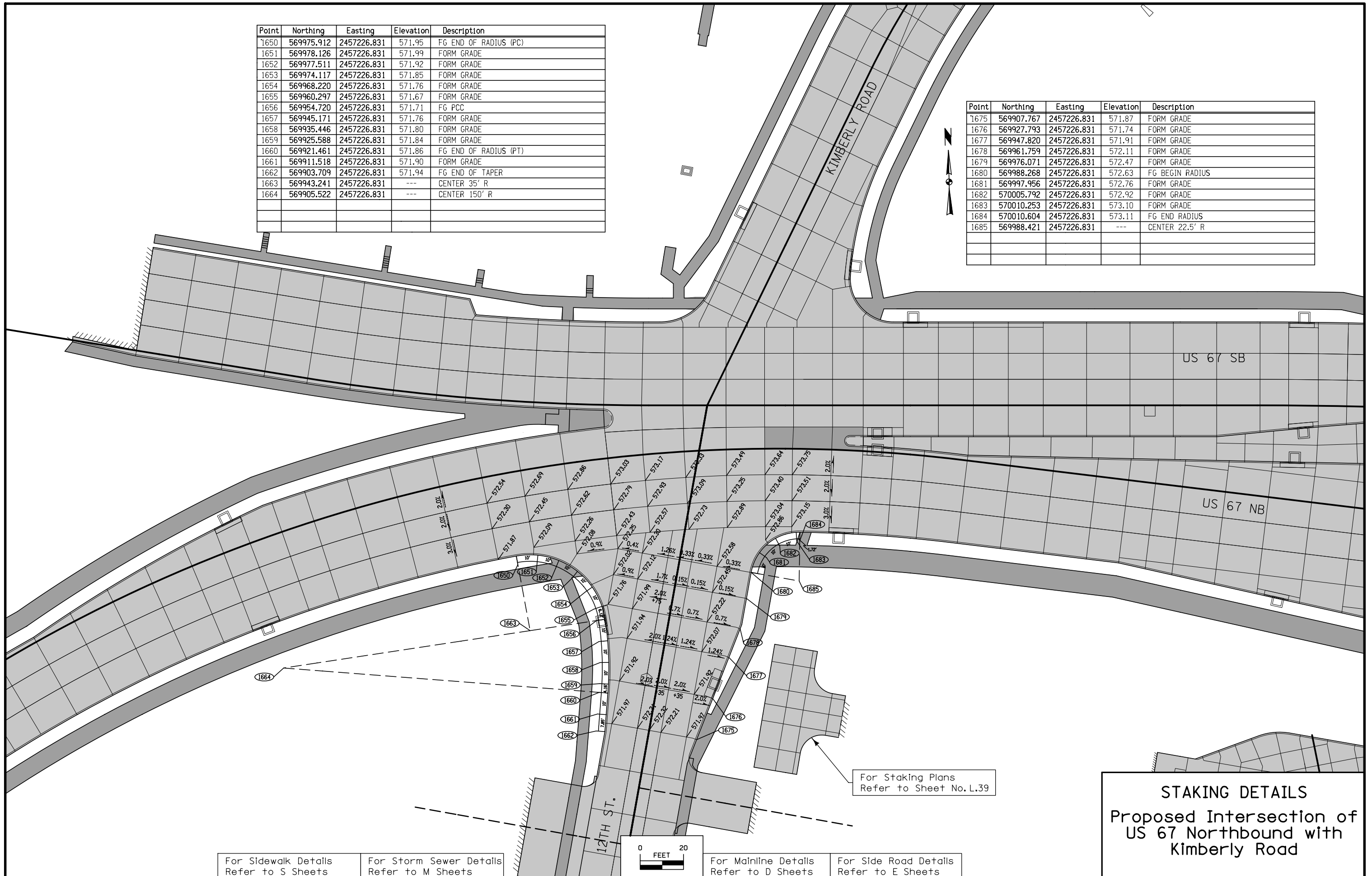
For Mainline Details Refer to D Sheets

For Side Road Details Refer to E Sheets

**GEOMETRIC DETAILS**  
 Proposed Intersection of  
 US 67 Northbound with  
 12th Street

Point	Northing	Easting	Elevation	Description
1650	569975.912	2457226.831	571.95	FG END OF RADIUS (PC)
1651	569978.126	2457226.831	571.99	FORM GRADE
1652	569977.511	2457226.831	571.92	FORM GRADE
1653	569974.117	2457226.831	571.85	FORM GRADE
1654	569968.220	2457226.831	571.76	FORM GRADE
1655	569960.297	2457226.831	571.67	FORM GRADE
1656	569954.720	2457226.831	571.71	FG PCC
1657	569945.171	2457226.831	571.76	FORM GRADE
1658	569935.446	2457226.831	571.80	FORM GRADE
1659	569925.588	2457226.831	571.84	FORM GRADE
1660	569921.461	2457226.831	571.86	FG END OF RADIUS (PT)
1661	569911.518	2457226.831	571.90	FORM GRADE
1662	569903.709	2457226.831	571.94	FG END OF TAPER
1663	569943.241	2457226.831	---	CENTER 35' R
1664	569905.522	2457226.831	---	CENTER 150' R

Point	Northing	Easting	Elevation	Description
1675	569907.767	2457226.831	571.87	FORM GRADE
1676	569927.793	2457226.831	571.74	FORM GRADE
1677	569947.820	2457226.831	571.91	FORM GRADE
1678	569961.759	2457226.831	572.11	FORM GRADE
1679	569976.071	2457226.831	572.47	FORM GRADE
1680	569988.268	2457226.831	572.63	FG BEGIN RADIUS
1681	569997.956	2457226.831	572.76	FORM GRADE
1682	570005.792	2457226.831	572.92	FORM GRADE
1683	570010.253	2457226.831	573.10	FORM GRADE
1684	570010.604	2457226.831	573.11	FG END RADIUS
1685	569988.421	2457226.831	---	CENTER 22.5' R



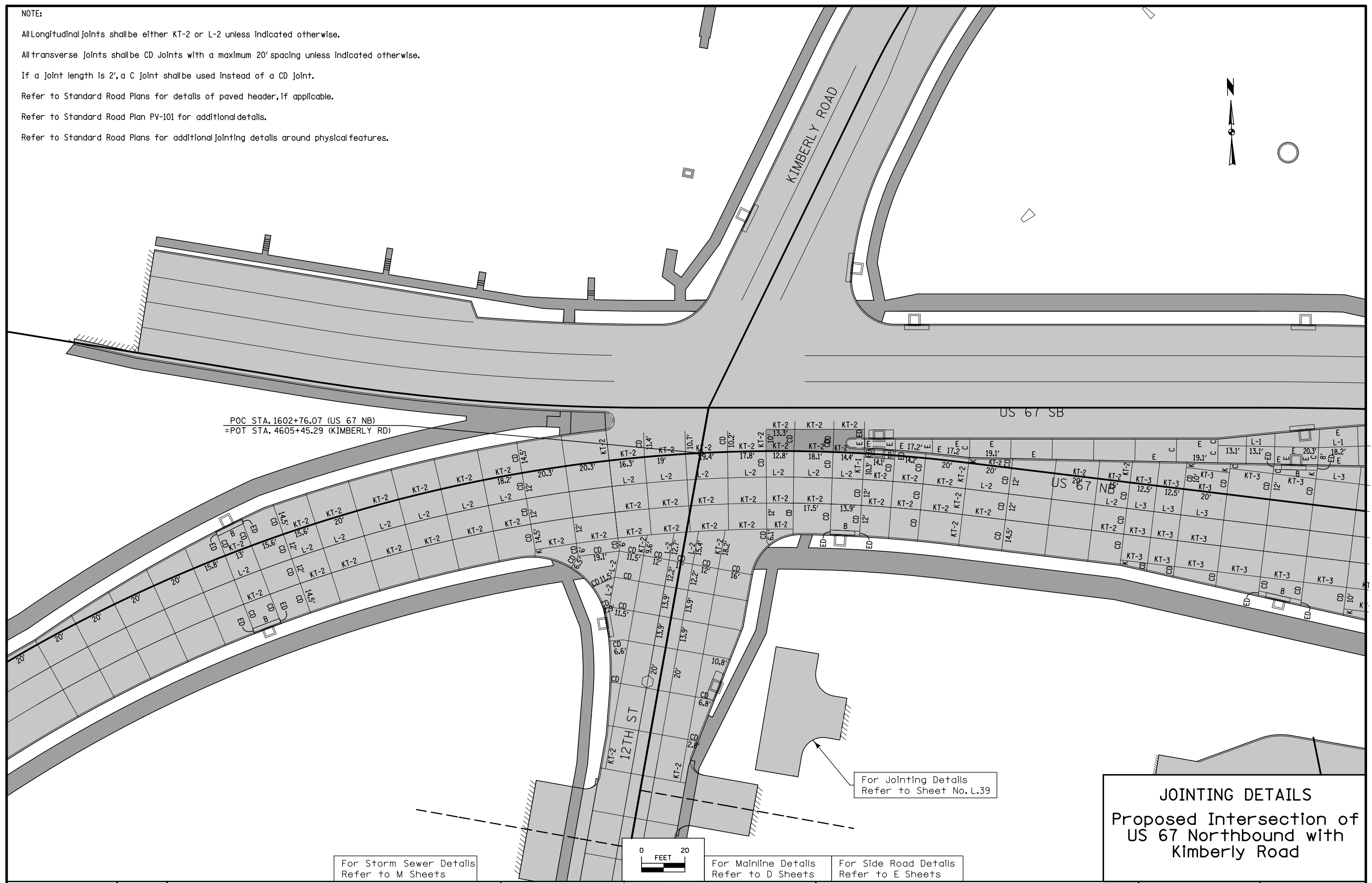
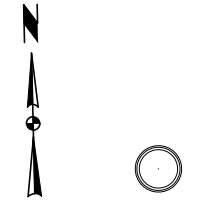
For Sidewalk Details Refer to S Sheets  
 For Storm Sewer Details Refer to M Sheets

For Mainline Details Refer to D Sheets  
 For Side Road Details Refer to E Sheets

**STAKING DETAILS**  
 Proposed Intersection of  
 US 67 Northbound with  
 Kimberly Road

**NOTE:**

- All Longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.
- All transverse joints shall be CD Joints with a maximum 20' spacing unless indicated otherwise.
- If a joint length is 2', a C joint shall be used instead of a CD joint.
- Refer to Standard Road Plans for details of paved header, if applicable.
- Refer to Standard Road Plan PV-101 for additional details.
- Refer to Standard Road Plans for additional jointing details around physical features.

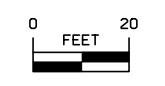


POC STA. 1602+76.07 (US 67 NB)  
 =POT STA. 4605+45.29 (KIMBERLY RD)

For Jointing Details  
 Refer to Sheet No. L.39

**JOINTING DETAILS**  
 Proposed Intersection of  
 US 67 Northbound with  
 Kimberly Road

For Storm Sewer Details  
 Refer to M Sheets



For Mainline Details  
 Refer to D Sheets

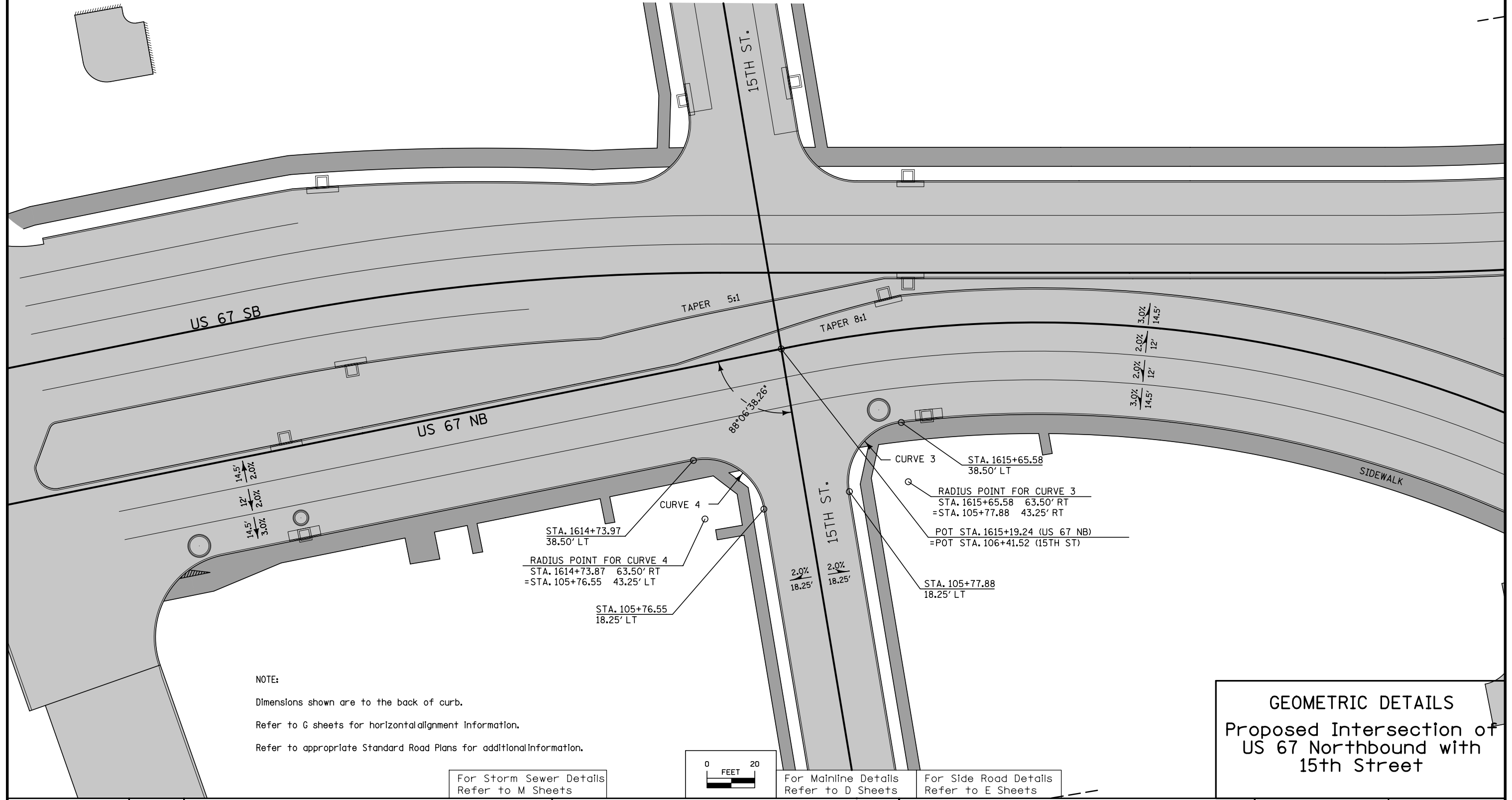
For Side Road Details  
 Refer to E Sheets

INTERSECTION CIRCULAR CURVE DATA						
Curve Number	$\Delta$	D	T	L	E	R
3	92° 46' 23.16" RT	229' 10" 59.22"	26.24'	40.48'	11.24'	25.00'
4	91° 53' 19.32" RT	229' 10" 59.22"	25.84'	40.09'	10.95'	25.00'

w101-10C-1

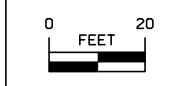
INTERSECTION CURVE COORDINATES						
Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
	3	570,064.382	2,458,560.485	570,065.126	2,458,585.473	570,090.122
4	570,086.181	2,458,497.577	570,061.225	2,458,499.051	570,061.970	2,458,524.040

w101-10C-2



NOTE:  
 Dimensions shown are to the back of curb.  
 Refer to G sheets for horizontal alignment information.  
 Refer to appropriate Standard Road Plans for additional information.

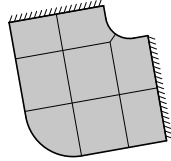
For Storm Sewer Details  
 Refer to M Sheets



For Mainline Details  
 Refer to D Sheets

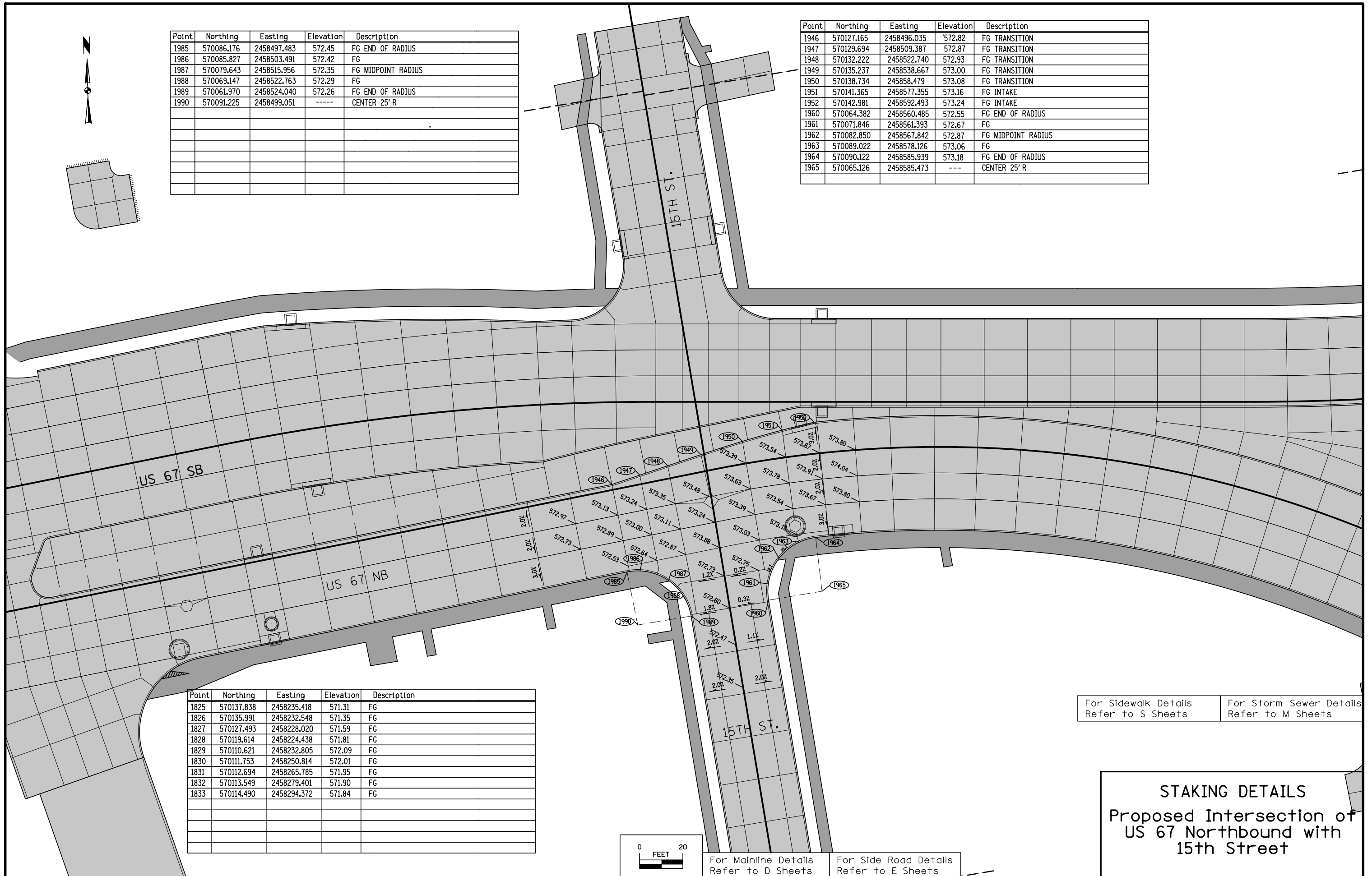
For Side Road Details  
 Refer to E Sheets

GEOMETRIC DETAILS  
 Proposed Intersection of  
 US 67 Northbound with  
 15th Street



Point	Northing	Easting	Elevation	Description
1985	570086.176	2458497.483	572.45	FG END OF RADIUS
1986	570085.827	2458503.491	572.42	FG
1987	570079.643	2458515.956	572.35	FG MIDPOINT RADIUS
1988	570069.147	2458522.763	572.29	FG
1989	570061.970	2458524.040	572.26	FG END OF RADIUS
1990	570091.225	2458499.051	----	CENTER 25' R

Point	Northing	Easting	Elevation	Description
1946	570127.165	2458496.035	572.82	FG TRANSITION
1947	570129.694	2458509.387	572.87	FG TRANSITION
1948	570132.222	2458522.740	572.93	FG TRANSITION
1949	570135.237	2458538.667	573.00	FG TRANSITION
1950	570138.734	245858.479	573.08	FG TRANSITION
1951	570141.365	2458577.355	573.16	FG INTAKE
1952	570142.981	2458592.493	573.24	FG INTAKE
1960	570064.382	2458560.485	572.55	FG END OF RADIUS
1961	570071.846	2458561.393	572.67	FG
1962	570082.850	2458567.842	572.87	FG MIDPOINT RADIUS
1963	570089.022	2458578.126	573.06	FG
1964	570090.122	2458585.939	573.18	FG END OF RADIUS
1965	570065.126	2458585.473	---	CENTER 25' R

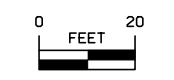


Point	Northing	Easting	Elevation	Description
1825	570137.838	2458235.418	571.31	FG
1826	570135.991	2458232.548	571.35	FG
1827	570127.493	2458228.020	571.59	FG
1828	570119.614	2458224.438	571.81	FG
1829	570110.621	2458232.805	572.09	FG
1830	570111.753	2458250.814	572.01	FG
1831	570112.694	2458265.785	571.95	FG
1832	570113.549	2458279.401	571.90	FG
1833	570114.490	2458294.372	571.84	FG

For Sidewalk Details Refer to S Sheets      For Storm Sewer Details Refer to M Sheets

For Mainline Details Refer to D Sheets      For Side Road Details Refer to E Sheets

**STAKING DETAILS**  
Proposed Intersection of  
US 67 Northbound with  
15th Street



NOTE:

All Longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.

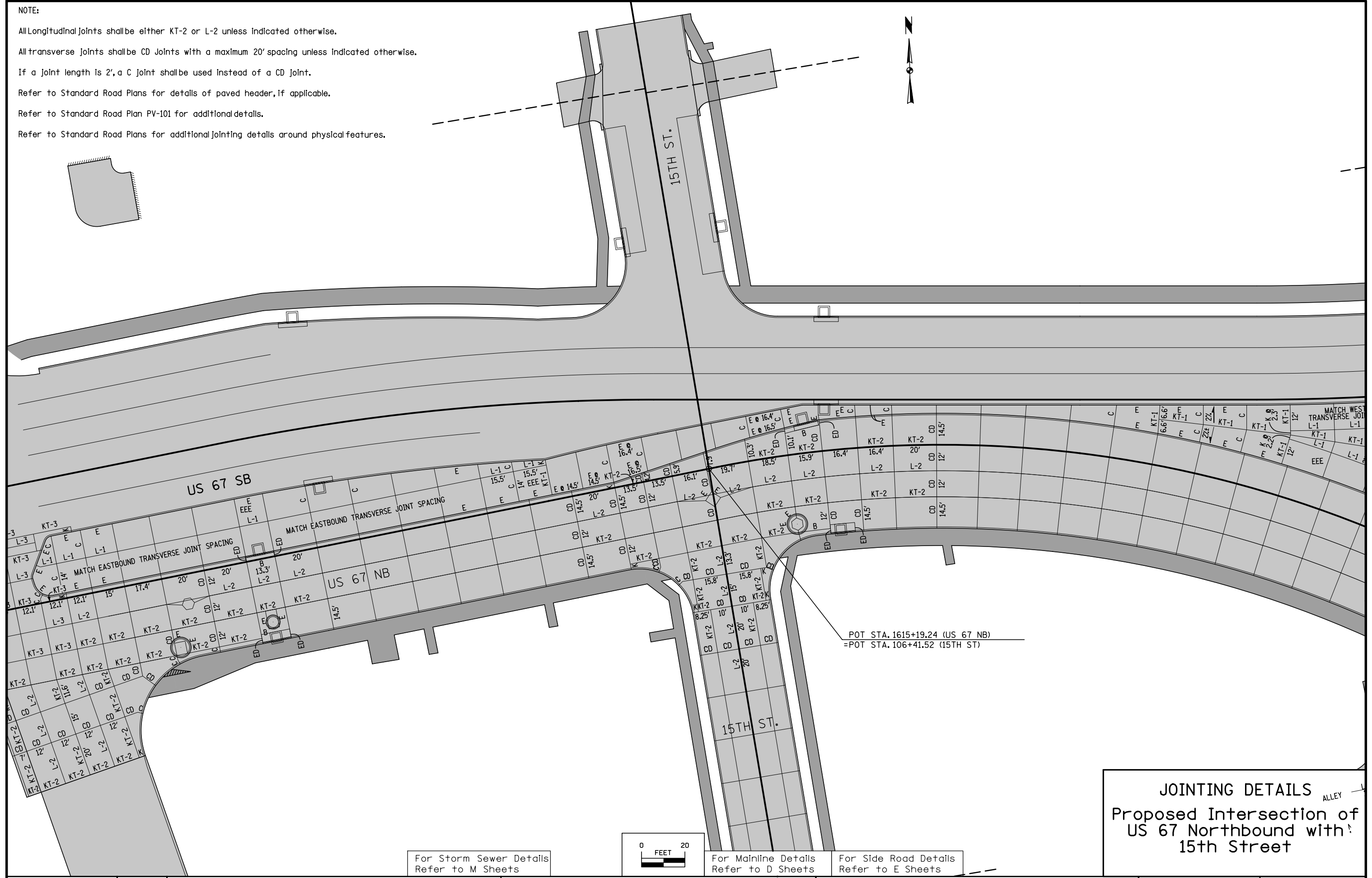
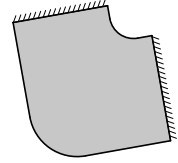
All transverse joints shall be CD Joints with a maximum 20' spacing unless indicated otherwise.

If a joint length is 2', a C joint shall be used instead of a CD joint.

Refer to Standard Road Plans for details of paved header, if applicable.

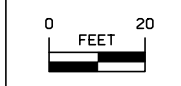
Refer to Standard Road Plan PV-101 for additional details.

Refer to Standard Road Plans for additional jointing details around physical features.



**JOINTING DETAILS**  
Proposed Intersection of  
US 67 Northbound with  
15th Street

For Storm Sewer Details  
Refer to M Sheets



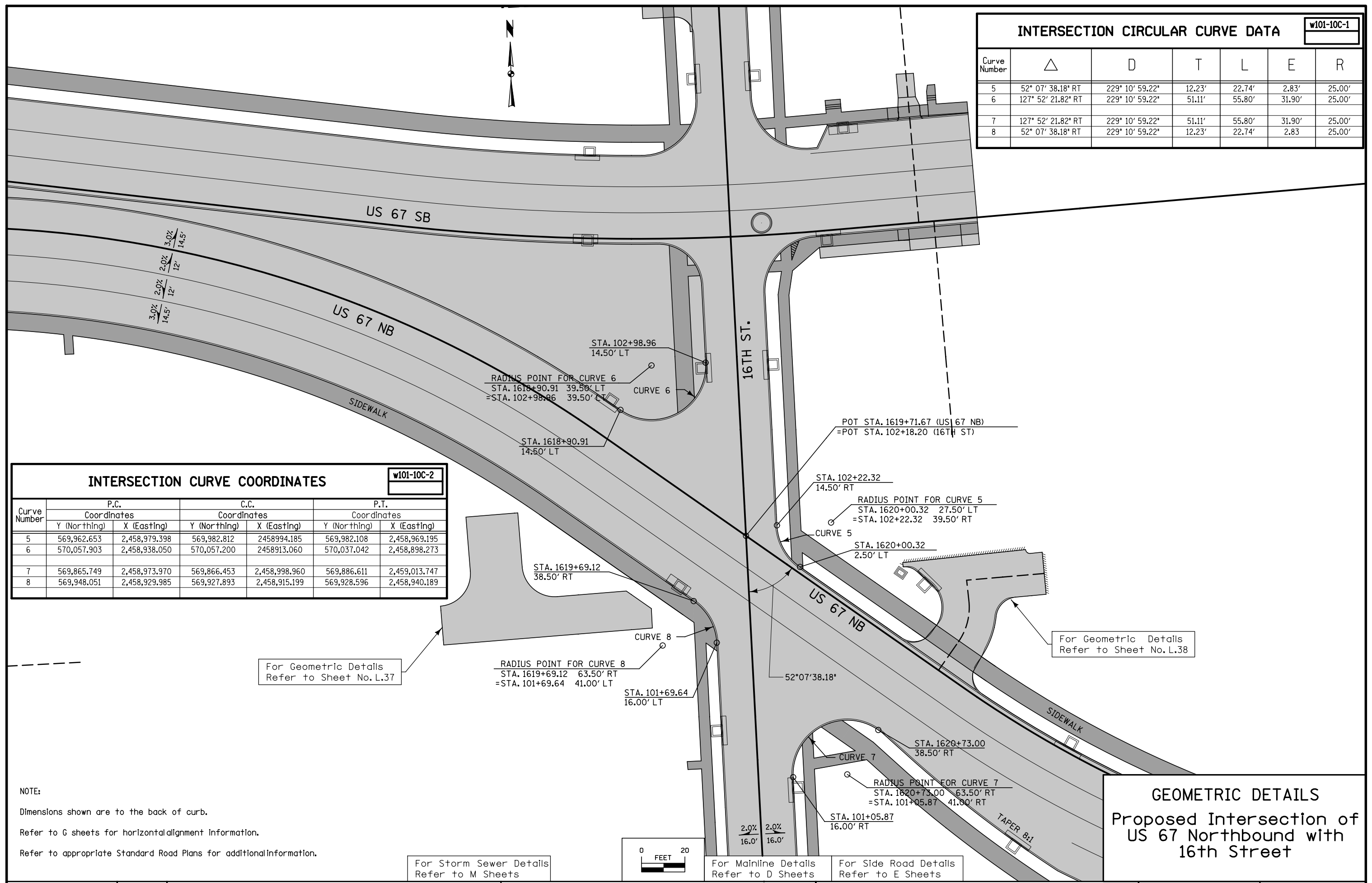
For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets

**INTERSECTION CIRCULAR CURVE DATA**

w101-10C-1

Curve Number	Δ	D	T	L	E	R
5	52° 07' 38.18" RT	229° 10' 59.22"	12.23'	22.74'	2.83'	25.00'
6	127° 52' 21.82" RT	229° 10' 59.22"	51.11'	55.80'	31.90'	25.00'
7	127° 52' 21.82" RT	229° 10' 59.22"	51.11'	55.80'	31.90'	25.00'
8	52° 07' 38.18" RT	229° 10' 59.22"	12.23'	22.74'	2.83'	25.00'



**INTERSECTION CURVE COORDINATES**

w101-10C-2

Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
5	569,962.653	2,458,979.398	569,982.812	2,458,994.185	569,982.108	2,458,969.195
6	570,057.903	2,458,938.050	570,057.200	2,458,913.060	570,037.042	2,458,898.273
7	569,865.749	2,458,973.970	569,866.453	2,458,998.960	569,886.611	2,459,013.747
8	569,948.051	2,458,929.985	569,927.893	2,458,915.199	569,928.596	2,458,940.189

For Geometric Details Refer to Sheet No. L.37

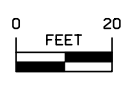
RADIUS POINT FOR CURVE 8  
STA. 1619+69.12 63.50' RT  
=STA. 101+69.64 41.00' LT

STA. 101+69.64  
16.00' LT

For Geometric Details Refer to Sheet No. L.38

NOTE:  
Dimensions shown are to the back of curb.  
Refer to G sheets for horizontal alignment information.  
Refer to appropriate Standard Road Plans for additional information.

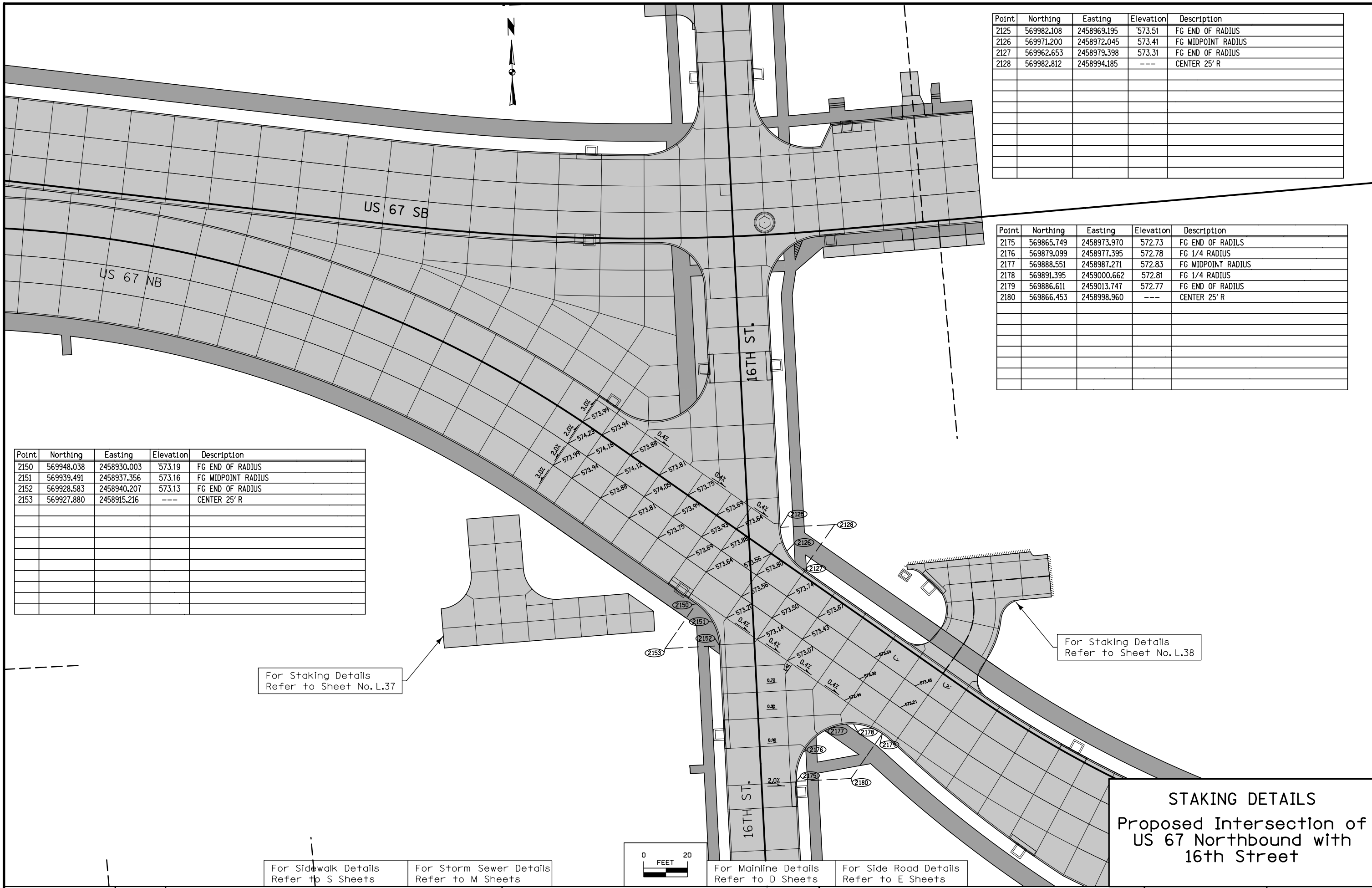
For Storm Sewer Details Refer to M Sheets



For Mainline Details Refer to D Sheets

For Side Road Details Refer to E Sheets

**GEOMETRIC DETAILS**  
Proposed Intersection of US 67 Northbound with 16th Street



Point	Northing	Easting	Elevation	Description
2125	569982.108	2458969.195	573.51	FG END OF RADIUS
2126	569971.200	2458972.045	573.41	FG MIDPOINT RADIUS
2127	569962.653	2458979.398	573.31	FG END OF RADIUS
2128	569982.812	2458994.185	---	CENTER 25' R

Point	Northing	Easting	Elevation	Description
2175	569865.749	2458973.970	572.73	FG END OF RADIUS
2176	569879.099	2458977.395	572.78	FG 1/4 RADIUS
2177	569888.551	2458987.271	572.83	FG MIDPOINT RADIUS
2178	569891.395	2459000.662	572.81	FG 1/4 RADIUS
2179	569886.611	2459013.747	572.77	FG END OF RADIUS
2180	569866.453	2458998.960	---	CENTER 25' R

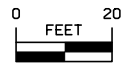
Point	Northing	Easting	Elevation	Description
2150	569948.038	2458930.003	573.19	FG END OF RADIUS
2151	569939.491	2458937.356	573.16	FG MIDPOINT RADIUS
2152	569928.583	2458940.207	573.13	FG END OF RADIUS
2153	569927.880	2458915.216	---	CENTER 25' R

For Staking Details  
Refer to Sheet No. L.37

For Staking Details  
Refer to Sheet No. L.38

For Sidewalk Details  
Refer to S Sheets

For Storm Sewer Details  
Refer to M Sheets

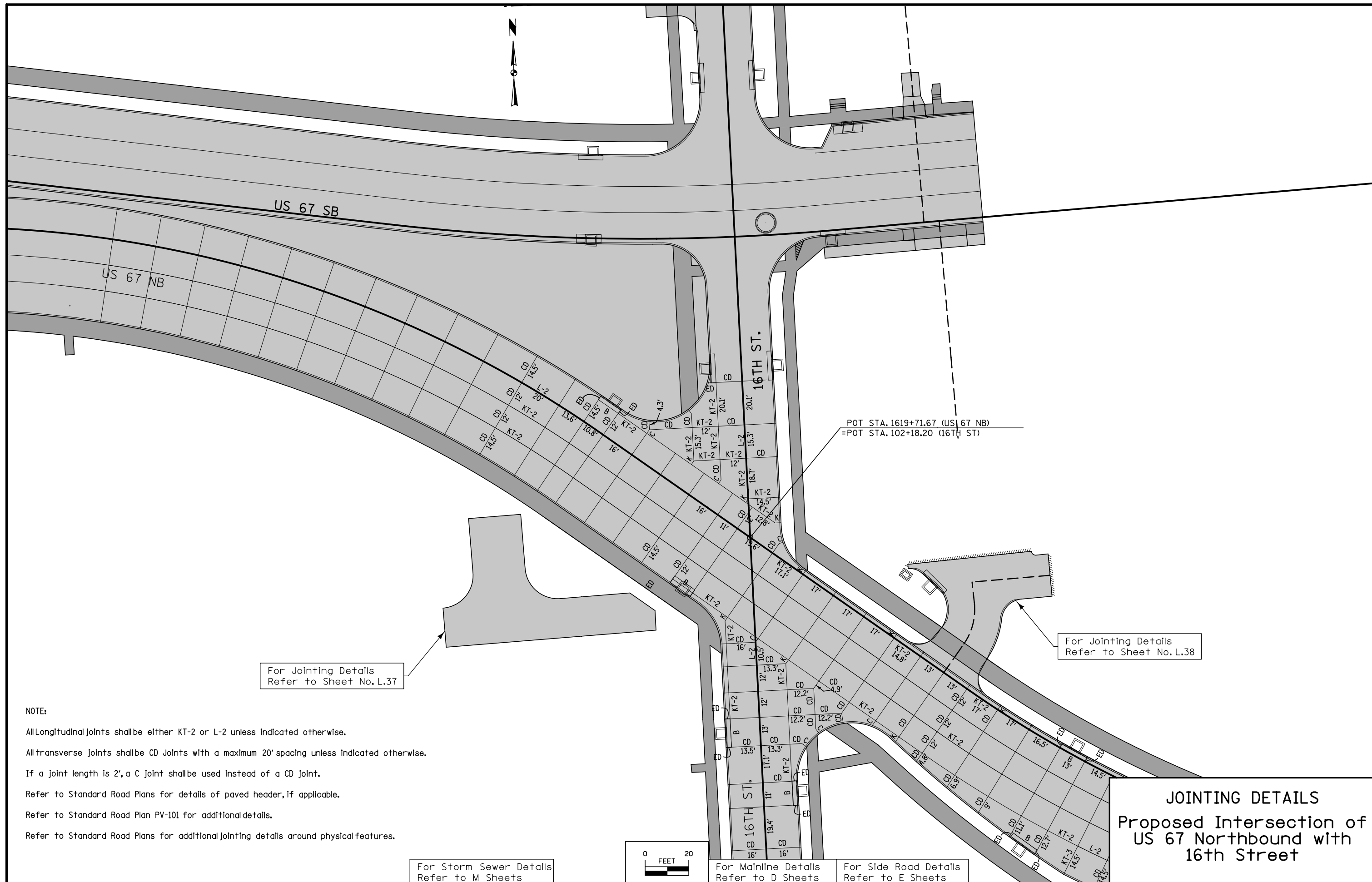


For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets

**STAKING DETAILS**  
Proposed Intersection of  
US 67 Northbound with  
16th Street





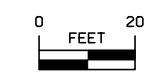
**NOTE:**

- All longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.
- All transverse joints shall be CD joints with a maximum 20' spacing unless indicated otherwise.
- If a joint length is 2', a C joint shall be used instead of a CD joint.
- Refer to Standard Road Plans for details of paved header, if applicable.
- Refer to Standard Road Plan PV-101 for additional details.
- Refer to Standard Road Plans for additional jointing details around physical features.

For Jointing Details  
Refer to Sheet No. L.37

For Jointing Details  
Refer to Sheet No. L.38

For Storm Sewer Details  
Refer to M Sheets



For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets

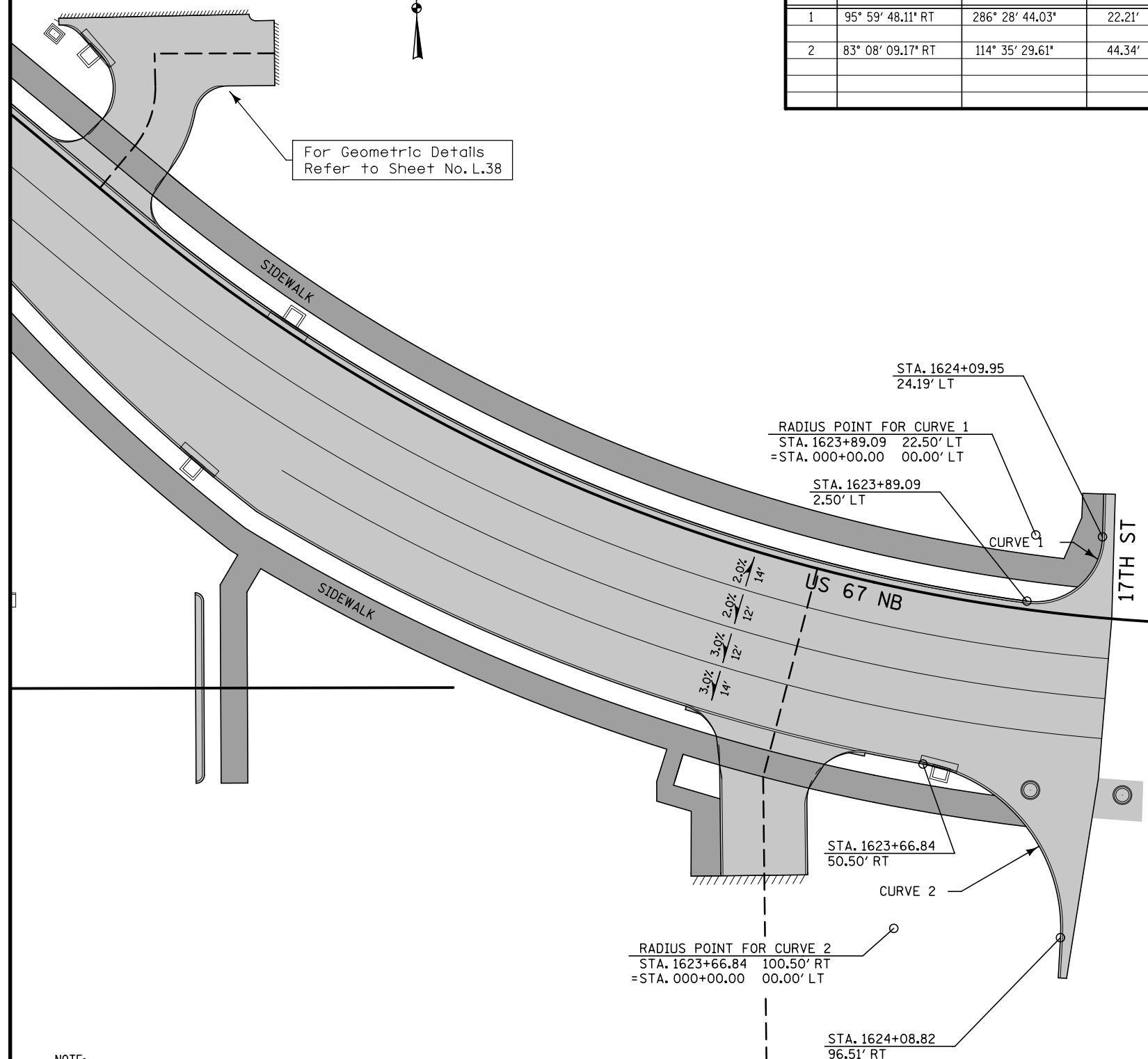
**JOINTING DETAILS**  
Proposed Intersection of  
US 67 Northbound with  
16th Street



INTERSECTION CIRCULAR CURVE DATA							w101-10C-1
Curve Number	$\Delta$	D	T	L	E	R	
1	95° 59' 48.11" RT	286° 28' 44.03"	22.21'	33.51'	9.89'	20.00'	
2	83° 08' 09.17" RT	114° 35' 29.61"	44.34'	72.55'	12.53'	50.00'	

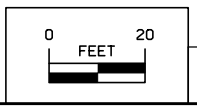
INTERSECTION CURVE COORDINATES						w101-10C-2
Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
1	569,827.607	2,459,348.314	569,826.956	2,459,328.325	569,807.008	2,459,326.884
2	569,756.417	2,459,298.789	569,706.745	2,459,293.078	569,707.011	2,459,343.077

For Geometric Details Refer to Sheet No. L.38



NOTE:  
 Dimensions shown are to the back of curb.  
 Refer to G sheets for horizontal alignment information.  
 Refer to appropriate Standard Road Plans for additional information.

For Storm Sewer Details Refer to M Sheets



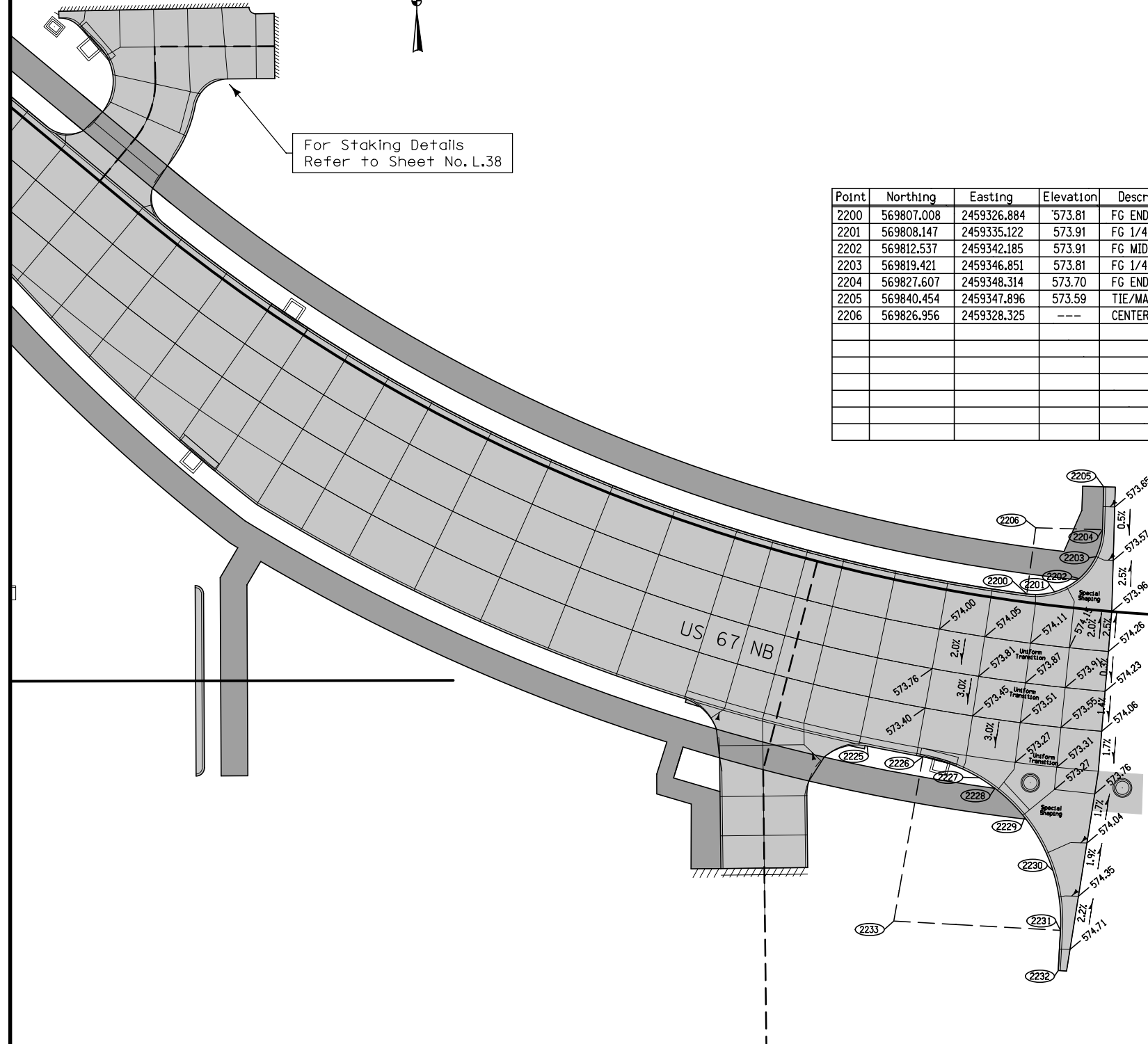
For Mainline Details Refer to D Sheets  
 For Side Road Details Refer to E Sheets

GEOMETRIC DETAILS  
 Proposed Intersection of  
 US 67 Northbound with  
 17th Street



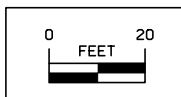
For Staking Details  
Refer to Sheet No. L.38

Point	Northing	Easting	Elevation	Description
2200	569807.008	2459326.884	573.81	FG END OF RADIUS
2201	569808.147	2459335.122	573.91	FG 1/4 RADIUS
2202	569812.537	2459342.185	573.91	FG MIDPOINT RADIUS
2203	569819.421	2459346.851	573.81	FG 1/4 RADIUS
2204	569827.607	2459348.314	573.70	FG END OF RADIUS
2205	569840.454	2459347.896	573.59	TIE/MATCH EXISTING
2206	569826.956	2459328.325	---	CENTER 50' R



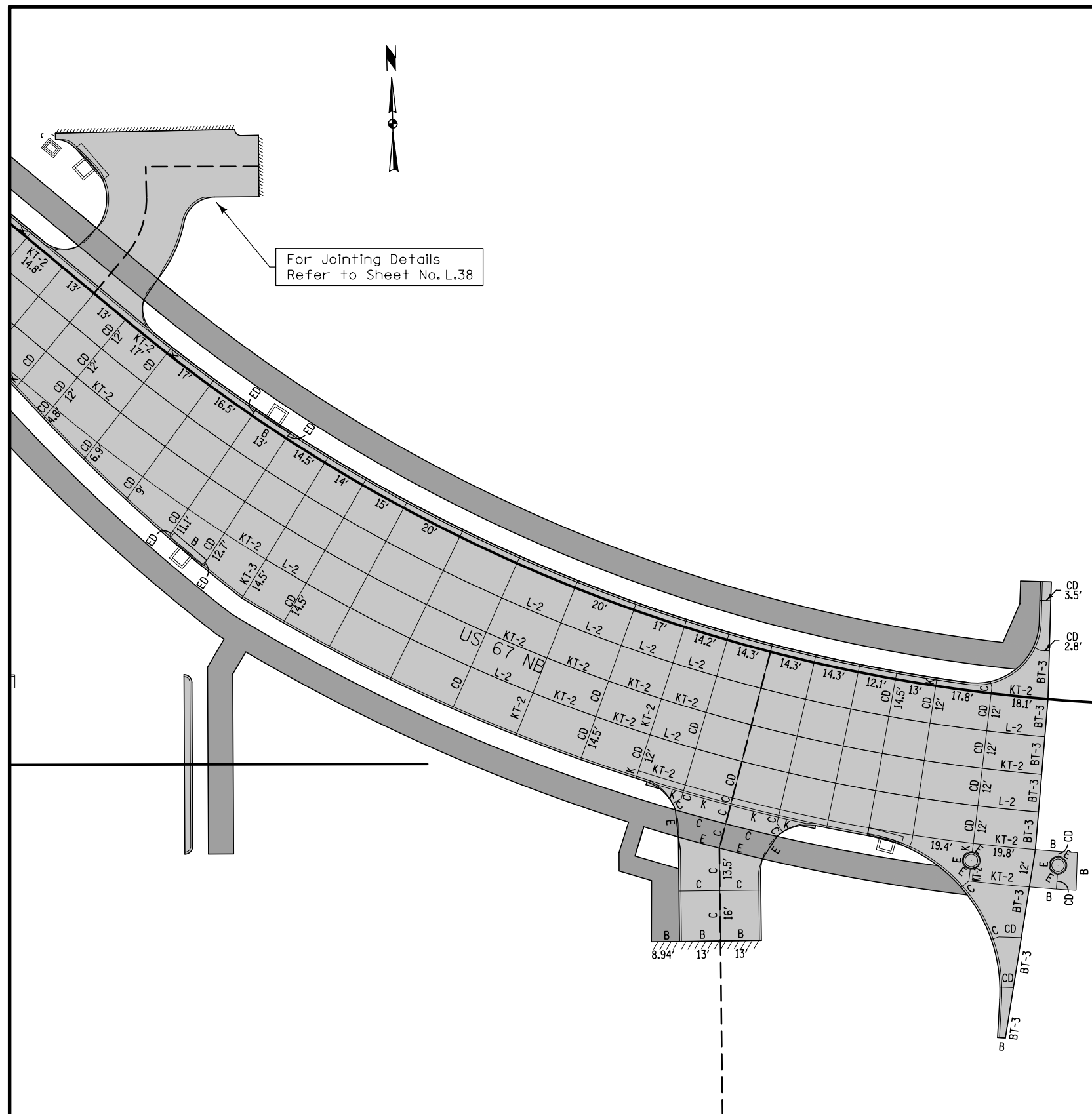
Point	Northing	Easting	Elevation	Description
2225	569758.612	2459281.908	572.91	FG END OF RADIUS
2226	569756.417	2459298.789	572.98	FG END OF RADIUS
2227	569751.158	2459316.044	573.49	FG 1/4 RADIUS
2228	569748.289	2459320.899	573.69	SIDEWALK
2229	569740.119	2459330.309	573.89	FG MIDPOINT RADIUS
2230	569724.736	2459339.729	574.33	FG 1/4 RADIUS
2231	569707.011	2459343.077	574.53	FG END OF RADIUS
2232	569694.950	2459343.142	574.73	TIE/MATCH EXISTING
2233	569706.745	2459293.078	---	CENTER 50' R

STAKING DETAILS  
Proposed Intersection of  
US 67 Northbound with  
17th Street



For Sidewalk Details Refer to S Sheets  
For Storm Sewer Details Refer to M Sheets

For Mainline Details Refer to D Sheets  
For Side Road Details Refer to E Sheets

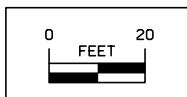


NOTE:  
 All Longitudinal Joints shall be either KT-2 or L-2 unless indicated otherwise.  
 All transverse joints shall be CD Joints with a maximum 20' spacing unless indicated otherwise.  
 If a joint length is 2', a C joint shall be used instead of a CD joint.  
 Refer to Standard Road Plans for details of paved header, if applicable.  
 Refer to Standard Road Plan PV-101 for additional details.  
 Refer to Standard Road Plans for additional jointing details around physical features.

For Jointing Details  
 Refer to Sheet No. L.38

**JOINTING DETAILS**  
 Proposed Intersection of  
 US 67 Northbound with  
 17th Street

For Storm Sewer Details  
 Refer to M Sheets



For Mainline Details  
 Refer to D Sheets

For Side Road Details  
 Refer to E Sheets

**INTERSECTION CURVE COORDINATES**

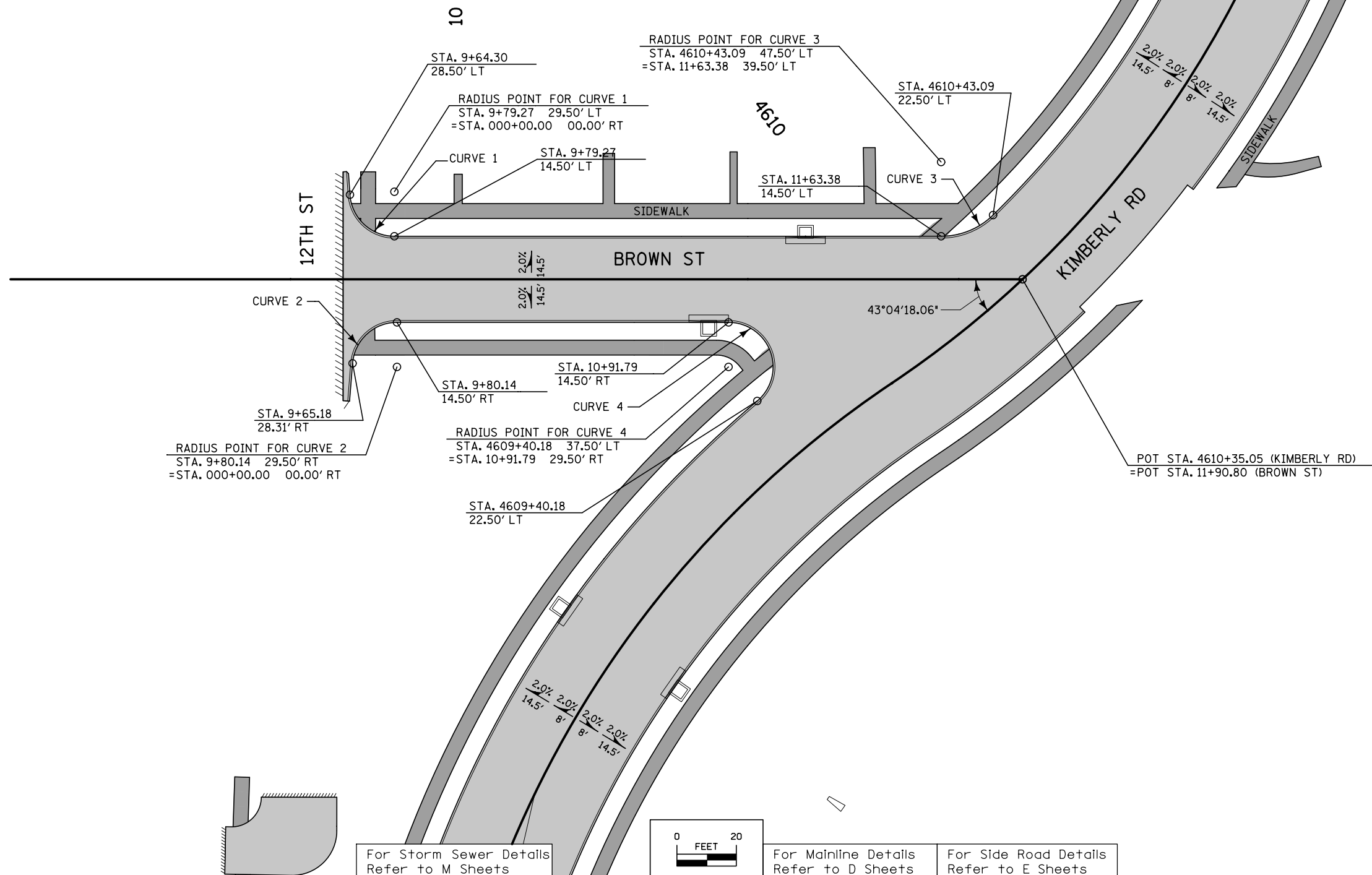
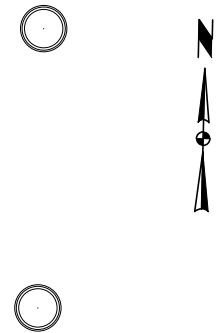
w101-10C-2

Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
1	570,464.213	2,457,313.760	570,479.209	5,457,313.412	570,477.857	2,457,298.473
2	570,421.090	2,457,300.674	570,420.245	2,457,315.651	570,435.241	2,457,315.303
3	570,476.021	2,457,515.140	570,493.476	2,457,497.243	570,468.483	2,457,497.823
4	570,437.830	2,457,426.931	570,422.835	2,457,427.279	570,411.594	2,457,437.211

**INTERSECTION CIRCULAR CURVE DATA**

w101-10C-1

Curve Number	Δ	D	T	L	E	R
1	86° 09' 38.70" RT	381' 58" 18.70"	14.03'	22.56'	5.54'	15.00'
2	85° 26' 29.30" RT	381' 58" 18.70"	13.85'	22.37'	5.42'	15.00'
3	44° 23' 14.20" RT	229' 10' 59.22"	10.20'	19.368'	2.00'	25.00'
4	139° 51' 52.10" RT	381' 58" 18.70"	41.06'	36.62'	28.72'	15.00'



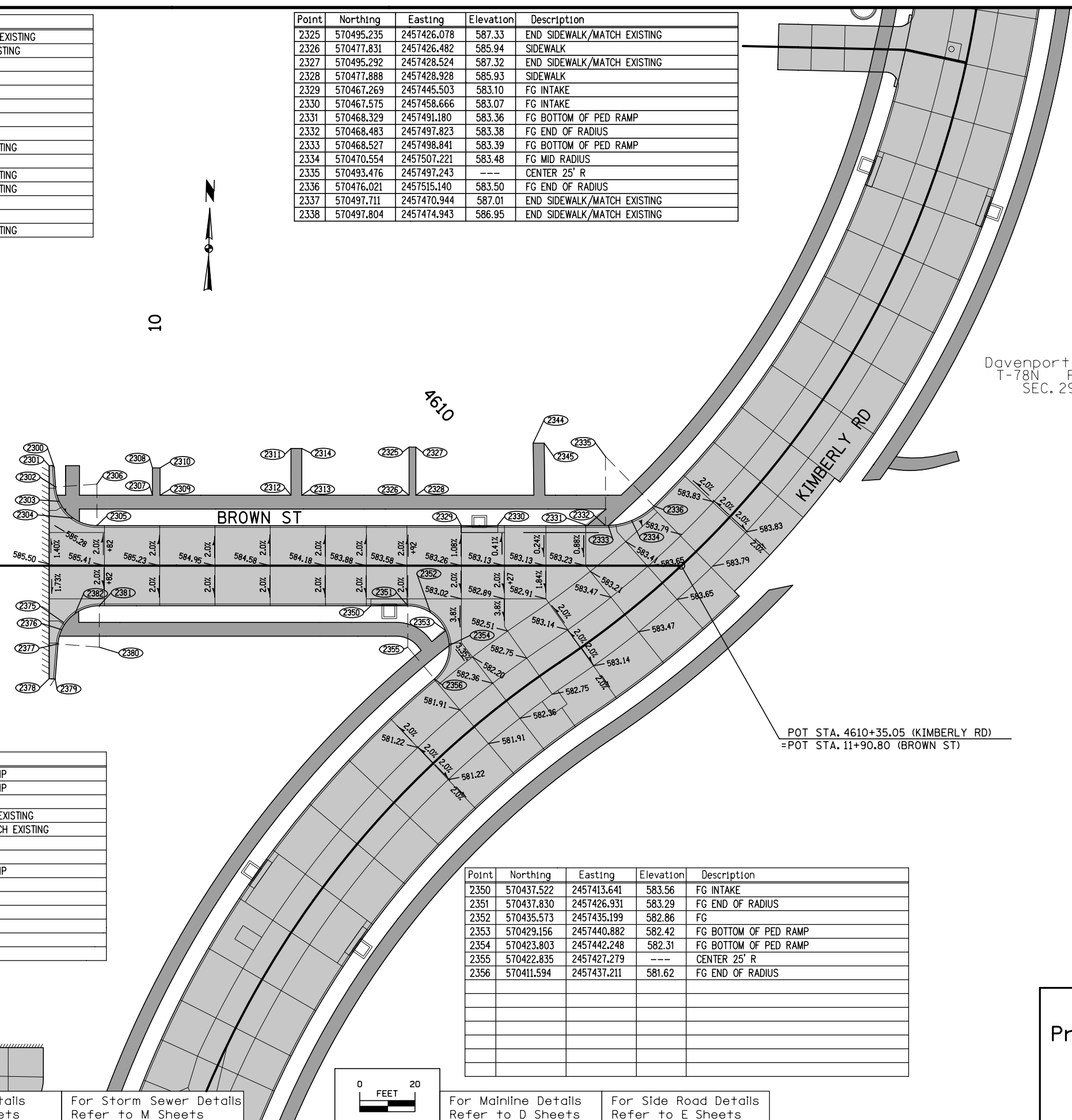
**GEOMETRIC DETAILS**  
Proposed Intersection of  
Brown Street with  
Kimberly Road and  
12th Street

Point	Northing	Easting	Elevation	Description
2300	570484.936	2457297.833	585.11	FG END PAVEMENT/MATCH EXISTING
2301	570485.502	2457295.996	585.15	END PAVEMENT/MATCH EXISTING
2302	570477.857	2457298.473	585.18	FG END OF RADIUS
2303	570472.656	2457299.919	585.18	FG BOTTOM OF PED RAMP
2304	570466.706	2457305.124	585.16	FG BOTTOM OF PED RAMP
2305	570464.213	2457313.760	585.14	FG END OF RADIUS
2306	570479.209	2457313.412	---	CENTER 15' R
2307	570475.676	2457333.603	585.83	SIDEWALK
2308	570485.563	2457333.374	586.62	END SIDEWALK/MATCH EXISTING
2309	570475.740	2457336.353	585.85	SIDEWALK
2310	570485.627	2457336.123	586.64	END SIDEWALK/MATCH EXISTING
2311	570493.755	2457383.354	587.11	END SIDEWALK/MATCH EXISTING
2312	570476.839	2457383.747	585.76	SIDEWALK
2313	570486.930	2457387.644	585.77	SIDEWALK
2314	570493.846	2457387.252	587.12	END SIDEWALK/MATCH EXISTING

Point	Northing	Easting	Elevation	Description
2325	570495.235	2457426.078	587.33	END SIDEWALK/MATCH EXISTING
2326	570477.831	2457426.482	585.94	SIDEWALK
2327	570495.292	2457428.524	587.32	END SIDEWALK/MATCH EXISTING
2328	570477.888	2457428.928	585.93	SIDEWALK
2329	570467.269	2457445.503	583.10	FG INTAKE
2330	570467.575	2457458.666	583.07	FG INTAKE
2331	570468.329	2457491.180	583.36	FG BOTTOM OF PED RAMP
2332	570468.483	2457497.823	583.38	FG END OF RADIUS
2333	570468.527	2457498.841	583.39	FG BOTTOM OF PED RAMP
2334	570470.554	2457507.221	583.48	FG MID RADIUS
2335	570493.476	2457497.243	---	CENTER 25' R
2336	570476.021	2457515.140	583.50	FG END OF RADIUS
2337	570497.711	2457470.944	587.01	END SIDEWALK/MATCH EXISTING
2338	570497.804	2457474.943	586.95	END SIDEWALK/MATCH EXISTING

Point	Northing	Easting	Elevation	Description
2375	570428.859	2457302.546	584.98	FG BOTTOM OF PED RAMP
2376	570426.260	2457301.910	584.92	FG BOTTOM OF PED RAMP
2377	570421.090	2457300.674	584.85	FG END OF RADIUS
2378	570408.453	2457297.784	584.89	END PAVEMENT/MATCH EXISTING
2379	570408.330	2457299.955	584.84	FG END PAVEMENT/MATCH EXISTING
2380	570420.245	2457315.651	---	CENTER 15' R
2381	570435.240	2457315.287	585.13	FG END OF RADIUS
2382	570432.185	2457306.571	585.06	FG BOTTOM OF PED RAMP

Point	Northing	Easting	Elevation	Description
2350	570437.522	2457413.641	583.56	FG INTAKE
2351	570437.830	2457426.931	583.29	FG END OF RADIUS
2352	570435.573	2457435.199	582.86	FG
2353	570429.156	2457440.882	582.42	FG BOTTOM OF PED RAMP
2354	570423.803	2457442.248	582.31	FG BOTTOM OF PED RAMP
2355	570422.835	2457427.279	---	CENTER 25' R
2356	570411.594	2457437.211	581.62	FG END OF RADIUS



Davenport TWP.  
T-78N R-4E  
SEC. 29

For Sidewalk Details  
Refer to S Sheets

For Storm Sewer Details  
Refer to M Sheets

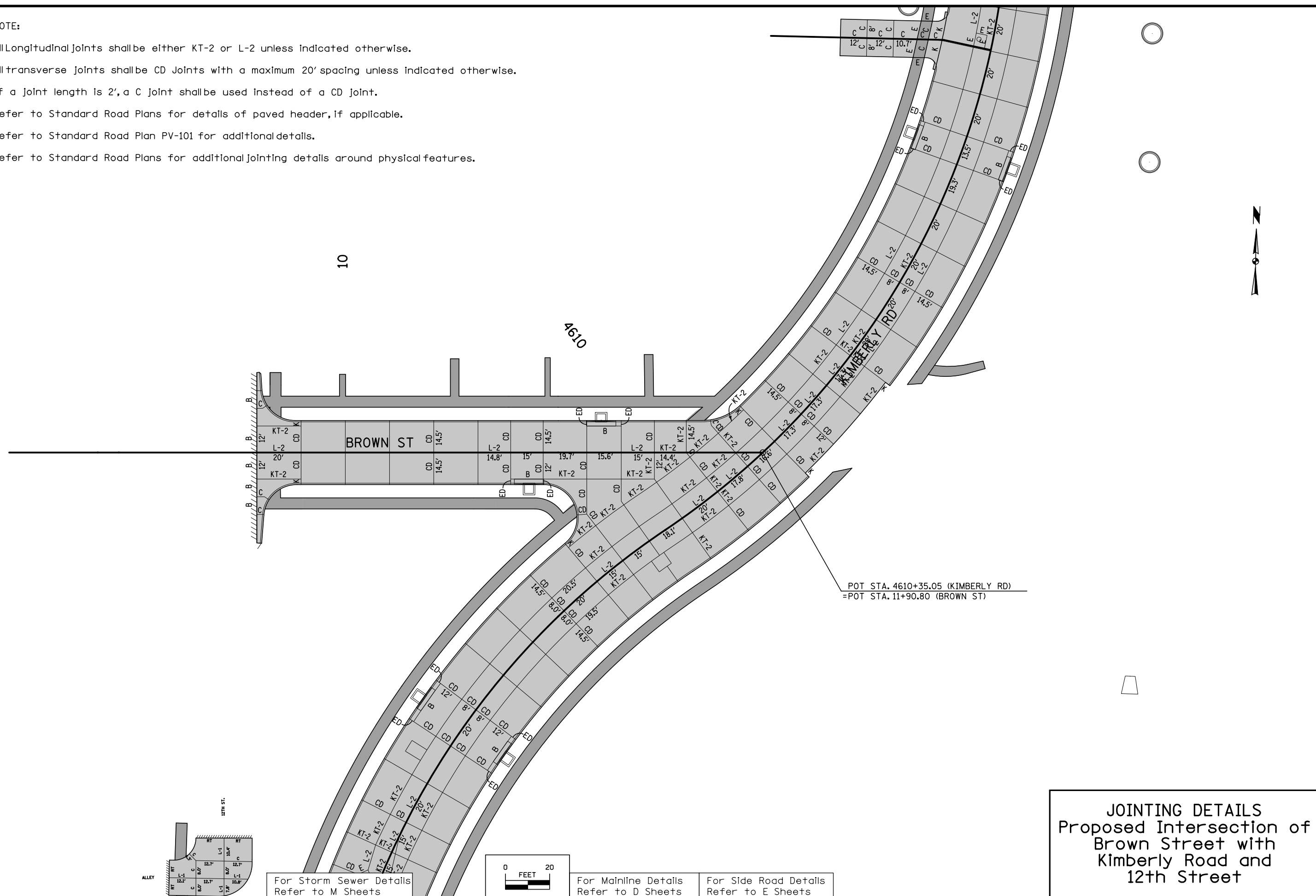
For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets

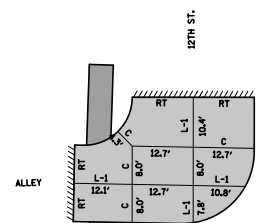
**STAKING DETAILS**  
Proposed Intersection of  
Brown Street with  
Kimberly Road and  
12th Street

**NOTE:**

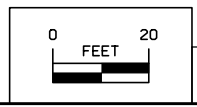
- All Longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.
- All transverse joints shall be CD joints with a maximum 20' spacing unless indicated otherwise.
- If a joint length is 2', a C joint shall be used instead of a CD joint.
- Refer to Standard Road Plans for details of paved header, if applicable.
- Refer to Standard Road Plan PV-101 for additional details.
- Refer to Standard Road Plans for additional jointing details around physical features.



POT STA. 4610+35.05 (KIMBERLY RD)  
 =POT STA. 11+90.80 (BROWN ST)



For Storm Sewer Details  
 Refer to M Sheets



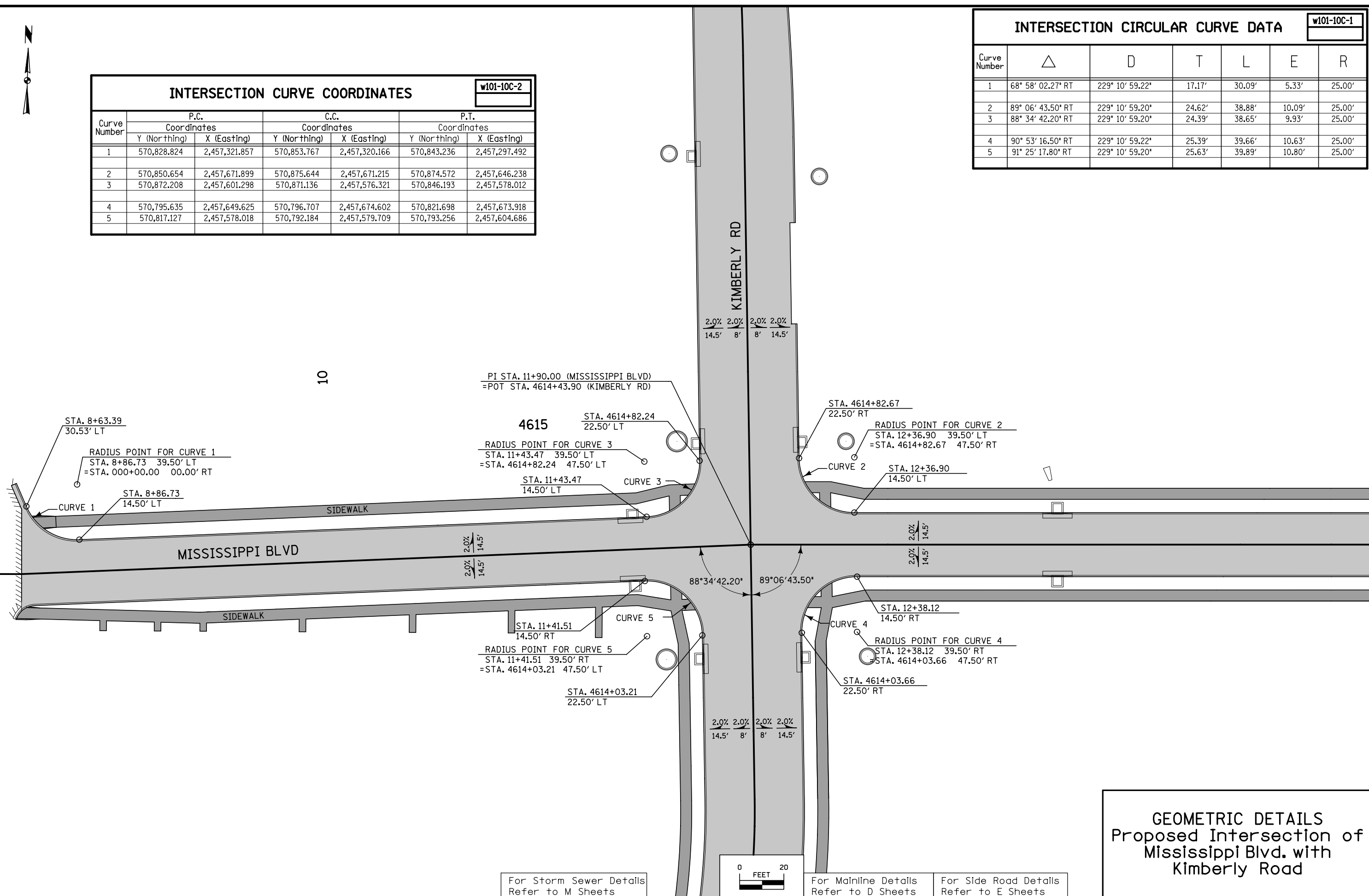
For Mainline Details  
 Refer to D Sheets

For Side Road Details  
 Refer to E Sheets

**JOINTING DETAILS**  
 Proposed Intersection of  
 Brown Street with  
 Kimberly Road and  
 12th Street

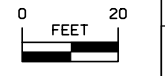
INTERSECTION CIRCULAR CURVE DATA						
Curve Number	△	D	T	L	E	R
1	68° 58' 02.27" RT	229° 10' 59.22"	17.17'	30.09'	5.33'	25.00'
2	89° 06' 43.50" RT	229° 10' 59.20"	24.62'	38.88'	10.09'	25.00'
3	88° 34' 42.20" RT	229° 10' 59.20"	24.39'	38.65'	9.93'	25.00'
4	90° 53' 16.50" RT	229° 10' 59.22"	25.39'	39.66'	10.63'	25.00'
5	91° 25' 17.80" RT	229° 10' 59.20"	25.63'	39.89'	10.80'	25.00'

INTERSECTION CURVE COORDINATES						
Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
	1	570,828.824	2,457,321.857	570,853.767	2,457,320.166	570,843.236
2	570,850.654	2,457,671.899	570,875.644	2,457,671.215	570,874.572	2,457,646.238
3	570,872.208	2,457,601.298	570,871.136	2,457,576.321	570,846.193	2,457,578.012
4	570,795.635	2,457,649.625	570,796.707	2,457,674.602	570,821.698	2,457,673.918
5	570,817.127	2,457,578.018	570,792.184	2,457,579.709	570,793.256	2,457,604.686



GEOMETRIC DETAILS  
Proposed Intersection of  
Mississippi Blvd. with  
Kimberly Road

For Storm Sewer Details  
Refer to M Sheets



For Mainline Details  
Refer to D Sheets

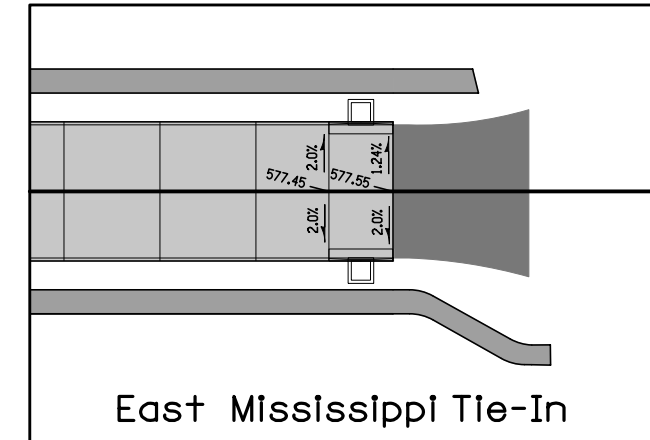
For Side Road Details  
Refer to E Sheets





Point	Northing	Easting	Elevation	Description
2425	570850.654	2457671.899	577.91	FG END OF RADIUS
2426	270856.206	2457655.493	578.46	FG BOTTOM OF PED RAMP
2427	570857.574	2457653.938	578.46	FG MIDPOINT RADIUS
2428	570861.082	2457650.894	578.47	FG BOTTOM OF PED RAMP
2429	570874.572	2457646.238	578.95	FG END OF RADIUS
2430	570877.399	2457646.116	579.00	FG INTAKE
2431	570888.389	2457645.645	579.22	FG INTAKE
2432	570874.572	2457671.244	---	CENTER 25' R

Point	Northing	Easting	Elevation	Description
2400	570886.459	2457600.686	579.22	FG INTAKE
2401	570875.470	2457601.158	579.00	FG INTAKE
2402	570872.208	2457601.298	578.94	FG END OF RADIUS
2403	570858.569	2457597.933	578.65	FG BOTTOM OF PED RAMP
2404	570854.462	2457594.948	578.64	FG MIDPOINT RADIUS
2405	570853.279	2457593.817	578.64	FG BOTTOM OF PED RAMP
2406	570846.193	2457578.012	578.30	FG END OF RADIUS
2407	570846.061	2457576.056	578.28	FG INTAKE
2408	570845.324	2457565.191	578.04	FG INTAKE
2409	570871.136	2457576.321	---	CENTER 25' R



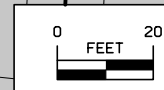
Point	Northing	Easting	Elevation	Description
2450	570816.390	2457567.153	578.04	FG INTAKE
2451	570817.135	2457578.128	578.03	FG END OF RADIUS
2452	570812.938	2457593.649	578.08	FG BOTTOM OF PED RAMP
2453	570810.812	2457596.383	578.09	FG MIDPOINT RADIUS
2454	570807.498	2457599.470	578.11	FG BOTTOM OF PED RAMP
2455	570793.256	2457604.686	578.16	FG END OF RADIUS
2456	570789.119	2457604.864	578.15	FG INTAKE
2457	570776.131	2457605.421	578.15	FG INTAKE
2458	570792.184	2457579.709	---	CENTER 25' R

Point	Northing	Easting	Elevation	Description
2475	570778.061	2457650.380	578.15	FG INTAKE
2476	570791.049	2457649.822	578.15	FG INTAKE
2477	570795.635	2457649.625	578.16	FG END OF RADIUS
2478	570810.273	2457653.603	577.94	FG BOTTOM OF PED RAMP
2479	570813.753	2457656.315	577.88	FG MIDPOINT RADIUS
2480	570816.190	2457658.936	577.81	FG BOTTOM OF PED RAMP
2481	570821.698	2457673.918	577.60	FG END OF RADIUS
2482	570796.707	2457674.602	---	CENTER 25' R

STAKING DETAILS  
Proposed Intersection of  
Mississippi Blvd. with  
Kimberly Road

For Sidewalk Details  
Refer to S Sheets

For Storm Sewer Details  
Refer to M Sheets

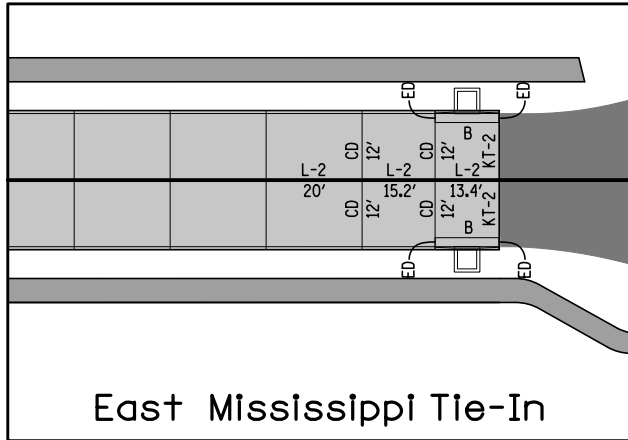
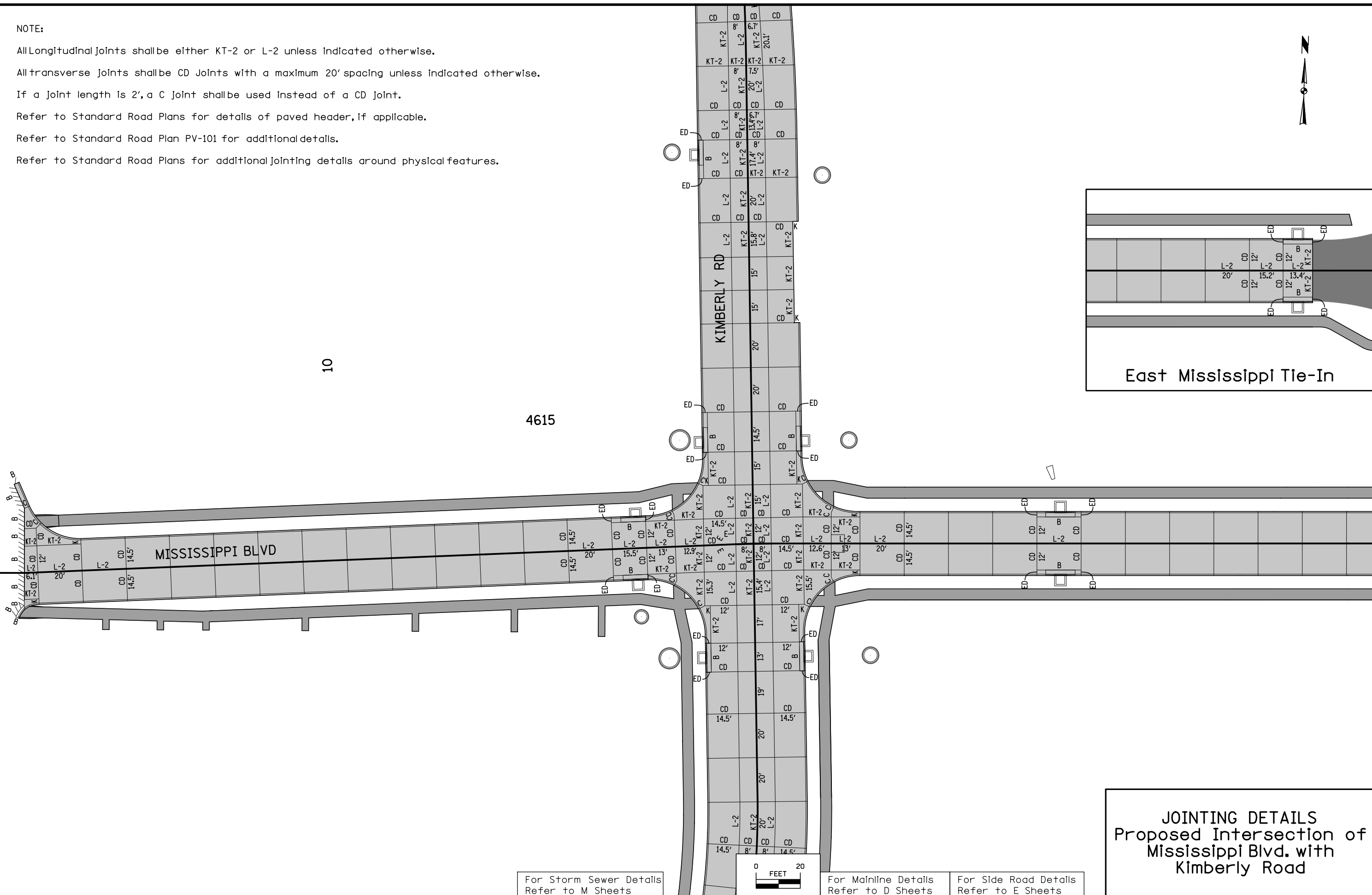


For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets

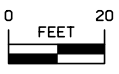
**NOTE:**

- All Longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.
- All transverse joints shall be CD joints with a maximum 20' spacing unless indicated otherwise.
- If a joint length is 2', a C joint shall be used instead of a CD joint.
- Refer to Standard Road Plans for details of paved header, if applicable.
- Refer to Standard Road Plan PV-101 for additional details.
- Refer to Standard Road Plans for additional jointing details around physical features.



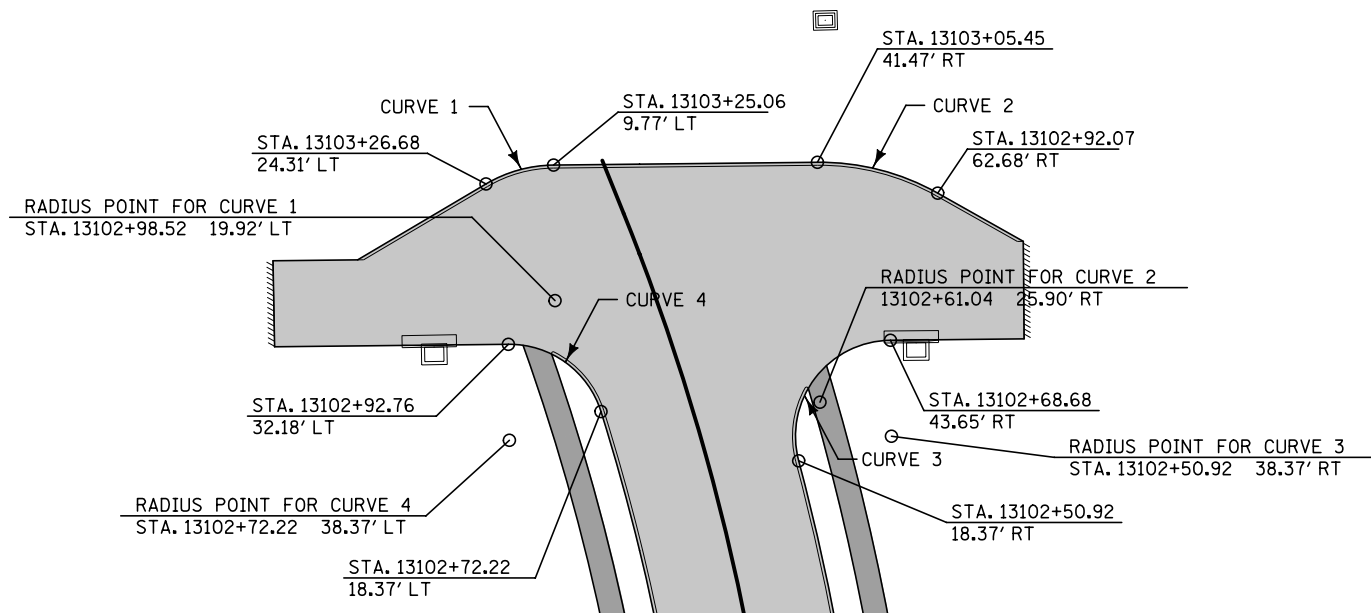
**JOINTING DETAILS**  
 Proposed Intersection of  
 Mississippi Blvd. with  
 Kimberly Road

For Storm Sewer Details  
 Refer to M Sheets



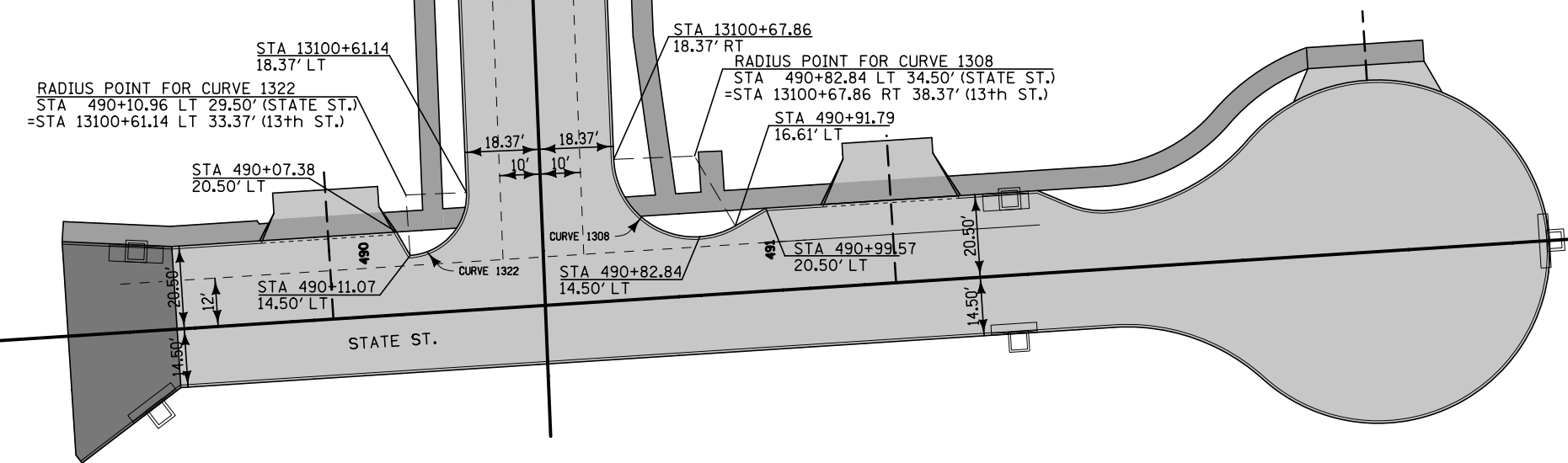
For Mainline Details  
 Refer to D Sheets

For Side Road Details  
 Refer to E Sheets



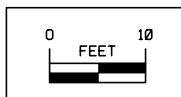
INTERSECTION CURVE COORDINATES						w101-10C-2
Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
1	569,960.755	2,457,592.667	569,932.507	2,457,592.965	569,960.755	2,457,578.583
2	569,954.902	2,457,672.723	569,911.339	2,457,648.182	569,961.336	2,457,647.653
3	569,899.122	2,457,643.747	569,904.246	2,457,663.080	569,924.245	2,457,662.868
4	569,923.404	2,457,583.249	569,903.405	2,457,583.460	569,909.387	2,457,602.545

INTERSECTION CIRCULAR CURVE DATA							w101-10C-1
Curve Number	Δ	D	T	L	E	R	
1	29° 59' 59.52" LT	202° 59' 00.91"	7.57'	14.79'	1.00'	28.25'	
2	30° 00' 00.00" LT	114° 35' 26.61"	14.00'	26.18'	1.76'	50.00'	
3	104° 14' 27.00" RT	286° 28' 44.03"	25.71'	36.39'	12.57'	20.00'	
4	73° 12' 08.68" RT	286° 28' 44.03"	14.85'	25.55'	4.91'	20.00'	
5	91° 57' 10.11" RT	286° 28' 44.03"	20.69'	32.10'	8.78"	20.00'	
6	91° 03' 50.98" RT	381° 58' 18.71"	15.28'	23.84'	6.41'	15.00'	
1322	88° 12' 12.36" RT	381° 58' 18.71"	14.54'	23.09'	5.89'	15.00'	
1308	117° 56' 27.20" RT	286° 28' 44.03"	33.25'	41.17'	18.80'	20.00'	



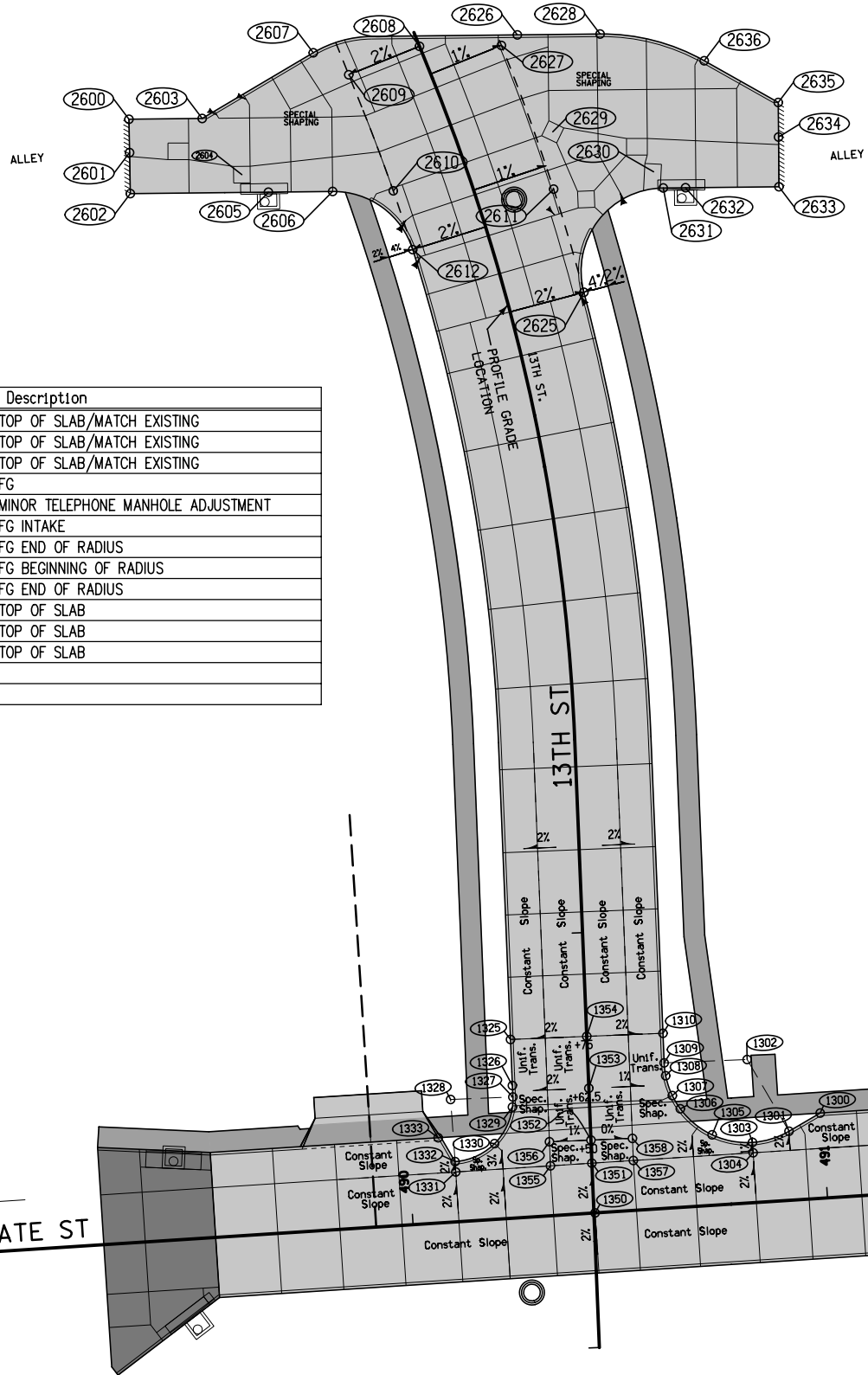
GEOMETRIC DETAILS  
Proposed Intersection of  
13th Street and  
State Street

For Storm Sewer Details  
Refer to M Sheets



For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets



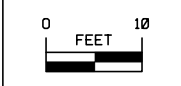
Point	Northing	Easting	Elevation	Description
2625	569899.122	2457643.747	571.65	FG BEGINNING OF RADIUS
2626	569961.126	2457627.725	571.83	FG
2627	569958.574	2457623.886	571.81	TOP OF SLAB
2628	569961.336	2457647.653	571.62	FG BEGINNING OF RADIUS
2629	569937.775	2457636.255	571.54	MINOR SANITARY SEWER MANHOLE ADJUSTMENT
2630	569928.149	2457660.541	571.23	MINOR TELEPHONE MANHOLE ADJUSTMENT
2631	569924.245	2457662.868	571.16	FG END OF RADIUS
2632	569924.302	2457668.225	571.12	FG INTAKE
2633	569924.539	2457690.723	571.46	TOP OF SLAB/MATCH EXISTING
2634	569936.512	2457690.625	571.35	TOP OF SLAB/MATCH EXISTING
2635	569944.856	2457690.557	571.54	FG/MATCH EXISTING
2636	569954.902	2457672.723	571.34	FG MATCH EXISTING

Point	Northing	Easting	Elevation	Description
2600	569940.813	2457534.199	570.88	TOP OF SLAB/MATCH EXISTING
2601	569932.772	2457534.347	570.29	TOP OF SLAB/MATCH EXISTING
2602	569922.889	2457534.530	570.85	TOP OF SLAB/MATCH EXISTING
2603	569941.000	2457551.835	570.79	FG
2604	569927.459	2457561.392	570.76	MINOR TELEPHONE MANHOLE ADJUSTMENT
2605	569923.240	2457597.778	570.69	FG INTAKE
2606	569923.415	2457583.249	571.04	FG END OF RADIUS
2607	569956.821	2457578.583	571.51	FG BEGINNING OF RADIUS
2608	569958.377	2457604.163	572.06	FG END OF RADIUS
2609	569951.506	2457587.126	571.69	TOP OF SLAB
2610	569923.558	2457597.849	571.36	TOP OF SLAB
2611	569923.966	2457636.462	571.47	TOP OF SLAB

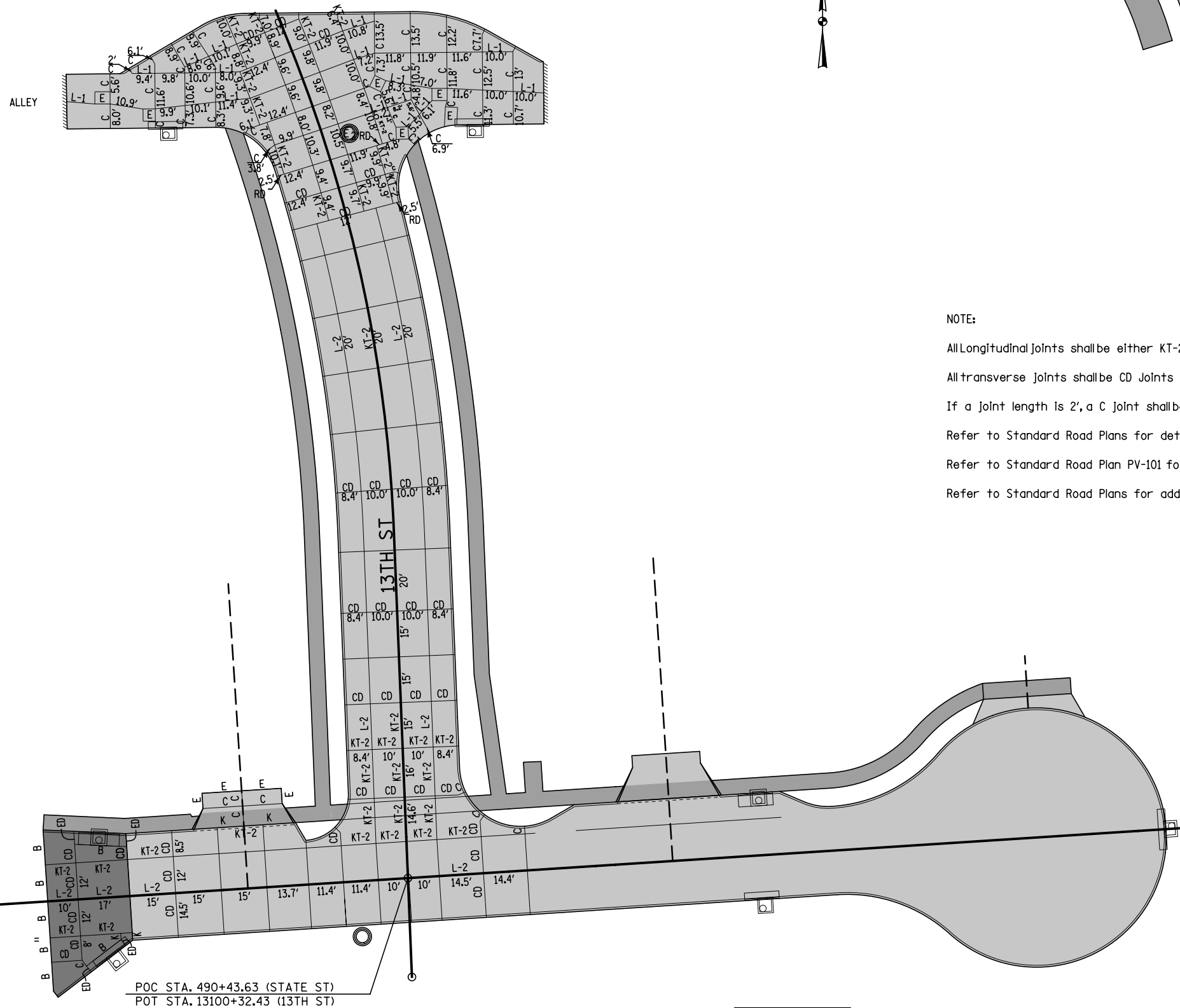
Point	Northing	Easting	Elevation	Description
1300	569701.463	2457700.654	572.61	FG
1302	569697.081	2457693.144	572.70	FG - END OF RADIUS
1303	569714.355	2457683.065	- - -	CENTER OF CURVE
1304	569694.397	2457684.354	572.76	FG - P.O.C.
1305	569691.901	2457684.502	572.81	TOP OF PAVEMENT
1306	569696.222	2457674.628	572.68	FG
1307	569702.486	2457666.967	572.58	FG
1308	569705.673	2457665.047	572.53	FG - AT SIDEWALK RAMP
1309	569710.454	2457663.449	572.43	FG - AT SIDEWALK RAMP
1310	569713.545	2457663.081	572.37	FG - END OF RADIUS
1311	569720.675	2457662.792	572.21	FG AT INTAKE
1325	569719.186	2457626.082	572.21	FG
1326	569708.087	2457626.532	572.28	FG - AT SIDEWALK RAMP
1327	569705.337	2457626.644	572.31	FG - END OF RADIUS
1328	569704.730	2457611.656	- - -	CENTER OF CURVE
1329	569702.910	2457626.545	572.33	FG - AT SIDEWALK RAMP
1330	569694.109	2457622.249	572.46	FG
1331	569687.273	2457612.894	572.59	TOP OF PAVEMENT
1332	569689.768	2457612.733	572.54	FG - END OF RADIUS
1333	569695.517	2457608.655	572.39	FG - AT DRIVEWAY
1350	569677.417	2457646.449	573.00	TOP OF PAVEMENT
1352	569689.392	2457645.675	572.76	TOP OF PAVEMENT
1352	569694.951	2457645.450	572.73	TOP OF PAVEMENT
1353	569707.441	2457644.943	572.68	TOP OF PAVEMENT
1354	569719.931	2457644.437	572.62	TOP OF PAVEMENT
1355	569688.747	2457635.693	572.72	TOP OF PAVEMENT
1356	569694.546	2457635.458	572.61	TOP OF PAVEMENT
1357	569690.037	2457655.657	572.79	TOP OF PAVEMENT
1358	569695.356	2457655.442	572.73	TOP OF PAVEMENT

**STAKING DETAILS**  
**Proposed Intersection of**  
**13th Street and**  
**State Street**

For Sidewalk Details Refer to S Sheets      For Storm Sewer Details Refer to M Sheets



For Mainline Details Refer to D Sheets      For Side Road Details Refer to E Sheets

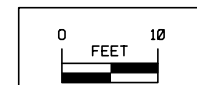


**NOTE:**

- All Longitudinal Joints shall be either KT-2 or L-2 unless indicated otherwise.
- All transverse joints shall be CD Joints with a maximum 20' spacing unless indicated otherwise.
- If a joint length is 2', a C Joint shall be used instead of a CD joint.
- Refer to Standard Road Plans for details of paved header, if applicable.
- Refer to Standard Road Plan PV-101 for additional details.
- Refer to Standard Road Plans for additional jointing details around physical features.

**JOINTING DETAILS**  
Proposed Intersection of  
13th Street and  
State Street

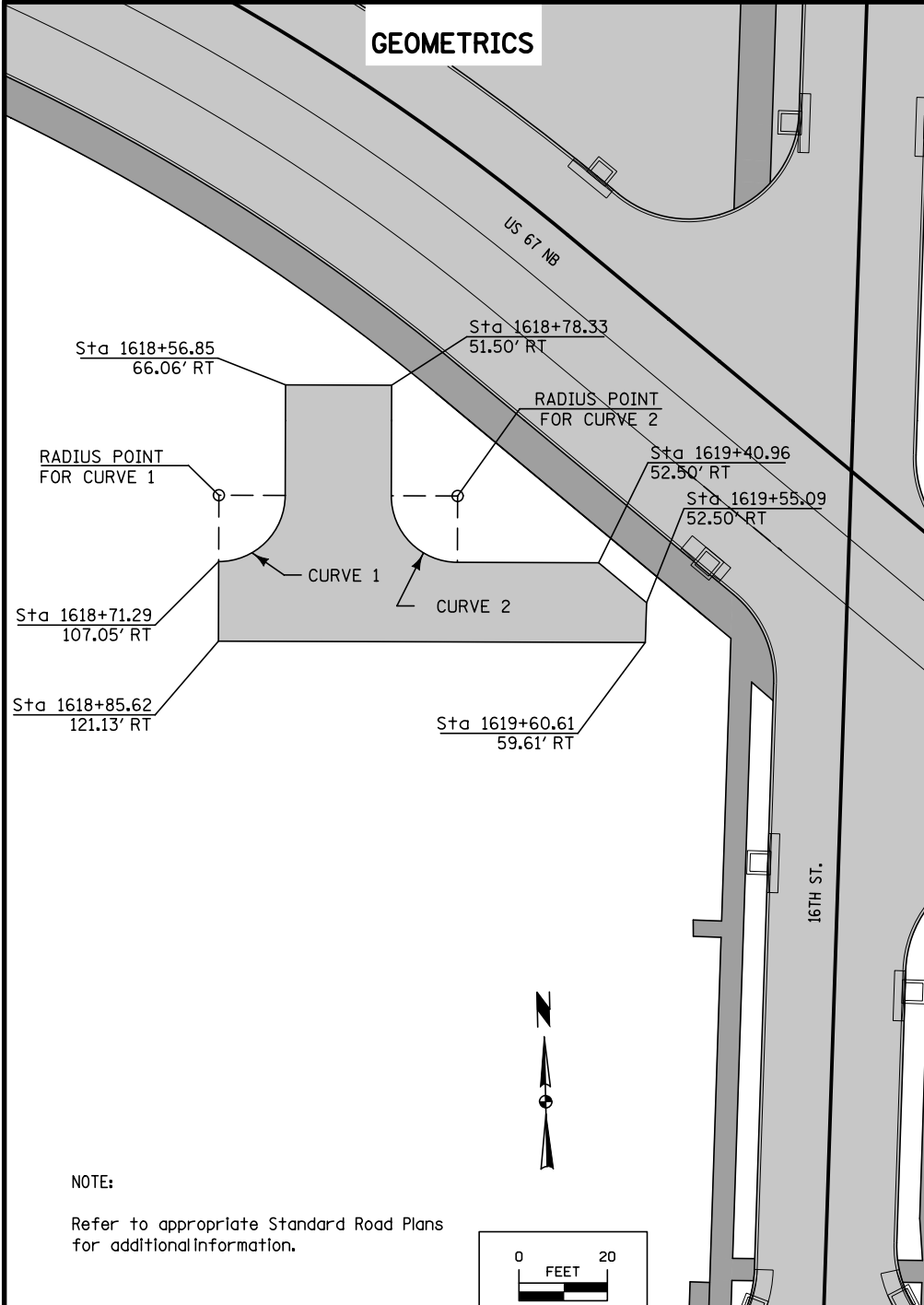
For Storm Sewer Details  
Refer to M Sheets



For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets

**GEOMETRICS**



NOTE:  
Refer to appropriate Standard Road Plans for additional information.

INTERSECTION CIRCULAR CURVE DATA							w101-10C-1
Curve Number	△	D	T	L	E	R	
1	90° 00' 00.00" RT	381° 58' 18.71"	15.00'	23.56'	6.21'	15.00'	
2	90° 00' 00.00" RT	381° 58' 18.71"	15.00'	23.56'	6.21'	15.00'	

INTERSECTION CURVE COORDINATES							w101-10C-2
Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates		
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	
1	569,964.285	2,458,827.304	569,963.417	2,458,812.329	569,948.443	2,458,813.197	
2	569,951.567	2,458,867.106	569,966.542	2,458,866.238	569,965.674	2,458,851.263	

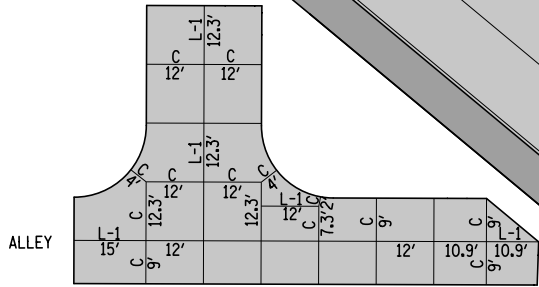


NOTES:  
1) All elevations are top of slab.  
2) Spot elevations shown are located at marked points on Geometric Details.  
3) Field verify tie-in paving elevations.

Point	Northing	Easting	Elevation	Description
3125	569989.243	2458825.857	573.90	TOP OF PAVEMENT
3126	569990.632	2458849.817	574.38	TOP OF PAVEMENT
3127	569965.674	2458851.263	573.88	TOP OF PAVEMENT/ END OF RADIUS
3128	569951.567	2458867.106	573.42	TOP OF PAVEMENT/ END OF RADIUS
3129	569953.416	2458899.001	573.75	TOP OF PAVEMENT
3130	569945.061	2458910.391	574.03	TOP OF PAVEMENT
3131	569936.060	2458910.638	574.21	TOP OF PAVEMENT
3132	569930.473	2458814.238	573.24	MATCH EXISTING/END OF RADIUS
3133	569948.443	2458813.197	573.14	TOP OF PAVEMENT/MATCH EXISTING
3134	569964.285	2458827.304	573.40	TOP OF PAVEMENT/ END OF RADIUS

**STAKING**

For Sidewalk Details Refer to S Sheets    For Storm Sewer Details Refer to M Sheets    For Mainline Details Refer to D Sheets    For Side Road Details Refer to E Sheets



NOTE:  
All longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.  
All transverse joints shall be CD joints with a maximum 20' spacing unless indicated otherwise.  
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Refer to Standard Road Plan PV-101 for additional details.  
Refer to Standard Road Plans for additional jointing details around physical features.

**JOINTING**

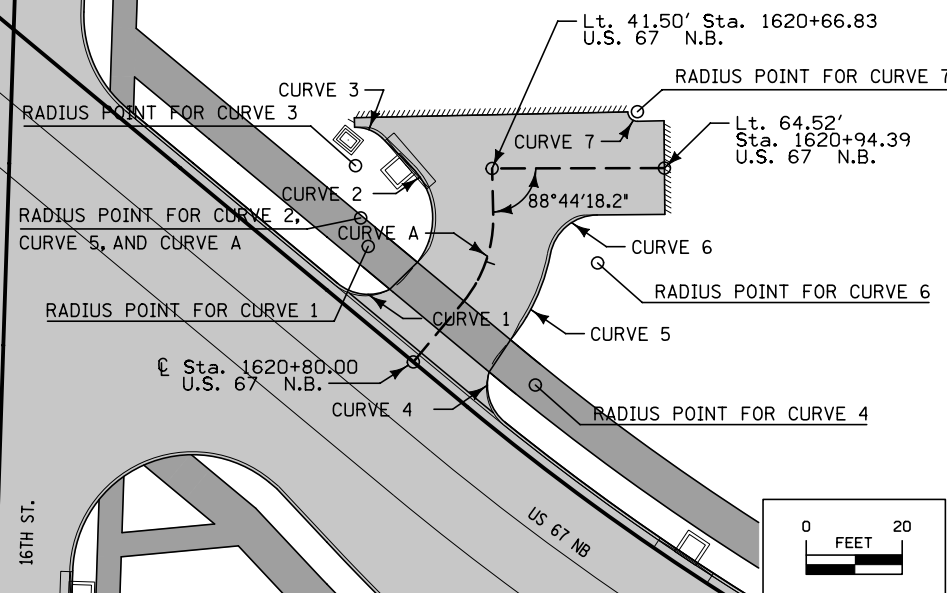
**GEOMETRIC, STAKING AND JOINTING DETAILS**  
Turnaround Southwest of 16th Street and US 67 Northbound Intersection

# GEOMETRICS



**NOTE:**

Refer to appropriate Standard Road Plans for additional information.



## INTERSECTION CIRCULAR CURVE DATA

w101-10C-1

Curve Number	$\Delta$	D	T	L	E	R
A	41° 08' 24.6" LT	207° 24' 19.93"	10.37'	19.84'	1.88'	27.625'
1	89° 59' 57.54" LT	212° 57' 28.06"	10.00'	15.71'	6.51'	10.00'
2	85° 13' 42.95" LT	21° 58' 18.71"	13.80'	22.31'	8.70'	15.00'
3	45° 54' 43.26" LT	356° 11' 50.08"	3.39'	6.41'	1.30	8.00'
4	90° 53' 12.62" RT	212° 57' 28.06"	10.16'	15.86'	6.64'	10.00'
5	28° 53' 46.96" LT	142° 20' 58.35"	10.37'	20.30'	2.57'	40.25'
6	78° 46' 05.49" RT	212° 57' 28.06"	8.21'	13.75'	4.92'	10.00'
7	89° 59' 59.15" RT	2504° 47' 20.31"	2.00'	3.14'	1.30'	2.00'

## INTERSECTION CURVE COORDINATES

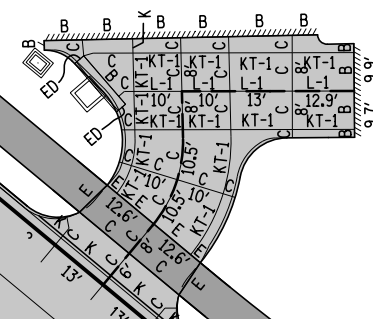
w101-10C-2

Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
1	569,928.909	2,459,025.402	569,936.973	2,459,031.316	569,931.058	2,459,039.379
2	569,933.925	2,459,041.482	569,942.797	2,459,029.387	569,954.113	2,459,039.235
3	569,959.629	2,459,032.896	569,953.594	2,459,027.644	569,961.565	2,459,026.963
4	569,902.105	2,459,062.050	569,910.259	2,459,067.839	569,916.173	2,459,056.776
5	569,919.048	2,459,061.884	569,942.797	2,459,029.387	569,937.708	2,459,069.314
6	569,937.708	2,459,069.314	569,936.444	2,459,079.234	569,946.420	2,459,078.542
7	569,966.415	2,459,085.758	569,968.408	2,459,085.593	569,968.244	2,459,083.600

Point	Northing	Easting	Elevation	Description
3100	569968.244	2459083.600	MATCH	TOP OF PAVEMENT/MATCH EXISTING (573.67)
3101	569966.415	2459085.758	MATCH	TOP OF PAVEMENT/END OF RADIUS (573.85)
3102	569966.868	2459091.246	MATCH	TOP OF PAVEMENT/MATCH EXISTING (573.75)
3103	569957.571	2459091.891	MATCH	MATCH EXISTING (573.44)
3104	569947.395	2459092.597	MATCH	TOP OF PAVEMENT/ MATCH EXISTING (573.66)
3105	569946.420	2459078.542	573.62	TOP OF PAVEMENT
3106	569937.708	2459069.314	573.63	FG END OF RADIUS
3107	569932.474	2459068.291	573.67	FG
3108	569919.048	2459061.884	573.81	FG
3109	569912.389	2459058.069	573.68	FG END OF RADIUS
3110	569902.105	2459062.050	573.04	FG END OF RADIUS
3111	569928.909	2459025.401	573.22	FG END OF RADIUS
3112	569928.327	2459036.340	573.79	FG END OF RADIUS
3113	569933.925	2459041.482	573.91	FG
3114	569945.982	2459039.599	573.44	FG
3115	569954.113	2459039.235	573.21	FG
3116	569959.629	2459032.896	573.41	FG
3117	569961.565	2459026.963	MATCH	TOP OF PAVEMENT/MATCH EXISTING (573.55)
3118	569963.558	2459025.793	MATCH	TOP OF PAVEMENT/MATCH EXISTING (573.61)
3119	569947.418	2459066.755	573.53	TOP OF PAVEMENT AT JOINT INTERSECTION

## STAKING

For Sidewalk Details Refer to S Sheets | For Storm Sewer Details Refer to M Sheets | For Mainline Details Refer to D Sheets | For Side Road Details Refer to E Sheets



ALLEY

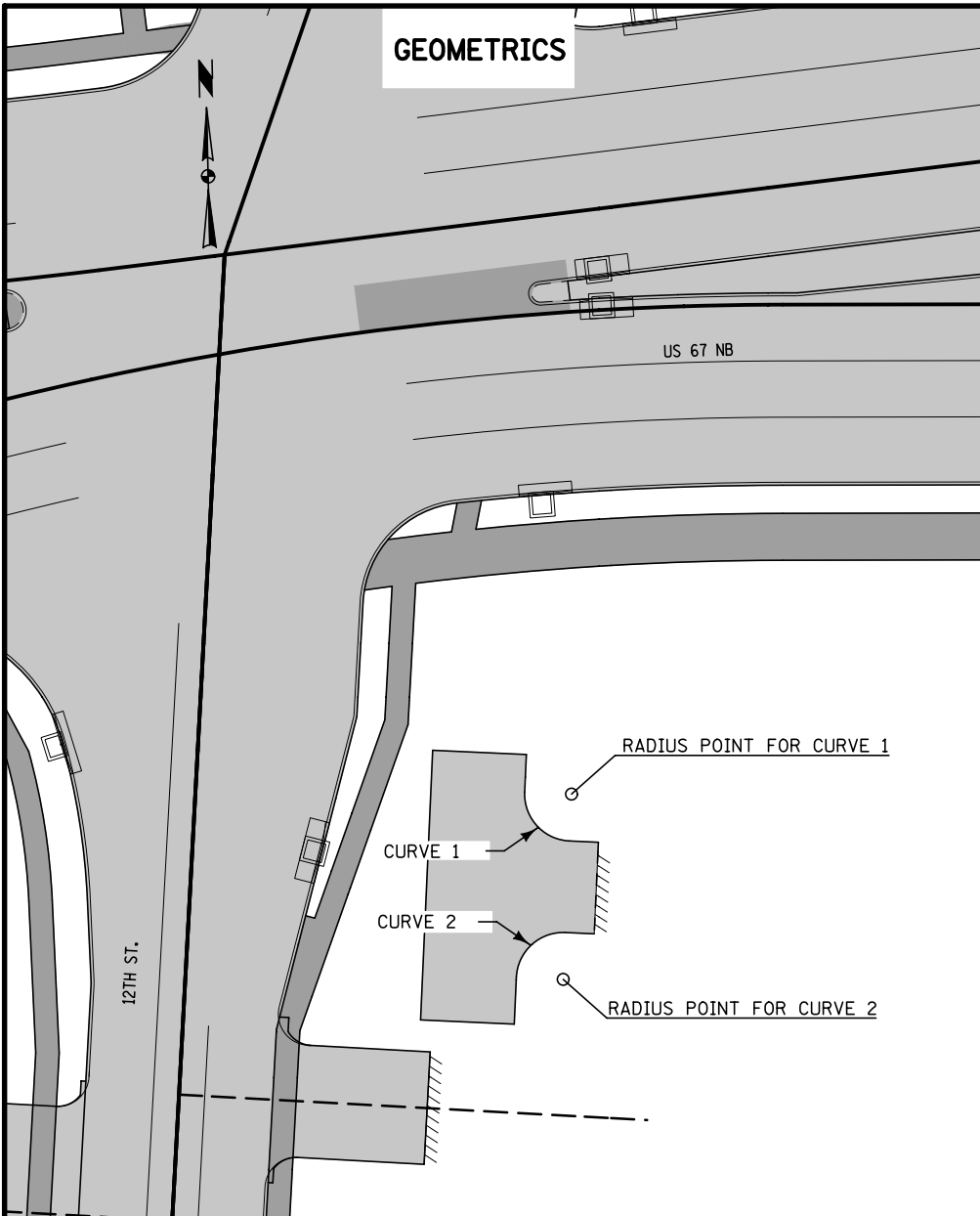
**NOTE:**

Refer to Standard Road Plan MI-210 and PV-101 for additional details.  
Refer to Standard Road Plans for additional jointing details around physical features.

**GEOMETRIC, STAKING AND JOINTING DETAILS**  
Entrance East of 16th Street and US 67 Northbound Intersection

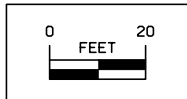
## JOINTING

**GEOMETRICS**



**NOTE:**

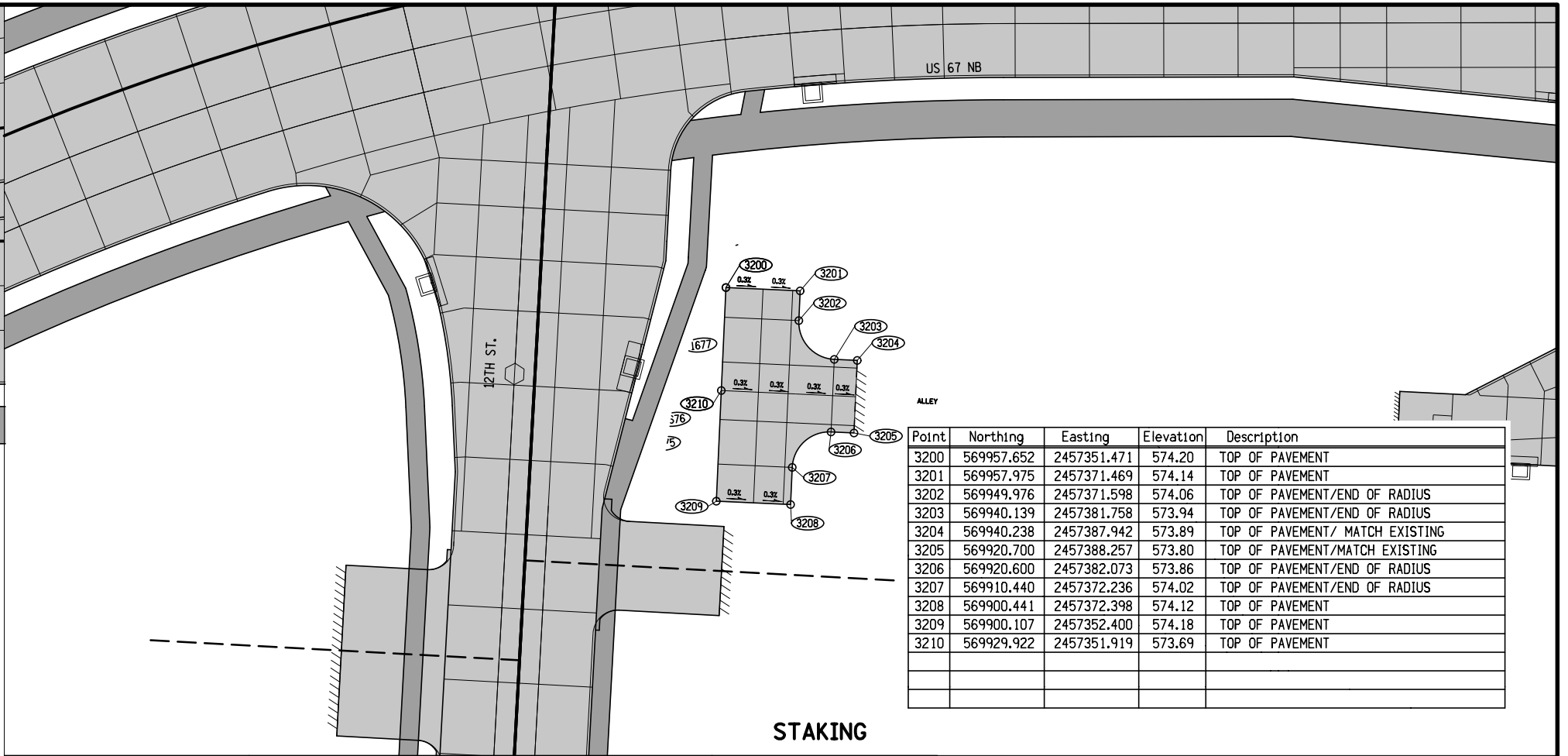
Refer to appropriate Standard Road Plans for additional information.



INTERSECTION CIRCULAR CURVE DATA							w101-10C-1
Curve Number	$\Delta$	D	T	L	E	R	
1	90° 00' 00.00" LT	212° 57' 28.06"	10.00'	15.71'	2.59'	10.00'	
2	90° 00' 00.00" LT	212° 57' 28.06"	10.00'	15.71'	2.59'	10.00'	

INTERSECTION CURVE COORDINATES							w101-10C-2
Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates		
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	
1	569,949.976	2,457,371.598	569,950.137	2,457,381.596	569,940.139	2,457,381.758	
2	569,920.600	2,457,382.073	569,910.601	2,457,382.235	569,910.440	2,457,372.236	

US 67 NB



Point	Northing	Easting	Elevation	Description
3200	569957.652	2457351.471	574.20	TOP OF PAVEMENT
3201	569957.975	2457371.469	574.14	TOP OF PAVEMENT
3202	569949.976	2457371.598	574.06	TOP OF PAVEMENT/END OF RADIUS
3203	569940.139	2457381.758	573.94	TOP OF PAVEMENT/END OF RADIUS
3204	569940.238	2457387.942	573.89	TOP OF PAVEMENT/ MATCH EXISTING
3205	569920.700	2457388.257	573.80	TOP OF PAVEMENT/MATCH EXISTING
3206	569920.600	2457382.073	573.86	TOP OF PAVEMENT/END OF RADIUS
3207	569910.440	2457372.236	574.02	TOP OF PAVEMENT/END OF RADIUS
3208	569900.441	2457372.398	574.12	TOP OF PAVEMENT
3209	569900.107	2457352.400	574.18	TOP OF PAVEMENT
3210	569929.922	2457351.919	573.69	TOP OF PAVEMENT

**STAKING**

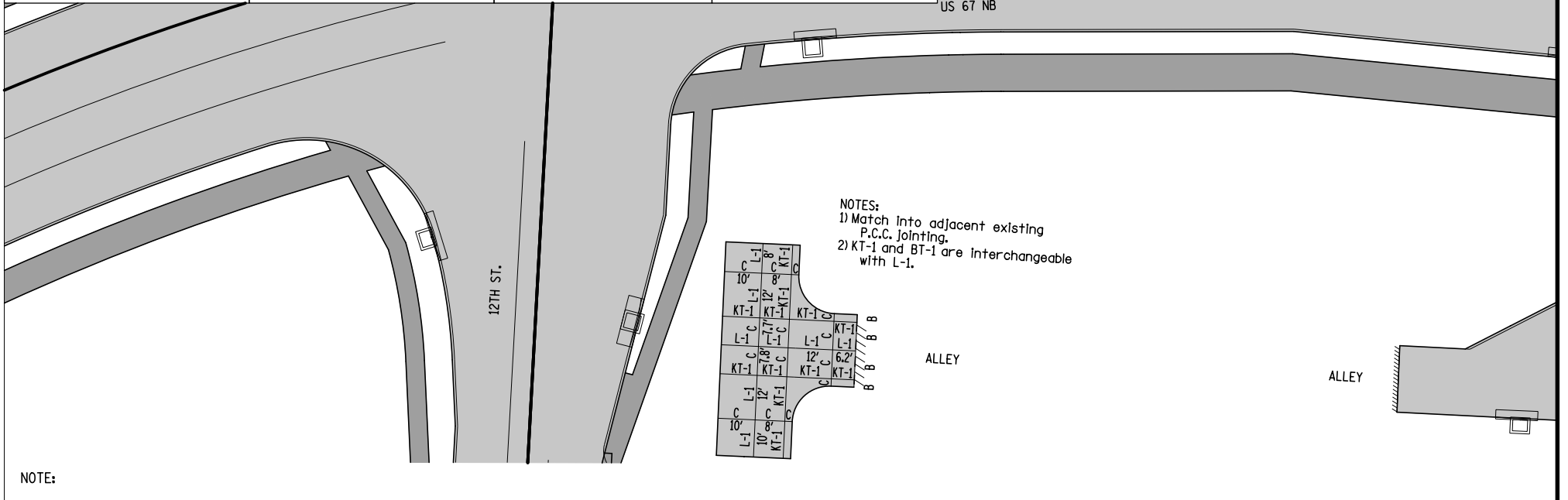
For Sidewalk Details Refer to S Sheets

For Storm Sewer Details Refer to M Sheets

For Mainline Details Refer to D Sheets

For Side Road Details Refer to E Sheets

US 67 NB



**NOTES:**

- 1) Match into adjacent existing P.C.C. Jointing.
- 2) KT-1 and BT-1 are Interchangeable with L-1.

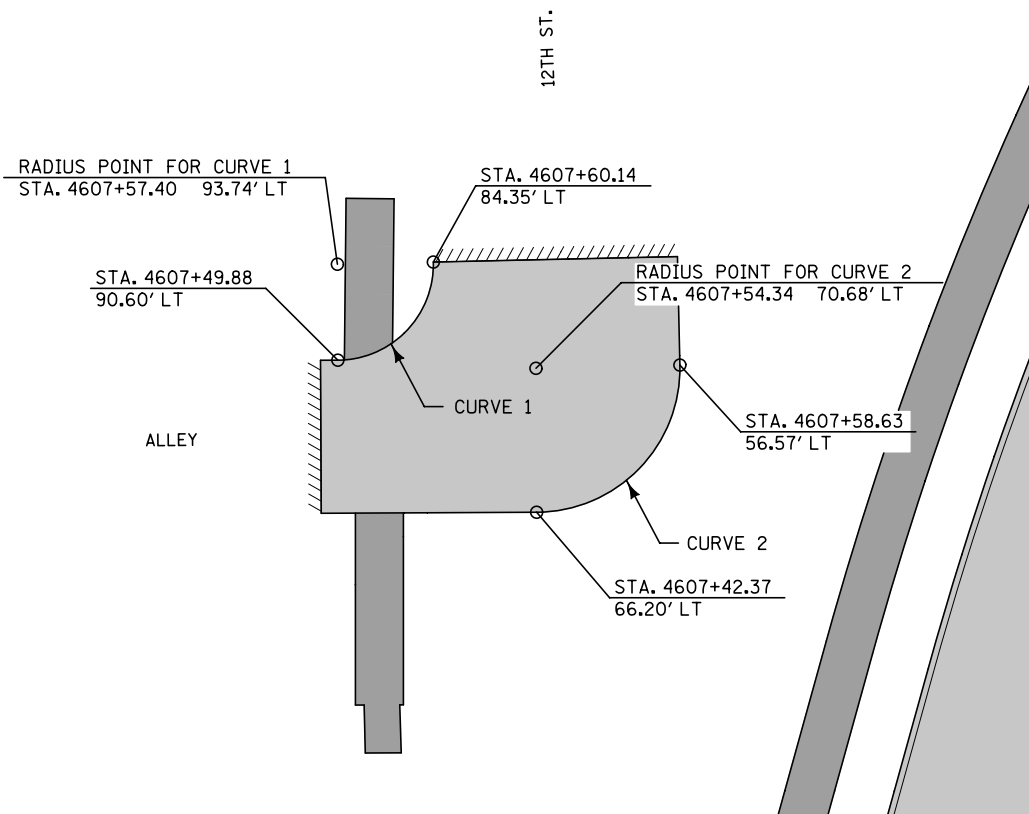
**NOTE:**

- All Longitudinal Joints shall be either KT-2 or L-2 unless indicated otherwise.
- All transverse Joints shall be CD Joints with a maximum 20' spacing unless indicated otherwise.
- If a Joint length is 2', a C Joint shall be used instead of a CD Joint.
- Refer to Standard Road Plans for details of paved header, if applicable.
- Refer to Standard Road Plan PV-101 for additional details.
- Refer to Standard Road Plans for additional jointing details around physical features.

**JOINTING**

**GEOMETRIC, STAKING AND JOINTING DETAILS**  
Turnaround Southeast of Kimberly Road and US 67 Northbound Intersection

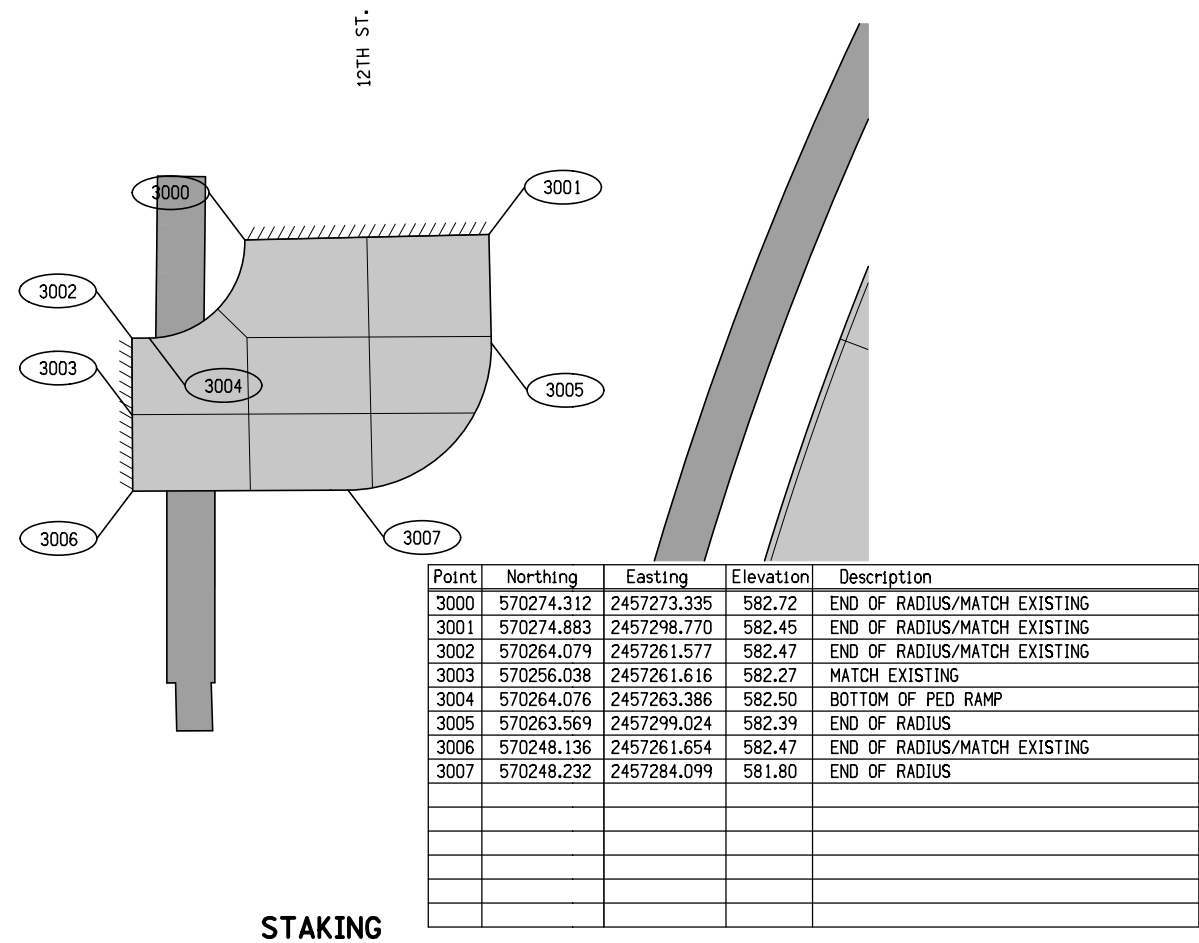
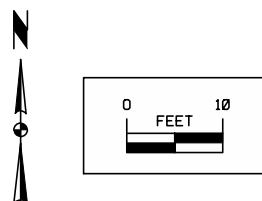




INTERSECTION CIRCULAR CURVE DATA						
Curve Number	$\Delta$	D	T	L	E	R
1	91° 00' 39.25" LT	212° 57' 28.06"	10.18'	15.88'	4.27'	10.00'
2	91° 00' 39.25" LT	21° 58' 18.71"	15.27'	23.83'	6.40'	15.00'

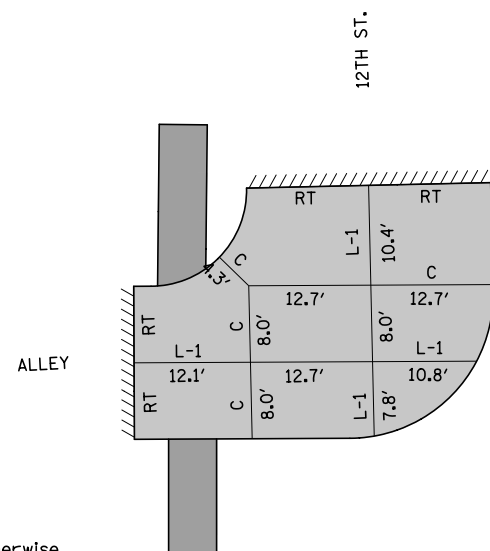
INTERSECTION CURVE COORDINATES						
Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
	1	570,264.088	2,457,263.386	570,274.088	2,457,263.338	570,274.312
2	570,248.244	2,457,284.099	570,263.243	2,457,284.027	570,263.580	2,457,299.023

**GEOMETRICS**



**STAKING**

For Sidewalk Details Refer to S Sheets | For Storm Sewer Details Refer to M Sheets | For Mainline Details Refer to D Sheets | For Side Road Details Refer to E Sheets

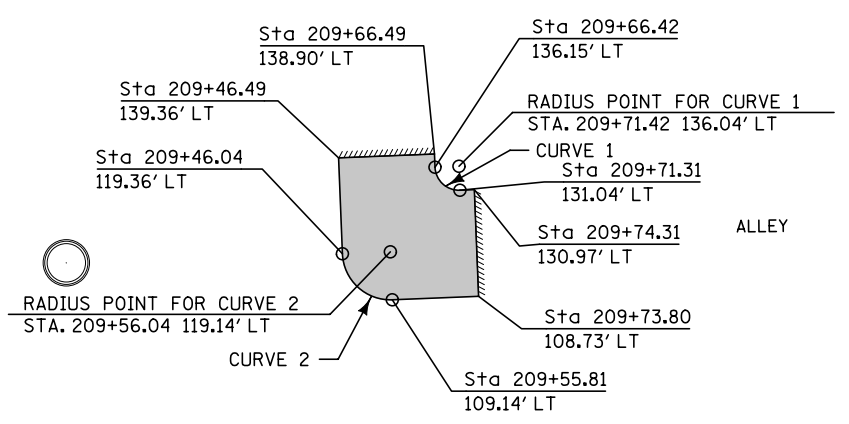
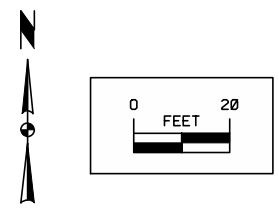


**NOTE:**

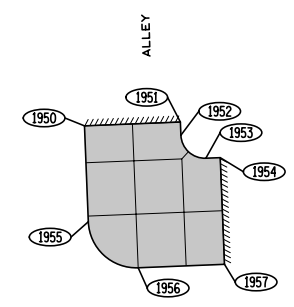
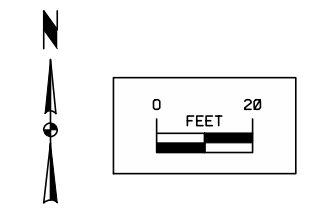
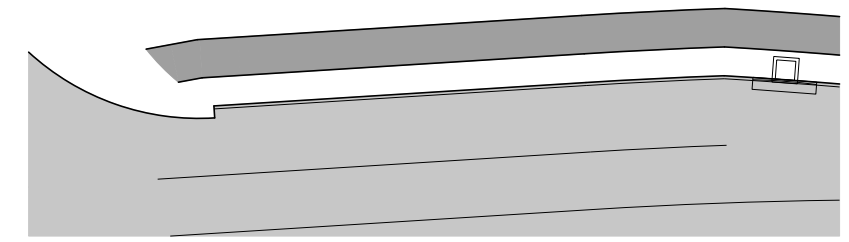
- All Longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.
- All transverse joints shall be CD joints with a maximum 20' spacing unless indicated otherwise.
- If a joint length is 2', a C joint shall be used instead of a CD joint.
- Refer to Standard Road Plans for details of paved header, if applicable.
- Refer to Standard Road Plan PV-101 for additional details.
- Refer to Standard Road Plans for additional jointing details around physical features.

**JOINTING**

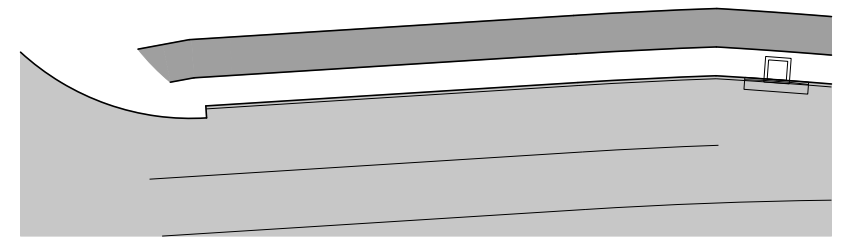
**GEOMETRIC, STAKING AND JOINTING DETAILS**  
Turnaround Southeast of Kimberly Road and US 67 Northbound Intersection



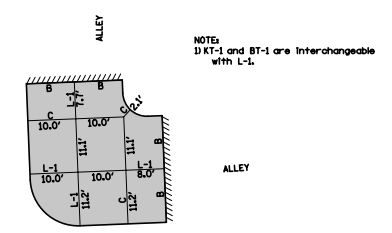
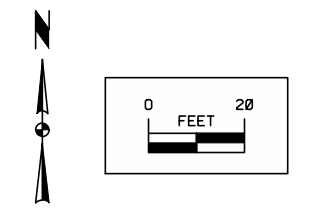
Stationing is from US 67 SB



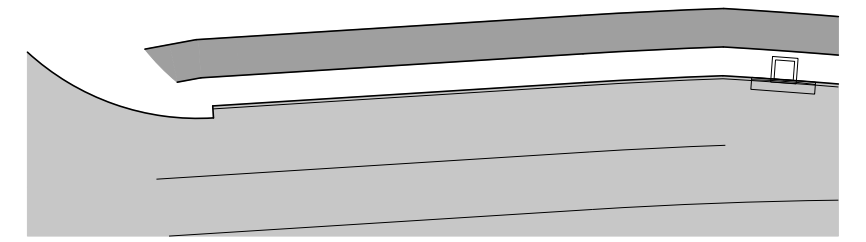
NOTES:  
 1) All elevations are top of slab.  
 2) Spot elevations shown are located at marked points on Geometric Details.  
 3) Field verify tie-in paving elevations.



Point	Northing	Easting	Elevation	Description
1950	570305.964	2458266.282	573.11	MATCH EXISTING
1951	570306.764	2458286.266	572.27	MATCH EXISTING
1952	570304.015	2458286.376	572.30	END OF RADIUS
1953	570299.219	2458291.572	572.30	END OF RADIUS
1954	570299.339	2458294.570	572.48	MATCH EXISTING
1955	570285.980	2458267.082	572.20	END OF RADIUS
1956	570276.388	2458277.475	572.14	END OF RADIUS
1957	570277.109	2458295.460	572.72	MATCH EXISTING



NOTE:  
 1) KT-1 and BT-1 are interchangeable with L-1.



NOTE:  
 All Longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.  
 All transverse joints shall be CD joints with a maximum 20' spacing unless indicated otherwise.  
 If a joint length is 2', a C joint shall be used instead of a CD joint.  
 Refer to Standard Road Plans for details of paved header, if applicable.  
 Refer to Standard Road Plan PV-1 for additional details.  
 Refer to Standard Road Plans for additional jointing details around physical features.

**JOINTING**

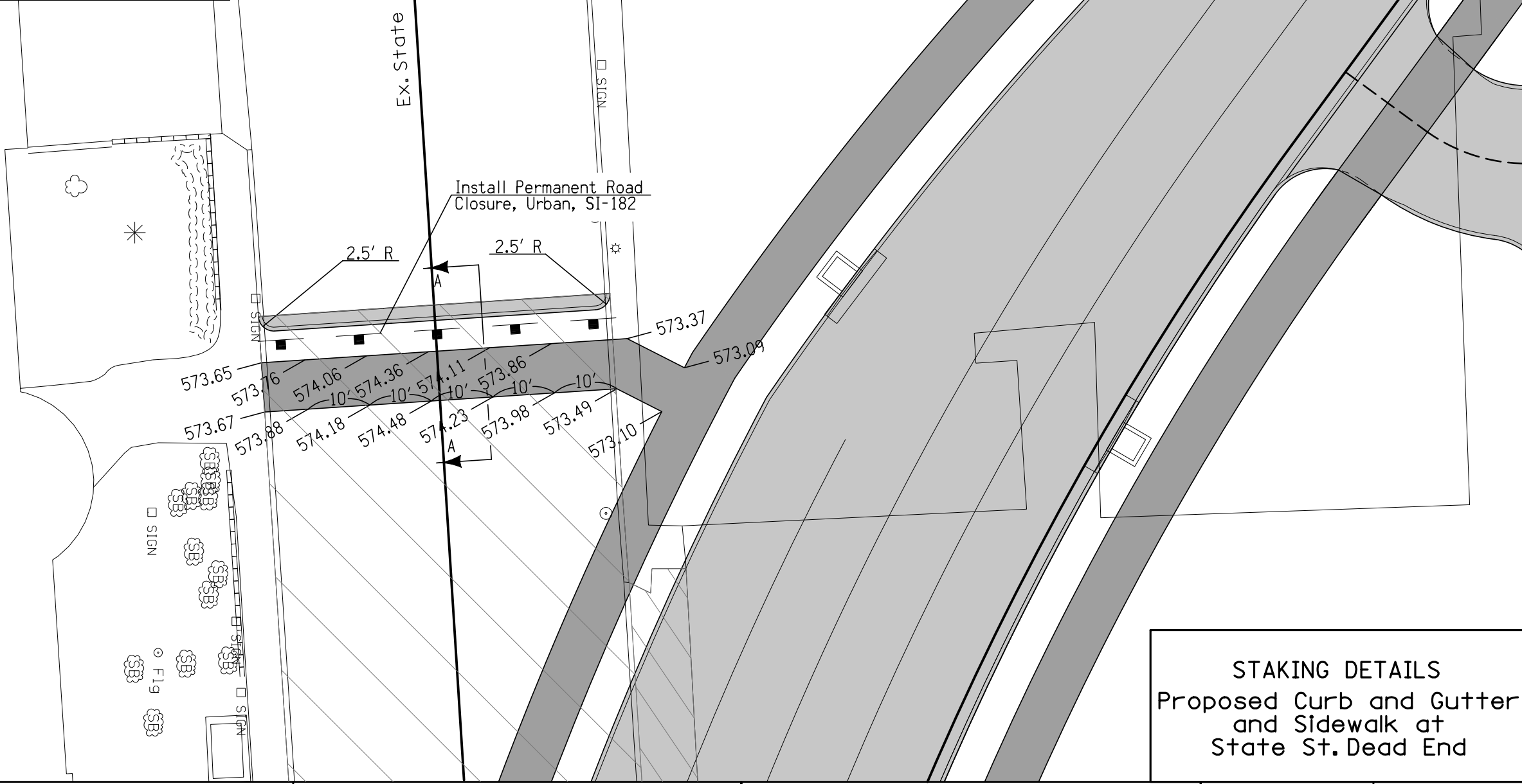
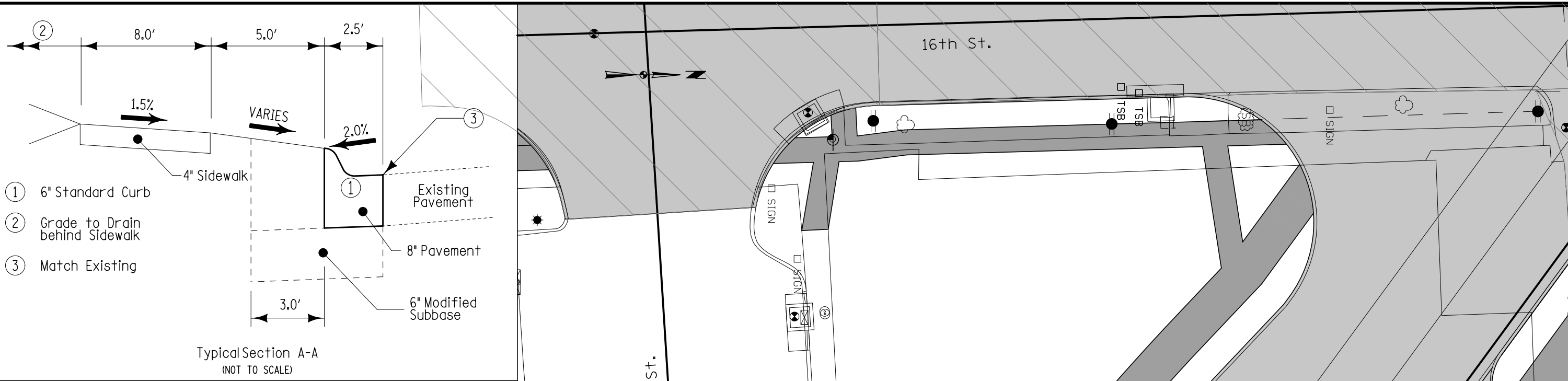
**GEOMETRIC, STAKING AND JOINTING DETAILS**  
 Tie-In Northeast of US 67 SB and Ramp D Intersection

INTERSECTION CIRCULAR CURVE DATA						
Curve Number	$\Delta$	D	T	L	E	R
1	90° 00' 00.00" LT	572° 57' 28.06"	10.00'	15.71'	4.14'	10.00'
2	90° 00' 00.00" LT	1145° 54' 56.12"	5.00'	7.85'	2.07'	5.00'

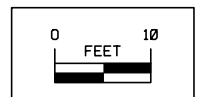
INTERSECTION CURVE COORDINATES						
Curve Number	P.C. Coordinates		C.C. Coordinates		P.T. Coordinates	
	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)	Y (Northing)	X (Easting)
1	570,304.015	2,458,286.376	570,304.215	2,458,291.372	570,299.219	2,458,291.572
2	570,285.980	2,458,267.082	570,286.380	2,458,277.074	570,276.388	2,458,277.475

**GEOMETRICS**

**STAKING**

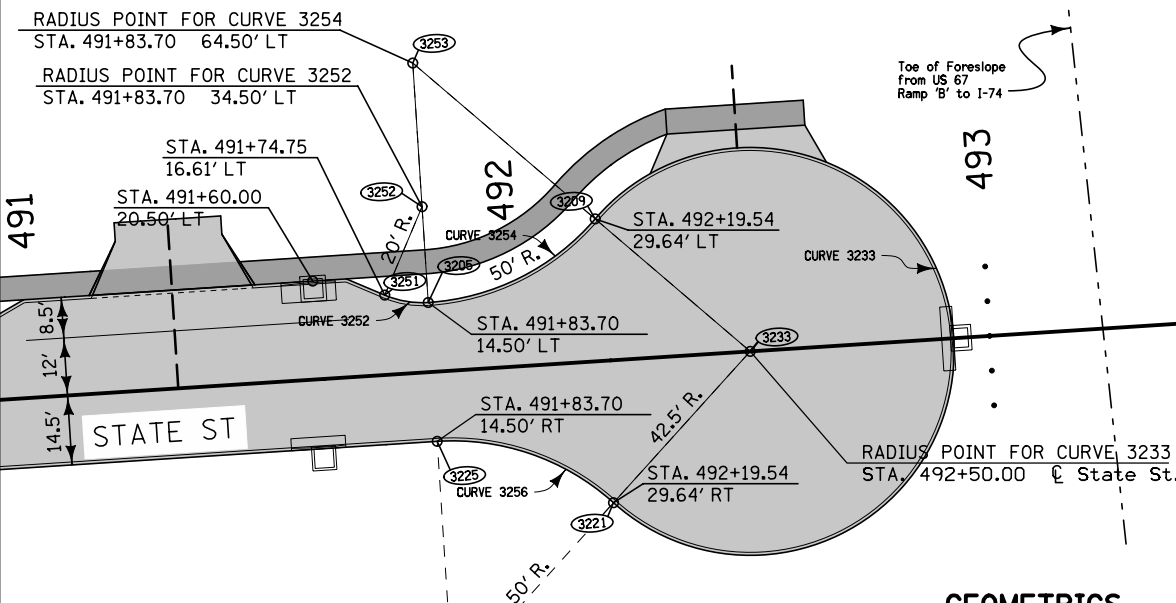


**STAKING DETAILS**  
Proposed Curb and Gutter  
and Sidewalk at  
State St. Dead End





Point	Northing	Easting	Elevation	Description
3251	569702.432	2457775.936	572.23	FG END OF RADIUS
3252	569720.860	2457783.708	- - - -	CENTER OF CURVE
3205	569700.901	2457784.997	572.26	FG - P.C.C. OF RADII
3253	569750.797	2457781.773	- - - -	CENTER OF CURVE
3209	569718.316	2457819.786	572.23	FG - P.R.C. OF RADII
3225	569671.962	2457786.868	572.26	FG - P.C.C. OF RADII
3256	569622.066	2457790.093	- - - -	CENTER OF CURVE
3221	569659.169	2457823.608	572.23	FG - P.R.C. OF RADII
3233	569690.708	2457852.097	571.95	CENTER OF CURVE



**GEOMETRICS**

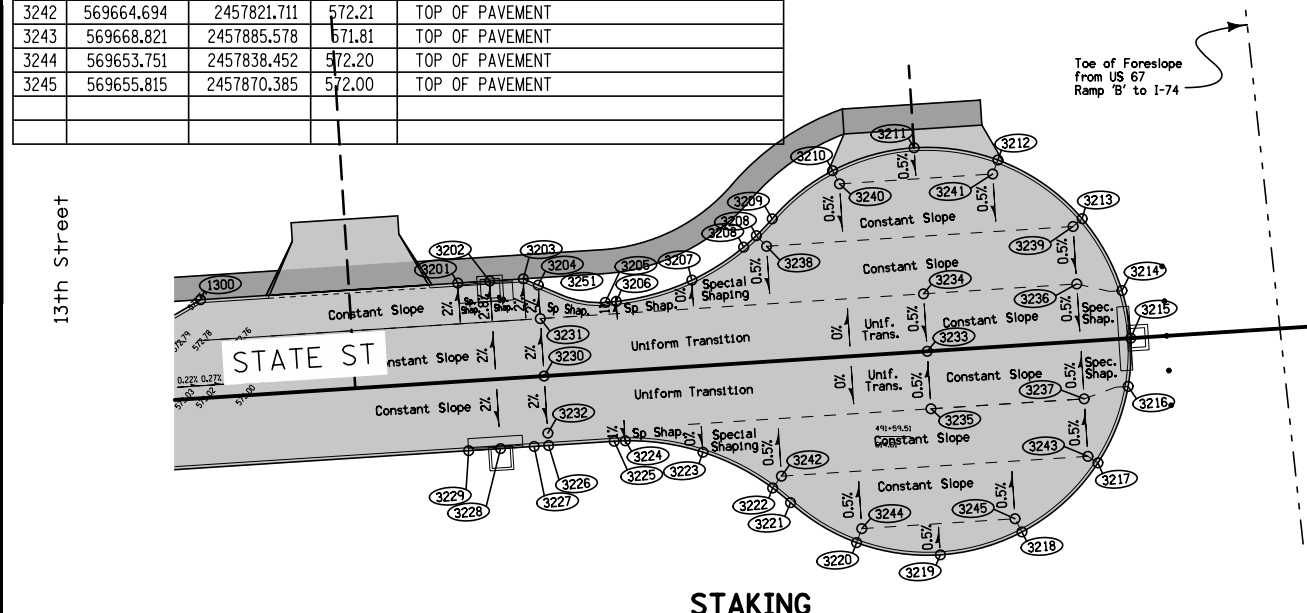
INTERSECTION CIRCULAR CURVE DATA							w101-10C-1
Curve Number	Δ	D	T	L	E	R	
3252	26°33'25.18" LT	286°28'44.03"	4.72'	9.27'	0.55'	20.00'	
3253	45°47'21.96" LT	114°35'29.61"	21.12'	39.96'	4.28'	50.00'	
3233	271°34'43.92" RT	134°48'48.96"	-41.35'	201.45'	-101.79'	42.50'	
3256	45°47'21.96" LT	114°35'29.61"	21.12'	39.96'	4.28'	50.00'	



**JOINTING**

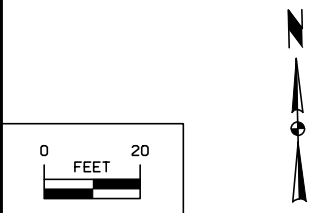
NOTES:  
Refer to Standard Road Plan PV-101 for additional details.  
Refer to Standard Road Plans for additional jointing details around physical features.

Point	Northing	Easting	Elevation	Description
3201	569704.928	2457754.272	572.31	FG
3202	569705.358	2457760.924	572.19	FG AT INTAKE
3203	569705.810	2457767.924	572.21	FG
3204	569704.497	2457771.039	572.22	FG
3205	569700.901	2457784.997	572.26	FG - P.C.C. OF RADII
3206	569701.103	2457787.292	572.25	FG
3207	569705.545	2457803.038	572.21	FG
3208	569714.807	2457816.481	572.22	FG
3209	569718.316	2457819.786	572.23	FG - P.R.C. OF RADII
3210	569728.351	2457832.367	572.23	FG
3211	569733.119	2457849.356	572.17	FG
3212	569730.577	2457866.817	572.01	FG
3213	569718.347	2457884.382	571.82	FG
3214	569703.351	2457892.673	571.66	FG
3215	569693.443	2457894.424	571.61	FG AT INTAKE
3216	569683.393	2457893.963	571.66	FG
3217	569667.454	2457887.671	571.82	FG
3218	569653.065	2457871.827	572.01	FG
3219	569648.296	2457854.838	572.17	FG
3220	569650.838	2457837.377	572.22	FG
3221	569659.169	2457823.608	572.23	FG - P.R.C. OF RADII
3222	569662.224	2457819.880	572.22	FG
3223	569669.679	2457805.356	572.21	FG
3224	569672.057	2457789.169	572.25	FG
3225	569671.962	2457786.868	572.26	FG - P.C. OF RADIUS
3226	569671.078	2457773.199	572.30	FG
3227	569670.883	2457770.181	572.32	FG
3228	569670.431	2457763.182	572.38	FG AT INTAKE
3229	569670.001	2457756.529	572.43	FG
3230	569685.547	2457772.248	572.58	TOP OF PAVEMENT
3231	569697.523	2457771.490	572.34	TOP OF PAVEMENT
3232	569673.645	2457773.033	572.34	TOP OF PAVEMENT
3233	569690.708	2457852.097	571.95	TOP OF PAVEMENT - CTR CUL-DE-SAC
3234	569702.683	2457851.323	572.01	TOP OF PAVEMENT
3235	569678.733	2457852.871	572.01	TOP OF PAVEMENT
3236	569704.746	2457883.256	571.75	TOP OF PAVEMENT
3237	569680.796	2457884.804	571.75	TOP OF PAVEMENT
3238	569712.594	2457818.616	572.21	TOP OF PAVEMENT
3239	569716.722	2457882.482	571.81	TOP OF PAVEMENT
3240	569725.601	2457833.808	572.20	TOP OF PAVEMENT
3241	569727.665	2457865.742	572.00	TOP OF PAVEMENT
3242	569664.694	2457821.711	572.21	TOP OF PAVEMENT
3243	569668.821	2457885.578	571.81	TOP OF PAVEMENT
3244	569653.751	2457838.452	572.20	TOP OF PAVEMENT
3245	569655.815	2457870.385	572.00	TOP OF PAVEMENT

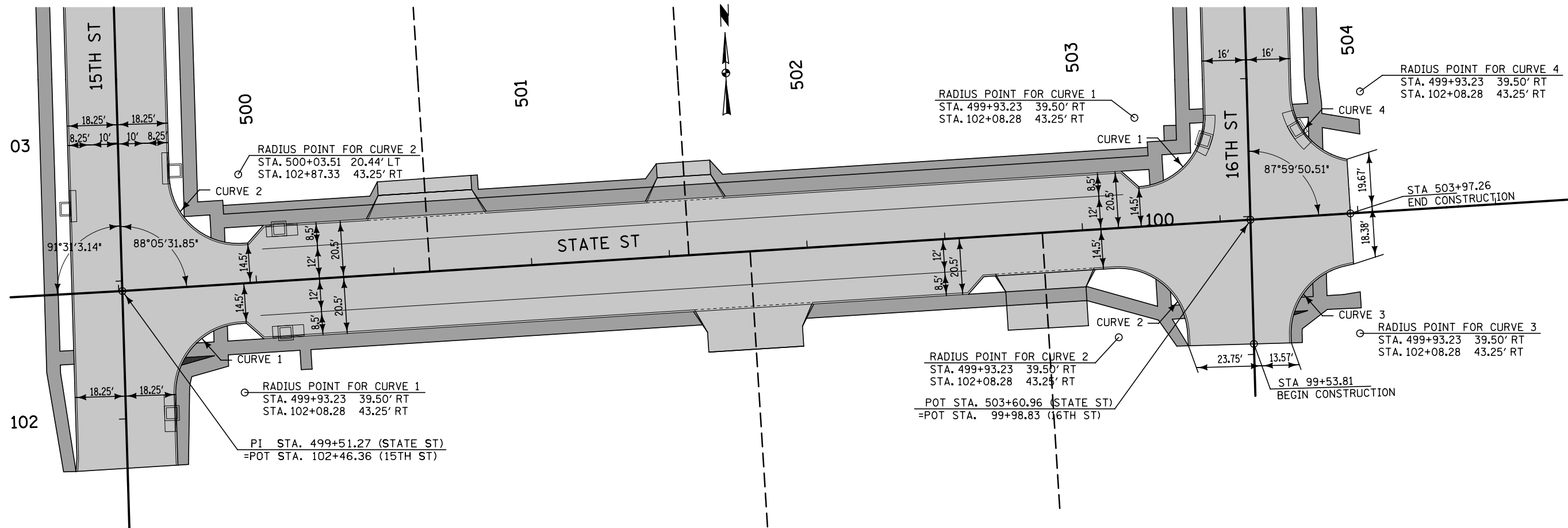


**STAKING**

For Storm Sewer Details Refer to M Sheets



**GEOMETRIC, STAKING AND JOINTING DETAILS**  
Cul-de-sac on State Street East of 13th Street

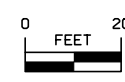


INTERSECTION CIRCULAR CURVE DATA							w101-10C-1
Curve Number	$\Delta$	D	T	L	E	R	
1	88° 05' 31.85° RT	229° 10' 59.22"	24.18'	38.44'	9.78'	25.00'	
2	91° 54' 27.67° LT	229° 10' 59.20"	25.85'	40.10'	10.96'	25.00'	

INTERSECTION CIRCULAR CURVE DATA							w101-10C-1
Curve Number	$\Delta$	D	T	L	E	R	
1	87° 59' 50.50° RT	229° 10' 59.22"	24.14'	38.40'	9.75'	25.00'	
2	90° 00' 00.46° RT	229° 10' 59.20"	25.00'	39.27'	10.36'	25.00'	
3	79° 02' 09.45° RT	229° 10' 59.20"	20.62'	34.49'	7.41'	25.00'	
4	77° 30' 53.51° RT	229° 10' 59.22"	20.07'	33.82'	7.06'	25.00'	

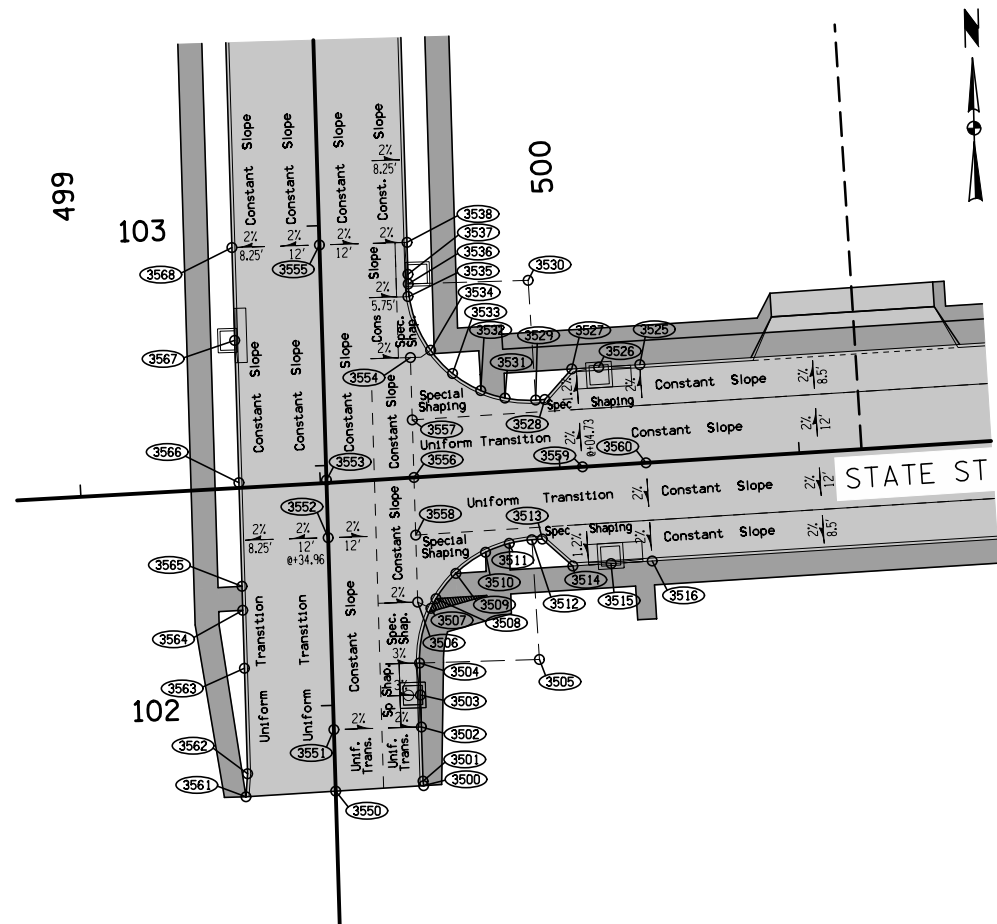
GEOMETRIC DETAILS  
Proposed Intersections of  
State St. and 15th St.  
State St. and 16th St.

For Storm Sewer Details  
Refer to M Sheets



For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets

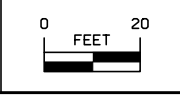


Point	Northing	Easting	Elevation	Description
3500	569669.399	2458572.351	570.70	FG - TIE FLUSH TO EXIST.
3501	569670.339	2458572.228	570.69	FG
3502	569680.621	2458571.892	570.53	FG
3503	569688.285	2458571.694	570.47	FG AT INTAKE
3504	569694.949	2458571.495	570.49	FG - END OF RADIUS
3505	569695.693	2458596.484	- - -	CENTER OF CURVE
3506	569707.643	2458571.116	570.57	TOP OF PAVEMENT
3507	569706.347	2458573.867	570.51	FG
3508	569708.360	2458574.930	570.54	FG - AT SIDEWALK RAMP
3509	569713.631	2458579.070	570.62	FG - AT SIDEWALK RAMP
3510	569718.030	2458585.255	570.72	FG - AT SIDEWALK RAMP
3511	569719.891	2458590.202	570.77	FG - AT SIDEWALK RAMP
3512	569720.644	2458594.907	570.79	FG - END OF RADIUS
3513	569720.781	2458597.080	570.80	FG
3514	569715.171	2458603.447	570.77	FG
3515	569715.675	2458611.415	570.74	FG AT INTAKE
3516	569716.216	2458619.988	570.84	FG
3525	569757.135	2458617.403	570.84	FG
3526	569756.593	2458608.830	570.74	FG AT INTAKE
3527	569756.238	2458603.210	570.77	FG
3528	569749.872	2458597.600	570.80	FG
3529	569749.752	2458595.705	570.81	FG - END OF RADIUS
3530	569771.292	2458569.220	- - -	CENTER OF CURVE
3531	569750.173	2458589.299	570.84	FG - AT SIDEWALK RAMP
3532	569751.737	2458584.250	570.82	FG - AT SIDEWALK RAMP
3533	569755.288	2458578.378	570.80	FG
3534	569760.135	2458573.811	570.79	FG
3535	569774.702	2458594.129	570.83	FG
3536	569773.957	2458569.140	570.84	FG - END OF RADIUS
3537	569775.957	2458569.080	570.85	FG AT INTAKE
3538	569782.620	2458568.882	570.87	FG
3550	569668.251	2458554.032	570.85	TOP OF PAVEMENT
3551	569681.078	2458553.650	570.89	TOP OF PAVEMENT
3552	569721.070	2458552.458	571.03	TOP OF PAVEMENT
3553	569732.469	2458552.118	571.07	TOP OF PAVEMENT
3554	569758.614	2458569.597	570.79	TOP OF PAVEMENT
3555	569782.077	2458550.640	571.24	TOP OF PAVEMENT
3556	569733.621	2458570.342	570.70	TOP OF PAVEMENT
3557	569745.622	2458569.985	570.75	TOP OF PAVEMENT
3558	569721.619	2458570.700	570.66	TOP OF PAVEMENT
3559	569735.840	2458605.465	571.10	TOP OF PAVEMENT
3560	569736.675	2458618.695	571.25	TOP OF PAVEMENT
3561	569667.079	2458535.330	570.56	FG - TIE FLUSH TO EXIST.
3562	569671.878	2458535.666	570.57	FG
3563	569693.861	2458535.011	572.62	FG
3564	569705.945	2458534.651	570.64	FG - AT SIDEWALK RAMP
3565	569710.944	2458534.502	570.65	FG - AT SIDEWALK RAMP
3566	569732.533	2458533.858	570.70	FG
3567	569762.191	2458532.975	570.81	FG AT INTAKE
3568	569781.533	2458532.398	570.87	FG

**STAKING DETAILS**  
Proposed Intersection of  
15th Street and  
State Street

For Sidewalk Details  
Refer to S Sheets

For Storm Sewer Details  
Refer to M Sheets

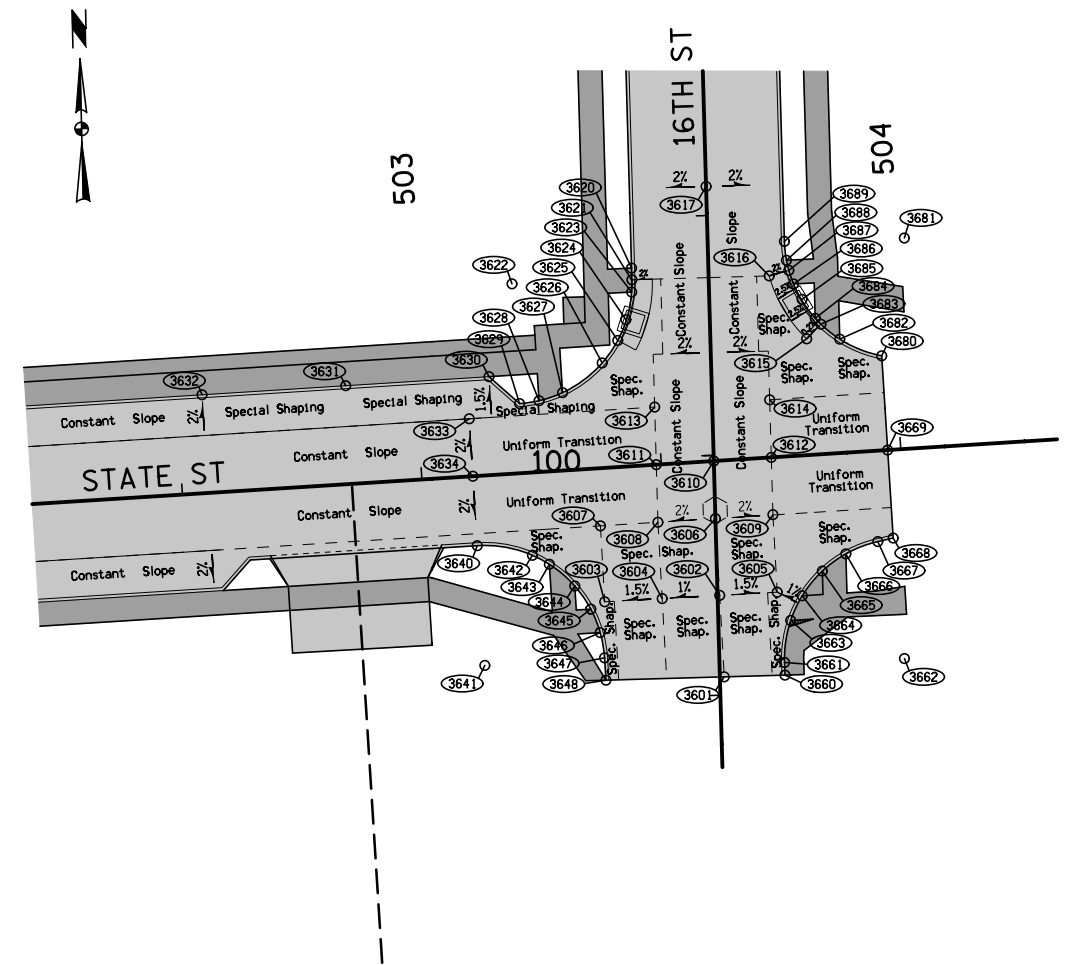


For Mainline Details  
Refer to D Sheets

For Side Road Details  
Refer to E Sheets

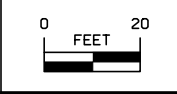
Point	Northing	Eastng	Elevation	Description
3601	569713.297	2458962.255	571.07	TOP OF PAVEMENT - TIE FLUSH TO EXIST.
3602	569730.278	2458961.777	572.66	TOP OF PAVEMENT
3603	569744.784	2458937.370	572.36	TOP OF PAVEMENT
3604	569729.632	2458950.236	572.54	TOP OF PAVEMENT
3605	569730.969	2458974.205	572.48	TOP OF PAVEMENT
3606	569746.298	2458961.326	572.78	TOP OF PAVEMENT
3607	569744.784	2458937.370	572.54	TOP OF PAVEMENT
3608	569745.540	2458949.343	572.54	TOP OF PAVEMENT
3609	569747.055	2458973.310	572.54	TOP OF PAVEMENT
3610	569758.300	2458960.988	572.87	TOP OF PAVEMENT
3611	569757.543	2458949.005	572.63	TOP OF PAVEMENT
3612	569759.057	2458972.972	572.63	TOP OF PAVEMENT
3613	569769.546	2458948.667	572.72	TOP OF PAVEMENT
3614	569771.060	2458972.634	572.72	TOP OF PAVEMENT
3615	569783.765	2458980.343	572.73	TOP OF PAVEMENT
3616	569796.879	2458972.532	572.91	TOP OF PAVEMENT
3617	569815.516	2458959.378	573.30	TOP OF PAVEMENT
3620	569798.513	2458943.850	572.86	FG - AT SIDEWALK RAMP
3621	569795.925	2458943.923	572.84	FG - END OF RADIUS - AT INTAKE INSERT 'ED' JOINT
3622	569795.222	2458918.933	- - - -	CENTER OF CURVE
3623	569793.508	2458943.874	572.82	FG - AT SIDEWALK RAMP
3624	569787.723	2458942.782	572.68	FG - AT INTAKE
3625	569783.428	2458940.976	572.65	FG - AT INTAKE INSERT 'ED' JOINT
3626	569778.737	2458937.727	572.62	FG
3627	569772.551	2458929.470	572.59	FG - AT SIDEWALK RAMP
3628	569770.865	2458924.566	572.56	FG - AT SIDEWALK RAMP
3629	569770.272	2458920.509	572.54	FG - END OF RADIUS
3630	569775.881	2458914.143	572.39	FG
3631	569773.995	2458884.279	572.30	FG
3632	569772.103	2458854.339	572.20	FG
3633	569767.105	2458910.027	572.50	TOP OF PAVEMENT
3634	569755.128	2458910.784	572.74	TOP OF PAVEMENT

Point	Northing	Eastng	Elevation	Description
3640	569740.657	2458911.698	572.45	FG - END OF RADIUS
3641	569715.707	2458913.274	- - - -	CENTER OF CURVE
3642	569738.654	2458923.197	572.41	FG
3643	569736.784	2458926.719	572.39	FG - AT SIDEWALK RAMP
3644	569732.253	2458932.015	572.36	FG - AT SIDEWALK RAMP
3645	569727.437	2458935.352	572.33	FG - AT SIDEWALK RAMP
3646	569722.551	2458937.319	572.32	FG - AT SIDEWALK RAMP
3647	569717.283	2458938.224	572.31	FG - END OF RADIUS
3648	569712.629	2458938.518	572.30	FG - TIE FLUSH TO EXIST.
3660	569713.679	2458975.822	572.41	FG - TIE FLUSH TO EXIST.
3661	569716.121	2458975.901	572.43	FG - END OF RADIUS
3662	569717.138	2459000.723	- - - -	CENTER OF CURVE
3663	569726.118	2458977.391	572.50	FG - END OF RADIUS
3664	569730.220	2458979.419	572.54	FG - AT SIDEWALK RAMP
3665	569735.359	2458983.605	572.65	FG - AT SIDEWALK RAMP
3666	569738.950	2458988.506	572.79	FG - AT SIDEWALK RAMP
3667	569741.514	2458995.171	573.02	FG - END OF RADIUS
3668	569742.245	2458998.380	573.12	FG - TIE FLUSH TO EXIST.
3669	569760.589	2458997.221	573.26	TOP OF PAVEMENT - TIE FLUSH TO EXIST.
3680	569780.222	2458995.981	573.20	FG - END OF RADIUS - TIE FLUSH TO EXIST.
3681	569804.773	2459000.696	- - - -	CENTER OF CURVE
3682	569783.700	2458987.246	572.79	FG - AT SIDEWALK RAMP
3683	569786.818	2458983.301	572.72	FG - AT INTAKE INSERT 'ED' JOINT
3684	569788.040	2458982.122	572.71	FG - AT SIDEWALK RAMP
3685	569791.985	2458979.381	572.69	FG AT INTAKE
3686	569795.199	2458977.603	572.76	FG - AT SIDEWALK RAMP
3687	569798.026	2458976.624	572.83	FG - AT INTAKE INSERT 'ED' JOINT
3688	569800.131	2458976.131	572.86	FG - AT SIDEWALK RAMP
3689	569804.070	2458975.706	572.89	FG - END OF RADIUS



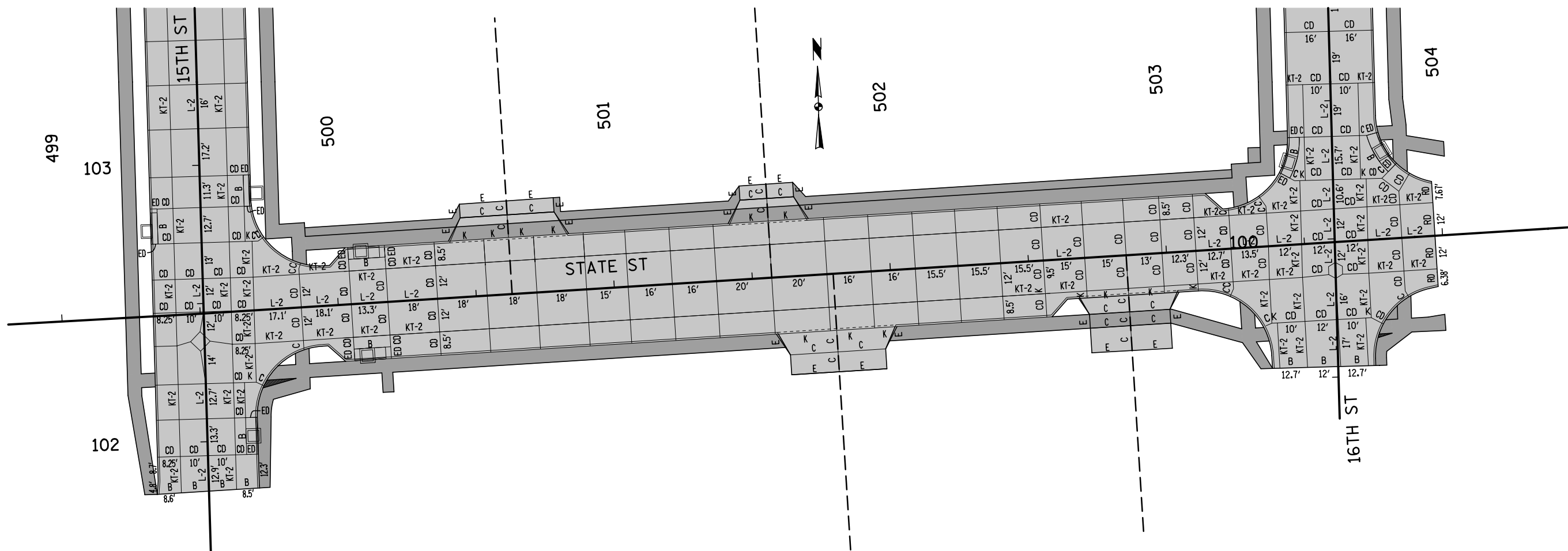
STAKING DETAILS  
Proposed Intersection of  
16th Street and  
State Street

For Sidewalk Details Refer to S Sheets      For Storm Sewer Details Refer to M Sheets



For Mainline Details Refer to D Sheets      For Side Road Details Refer to E Sheets



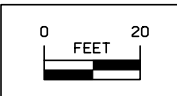


**NOTE:**

- All Longitudinal joints shall be either KT-2 or L-2 unless indicated otherwise.
- All transverse joints shall be CD joints with a maximum 20' spacing unless indicated otherwise.
- If a joint length is 2', a C joint shall be used instead of a CD joint.
- Refer to Standard Road Plans for details of paved header, if applicable.
- Refer to Standard Road Plan PV-101 for additional details.
- Refer to Standard Road Plans for additional jointing details around physical features.

**JOINTING DETAILS**  
 Proposed Intersections of  
 State St. and 15th St.  
 State St. and 16th St.

For Storm Sewer Details  
 Refer to M Sheets



For Side Road Details  
 Refer to E Sheets







### SURVEY SYMBOLS

	Interstate Highway Symbol		Septic Tank
	U.S. Highway Symbol		Cistern
	Iowa Highway Symbol		L.P. Gas Tank (No Footing)
	County Road Highway Symbol		Underground Storage Tank
	Evergreen Tree		Latrine
	Deciduous Tree		Luminaire
	Fruit Tree		Traffic Signal
	Shrub (Bushes)		Traffic Signal with Luminaire
	Timber		TP Telephone Pedestal
	Hedge		TVP Television Pedestal
	Stump		Telephone Pole
	Swamp		Telephone Pole (Second Company)
	Rock Outcrop		Telephone Pole (Third Company)
	Broken Concrete		Telephone Pole (Fourth Company)
	Revetment (Rip Rap)		Telephone Pole (Fifth Company)
	Cemetery		Power Pole
	Grave		Power Pole (Second Company)
	Cave		Power Pole (Third Company)
	Sink Hole		Power Pole (Fourth Company)
	Board Fence		Power Pole (Fifth Company)
	Chain Link or Security Fence		Electrical Highline Tower (Metal or Concrete)
	Wire Fence		Telephone Riser Pole
	Terrace		Power Riser Pole
	Earth Dam or Dike (Existing)		Telegraph Pole
	Earth Dam or Dike (Proposed)		Satellite TV Dish
	Tile Outlet		Guardrail (Beam or Cable)
	Edge of Water		GP Guard Post (one or two)
	Existing Drainage		GP Guard Post (over two)
	Proposed Drainage		FP Filler Pipe
	Right of Way Rail or Lot Corner		GV Gas Valve
	Concrete Monument		WV Water Valve
	Well		SL Speed Limit Sign
	Windmill		MM Mile Marker Post
	Beehive Intake		SIGN Sign
	Existing Intake		WHU Water Hook Up
	Proposed Intake		RT Radio Tower
	Existing Utility Access (Manhole)		TA Tower Anchor
	Proposed Utility Access (Manhole)		EB Electric Box
	Fire Hydrant		TCB Traffic Signal Control Box
	Water Hydrant (Rural)		RRB Rail Road Signal Control Box
			TSB Telephone Switch Box

### UTILITY LEGEND

— F0 —	Existing Fiber Optics (Central Scott)
— F02 —	Existing Fiber Optics (McLeod USA)
— F03 —	Existing Fiber Optics (Qwest)
— F04 —	Existing Fiber Optics (ATT)
— F06 —	Existing Fiber Optics (MediaCom)
— F08 —	Existing Fiber Optics (Bettendorf)
— F09 —	Existing Fiber Optics (IowaDOT)
— E —	Existing Power Line (MidAmerican)
— E2 —	Existing Power Line (MidAmerican)
— E3 —	Existing Power Line (MidAmerican)
— E4 —	Existing Power Line (MidAmerican)
— E5 —	Existing Power Line (IowaDOT)
— G —	Existing Gas Line (MidAmerican)
— G-HP —	Existing High Pressure Gas Line (MidAmerican)
— San. —	Existing Sanitary Sewer Line (Bettendorf)
— San.2 —	Existing Sanitary Sewer Line (Davenport)
— T —	Existing Telephone Line (Qwest)
— TV —	Existing Cable Television Line (MediaCom)
— TV2 —	Existing Cable Television Line (MediaCom)
— W —	Existing Water Line (IA American)

### PLAN VIEW COLOR LEGEND OF STORM SEWER SHEETS

LINEWORK	Design Color No.	Description
Gray, Dark	(112)	Existing Topographic Features, Utilities, and Labels
Black	(17)	Proposed Storm Sewer Details, Alignment, Stationing, Tic Marks, and Alignment Annotation
SHADING	Design Color No.	Description
Gray, Light	(48)	Proposed Pavement Shading

### PROFILE VIEW COLOR LEGEND OF STORM SEWER SHEETS

LINEWORK	Design Color No.	Description
Gray, Dark	(112)	Existing Ground Line Profile and Existing Utilities Information
Black	(17)	Proposed Pipes and Intakes

### PLAN VIEW LINE STYLE LEGEND OF STORM SEWER SHEETS

	Plug and Abandon Existing Pipe or Structure
	Removal of Existing Pipe or Structure
	Previously Constructed Pipe or Structure
	Direction of Pipe Flow

### PROFILE VIEW LINE STYLE LEGEND OF STORM SEWER SHEETS

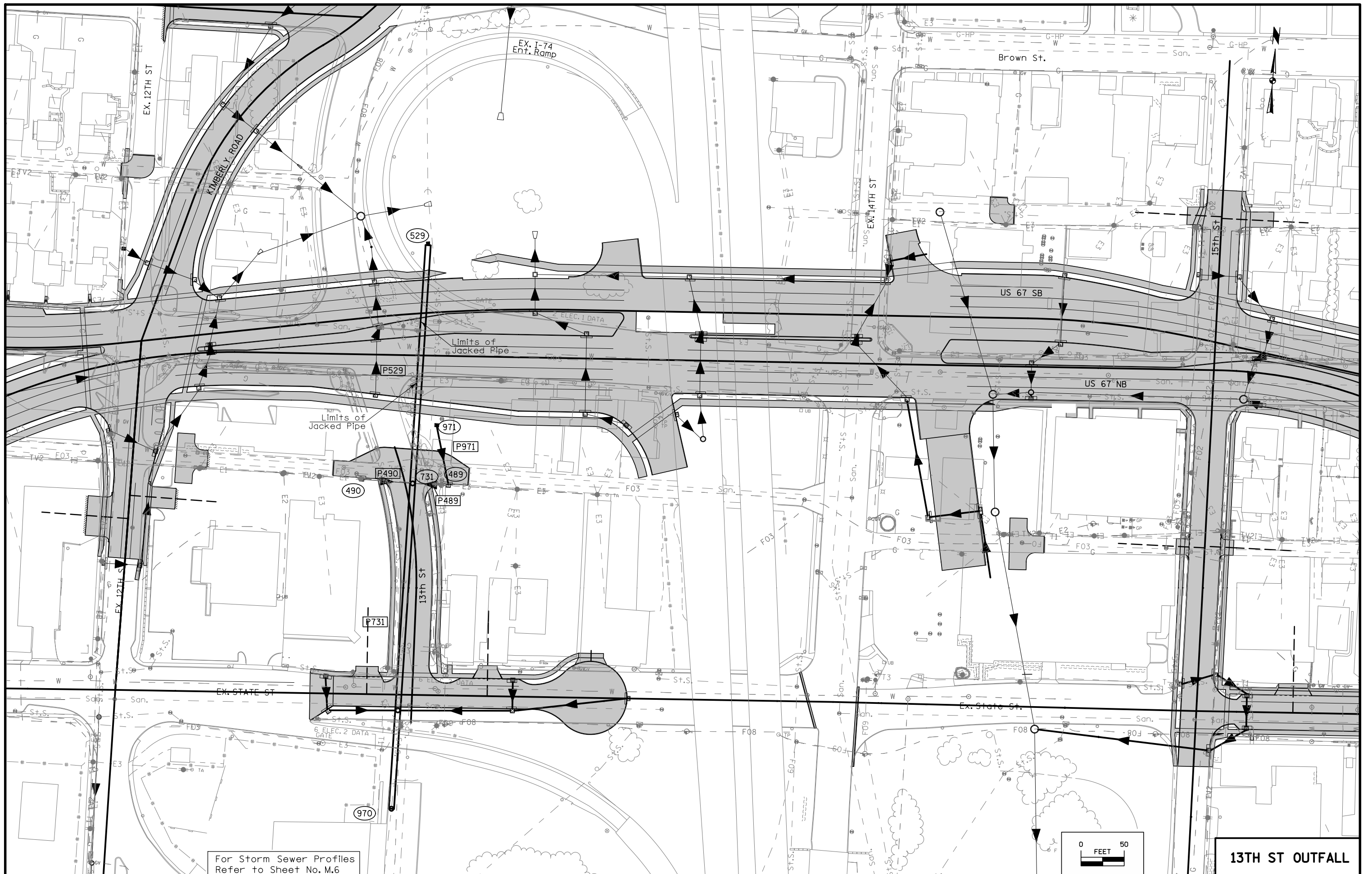
	Existing Ground
	Proposed Ground
	Previously Constructed Pipe or Structure
	Proposed Pipe or Structure

Symbol	Description
	Reference Point
	Station
	Survey Line
	Section Corner
	Ground Line Intercept
	Saw Cut
	Guardrail
	Clearing & Grubbing Area
	Pavement Removal

RIGHT-OF-WAY LEGEND	
	Proposed Right-of-Way
	Existing and Proposed Right-of-Way
	Easement and Existing Right-of-Way
	Borrow
	Easement (Temporary)
	Easement
	Excess
	A/C Access Control

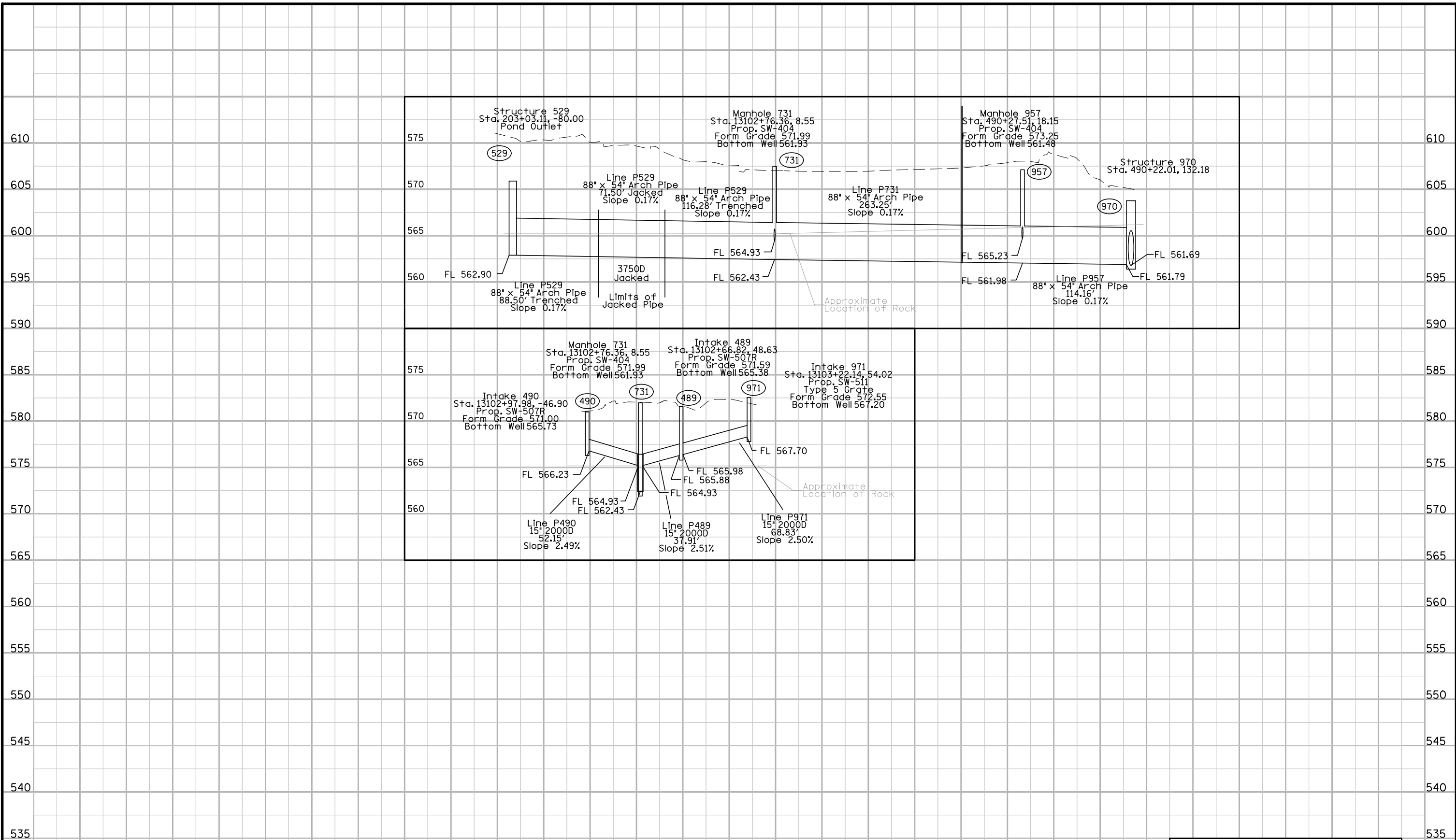
# STORM SEWER LEGEND AND SYMBOL INFORMATION SHEET

(COVERS SHEET SERIES M)



For Storm Sewer Profiles  
Refer to Sheet No. M.6

**13TH ST OUTFALL**

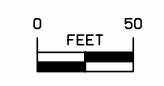


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**13TH ST OUTFALL**

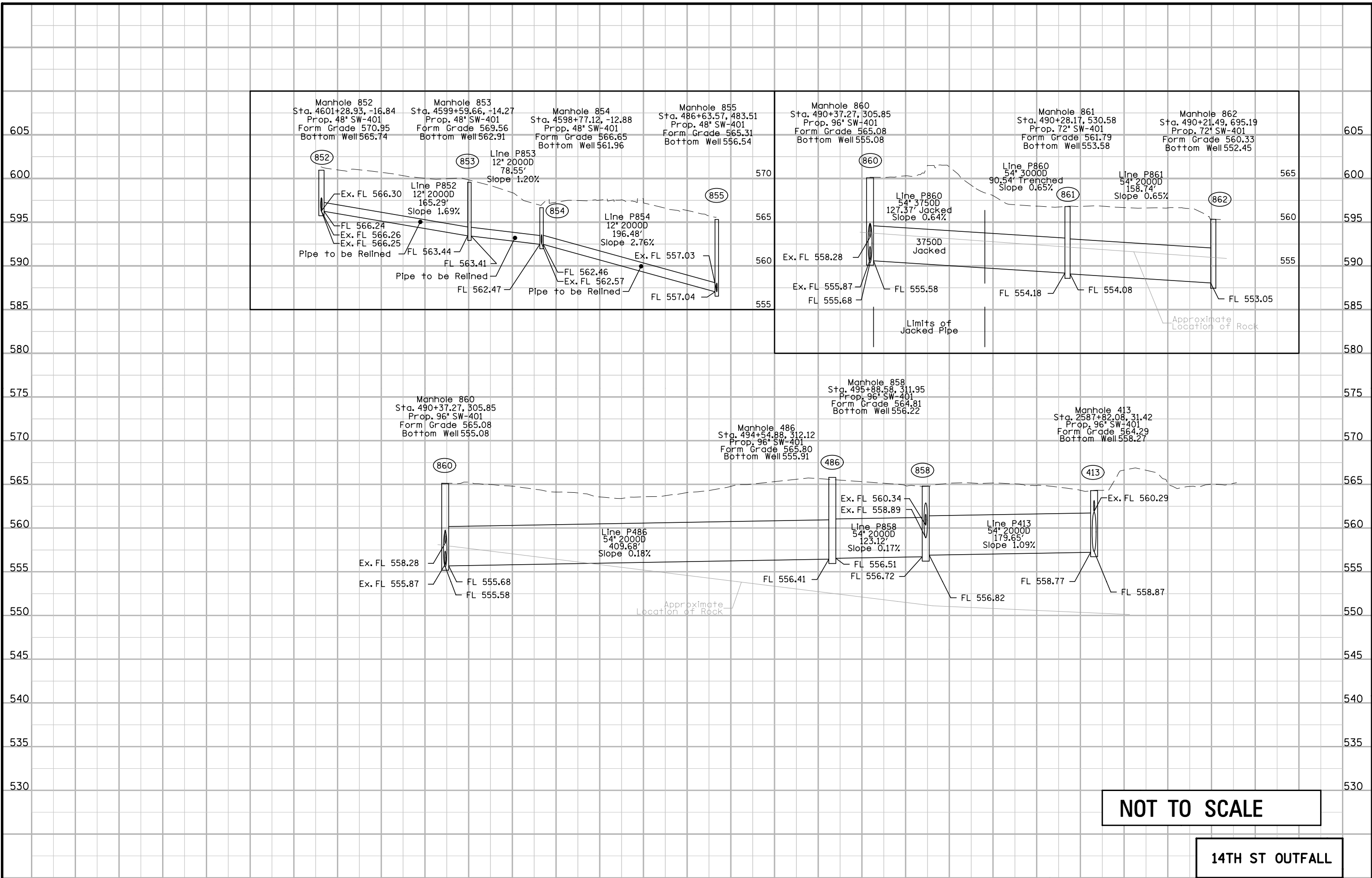


For Storm Sewer Profiles  
Refer to Sheet No. M.8



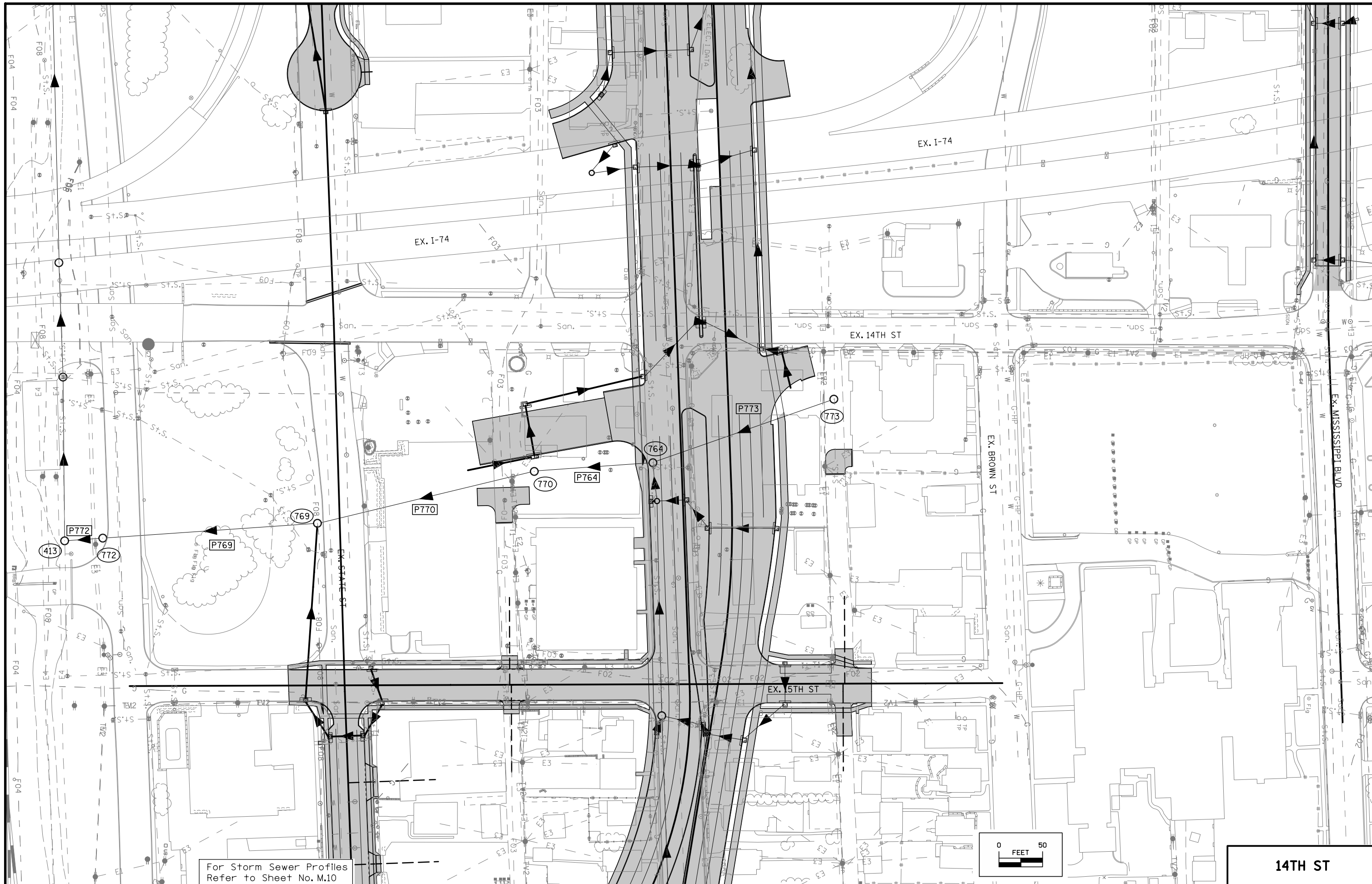
**14TH ST OUTFALL**



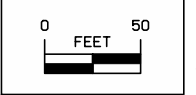


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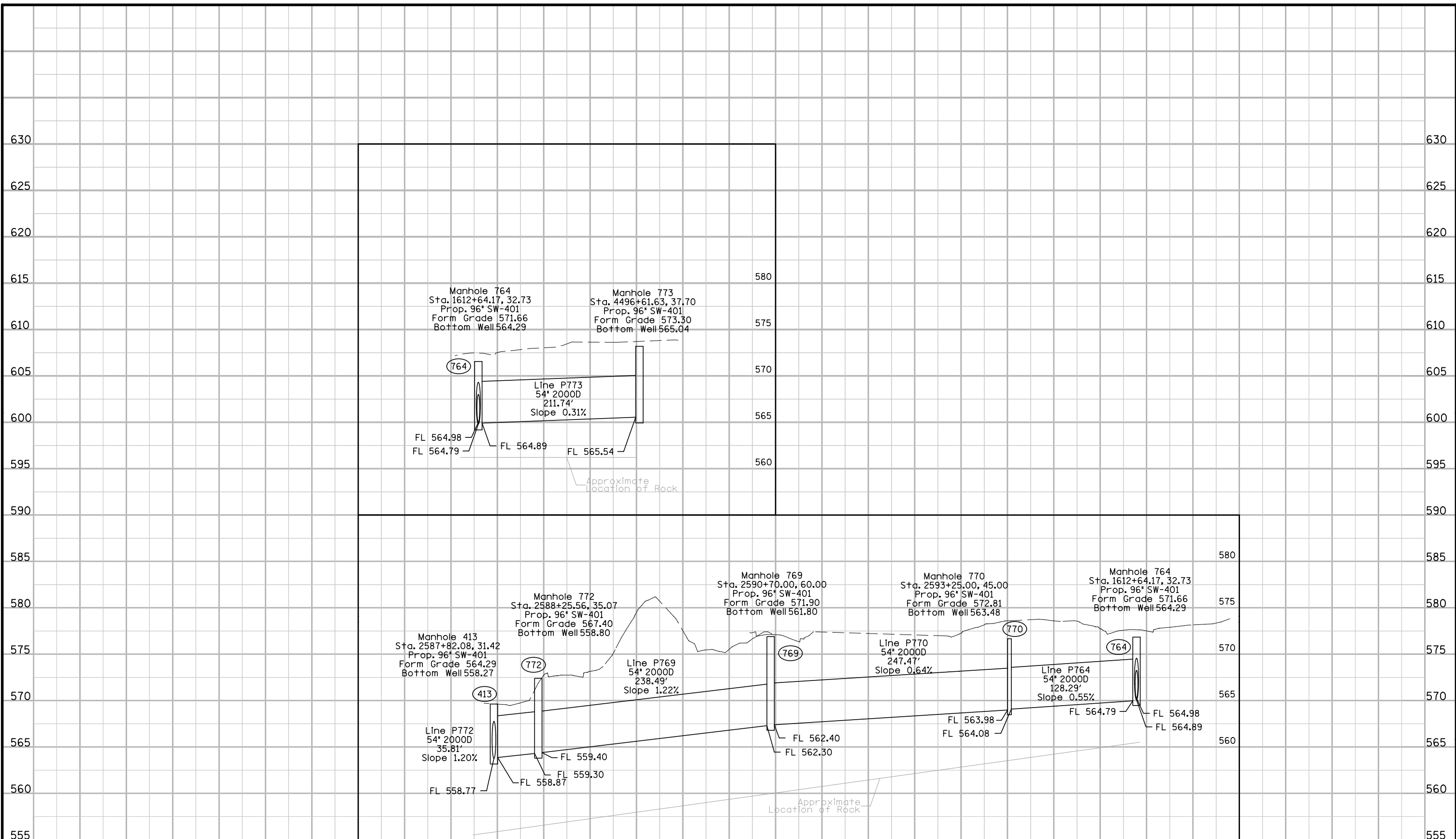
**14TH ST OUTFALL**



For Storm Sewer Profiles  
Refer to Sheet No. M.10

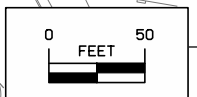
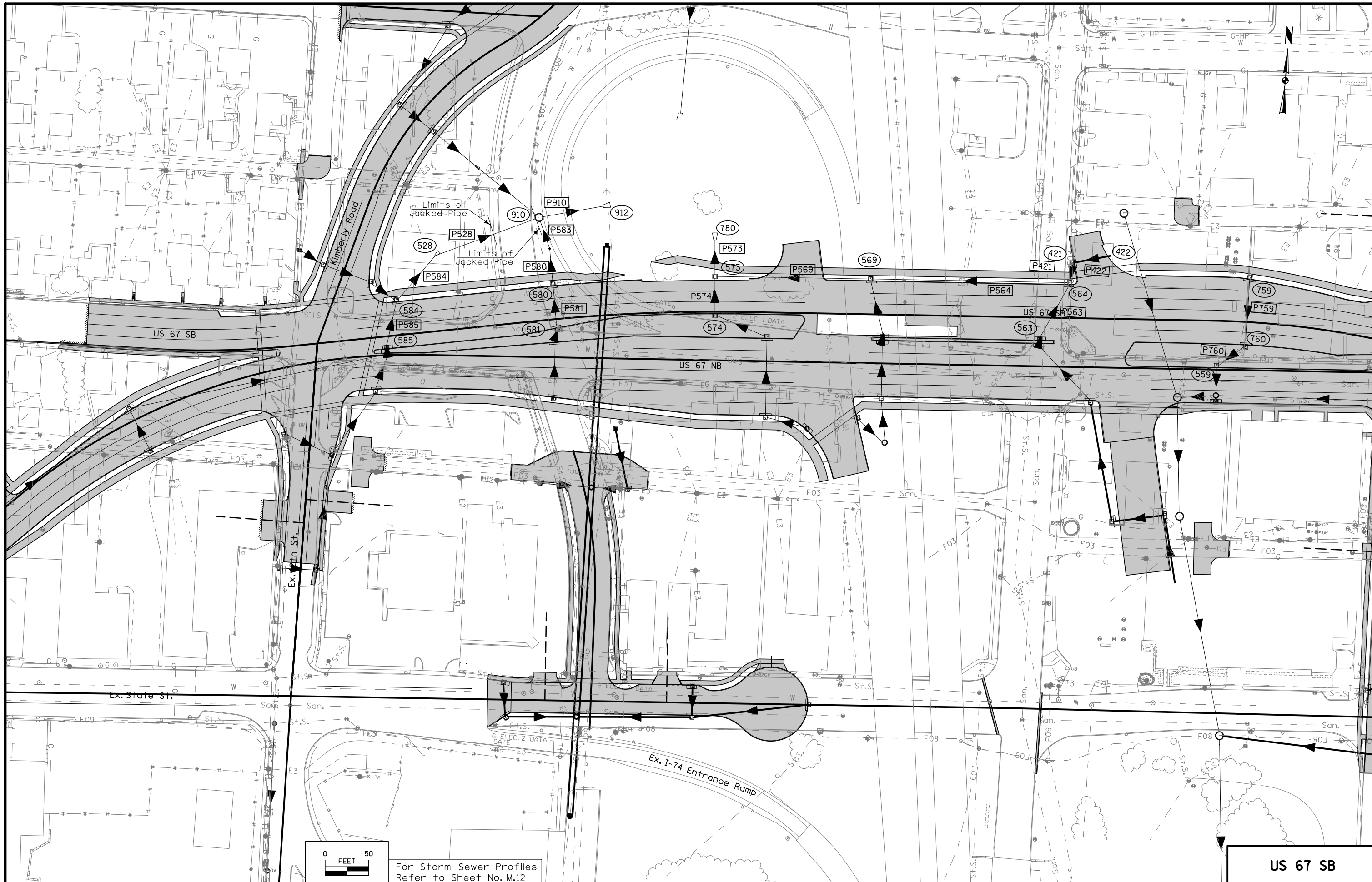


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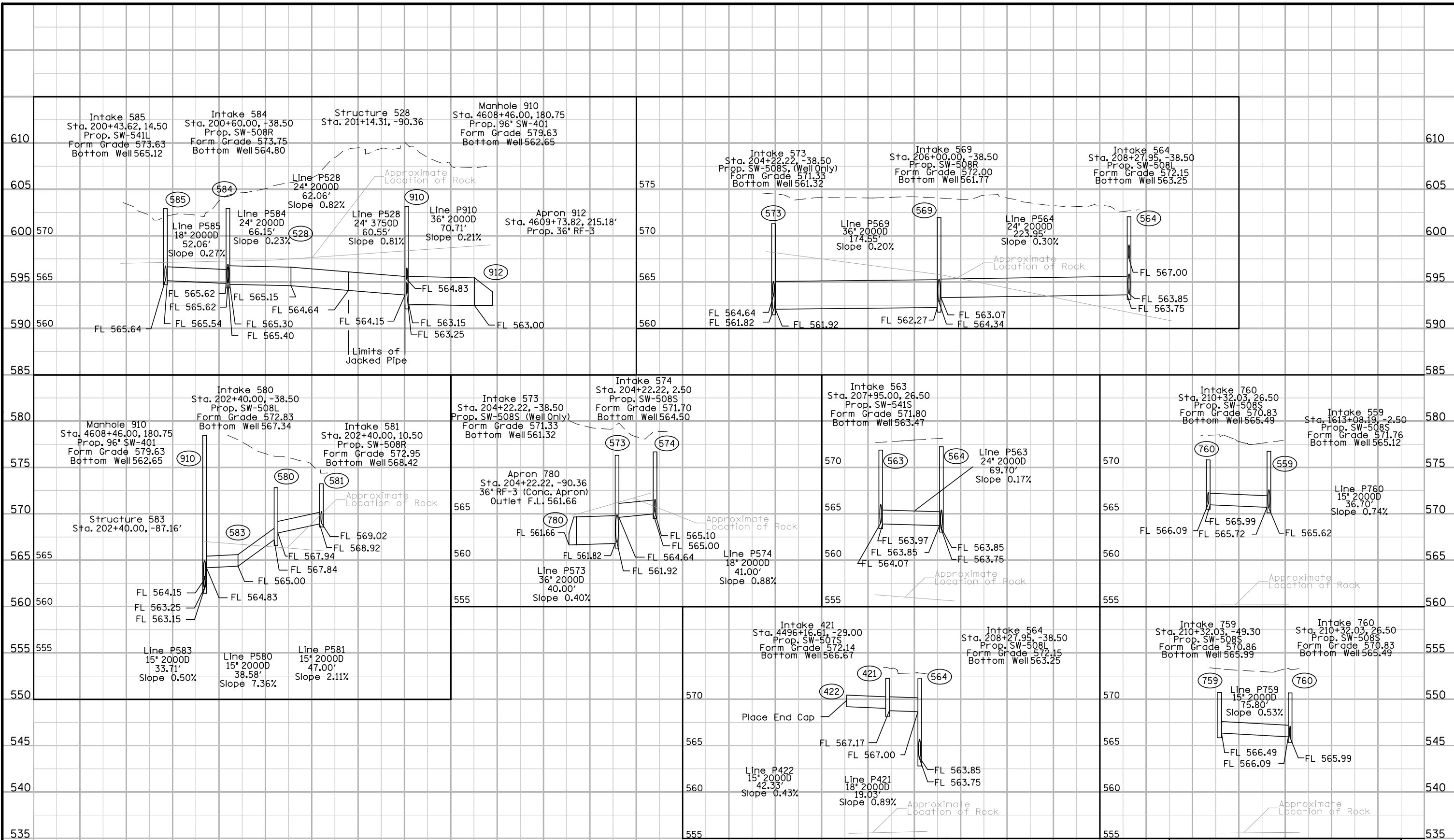
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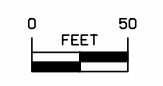
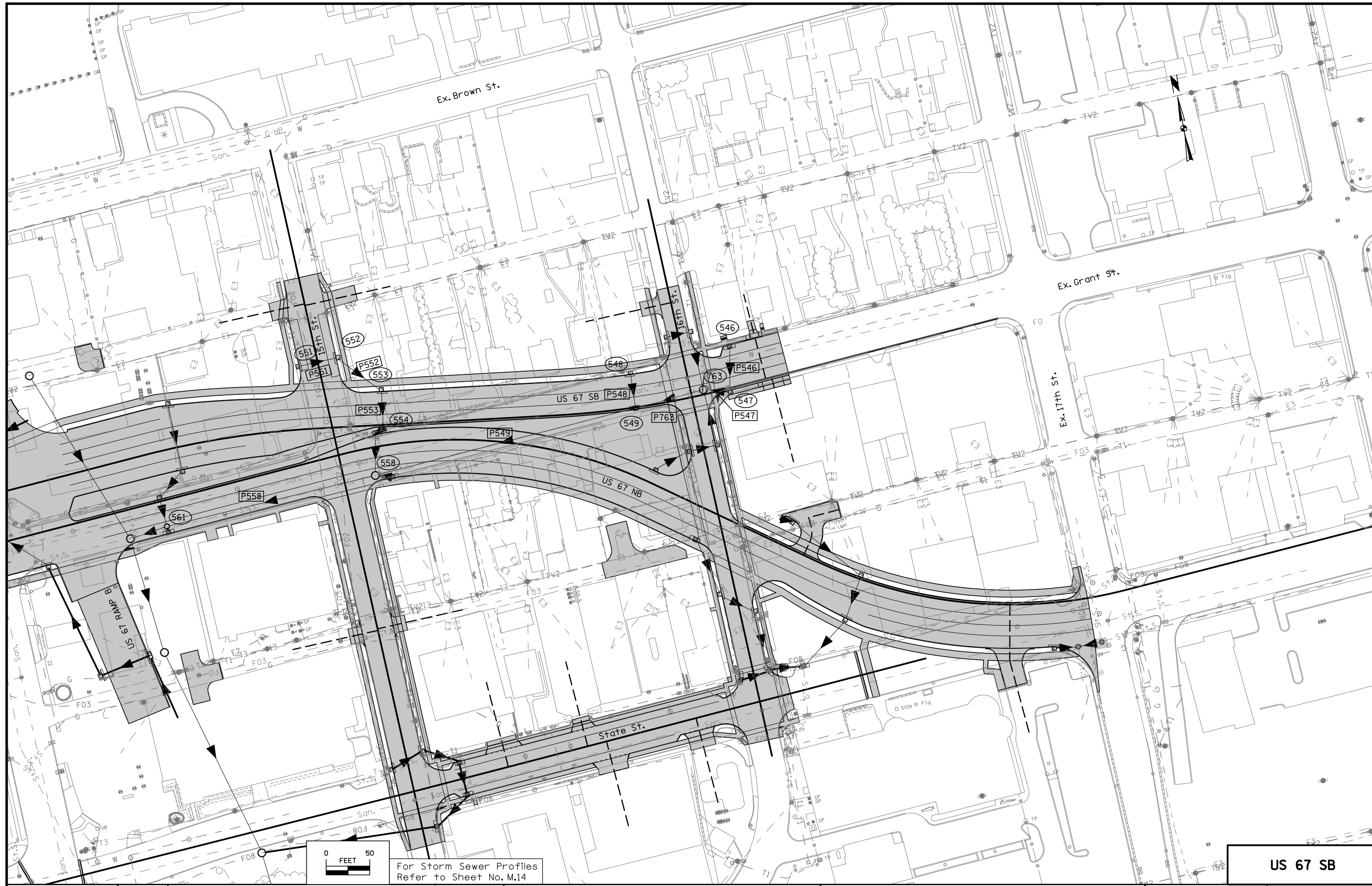
For Storm Sewer Profiles  
Refer to Sheet No. M.12

**US 67 SB**



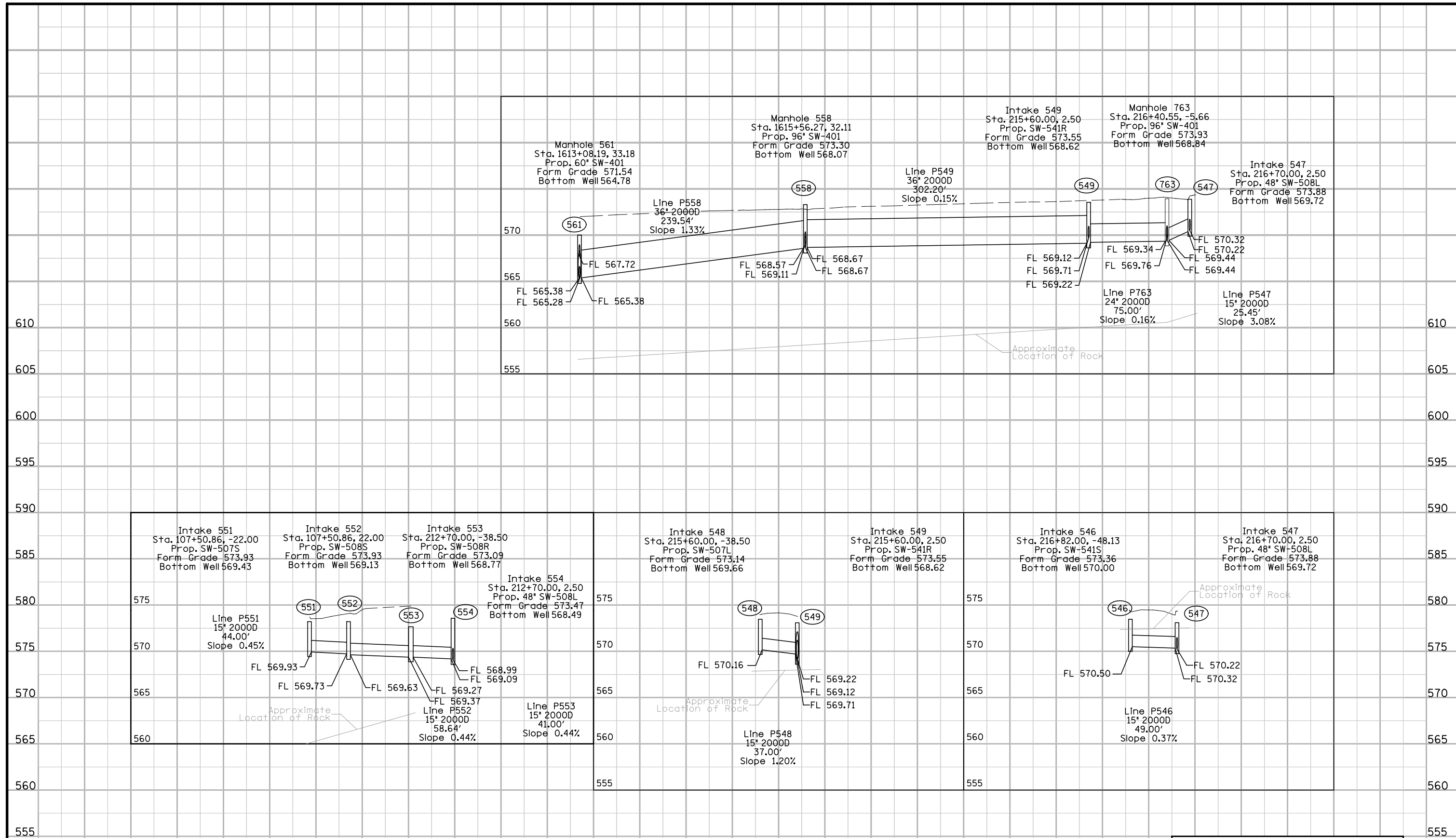
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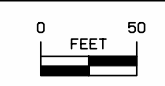
For Storm Sewer Profiles  
Refer to Sheet No. M.14

**US 67 SB**



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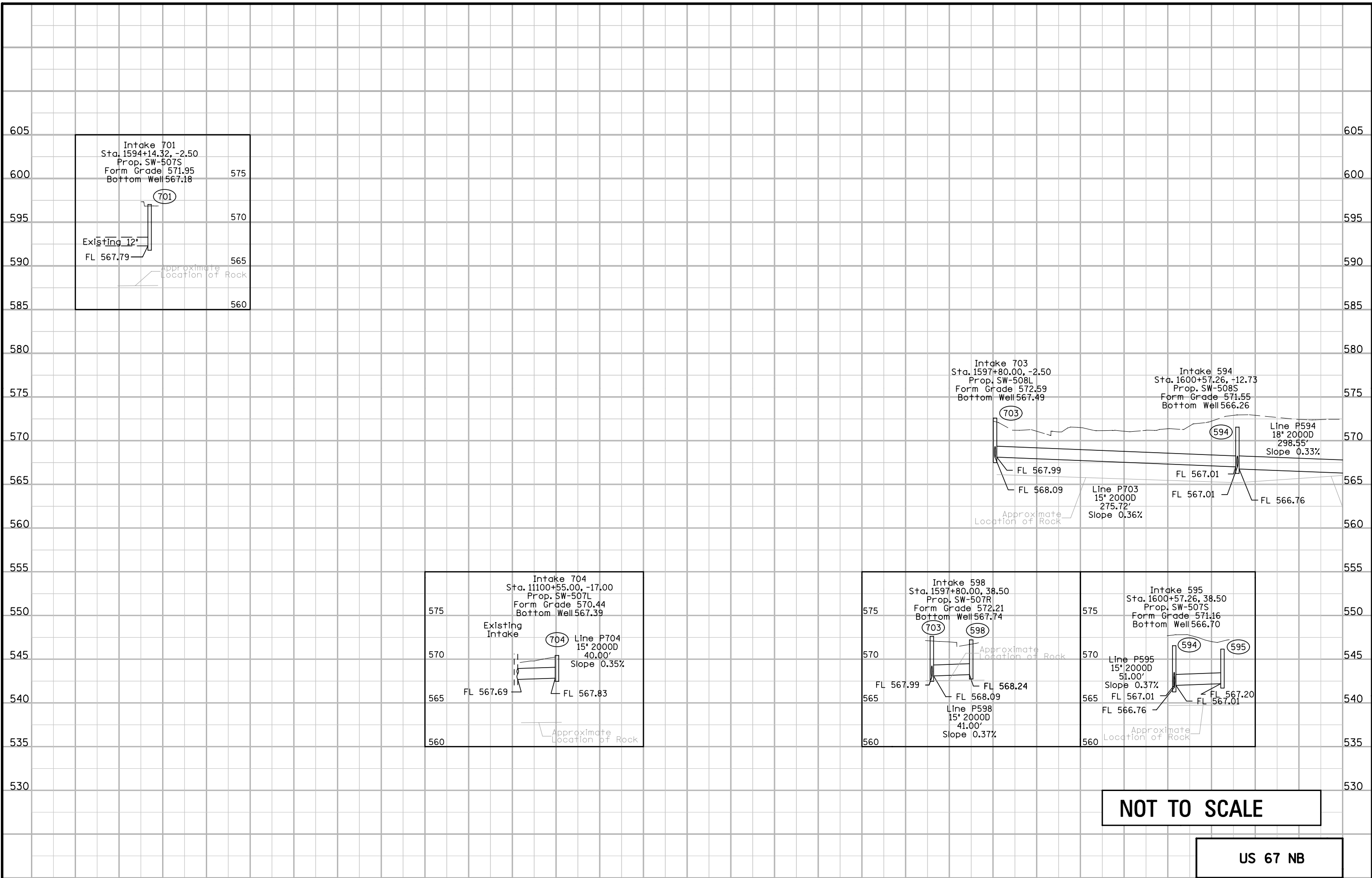
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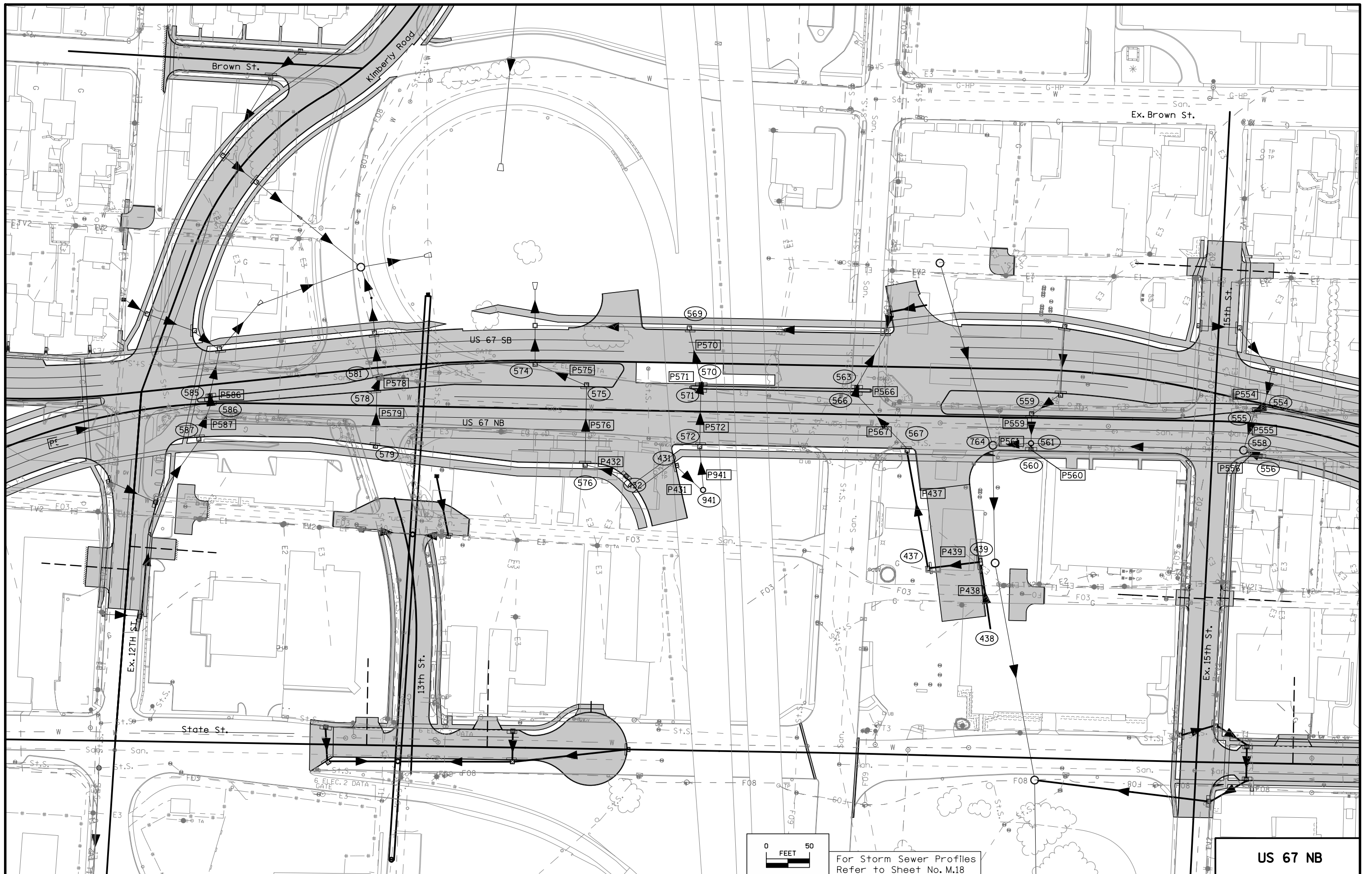


For Storm Sewer Profiles  
Refer to Sheet No. M.16

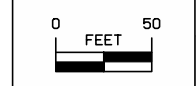
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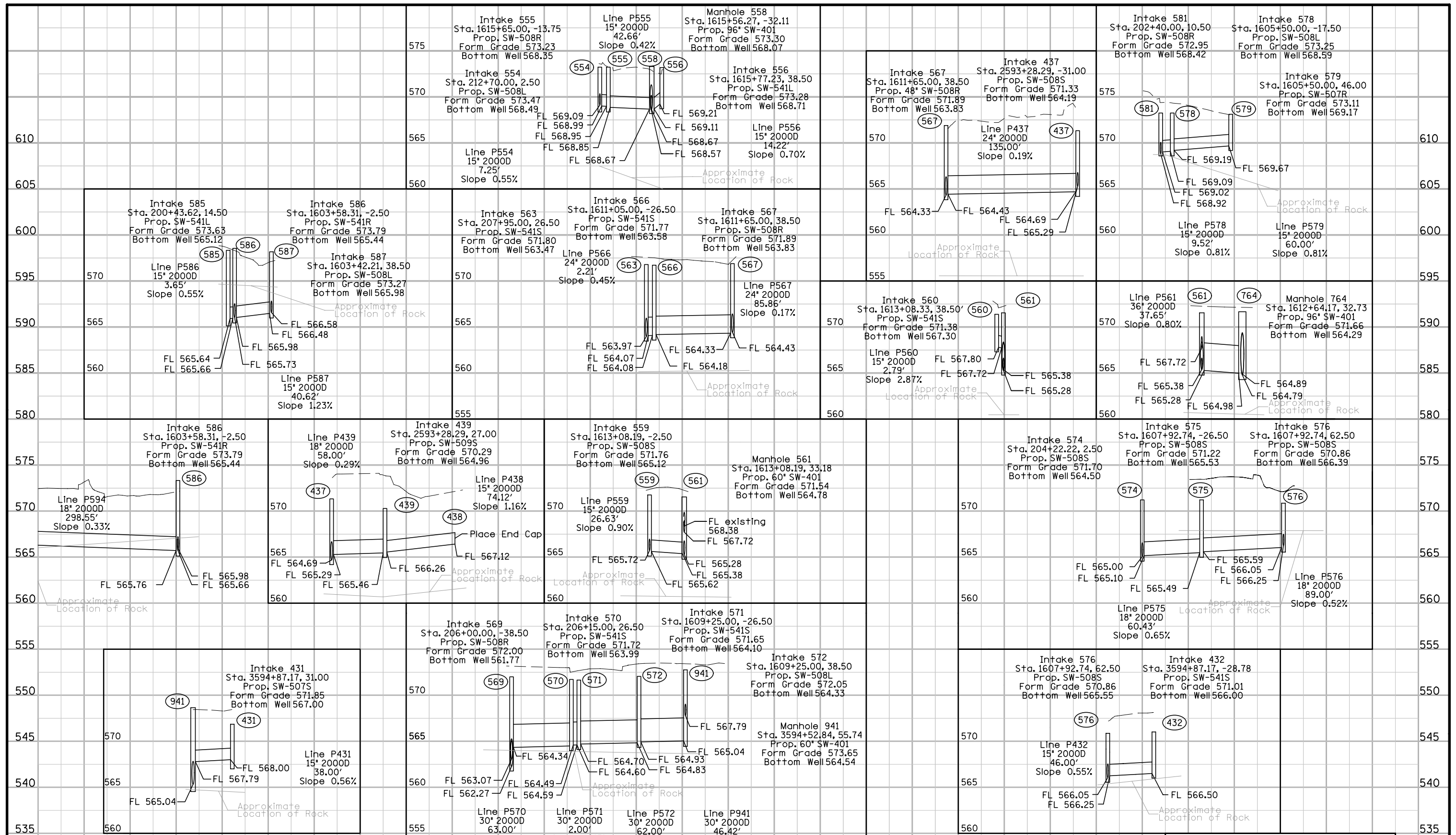




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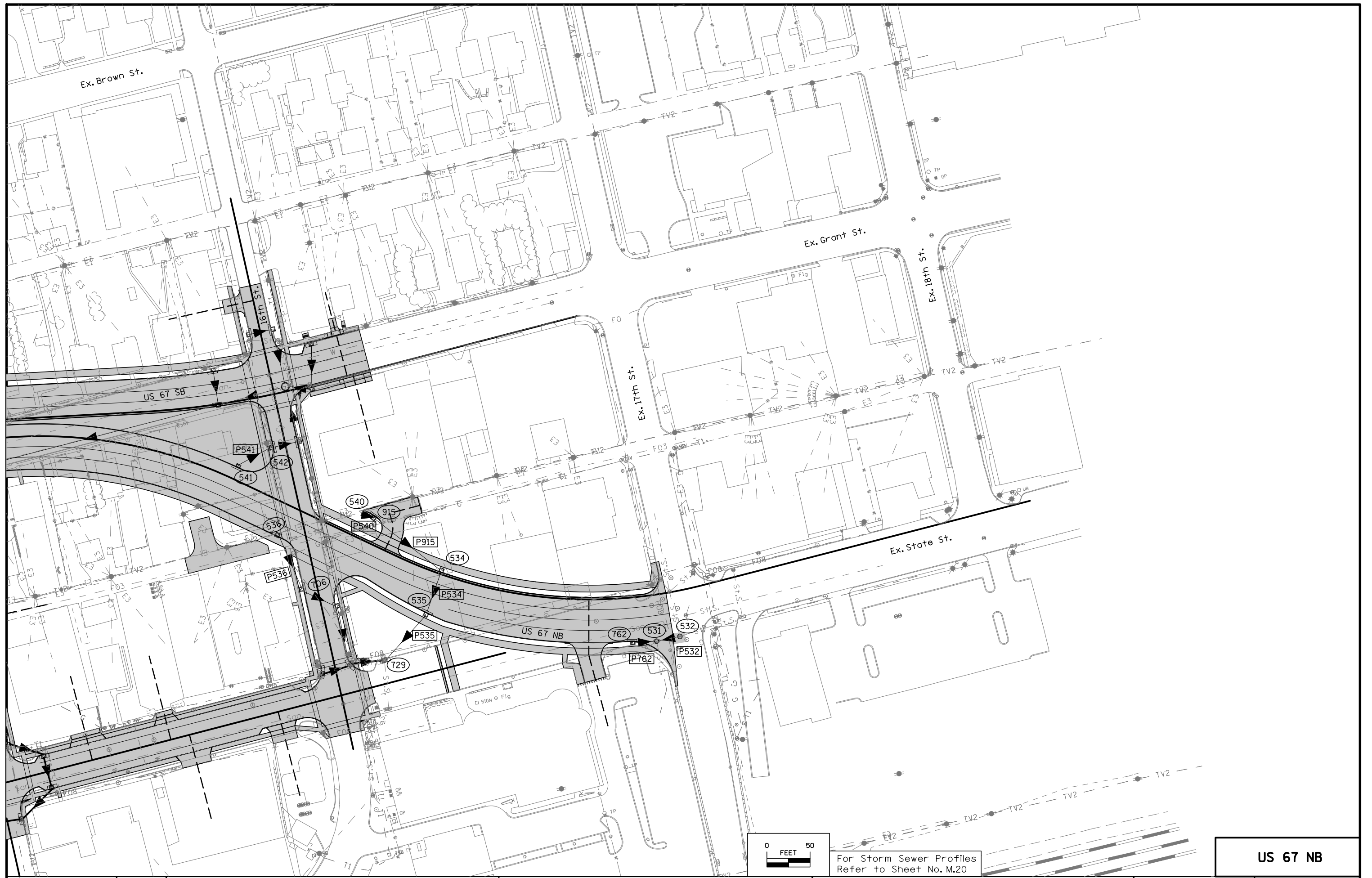


For Storm Sewer Profiles  
Refer to Sheet No. M.18

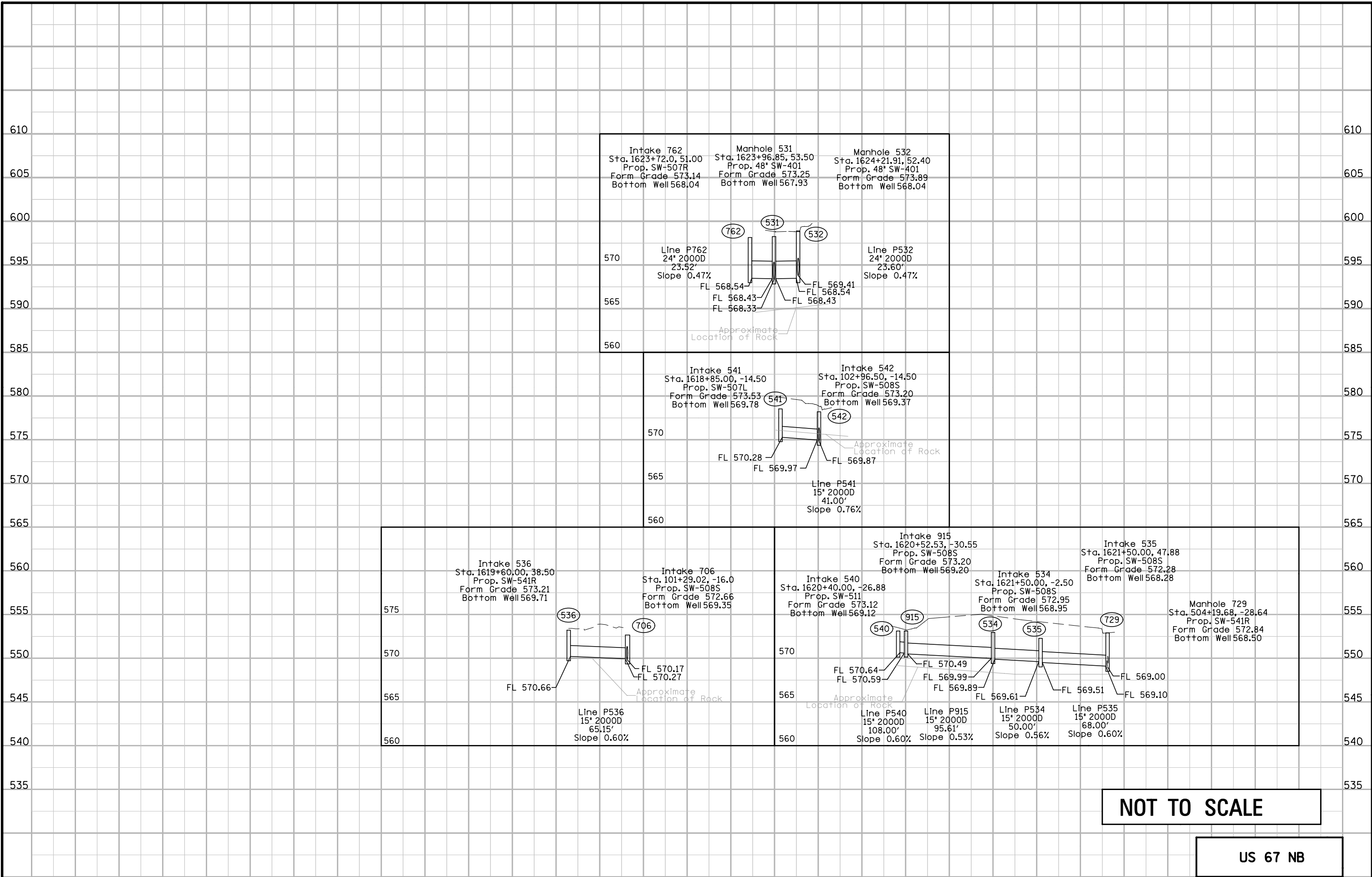


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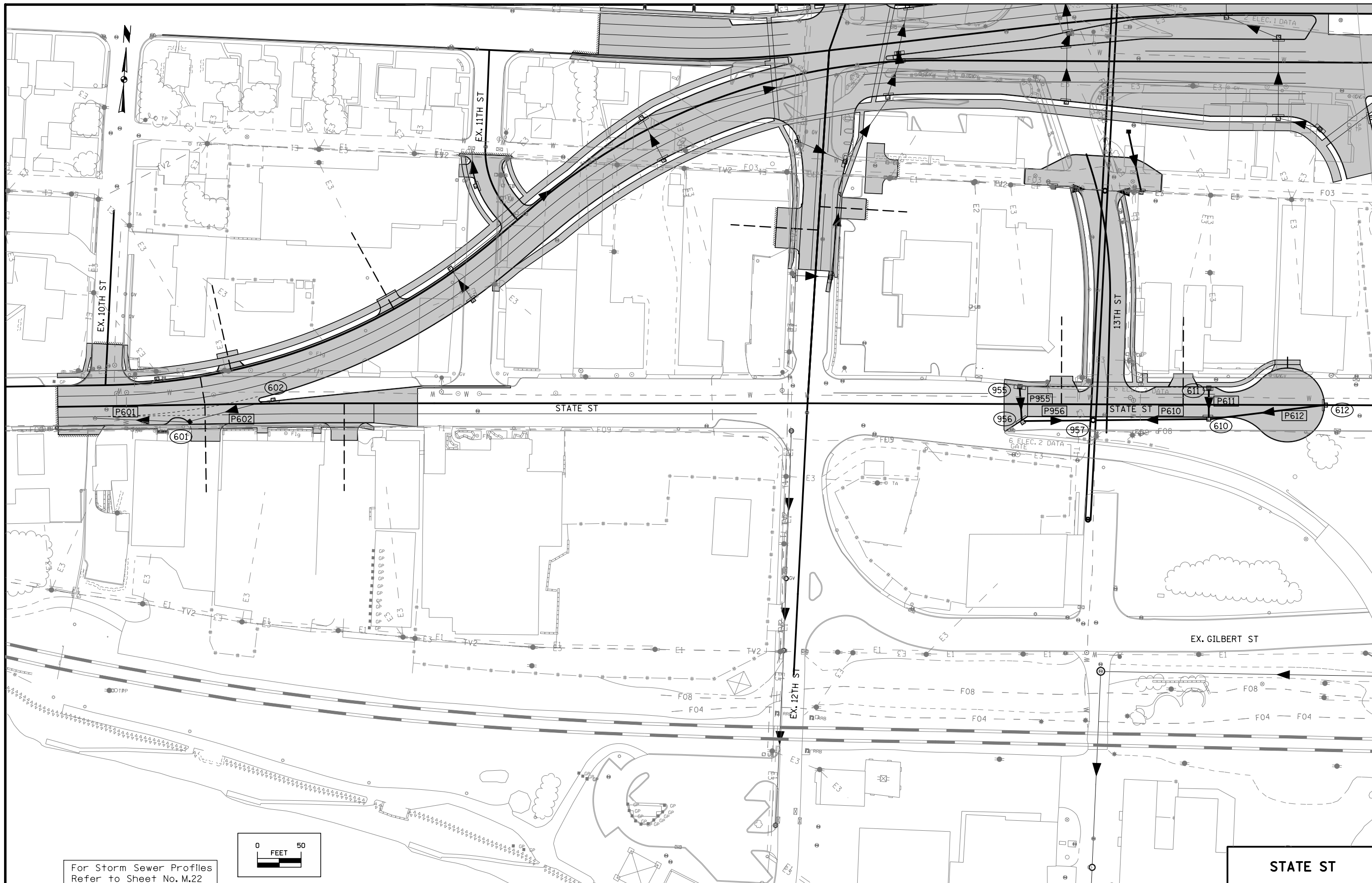


For Storm Sewer Profiles  
Refer to Sheet No. M.20

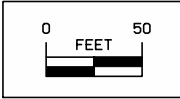


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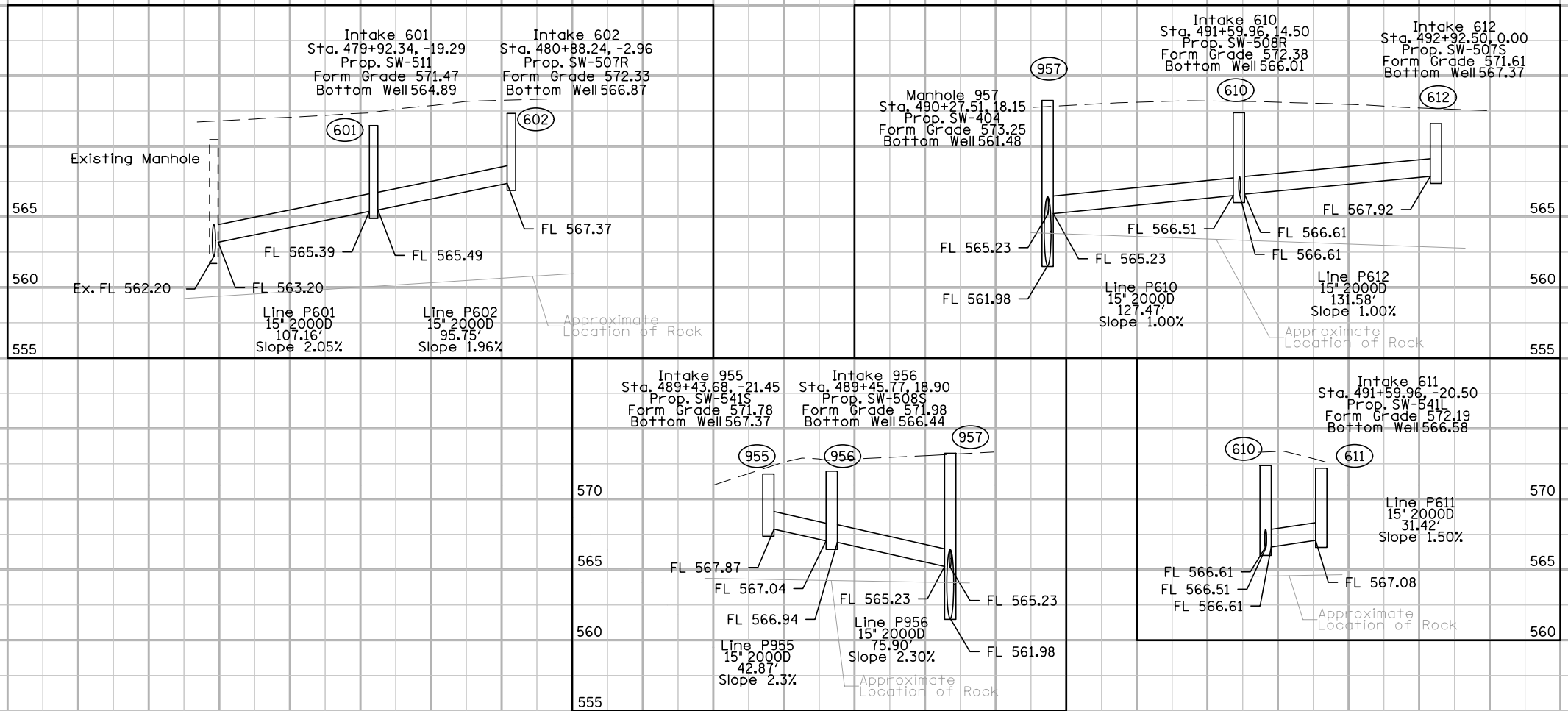
**US 67 NB**



For Storm Sewer Profiles  
Refer to Sheet No. M.22

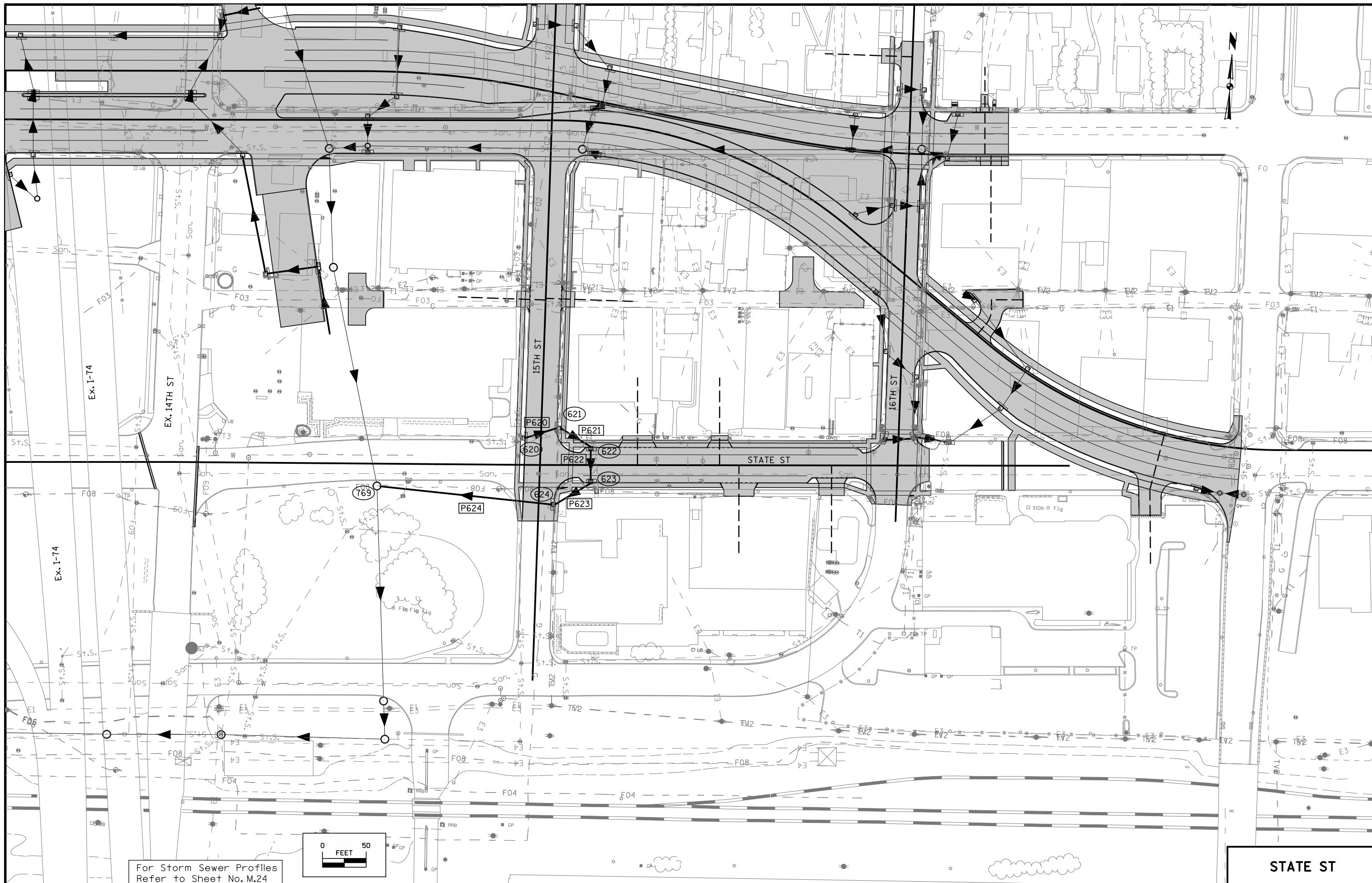


**STATE ST**

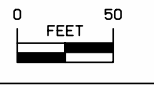


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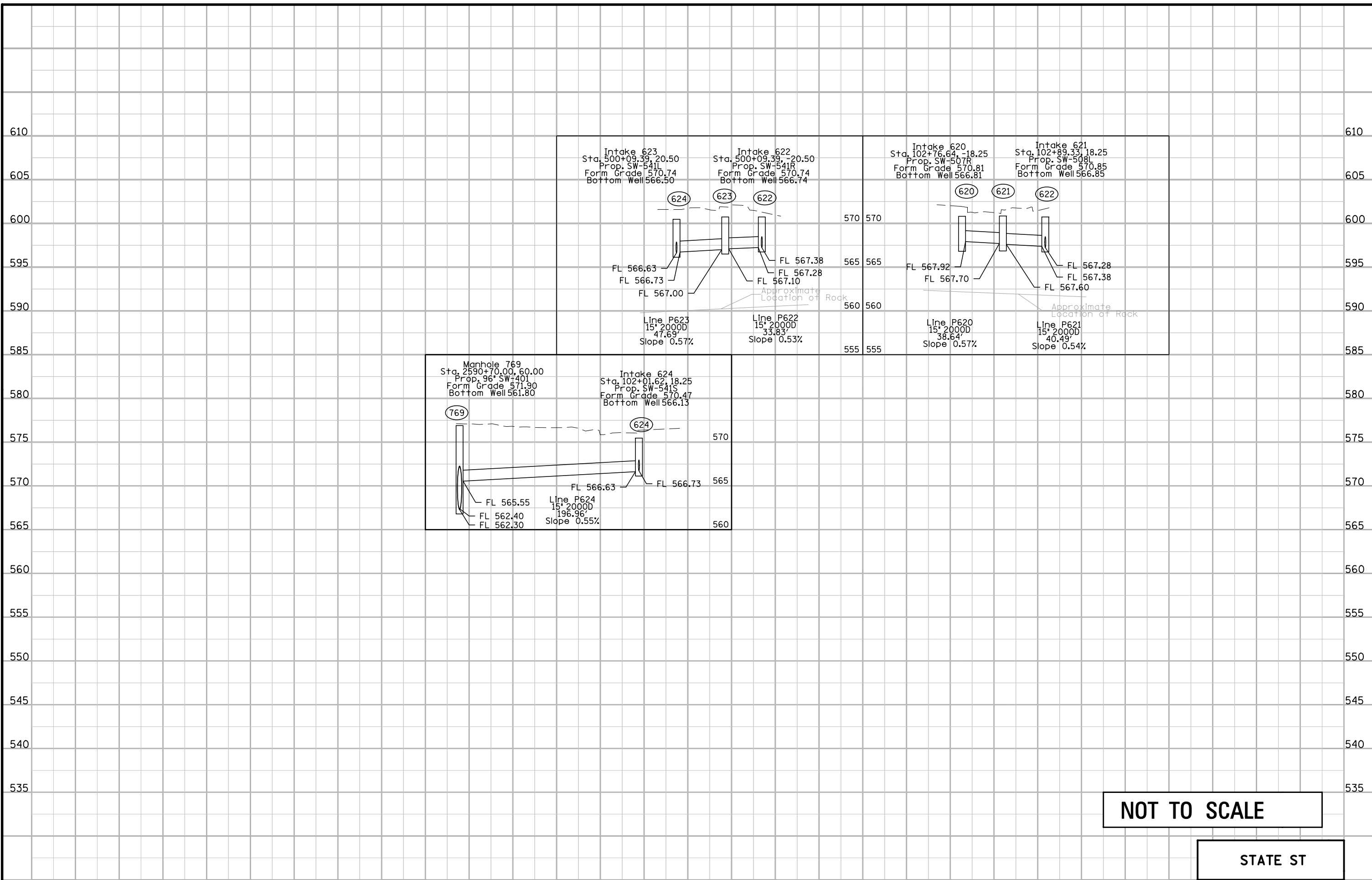


For Storm Sewer Profiles  
Refer to Sheet No. M.24



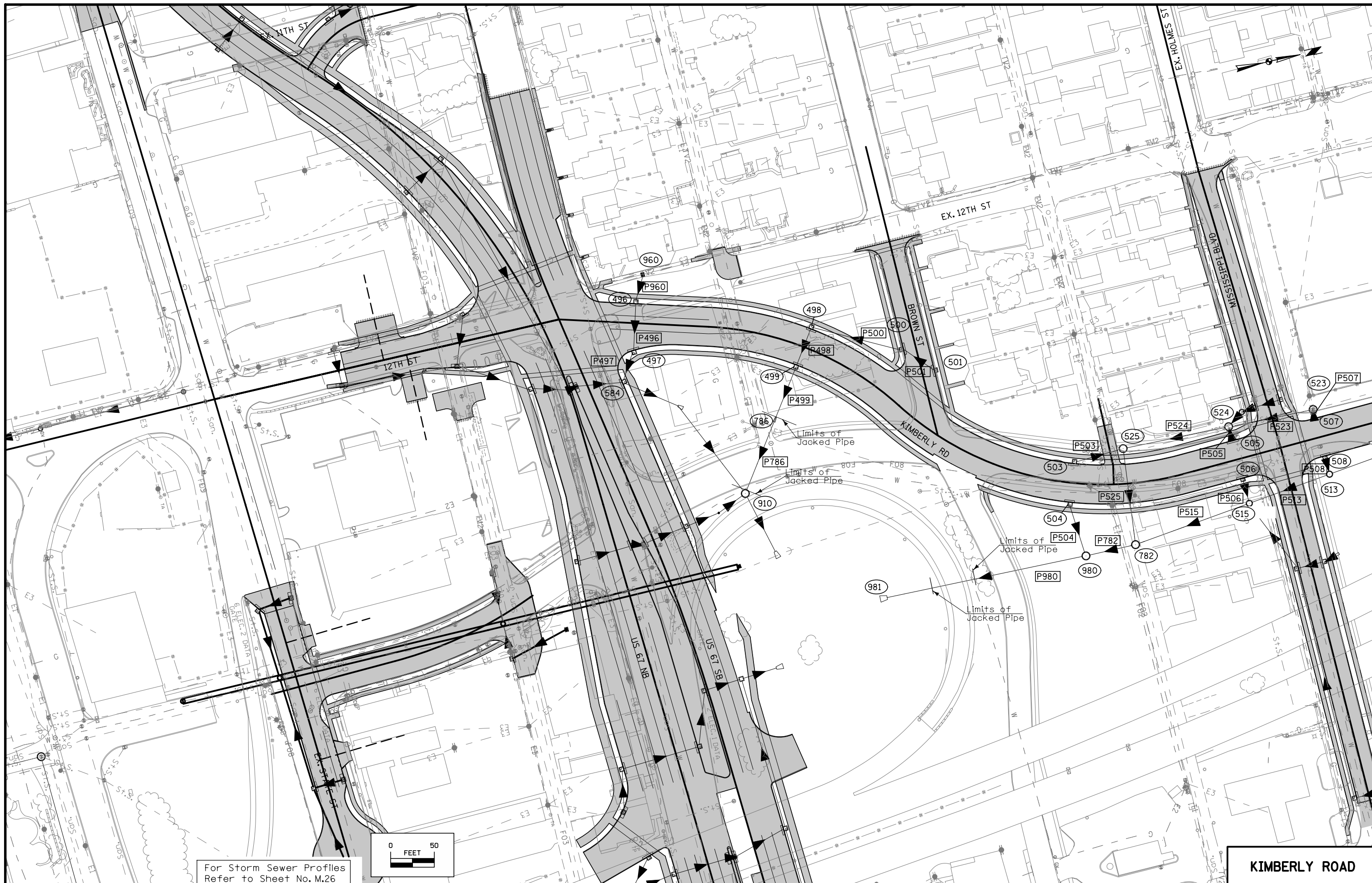
STATE ST



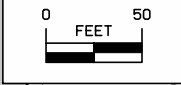


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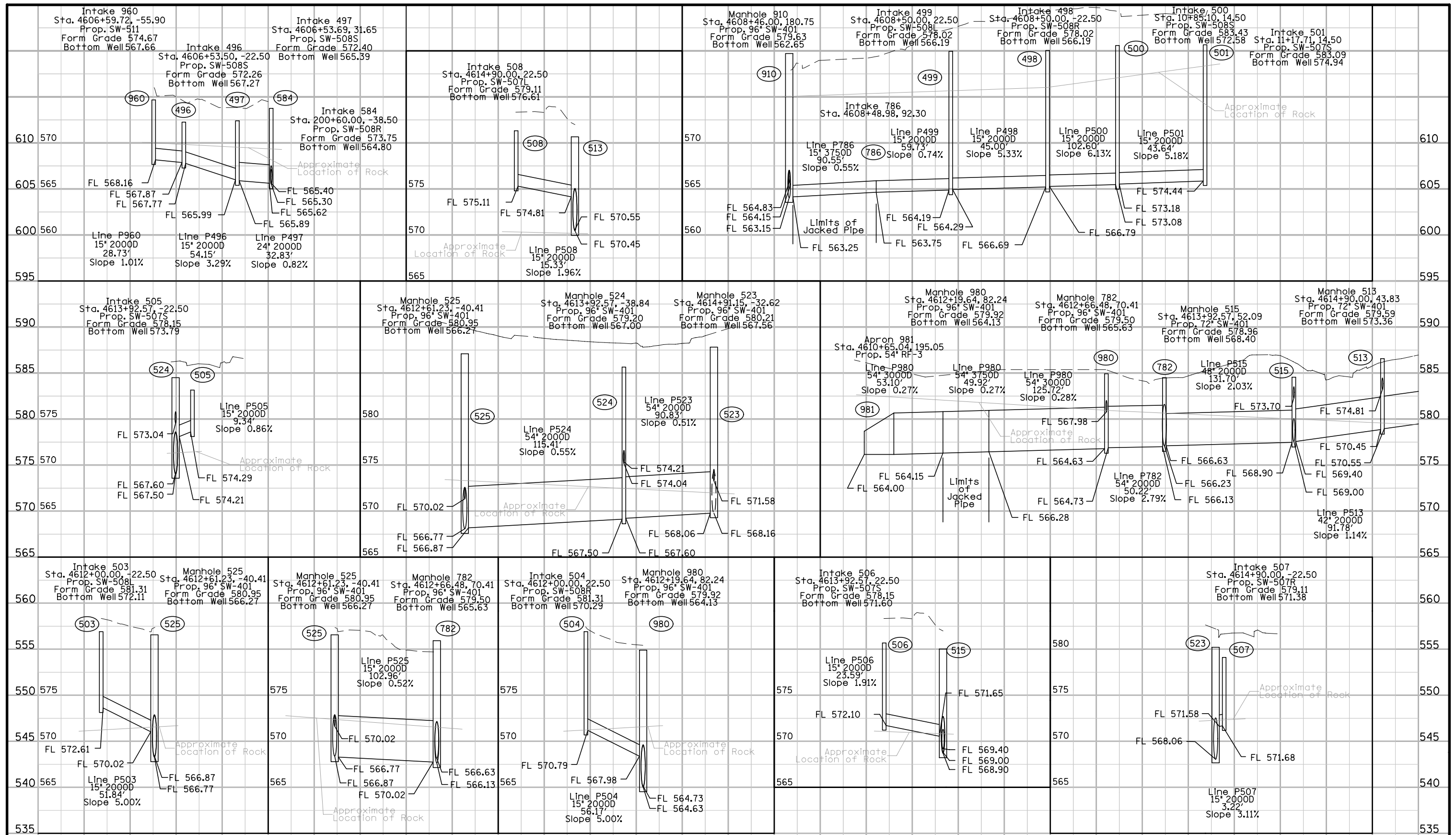
**STATE ST**



For Storm Sewer Profiles  
Refer to Sheet No. M.26



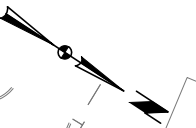
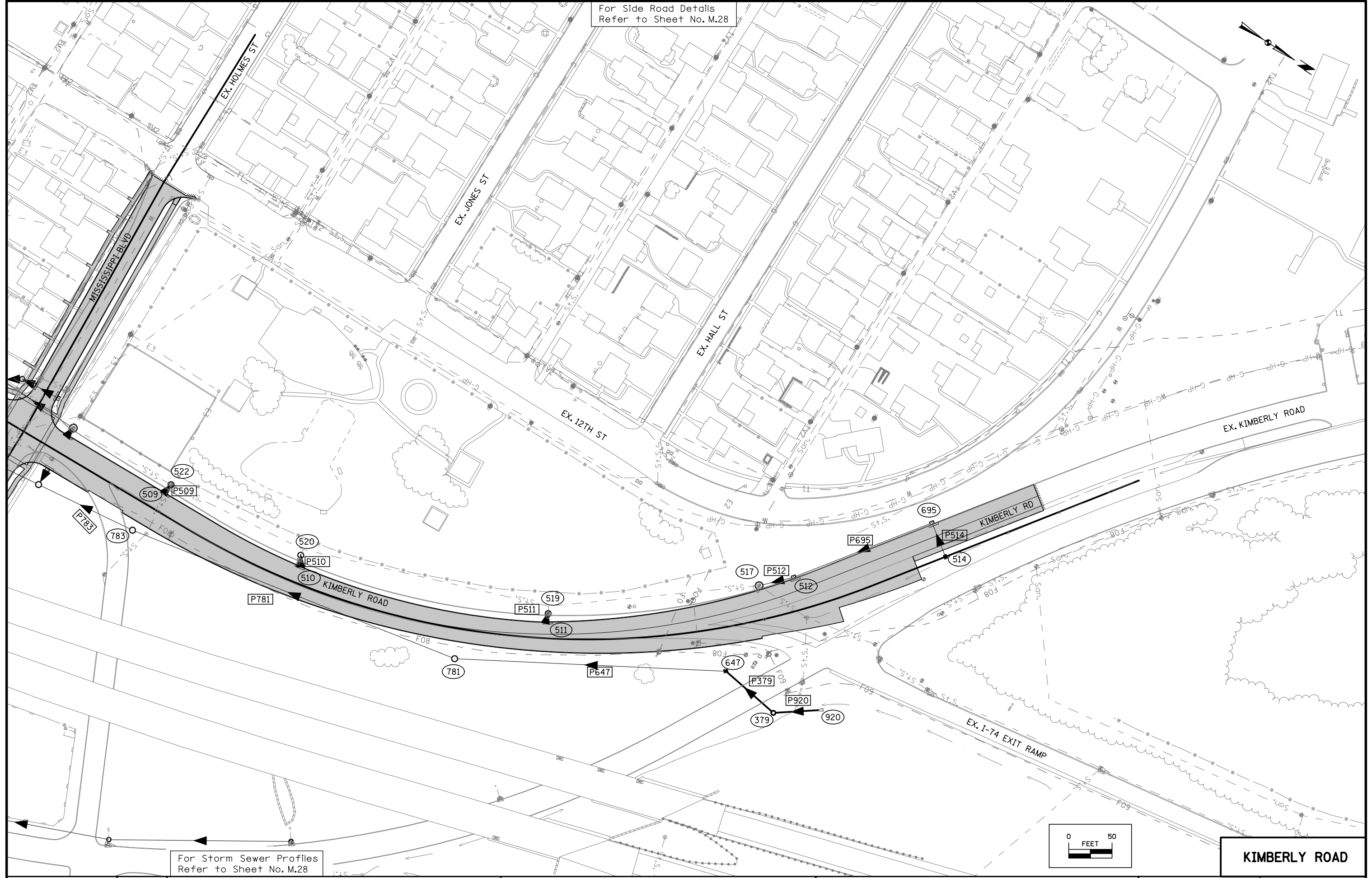
**KIMBERLY ROAD**



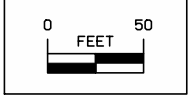
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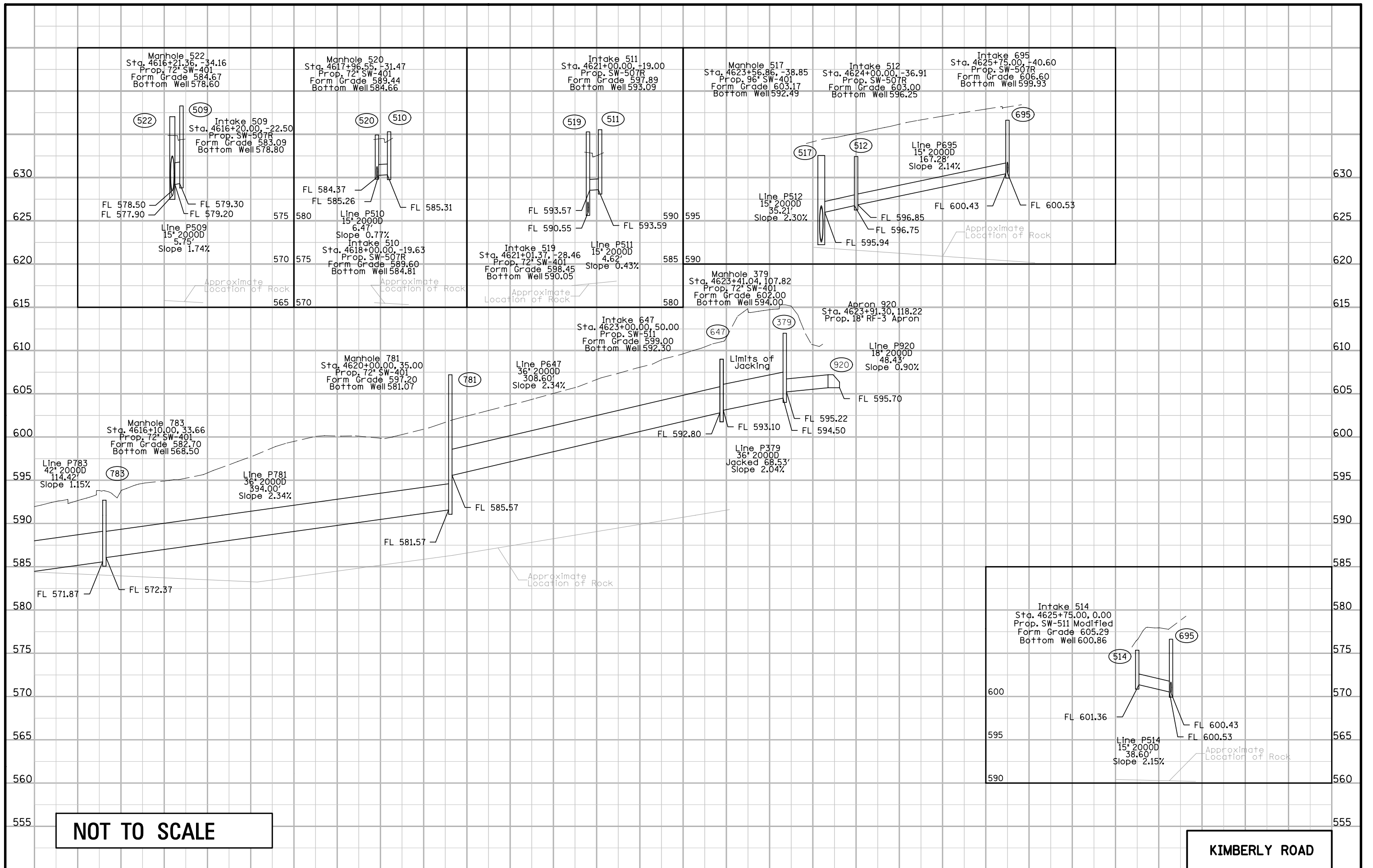
For Side Road Details  
Refer to Sheet No. M.28



For Storm Sewer Profiles  
Refer to Sheet No. M.28

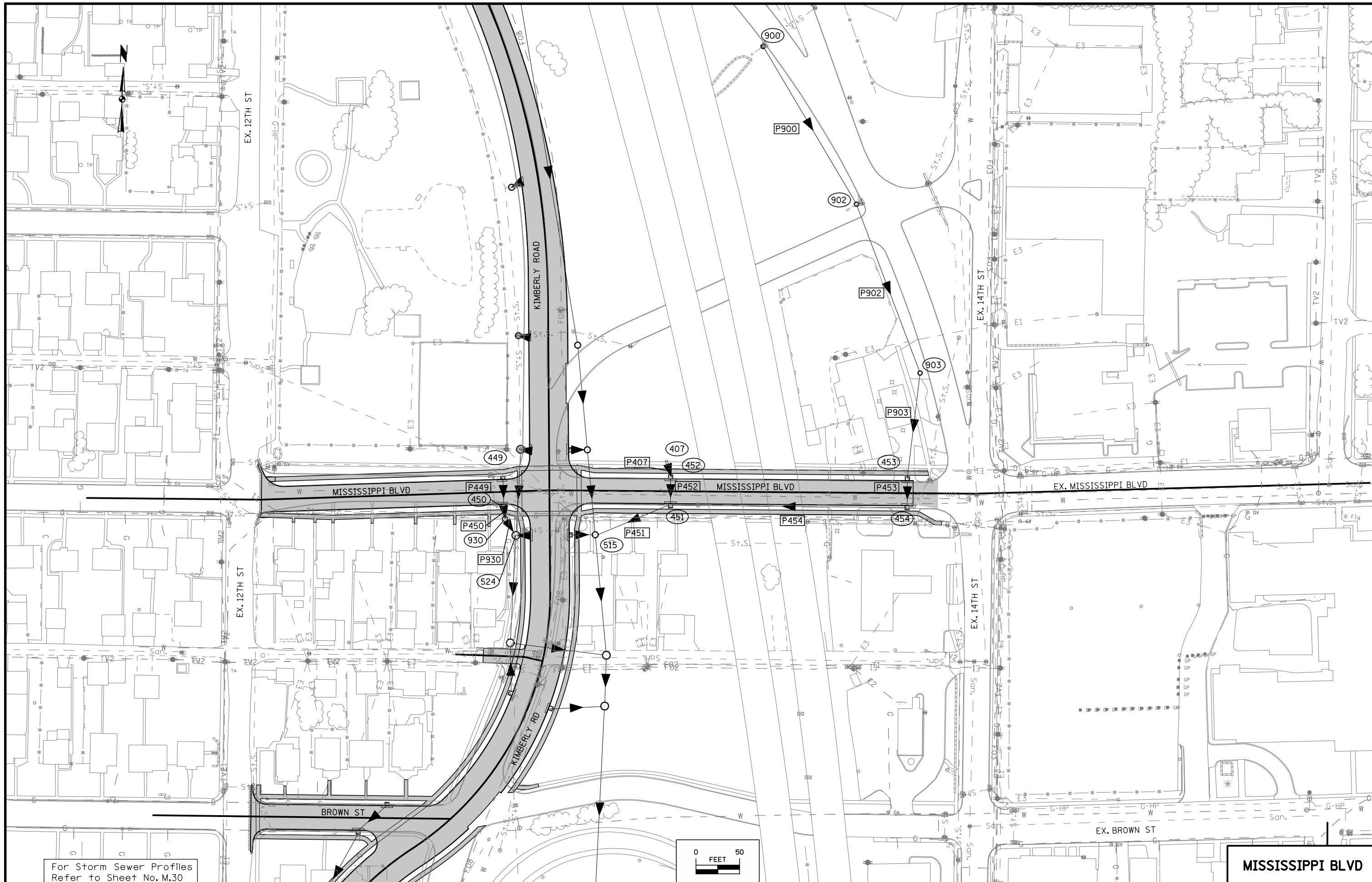


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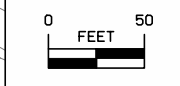


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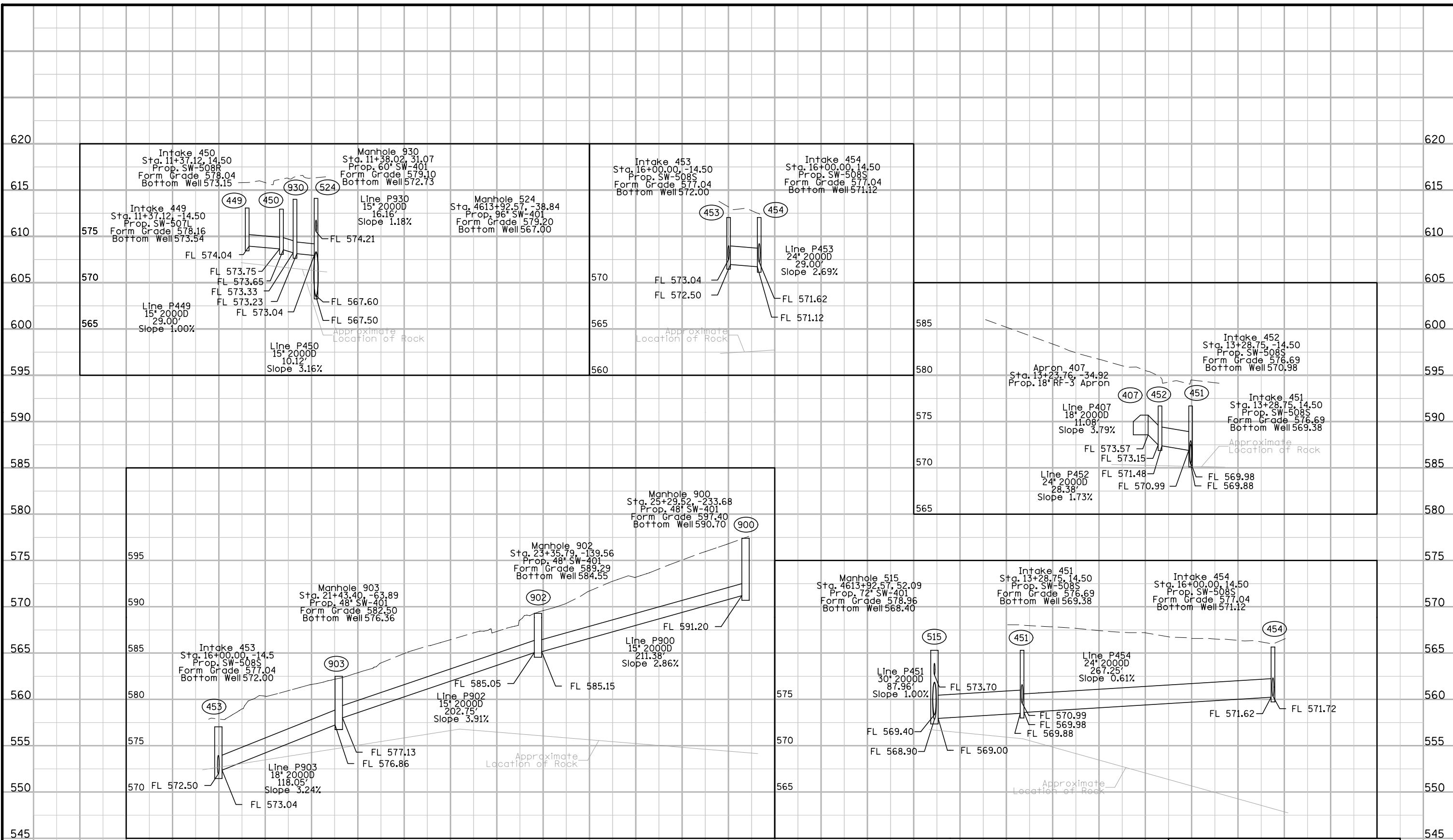
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For Storm Sewer Profiles  
Refer to Sheet No. M.30



**MISSISSIPPI BLVD**

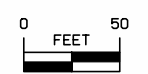


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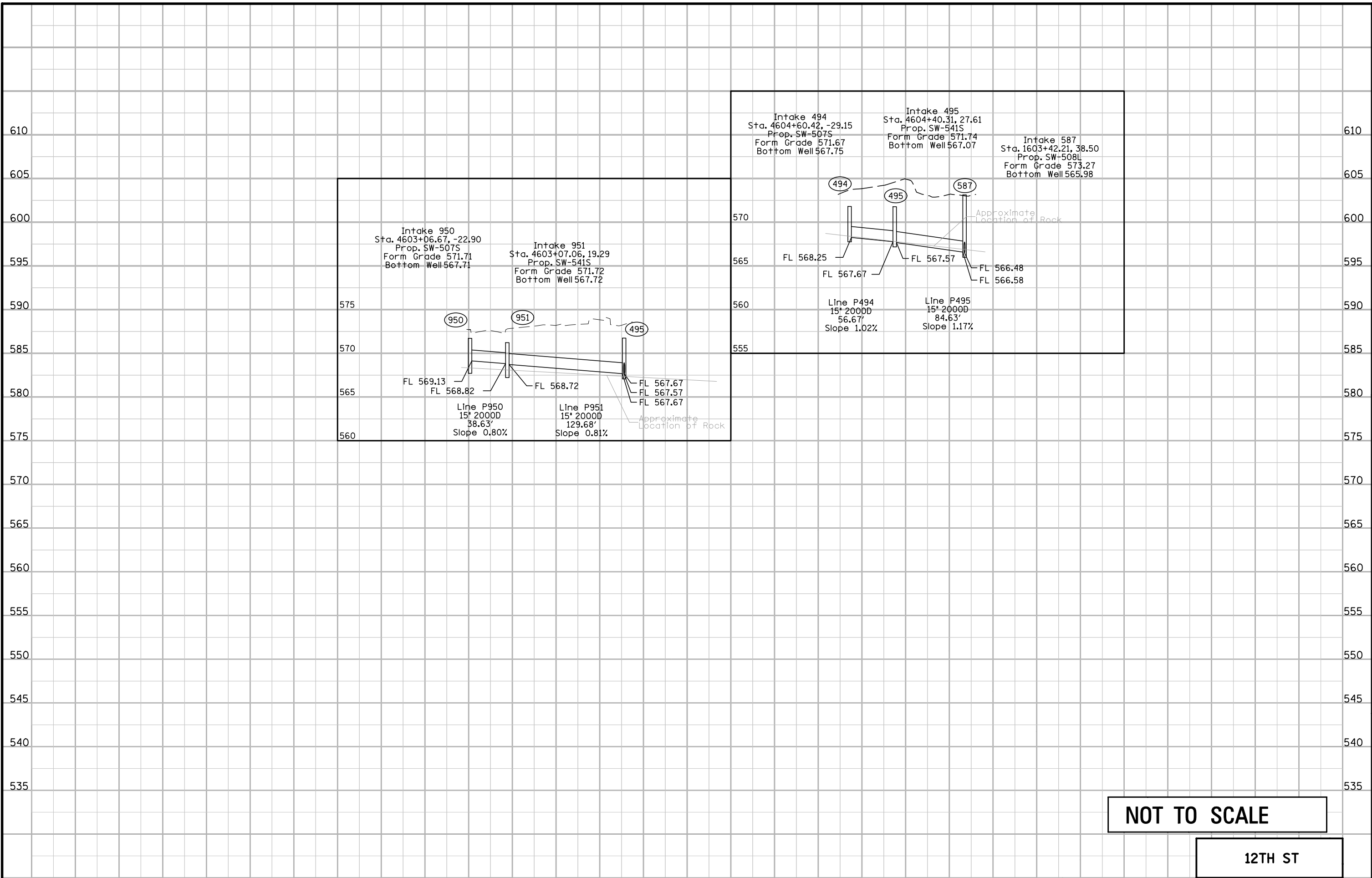


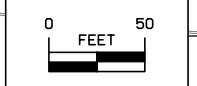
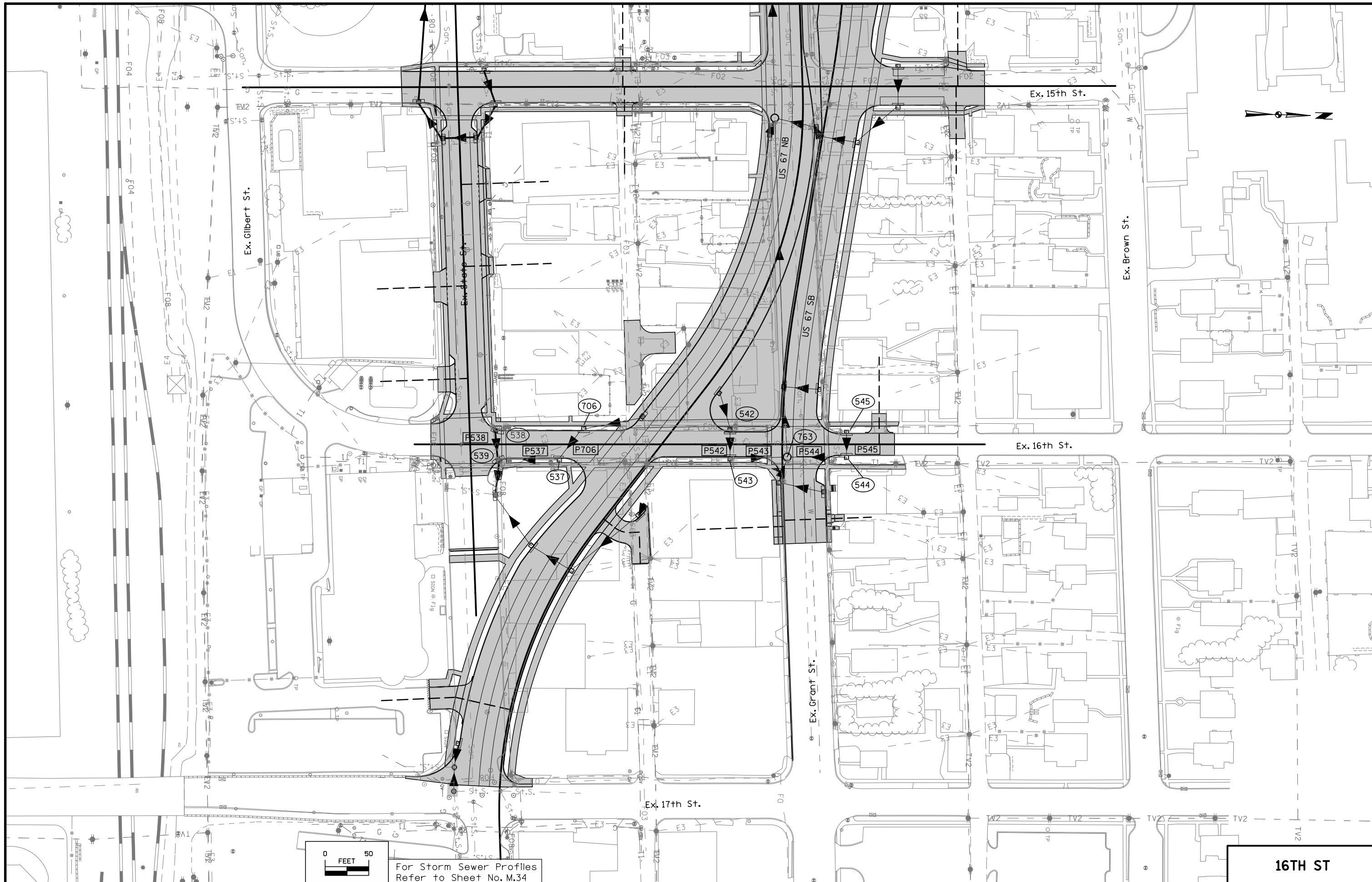
12TH ST



For Storm Sewer Profiles  
Refer to Sheet No. M.32

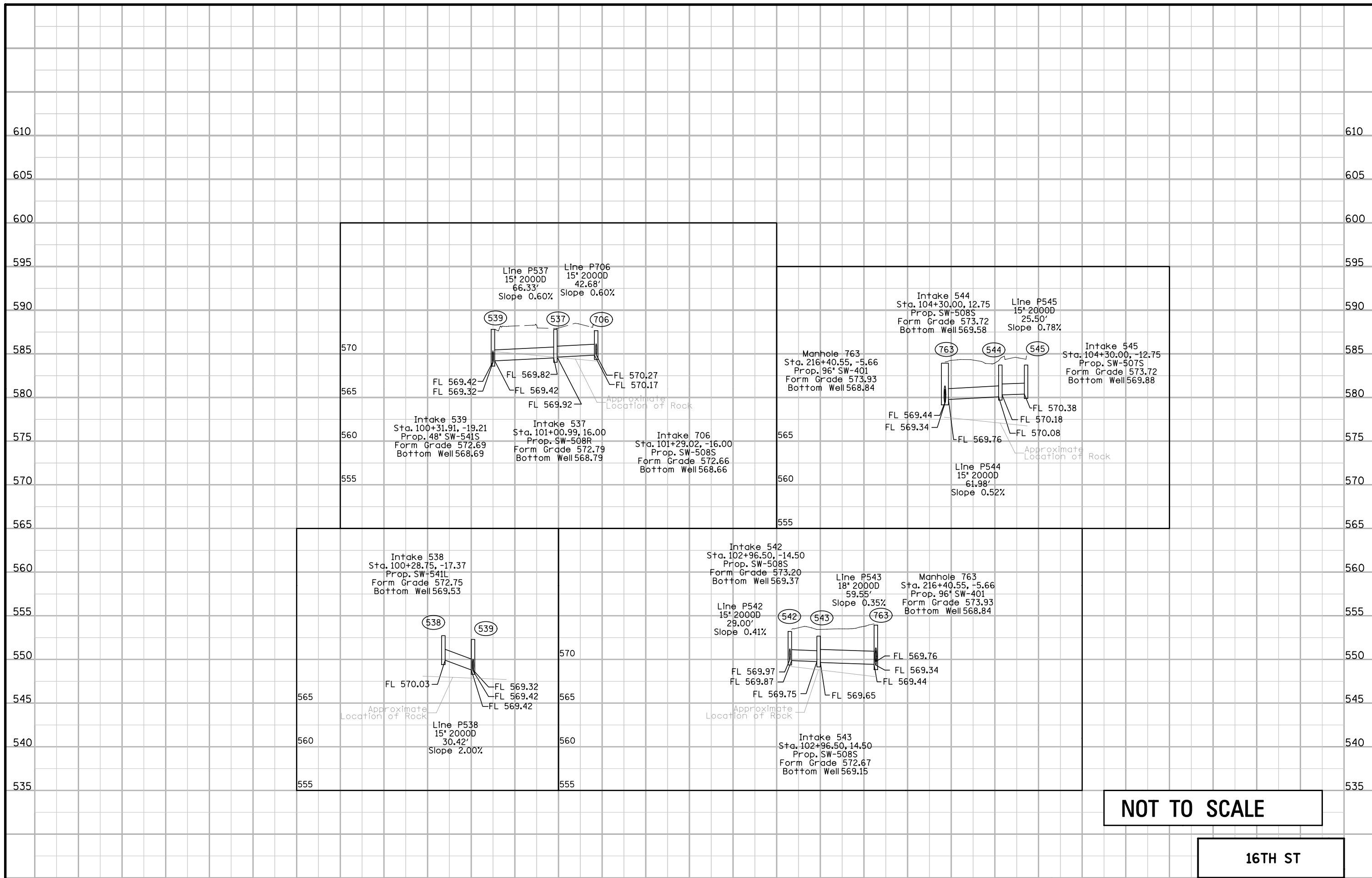






For Storm Sewer Profiles  
Refer to Sheet No. M.34

**16TH ST**



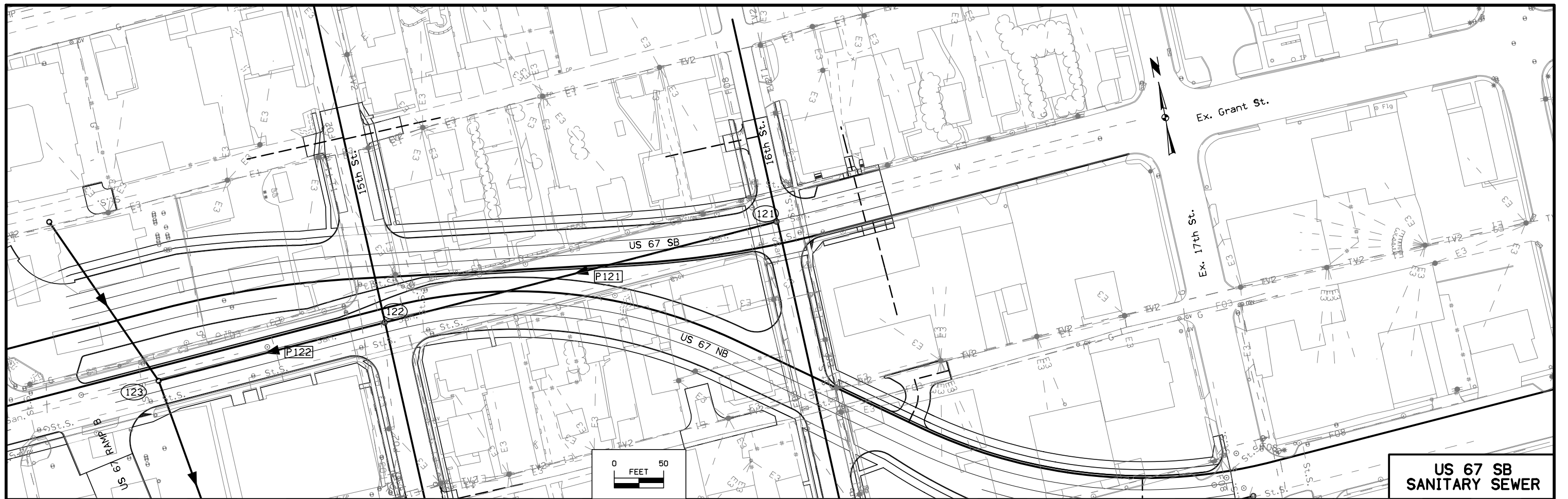
**NOT TO SCALE**

**16TH ST**

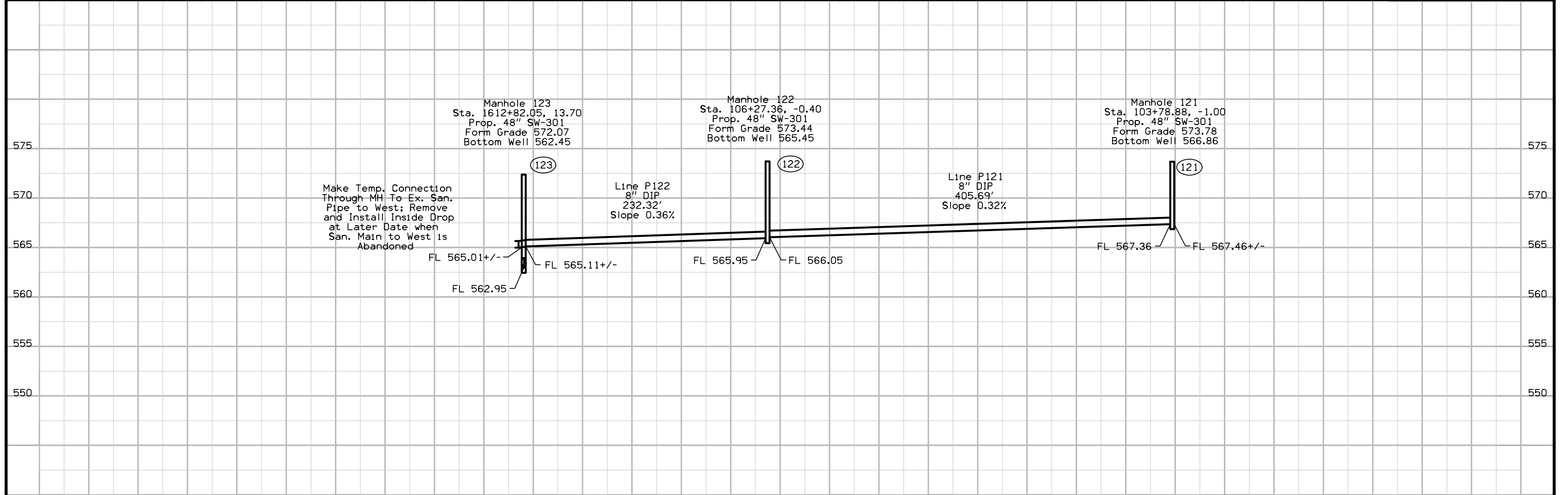
**SANITARY SEWER**

\* Bid Item

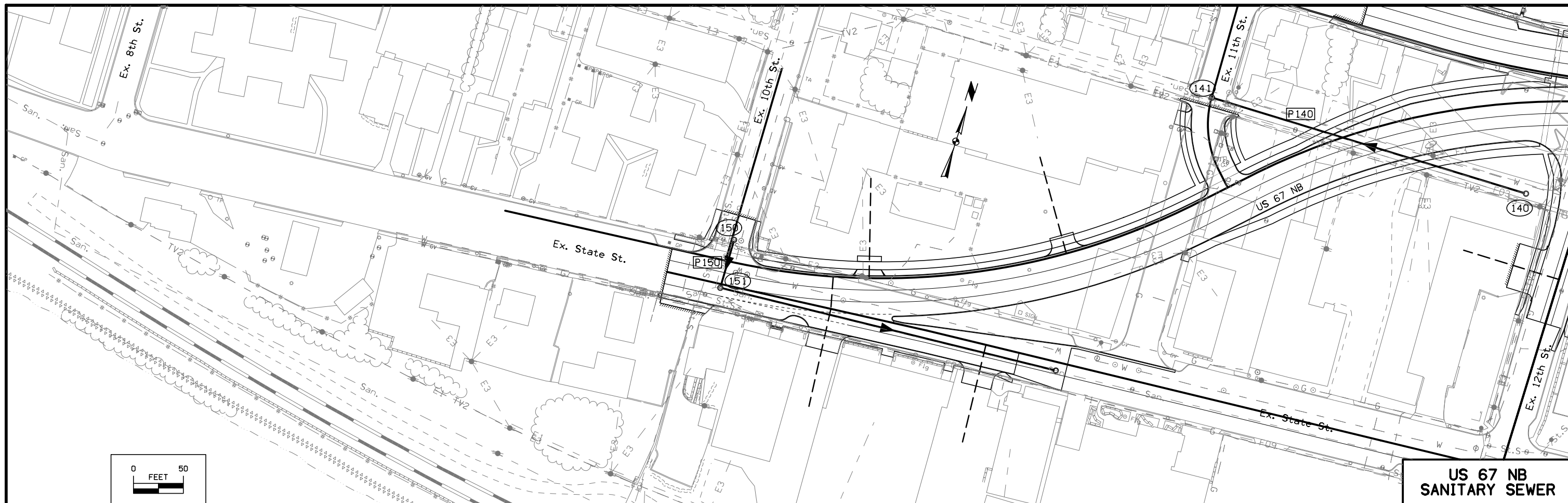
UTILITY ACCESSES						PIPES													
						Design Length, Slope, and Flowlines are calculated from inside wall to inside wall along CL of pipe. An additional 6 ft length is added to Design Length to account for estimated length to center of structures.													
No.	Location Station and Offset	*Type or Standard Road Plan	Form	Bottom	Extension Length	Notes	Line Number	Utility Access No.		Type of Pipe	Pipe Diameter	Bid* Length	Design Length	Slope %	Flow Lines			Pipe Profile Sheet No.	Notes
			Grade	Well				From	To						Inlet Elevation	Outlet Elevation	Other Elevation		
			Elev.	Elev.	FT						IN	FT	FT						
110	4614+78.29, 89.19' Lt.	48" SW-301	581.67	574.36		Kimberly Rd. Div. (2)	P110	110	111	DIP	8	134.11	128.11	0.4	574.9	574.3		M.40	Div. (2)
111	4614+42.88, 38.09' Rt.	48" SW-301	578.09	573.72		Kimberly Rd. Div. (2)	P111	111	113	DIP	8	192.55	186.55	0.3	574.2	573.7		M.40	Div. (2)
						Kimberly Rd. Div. (2)	P201	EX.	113	DIP	8	111.36	105.36		EX.	573.7		M.40	Connect to Ex. San. Pipe Div. (2)
						Kimberly Rd. Div. (2)	P201	EX.	113	DIP	8	111.36	105.36		EX.	573.7		M.40	Connect to Ex. San. Pipe Div. (2)
113	4612+60.19, 42.56' Rt.	48" SW-301	579.52	573.11		Kimberly Rd. Div. (2)	P113	113	114	DIP	10	150.49	144.49	2.3	573.4	570.1		M.40	Div. (2)
114	4611+26.69, 40.74' Rt.	48" SW-301	584.53	569.53		Kimberly Rd. Div. (2)	P114	114	115	DIP	10	171.96	165.96	1.3	570.0	567.8		M.40	Pipe to be Jacked Div. (2)
115	4609+68.83, 10.94' Rt.	48" SW-301	582.66	567.24		Kimberly Rd. Div. (2)	P115	115	116	DIP	10	141.88	135.88	3.1	567.7	563.6		M.40	Div. (2)
116	4608+30.08, 12.03' Lt.	48" SW-301	577.38	562.95		Kimberly Rd. Div. (2)	P116	116	117	DIP	10	208.93	202.93	0.7	563.5	562.1		M.40	Div. (2)
117	4606+25.23, 0.00' Rt.	48" SW-301	572.85	561.45		Kimberly Rd. Div. (2)	P117	117	118	DIP	10	189.07	183.07	0.7	562.0	560.7		M.40	Pipe to be Jacked Div. (2)
118	4604+37.16, 5.50' Lt.	48" SW-301	572.16	560.08		Kimberly Rd. Div. (2)	P118	118	119	DIP	10	342.21	336.21	0.7	560.6	558.3		M.41	Div. (2)
119	13102+88.47, 22.58 Rt.	48" SW-301	571.54	557.69		13th St. Div. (2)	P119	119	131	DIP	10	274.29	268.29	0.5	558.2	556.8		M.41	Div. (2)
120	209+04.92, 113.82' Lt.	48" SW-301	573.23	566.65		US 67 WB Div. (2)	P120	120	123	DIP	12	197.27	191.27	2.1	567.2	563.1		M.39	Div. (2)
121	103+78.88, 1.00' Lt.	48" SW-301	573.78	566.86		16th St. Div. (2)	P121	121	122	DIP	8	411.69	405.69	0.3	567.4	566.1		M.36	Div. (2)
122	106+27.36, 0.40' Lt.	48" SW-301	573.44	565.45		15th St. Div. (2)	P122	122	123	DIP	8	238.32	232.32	0.4	566.0	565.1		M.36	Extend Pipe West through MH Div. (2)
123	1612+82.05, 13.70' Rt.	48" SW-301	572.07	562.45		US 67 EB Div. (2)	P123	123	124	DIP	12	215.38	209.38	0.9	563.0	561.1		M.39	Div. (2)
124	497+45.96, 167.47' Lt.	48" SW-301	571.74	560.45		State St. Div. (2)	P124	124	128	DIP	12	173.37	167.37	1.4	561.0	558.6		M.39	Div. (2)
125	1624+32.49, 29.54' Rt.	48" SW-301	574.17	560.80		US 67 EB Div. (2)	P125	125	126	DIP	15	411.82	405.82		561.3	561.3		M.38	Match Ex. Pipe Grade Div. (2)
126	503+60.68, 10.33' Rt.	48" SW-301	571.00	560.70		State St. Div. (2)	P126	126	127	DIP	15	413.26	407.26	0.5	561.2	559.0		M.43	Div. (2)
127	499+49.49, 10.00' Rt.	48" SW-301	572.74	558.39		State St. Div. (2)	P127	127	128	DIP	15	181.38	175.38	0.2	558.9	558.5		M.43	Div. (2)
128	497+70.28, 2.17' Rt.	48" SW-301	571.59	557.88		State St. Div. (2)	P128	128	129	DIP	15	200.62	194.62	0.2	558.4	557.9		M.43	Div. (2)
129	495+71.65, 2.97' Rt.	48" SW-301	572.96	557.31		State St. Div. (2)	P129	129		DIP	15	77.04	71.04	0.3	557.8	557.6		M.43	Pipe to be Jacked Div. (2)
130	493+32.84, 12.06' Rt.	48" SW-301	572.86	556.62		State St. Div. (2)	P129		130	DIP	15	169.91	163.91	0.3	557.6	557.2		M.43	Pipe to be Trenched Div. (2)
131	490+40.53, 12.13' Rt.	60" SW-301	572.71	555.08		State St. Div. (2)	P130	130	131	DIP	15	293.81	287.81	0.3	557.1	556.3		M.42	Div. (2)
132	490+31.70, 250.16' Rt.	60" SW-301	566.45	554.29		State St. Div. (2)	P202	EX.	131	DIP	15	108.65	102.65	0.2	556.5	556.3		M.42	Connect to Ex. San. Pipe Div. (2)
							P131	131	132	DIP	24	239.19	233.19	0.3	555.6	554.9		M.41	Div. (2)
140	4604+32.83, 56.88' Lt.	48" SW-301	573.07	566.25		Kimberly Rd. Div. (2)	P140	140	141	DIP	8	331.18	325.18	0.7	566.8	564.5		M.37	Div. (2)
141	11100+94.34, 0.28' Rt.	48" SW-301	569.98	563.95		11th St. Div. (2)													
150	10100+22.27, 2.30' Rt.	48" SW-301	571.65	561.50		10th St. Div. (2)	P150	150	151	DIP	15	51.98	45.98	8.0	562.0	558.3		M.37	Div. (2)
151	478+97.04, 3.81' Rt.	48" SW-301	571.53	557.70		State St. Div. (2)	P151	151	152	DIP	15	348.40	342.40	0.2	558.2	557.5		M.42	Div. (2)
152	482+43.52, 8.56' Rt.	48" SW-301	572.25	556.93		State St. Div. (2)													



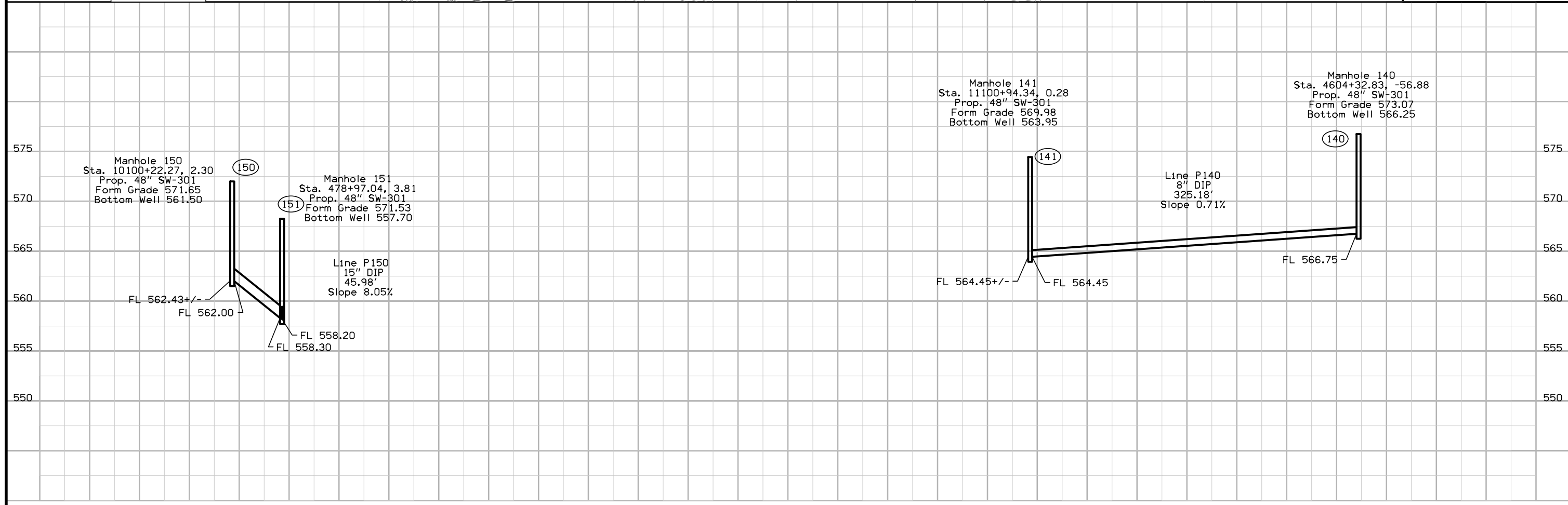
**US 67 SB  
SANITARY SEWER**

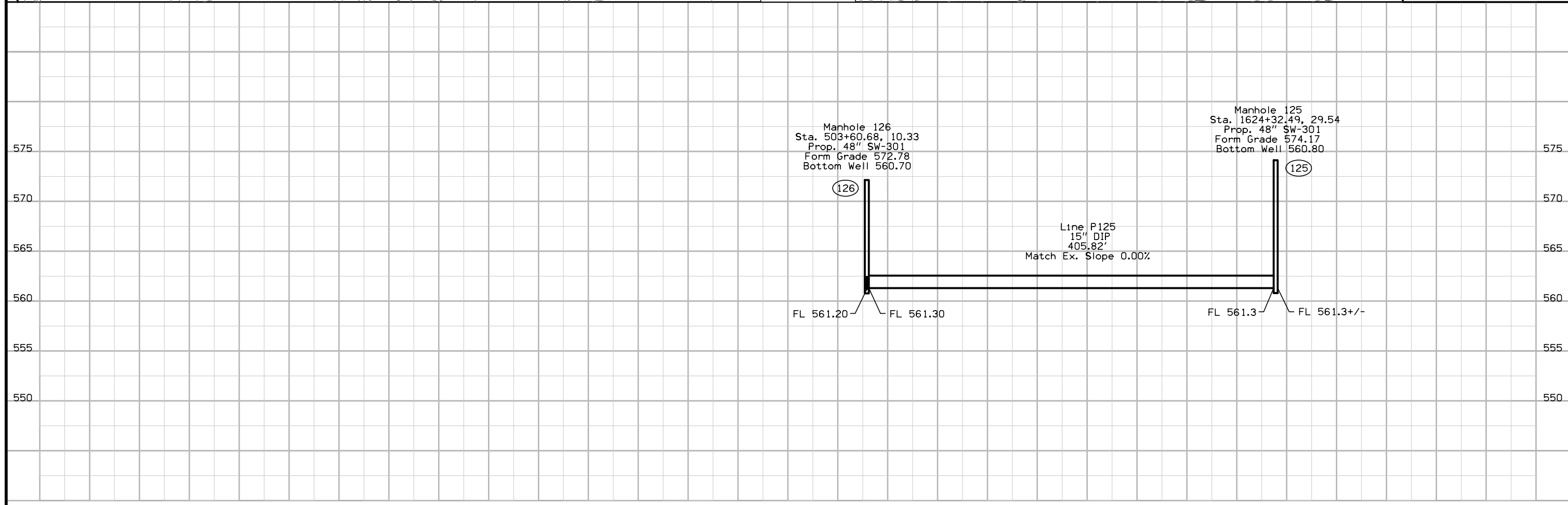
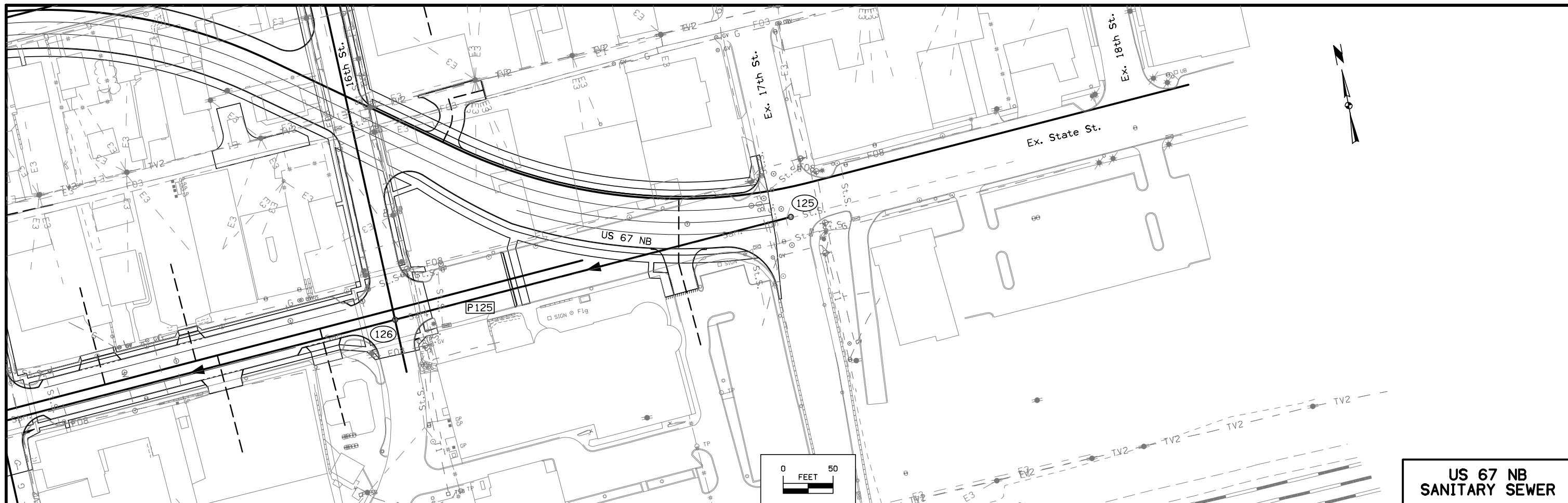


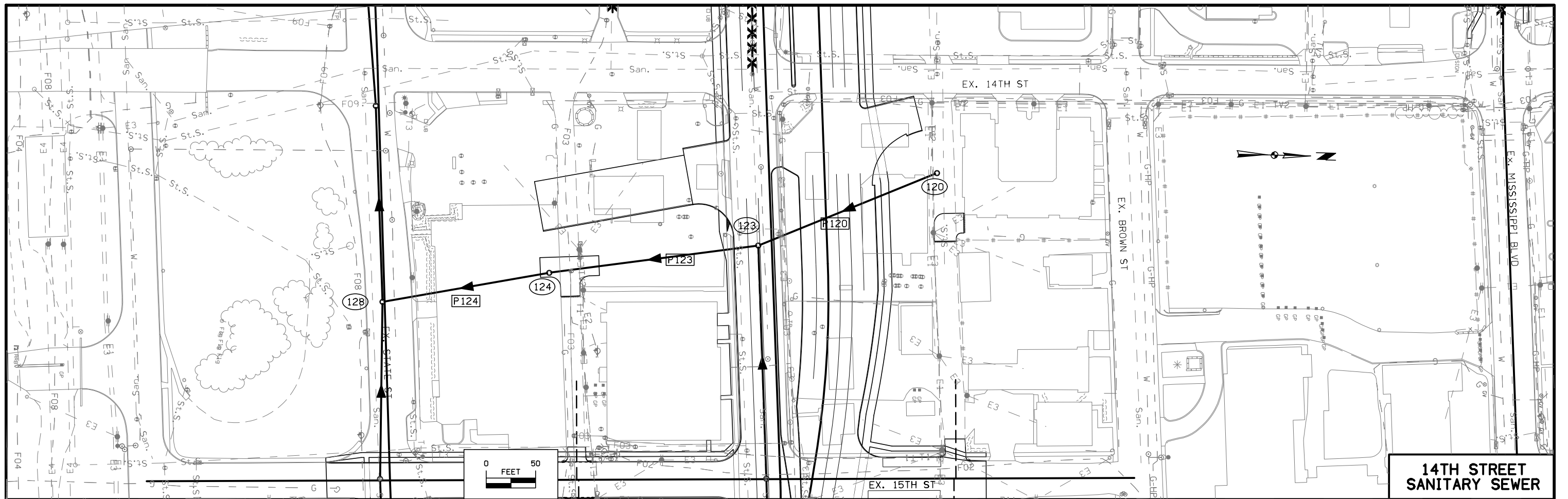
Make Temp. Connection  
Through MH To Ex. San.  
Pipe to West; Remove  
and Install Inside Drop  
at Later Date when  
San. Main to West is  
Abandoned



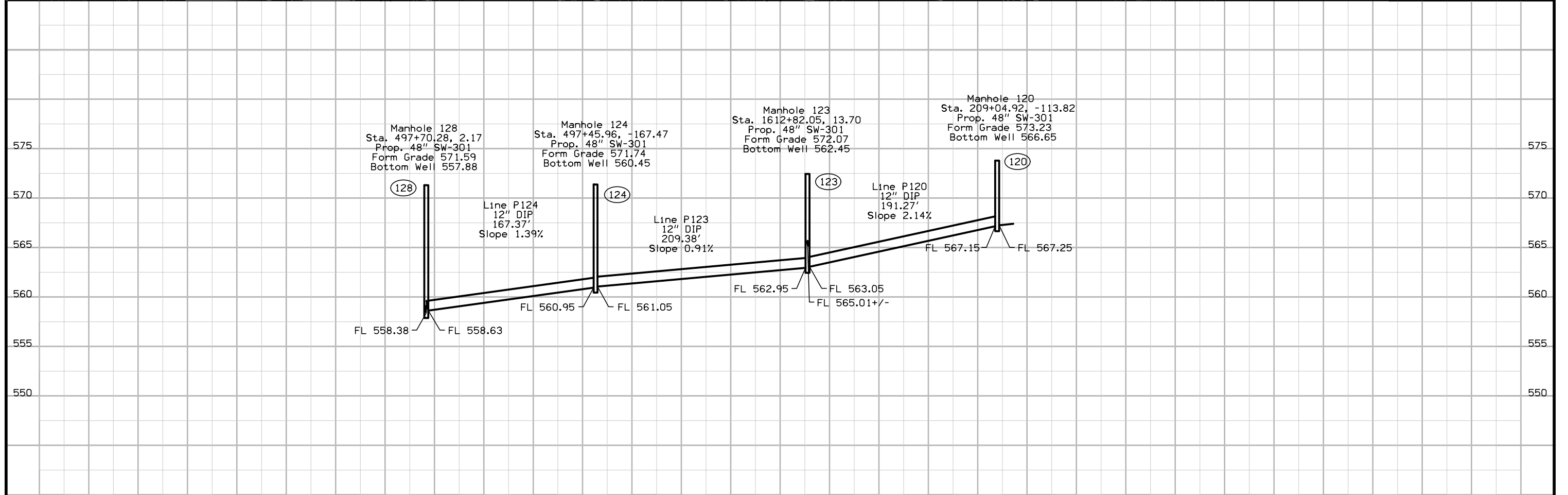
**US 67 NB  
SANITARY SEWER**



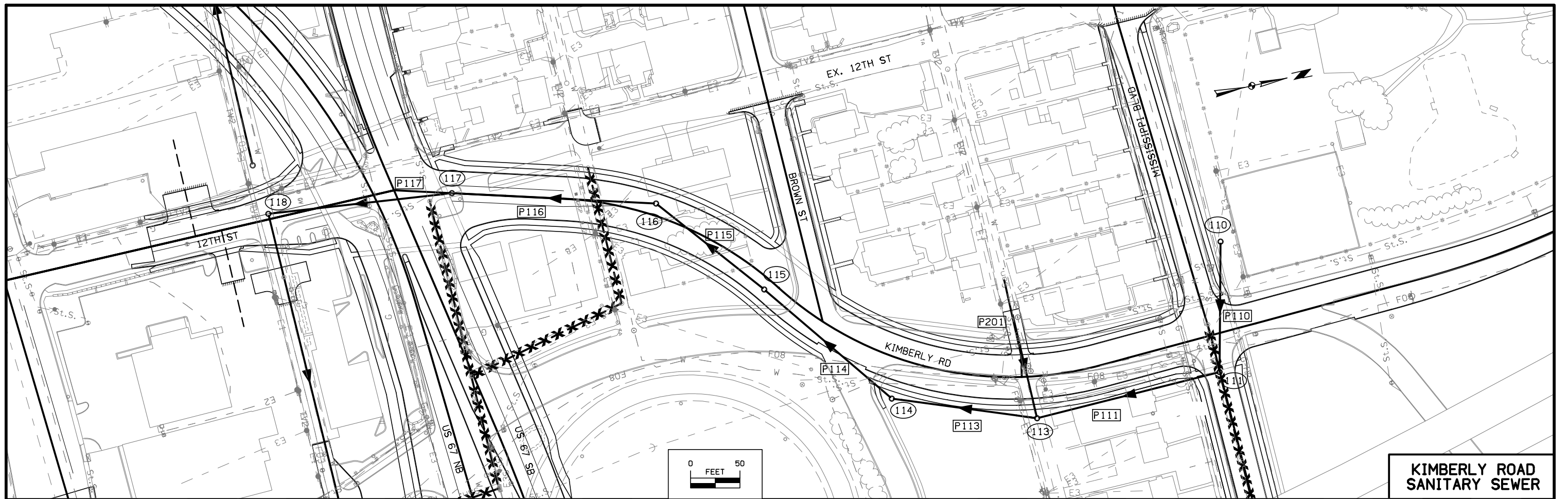




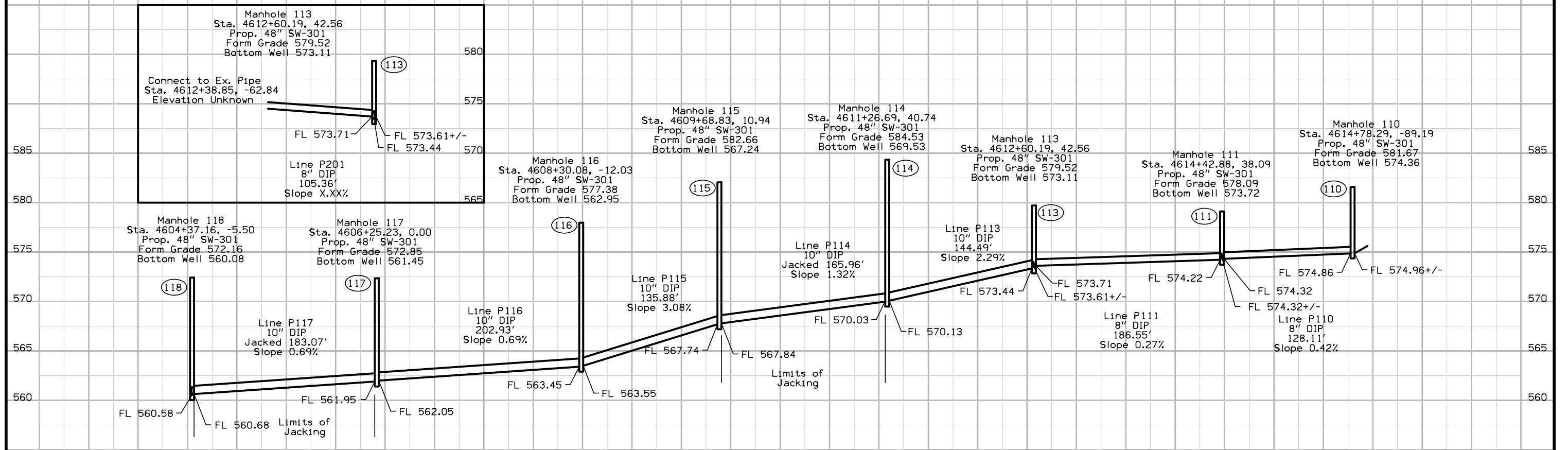
**14TH STREET  
SANITARY SEWER**

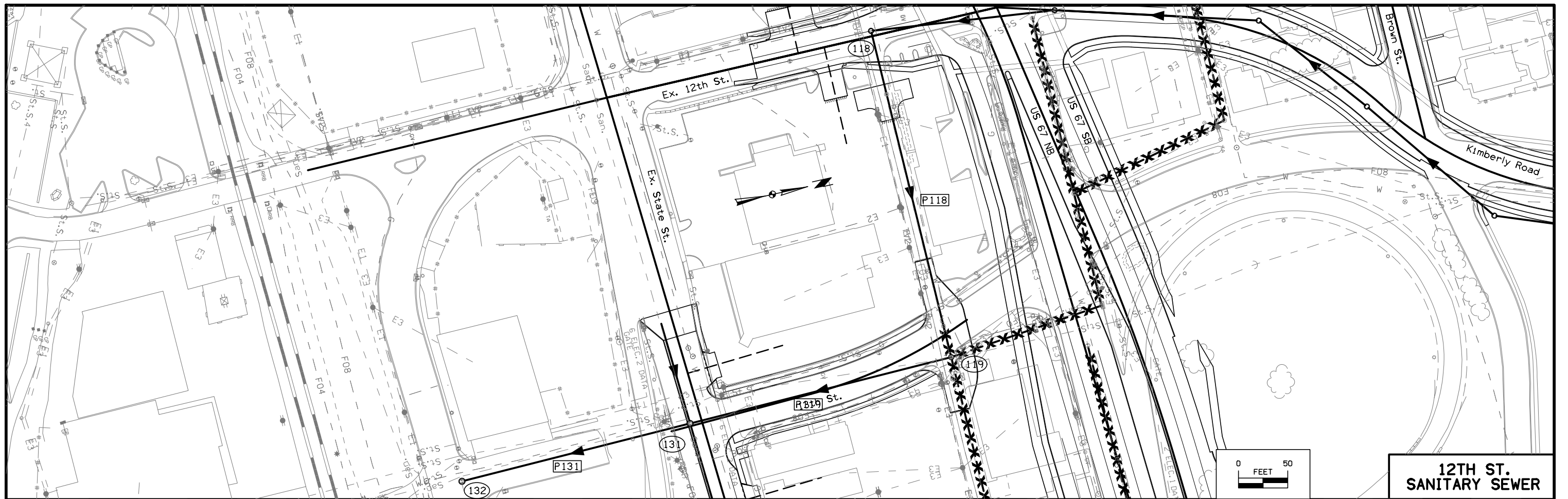




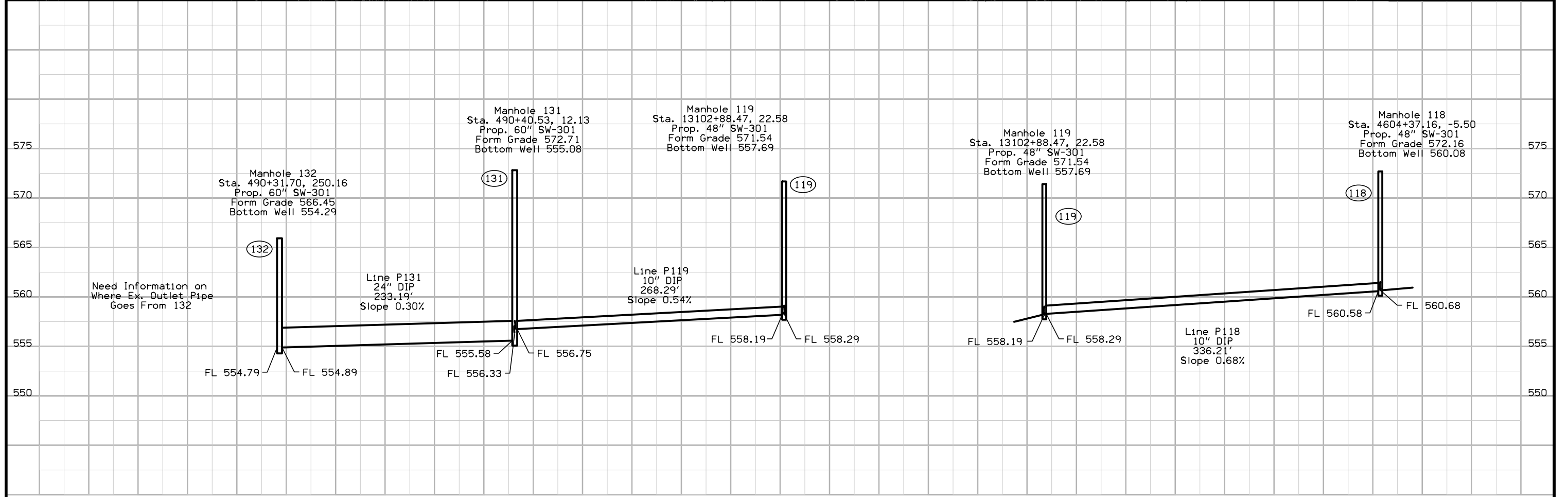


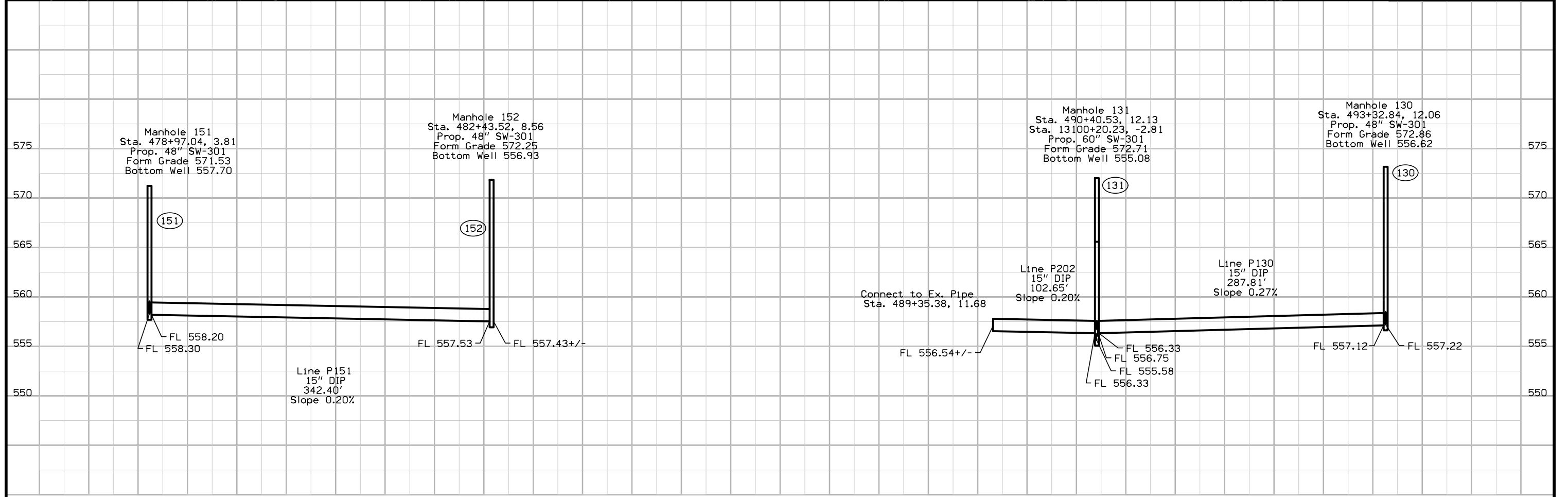
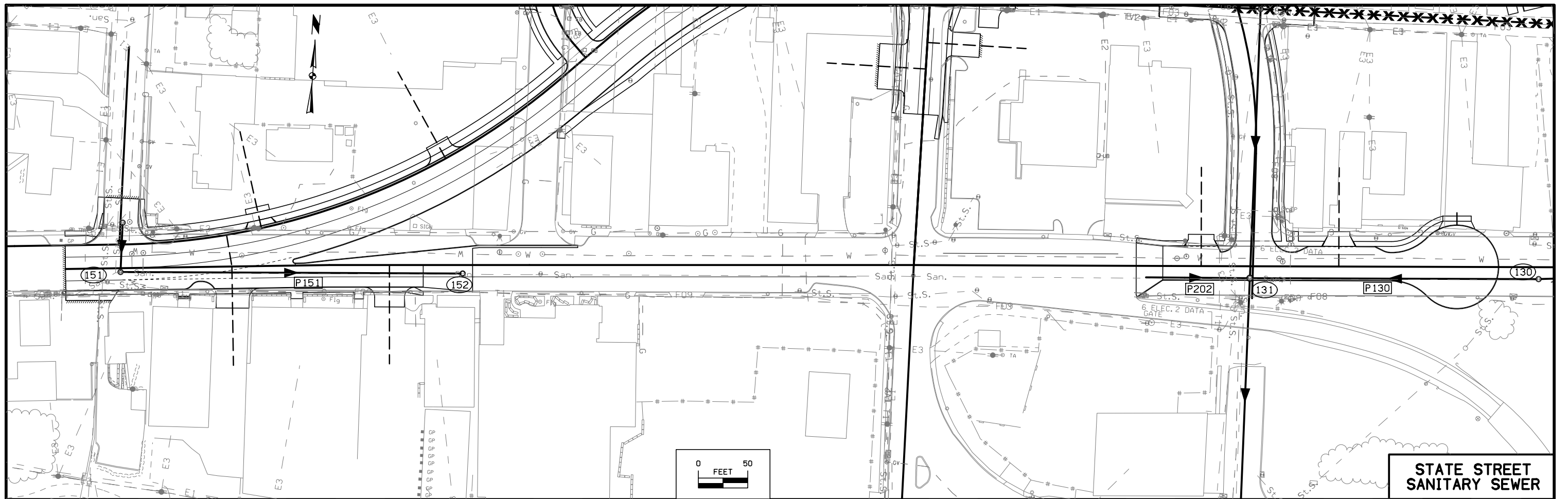
**KIMBERLY ROAD  
SANITARY SEWER**

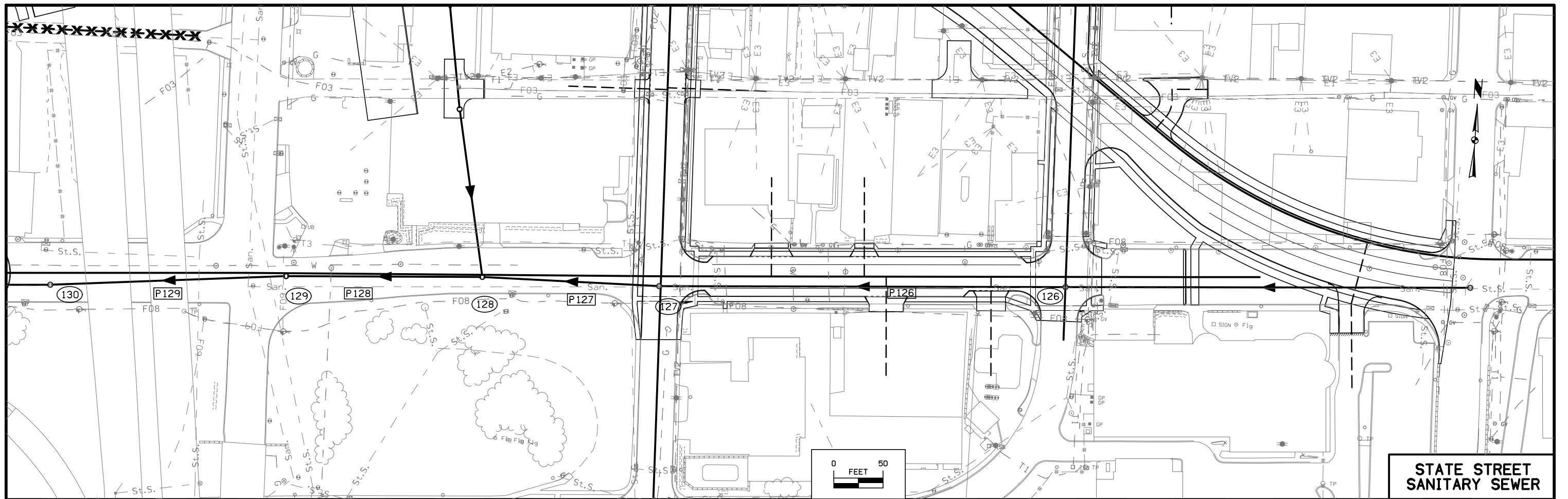




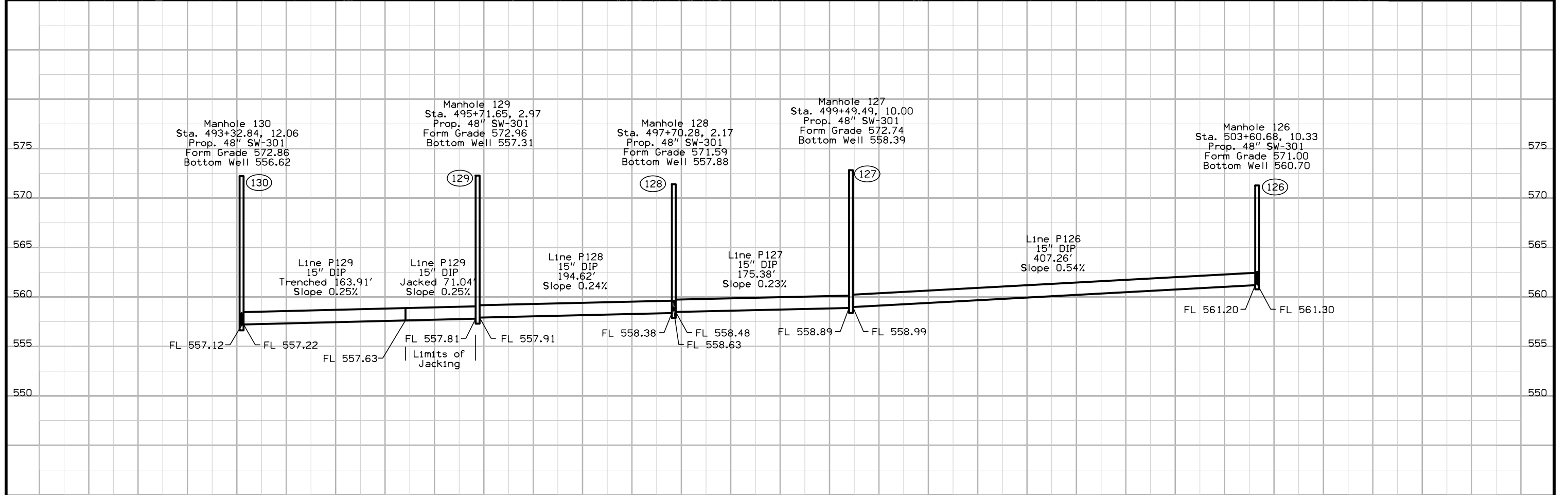
**12TH ST.  
SANITARY SEWER**







**STATE STREET  
SANITARY SEWER**



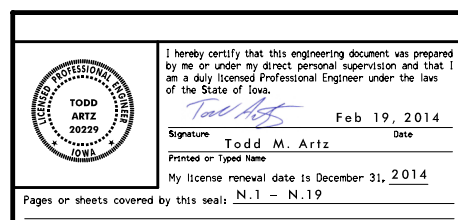
# TRAFFIC SIGNAL GENERAL NOTES

- ALL QUANTITIES SHOWN IN THE PLANS AND SPECIFICATIONS ARE FOR INFORMATIONAL AND ESTIMATING PURPOSES ONLY. THE CONTRACTOR'S LUMP SUM BID FOR THIS PROJECT SHALL INCLUDE ALL LABOR AND MATERIAL NECESSARY TO PROVIDE A COMPLETE AND FUNCTIONAL TRAFFIC SIGNAL SYSTEM IN CONFORMANCE WITH THE PLANS AND SPECIFICATIONS.
- THE PLAN LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE ONLY. THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES PRIOR TO EXCAVATION ON THE PROJECT.
- THE LOCATIONS OF ALL FOOTINGS, HANDHOLES, AND CONDUIT ARE TO BE COORDINATED WITH THE ENGINEER AND ARE SUBJECT TO ADJUSTMENT IN THE FIELD BY THE ENGINEER.
- THE STATIONS AND OFFSETS LISTED ON THE SIGNAL PLANS ARE TO THE CENTER OF THE ITEM UNLESS OTHERWISE NOTED.
- ANY COMPONENT OF AN EXISTING TRAFFIC SIGNAL INSTALLATION TO BE REMOVED SHALL BE REMOVED BY THE CONTRACTOR AND DELIVERED TO THE IOWA DEPARTMENT OF TRANSPORTATION, UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER.
- THE CONTRACTOR HAS THE OPTION TO BORE OR TRENCH CONDUIT; HOWEVER, THE CONDUIT SHALL BE BORED UNDER NEW OR EXISTING PAVEMENT. PAVEMENT SHALL NOT BE DISTURBED BY SIGNAL CONSTRUCTION.
- AREAS DISTURBED BY CONSTRUCTION SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
- ALL MOUNTED SIGNALS SHALL HEADS SHALL COME FURNISHED WITH BACK-PLATES
- THE TRAFFIC SIGNAL CONTROLLERS SHALL BE ORIENTED WITH THE BACK OF THE CABINET TOWARD THE INTERSECTION SUCH THAT THE SIGNAL HEADS CAN BE VIEWED WHILE FACING THE CONTROLLER, UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER.
- A #6 A.W.G. BARE COPPER GROUND WIRE SHALL BE INSTALLED IN ALL PVC CONDUITS THAT CARRY 120 VOLT SIGNAL CABLES. ALL STEEL CONDUIT, POLES, PEDESTALS, AND CONTROLLER CABINETS AT EACH INTERSECTION SHALL BE BONDED TO FORM A CONTINUOUS SYSTEM AND BE EFFECTIVELY GROUNDED. BONDING JUMPERS SHALL BE #6 A.W.G. BARE COPPER WIRE CONNECTED BY APPROVED CLAMPS.
- THE CONTRACTOR SHALL COMPLETELY REMOVE ANY EXISTING SIGNAL POLE AND CONTROLLER FOOTINGS.
- THE CONTRACTOR SHALL INSTALL ONE SIGNAL CABLE FROM EACH SIGNAL HEAD TO THE BASE OF THE POLE. A 5-CONDUCTOR CABLE SHALL BE USED FOR A 3-SECTION SIGNAL HEAD AND A 7-CONDUCTOR CABLE SHALL BE USED FOR EITHER A 4 OR 5 SECTION SIGNAL HEAD.
- EXISTING STREET SIGNS AT INTERSECTIONS WITH TEMPORARY TRAFFIC SIGNALS SHALL BE MAINTAINED AS A PART OF THE TEMPORARY TRAFFIC SIGNAL PAY ITEM.
- THE CONTRACTOR SHALL REMOVE THE EXISTING TRAFFIC SIGNAL AT THE LOCATION(S) SPECIFIED IN THE DELIVERY AND STOCKPILING TABLE. REMOVAL SHALL CONSIST OF, BUT NOT LIMITED TO, ALL TRAFFIC SIGNAL POSTS, POLES, MAST ARMS, SIGNAL HEADS, CONTROLLER, CABINETS, WIRING, AND HAND HOLES.
- ALL PEDESTRIAN SIGNAL HEADS ON TEMPORARY SIGNALS SHALL NOT BE TURNED ON AND SHALL BE COVERED, UNTILL LATER STAGES WHEN PEDERTRIAN CROSS WALKS ARE CONSTRUCTED.

# SIGNAL LEGEND

	CABINET AND CONTROLLER
	④ TRAFFIC SIGNAL POLE, FOOTING SYMBOL, AND IDENTIFYING NUMBER
	④ PEDESTAL POLE, FOOTING SYMBOL, AND IDENTIFYING NUMBER
	④ TRAFFIC SIGNAL HEAD WITH BACKPLATE SYMBOL AND IDENTIFYING NUMBER
	④ PEDESTRIAN HEAD SYMBOL AND IDENTIFYING NUMBER
	④ MAST ARM MOUNTED SIGN SYMBOL AND IDENTIFYING NUMBER
	④ 30"x40" "TUB" HANDHOLE SYMBOL AND IDENTIFYING NUMBER
	④ 24" HANDHOLE SYMBOL AND IDENTIFYING NUMBER
	11 VIDEO DETECTION AREA SYMBOL AND IDENTIFYING NUMBER
	VIDEO DETECTION CAMERA
	EMERGENCY VEHICLE PREEMPTION
	LUMINAIRE

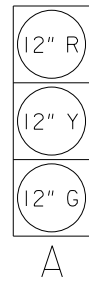
DELIVERY AND STOCKPILING					110-13
					04-20-10
Item Description	Quantity	Units	Delivery Location	Contact Name & Number	Remarks
Existing traffic signal at Grant Street and 13th Street	1	EACH	Bettendorf Public Works Facility 4403 Devils Glen Rd Bettendorf, IA 52722	563-344-4055	(4)
Existing traffic signal at Kimberly and I-74 EB exit ramp	1	EACH			
Partial Existing traffic signal at Kimberly and I-74 EB exit ramp	1	EACH			
PROJECT NOTE:					
THE PAY ITEMS HAVE BEEN DIVIDED IN THE REMARKS AS FOLLOWS:					
(1) DIVISION 1 PAY ITEM (IOWA DOT COST)					
(2) DIVISION 2 PAY ITEM (CITY OF BETTENDORF COST)					
(3) DIVISION 3 PAY ITEM (72.23% IOWA/27.7% BETTENDORF COST)					
(4) DIVISION 4 PAY ITEM (NON-PARTICIPATING)					



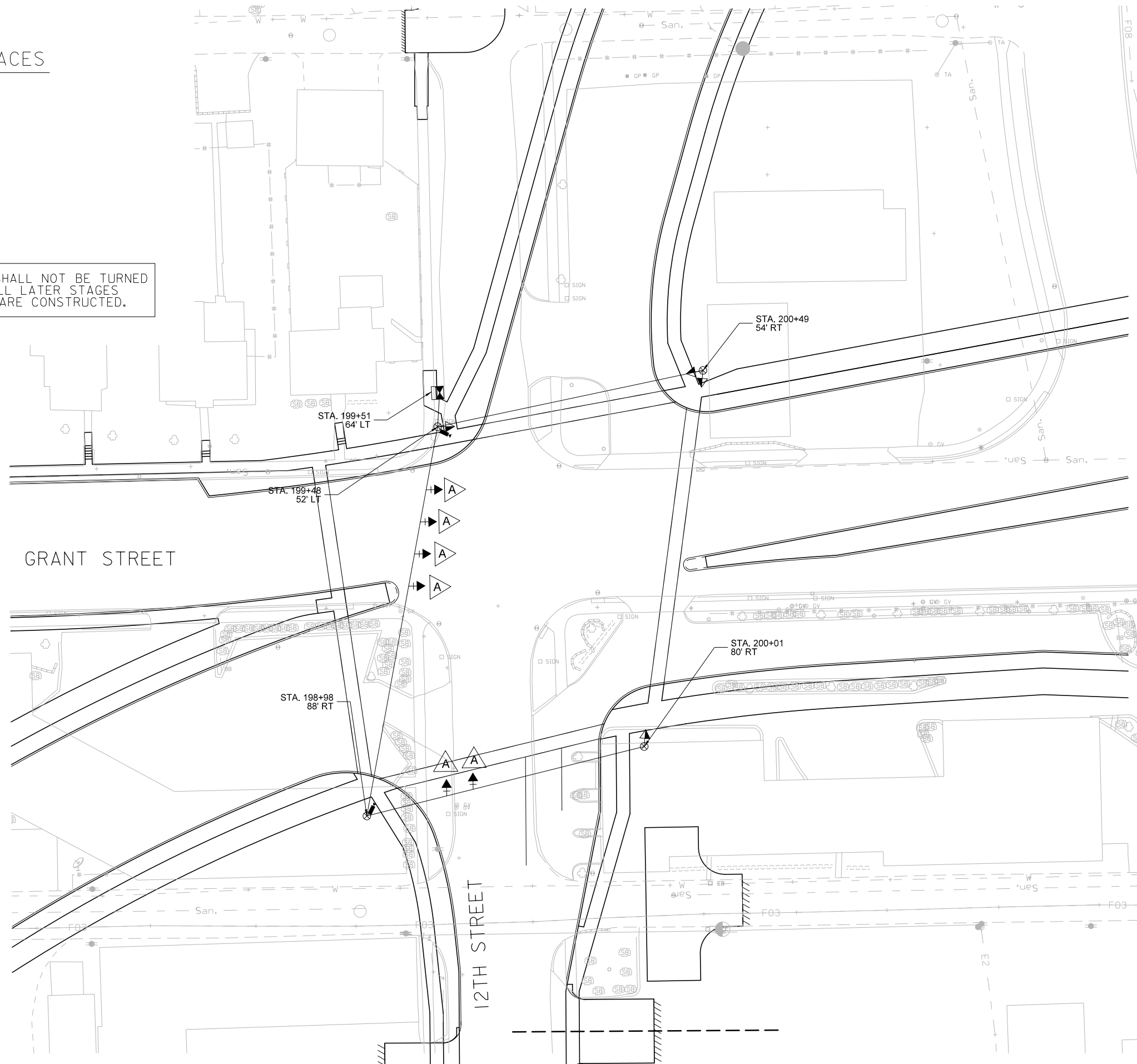
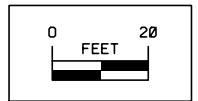
**TRAFFIC SIGNAL PLAN**  
**GENERAL NOTES**

LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11

TRAFFIC SIGNAL FACES



ALL PEDESTRIAN SIGNAL HEADS SHALL NOT BE TURNED ON AND SHALL BE COVERED, UNTILL LATER STAGES WHEN PEDERTRIAN CROSS WALKS ARE CONSTRUCTED.



**TRAFFIC SIGNAL PLAN**  
**GRANT STREET AND 12TH STREET**

LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11

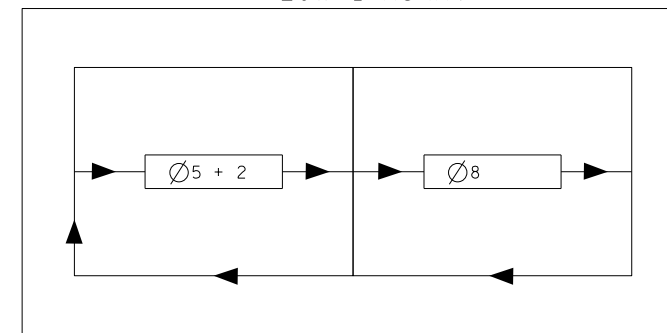
TRAFFIC PHASING AND COLOR SCHEME

TRAFFIC PHASING LEGEND

→	ACTUATED VEHICLE MOVEMENT
R/W	RIGHT OF WAY INTERVAL
∅	TRAFFIC PHASE
R	CIRCULAR RED
Y	CIRCULAR YELLOW
G	CIRCULAR GREEN
YA	YELLOW ARROW
GA	GREEN ARROW
AO	ALL OTHERS

APPROACH	FACE	∅5 + 2		∅8		CLEAR TO ∅		CLEAR TO ∅		CLEAR TO ∅		CLEAR TO ∅		FLASH
		R/W	AO	R/W	AO	R/W	AO	R/W	AO	R/W	AO			
WESTBOUND	A	G	Y	R	R									R
SOUTHBOUND	A	R	R	G	Y									R

FLOW DIAGRAM



THE RED CLEARANCE INTERVAL SHALL BE INCLUDED IN THE COLOR SEQUENCE WHEN CALLED FOR BY THE CONTROLLER TIMING.

NOTE:

→ INDICATES FLOW PATH WITH ALL PHASES DEMANDED

SUGGESTED TIMINGS (SECONDS)

PHASE	1	2	3	4	5	6	7	8
MINIMUM GREEN								
PASSAGE								
MAXIMUM I								
MAXIMUM II								
YELLOW CHANGE								
RED CLEARANCE								
WALK								
PEDESTRIAN CLEARANCE								
ADDED INITIAL								
TIME TO REDUCE								
TIME BEFORE REDUCTION								
MAXIMUM GAP								

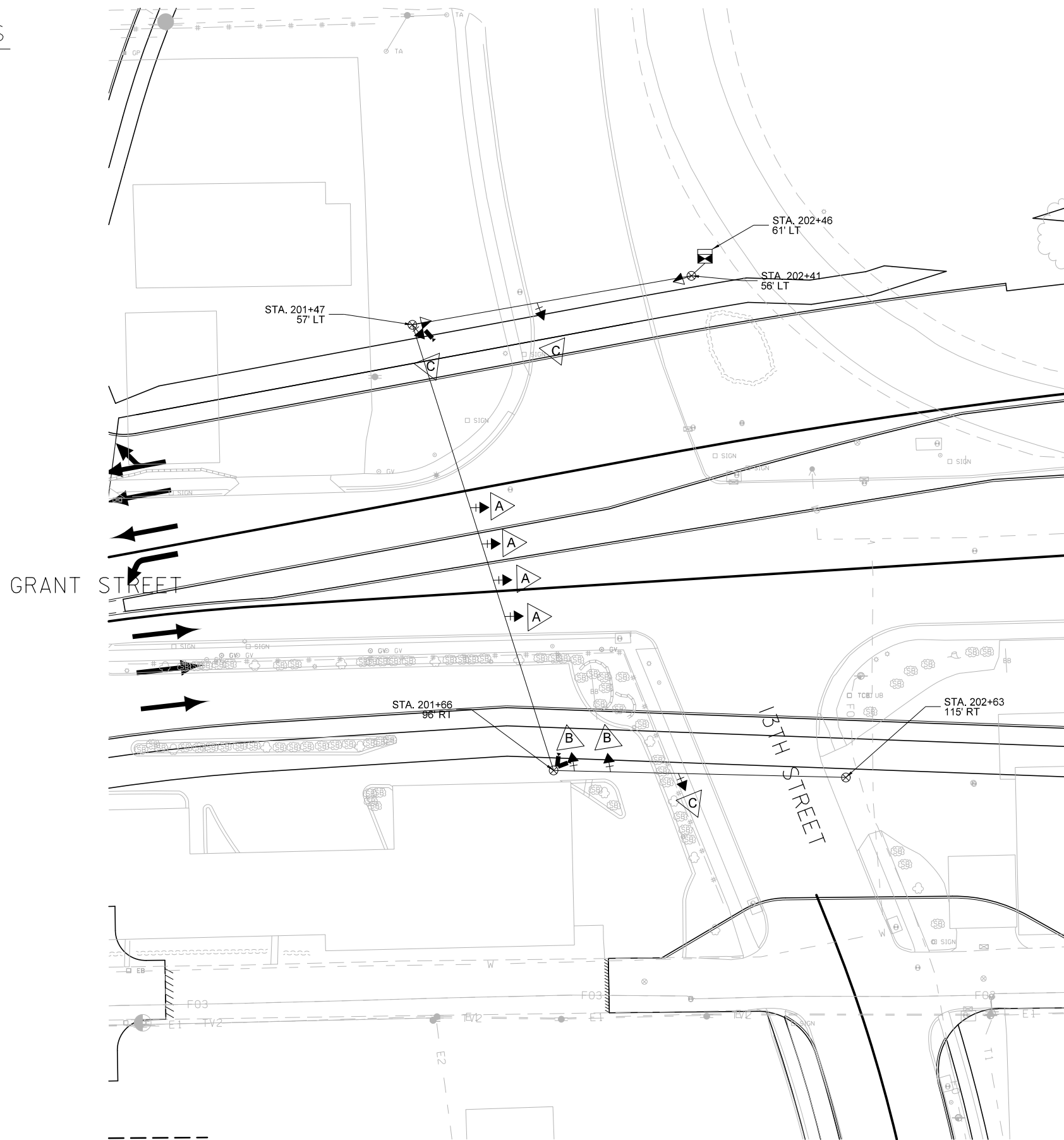
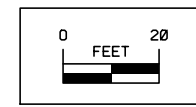
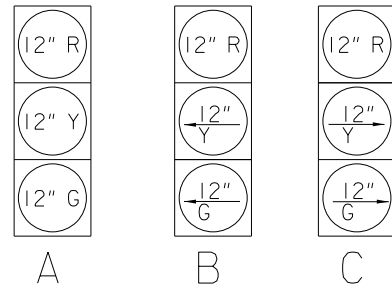
ALL TEMPORARY TRAFFIC SIGNAL TIMING WILL BE ESTABLISHED BY THE DEPARTMENT OF TRANSPORTATION AND MAY BE ADJUSTED THROUGHOUT CONSTRUCTION.

DETECTOR UNITS SHALL ALLOW FOR ADJUSTABLE DELAY SETTING. INITIAL DELAY SETTING SHALL BE 5 SECONDS.

**TRAFFIC SIGNAL PLAN**  
**GRANT STREET AND 12TH STREET**

LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11

TRAFFIC SIGNAL FACES



ALL PEDESTRIAN SIGNAL HEADS SHALL NOT BE TURNED ON AND SHALL BE COVERED, UNTILL LATER STAGES WHEN PEDERTRIAN CROSS WALKS ARE CONSTRUCTED.

**TRAFFIC SIGNAL PLAN**  
**GRANT STREET AND 13TH STREET**

LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11



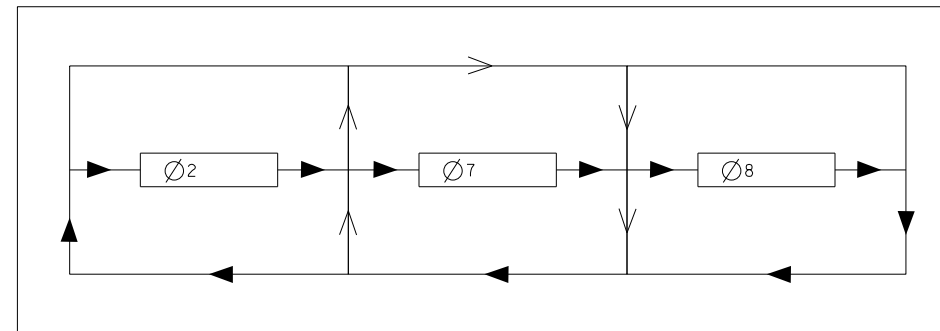
TRAFFIC PHASING AND COLOR SCHEME

TRAFFIC PHASING LEGEND

→	ACTUATED VEHICLE MOVEMENT
R/W	RIGHT OF WAY INTERVAL
∅	TRAFFIC PHASE
R	CIRCULAR RED
Y	CIRCULAR YELLOW
G	CIRCULAR GREEN
YA	YELLOW ARROW
GA	GREEN ARROW
AO	ALL OTHERS

APPROACH	FACE	∅2				∅7				∅8				FLASH				
		CLEAR TO ∅				CLEAR TO ∅				CLEAR TO ∅								
		R/W	AO			R/W	AO			R/W	AO				R/W			
WESTBOUND	A	G	Y			R	R			R	R							R
NORTHBOUND	C	R	R			G	Y			R	R							R
SOUTHBOUND	B	R	R			R	R			G	Y							R

FLOW DIAGRAM



THE RED CLEARANCE INTERVAL SHALL BE INCLUDED IN THE COLOR SEQUENCE WHEN CALLED FOR BY THE CONTROLLER TIMING.

NOTE:

- INDICATES FLOW PATH WITH ALL PHASES DEMANDED
- INDICATES OPTIONAL FLOW PATHS WHEN SOME PHASES ARE NOT DEMANDED AT THE TIME OF RIGHT-OF-WAY TRANSFER.

SUGGESTED TIMINGS (SECONDS)

PHASE	1	2	3	4	5	6	7	8
MINIMUM GREEN								
PASSAGE								
MAXIMUM I								
MAXIMUM II								
YELLOW CHANGE								
RED CLEARANCE								
WALK								
PEDESTRIAN CLEARANCE								
ADDED INITIAL								
TIME TO REDUCE								
TIME BEFORE REDUCTION								
MAXIMUM GAP								

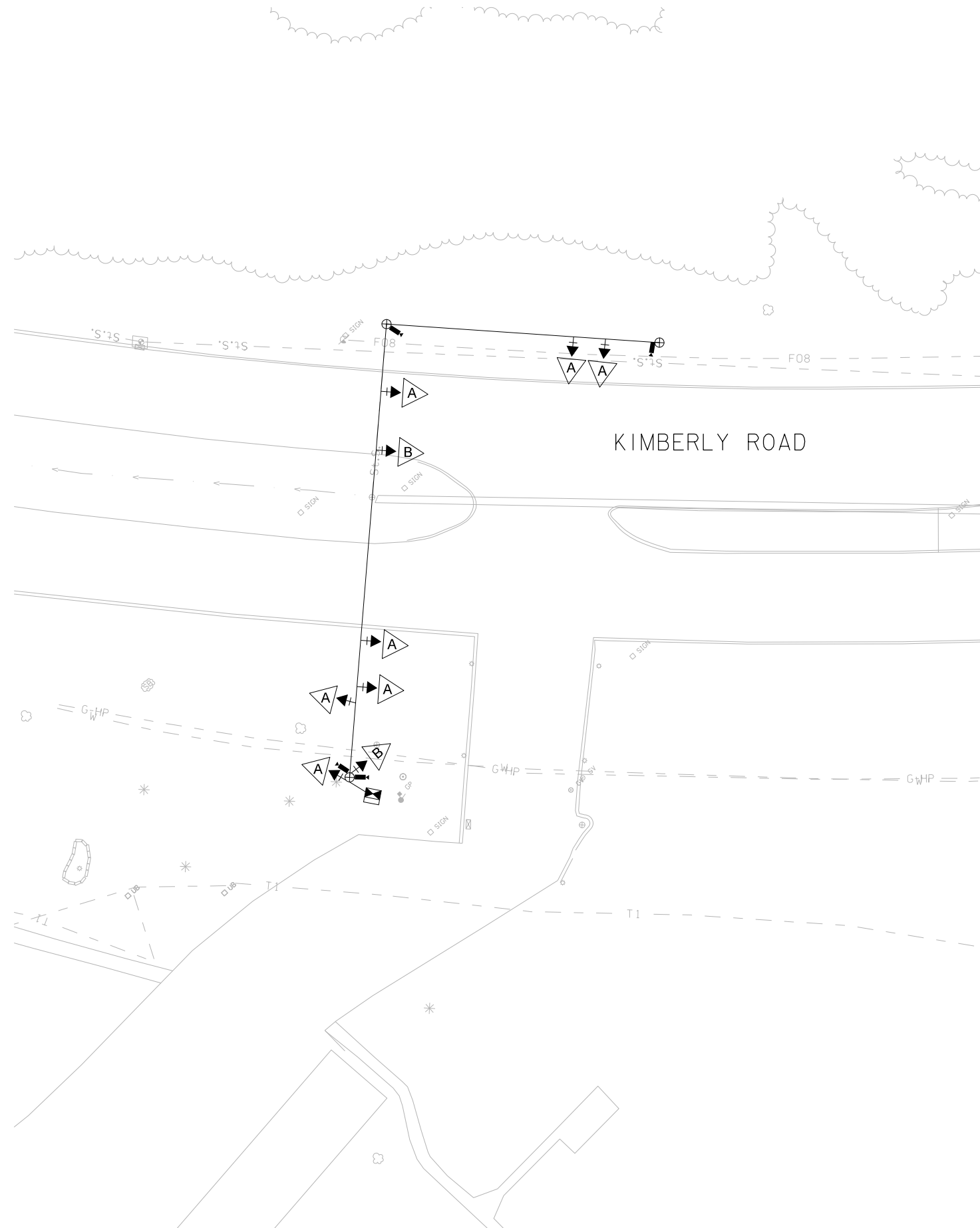
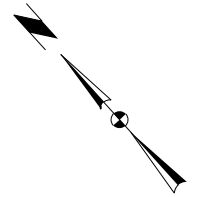
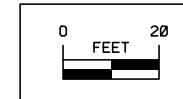
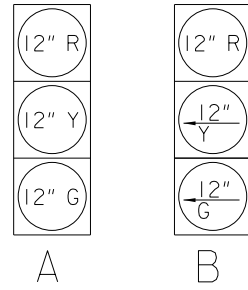
ALL TEMPORARY TRAFFIC SIGNAL TIMING WILL BE ESTABLISHED BY THE DEPARTMENT OF TRANSPORTATION AND MAY BE ADJUSTED THROUGHOUT CONSTRUCTION.

DETECTOR UNITS SHALL ALLOW FOR ADJUSTABLE DELAY SETTING. INITIAL DELAY SETTING SHALL BE 5 SECONDS.

**TRAFFIC SIGNAL PLAN**  
**GRANT STREET AND 13TH STREET**

LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11

TRAFFIC SIGNAL FACES



TRAFFIC SIGNAL PLAN  
KIMBERLY ROAD AND 12TH STREET

LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11

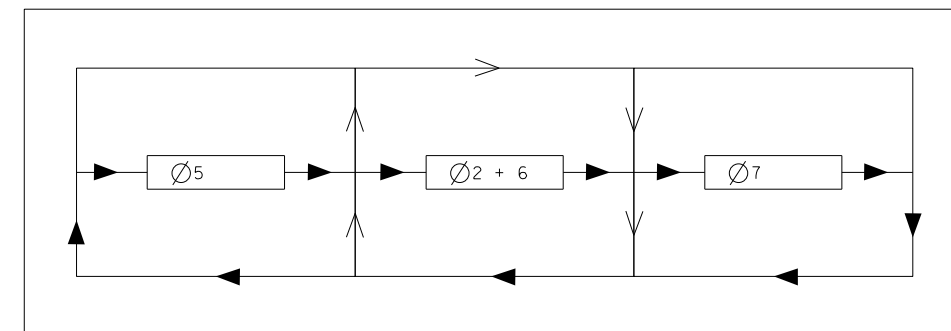
TRAFFIC PHASING LEGEND

→	ACTUATED VEHICLE MOVEMENT
R/W	RIGHT OF WAY INTERVAL
∅	TRAFFIC PHASE
R	CIRCULAR RED
Y	CIRCULAR YELLOW
G	CIRCULAR GREEN
YA	YELLOW ARROW
GA	GREEN ARROW
AO	ALL OTHERS

TRAFFIC PHASING AND COLOR SCHEME

		∅5				∅2 + 6				∅7					
		CLEAR TO ∅				CLEAR TO ∅				CLEAR TO ∅					
APPROACH	FACE	R/W	AO			R/W	AO			R/W	AO			R/W	FLASH
WESTBOUND	A	R	R			G	Y			R	R				R
	B	GA	YA			R	R			R	R				R
EASTBOUND	A	R	R			G	Y			R	R				
NORTHBOUND	A	R	R			R	R			G	Y				

FLOW DIAGRAM



THE RED CLEARANCE INTERVAL SHALL BE INCLUDED IN THE COLOR SEQUENCE WHEN CALLED FOR BY THE CONTROLLER TIMING.

NOTE:

- INDICATES FLOW PATH WITH ALL PHASES DEMANDED
- INDICATES OPTIONAL FLOW PATHS WHEN SOME PHASES ARE NOT DEMANDED AT THE TIME OF RIGHT-OF-WAY TRANSFER.

SUGGESTED TIMINGS (SECONDS)

PHASE	1	2	3	4	5	6	7	8
MINIMUM GREEN								
PASSAGE								
MAXIMUM I								
MAXIMUM II								
YELLOW CHANGE								
RED CLEARANCE								
WALK								
PEDESTRIAN CLEARANCE								
ADDED INITIAL								
TIME TO REDUCE								
TIME BEFORE REDUCTION								
MAXIMUM GAP								

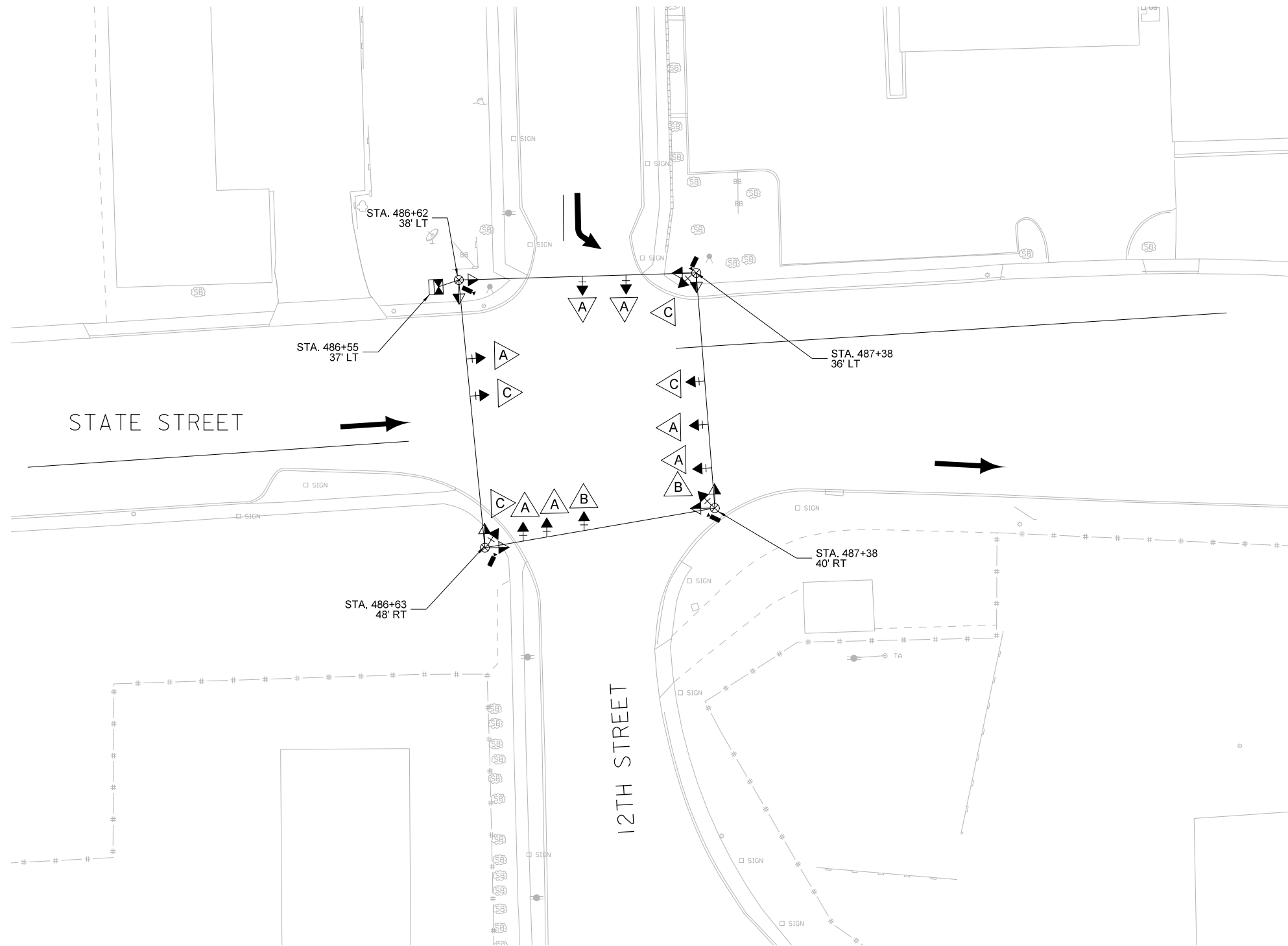
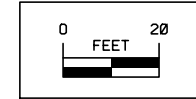
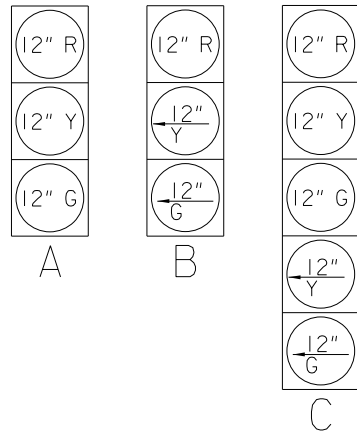
ALL TEMPORARY TRAFFIC SIGNAL TIMING WILL BE ESTABLISHED BY THE DEPARTMENT OF TRANSPORTATION AND MAY BE ADJUSTED THROUGHOUT CONSTRUCTION.

DETECTOR UNITS SHALL ALLOW FOR ADJUSTABLE DELAY SETTING. INITIAL DELAY SETTING SHALL BE 5 SECONDS.

**TRAFFIC SIGNAL PLAN**  
**KIMBERLY ROAD AND 12TH STREET**

LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11

TRAFFIC SIGNAL FACES



**TRAFFIC SIGNAL PLAN**  
**STATE STREET AND 12TH STREET**

LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11

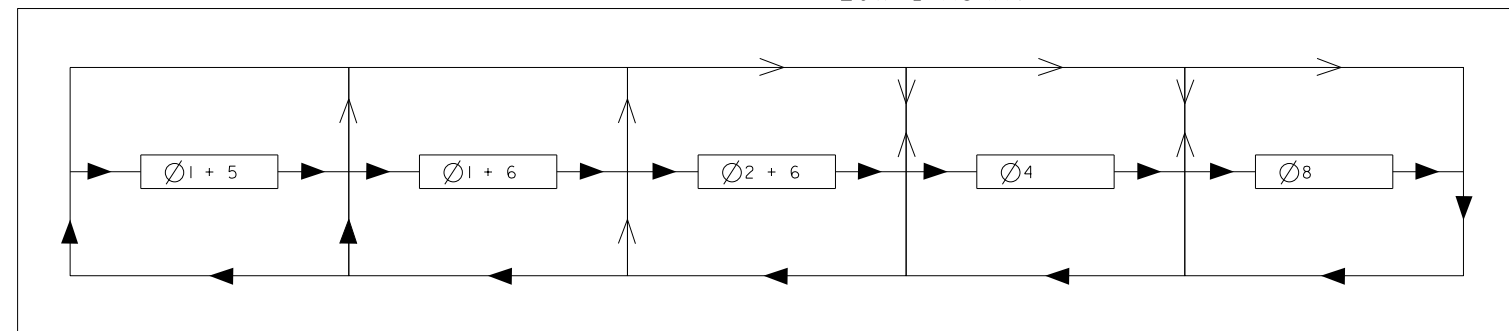
TRAFFIC PHASING AND COLOR SCHEME

TRAFFIC PHASING LEGEND

→	ACTUATED VEHICLE MOVEMENT
→	PARTIALLY RESTRICTED VEHICLE MOVEMENT
R/W	RIGHT OF WAY INTERVAL
∅	TRAFFIC PHASE
R	CIRCULAR RED
Y	CIRCULAR YELLOW
G	CIRCULAR GREEN
YA	YELLOW ARROW
GA	GREEN ARROW
AO	ALL OTHERS

APPROACH	FACE	∅1 + 5		∅1 + 6			∅2 + 6			∅4			∅8			FLASH	
		CLEAR TO ∅		CLEAR TO ∅			CLEAR TO ∅			CLEAR TO ∅			CLEAR TO ∅				
		R/W	AO	R/W	2+6	AO	R/W	AO	R/W	AO	R/W	AO	R/W	AO			
EASTBOUND	C	GA	YA		G,GA	Y,YA	Y,YA	R	R		R	R		R	R		R
	A	R	R		G	G	Y	G	Y		R	R		R	R		R
WESTBOUND	C	GA	YA		R	R	R	G	Y		R	R		R	R		R
	A	R	R		R	R	R	G	Y		R	R		R	R		R
NORTHBOUND	A	R	R		R	R	R	R	R		G	Y		R	R		R
SOUTHBOUND	B	R	R		R	R	R	R	R		R	R		GA	YA		R
	A	R	R		R	R	R	R	R		R	R		G	Y		R

FLOW DIAGRAM



THE RED CLEARANCE INTERVAL SHALL BE INCLUDED IN THE COLOR SEQUENCE WHEN CALLED FOR BY THE CONTROLLER TIMING.

NOTE:

- INDICATES FLOW PATH WITH ALL PHASES DEMANDED
- INDICATES OPTIMAL FLOW PATHS WHEN SOME PHASES ARE NOT DEMANDED AT THE TIME OF RIGHT-OF-WAY TRANSFER

SUGGESTED TIMINGS (SECONDS)

PHASE	1	2	3	4	5	6	7	8
MINIMUM GREEN								
PASSAGE								
MAXIMUM I								
MAXIMUM II								
YELLOW CHANGE								
RED CLEARANCE								
WALK								
PEDESTRIAN CLEARANCE								
ADDED INITIAL								
TIME TO REDUCE								
TIME BEFORE REDUCTION								
MAXIMUM GAP								

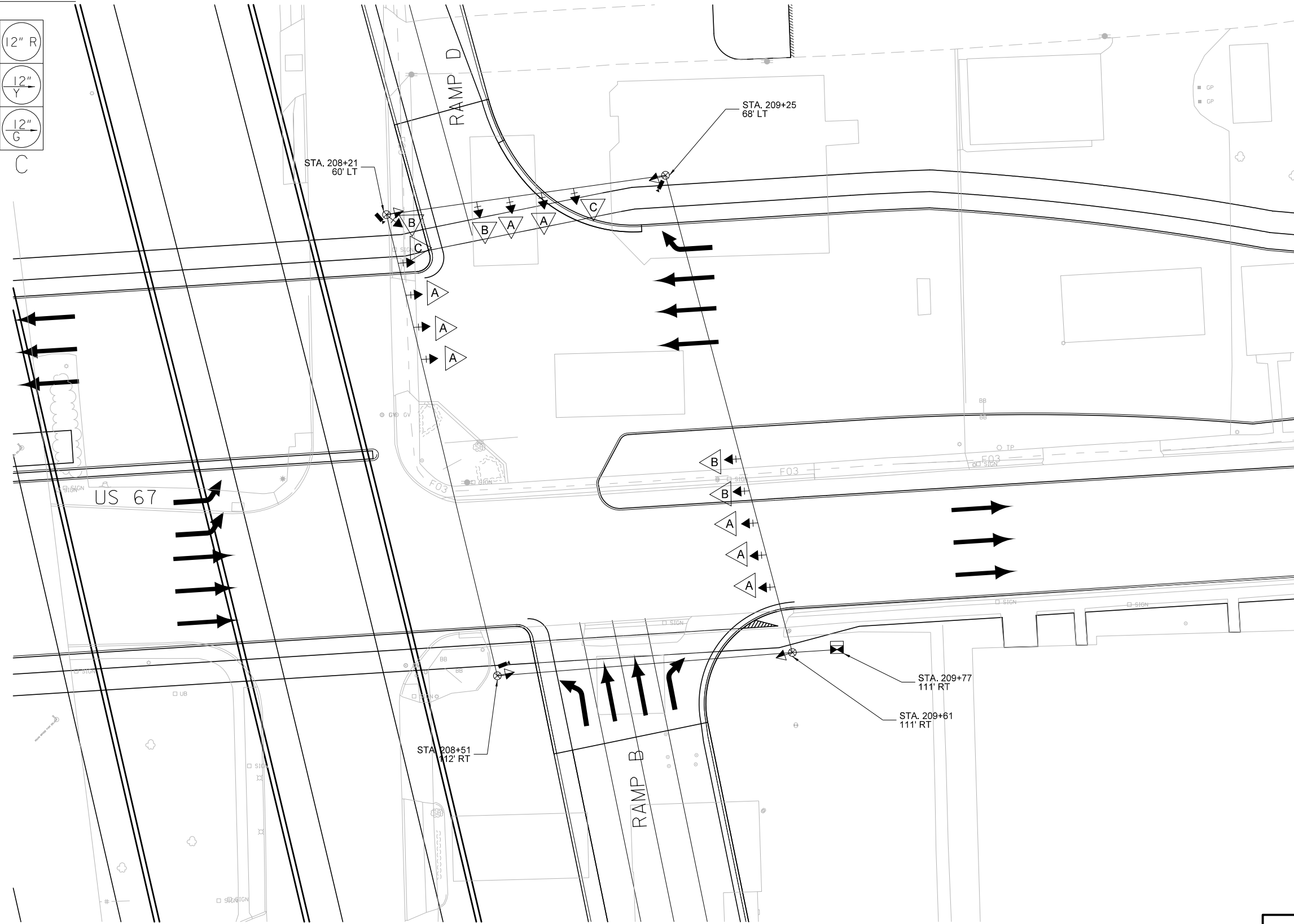
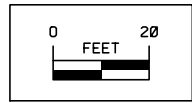
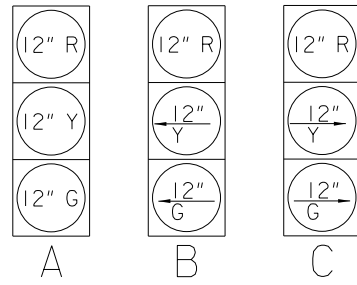
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DETECTOR UNITS SHALL ALLOW FOR ADJUSTABLE DELAY SETTING. INITIAL DELAY SETTING SHALL BE 5 SECONDS.

**TRAFFIC SIGNAL PLAN**  
**STATE STREET AND 12TH STREET**

LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11

TRAFFIC SIGNAL FACES





**TRAFFIC SIGNAL PLAN**  
**US 67 AND WB RAMP B AND D**

LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11

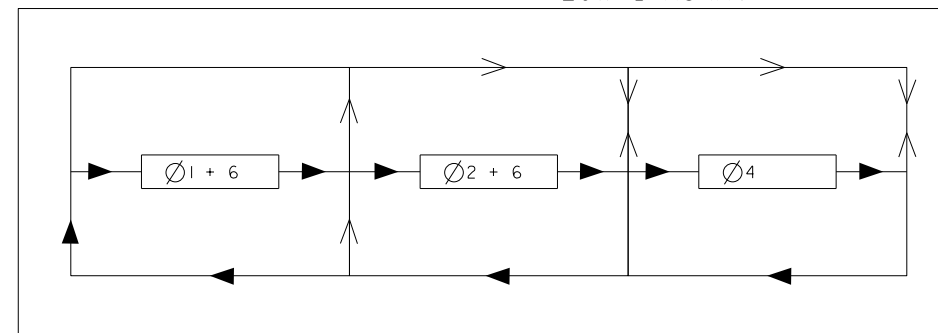
TRAFFIC PHASING AND COLOR SCHEME

TRAFFIC PHASING LEGEND

	ACTUATED VEHICLE MOVEMENT
	PARTIALLY RESTRICTED VEHICLE MOVEMENT
R/W	RIGHT OF WAY INTERVAL
Ø	TRAFFIC PHASE
R	CIRCULAR RED
Y	CIRCULAR YELLOW
G	CIRCULAR GREEN
YA	YELLOW ARROW
GA	GREEN ARROW
AO	ALL OTHERS

APPROACH	FACE	Ø1 + 6		Ø2 + 6		Ø4		CLEAR TO Ø		FLASH	
		CLEAR TO Ø		CLEAR TO Ø		CLEAR TO Ø		CLEAR TO Ø			
		R/W	2+6	AO	R/W	AO	R/W	AO	R/W		AO
EASTBOUND	B	GA	YA	YA	R	R		R	R		R
	A	G	G	Y	G	Y		R	R		R
WESTBOUND	A	R	R	R	G	Y		R	R		R
NORTHBOUND	C	R	R	R	R	R		GA	YA		R
	B	R	R	R	R	R		GA	YA		R
	A	R	R	R	R	R		G	Y		R

FLOW DIAGRAM



THE RED CLEARANCE INTERVAL SHALL BE INCLUDED IN THE COLOR SEQUENCE WHEN CALLED FOR BY THE CONTROLLER TIMING.

NOTE:

-  INDICATES FLOW PATH WITH ALL PHASES DEMANDED
-  INDICATES OPTIMAL FLOW PATHS WHEN SOME PHASES ARE NOT DEMANDED AT THE TIME OF RIGHT-OF-WAY TRANSFER

SUGGESTED TIMINGS (SECONDS)

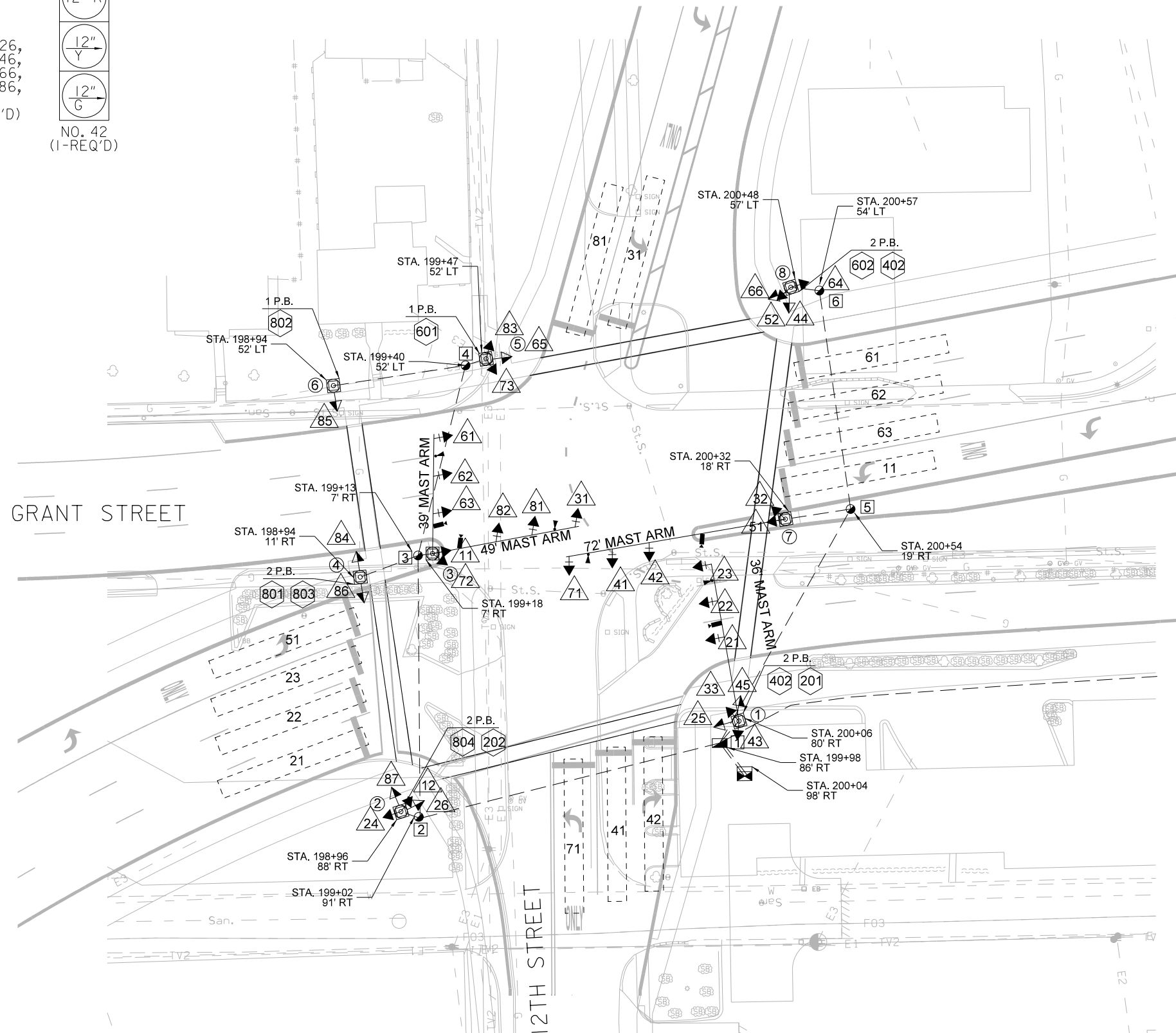
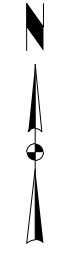
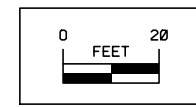
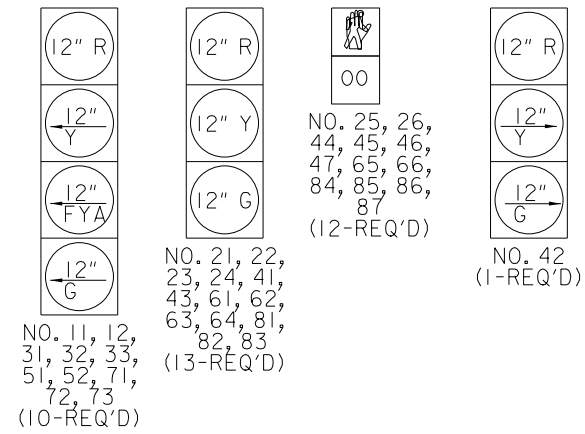
PHASE	1	2	3	4	5	6	7	8
MINIMUM GREEN								
PASSAGE								
MAXIMUM I								
MAXIMUM II								
YELLOW CHANGE								
RED CLEARANCE								
WALK								
PEDESTRIAN CLEARANCE								
ADDED INITIAL								
TIME TO REDUCE								
TIME BEFORE REDUCTION								
MAXIMUM GAP								

ALL TEMPORARY TRAFFIC SIGNAL TIMING WILL BE ESTABLISHED BY THE DEPARTMENT OF TRANSPORTATION AND MAY BE ADJUSTED THROUGHOUT CONSTRUCTION.

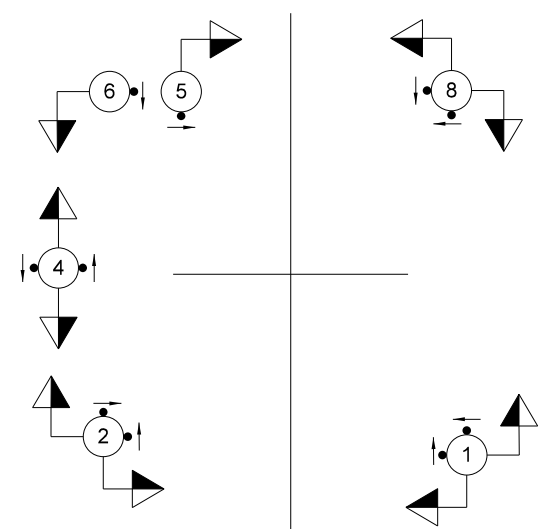
DETECTOR UNITS SHALL ALLOW FOR ADJUSTABLE DELAY SETTING. INITIAL DELAY SETTING SHALL BE 5 SECONDS.

**TRAFFIC SIGNAL PLAN**  
**US 67 AND WB RAMP B AND D**

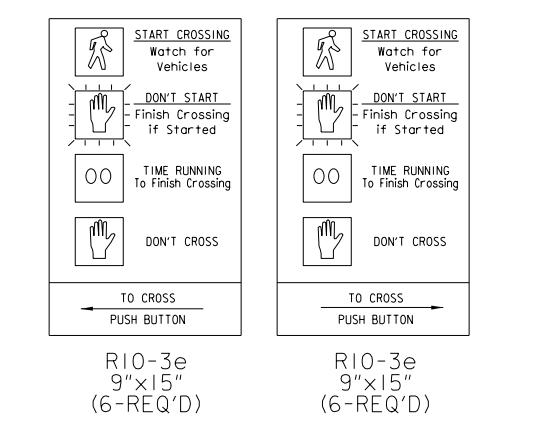
TRAFFIC SIGNAL FACES



PEDESTRIAN PUSHBUTTON AND SIGNAL FACE MOUNTING



PEDESTRIAN PUSHBUTTON SIGNS



TRAFFIC SIGNAL PLAN  
GRANT STREET AND  
12TH STREET

LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11



SUGGESTED TIMINGS (SECONDS)

PHASE	1	2	3	4	5	6	7	8
MINIMUM GREEN	7	10	7	7	7	10	7	7
PASSAGE	3	3	3	3	3	3	3	3
MAXIMUM I	30	60	30	30	30	60	30	30
MAXIMUM II								
YELLOW CHANGE	4	4	4	4	4	4	4	4
RED CLEARANCE	1	2	1	3	1	2	1	3
WALK		7		7		7		7
PEDESTRIAN CLEARANCE		23		17		23		17
ADDED INITIAL								
TIME TO REDUCE								
TIME BEFORE REDUCTION								
MAXIMUM GAP								

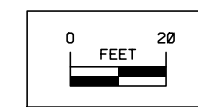
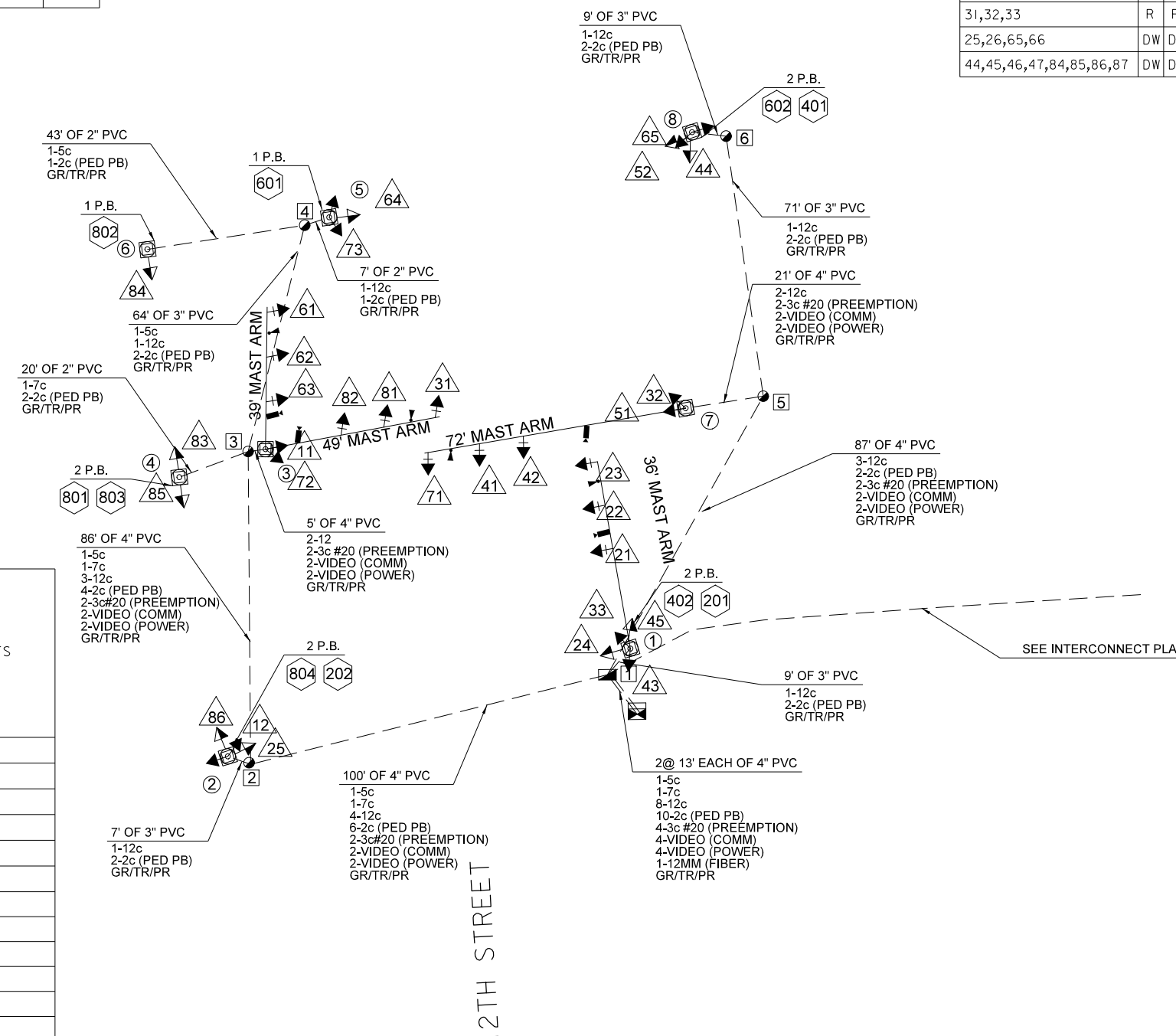
PHASE SEQUENCE CHART

PHASE NUMBER	PHASE				FLASHING														
	01+05	02+06	03+07	04+08															
61,62,63,64	R	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R
11,12	G	G	Y	R	FY	FY	Y	R	R	R	R	R	R	R	R	R	R	R	R
21,22,23,24	R	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R
51,52	G	G	Y	R	FY	FY	Y	R	R	R	R	R	R	R	R	R	R	R	R
41,42,43	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y	R	R
71,72,73	R	R	R	R	R	R	R	R	G	G	Y	R	FY	FY	Y	R	R	R	R
81,82,83	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y	R	R
31,32,33	R	R	R	R	R	R	R	R	G	G	Y	R	FY	FY	Y	R	R	R	R
25,26,65,66	DW	DW	DW	DW	W	FDW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	-
44,45,46,47,84,85,86,87	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	W	FDW	DW	DW	-

GRANT STREET

DETECTOR SUMMARY

DETECTOR NUMBER	LOOP SIZE	PED. PUSH BUTTON	DELAY TIME (SEC.)	PHASE CALLED	NO. OF CHANNELS	PRESENCE MODE	IMPULSE MODE	WIRED IN SERIES	COMMENTS
61,62,63	VIDEO			6	1	X			
11	VIDEO			1	1	X			
21,22,23	VIDEO			2	1	X			
51	VIDEO			5	1	X			
41,42	VIDEO			4	1	X			
71	VIDEO			7	1	X			
81	VIDEO			8	1	X			
31	VIDEO			3	1	X			
201,202		X		2					
401,402,403,404		X		4					
601,602		X		6					
801,802,803,804		X		8					

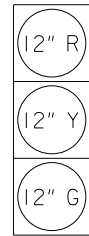


**TRAFFIC SIGNAL PLAN**  
**GRANT STREET AND 12TH STREET**

LAYOUT  
DRAWN  
REVIEWED

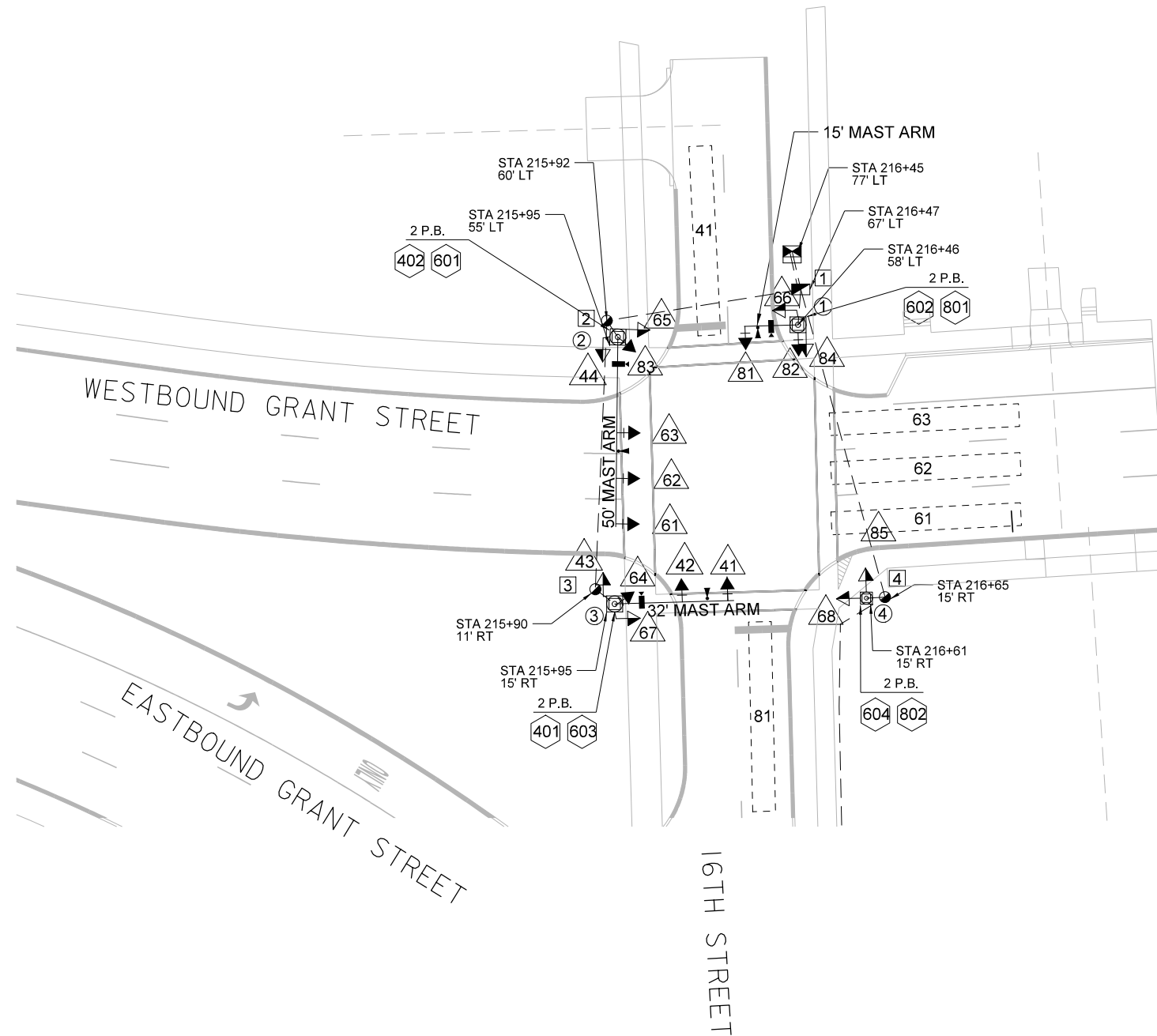
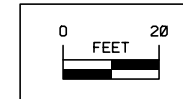
TMA 4/11/11  
RLA 4/8/11  
KMB 4/13/11

TRAFFIC SIGNAL FACES

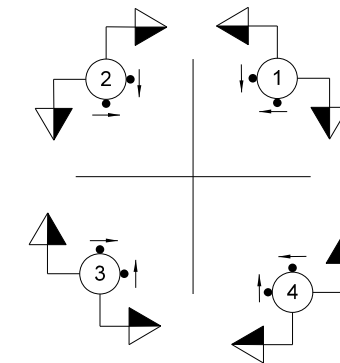


NO. 43, 44,  
65, 66, 67,  
68, 84, 85,  
(8-REQ'D)

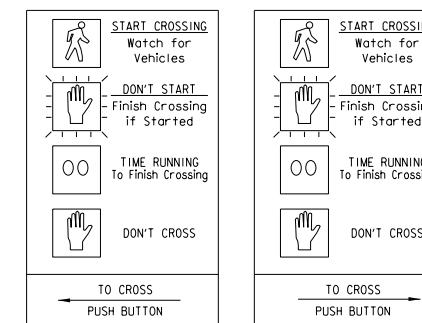
NO. 41, 42,  
61, 62, 63,  
64, 81, 82,  
83  
(9-REQ'D)



PEDESTRIAN PUSHBUTTON AND SIGNAL FACE MOUNTING



PEDESTRIAN PUSHBUTTON SIGNS



R10-3e  
9"x15"  
(4-REQ'D)

R10-3e  
9"x15"  
(4-REQ'D)

TRAFFIC SIGNAL PLAN

WESTBOUND GRANT AND 16TH STREET

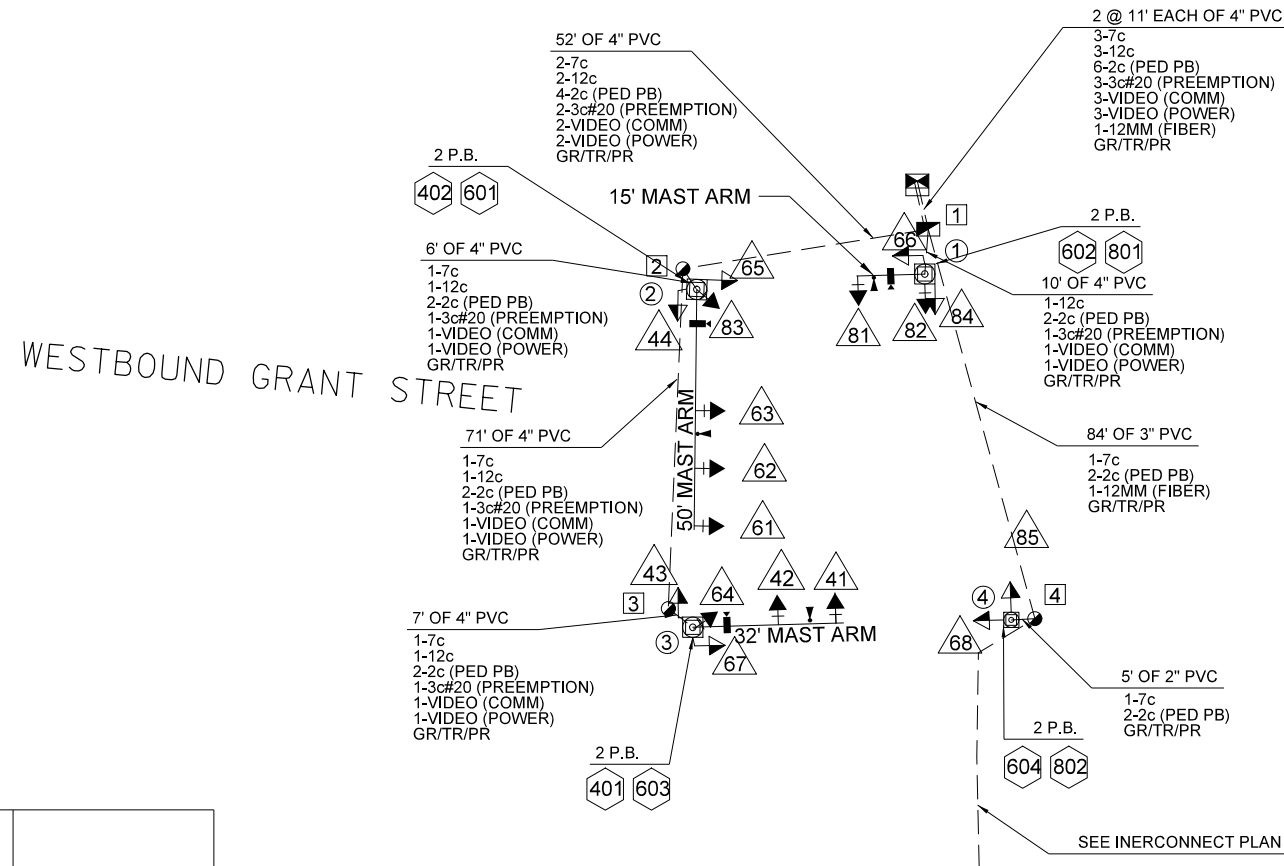
LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11

### SUGGESTED TIMINGS (SECONDS)

PHASE	1	2	3	4	5	6	7	8
MINIMUM GREEN				7		10		7
PASSAGE				3		3		3
MAXIMUM I				30		90		30
MAXIMUM II								
YELLOW CHANGE				4		4		4
RED CLEARANCE				2		1		2
WALK				7		7		7
PEDESTRIAN CLEARANCE				13		9		13
ADDED INITIAL								
TIME TO REDUCE								
TIME BEFORE REDUCTION								
MAXIMUM GAP								

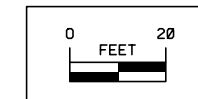
### PHASE SEQUENCE CHART

PHASE NUMBER	← 06 →		↑ 05 ↓		← 04 →						FLASHING	
	06		04+08									
61,62,63,64	G	G	Y	R	R	R	Y	R				R
41,42	R	R	R	R	G	G	Y	R				R
81,82	R	R	R	R	G	G	Y	R				R
65,66,67,68	W	FDW	DW	DW	DW	DW	DW	DW				-
43,44,84,85	DW	DW	DW	DW	W	FDW	DW	DW				-



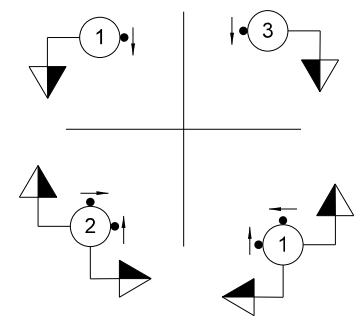
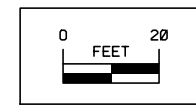
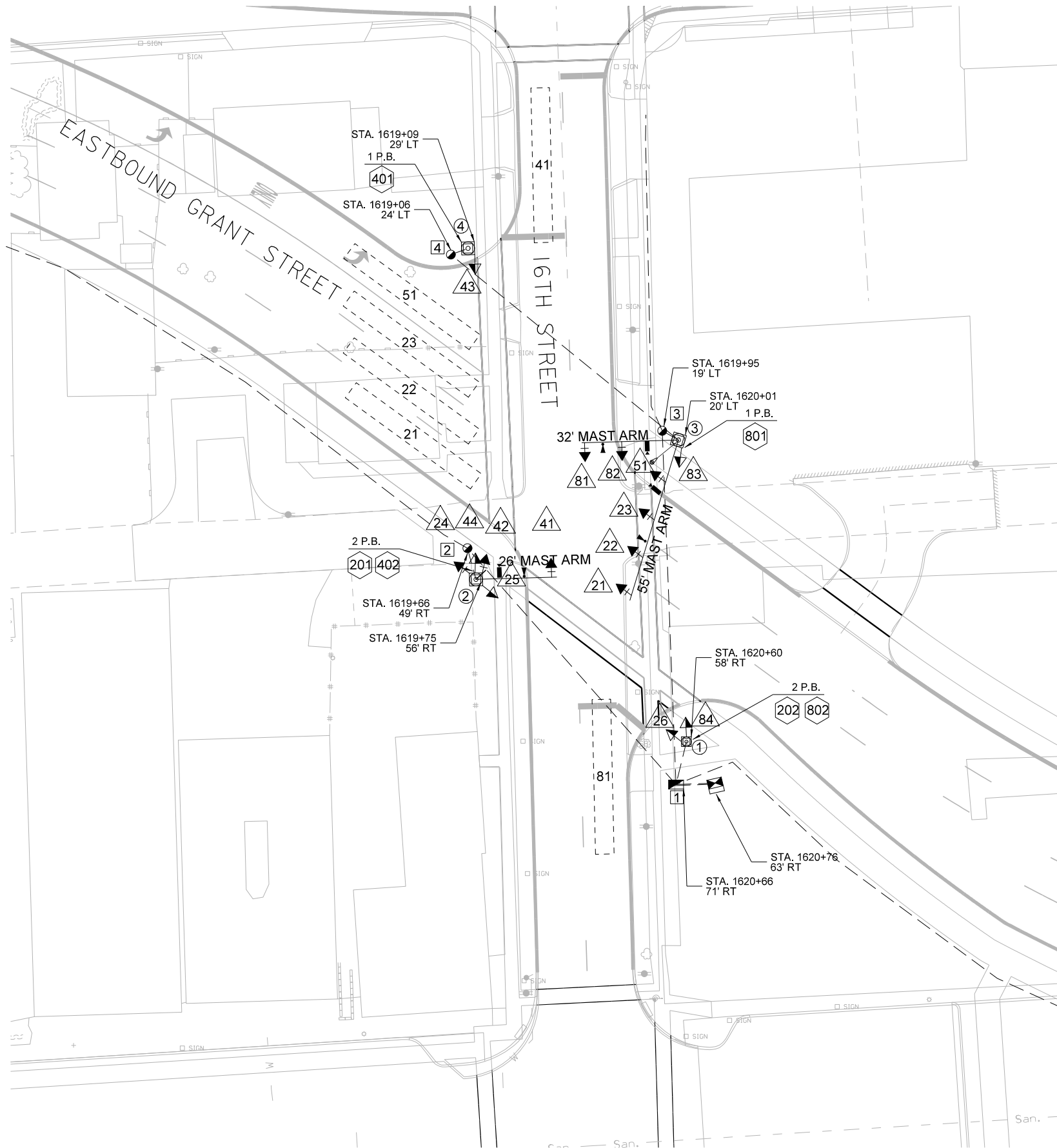
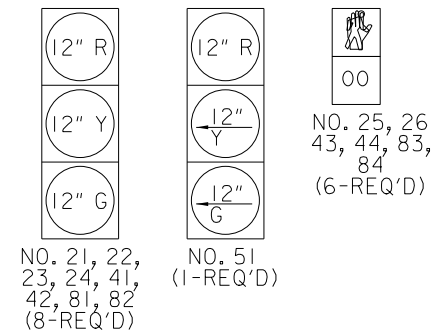
### DETECTOR SUMMARY

DETECTOR NUMBER	LOOP SIZE	PED. PUSH BUTTON	DELAY TIME (SEC.)	PHASE CALLED	NO. OF CHANNELS	PRESENCE MODE	IMPULSE MODE	WIRED IN SERIES	COMMENTS
61,62,63	VIDEO			6	1	X			
41	VIDEO			4	1	X			
81	VIDEO			8	1	X			
401,402		X		4					
601,602,603,604		X		6					
801,802		X		8					

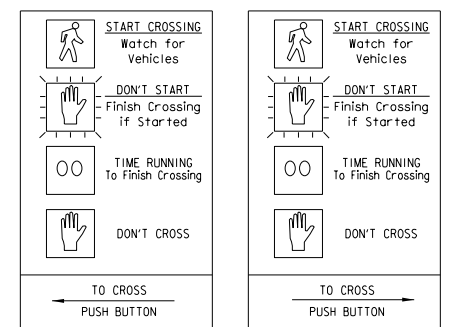


**TRAFFIC SIGNAL PLAN**  
**WESTBOUND GRANT AND 16TH STREET**

TRAFFIC SIGNAL FACES



PEDESTRIAN PUSHBUTTON SIGNS



R10-3e  
9"x15"  
(3-REQ'D)

R10-3e  
9"x15"  
(3-REQ'D)

**TRAFFIC SIGNAL PLAN**  
**EASTBOUND GRANT AND 16TH STREET**

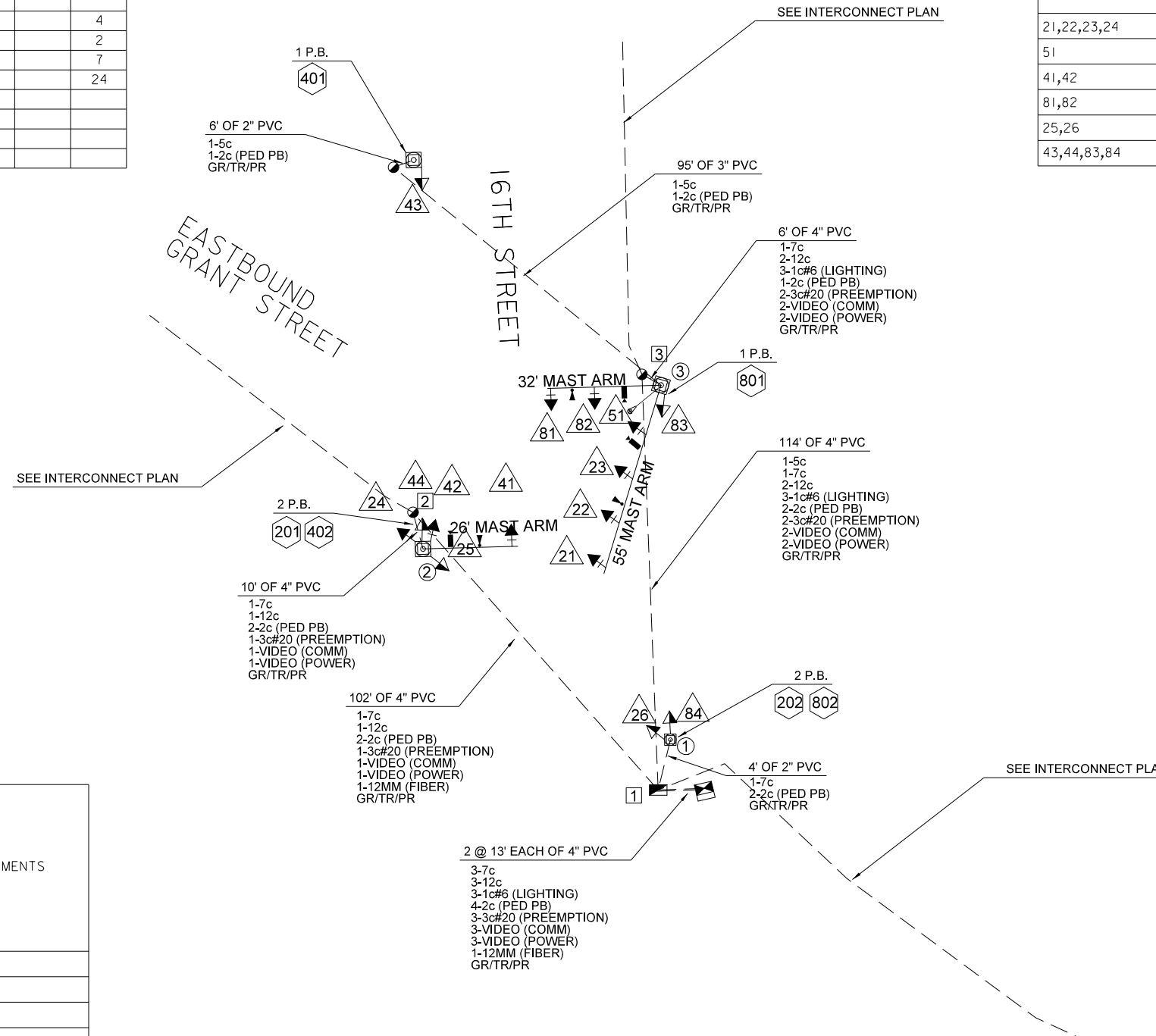
LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11

### SUGGESTED TIMINGS (SECONDS)

PHASE	1	2	3	4	5	6	7	8
MINIMUM GREEN		10		7				7
PASSAGE		3		3				3
MAXIMUM I		90		30				30
MAXIMUM II								
YELLOW CHANGE		4		4				4
RED CLEARANCE		1		2				2
WALK		7		7				7
PEDESTRIAN CLEARANCE		18		24				24
ADDED INITIAL								
TIME TO REDUCE								
TIME BEFORE REDUCTION								
MAXIMUM GAP								

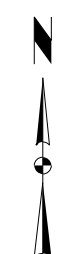
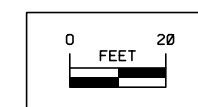
### PHASE SEQUENCE CHART

PHASE NUMBER	②		③		④		FLASHING
	02	04+08					
21,22,23,24	G	G	Y	R	R	R	R
51	G	G	Y	R	R	R	R
41,42	R	R	R	R	G	G	Y
81,82	R	R	R	R	G	G	Y
25,26	W	FDW	DW	DW	DW	DW	DW
43,44,83,84	DW	DW	DW	DW	W	FDW	DW



### DETECTOR SUMMARY

DETECTOR NUMBER	LOOP SIZE	PED. PUSH BUTTON	DELAY TIME (SEC.)	PHASE CALLED	NO. OF CHANNELS	PRESENCE MODE	IMPULSE MODE	WIRED IN SERIES	COMMENTS
21,22,23	VIDEO			2	1	X			
51	VIDEO			2	1	X			
41	VIDEO			4	1	X			
81	VIDEO			8	1	X			
201,202		X		2					
401,402		X		4					
801,802		X		8					

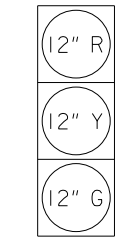


**TRAFFIC SIGNAL PLAN**  
**EASTBOUND GRANT AND 16TH STREET**

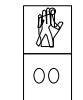
LAYOUT  
 DRAWN  
 REVIEWED

TMA 4/11/11  
 RLA 4/8/11  
 KNB 4/13/11

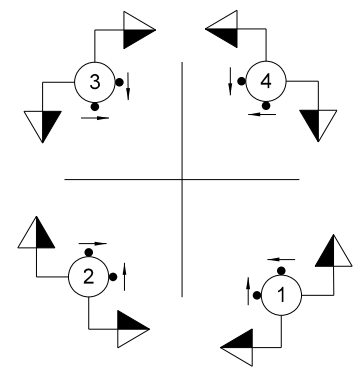
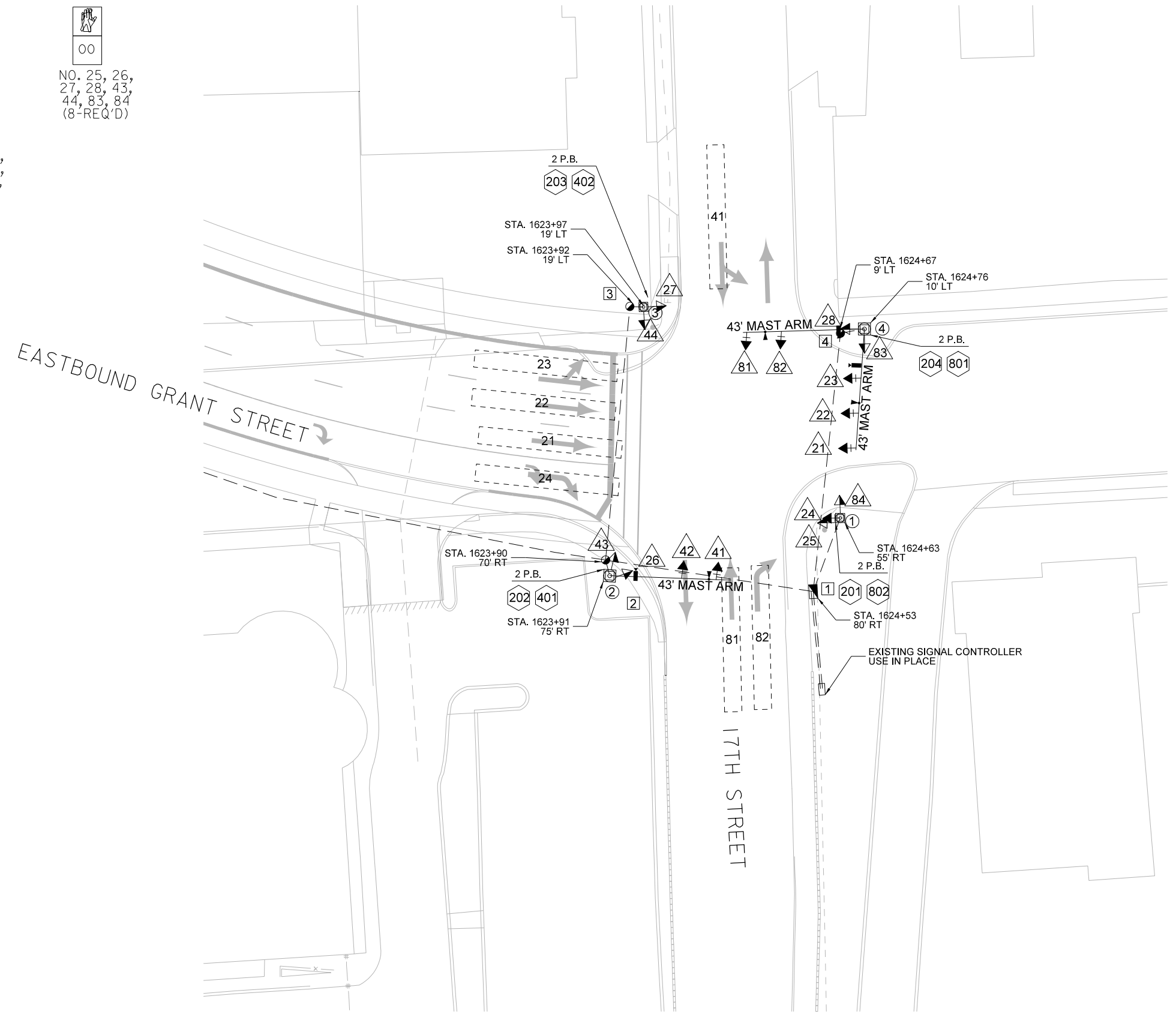
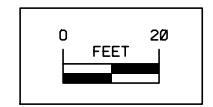
TRAFFIC SIGNAL FACES



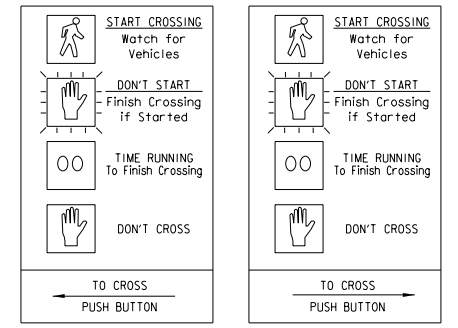
NO. 21, 22,  
23, 24, 25,  
41, 42, 81,  
82  
(7-REQ'D)



NO. 25, 26,  
27, 28, 43,  
44, 83, 84  
(8-REQ'D)



PEDESTRIAN PUSHBUTTON SIGNS



R10-3e  
9"x15"  
(4-REQ'D)

R10-3e  
9"x15"  
(4-REQ'D)

**TRAFFIC SIGNAL PLAN**  
**EASTBOUND GRANT AND 17TH STREET**

LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11

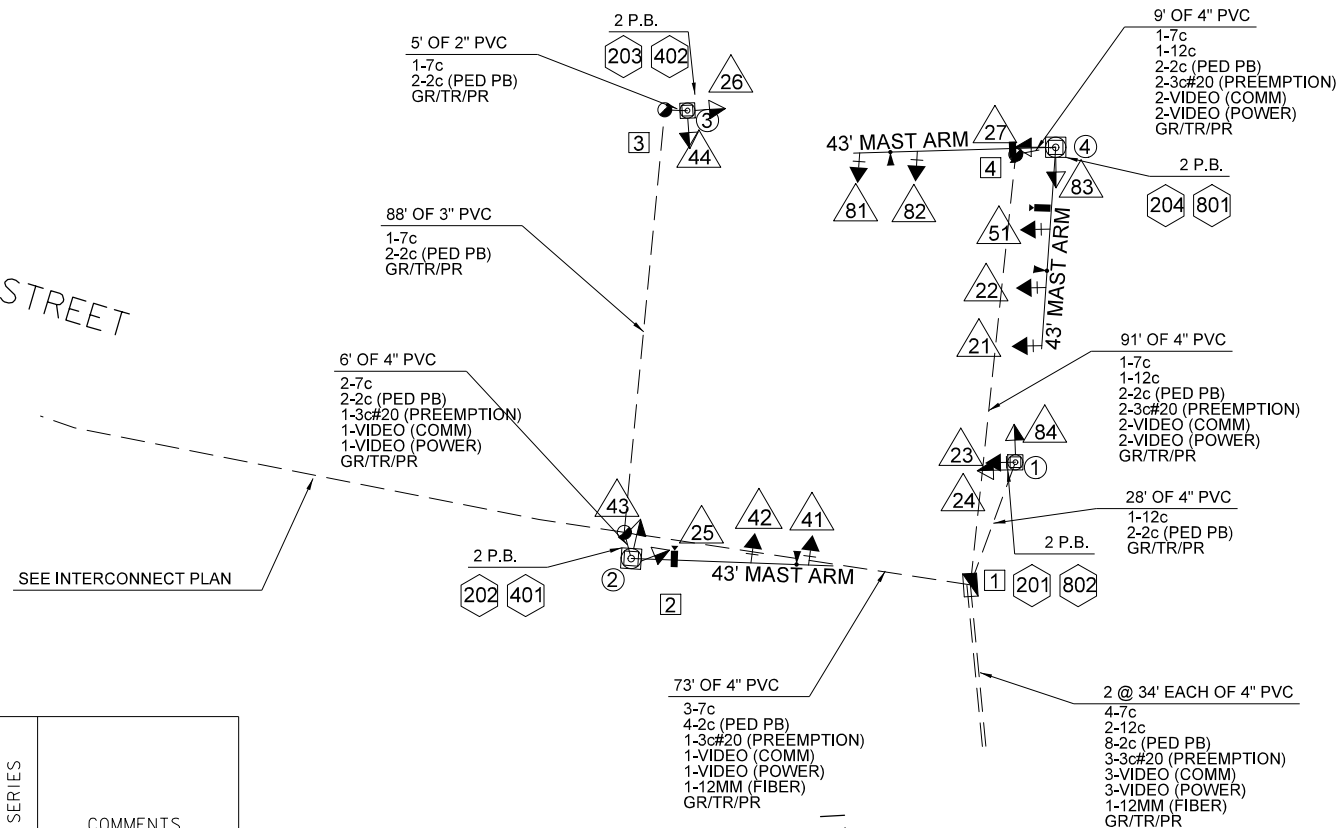
SUGGESTED TIMINGS (SECONDS)

PHASE	1	2	3	4	5	6	7	8
MINIMUM GREEN		10		7				7
PASSAGE		3		3				3
MAXIMUM I		90		30				30
MAXIMUM II								
YELLOW CHANGE		4		4				4
RED CLEARANCE		1		2				2
WALK		7		7				7
PEDESTRIAN CLEARANCE		15		20				20
ADDED INITIAL								
TIME TO REDUCE								
TIME BEFORE REDUCTION								
MAXIMUM GAP								

PHASE SEQUENCE CHART

PHASE NUMBER	②		⑧ ④								FLASHING	
	02		08+04									
21,22,23,24	G	G	Y	R	R	R	R					R
41,42	R	R	R	R	G	G	Y	R				R
81,82	R	R	R	R	G	G	Y	R				R
25,26,27,28	W	FDW	DW	DW	DW	DW	DW	DW				-
43,44,83,84	DW	DW	DW	DW	W	FDW	DW	DW				-

EASTBOUND GRANT STREET

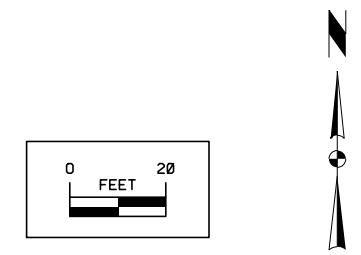


DETECTOR SUMMARY

DETECTOR NUMBER	LOOP SIZE	PED. PUSH BUTTON	DELAY TIME (SEC.)	PHASE CALLED	NO. OF CHANNELS	PRESENCE MODE	IMPULSE MODE	WIRED IN SERIES	COMMENTS
21,22,23	VIDEO			2	I	X			
51	VIDEO			2	I	X			
41	VIDEO			4	I	X			
81,82	VIDEO			8	I	X			
201,202,203,204		X		2					
401,402		X		4					
801,802		X		8					

SEE INTERCONNECT PLAN

17TH STREET



TRAFFIC SIGNAL PLAN  
EASTBOUND GRANT AND 17TH STREET

LAYOUT	TMA	4/11/11
DRAWN	RLA	4/8/11
REVIEWED	KNB	4/13/11

100-1D 10-18-05
<b>ITS PROJECT DESCRIPTION</b>
INTELLIGENT TRANSPORTATION SYSTEMS (ITS) PROJECT WORK SUMMARY
IOWA LOCAL ROADS PROJECT IM-074-1(226)4--13-82 INVOLVES FURNISHING AND INSTALLING ITS INFRASTRUCTURE ALONG US 67. ITS WORK INCLUDES FURNISHING AND INSTALLING CONDUIT, HANDHOLES, CABINETS, ARTERIAL DMS SIGN STRUCTURES, AND ITS ELECTRICAL CIRCUITS. PROJECT ALSO INCLUDES REMOVING AND RELOCATING THE AUTOMATED RAMP MANAGEMENT SYSTEM ALONG US 67.

111-01 04-17-12																
<b>ITS COORDINATED OPERATIONS</b>																
Other work in progress during the same period of time will include the construction of the projects listed. Coordinate operations with those of other contractors working within the same area.																
<table border="1"> <thead> <tr> <th>Project</th> <th>Type of Work</th> </tr> </thead> <tbody> <tr> <td>BRFIM-074-1(199)5--05-82</td> <td>Westbound Iowa Viaduct</td> </tr> <tr> <td>BRFIM-074-1(200)5--05-82</td> <td>Eastbound Iowa Viaduct</td> </tr> <tr> <td>ITS-074-1(221)5--05-82</td> <td>ITS Deployment and Integration</td> </tr> <tr> <td>ITS-074-1(222)5--05-82</td> <td>Fiber Optics</td> </tr> <tr> <td>IM-074-1(205)5--13-82</td> <td>Iowa Roadway B</td> </tr> <tr> <td>IM-074-1(206)5--13-82</td> <td>Iowa Roadway C</td> </tr> <tr> <td>IM-074-1(207)5--13-82</td> <td>Iowa Roadway D</td> </tr> </tbody> </table>	Project	Type of Work	BRFIM-074-1(199)5--05-82	Westbound Iowa Viaduct	BRFIM-074-1(200)5--05-82	Eastbound Iowa Viaduct	ITS-074-1(221)5--05-82	ITS Deployment and Integration	ITS-074-1(222)5--05-82	Fiber Optics	IM-074-1(205)5--13-82	Iowa Roadway B	IM-074-1(206)5--13-82	Iowa Roadway C	IM-074-1(207)5--13-82	Iowa Roadway D
Project	Type of Work															
BRFIM-074-1(199)5--05-82	Westbound Iowa Viaduct															
BRFIM-074-1(200)5--05-82	Eastbound Iowa Viaduct															
ITS-074-1(221)5--05-82	ITS Deployment and Integration															
ITS-074-1(222)5--05-82	Fiber Optics															
IM-074-1(205)5--13-82	Iowa Roadway B															
IM-074-1(206)5--13-82	Iowa Roadway C															
IM-074-1(207)5--13-82	Iowa Roadway D															

LISTING OF ITS CONDUIT WORK								
Conduit Run	Location		Conduit Length	Install 2" Conduit HDPE	INSTALL 24 SM FIBER	Install #2 Cable	Install #6 Cable	Install #12 Tracer
	From	To						
23A	DMS CABINET 1	HH23-1/HH23-2	15	2	1		3	1
23B	HH23-1/HH23-2	HH23-3/HH23-4	150	2	1		3	1
23C	HH23-3/HH23-4	HH23-5/HH23-6	135	2	1		3	1
23D	HH23-5	POWER SERVICE 1	15	1		3	2	
23E	HH23-5/HH23-6	HH24-1/HH24-2	200	2	1	3		1
24A	HH24-2	HH24-3	170	1		3		
24B	HH24-3	LED SIGN	15	1		3		
24C	HH24-3	HH24-4	60	1				
24D	HH24-4	AUTOMATED GATE	60	1				
24E	HH24-1	HH24-5	310	1	1			1
24F	HH24-5	HH24-6	80	1	1			1
24G	HH24-6	HH24-7	140	1	1			1
24H	HH24-7	HH24-8	110	1	1			1
24I	HH24-8	CABINET 3	20	1	2			1
24J	HH24-8	EX CABINET 4	340	1	1			1
24K	HH24-9	POWER SERVICE 2	20	1		3		
24L	HH24-8/HH24-9	HH24-10/HH24-11	215	2	1	3		1
24M	HH24-11	HH24-12	130	1		3		
24N	HH24-12	LED SIGN	20	1		3		
24O	HH24-8	HH24-13	100	2	1			2
24P	HH24-13	HH24-14	240	2	1			2
24Q	HH24-14	HH25-1	100	2	1			2
25A	HH25-1	HH25-2	145	1	1			1
25B	HH25-2	DMS CABINET 2	15	1	1			1
25C	HH25-3	DMS CABINET 2	15	1			3	
25D	HH25-3	HH25-4	130	1			3	
25E	HH25-4	POWER SERVICE 3	70	1			3	
25F	HH25-1	HH25-5	360	2	1			2
25G	HH25-5	HH25-6	80	2	1			2
25H	HH25-6	HH25-7	150	2	1			2
25I	HH25-7	BETTENDORF FIBER	20	2	1			2

LISTING OF ITS HANDHOLE WORK				
HANDHOLE LABEL	HANDHOLE TYPE	Coil 24 SM FIBER	Coil #2 Cable	Coil #6 Cable
HH23-1	III	60		
HH23-2	I			
HH23-3	III			140
HH23-4	I			
HH23-5	I			
HH23-6	III			
HH24-1	III			
HH24-2	I			
HH24-3	I		140	
HH24-4	I			
HH24-5	III			
HH24-6	III			
HH24-7	III			
HH24-8	III	60		
HH24-9	I			
HH24-10	III			
HH24-11	I			
HH24-12	I		140	
HH24-13	III			
HH24-14	III			
HH25-1	III	60		
HH25-2	III	60		
HH25-3	I			140
HH25-4	I			
HH24-5	III			
HH25-6	III			
HH25-7	III	60		

LISTING OF ITS CABINET WORK				
Cabinet Label	Sheet Number	Cabinet Type	Pole Mount	Pad Mount
CABINET 1	N.23	DMS	X	
CABINET 2	N.24	FIBER HUB		X
CABINET 3	N.25	DMS	X	
EX CABINET 4	N.24	JOINT		X
1609 STATE ST	N.25	FIBER HUB		

ITS DESIGN	
I hereby certify that this engineering document was prepared by me or under my direct personal supervision and that I am a duly licensed Professional Engineer under the laws of the State of Iowa.	
Signature <u>STEVEN GARBE, P.E.</u>	Date
Printed or Typed Name	
My license renewal date is December 31, 20 <u>12</u> .	
Pages or sheets covered by this seal: <u>N.20-N.33</u>	

ITS QUANTITIES



### GENERAL NOTES

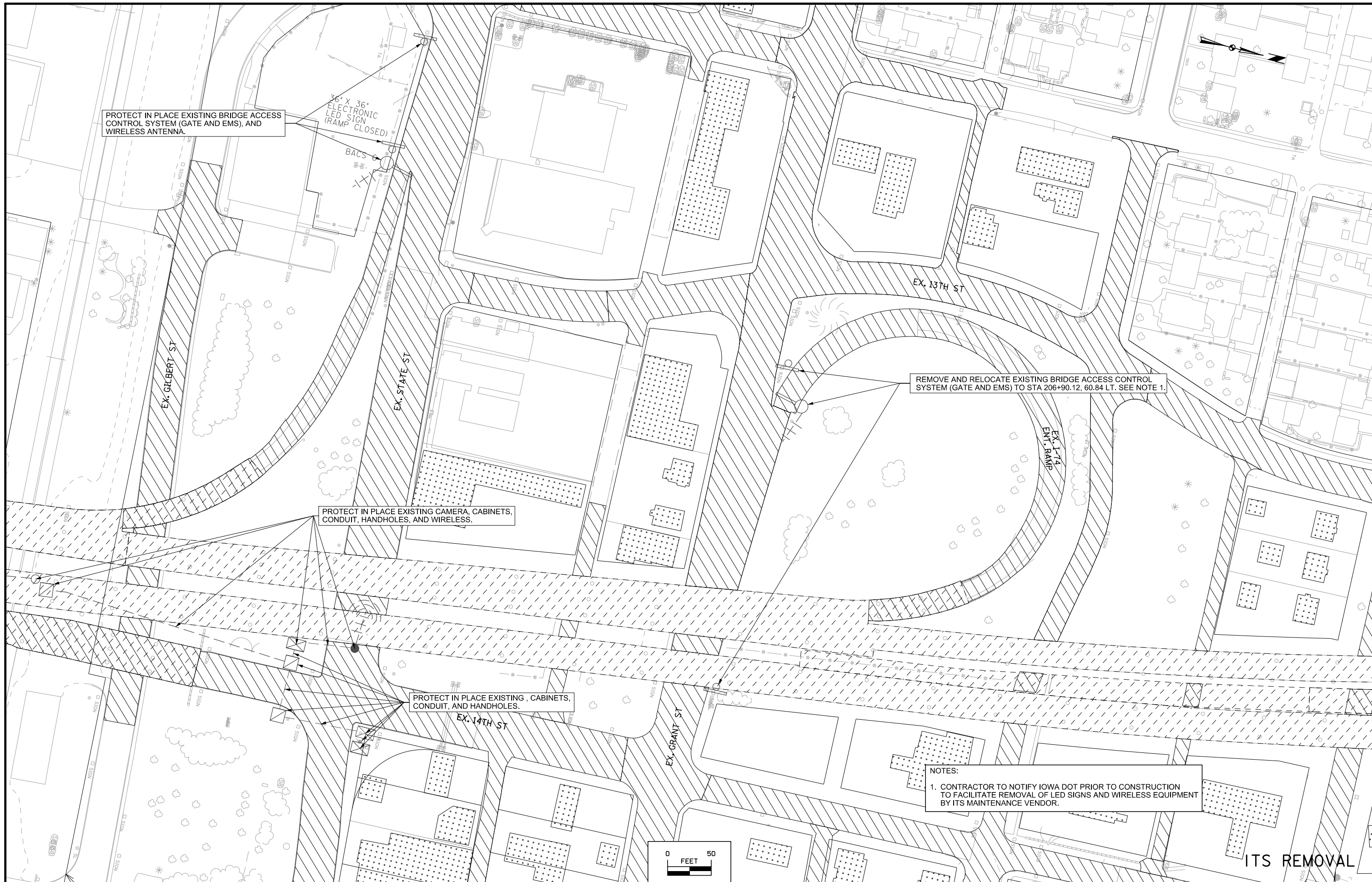
1. THE CONTRACTOR'S BID SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIAL NECESSARY TO PROVIDE A COMPLETE AND FUNCTIONAL ITS INSTALLATION IN CONFORMANCE WITH THE PLANS AND SPECIFICATIONS.
2. THE PLAN LOCATIONS OF UNDERGROUND UTILITIES, WHEN SHOWN, ARE APPROXIMATE ONLY. IN ADDITION, A PORTION OF UTILITY INFORMATION MAY NOT HAVE BEEN PROVIDED. ALL UTILITIES SHALL BE LOCATED AND MARKED PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING UTILITIES AND LOCATOR SERVICES AND SCHEDULING THE LOCATION OF UNDERGROUND UTILITIES. THE CONTRACTOR SHALL ALSO CONTACT ANY AND ALL UTILITIES AND LOCAL GOVERNMENT AGENCIES NOT PARTICIPATING IN LOCATION SERVICES.
3. PROPOSED ITS EQUIPMENT LOCATIONS ARE APPROXIMATE AND MAY REQUIRE MODIFICATION TO AVOID CONFLICTS WITH UNDERGROUND UTILITIES OR OTHER OBSTRUCTIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ANY CONFLICTS WITH EXISTING UTILITIES AT SITES IN THE FIELD PRIOR TO INITIATION OF CONSTRUCTION AT THAT SITE. AS THE CCTV AND SENSOR LOCATIONS ARE LOCATION SENSITIVE, THE CONTRACTOR SHALL RECEIVE WRITTEN APPROVAL FROM THE ENGINEER PRIOR TO REVISING THE PLAN LOCATION OF ANY CONDUIT, POLES, FOUNDATIONS, OR CABINETS.
4. ABOVE GROUND RISERS SHALL BE RIGID STEEL CONDUIT. ALL OTHER CONDUIT SHALL BE HDPE CONDUIT. RIGID P.V.C. CONDUIT (SCHEDULE 40 OR AS APPROVED) MAY BE SUBSTITUTED FOR CONDUIT RUNS UNDER 50 FEET.
5. ANY AND ALL IMPROVEMENTS SUCH AS ASPHALT OR CONCRETE PAVEMENTS, CURBS, GUTTERS, WALKS, DRAINAGE DITCHES, CULVERTS, DRAIN TILES, EMBANKMENTS, SHRUBS, TREES, GRASS, SOD, ETC., IF DAMAGED, SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS (OR BETTER) AS DIRECTED BY THE ENGINEER.
6. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR EXISTING CONDUIT, CONDUCTORS, OR OTHER FACILITIES DAMAGED DURING CONSTRUCTION. ALL EXISTING INFRASTRUCTURE REMOVED OR DAMAGED BY THE CONTRACTOR SHALL BE REPLACED IN KIND BY THE CONTRACTOR, WITH NO ADDITIONAL COMPENSATION.
7. THE CONTRACTOR SHALL NOT DISTURB ANY EXISTING UTILITIES EXCEPT AS SPECIFICALLY DEFINED WITHIN THE SCOPE OF WORK FOR THIS CONTRACT. WHERE WORK AFFECTS OR IS AFFECTED BY THE EXISTING UTILITIES, THE WORK SHALL BE COORDINATED WITH THE UTILITY COMPANY AND/OR OWNER. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING WITH THE DOT.
8. UTILITY COMPANIES WHOSE FACILITIES ARE SHOWN ON THE PLANS OR KNOWN TO BE WITHIN THE CONSTRUCTION LIMITS SHALL BE NOTIFIED BY THE CONTRACTOR OF THE STARTING CONSTRUCTION DATE.
9. ALL ITS CONDUITS SHALL INCLUDE ONE POLYPROPYLENE PULL ROPE WITH A MINIMUM 2,670N PROPER TENSILE STRENGTH (COST INCIDENTAL TO THE CONDUIT).
10. ALL CONDUIT SHALL BE PLACED AT A 36 INCH MINIMUM COVER UNLESS OTHERWISE SPECIFIED ON THE PLANS.
11. THE CONTRACTOR SHALL BORE UNDER ANY EXISTING ASPHALT OR CONCRETE PAVEMENT, RAILROAD, OR OTHER STRUCTURE.
12. THE CONTRACTOR SHALL TRENCH ALL CONDUIT WHERE EXISTING CONDITIONS ALLOW UNLESS OTHERWISE SPECIFIED ON THE PLANS. THE CONTRACTOR MAY BORE IN LIEU OF TRENCHING AT THE CONTRACTOR'S EXPENSE.
13. THE MINIMUM BENDING RADIUS OF CONDUIT AND MULTIDUCT SYSTEMS SHALL BE THE LARGER OF THE FIBER OPTIC CABLE MANUFACTURER'S RECOMMENDATION OR NATIONAL ELECTRIC CODE (NEC) REQUIREMENTS. ALL CONDUIT SWEEP RADII SHALL BE GREATER AND/OR EQUAL TO 15 INCHES.
14. ALL WIRING AND GROUNDING SYSTEMS SHALL BE IN ACCORDANCE WITH THE NATIONAL ELECTRIC CODE.

### GENERAL NOTES

15. THIS PROJECT DOES NOT INCLUDE PURCHASING, OR INSTALLTION OF, ANY CAMERA OR SENSOR EQUIPMENT.
16. LINEAR MEASUREMENTS ARE TAKEN BETWEEN POLE BASE, HANDHOLE, AND JUNCTION BOX CENTERS AND DO NOT INCLUDE ALLOWANCES FOR VERTICAL RISES OR SPLICES.
17. MATERIALS AND CONSTRUCTION SHALL CONFORM TO THE REQUIREMENTS OF ALL APPLICABLE SECTIONS INCLUDING BUT NOT LIMITED TO SECTION 2523 AND 2525 OF THE "IOWA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION, SERIES 2012" PLUS CURRENT SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.
18. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ANTICIPATE, COMMUNICATE, AND COORDINATE THIS WORK WITH ADJACENT CONSTRUCTION PROJECTS, THAT INCLUDE BUT LIMITED TO ADJACENT ROADWAY AND VIADUCT PROJECTS.

### GENERAL NOTES

ITS GENERAL NOTES



PROTECT IN PLACE EXISTING BRIDGE ACCESS CONTROL SYSTEM (GATE AND EMS), AND WIRELESS ANTENNA.

36" X 36" ELECTRONIC LED SIGN (RAMP CLOSED)

BACS

REMOVE AND RELOCATE EXISTING BRIDGE ACCESS CONTROL SYSTEM (GATE AND EMS) TO STA 206+90.12, 60.84 LT. SEE NOTE 1.

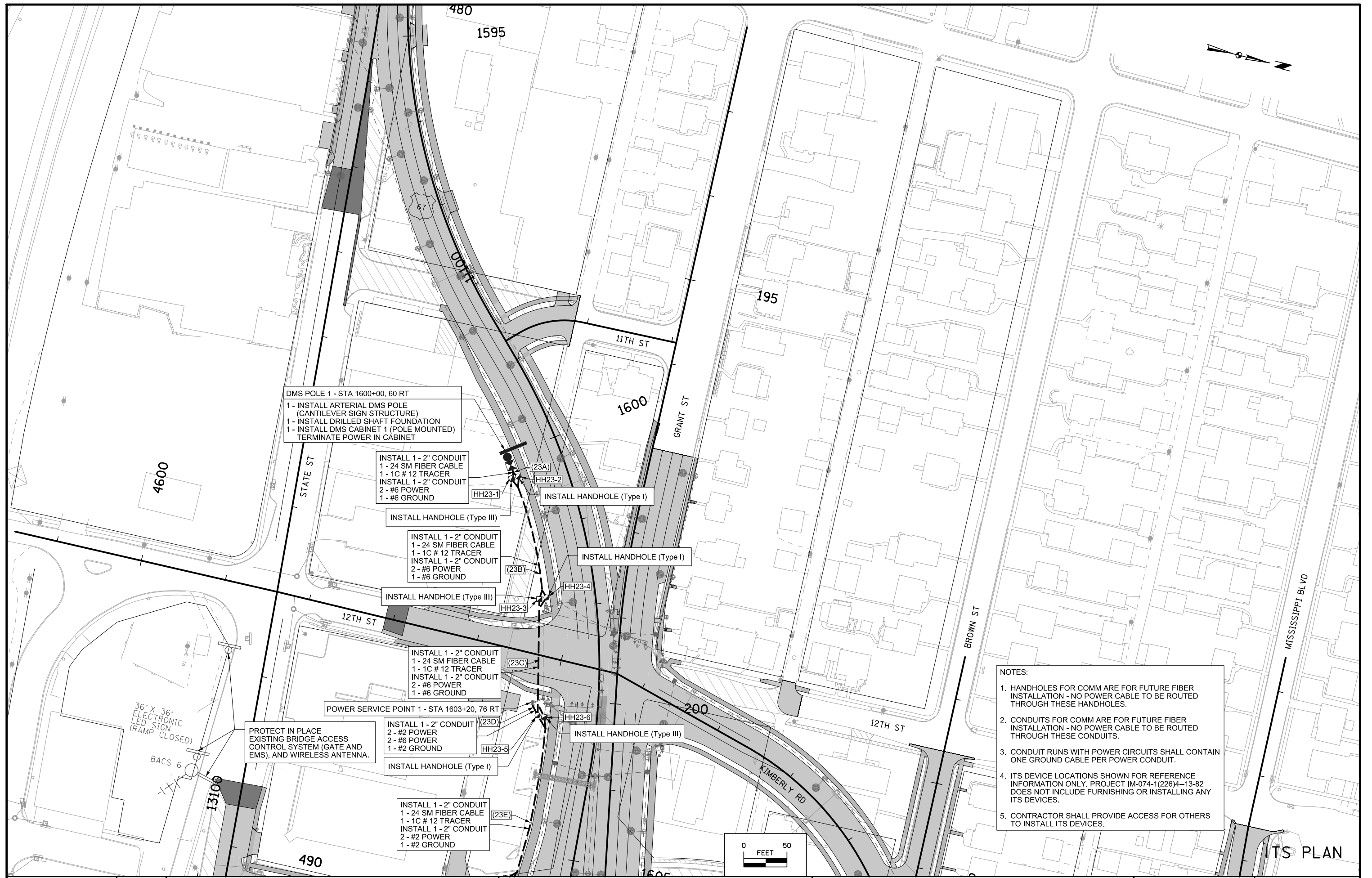
PROTECT IN PLACE EXISTING CAMERA, CABINETS, CONDUIT, HANDHOLES, AND WIRELESS.

PROTECT IN PLACE EXISTING , CABINETS, CONDUIT, AND HANDHOLES.

NOTES:  
1. CONTRACTOR TO NOTIFY IOWA DOT PRIOR TO CONSTRUCTION TO FACILITATE REMOVAL OF LED SIGNS AND WIRELESS EQUIPMENT BY ITS MAINTENANCE VENDOR.



ITS REMOVAL



DMS POLE 1 - STA 1600+00, 60 RT  
 1 - INSTALL ARTERIAL DMS POLE (CANTILEVER SIGN STRUCTURE)  
 1 - INSTALL DRILLED SHAFT FOUNDATION  
 1 - INSTALL DMS CABINET 1 (POLE MOUNTED) TERMINATE POWER IN CABINET

INSTALL 1 - 2" CONDUIT  
 1 - 24 SM FIBER CABLE  
 1 - 1C # 12 TRACER  
 INSTALL 1 - 2" CONDUIT  
 2 - #6 POWER  
 1 - #6 GROUND

INSTALL HANDHOLE (Type III)

INSTALL 1 - 2" CONDUIT  
 1 - 24 SM FIBER CABLE  
 1 - 1C # 12 TRACER  
 INSTALL 1 - 2" CONDUIT  
 2 - #6 POWER  
 1 - #6 GROUND

INSTALL HANDHOLE (Type III)

INSTALL 1 - 2" CONDUIT  
 1 - 24 SM FIBER CABLE  
 1 - 1C # 12 TRACER  
 INSTALL 1 - 2" CONDUIT  
 2 - #6 POWER  
 1 - #6 GROUND

POWER SERVICE POINT 1 - STA 1603+20, 76 RT

INSTALL 1 - 2" CONDUIT  
 2 - #2 POWER  
 2 - #6 POWER  
 1 - #2 GROUND

PROTECT IN PLACE EXISTING BRIDGE ACCESS CONTROL SYSTEM (GATE AND EMS), AND WIRELESS ANTENNA.

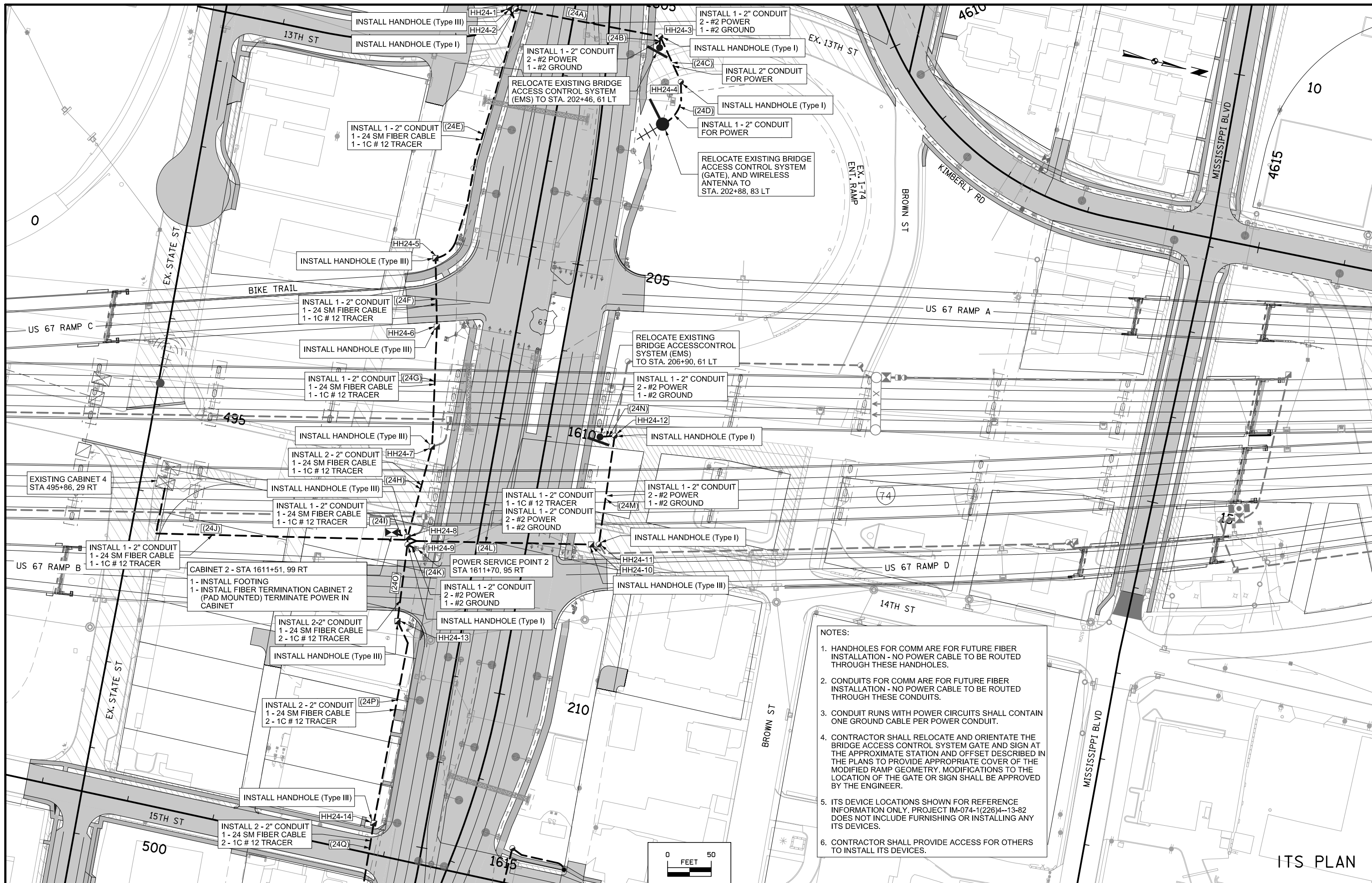
INSTALL HANDHOLE (Type I)

INSTALL 1 - 2" CONDUIT  
 1 - 24 SM FIBER CABLE  
 1 - 1C # 12 TRACER  
 INSTALL 1 - 2" CONDUIT  
 2 - #2 POWER  
 1 - #2 GROUND

- NOTES:
- HANDHOLES FOR COMM ARE FOR FUTURE FIBER INSTALLATION - NO POWER CABLE TO BE ROUTED THROUGH THESE HANDHOLES.
  - CONDUITS FOR COMM ARE FOR FUTURE FIBER INSTALLATION - NO POWER CABLE TO BE ROUTED THROUGH THESE CONDUITS.
  - CONDUIT RUNS WITH POWER CIRCUITS SHALL CONTAIN ONE GROUND CABLE PER POWER CONDUIT.
  - ITS DEVICE LOCATIONS SHOWN FOR REFERENCE INFORMATION ONLY. PROJECT IM-074-1(226)4--13-82 DOES NOT INCLUDE FURNISHING OR INSTALLING ANY ITS DEVICES.
  - CONTRACTOR SHALL PROVIDE ACCESS FOR OTHERS TO INSTALL ITS DEVICES.

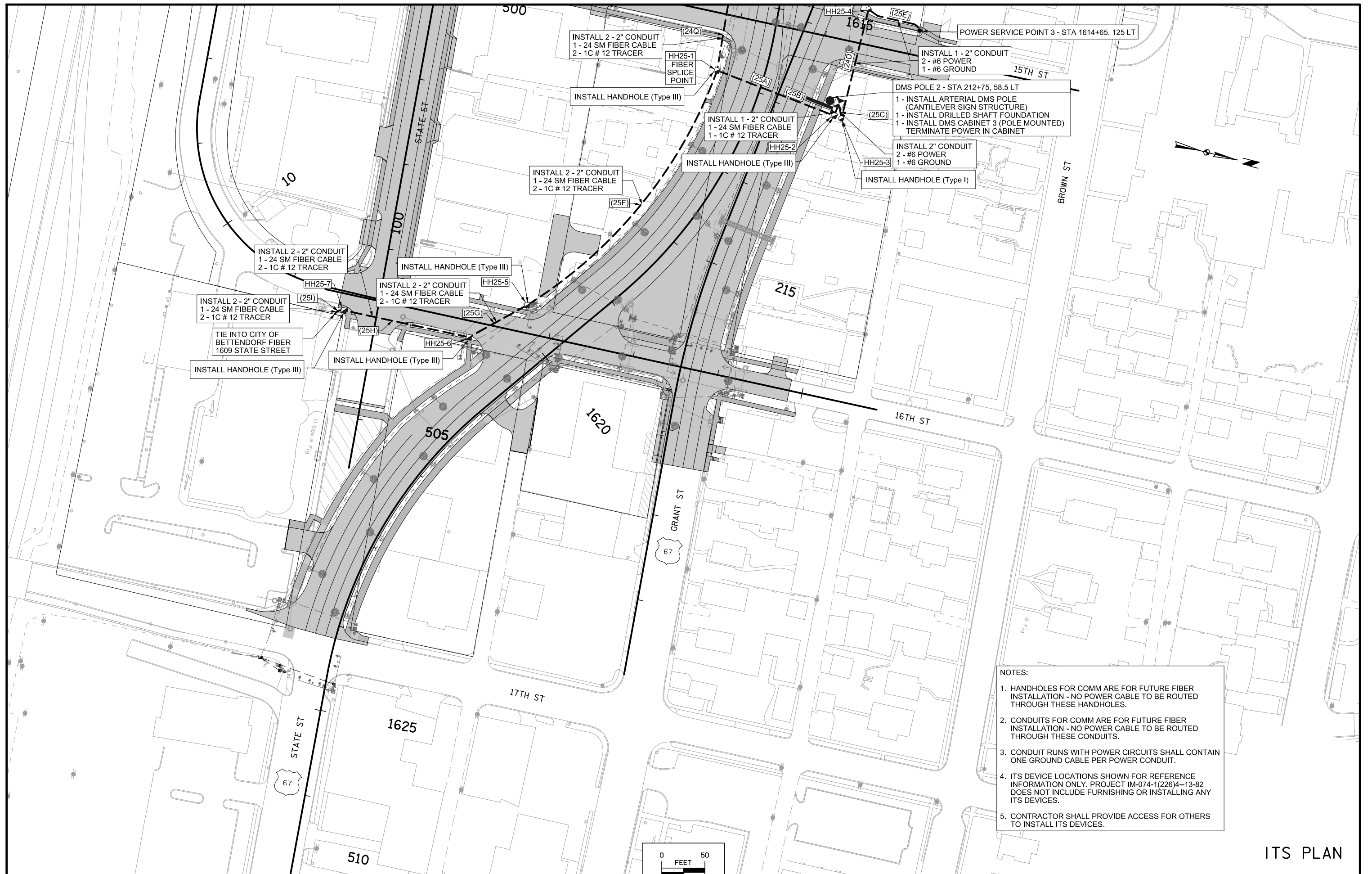


ITS PLAN

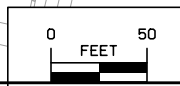


- NOTES:
1. HANDHOLES FOR COMM ARE FOR FUTURE FIBER INSTALLATION - NO POWER CABLE TO BE ROUTED THROUGH THESE HANDHOLES.
  2. CONDUITS FOR COMM ARE FOR FUTURE FIBER INSTALLATION - NO POWER CABLE TO BE ROUTED THROUGH THESE CONDUITS.
  3. CONDUIT RUNS WITH POWER CIRCUITS SHALL CONTAIN ONE GROUND CABLE PER POWER CONDUIT.
  4. CONTRACTOR SHALL RELOCATE AND ORIENTATE THE BRIDGE ACCESS CONTROL SYSTEM GATE AND SIGN AT THE APPROXIMATE STATION AND OFFSET DESCRIBED IN THE PLANS TO PROVIDE APPROPRIATE COVER OF THE MODIFIED RAMP GEOMETRY. MODIFICATIONS TO THE LOCATION OF THE GATE OR SIGN SHALL BE APPROVED BY THE ENGINEER.
  5. ITS DEVICE LOCATIONS SHOWN FOR REFERENCE INFORMATION ONLY. PROJECT IM-074-1(226)4--13-82 DOES NOT INCLUDE FURNISHING OR INSTALLING ANY ITS DEVICES.
  6. CONTRACTOR SHALL PROVIDE ACCESS FOR OTHERS TO INSTALL ITS DEVICES.

ITS PLAN



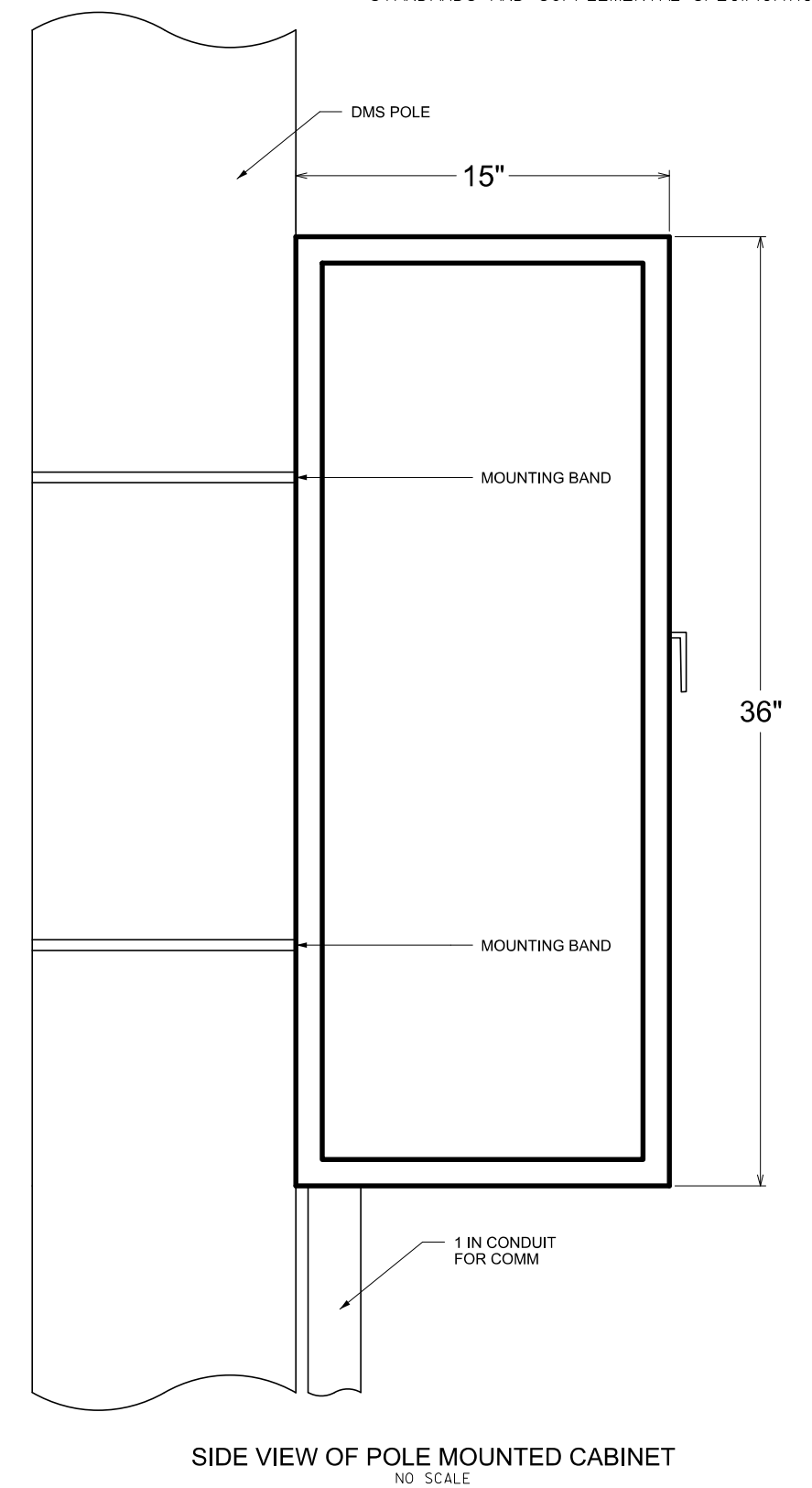
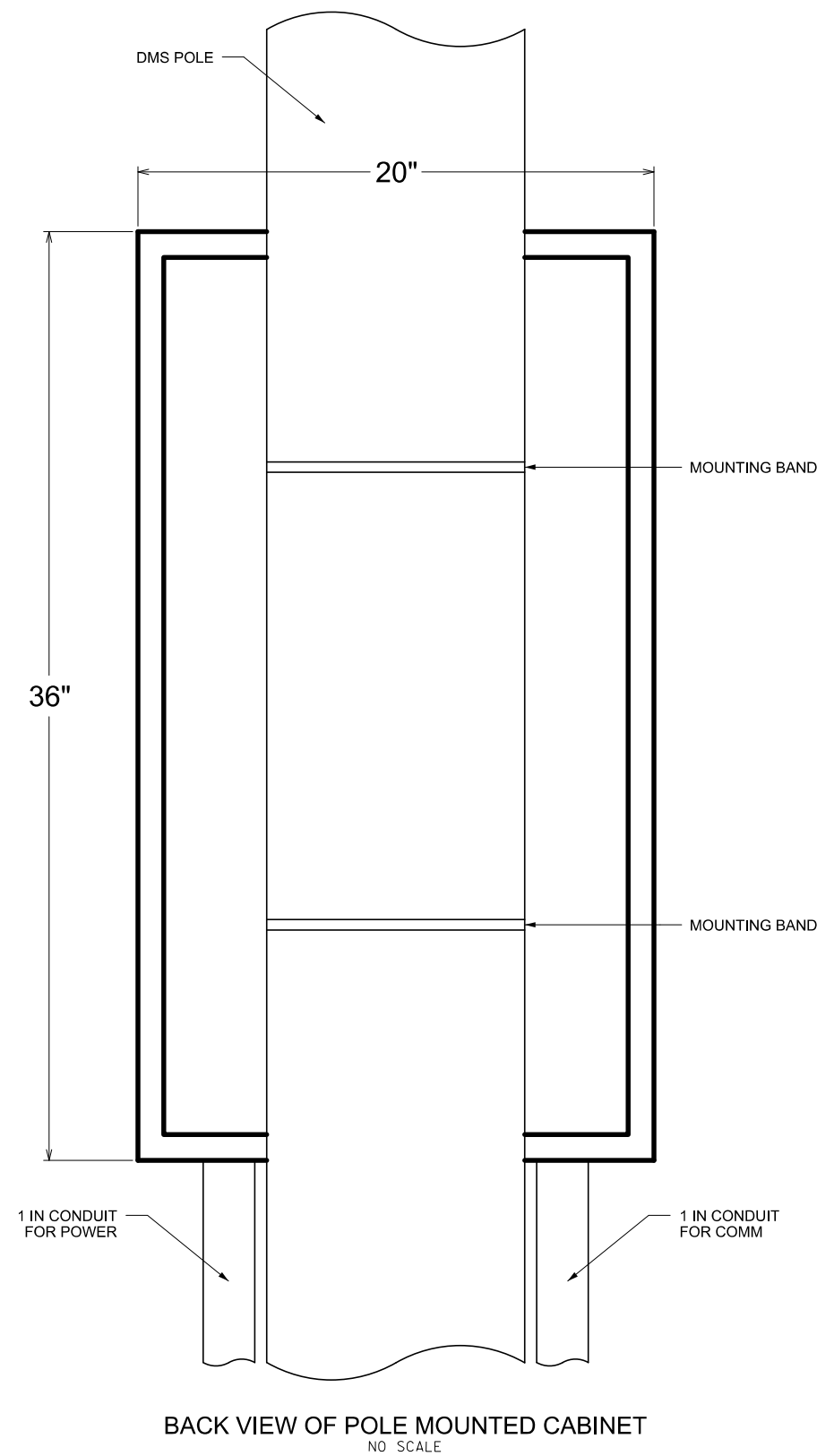
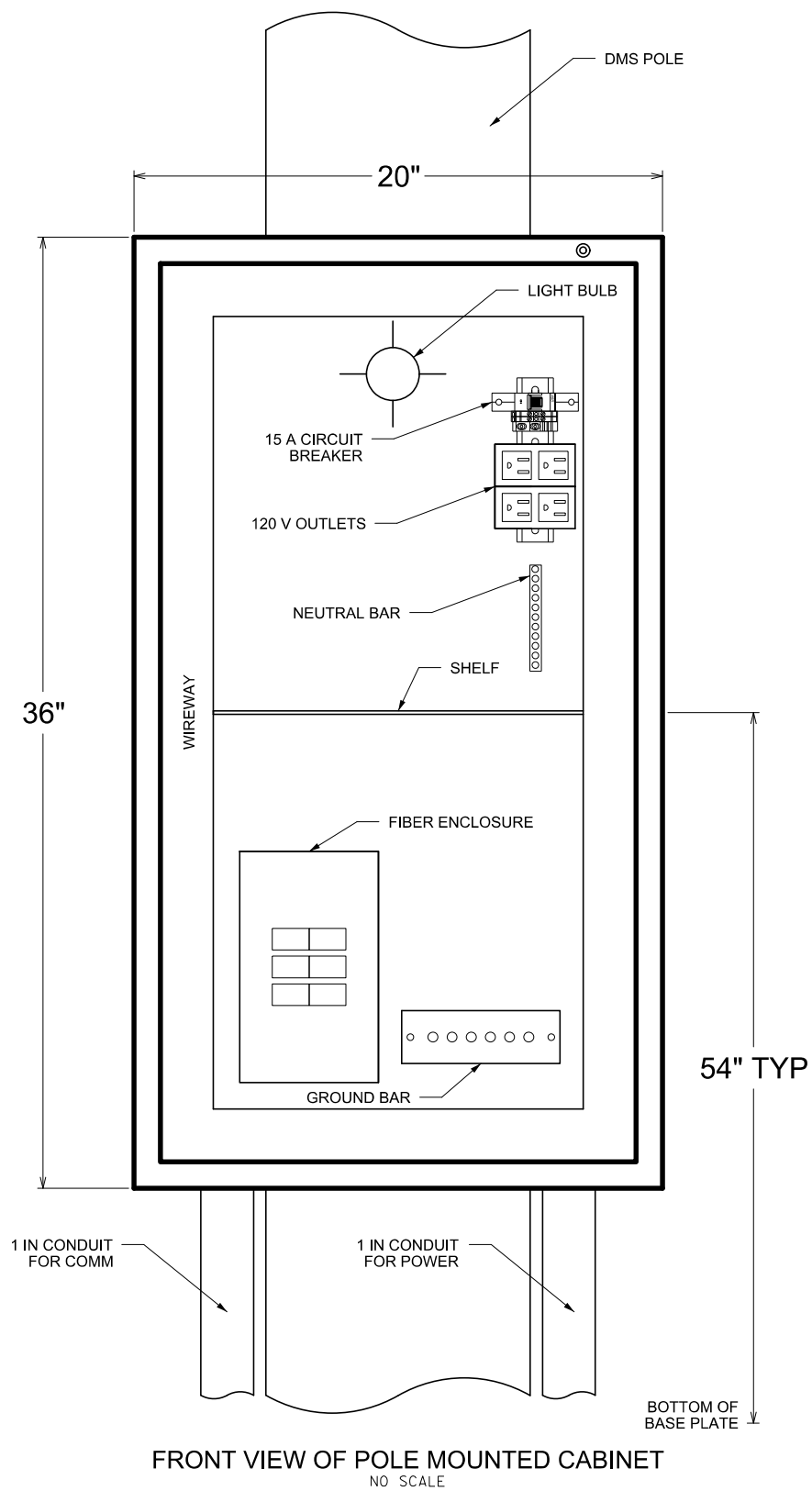
- NOTES:
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  - CONDUIT RUNS WITH POWER CIRCUITS SHALL CONTAIN ONE GROUND CABLE PER POWER CONDUIT.
  - ITS DEVICE LOCATIONS SHOWN FOR REFERENCE INFORMATION ONLY. PROJECT IM-074-1(226)4--13-82 DOES NOT INCLUDE FURNISHING OR INSTALLING ANY ITS DEVICES.
  - CONTRACTOR SHALL PROVIDE ACCESS FOR OTHERS TO INSTALL ITS DEVICES.



ITS PLAN

GENERAL NOTES

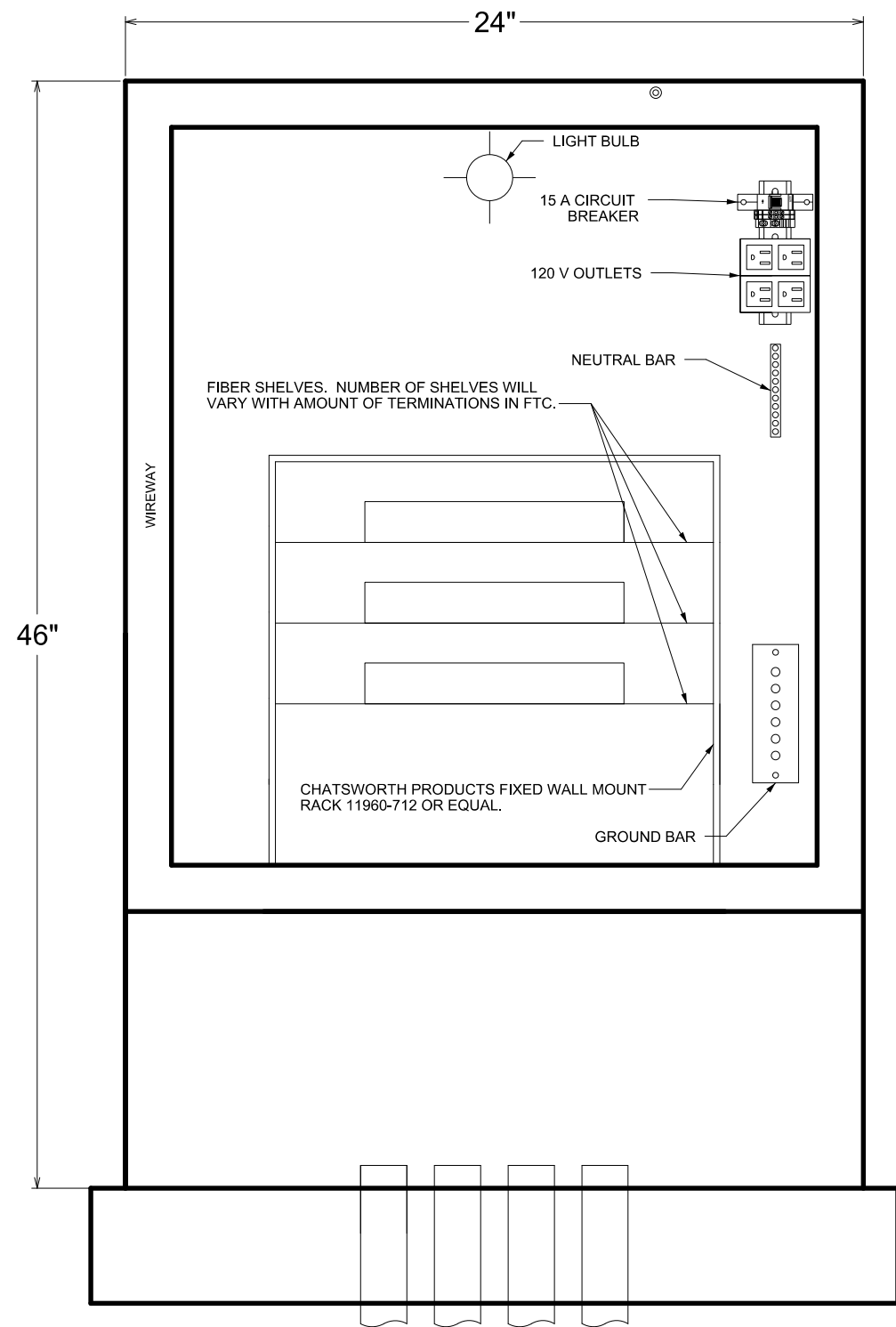
1. MATERIALS AND METHODS OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH CURRENT STANDARDS AND SUPPLEMENTAL SPECIFICATIONS.



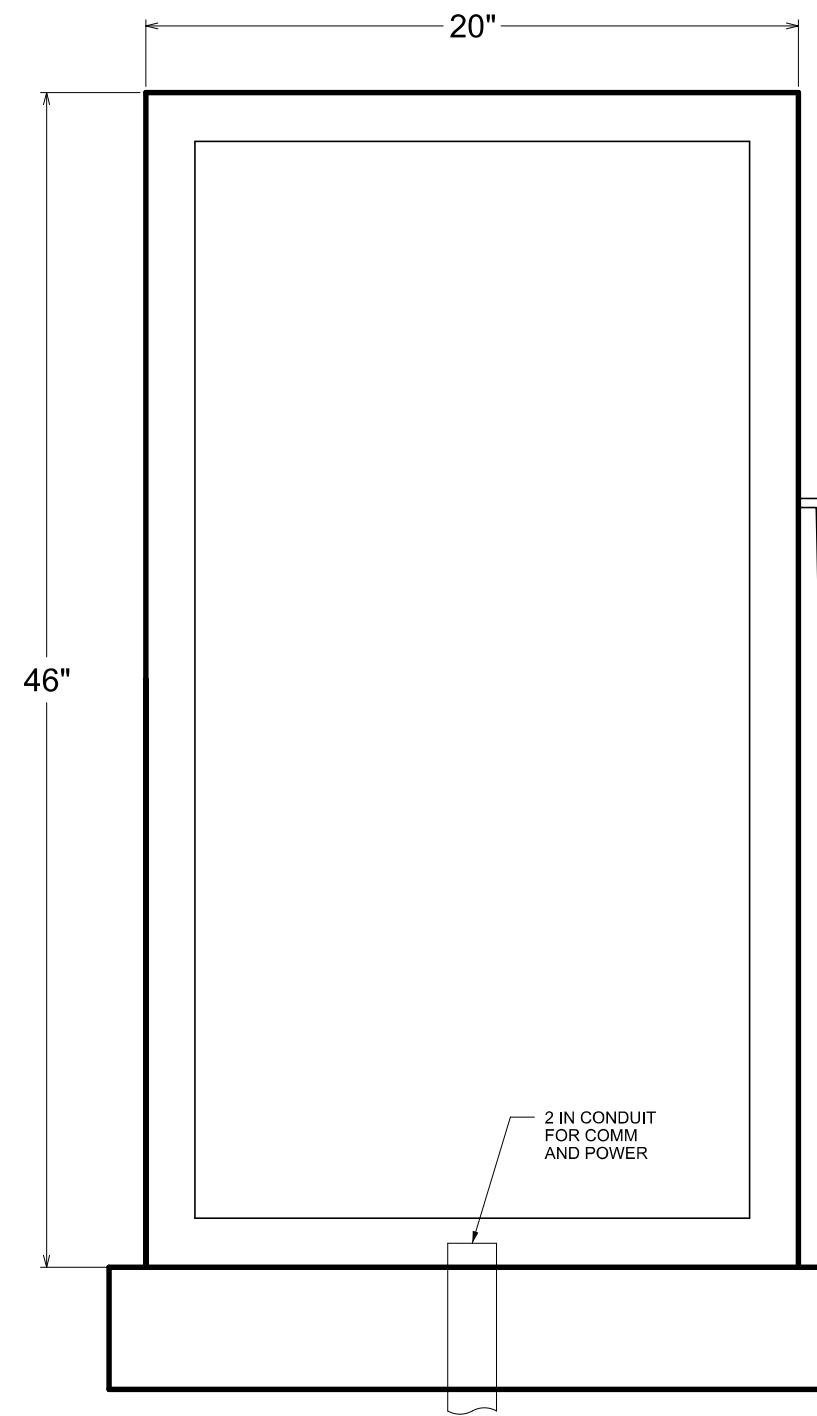
TYPICAL POLE MOUNTED CABINET  
ITS DETAIL

GENERAL NOTES

1. MATERIALS AND METHODS OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH CURRENT STANDARDS AND SUPPLEMENTAL SPECIFICATIONS.



FRONT VIEW OF PAD MOUNTED CABINET  
NO SCALE

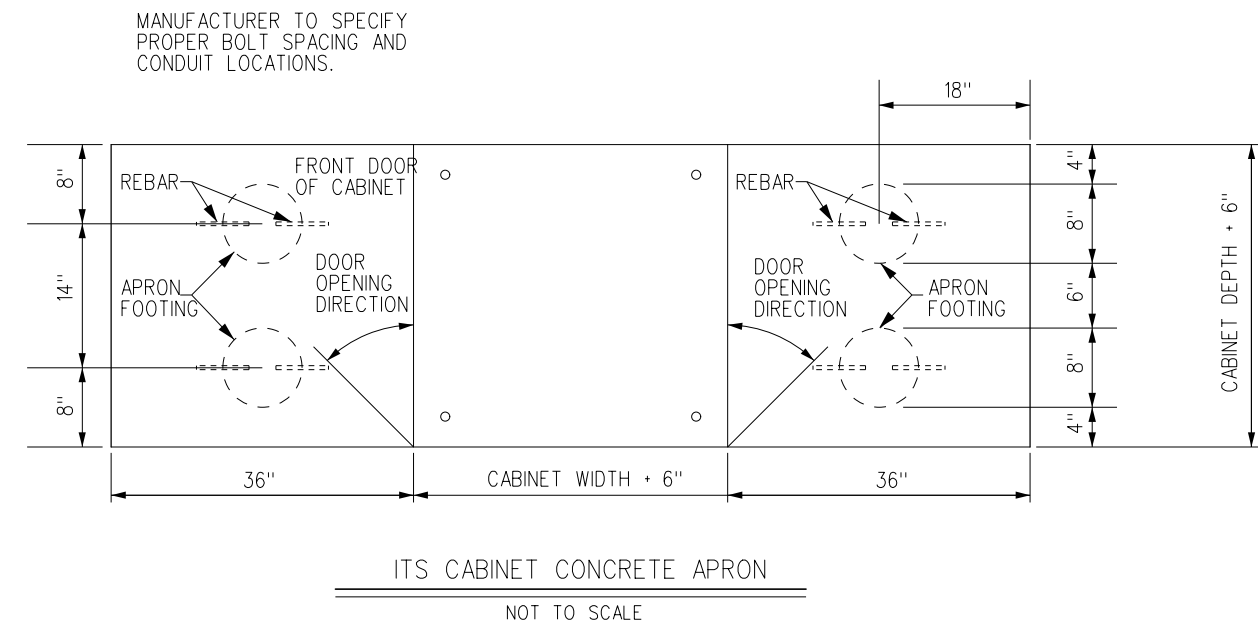
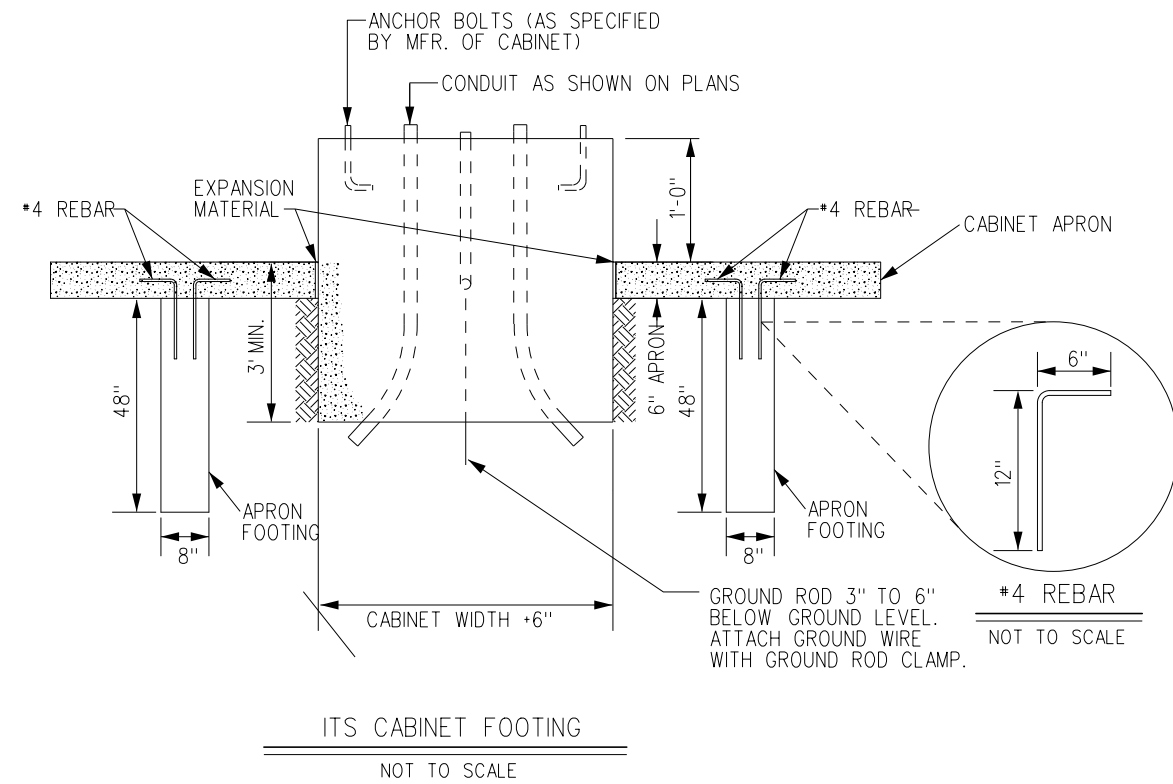


SIDE VIEW OF PAD MOUNTED CABINET  
NO SCALE

TYPICAL PAD MOUNTED CABINET  
ITS DETAIL

GENERAL NOTES

1. MATERIALS AND METHODS OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH CURRENT STANDARDS AND SUPPLEMENTAL SPECIFICATIONS.
2. REFER TO APPROPRIATE STANDARD ROAD PLANS AND PROJECT PLANS FOR ADDITIONAL DETAILS.
3. CABINET HARDWARE SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS.
4. COVERS OF JUNCTION BOXES INSTALLED IN LOCATIONS SUBJECT TO PEDESTRIAN TRAFFIC SHALL HAVE AN APPROVED ANTI-SKID PATTERN.
5. CONDUIT SIZES FOR ALL DETAILS SHALL MATCH THOSE INDICATED ON THE PLANS.
6. LOCATION OF INTERNAL CABINET HARDWARE MAY VARY FROM LOCATION SHOWN IN TYPICAL DETAIL.



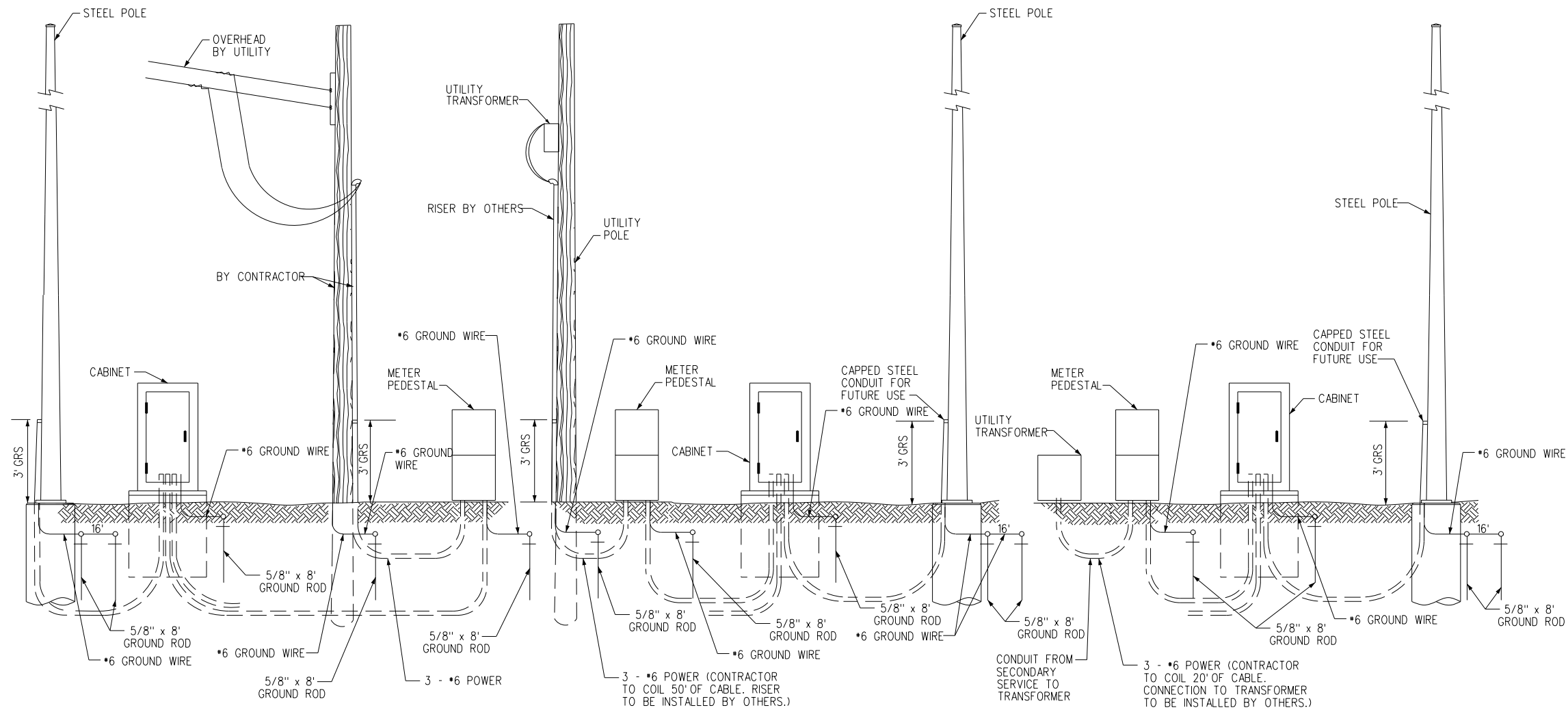
TYPICAL CABINET FOUNDATION  
ITS DETAIL



GENERAL NOTES

1. MATERIALS AND METHODS OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH CURRENT STANDARDS AND SUPPLEMENTAL SPECIFICATIONS.
2. REFER TO APPROPRIATE STANDARD ROAD PLANS AND PROJECT PLANS FOR ADDITIONAL DETAILS.
3. ALL CONDUITS SHALL ENTER FOUNDATIONS FROM BOTTOM. SIDE PENETRATIONS WILL NOT BE PERMITTED UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.
4. FOUNDATION DIMENSIONS SHOWN ARE MINIMUM DIMENSIONS AND SHALL BE INCREASED WHEN NECESSARY AS DETERMINED BY POLE OR CABINET MANUFACTURER.
5. CONDUIT SIZES FOR ALL DETAILS SHALL MATCH THOSE INDICATED ON THE PLANS.
6. MINIMUM TWO GROUND RODS PER STEEL POLE, MAXIMUM GROUND RESISTANCE SHALL BE 20 OHMS OR LESS ALL GROUND CONNECTIONS SHALL BE CADWELDED.

SECONDARY SERVICE OPTIONS  
NOT TO SCALE



TYPICAL POWER SERVICE  
ITS DETAIL

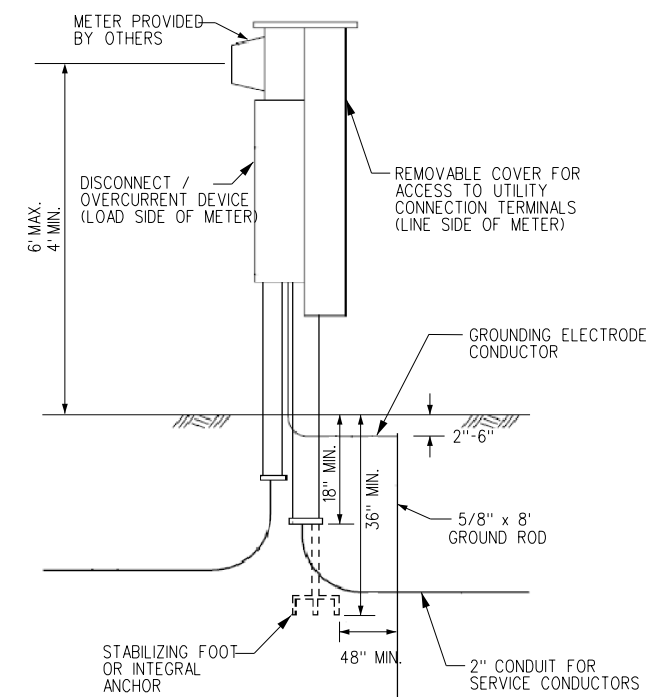
GENERAL NOTES

1. MATERIALS AND METHODS OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH CURRENT STANDARDS AND SUPPLEMENTAL SPECIFICATIONS.
2. REFER TO APPROPRIATE STANDARD ROAD PLANS AND PROJECT PLANS FOR ADDITIONAL DETAILS.
3. MOUNTING HEIGHTS SHALL BE AS INDICATED OR AT MINIMUM REQUIREMENT ALLOWED UNDER CURRENT APPLICABLE ELECTRICAL CODES, WHICHEVER IS GREATER. NOTIFY ENGINEER IF MOUNTING HEIGHT IS GREATER THAN SHOWN.
4. CONDUIT SIZES FOR ALL DETAILS SHALL MATCH THOSE INDICATED ON THE PLANS.



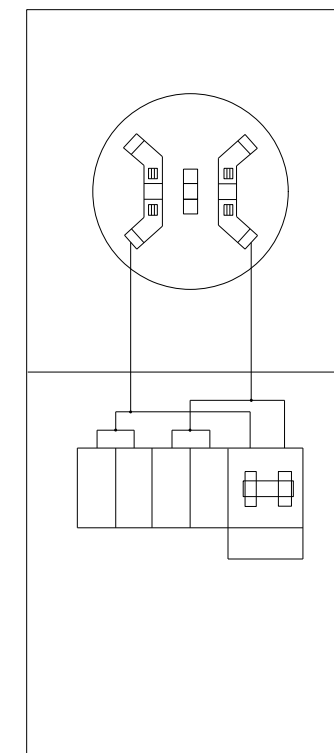
TYPICAL BREAKER INSTALLATION IN SIGNAL/CONTROLLER CABINET

NOT TO SCALE



METER PEDESTAL INSTALLATION

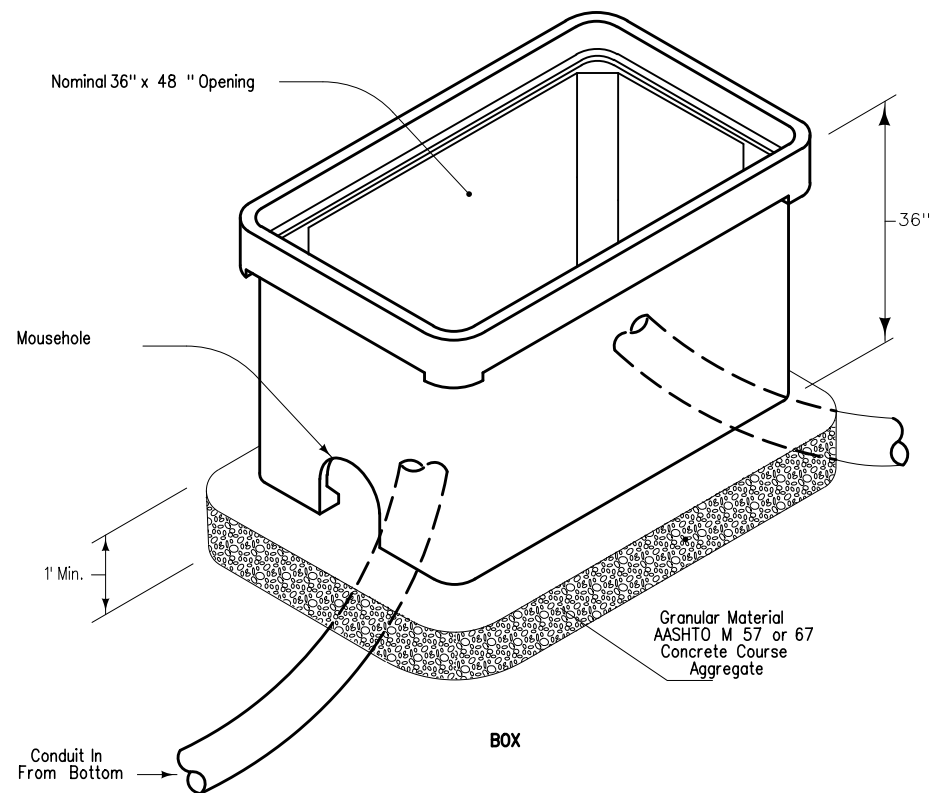
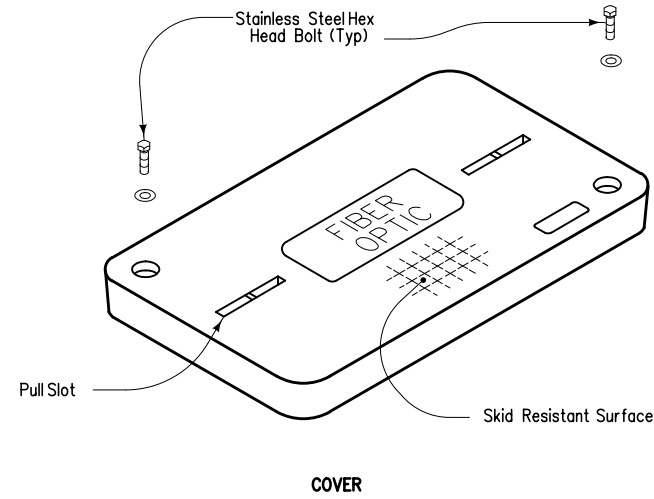
NOT TO SCALE



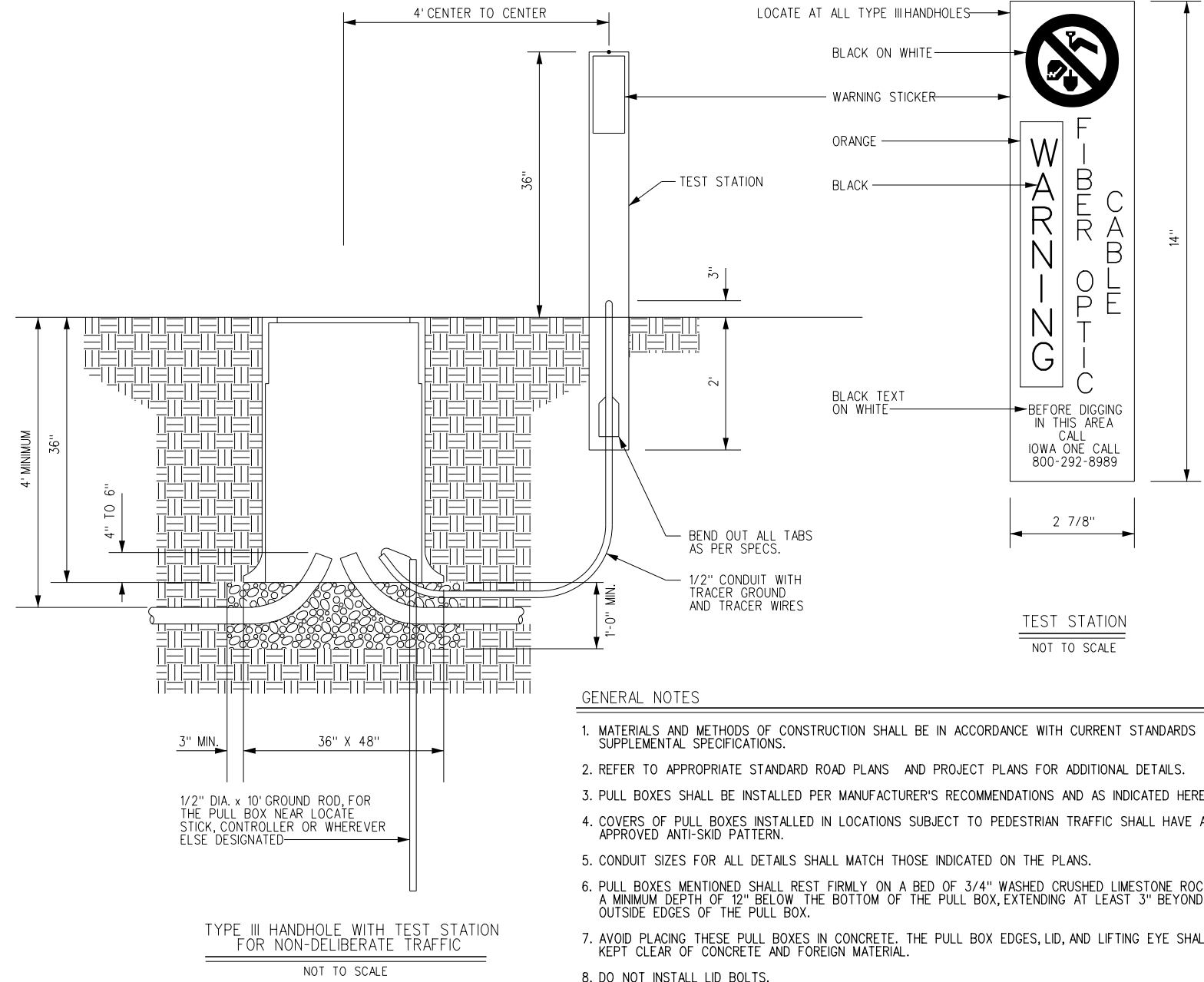
OPTION B

- Features:
- 6 circuit interior to accept standard plug-in type circuit breakers
  - 100 amp, 120/240 volt circuit breaker

TYPICAL POWER SERVICE  
ITS DETAIL



TYPE III HANDHOLE  
( FIBER REINFORCED CONCRETE )  
NOT TO SCALE

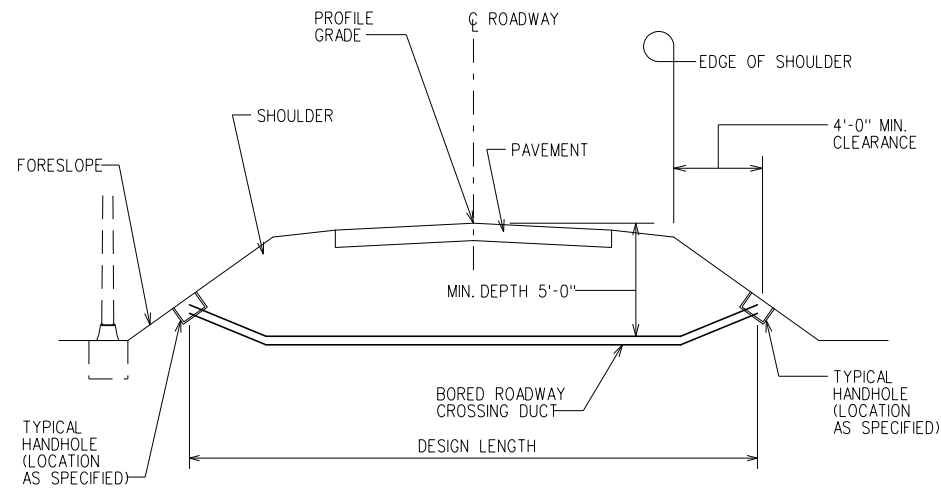


TYPE III HANDHOLE WITH TEST STATION  
FOR NON-DELIBERATE TRAFFIC  
NOT TO SCALE

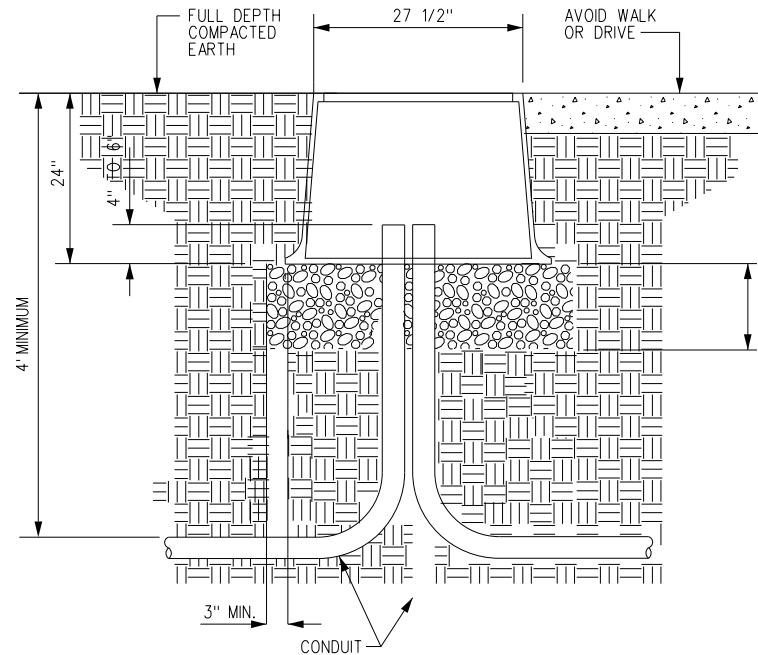
GENERAL NOTES

1. MATERIALS AND METHODS OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH CURRENT STANDARDS AND SUPPLEMENTAL SPECIFICATIONS.
2. REFER TO APPROPRIATE STANDARD ROAD PLANS AND PROJECT PLANS FOR ADDITIONAL DETAILS.
3. PULL BOXES SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS AND AS INDICATED HEREIN.
4. COVERS OF PULL BOXES INSTALLED IN LOCATIONS SUBJECT TO PEDESTRIAN TRAFFIC SHALL HAVE AN APPROVED ANTI-SKID PATTERN.
5. CONDUIT SIZES FOR ALL DETAILS SHALL MATCH THOSE INDICATED ON THE PLANS.
6. PULL BOXES MENTIONED SHALL REST FIRMLY ON A BED OF 3/4" WASHED CRUSHED LIMESTONE ROCK WITH A MINIMUM DEPTH OF 12" BELOW THE BOTTOM OF THE PULL BOX, EXTENDING AT LEAST 3" BEYOND THE OUTSIDE EDGES OF THE PULL BOX.
7. AVOID PLACING THESE PULL BOXES IN CONCRETE. THE PULL BOX EDGES, LID, AND LIFTING EYE SHALL BE KEPT CLEAR OF CONCRETE AND FOREIGN MATERIAL.
8. DO NOT INSTALL LID BOLTS.
9. THE TYPE, SIZE, AND LOCATION OF ELECTRICAL ROADWAY DUCTS WILL BE SHOWN ON THE PROJECT PLANS. ROADWAY CROSSINGS SHALL BE INSTALLED AS SHOWN HEREON UNLESS OTHERWISE SPECIFIED OR DIRECTED BY THE ENGINEER.
10. CROSSINGS ARE TO BE PLACED WITHOUT DISTURBING THE EXISTING ROADWAY SURFACE BY JACKING OR BORING METHODS APPROVED BY THE ENGINEER. NO ACCESS TO DUCT OR JACKING OF DUCT FROM MEDIAN WILL BE ALLOWED WITHOUT THE SPECIFIC APPROVAL OF THE ENGINEER.
11. AFTER CABLE IS INSTALLED, ALL DUCT TERMINALS ENDS IN HANDHOLES, TRANSFORMER BASES, LIGHT POLE SHAFTS, OR SIMILAR LOCATIONS AS DIRECTED BY THE ENGINEER SHALL BE SEALED AGAINST ENTRY OF MOISTURE. SEALANTS SHALL BE EITHER APPROVED SEALING BUSHINGS OR A NON-HARDENING SEALING COMPOUND.
12. ALL LIDS SHALL BE LABELED. LID TEXT SHALL BE STANDARDIZED FOR DESIGNED USE ACCEPTABLE TO THE ENGINEER.

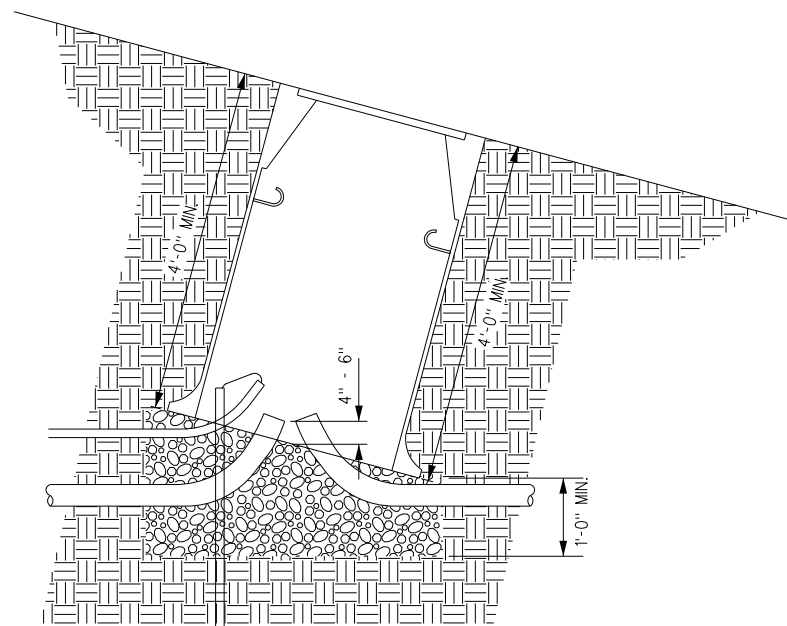
TYPICAL TYPE III HANDHOLE  
ITS DETAIL



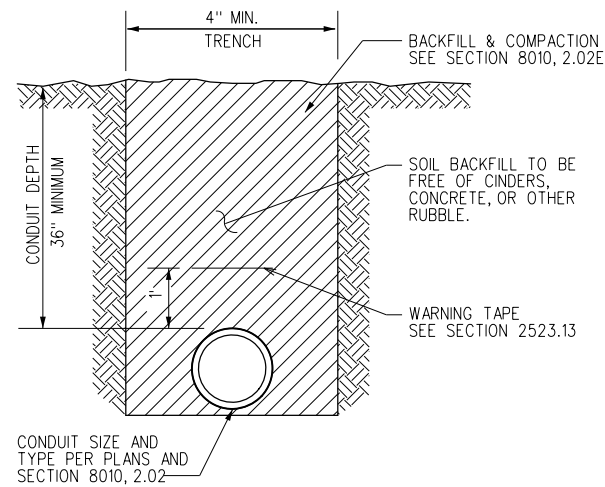
TYPICAL ROADWAY TRANSVERSE SECTION WITH HANDHOLE ON SLOPE  
NOT TO SCALE



TYPE I HANDHOLE  
FOR NON-DELIBERATE TRAFFIC  
NOT TO SCALE



HANDHOLE ON SLOPE  
NOT TO SCALE



CONDUIT IN TRENCH  
NOT TO SCALE

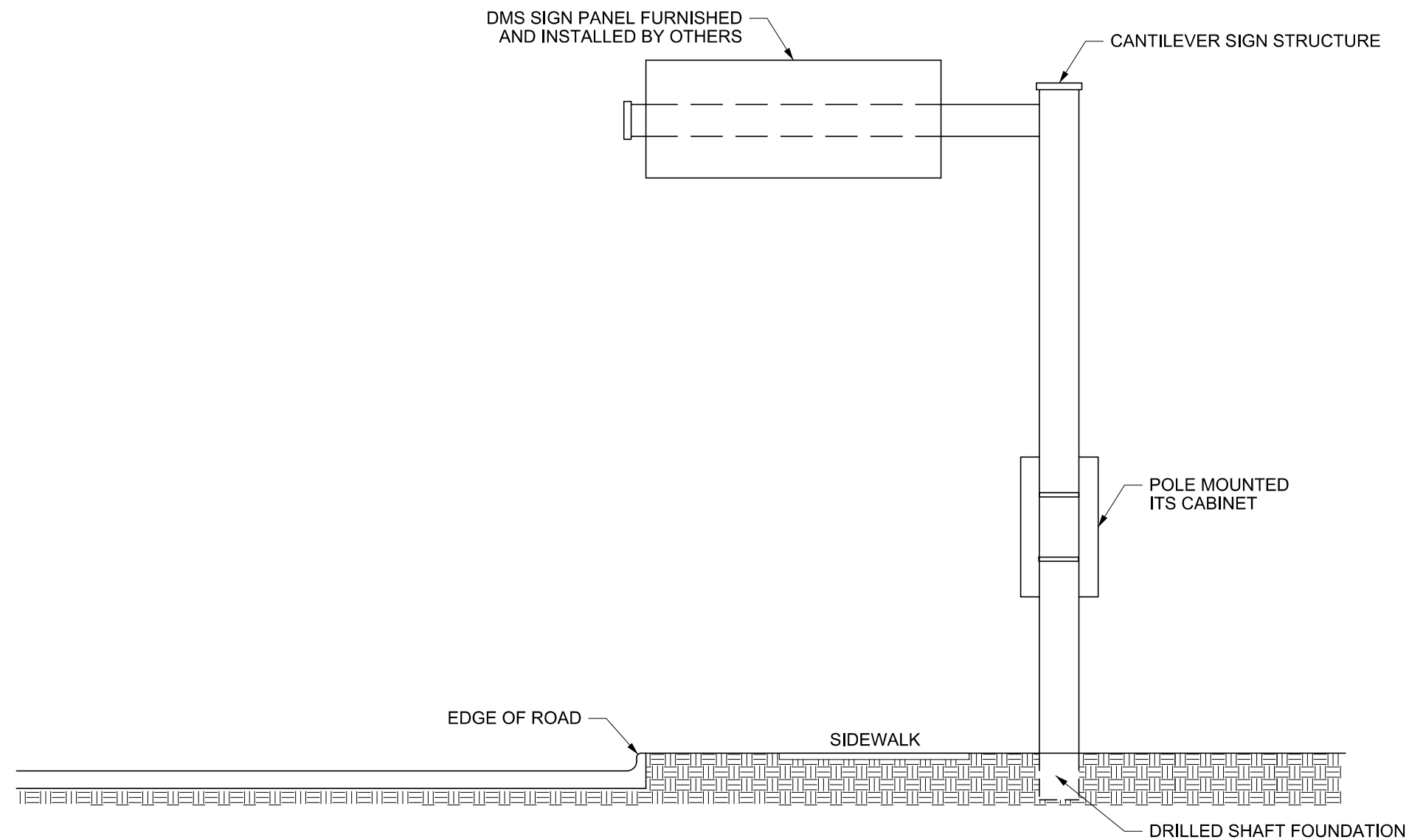
GENERAL NOTES

1. MATERIALS AND METHODS OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH CURRENT STANDARDS AND SUPPLEMENTAL SPECIFICATIONS.
2. REFER TO APPROPRIATE STANDARD ROAD PLANS AND PROJECT PLANS FOR ADDITIONAL DETAILS.
3. PULL BOXES SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS AND AS INDICATED HEREIN.
4. COVERS OF PULL BOXES INSTALLED IN LOCATIONS SUBJECT TO PEDESTRIAN TRAFFIC SHALL HAVE AN APPROVED ANTI-SKID PATTERN.
5. CONDUIT SIZES FOR ALL DETAILS SHALL MATCH THOSE INDICATED ON THE PLANS.
6. PULL BOXES MENTIONED SHALL REST FIRMLY ON A BED OF 3/4" WASHED CRUSHED LIMESTONE ROCK WITH A MINIMUM DEPTH OF 12" BELOW THE BOTTOM OF THE PULL BOX, EXTENDING AT LEAST 3" BEYOND THE OUTSIDE EDGES OF THE PULL BOX.
7. AVOID PLACING THESE PULL BOXES IN CONCRETE. THE PULL BOX EDGES, LID, AND LIFTING EYE SHALL BE KEPT CLEAR OF CONCRETE AND FOREIGN MATERIAL.
8. DO NOT INSTALL LID BOLTS.
9. AFTER CABLE IS INSTALLED, ALL DUCT TERMINALS ENDS IN HANDHOLES, TRANSFORMER BASES, LIGHT POLE SHAFTS, OR SIMILAR LOCATIONS AS DIRECTED BY THE ENGINEER SHALL BE SEALED AGAINST ENTRY OF MOISTURE. SEALANTS SHALL BE EITHER APPROVED SEALING BUSHINGS OR A NON-HARDENING SEALING COMPOUND.
10. ALL LIDS FOR TYPE II HANDHOLES SHALL BE LABELED "FIBER OPTICS".
11. THE DETAILS INDICATED HEREIN ARE FOR THE INSTALLATION OF A PRECAST HANDHOLE WITH COVER FOR ELECTRICAL WIRING AND CONDUIT.
12. REFER TO APPROPRIATE STANDARD ROAD PLANS AND PROJECT PLANS FOR LOCATIONS AND ADDITIONAL DETAILS.
13. BODY OF RM-42 HANDHOLE SHALL COMPLY WITH THE REQUIREMENTS OF STANDARD ROAD PLAN RF-1 FOR CLASS 1500 D CONCRETE PIPE INsofar AS APPLICABLE. THE PIPE SECTION SHALL BE PLACED WITH TONGUE END AT TOP OF HANDHOLE.
14. SLOTS FOR CONDUIT ACCESS MAY BE CAST AT 90 DEGREE SPACING, AS SHOWN, OR CAST ONLY AS NECESSARY FOR CONDUIT INSTALLATION AS PROJECT PLANS REQUIRES. SLOTS SHALL BE GROUTED WITH MORTAR AFTER NECESSARY CONDUITS ARE INSTALLED. CABLE HOOKS MAY BE PRECAST WITH HANDHOLE OR MAY BE INSTALLED AS NECESSARY BY A METHOD APPROVED BY THE ENGINEER.
15. CABLE HOOKS AND HANDLING LOOPS SHALL BE GALVANIZED IN ACCORDANCE WITH CURRENT SPECIFICATIONS.
16. ACCESS CONDUIT SHALL BE THE SAME AS REQUIRED FOR THE CIRCUIT DUCTS UNLESS OTHERWISE SPECIFIED.
17. DIRECT BURY CABLE INSTALLATIONS ARE NOT ALLOWED UNLESS DIRECTED BY THE ENGINEER.
18. DIMENSIONS SHOWN ARE APPROXIMATE. APPROVED PRODUCT MANUFACTURER'S DIMENSION SHALL PREVAIL IF DIFFERENT FROM THOSE SHOWN.
19. THE CONTRACT ITEM IS "HANDHOLE AND JUNCTION BOXES"
  - (A) 3/8" DIAMETER GALVANIZED STEEL ROD OR APPROVED EQUIVALENT
  - (B) GRANULAR MATERIAL MEETING REQUIREMENTS FOR GRADATION NUMBER 3 OR 5. (SHOWN IN THE AGGREGATE GRADATION TABLE FROM THE CURRENT GENERAL SUPPLEMENTAL SPECIFICATIONS).

TYPICAL TYPE I HANDHOLE AND CONDUIT  
ITS DETAIL

GENERAL NOTES

1. DETAIL IS IN PROGRESS. POLE WILL NOT BE CONTRACTOR DESIGN SUBMITTED FOR REVIEW.
2. MATERIALS AND METHODS OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH CURRENT STANDARDS AND SUPPLEMENTAL SPECIFICATIONS.
3. REFER TO APPROPRIATE STANDARD ROAD PLANS AND PROJECT PLANS FOR ADDITIONAL DETAILS.
4. THE POLE MANUFACTURER SHALL FURNISH A NOTARIZED CERTIFICATE STATING THAT THE POLES AND ASSOCIATED MATERIALS COMPLY WITH THE STRUCTURAL, WIND LOADING, AND FINISH REQUIREMENTS OF THE PLANS AND SPECIAL PROVISIONS.
5. THE POLE SHALL BE DESIGNED TO SATISFY REQUIREMENTS SET FORTH IN THE CURRENT EDITION AASHTO "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS."
6. ANY POLE RELOCATION SHALL BE APPROVED BY THE ENGINEER.
7. SHOP DRAWINGS SHALL BE SUBMITTED FOR REVIEW.
8. MINIMUM TWO GROUND RODS PER POLE, MAXIMUM RESISTANCE SHALL BE 20 OHMS OR LESS.
9. CABINET HARDWARE SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS.
10. CONDUIT SIZES FOR ALL DETAILS SHALL MATCH THOSE INDICATED ON THE PLANS.



TYPICAL ARTERIAL DMS  
NOT TO SCALE

TYPICAL ARTERIAL DMS  
ITS DETAIL

**HIGHWAY LIGHTING DATA**

108-11A  
10-29-02

ELECTRICAL SERVICE:  
MIDAMERICAN ENERGY IS PROVIDING THE PROPOSED SECONDARY SERVICE (120/240 VOLTS SINGLE PHASE) TO THE PROPOSED CONTROL STATIONS. THE EXACT LOCATION OF THE CONTROL STATION SHALL BE DETERMINED BY THE ENGINEER AND APPROVED BY THE UTILITY COMPANY. THE UTILITY WILL FURNISH THE METER. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MAKE THE NECESSARY CONTACTS WITH THE UTILITY COMPANY WITH REGARD TO CONNECTIONS TO THE SERVICE DROP. THE MIDAMERICAN ENERGY CONTACT IS: LINDA WILSON. (563) 333-8776.

- NO. 4 AWG - CIRCUIT K1 FROM LIGHTING CONTROLLER TO K0106
- NO. 4 AWG - CIRCUIT K2 FROM LIGHTING CONTROLLER TO K0205
- NO. 4 AWG - CIRCUIT K3 FROM LIGHTING CONTROLLER TO K0302
- NO. 4 AWG - CIRCUIT K4 FROM LIGHTING CONTROLLER TO K0402
- NO. 2 AWG - CIRCUIT K5 FROM LIGHTING CONTROLLER TO K0502
- NO. 2 AWG - CIRCUIT K5 FROM HANDHOLE TO K0513
- NO. 2 AWG - CIRCUIT K6 FROM LIGHTING CONTROLLER TO K0615
- NO. 2 AWG - CIRCUIT K6 FROM K0503/K0601 TO K0602
- NO. 4 AWG - CIRCUIT K7 FROM LIGHTING CONTROLLER TO K0704
- NO. 4 AWG - CIRCUIT K7 FROM HANDHOLE TO K0706
- NO. 4 AWG - CIRCUIT K7 FROM HANDHOLE TO K0710
- NO. 4 AWG - CIRCUIT K8 FROM LIGHTING CONTROLLER TO K0807
- NO. 6 AWG - CIRCUIT L1 FROM LIGHTING CONTROLLER TO L0101
- NO. 6 AWG - CIRCUIT L1 FROM HANDHOLE TO L0108
- NO. 6 AWG - CIRCUIT L2 FROM LIGHTING CONTROLLER TO L0201
- NO. 6 AWG - CIRCUIT L2 FROM LIGHTING CONTROLLER TO L0208
- NO. 10 AWG - CIRCUIT L3 FROM LIGHTING CONTROLLER TO L0301
- NO. 10 AWG - CIRCUIT L3 FROM LIGHTING CONTROLLER TO L0306
- NO. 10 AWG - CIRCUIT L3 FROM LIGHTING CONTROLLER TO L0311
- NO. 10 AWG - CIRCUIT L4 FROM LIGHTING CONTROLLER TO L0401
- NO. 10 AWG - CIRCUIT L4 FROM L0406 TO L0410
- NO. 6 AWG - CIRCUIT A2 FROM EX. LIGHTING CONTROLLER TO EX. HANDHOLE
- NO. 6 AWG - CIRCUIT A3 FROM EX. LIGHTING CONTROLLER TO EX. HANDHOLE
- NO. 6 AWG - CIRCUIT A4 FROM EX. LIGHTING CONTROLLER TO EX. HANDHOLE
- NO. 6 AWG - EXISTING CIRCUIT B1 FROM EX. LIGHT 1 TO EX. LIGHT 3

**PROJECT NOTE**

- THE PAY ITEMS HAVE BEEN DIVIDED IN THE REMARKS AS FOLLOWS:
- (1) DIVISION 1 PAY ITEM (IOWA DOT COST)
  - (2) DIVISION 2 PAY ITEM (CITY OF BETTENDORF COST)
  - (3) DIVISION 3 PAY ITEM (72.23% IOWA/27.77% BETTENDORF COST)
  - (4) DIVISION 4 PAY ITEM (NON-PARTICIPATING)

**TABULATION OF ELECTRICAL DUCTS**

108-2  
08-20-85

Location	Conduit Type	No. Of Cond.	Dia. Inches	Length Feet	Remarks
K0106 TO K0102	SCH 40 PLASTIC	1	2	881	CIRCUIT K1 (2)
K0102 TO HANDHOLE	SCH 80 PVC	1	2	246	CIRCUIT K1 (2)
K0205 TO K0202	SCH 40 PLASTIC	1	2	882	CIRCUIT K2 (2)
K0202 TO K0201	SCH 80 PVC	1	2	191	CIRCUIT K2 (2)
K0201 TO HANDHOLE	SCH 40 PLASTIC	1	2	180	CIRCUIT K2 (2)
K0402 TO K0401	SCH 80 PVC	1	2	220	CIRCUIT K4 (2)
K0401 TO HANDHOLE	SCH 40 PLASTIC	1	2	87	CIRCUIT K4 (2)
HANDHOLE TO HANDHOLE	SCH 80 PVC	2	3	51	CIRCUITS K2 & K4 (2)
K06012 TO K0609	SCH 40 PLASTIC	1	2	269	CIRCUIT K6 (2)
K0609 TO K0608	SCH 80 PVC	1	2	88	CIRCUIT K6 (2)
K06013 TO K0603	SCH 40 PLASTIC	1	2	549	CIRCUIT K6 (2)
K0603 TO K0503/K0601	SCH 80 PVC	1	2	85	CIRCUITS K6 & K9 (2)
K0512 TO K0504	SCH 40 PLASTIC	1	2	850	CIRCUIT K5 (2)
K0602 TO HANDHOLE	SCH 40 PLASTIC	1	2	45	CIRCUITS K5 & K6 (2)
HANDHOLE TO K0503/K0601	SCH 40 PLASTIC	1	2	82	CIRCUITS K5, K6 & K9 (2)
K0503/K0601 TO HANDHOLE	SCH 80 PVC	1	2	49	CIRCUITS K5, K6 & K9 (2)
K0502 TO HANDHOLE	SCH 40 PLASTIC	1	2	189	CIRCUIT K5 (2)
HANDHOLE TO HANDHOLE	SCH 40 PLASTIC	1	2	63	CIRCUITS K5, K6 & K9 (2)
K0710 TO HANDHOLE	SCH 40 PLASTIC	1	2	350	CIRCUIT K7 (2)
K0706 TO HANDHOLE	SCH 40 PLASTIC	1	2	68	CIRCUIT K7 (2)
HANDHOLE TO K0801/K0802	SCH 80 PVC	1	2	49	CIRCUIT K7 (2)
K0808 TO K0806	SCH 40 PLASTIC	1	2	16	CIRCUIT K8 (2)
K0807 TO K0801/K0802	SCH 40 PLASTIC	1	2	358	CIRCUIT K8 (2)
K0801/K0802 TO HANDHOLE	SCH 80 PVC	1	2	61	CIRCUITS K7 & K8 (2)
K0705 TO HANDHOLE	SCH 40 PLASTIC	1	2	290	CIRCUIT K7 (2)
HANDHOLE TO K0701/K0702	SCH 40 PLASTIC	1	2	33	CIRCUITS K7 & K8 (2)
K0701/K0702 TO HANDHOLE	SCH 80 PVC	1	2	80	CIRCUITS K7 & K8 (2)
HANDHOLE TO K0302	SCH 40 PLASTIC	1	3	209	CIRCUITS K5, K6, K7, K8 & K9 (2)
K0302 TO HANDHOLE	SCH 40 PLASTIC	1	3	392	CIRCUITS K3, K5, K6, K7, K8 & K9 (2)
HANDHOLE TO CABINET K	SCH 40 PLASTIC	2	3	10	CIRCUITS K1, K2, K3, K4, K5, K6, K7, K8 & K9 (2)
L0101 TO L0103	SCH 40 PLASTIC	1	2	192	CIRCUIT L1 (2)
L0103 TO L0104	SCH 80 PVC	1	2	75	CIRCUIT L1 (2)
L0104 TO HANDHOLE	SCH 40 PLASTIC	1	2	372	CIRCUIT L1 (2)
L0108 TO L0207	SCH 80 PVC	1	2	131	CIRCUIT L1 (2)
L0201 TO L0203	SCH 40 PLASTIC	1	2	158	CIRCUIT L2 (2)
L0301 TO L0203	SCH 40 PLASTIC	1	2	159	CIRCUIT L3 (2)
L0203 TO L0206	SCH 40 PLASTIC	1	2	343	CIRCUITS L2 & L3 (2)
L0305 TO L0206	SCH 40 PLASTIC	1	2	18	CIRCUIT L3 (2)
L0206 TO L0207	SCH 40 PLASTIC	1	2	128	CIRCUITS L2 & L3 (2)
L0207 TO CABINET L	SCH 40 PLASTIC	1	2	52	CIRCUITS L1, L2 & L3 (2)
L0208 TO L0209	SCH 40 PLASTIC	1	2	98	CIRCUIT L2 (2)
L0311 TO L0209	SCH 40 PLASTIC	1	2	481	CIRCUIT L3 (2)
L0209 TO CABINET L	SCH 80 PVC	1	2	61	CIRCUITS L2 & L3 (2)
L0401 TO L0402	SCH 40 PLASTIC	1	2	143	CIRCUIT L4 (2)
L0402 TO L0403	SCH 80 PVC	1	2	77	CIRCUIT L4 (2)
L0403 TO L0406	SCH 40 PLASTIC	1	2	326	CIRCUIT L4 (2)
L0410 TO L0409	SCH 80 PVC	1	2	129	CIRCUIT L4 (2)
L0409 TO L0407	SCH 40 PLASTIC	1	2	194	CIRCUIT L4 (2)
L0407 TO HANDHOLE	SCH 80 PVC	1	2	90	CIRCUIT L4 (2)
HANDHOLE TO L0406	SCH 40 PLASTIC	1	2	57	CIRCUIT L4 (2)
L0406 TO L0306	SCH 80 PVC	1	2	72	CIRCUIT L4 (2)
L0306 TO CABINET L	SCH 40 PLASTIC	1	2	14	CIRCUITS L3 & L4 (2)
K0705 TO CAP	SCH 40 PLASTIC	1	2	15	EMPTY (2)
L0901 TO CAP	SCH 40 PLASTIC	1	2	15	EMPTY (2)
K0612 TO K0614	SCH 40 PLASTIC	1	2	66	CIRCUIT K6 (2)
K0512 TO K0513	SCH 40 PLASTIC	1	2	115	CIRCUIT K5 (2)
B0101 TO B0103 (RELOCATED STATE RAMP LIGHTS)	SCH 40 PLASTIC	1	2	245	EXISTING CIRCUIT B1 (1)

**DELIVERY AND STOCKPILING**

110-13  
MODIFIED

ITEM DESCRIPTION	QUANTITY	UNITS	DELIVERY LOCATION	CONTACT NAME & NUMBER	REMARKS
LIGHTING POLE	3	EACH	IOWA DOT DAVENPORT MAINTENANCE SHOP 8721 NORTHWEST BLVD DAVENPORT, IA 52806	563-391-3920	ML LIGHTING REMOVAL (4)
LIGHTING POLE	10	EACH	BETTENDORF PUBLIC WORKS FACILITY 4403 DEVILS GLEN RD BETTENDORF, IA 52722	563-344-4405	LOCAL LIGHTING REMOVAL (4)

**LIGHTING QUANTITIES**

TABULATION OF LIGHTING INSTALLATIONS								108-1 MODIFIED
① RM-39								
LOCATION				RM-31			FOOTING TYPE ①	REMARKS
NO.	STATION	OFFSET	ALIGNMENT	TYPE	A	E		
K0101	4612+90.0	25.5' LT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0102	4614+82.2	25.5' LT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0103	4617+07.9	24.7' LT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0104	4619+34.0	21.1' LT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0105	4621+59.8	25.6' LT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0106	4623+83.2	38.9' LT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0201	4614+03.7	25.5' RT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0202	4615+94.2	25.5' RT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0203	4618+21.1	20.2' RT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0204	4620+46.2	18.2' RT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0205	4622+71.3	21.8' RT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0301	4610+43.1	25.5' LT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0302	4608+30.1	25.5' LT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0401	4611+50.0	25.5' RT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0402	4609+40.3	25.5' RT	KIMBERLY RD	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0501	198+44.2	53.3' LT	US-67 WB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0502	197+20.4	55.4' LT	US-67 WB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0503/K0601			US-67 WB	-	-	-	-	SEE PLAN NOTE 4 (2)
K0504	1601+41.7	17.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0505	1600+24.2	12.4' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0506	1599+01.9	5.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0507	1597+84.4	5.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0508	1596+67.3	5.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0509	1595+87.6	5.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0510	1594+78.6	5.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0511	1593+95.7	5.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0512	1593+13.8	3.7' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0602	197+82.7	8.7' RT	US-67 WB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0603	1601+97.4	58.2' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0604	1600+82.3	41.5' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0605	1599+64.9	41.5' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0606	1598+45.8	41.5' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0607	1597+37.1	41.8' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0608	1596+84.3	51.2' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0609	1596+02.9	60.0' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0610	1595+41.0	53.5' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0611	1594+36.7	58.1' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0612	1593+55.7	52.0' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0613	1596+91.7	88.0' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0701 / K0702			US-67 WB	-	-	-	-	SEE PLAN NOTE 4 (2)
K0703	201+45.5	41.5' LT	US-67 WB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0704	202+54.8	41.5' LT	US-67 WB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0705	203+62.9	41.5' LT	US-67 WB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0706			US-67 EB	-	-	-	-	SEE PLAN NOTE 4 (2)
K0707	1604+60.9	41.5' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0708	1605+76.2	51.6' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0709	1606+72.2	61.2' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0710	1607+47.5	65.5' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
K0711			US-67 EB	-	-	-	-	SEE PLAN NOTE 4 (2)
K0801 / K0802	200+84.8	18.7' RT	US-67 WB	3	-	-	RM-39	PLAN NOTE 3/DETAIL TYPE II SHEET P.17(2)

TABULATION OF LIGHTING INSTALLATIONS (CONT'D)								108-1 MODIFIED
① RM-39								
LOCATION				RM-31			FOOTING TYPE ①	REMARKS
NO.	STATION	OFFSET	ALIGNMENT	TYPE	A	E		
K0803 / K0804	201+98.1	19.9' RT	US-67 WB	3	-	-	RM-39	PLAN NOTE 3/DETAIL TYPE II SHEET P.17 (2)
K0805	203+10.1	6.5' RT	US-67 WB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0806	204+22.3	5.5' RT	US-67 WB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0807	1607+32.7	29.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
K0808	1606+22.4	27.8' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0101	209+72.5	53.5' LT	US-67 WB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
L0102	210+65.5	49.0' LT	US-67 WB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
L0103	211+55.7	41.5' LT	US-67 WB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
L0104	212+26.9	59.9' LT	US-67 WB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
L0105	212+48.6	41.5' LT	US-67 WB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
L0106	213+93.5	41.5' LT	US-67 WB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
L0107	215+24.6	41.5' LT	US-67 WB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
L0108			US-67 WB	-	-	-	-	SEE PLAN NOTE 4 (2)
L0201	209+69.2	29.5' RT	US-67 WB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0202	210+66.7	29.5' RT	US-67 WB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0203	211+30.8	29.5' RT	US-67 WB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0204 / L0303	212+11.1	18.8' RT	US-67 WB	3	-	-	RM-39	PLAN NOTE 3/DETAIL TYPE II SHEET P.17 (2)
L0205 / L0304	213+24.2	4.4' RT	US-67 WB	3	-	-	RM-39	PLAN NOTE 3/DETAIL TYPE II SHEET P.17 (2)
L0206	214+69.3	5.5' RT	US-67 WB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0207	215+95.1	15.1' RT	US-67 WB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0208	216+94.2	8.7' RT	US-67 WB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0209	1619+42.9	65.4' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
L0301	1612+79.8	5.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0302	1613+94.7	5.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0305	1617+59.7	17.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0306	1618+88.1	17.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0307			US-67 EB	-	-	-	-	SEE PLAN NOTE 4 (2)
L0308	1620+50.0	5.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
L0309	1621+80.7	5.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
L0310	1622+86.5	5.5' LT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
L0311	1623+97.0	20.7' LT	US-67 EB	-	-	-	-	SEE PLAN NOTE 4 (2)
L0401	1613+34.3	47.7' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0402	1614+77.3	47.7' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0403	1615+57.3	47.7' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0404	1616+91.3	47.7' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0405	1618+14.5	47.7' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0406	1619+17.5	47.7' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 2/DETAIL TYPE I SHEET P.17 (2)
L0407			US-67 EB	-	-	-	-	SEE PLAN NOTE 4 (2)
L0408	1621+25.9	47.9' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
L0409	1622+34.4	53.5' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
L0410	1623+51.3	53.5' RT	US-67 EB	3	-	-	RM-39	PLAN NOTE 1/DETAIL TYPE I SHEET P.17 (2)
B0101	1607+25.1	59.2' LT	RAMP US-67 TO I-74 EB	1	-	-	RM-39	SEE PLAN NOTE 12 (1)
B0102	1605+96.0	75.6' LT	RAMP US-67 TO I-74 EB	1	-	-	RM-39	SEE PLAN NOTE 12 (1)

TABULATION OF WOOD POLE INSTALLATIONS								
① RM-39								
LOCATION				RM-31			FOOTING TYPE ①	REMARKS
NO.	STATION	OFFSET	ALIGNMENT	TYPE	A	E		
W1	4624+07.9	60.7' LT	KIMBERLY RD	-	-	-	-	SEE PLAN NOTE 5 (1)
W2	4625+37.8	55.6' LT	KIMBERLY RD	-	-	-	-	SEE PLAN NOTE 5 (1)
W3	4625+33.4	62.7' RT	KIMBERLY RD	-	-	-	-	SEE PLAN NOTE 5 (1)
W4	4625+09.8	122.1' RT	KIMBERLY RD	-	-	-	-	SEE PLAN NOTE 5 (1)

LIGHTING  
QUANTITIES

**TABULATION OF WIRE, CABLE AND CONNECTORS**

108-12  
02-11-00

CIRCUIT NUMBER	RM-40 CONNECTORS								PHASE LINES								GROUND		REMARKS		
	TYPE	QUAN. NO.	TYPE	QUAN. NO.	TYPE	QUAN. NO.	TYPE	QUAN. NO.	SIZE A.W.G.	QUAN. L.F.	SIZE A.W.G.	QUAN. L.F.	SIZE A.W.G.	QUAN. L.F.	SIZE A.W.G.	QUAN. L.F.	SIZE A.W.G.	QUAN. L.F.			
K1	L-1	13	L-2	2	Y-1	11	Y-3	-	-	-	4	3000	-	-	-	-	10	630	-	-	(2)
K2	L-1	11	L-2	4	Y-1	9	Y-3	-	-	-	4	2952	-	-	-	-	10	540	-	-	(2)
K3	L-1	4	L-2	2	Y-1	2	Y-3	-	-	-	4	888	-	-	-	-	10	180	-	-	(2)
K4	L-1	4	L-2	4	Y-1	2	Y-3	-	-	-	4	822	-	-	-	-	10	180	-	-	(2)
K5	L-1	24	L-2	10	Y-1	20	Y-3	-	2	4216	-	-	-	-	-	-	10	1080	-	-	(2)
K6	L-1	26	L-2	16	Y-1	20	Y-3	4	2	4267	-	-	-	-	-	-	10	1170	-	-	(2)
K7	L-1	22	L-2	12	Y-1	16	Y-3	4	-	-	4	3720	-	-	-	-	10	990	-	-	(2)
K8	L-1	14	L-2	6	Y-1	12	Y-3	-	-	-	4	2647	-	-	-	-	10	630	-	-	(2)
K9	L-1	-	L-2	8	Y-1	-	Y-3	-	2	1616	-	-	-	-	-	-	-	-	-	-	(2)
L1	L-1	16	L-2	4	Y-1	12	Y-3	2	-	-	-	-	6	2036	-	-	10	720	-	-	(2)
L2	L-1	18	L-2	-	Y-1	14	Y-3	-	-	-	-	-	6	2130	-	-	10	810	-	-	(2)
L3	L-1	22	L-2	4	Y-1	14	Y-3	2	-	-	-	-	-	-	10	3073	10	990	-	-	(2)
L4	L-1	20	L-2	6	Y-1	16	Y-3	2	-	-	-	-	-	-	10	2724	10	900	-	-	(2)
A2	L-1	-	L-2	-	Y-1	-	Y-3	-	-	-	-	-	6	840	-	-	-	-	-	-	AERIAL CABLE WITH MESSENGER WIRE (1)
A3	L-1	-	L-2	-	Y-1	-	Y-3	-	-	-	-	-	6	840	-	-	-	-	-	-	AERIAL CABLE WITH MESSENGER WIRE (1)
A4	L-1	-	L-2	-	Y-1	-	Y-3	-	-	-	-	-	6	840	-	-	-	-	-	-	AERIAL CABLE WITH MESSENGER WIRE (1)
B1	L-1	2	L-2	-	Y-1	1	Y-3	-	-	-	-	-	6	-	-	-	-	-	-	-	(1)
COMMON GROUND																			6	11024	(2)

TABULATION OF LIGHTING REMOVALS		
LOCATION		
STATION	OFFSET	ALIGNMENT
1593+83.0	11.3' LT	PROPOSED 67 EB
1594+37.4	53.8' RT	PROPOSED 67 EB
1595+56.8	20.2' RT	PROPOSED 67 EB
1596+16.2	107.1' RT	PROPOSED 67 EB
1597+04.8	90.7' RT	PROPOSED 67 EB
1597+43.1	187.4' RT	PROPOSED 67 EB
1598+45.7	209.4' RT	PROPOSED 67 EB
1598+60.0	301.8' RT	PROPOSED 67 EB
1600+18.3	308.1' RT	PROPOSED 67 EB
1601+40.0	421.1' RT	PROPOSED 67 EB
1603+74.6	364.2' RT	PROPOSED 67 EB
1605+77.0	366.3' RT	PROPOSED 67 EB
1606+37.4	426.5' RT	PROPOSED 67 EB
1607+37.0	360.3' RT	PROPOSED 67 EB
1608+26.1	423.6' RT	PROPOSED 67 EB
1609+00.1	359.9' RT	PROPOSED 67 EB
1610+38.9	424.0' RT	PROPOSED 67 EB
1611+39.1	356.4' RT	PROPOSED 67 EB
1612+12.8	419.9' RT	PROPOSED 67 EB
1613+01.8	360.7' RT	PROPOSED 67 EB
1613+92.0	414.7' RT	PROPOSED 67 EB
1614+76.2	365.2' RT	PROPOSED 67 EB
1617+80.2	410.1' RT	PROPOSED 67 EB
1618+94.5	311.6' RT	PROPOSED 67 EB
1620+02.9	298.4' RT	PROPOSED 67 EB
1620+32.5	195.4' RT	PROPOSED 67 EB
1621+62.5	83.5' RT	PROPOSED 67 EB
1622+51.2	96.4' RT	PROPOSED 67 EB
1623+09.6	9.7' RT	PROPOSED 67 EB
1624+85.2	12.2' LT	PROPOSED 67 EB
1607+85.3	630.9' RT	PROPOSED 67 EB
1609+35.6	621.9' RT	PROPOSED 67 EB
1611+35.5	616.4' RT	PROPOSED 67 EB
4622+04.2	36.2' LT	PROPOSED KIMBERLY
4623+54.1	0.4' RT	PROPOSED KIMBERLY
4625+43.1	59.2' RT	PROPOSED KIMBERLY

**LIGHTING QUANTITIES**



ROADWAY LIGHTING GENERAL NOTES:

- A. ALL ELECTRICAL WORK SHALL CONFORM TO NATIONAL, STATE, AND LOCAL CODES.
- B. IT IS THE INTENT OF THESE PLANS AND SPECIFICATIONS TO PROVIDE A COMPLETE AND PROPERLY OPERATING ELECTRICAL LIGHTING SYSTEM. THE EQUIPMENT SHALL BE FURNISHED AS SPECIFIED AND SHALL INCLUDE ALL INCIDENTAL ITEMS NECESSARY TO PROVIDE A COMPLETE WORKING SYSTEM. INCIDENTAL ITEMS SHALL INCLUDE, BUT NOT BE LIMITED TO THE FOLLOWING ITEMS: ANCHORAGES, MOUNTING HARDWARE, CONNECTORS, LUGS, FUSES, ETC.
- C. CONTRACTOR SHALL FIELD VERIFY EXACT LOCATIONS, QUANTITIES, AND TYPE OF UTILITIES IN AREAS TO BE EXCAVATED PRIOR TO THE COMMENCEMENT OF ANY WORK AND SHALL HAND EXCAVATE AS REQUIRED IN ORDER TO NOT INTERRUPT ANY EXISTING SERVICES. SEE CIVIL DRAWINGS FOR LOCATIONS OF EXISTING AND NEW UTILITIES. IF, IN PERFORMING WORK, DAMAGE TO EXISTING UTILITIES OCCURS, THE CONTRACTOR SHALL NOTIFY UTILITY IMMEDIATELY AND PAY ANY COST INCURRED FOR REPAIR OR REPLACEMENT.
- D. ELECTRICAL EQUIPMENT, RACEWAY, ETC. ARE SHOWN IN APPROXIMATE LOCATIONS ONLY. CONTRACTOR SHALL INSTALL ELECTRICAL EQUIPMENT, RACEWAYS, ETC. WHERE DIRECTED BY THE ENGINEER IN ORDER TO BEST SUIT JOB CONDITIONS.
- E. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL EXISTING STATE OWNED LIGHTING AND/OR TRAFFIC SIGNAL UTILITIES WITHIN THE PROJECT LIMITS.
- F. COMPLETELY REMOVE ALL ABANDONED LIGHT POLE FOOTINGS AND HANDHOLES, BACKFILL AND COMPACT.
- G. ALL REPLACEMENT AND TEMPORARY WIRING SHALL BE EQUAL TO OR GREATER THAN THE EXISTING WIRE GAUGE. ALL PERMANENTLY INSTALLED CURRENT CARRYING CONDUCTORS SHALL BE TYPE RHW.
- H. NO SPLICING OF EXISTING-TO-NEW WIRING IS ALLOWED INSIDE ELECTRICAL DUCTS. ALL WIRING INTERCONNECTIONS SHALL BE INSTALLED IN ACCESSIBLE AREAS AND SHALL BE MADE WITH IOWA DOT APPROVED RM-40 CONNECTORS.

ELECTRICAL PLAN NOTES:

1. TYPE I LIGHT POLE WITH SIDEWALK LUMINAIRE:  
INSTALL PROPOSED LIGHT POLE FOUNDATION, LIGHT POLE, MAST ARM, ROADWAY LUMINAIRE, SIDEWALK LUMINAIRE, AND BANNER ARM PER DETAIL SHOWN ON SHEET P.17. LIGHT POLE FOUNDATION SHALL BE PER RM-39. A TYPE 1 HANDHOLE SHALL BE INSTALLED AT EACH LIGHT POLE FOUNDATION.
2. TYPE I LIGHT POLE WITHOUT SIDEWALK LUMINAIRE:  
INSTALL PROPOSED LIGHT POLE FOUNDATION, LIGHT POLE, MAST ARM AND ROADWAY LUMINAIRE PER DETAIL SHOWN ON SHEET P.17. LIGHT POLE FOUNDATION SHALL BE PER RM-39. A TYPE 1 HANDHOLE SHALL BE INSTALLED AT EACH LIGHT POLE FOUNDATION.
3. TYPE II LIGHT POLE:  
INSTALL PROPOSED LIGHT POLE FOUNDATION, LIGHT POLE, MAST ARMS AND ROADWAY LUMINAIRES PER DETAIL SHOWN ON SHEET P.17. LIGHT POLE FOUNDATION SHALL BE PER RM-39. A TYPE 1 HANDHOLE SHALL BE INSTALLED AT EACH LIGHT POLE FOUNDATION.
4. LUMINAIRE AT COMBINATION POLE:  
INSTALL PROPOSED LUMINAIRE AND MAST ARM ON COMBINATION TRAFFIC SIGNAL POLE. SEE TRAFFIC SIGNAL PLANS FOR EXACT POLE LOCATION. A TYPE 1 HANDHOLE SHALL BE INSTALLED AT EACH COMBINATION POLE FOUNDATION.
5. INSTALL 50' WOOD POLE, CLASS 3 OR BETTER WITHOUT LUMINAIRE. THIS WORK SHALL BE COVERED UNDER DIVISION 1.
6. INSTALL PROPOSED CONTROL CABINET (PAD MOUNTED) AND FOUNDATION PER RM-36.
7. INSTALL PRECAST HANDHOLE, TYPE 2, PER RM-42.
8. INSTALL AERIAL CABLE, 6-1/C NO. 6 WITH MESSENGER WIRE.
9. RUN AERIAL CABLE DOWN WOOD POLE IN RISER AS SHOWN IN DETAIL T1 ON SHEET P.18. REPLACE L-2 CONNECTIONS IN HANDHOLE AND CONNECT TO EXISTING CIRCUITS. THIS WORK SHALL BE COVERED UNDER DIVISION 1.
10. MAKE TEMPORARY AERIAL CONNECTIONS TO CIRCUITS A-2, A-3, AND A-4 PER DETAIL T-2 ON SHEET P.18. WORK SHALL BE INCIDENTAL TO ITEM 2523-0000200: "ELECTRICAL CIRCUITS". THIS WORK SHALL BE COVERED UNDER DIVISION 1.
11. REMOVE EXISTING LIGHT POLE, MAST ARM, AND LUMINAIRE. SALVAGE LIGHT POLE AND MAST ARM TO OWNER. REMOVE FOOTING AND DISPOSE, AS NECESSARY, OR BREAK DOWN TO A POINT THREE FEET BELOW GRADE AND DISPOSE. BACKFILL AND COMPACT.
12. EXISTING LIGHT POLE TO BE RELOCATED. REMOVE EXISTING LIGHT POLE, MAST ARM, END LUMINAIRE. STORE OFF-SITE UNTIL TIME OF REINSTALLATION. REMOVE FOOTING AND DISPOSE AS NECESSARY, OR BREAK DOWN TO POINT THREE FEET BELOW GRADE AND DISPOSE. BACKFILL AND COMPACT.
13. REMOVE EXISTING HANDHOLE AND DISPOSE. BACKFILL AND COMPACT.
14. REMOVE EXISTING CIRCUIT CONDUCTORS FROM CONDUIT. INSTALL NEW CIRCUIT CONDUCTORS TO NEAREST LIGHT POLE OR HANDHOLE.
15. LUMINAIRE, MAST ARM AND WOOD POLE TO BE REMOVED BY OTHERS.
16. WOOD POLE TO BE REMOVED BY OTHERS.
17. STUB 2" SCHEDULE 40 TO 2' BEHIND PROPOSED SIDEWALK. CAP AND SEAL CONDUIT FOR FUTURE CONNECTION.  
  
INSTALL PROPOSED CONTROL CABINET (POLE MOUNTED) AND WOOD POLE PER RM-35.

**LIGHTING GENERAL  
& PLAN NOTES**

**LEGEND:**

EXISTING LIGHT POLE TO BE REMOVED



EXISTING LIGHT POLE TO BE RELOCATED



EXISTING LUMINAIRE, MAST ARM, AND WOOD POLE TO BE REMOVED BY OTHERS



EXISTING HANDHOLE TO BE REMOVED



EXISTING LIGHTING CONTROLLER TO BE REMOVED



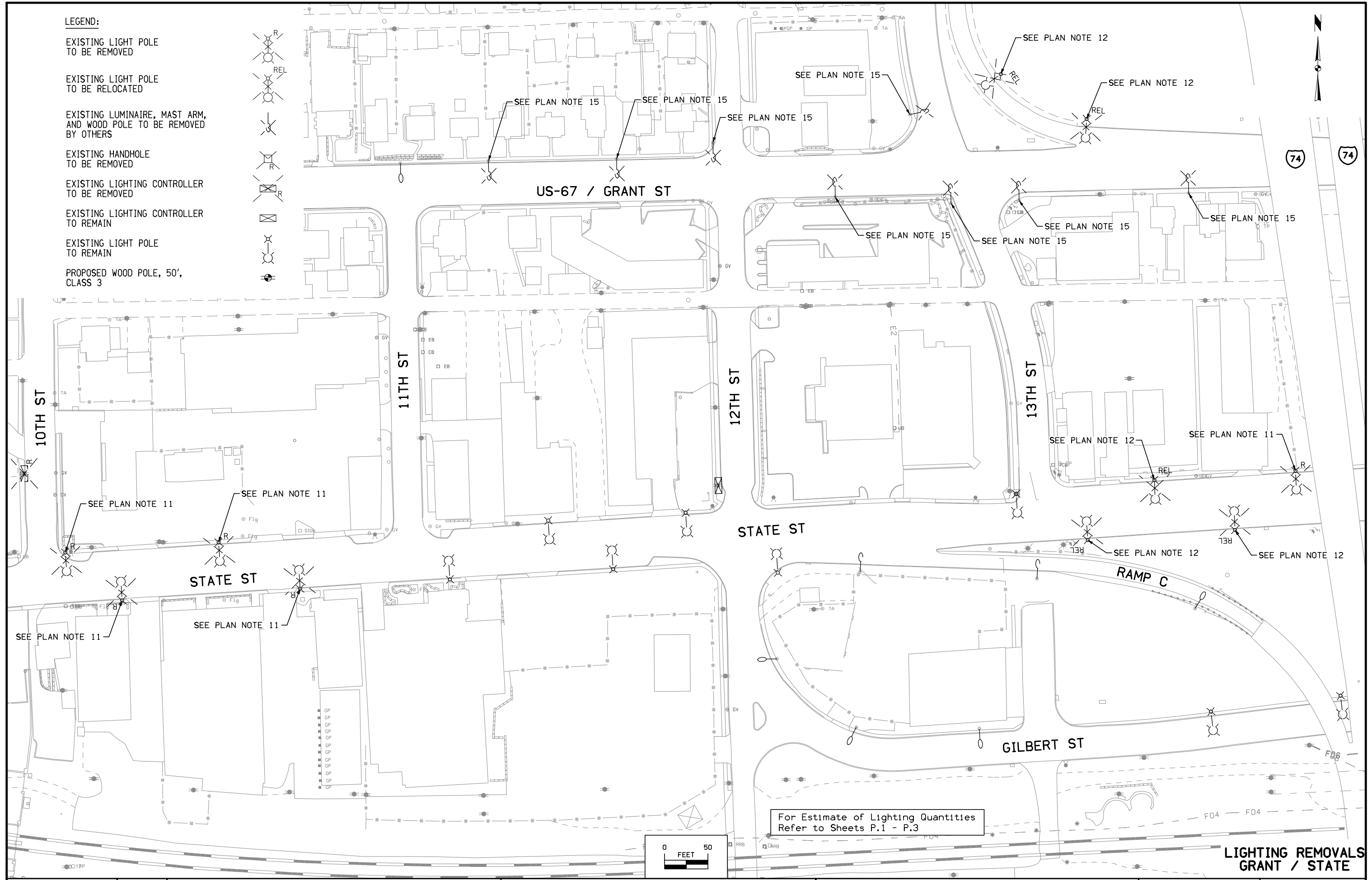
EXISTING LIGHTING CONTROLLER TO REMAIN



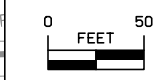
EXISTING LIGHT POLE TO REMAIN



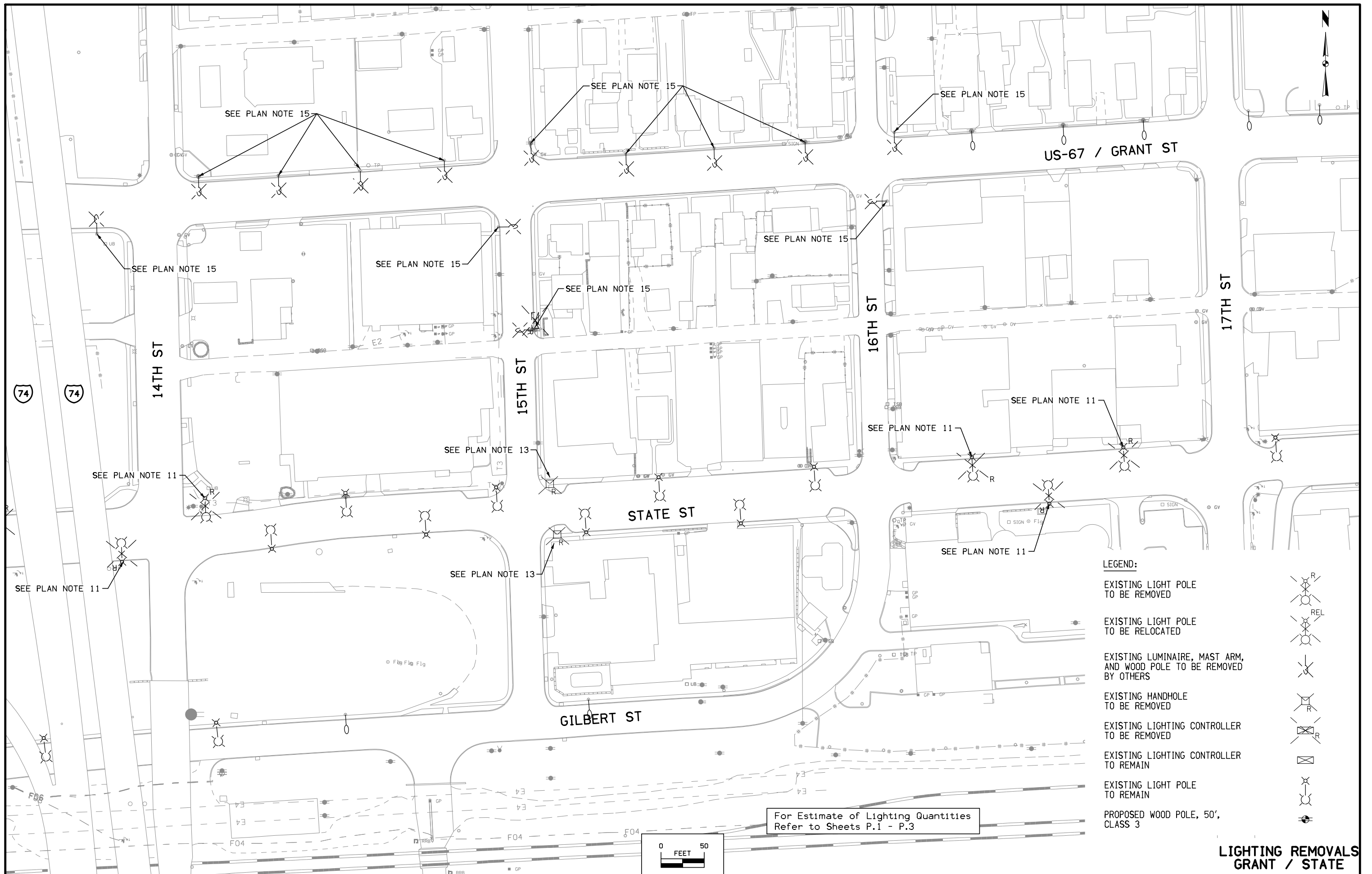
PROPOSED WOOD POLE, 50', CLASS 3



For Estimate of Lighting Quantities Refer to Sheets P.1 - P.3



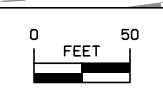
**LIGHTING REMOVALS  
GRANT / STATE**



**LEGEND:**

- EXISTING LIGHT POLE TO BE REMOVED
- EXISTING LIGHT POLE TO BE RELOCATED
- EXISTING LUMINAIRE, MAST ARM, AND WOOD POLE TO BE REMOVED BY OTHERS
- EXISTING HANDHOLE TO BE REMOVED
- EXISTING LIGHTING CONTROLLER TO BE REMOVED
- EXISTING LIGHTING CONTROLLER TO REMAIN
- EXISTING LIGHT POLE TO REMAIN
- PROPOSED WOOD POLE, 50', CLASS 3

For Estimate of Lighting Quantities  
Refer to Sheets P.1 - P.3



**LIGHTING REMOVALS  
GRANT / STATE**



**LEGEND:**

EXISTING LIGHT POLE TO BE REMOVED



EXISTING LIGHT POLE TO BE RELOCATED



EXISTING LUMINAIRE, MAST ARM, AND WOOD POLE TO BE REMOVED BY OTHERS



EXISTING HANDHOLE TO BE REMOVED



EXISTING LIGHTING CONTROLLER TO BE REMOVED



EXISTING LIGHTING CONTROLLER TO REMAIN



EXISTING LIGHT POLE TO REMAIN



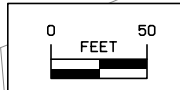
PROPOSED WOOD POLE, 50', CLASS 3



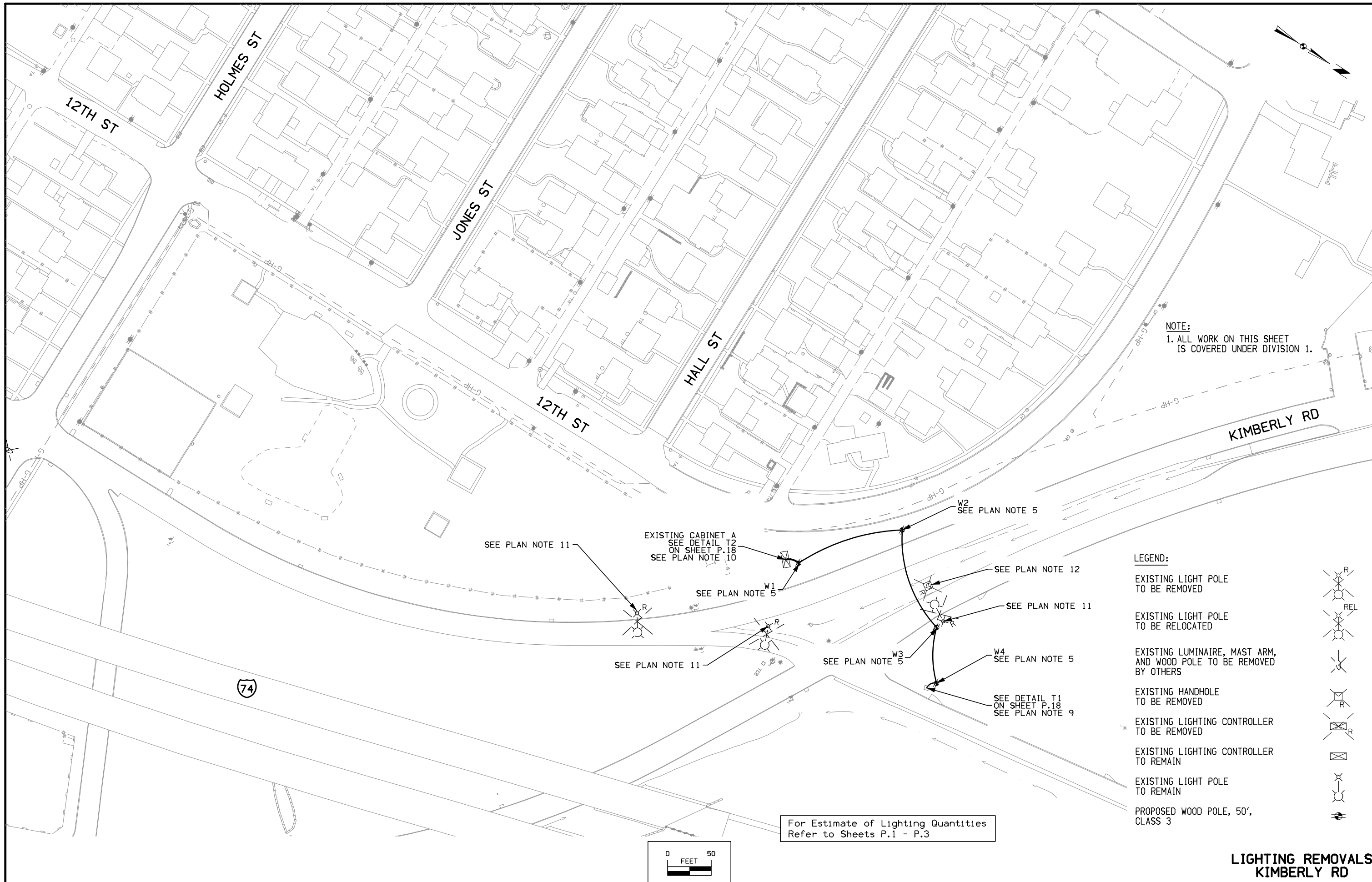
SEE PLAN NOTE 14

SEE PLAN NOTE 14

For Estimate of Lighting Quantities Refer to Sheets P.1 - P.3



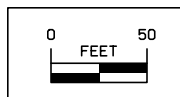
**LIGHTING REMOVALS  
KIMBERLY RD**



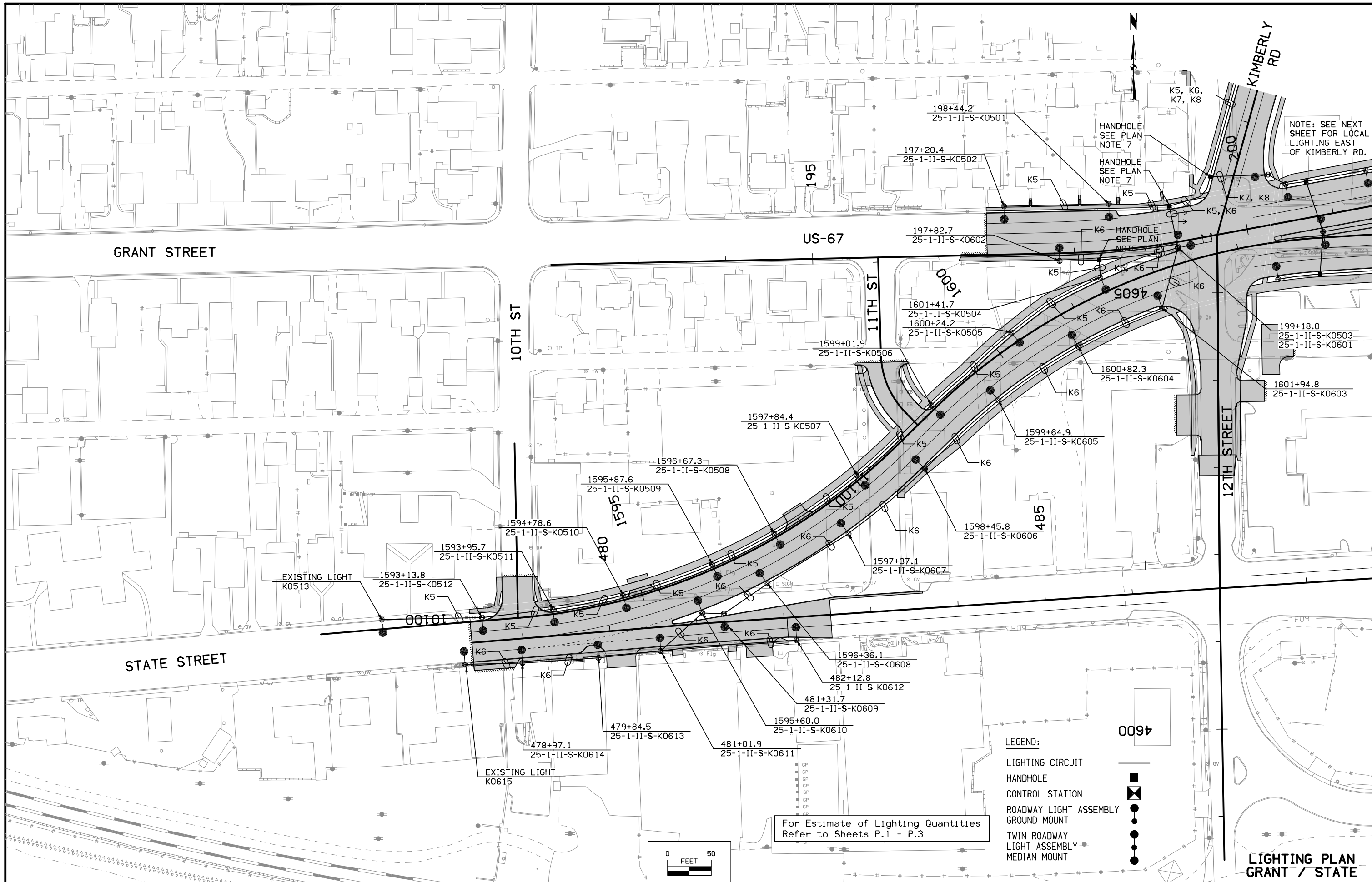
**NOTE:**  
1. ALL WORK ON THIS SHEET IS COVERED UNDER DIVISION 1.

- LEGEND:**
- EXISTING LIGHT POLE TO BE REMOVED
  - EXISTING LIGHT POLE TO BE RELOCATED
  - EXISTING LUMINAIRE, MAST ARM, AND WOOD POLE TO BE REMOVED BY OTHERS
  - EXISTING HANDHOLE TO BE REMOVED
  - EXISTING LIGHTING CONTROLLER TO BE REMOVED
  - EXISTING LIGHTING CONTROLLER TO REMAIN
  - EXISTING LIGHT POLE TO REMAIN
  - PROPOSED WOOD POLE, 50', CLASS 3

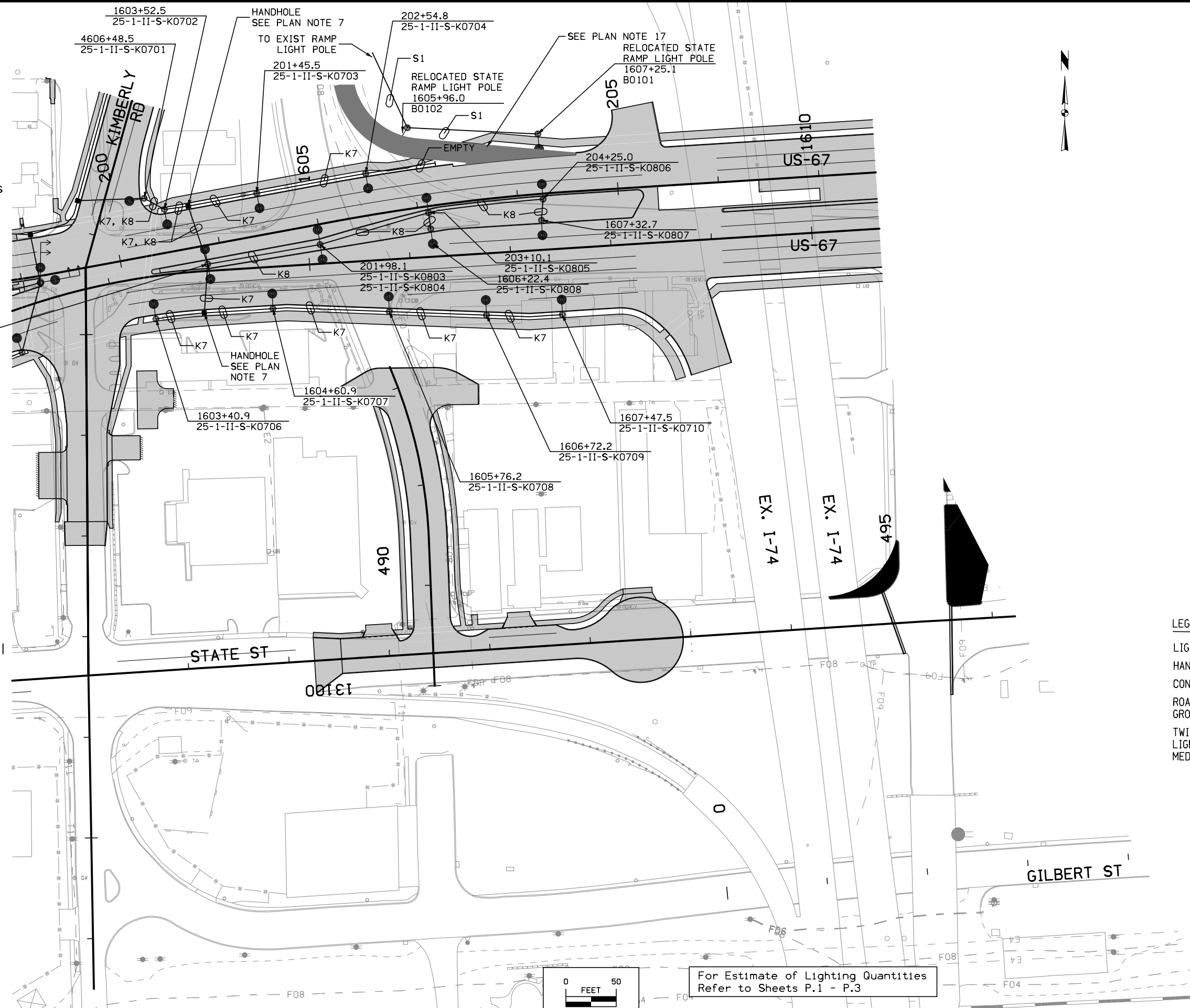
For Estimate of Lighting Quantities Refer to Sheets P.1 - P.3



**LIGHTING REMOVALS  
KIMBERLY RD**

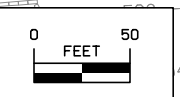


NOTE: SEE PREVIOUS SHEET FOR LOCAL LIGHTING WEST OF KIMBERLY RD.

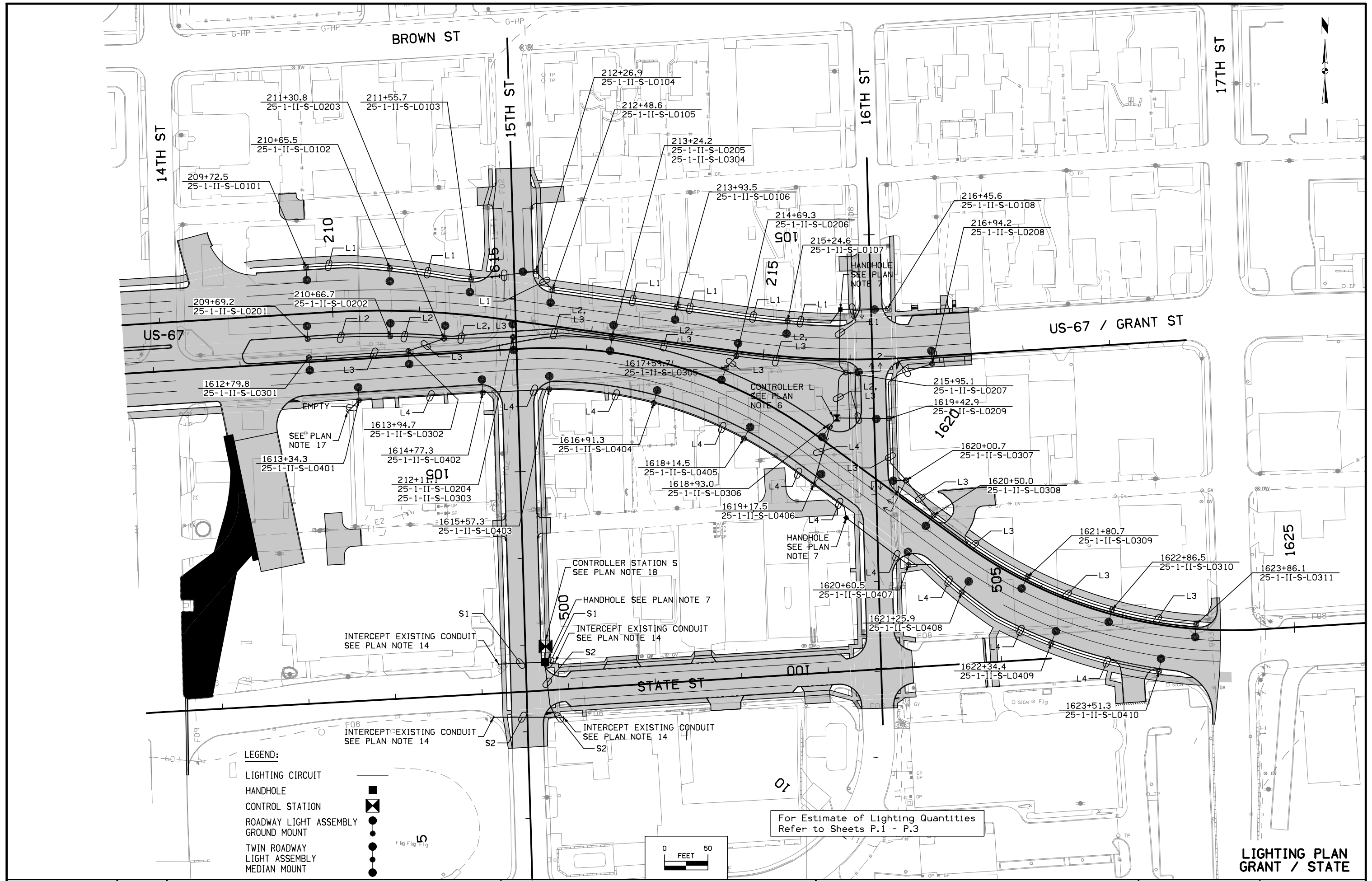


- LEGEND:
- LIGHTING CIRCUIT ———
  - HANDHOLE ■
  - CONTROL STATION □
  - ROADWAY LIGHT ASSEMBLY ●
  - GROUND MOUNT ●
  - TWIN ROADWAY LIGHT ASSEMBLY ●●
  - MEDIAN MOUNT ●●

For Estimate of Lighting Quantities Refer to Sheets P.1 - P.3



LIGHTING PLAN  
GRANT / STATE



**LEGEND:**

- LIGHTING CIRCUIT
- HANDHOLE
- CONTROL STATION
- ROADWAY LIGHT ASSEMBLY
- GROUND MOUNT
- TWIN ROADWAY LIGHT ASSEMBLY
- MEDIAN MOUNT

For Estimate of Lighting Quantities  
Refer to Sheets P.1 - P.3



**LIGHTING PLAN  
GRANT / STATE**

ENGLISH

IOWA DOT

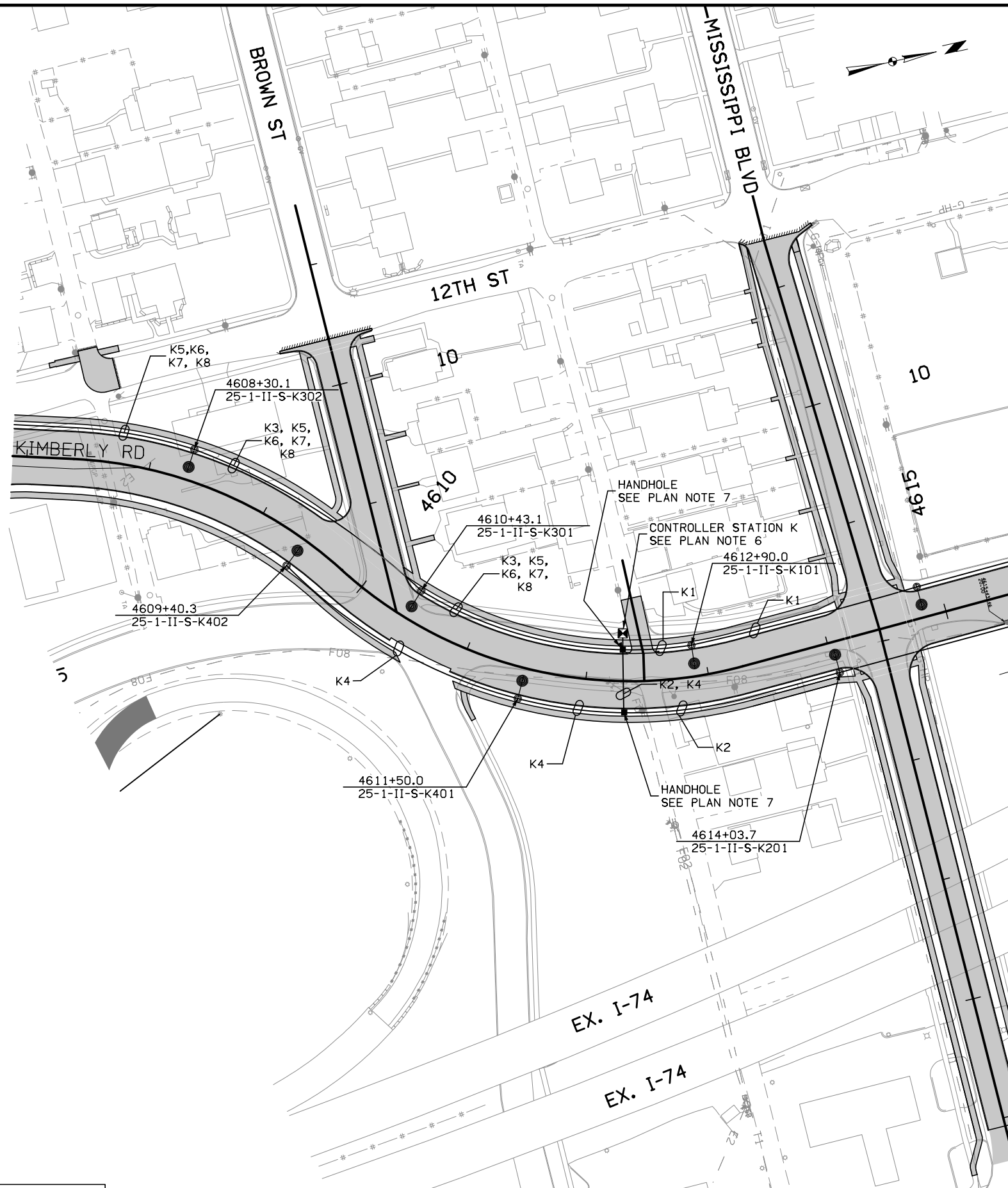
DESIGN TEAM **BENESCH**

SCOTT COUNTY

PROJECT NUMBER **IM-074-1(226)5--13-82**

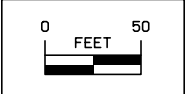
SHEET NUMBER **P.11**



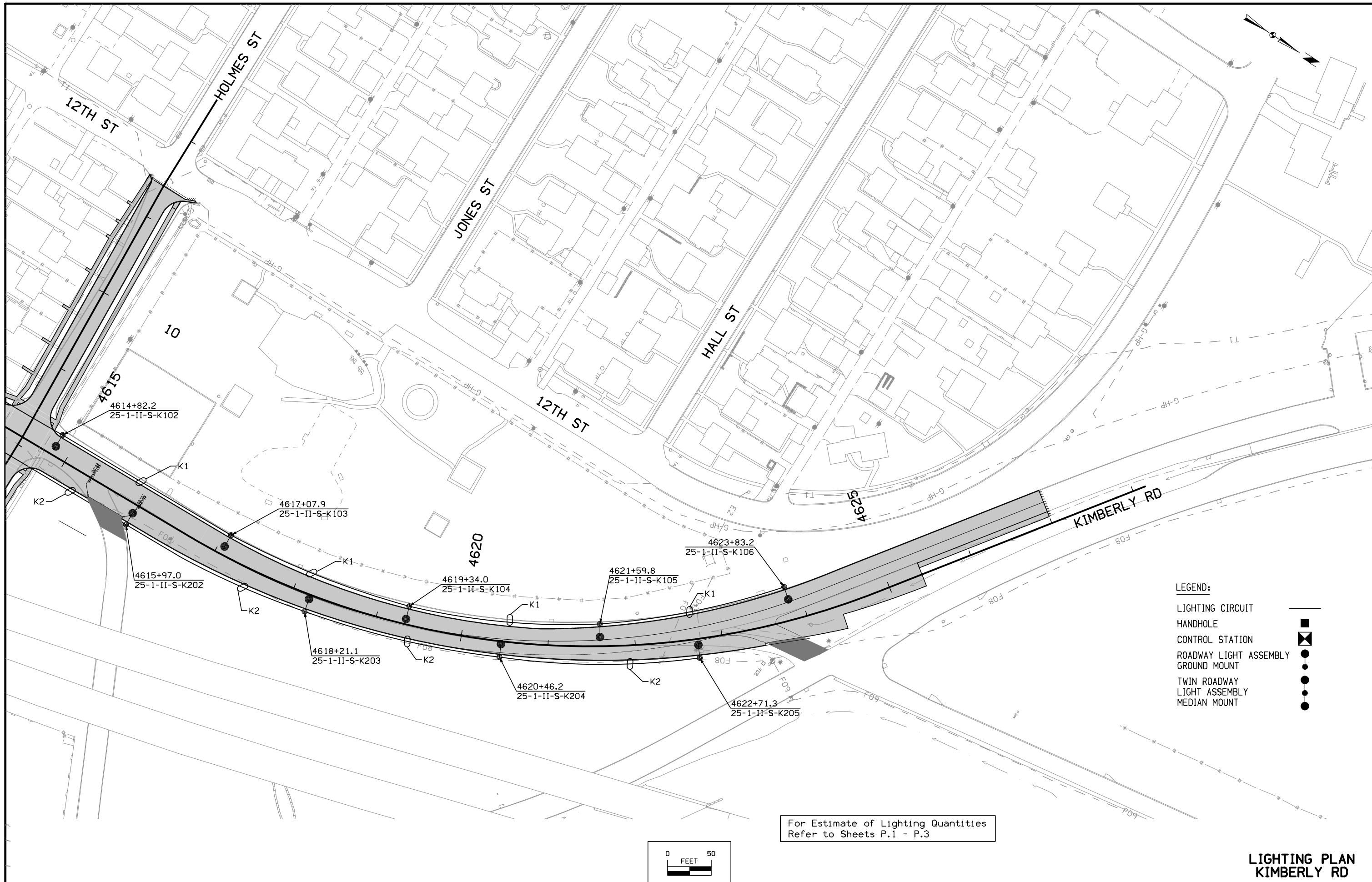


- LEGEND:**
- LIGHTING CIRCUIT ———
  - HANDHOLE ■
  - CONTROL STATION ■
  - ROADWAY LIGHT ASSEMBLY ●
  - GROUND MOUNT ●
  - TWIN ROADWAY LIGHT ASSEMBLY ● ●
  - MEDIAN MOUNT ● ● ●

For Estimate of Lighting Quantities  
Refer to Sheets P.1 - P.3

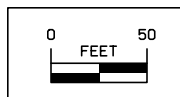


**LIGHTING PLAN  
KIMBERLY RD**

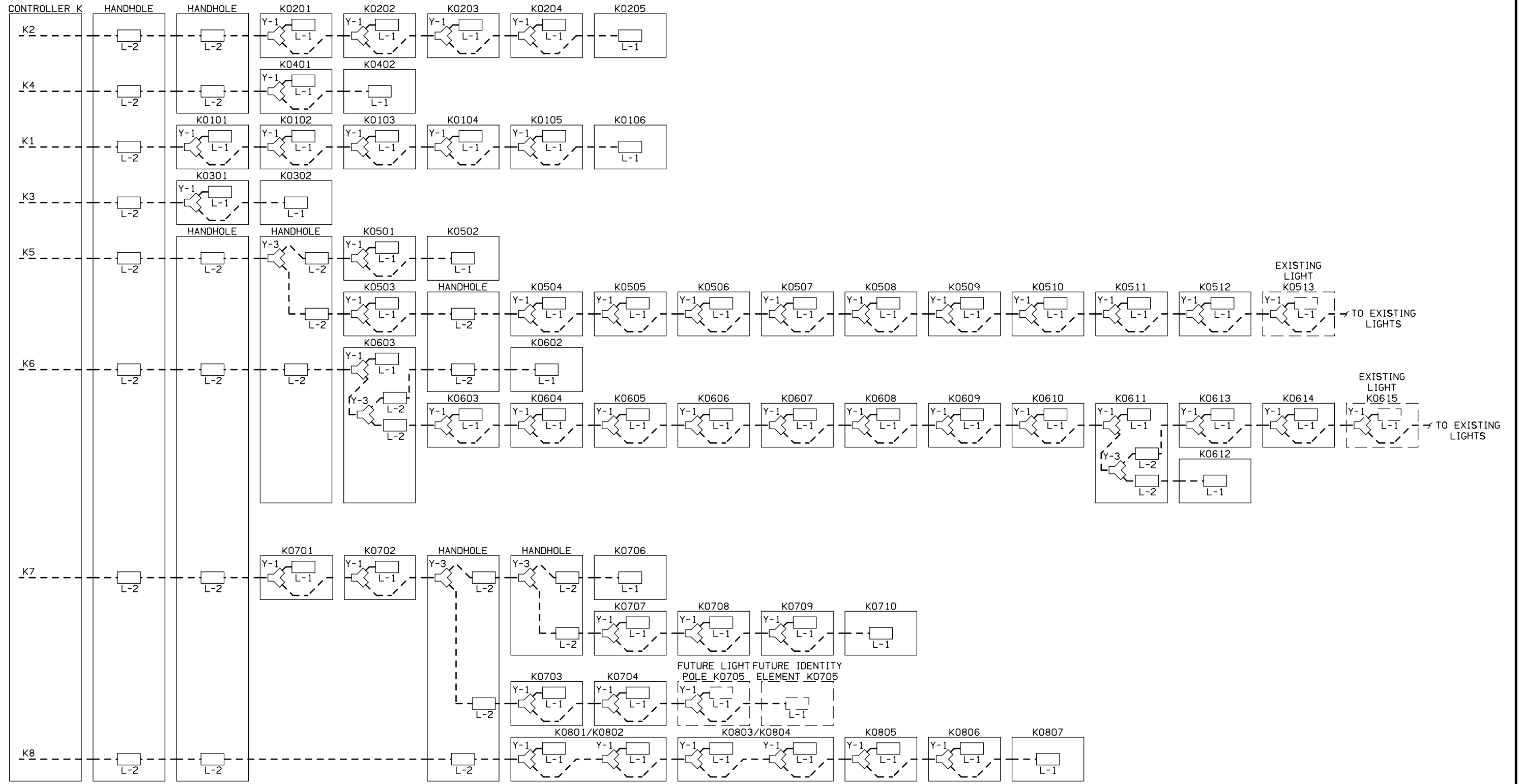


- LEGEND:**
- LIGHTING CIRCUIT
  - HANDHOLE
  - CONTROL STATION
  - ROADWAY LIGHT ASSEMBLY
  - GROUND MOUNT
  - TWIN ROADWAY LIGHT ASSEMBLY
  - MEDIAN MOUNT

For Estimate of Lighting Quantities  
Refer to Sheets P.1 - P.3



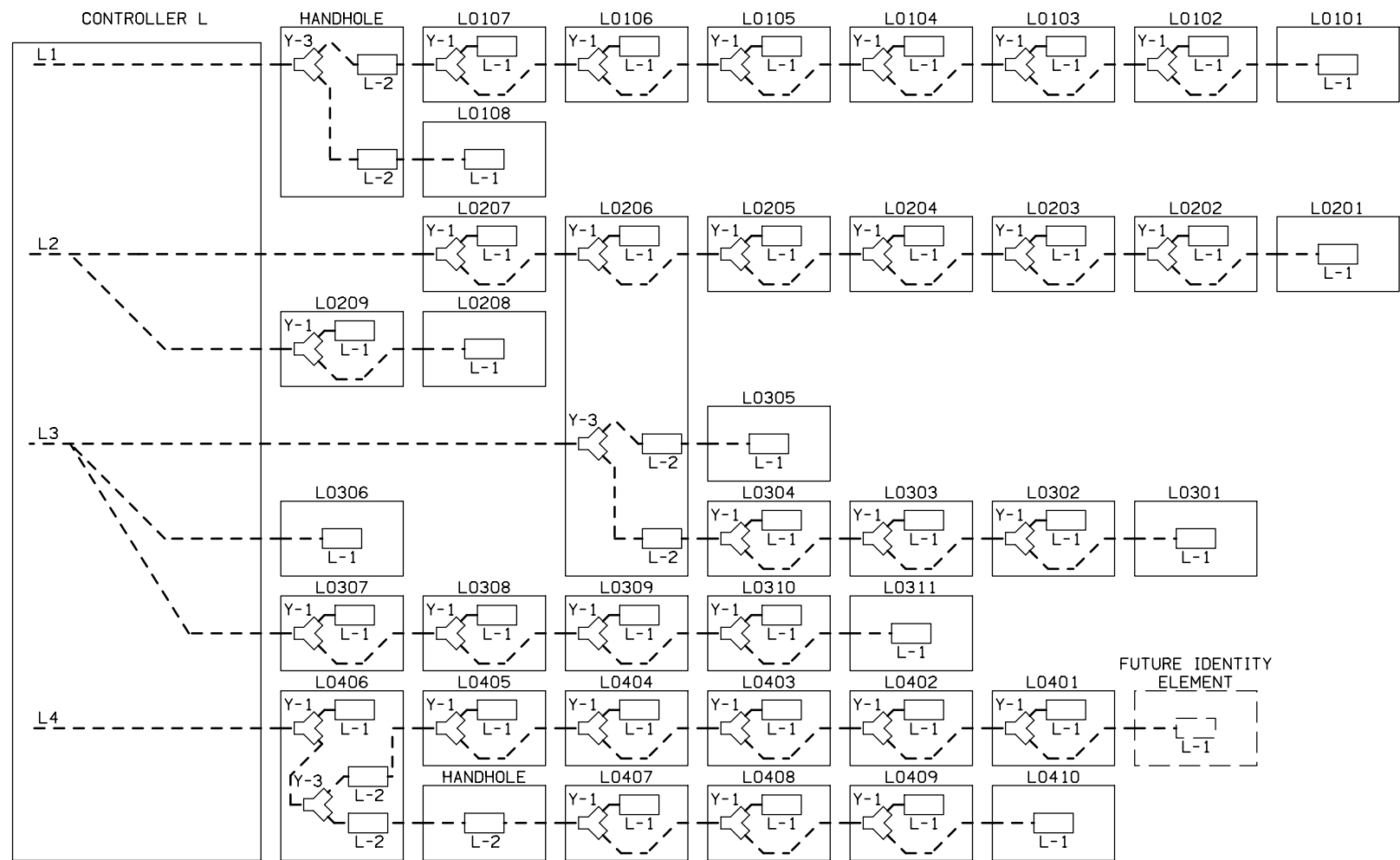
**LIGHTING PLAN  
KIMBERLY RD**



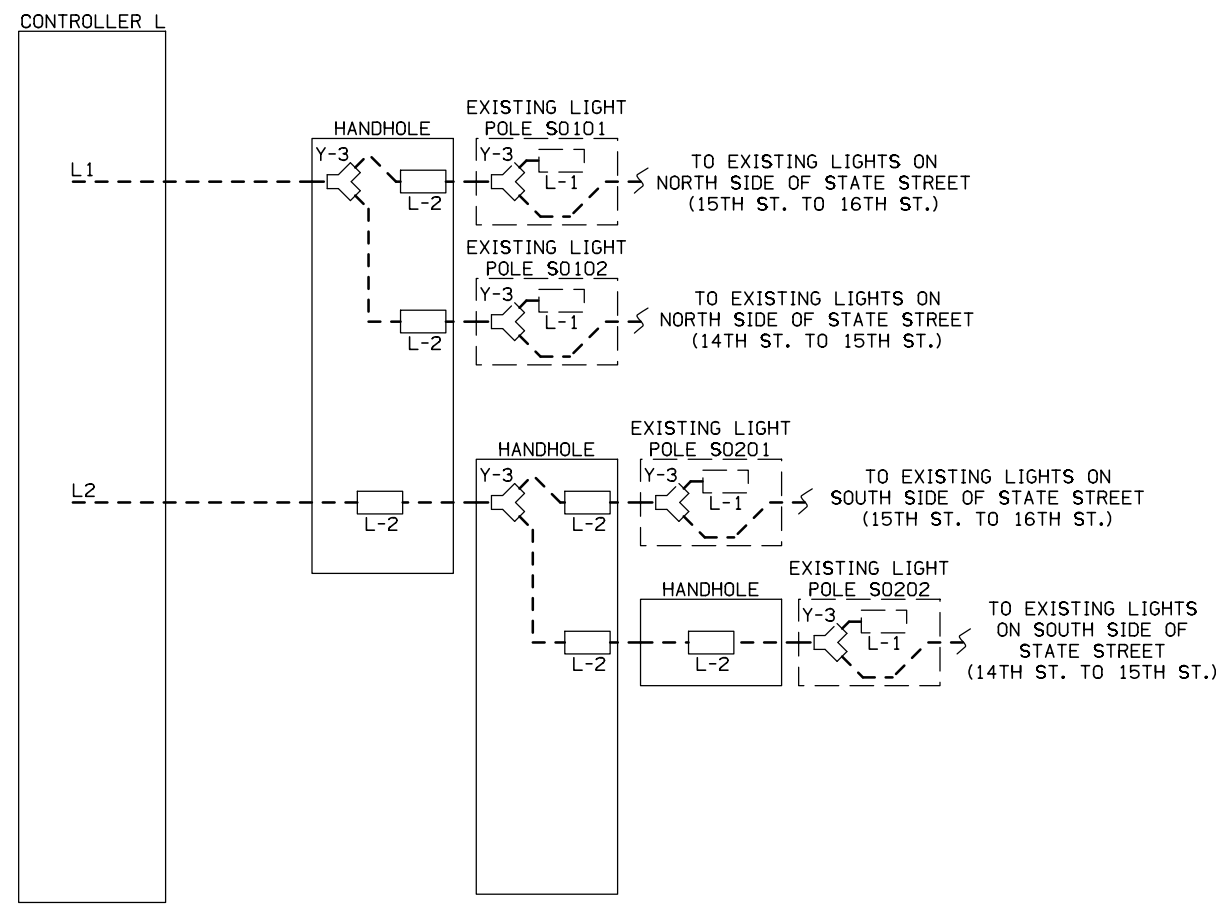
CABINET K WIRING DETAIL - 200 AMP MAIN

\* INSTALL FEMALE PORTION OF CONNECTOR.  
TAPE AND REAL FOR FUTURE CONNECTION.

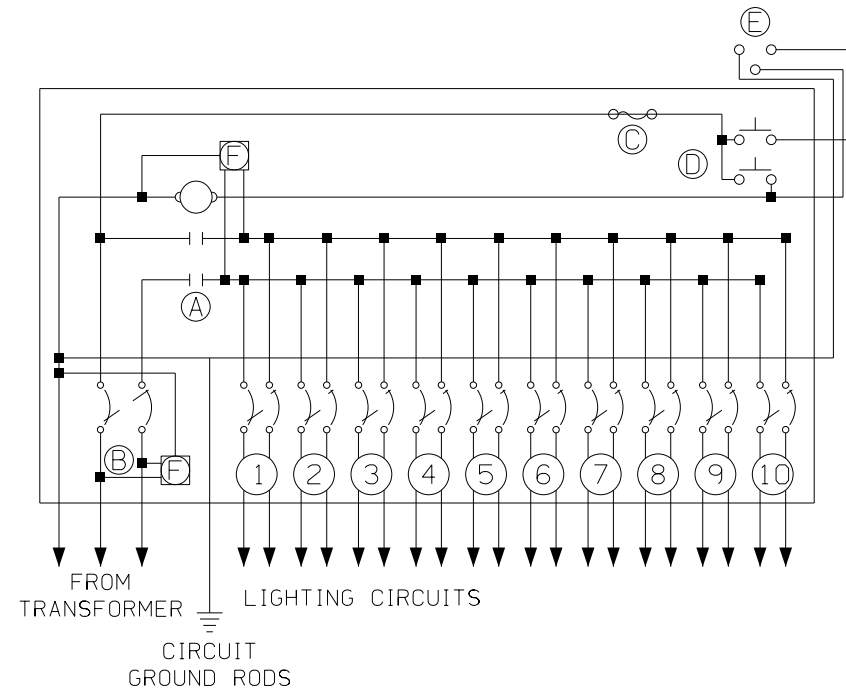
**LIGHTING  
DETAILS**



CABINET L WIRING DETAIL - 200 AMP MAIN

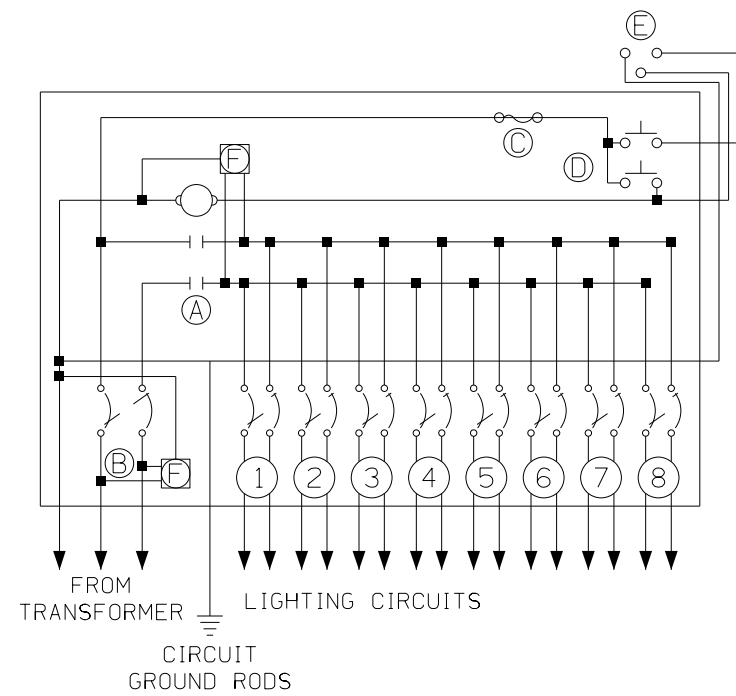


CABINET S WIRING DIAGRAM - 100 AMP MAIN



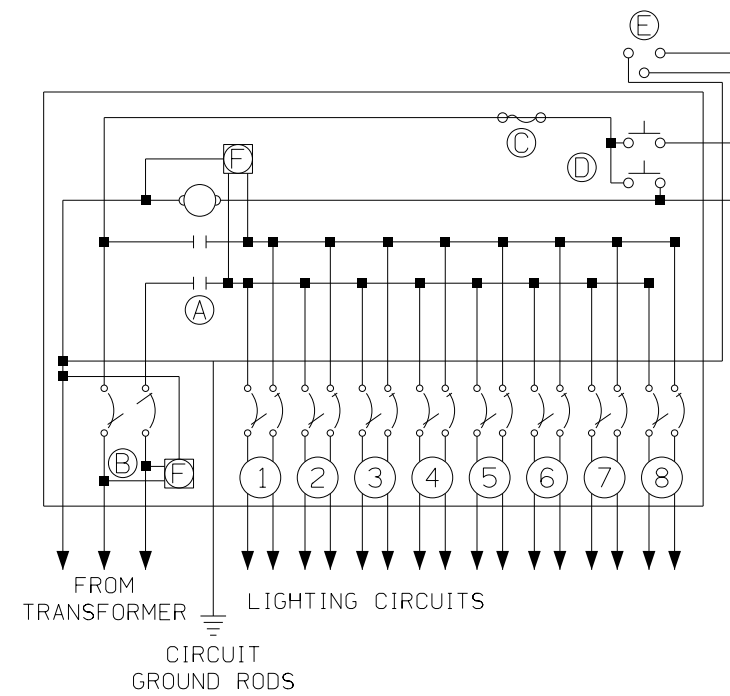
- A. CONTACTOR (2P.)
- B. LINE BREAKER (2P.)
- C. CONTROL FUSE
- D. TEST SWITCH
- E. PHOTOCELL
- F. SURGE SUPPRESSOR
- 1 THRU 10. BRANCH CIRCUIT BREAKERS (2P.)
  - 1. ROADWAY LIGHTS ON KIMBERLY ROAD
  - 2. ROADWAY LIGHTS ON KIMBERLY ROAD
  - 3. ROADWAY LIGHTS ON KIMBERLY ROAD
  - 4. ROADWAY LIGHTS ON KIMBERLY ROAD
  - 5. ROADWAY LIGHTS ON US-67 / GRANT ST / STATE ST
  - 6. ROADWAY LIGHTS ON US-67 / GRANT ST / STATE ST
  - 7. ROADWAY LIGHTS ON US-67 / GRANT ST / STATE ST
  - 8. ROADWAY LIGHTS ON US-67 / GRANT ST / STATE ST
  - 9. ROADWAY LIGHTS ON STATE ST
  - 10. BLANK

CIRCUIT K SCHEMATIC DIAGRAM



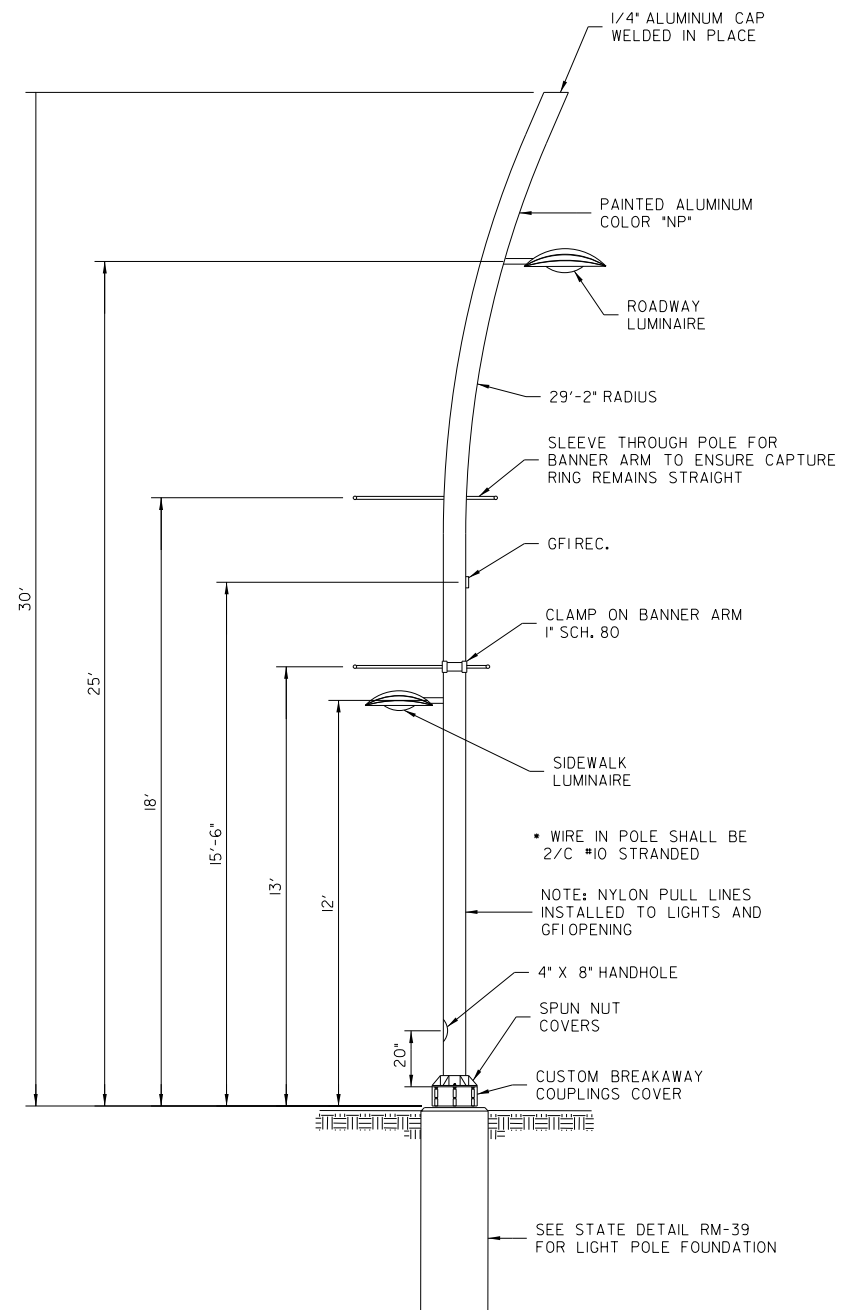
- A. CONTACTOR (2P.)
- B. LINE BREAKER (2P.)
- C. CONTROL FUSE
- D. TEST SWITCH
- E. PHOTOCELL
- F. SURGE SUPPRESSOR
- 1 THRU 8. BRANCH CIRCUIT BREAKERS (2P.)
  - 1. ROADWAY LIGHTS ON US-67 / GRANT ST
  - 2. ROADWAY LIGHTS ON US-67 / GRANT ST
  - 3. ROADWAY LIGHTS ON US-67 / GRANT ST
  - 4. ROADWAY LIGHTS ON US-67 / GRANT ST
  - 5. BLANK
  - 6. BLANK
  - 7. BLANK
  - 8. BLANK

CIRCUIT L SCHEMATIC DIAGRAM

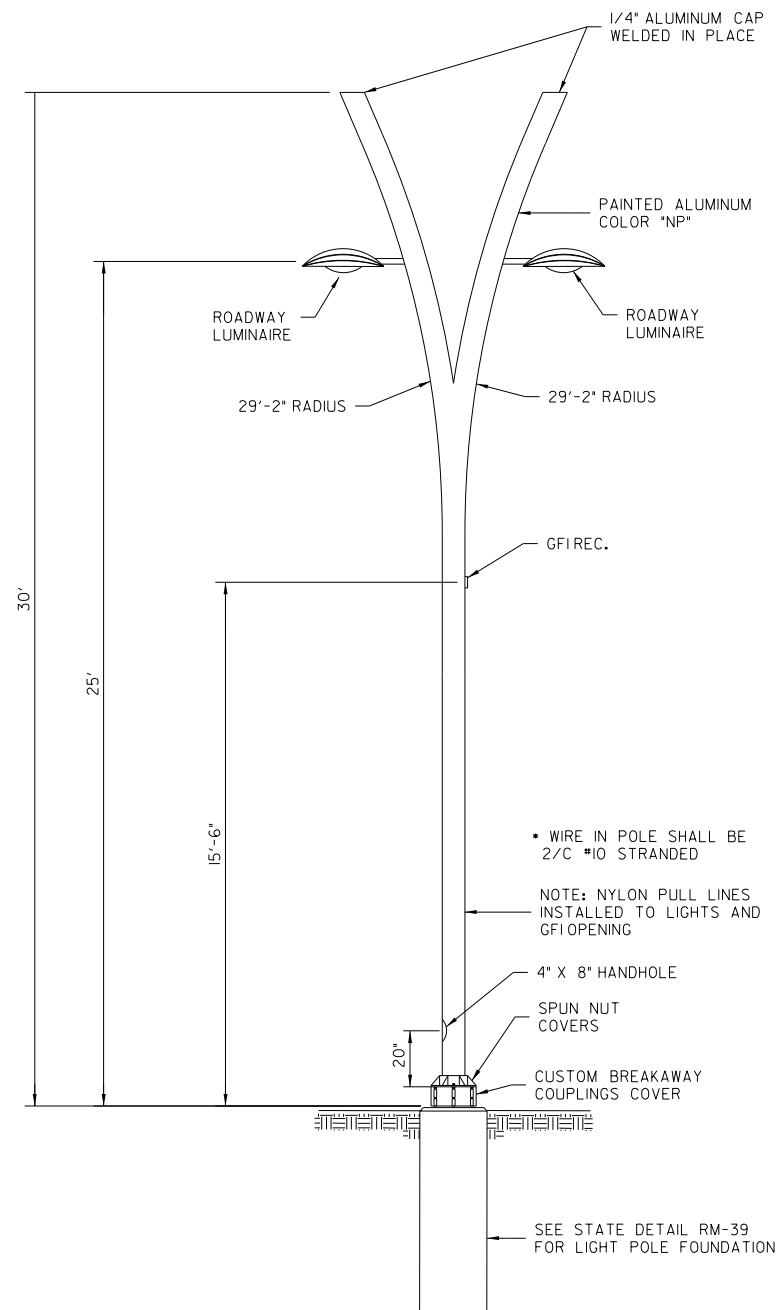


- A. CONTACTOR (2P.)
- B. LINE BREAKER (2P.)
- C. CONTROL FUSE
- D. TEST SWITCH
- E. PHOTOCELL
- F. SURGE SUPPRESSOR
- 1 THRU 8. BRANCH CIRCUIT BREAKERS (2P.)
  - 1. ROADWAY LIGHTS ON STATE ST
  - 2. ROADWAY LIGHTS ON STATE ST
  - 3. BLANK
  - 4. BLANK
  - 5. BLANK
  - 6. BLANK
  - 7. BLANK
  - 8. BLANK

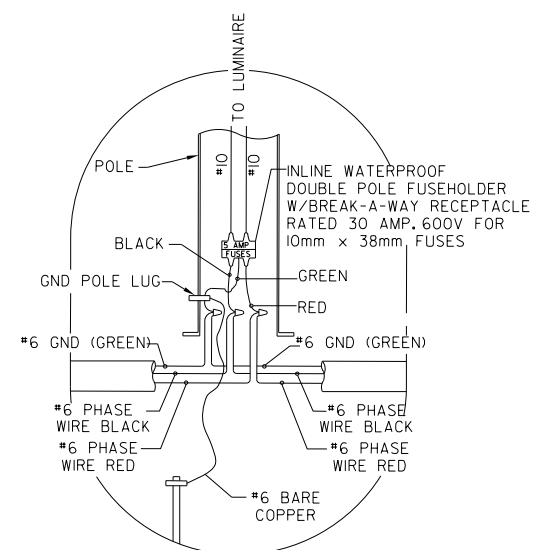
CIRCUIT S SCHEMATIC DIAGRAM



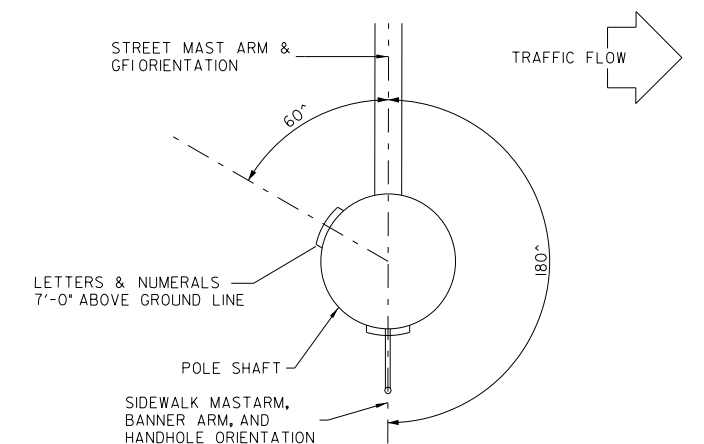
TYPE I  
DAVIT POLE - CITY OF BETTENDORF  
NO SCALE



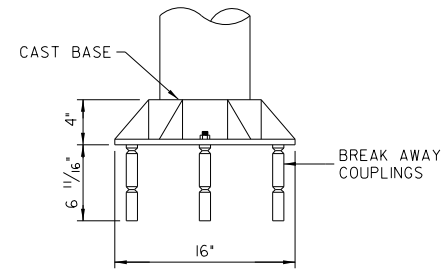
TYPE II  
DAVIT POLE - CITY OF BETTENDORF  
NO SCALE



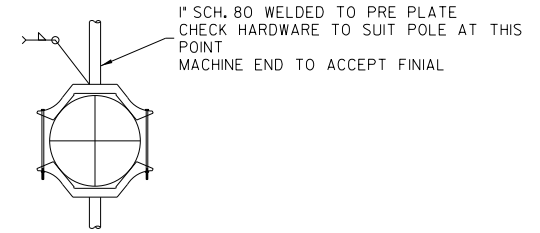
POLE WIRING DETAIL  
NO SCALE



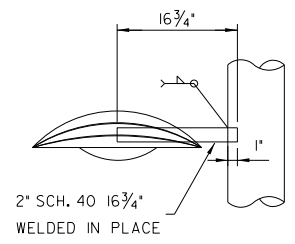
POSITION OF HANDHOLE AND POLE NUMBER  
FOR SINGLE MAST ARM POLES



AB-8-16 BASE  
NO SCALE

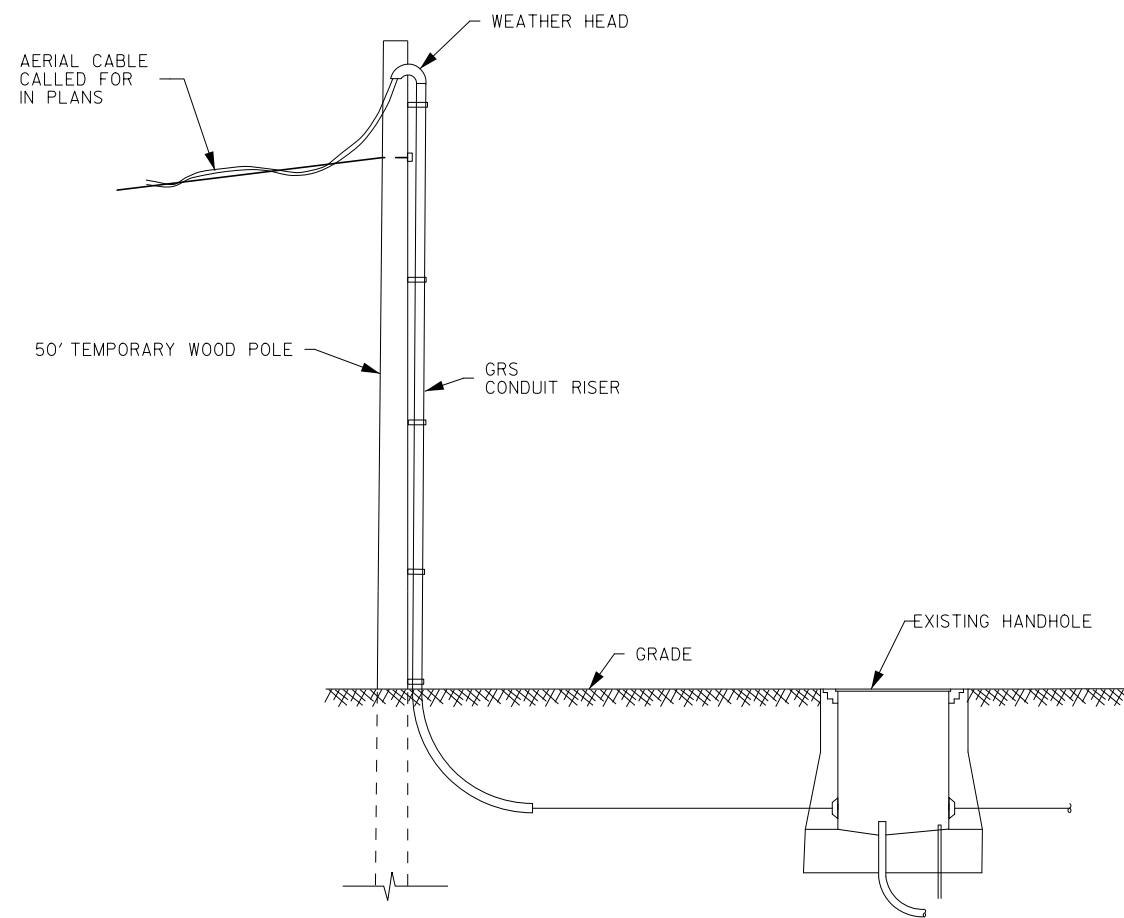


BANNER ARM DETAIL  
NO SCALE

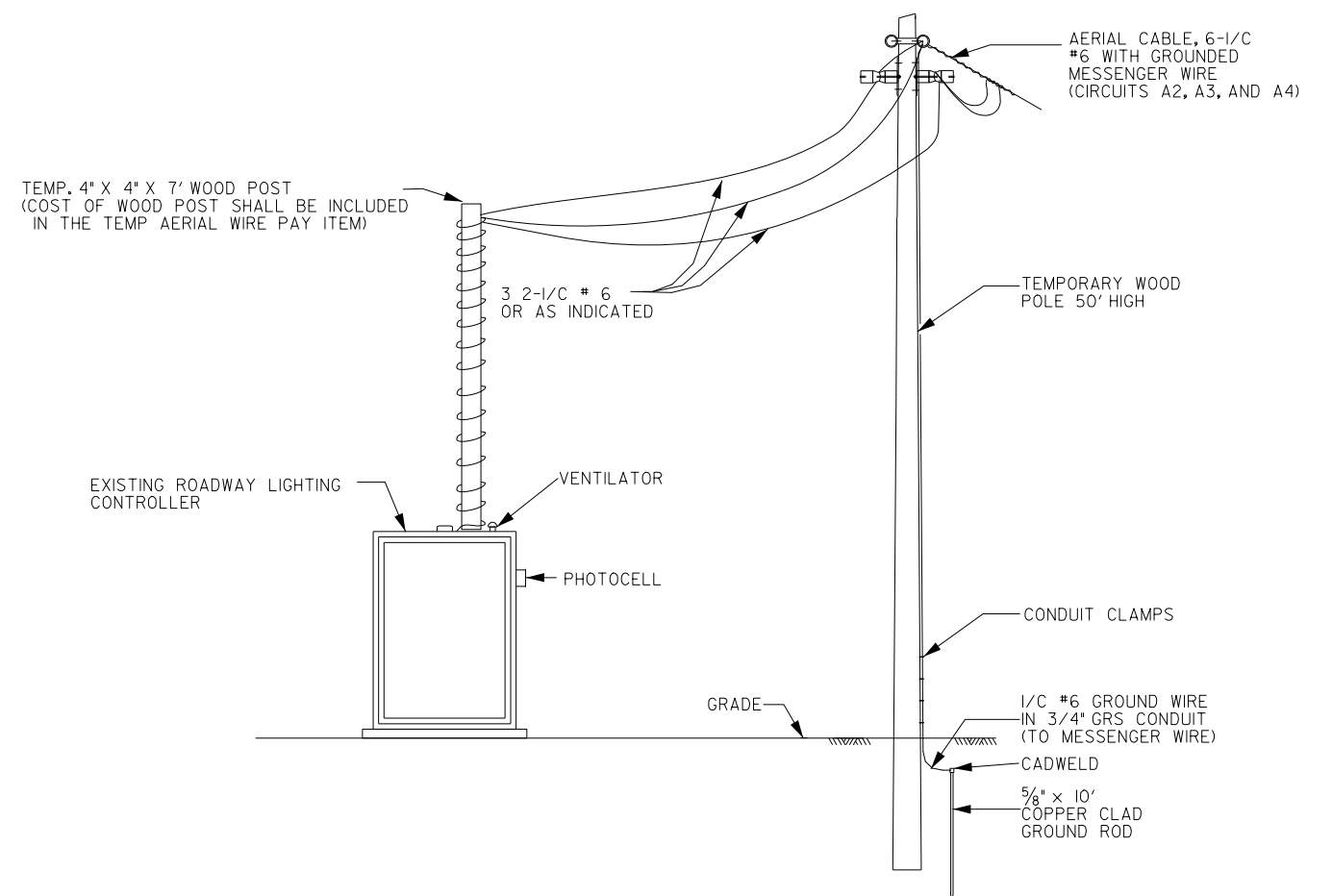


MAST ARM DETAIL  
NO SCALE

LIGHTING  
DETAILS

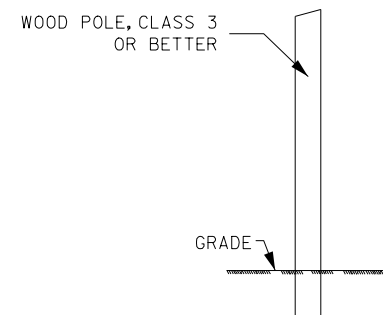


PROPOSED TEMPORARY POLE/  
HAND HOLE CONNECTION  
DETAIL T1  
NOT TO SCALE



TEMPORARY WOOD POLE/  
CONTROLLER CONNECTION  
DETAIL T2

NOT TO SCALE



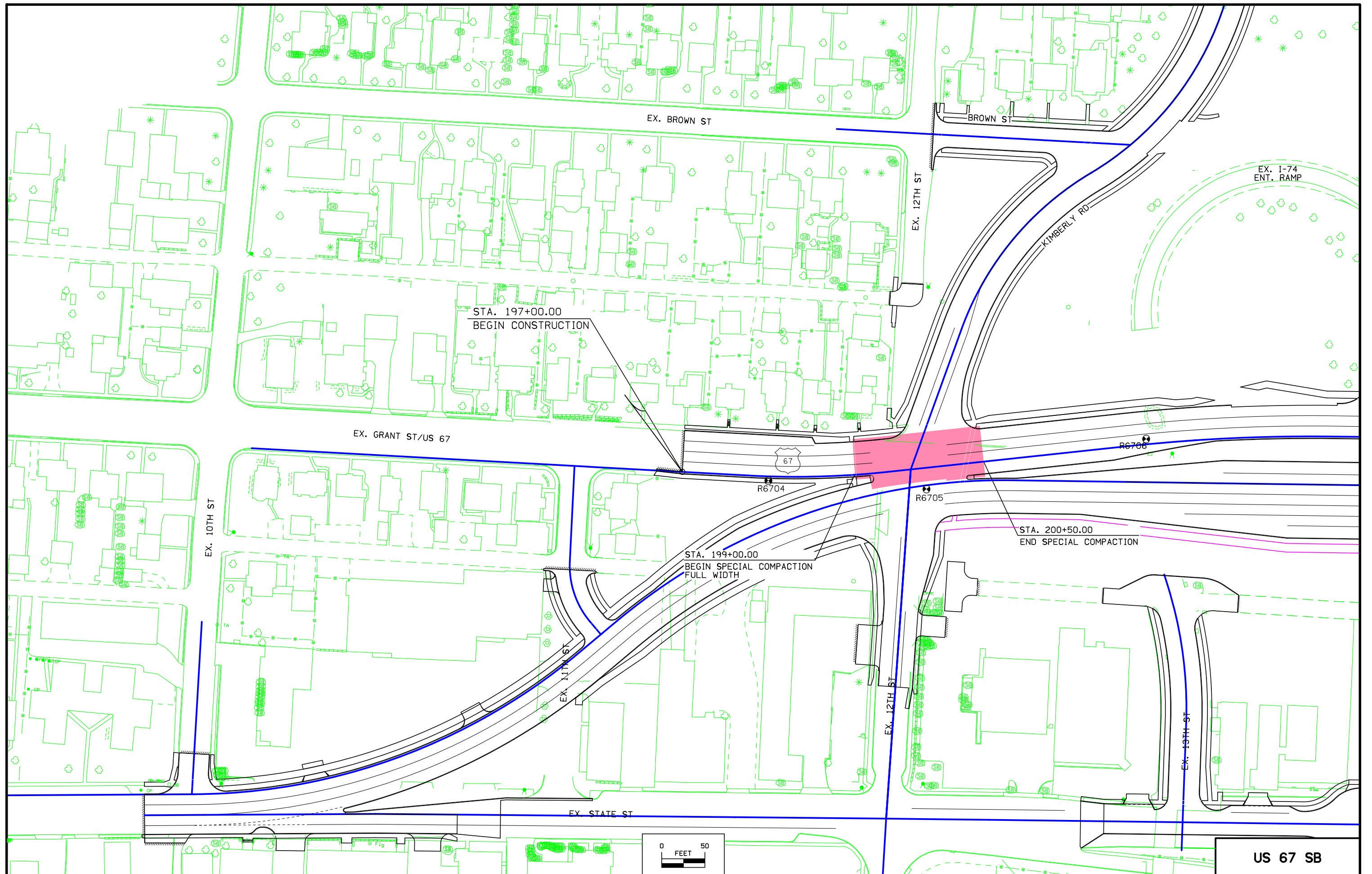
POLE LENGTH	DEPTH IN GROUND
65'	12'
60'	10'
55'	9'
50'	8'
45'	7'
40'	6'-6"
35'	6'
30'	5'-6"

WOOD POLE DETAIL

NOT TO SCALE

**NOTE:**

1. ALL WORK ON THIS SHEET IS COVERED UNDER DIVISION I.

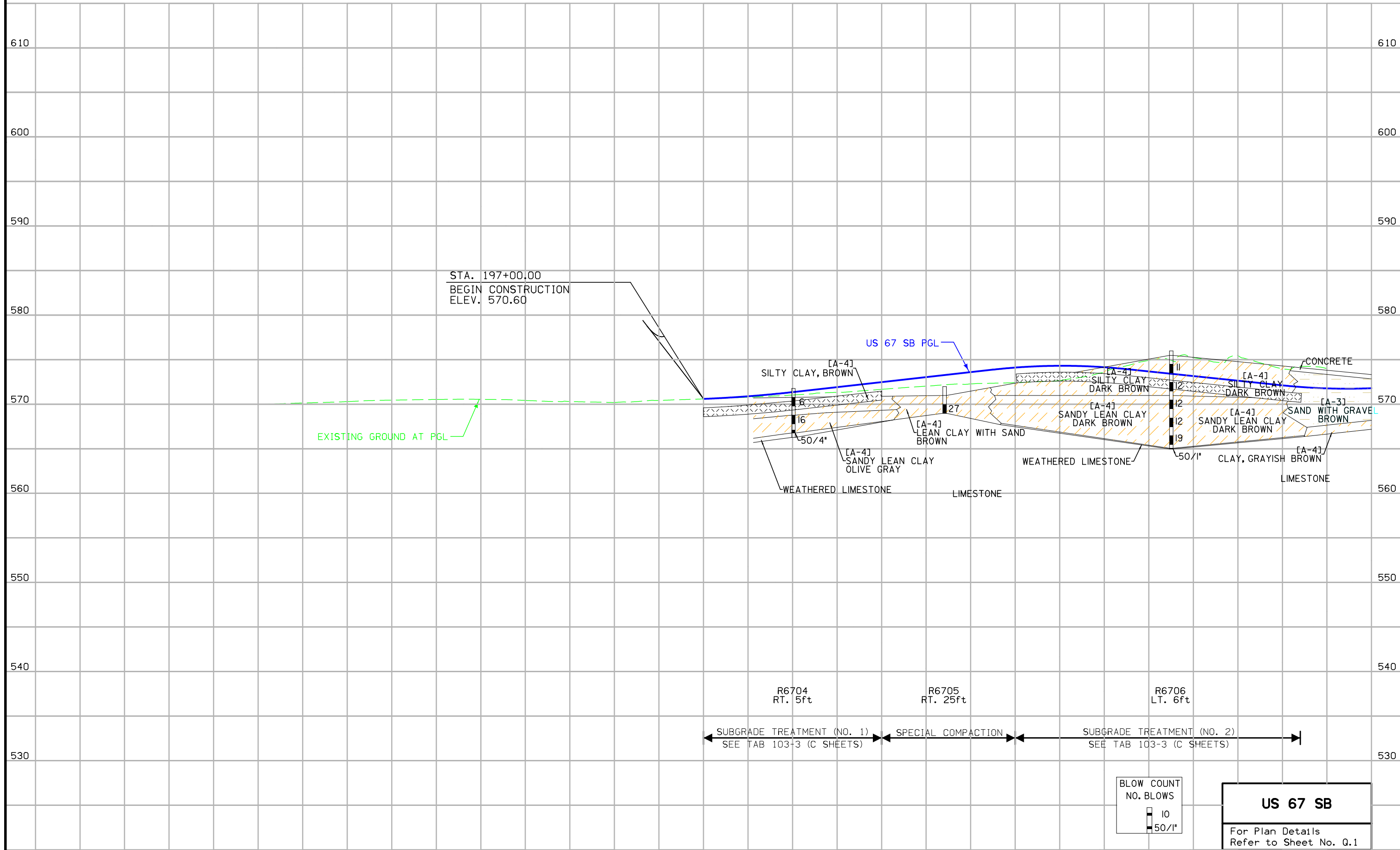


US 67 SB



CUT MOISTURE  
 CUT DENSITY (lb/cu ft)  
 PLASTIC LIMIT

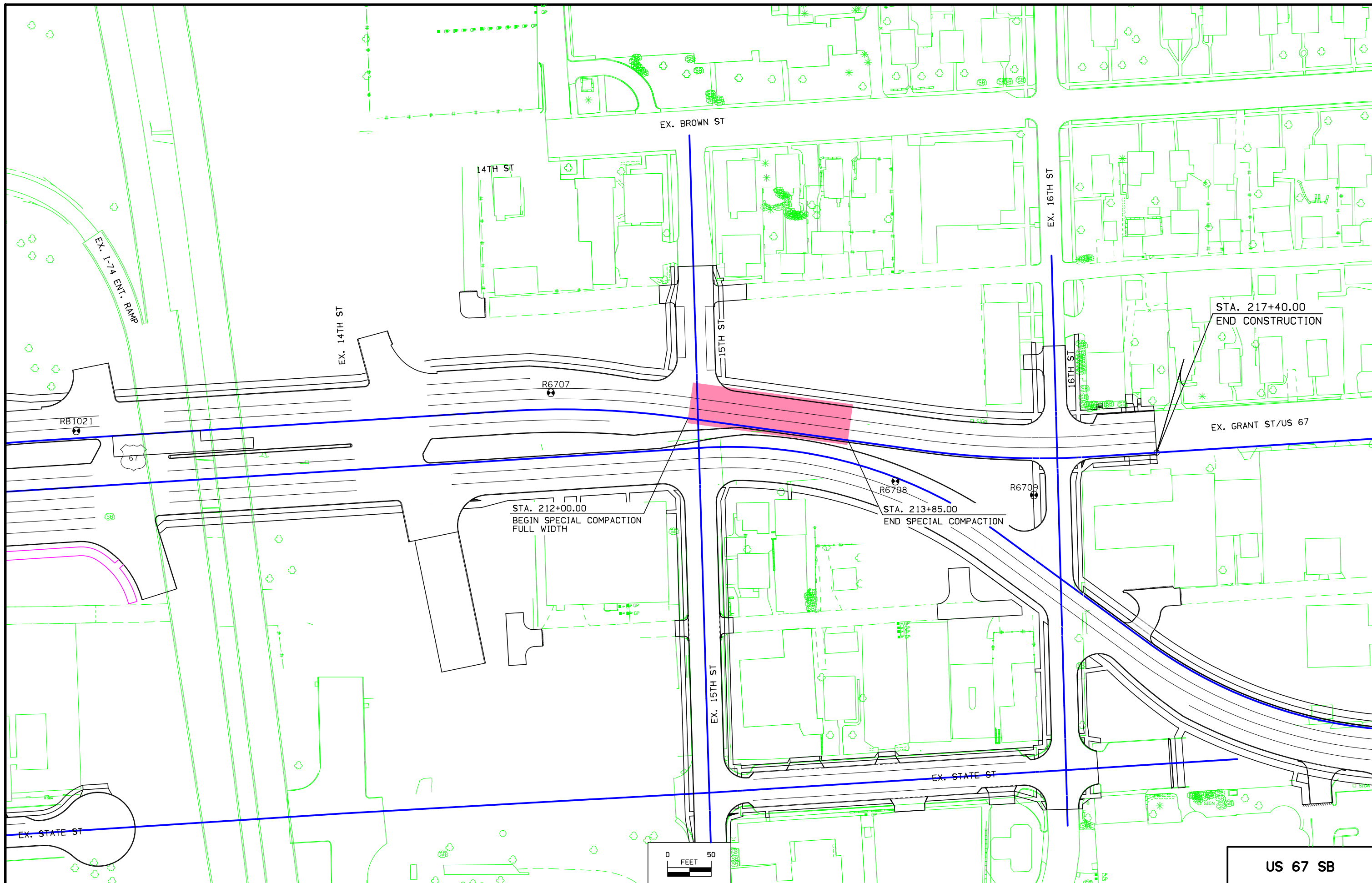
21.4  
 18



- UNSUITABLE
- SUBGRADE TREATMENT
- SHALE
- ROCK
- SELECT SOIL
- SANDY SOIL
- DENS. CORE
- SELECT SAND
- SHELBY
- BLOW
- SAMPLE
- PLUGGED
- WATER
- MOISTURE

BLOW COUNT  
 NO. BLOWS  
 10  
 50/1'

**US 67 SB**  
 For Plan Details  
 Refer to Sheet No. Q.1

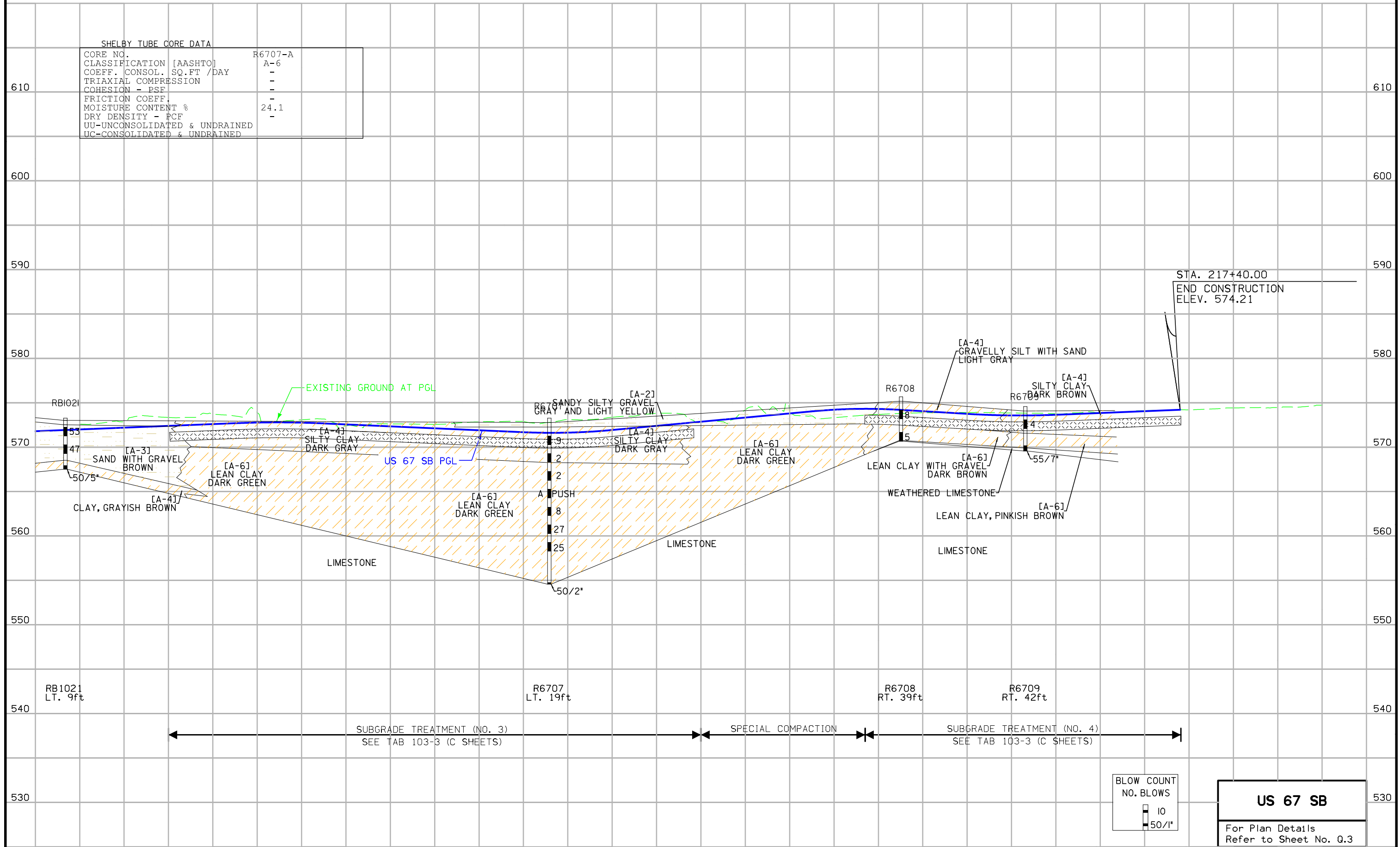


CUT MOISTURE  
 CUT DENSITY (lb/cu ft)  
 PLASTIC LIMIT

30.1  
 -  
 31

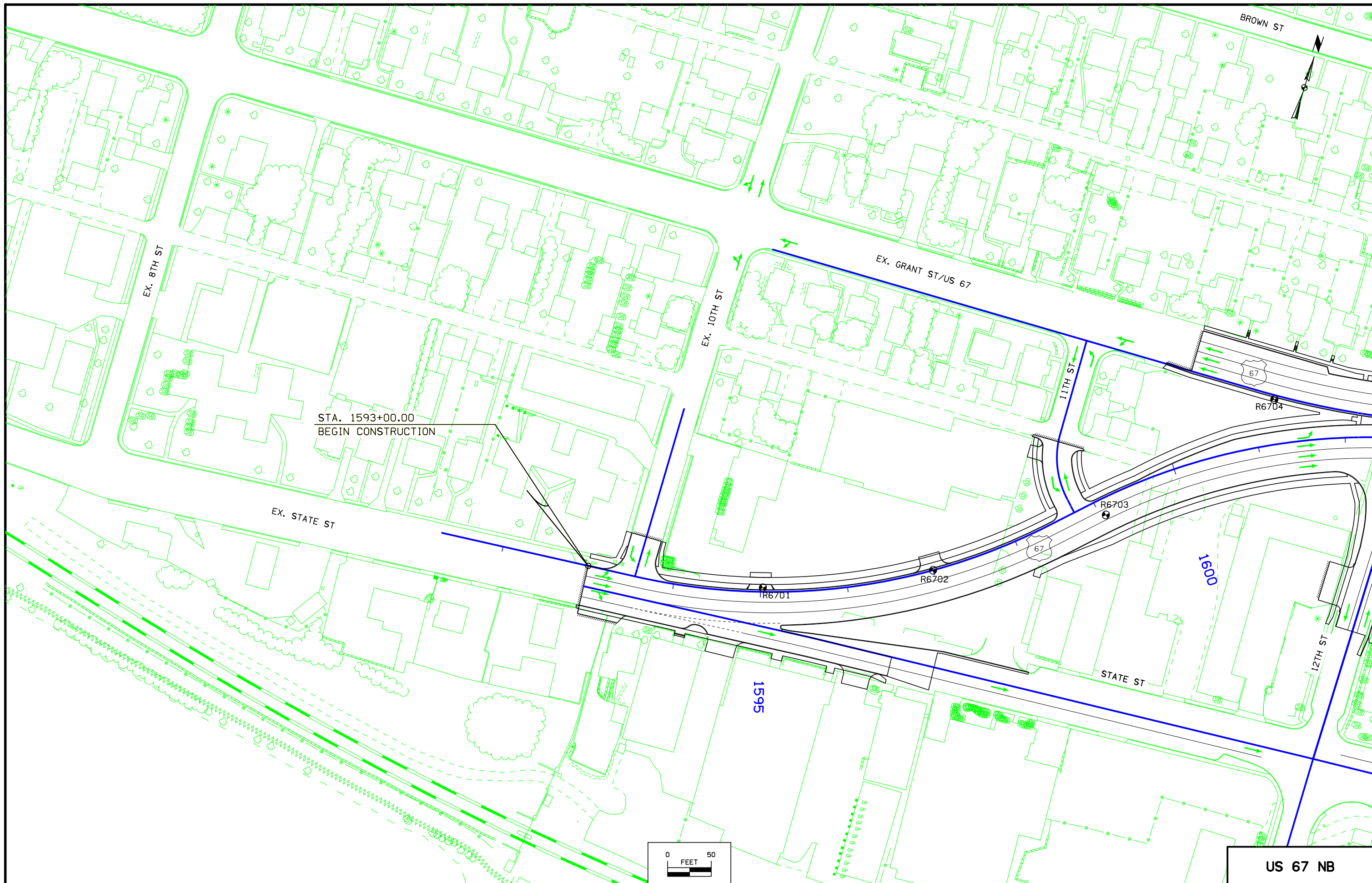
-  
 -  
 23.8  
 -  
 17

SHELBY TUBE CORE DATA	
CORE NO.	R6707-A
CLASSIFICATION [AASHTO]	A-6
COEFF. CONSOL. SQ.FT /DAY	-
TRIAXIAL COMPRESSION	-
COHESION - PSF	-
FRICTION COEFF.	-
MOISTURE CONTENT %	24.1
DRY DENSITY - PCF	-
UU-UNCONSOLIDATED & UNDRAINED	-
UC-CONSOLIDATED & UNDRAINED	-

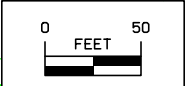


BLOW COUNT  
 NO. BLOWS  
 10  
 50/1'

**US 67 SB**  
 For Plan Details  
 Refer to Sheet No. Q.3



STA. 1593+00.00  
BEGIN CONSTRUCTION

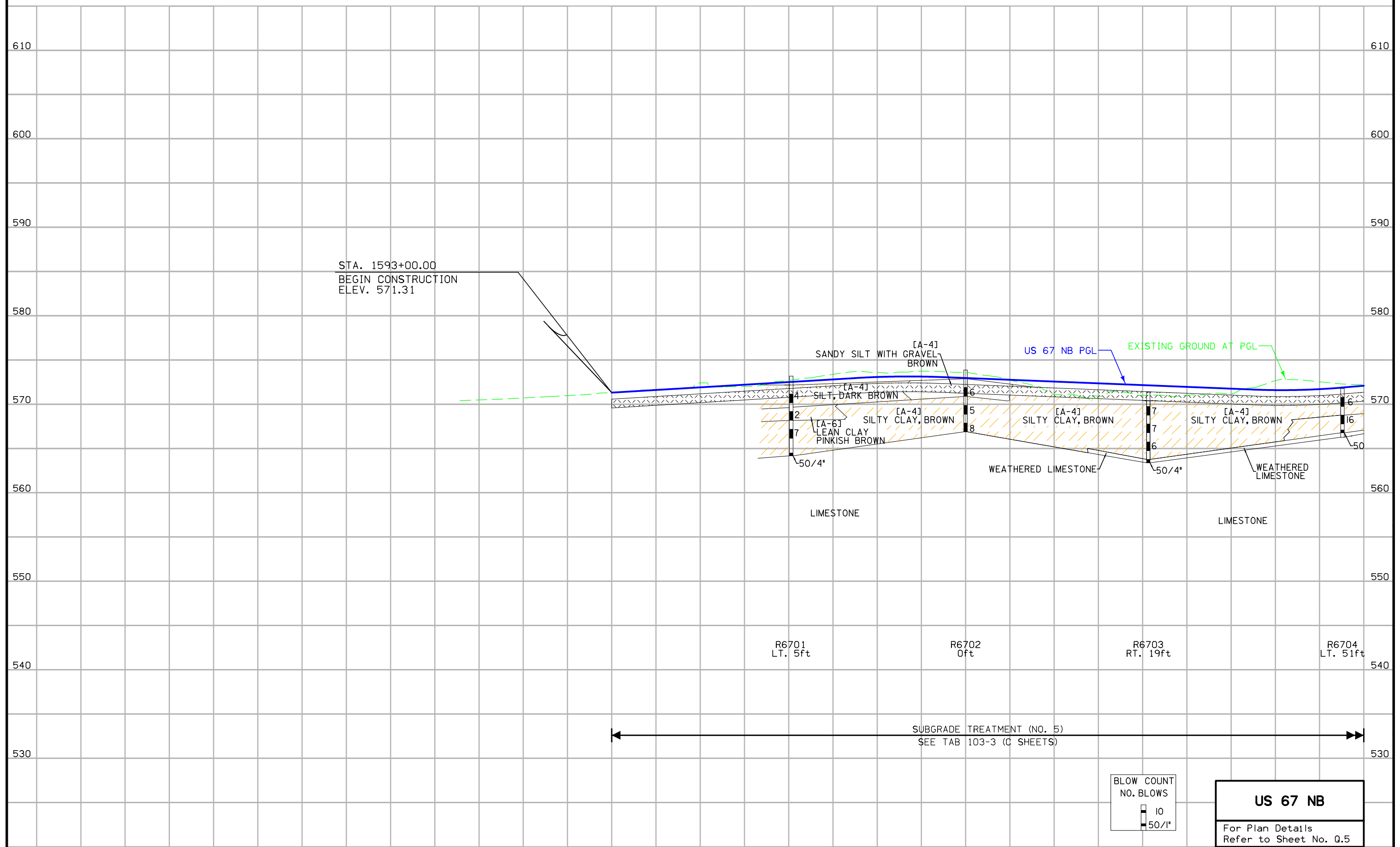


US 67 NB

CUT MOISTURE  
 CUT DENSITY (lb/cu ft)  
 PLASTIC LIMIT

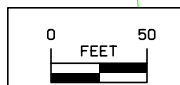
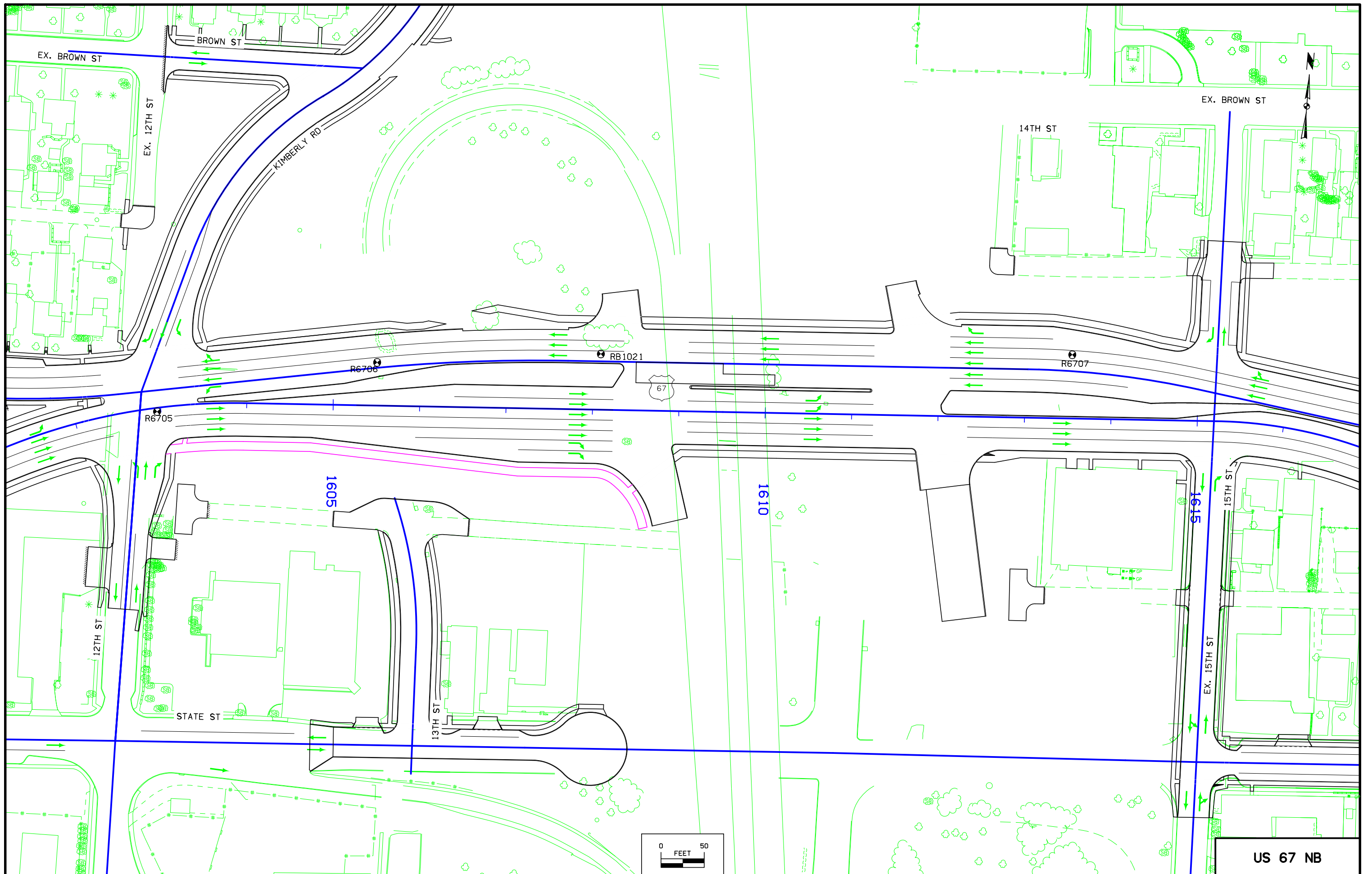
28.1  
 -  
 17

27.3  
 -  
 23



BLOW COUNT  
 NO. BLOWS  
 10  
 50/1'

**US 67 NB**  
 For Plan Details  
 Refer to Sheet No. Q.5



**US 67 NB**

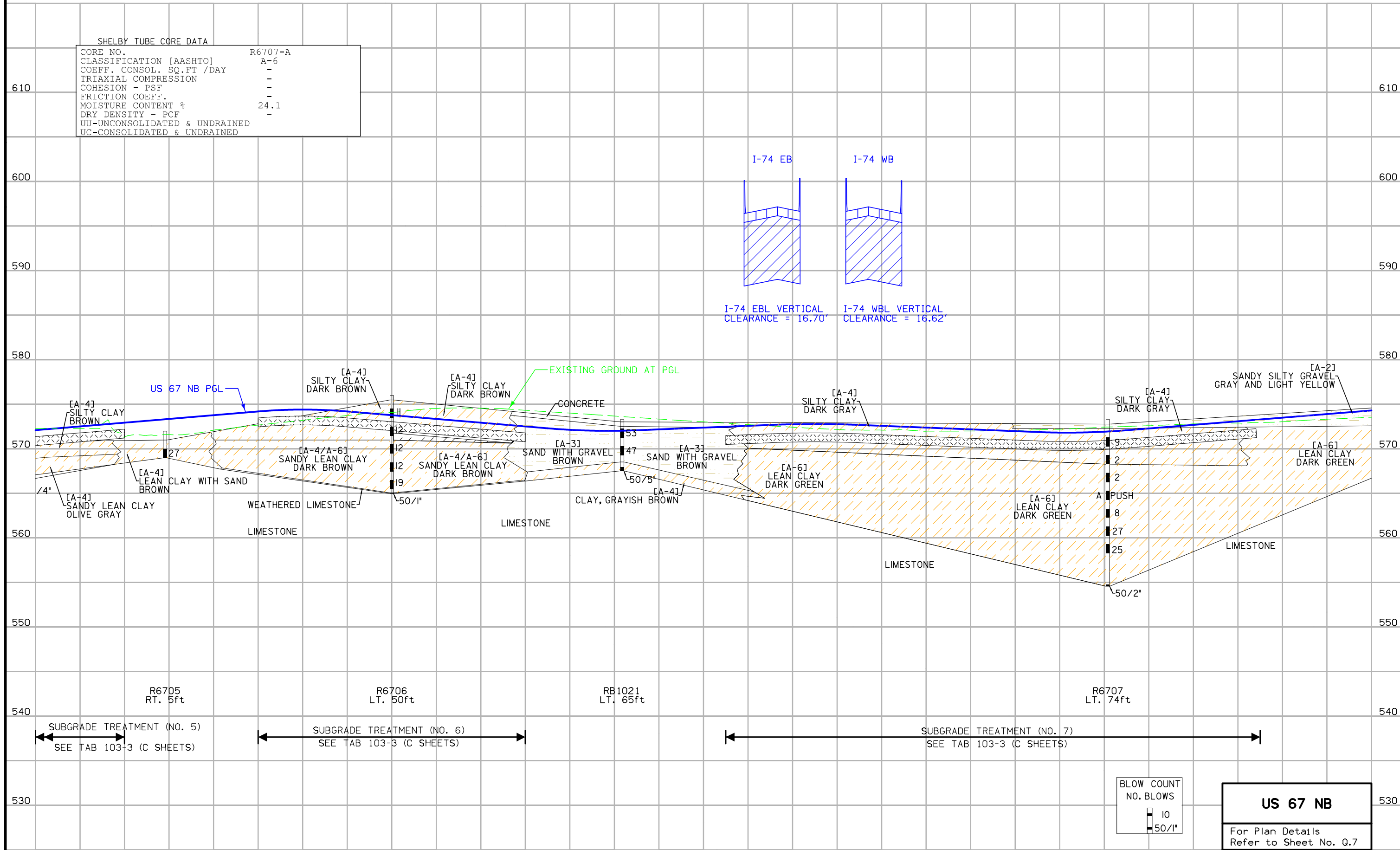
CUT MOISTURE  
 CUT DENSITY (lb/cu ft.)  
 PLASTIC LIMIT

21.4  
 -  
 18

30.1  
 -  
 31

SHELBY TUBE CORE DATA

CORE NO.	R6707-A
CLASSIFICATION [AASHTO]	A-6
COEFF. CONSOL. SQ.FT /DAY	-
TRIAxIAL COMPRESSION	-
COHESION - PSF	-
FRICTION COEFF.	-
MOISTURE CONTENT %	24.1
DRY DENSITY - PCF	-
UU-UNCONSOLIDATED & UNDRAINED	-
UC-CONSOLIDATED & UNDRAINED	-

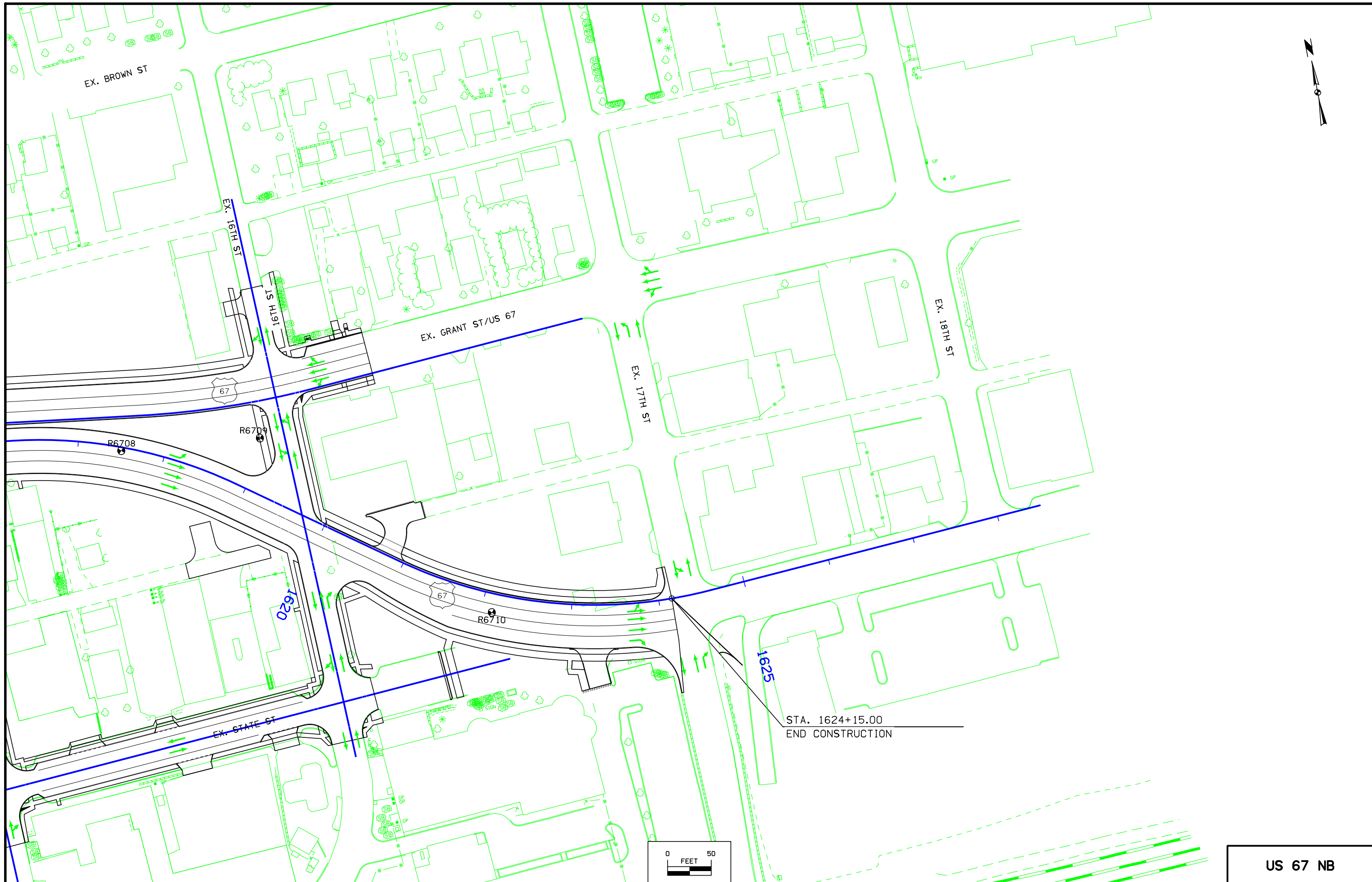


UNUSABLE TREATMENT  
 SHALE  
 ROCK  
 SELECT SOIL  
 SANDY SOIL  
 DENS. CORE  
 SELECT SAND  
 SHELBY  
 BLOW  
 SAMPLE  
 PLUGGED  
 WATER  
 MOISTURE

BLOW COUNT  
 NO. BLOWS

10
50/1'

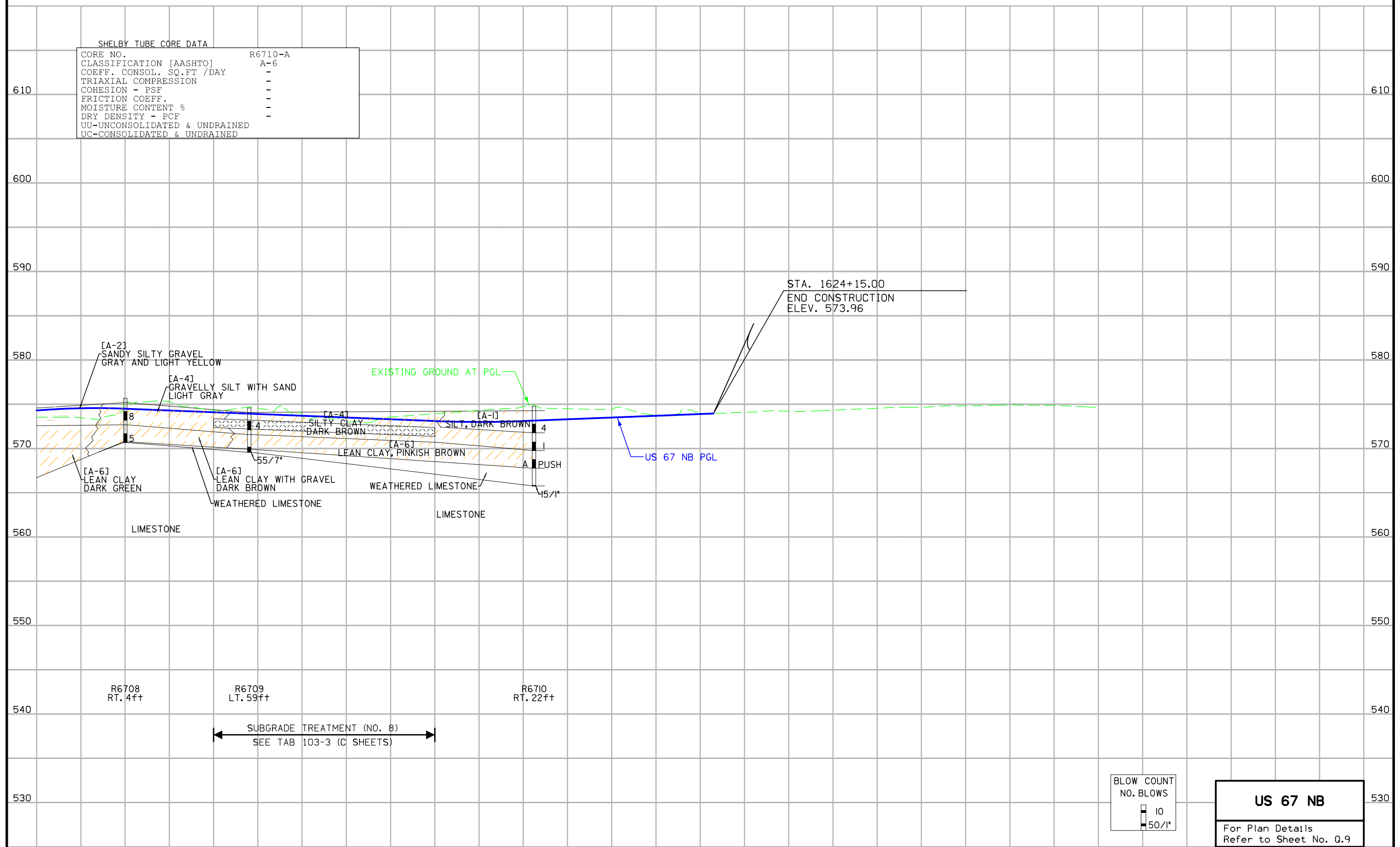
**US 67 NB**  
 For Plan Details  
 Refer to Sheet No. Q.7





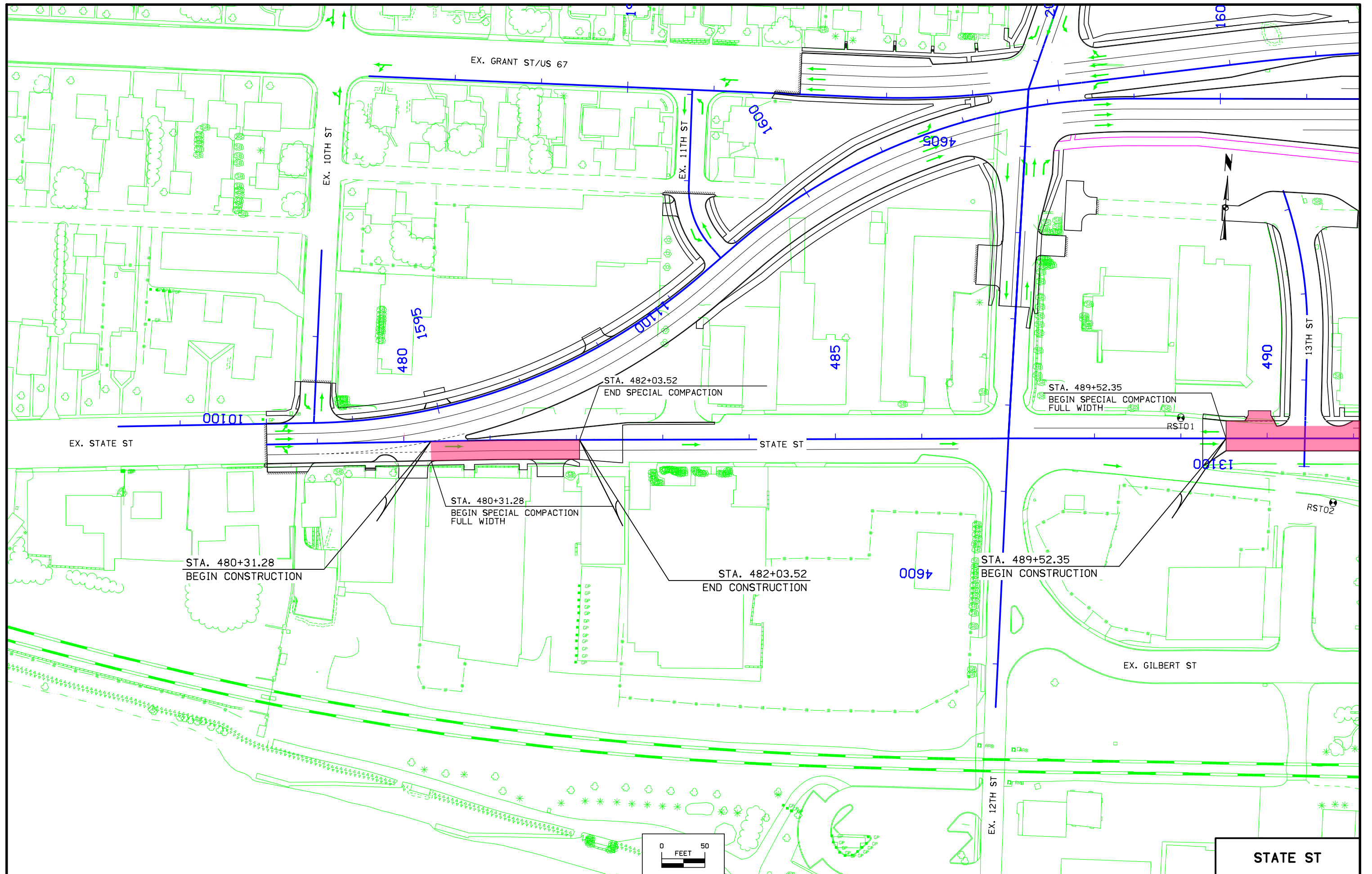
CUT MOISTURE 20.7  
 CUT DENSITY (lb/cu ft) -  
 PLASTIC LIMIT -

SHELBY TUBE CORE DATA	
CORE NO.	R6710-A
CLASSIFICATION [AASHTO]	A-6
COEFF. CONSOL. SQ.FT /DAY	-
TRIAxIAL COMPRESSION	-
COHESION - PSF	-
FRICTION COEFF.	-
MOISTURE CONTENT %	-
DRY DENSITY - PCF	-
UU-UNCONSOLIDATED & UNDRAINED	-
UC-CONSOLIDATED & UNDRAINED	-

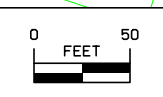


BLOW COUNT  
 NO. BLOWS  
 10  
 50/1'

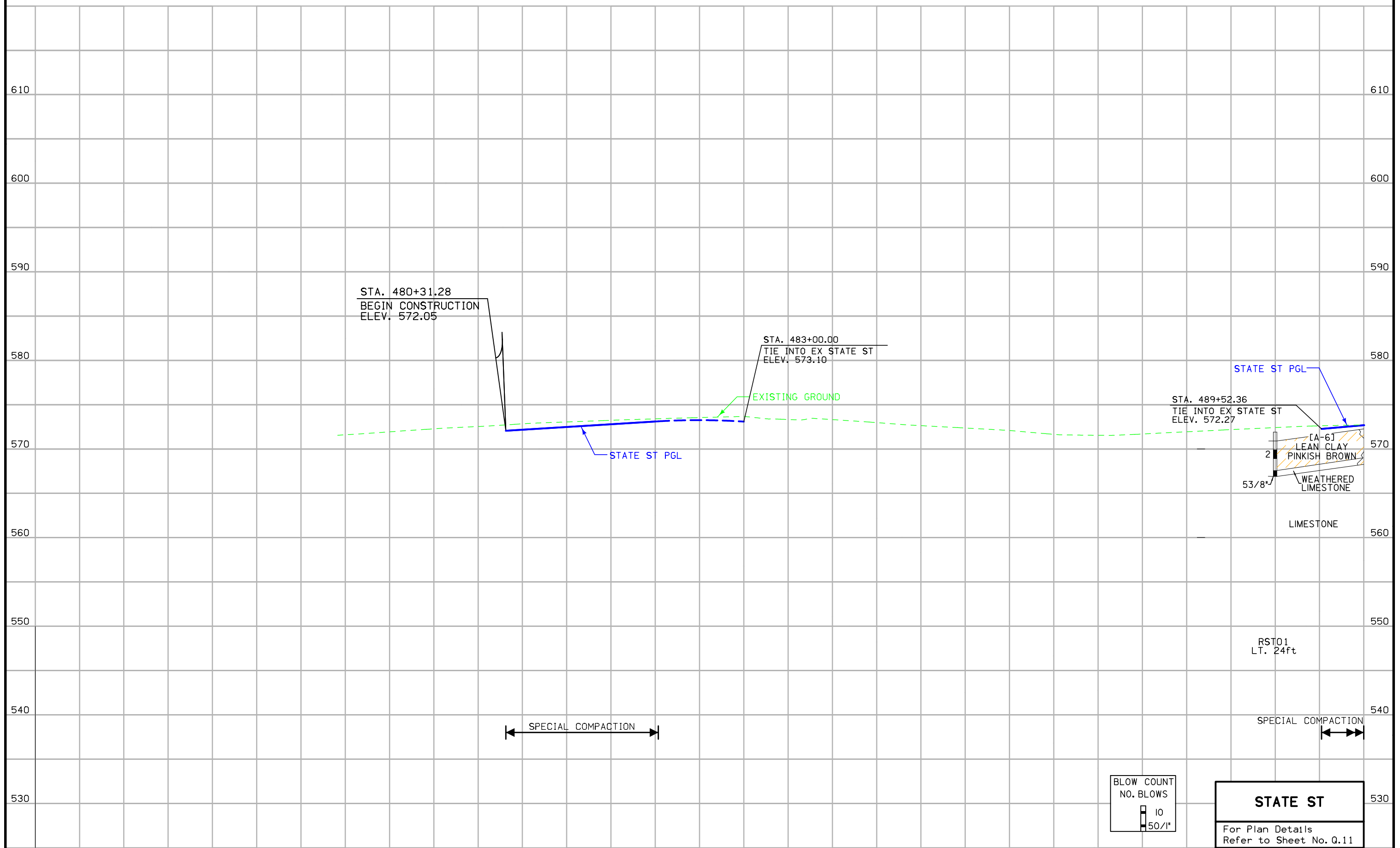
**US 67 NB**  
 For Plan Details  
 Refer to Sheet No. Q.9



STATE ST



CUT MOISTURE  
 CUT DENSITY (lb/cu ft)  
 PLASTIC LIMIT



STA. 480+31.28  
 BEGIN CONSTRUCTION  
 ELEV. 572.05

STA. 483+00.00  
 TIE INTO EX STATE ST  
 ELEV. 573.10

STA. 489+52.36  
 TIE INTO EX STATE ST  
 ELEV. 572.27

[A-6]  
 LEAN CLAY  
 PINKISH BROWN  
 WEATHERED LIMESTONE  
 53/8"

LIMESTONE

RST01  
 LT. 24ft

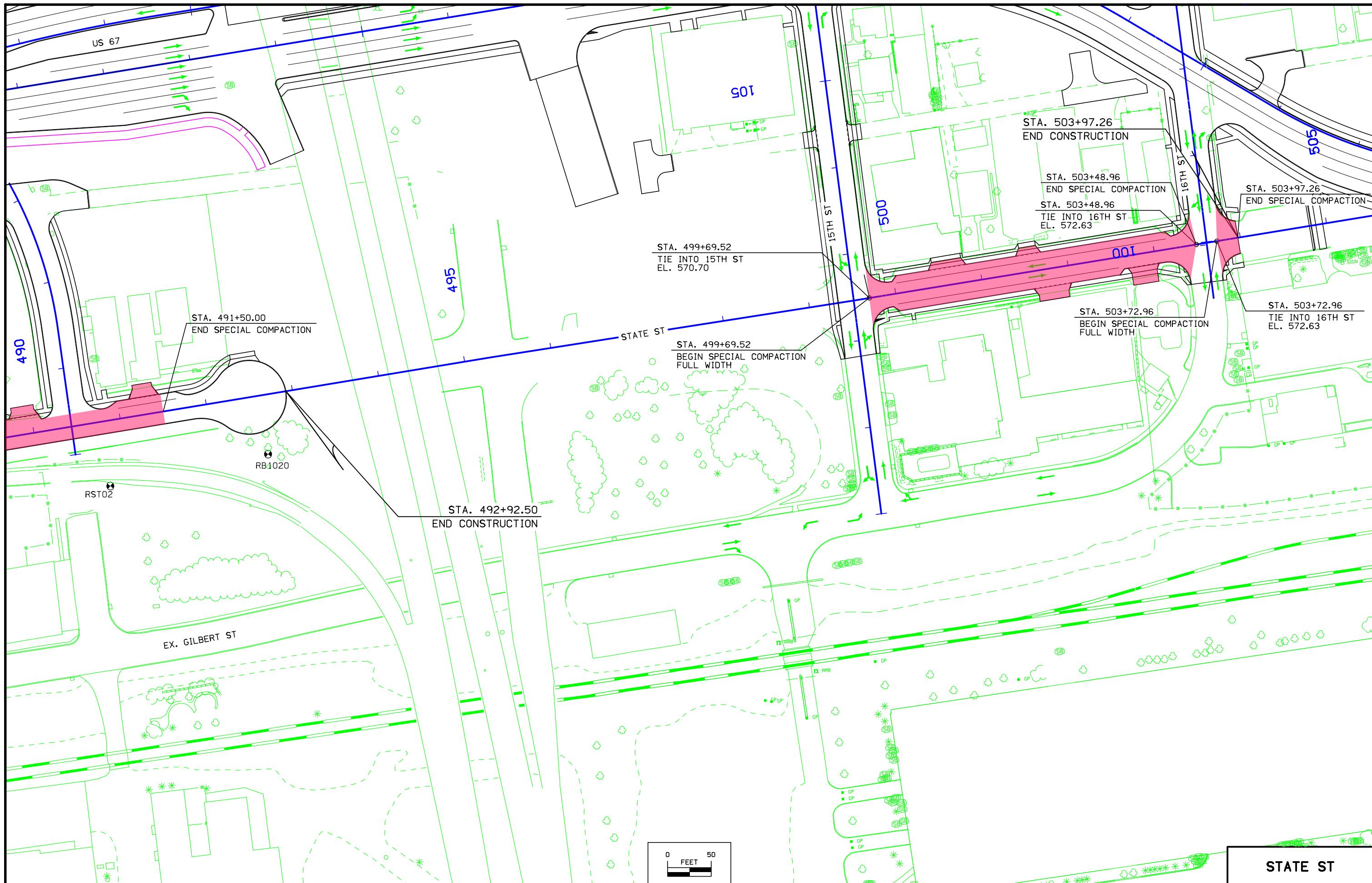
SPECIAL COMPACTION

SPECIAL COMPACTION

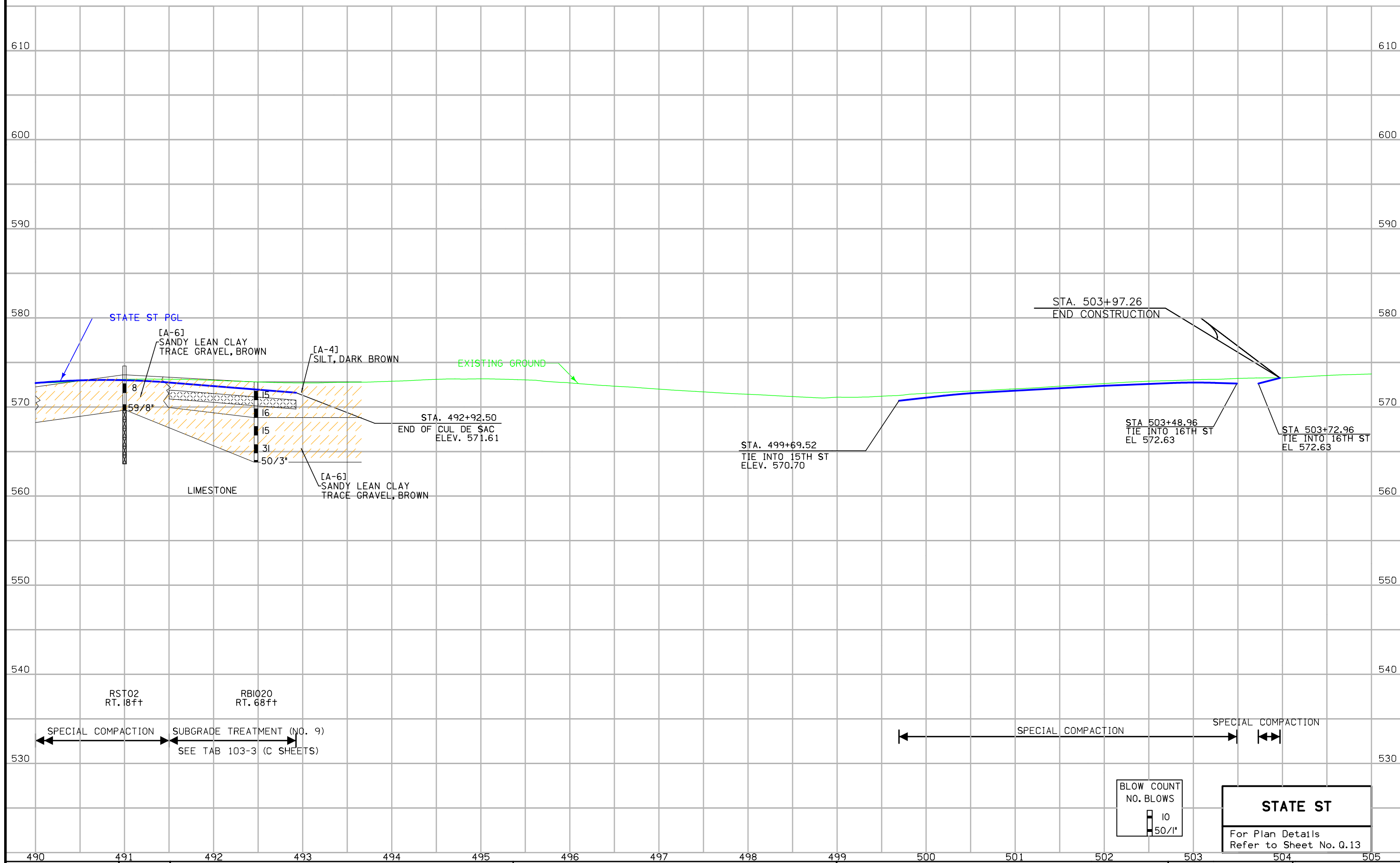
BLOW COUNT  
 NO. BLOWS  
 10  
 50/1'

**STATE ST**  
 For Plan Details  
 Refer to Sheet No. Q.11

- UNSUITABLE
- SUBGRADE TREATMENT
- SHALE
- ROCK
- SELECT SOIL
- SANDY SOIL
- DENS. CORE
- SELECT SAND
- SHELBY
- BLOW
- SAMPLE
- PLUGGED
- WATER
- MOISTURE

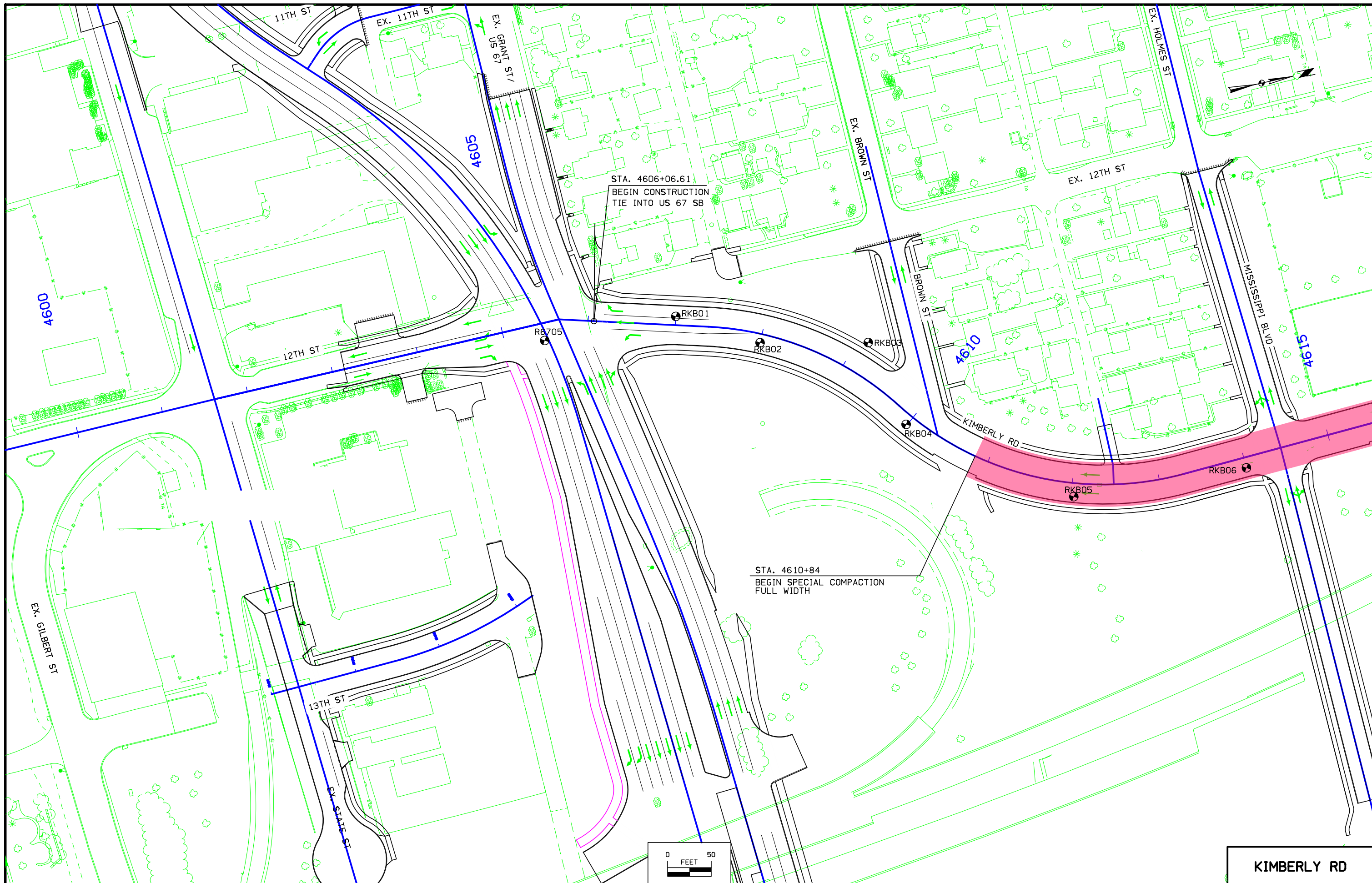


CUT MOISTURE 17.1  
 CUT DENSITY (lb/cu ft) 28.9  
 PLASTIC LIMIT 19 21

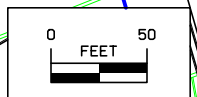


BLOW COUNT  
 NO. BLOWS  
 10  
 50/1'

**STATE ST**  
 For Plan Details  
 Refer to Sheet No. Q.13



**KIMBERLY RD**

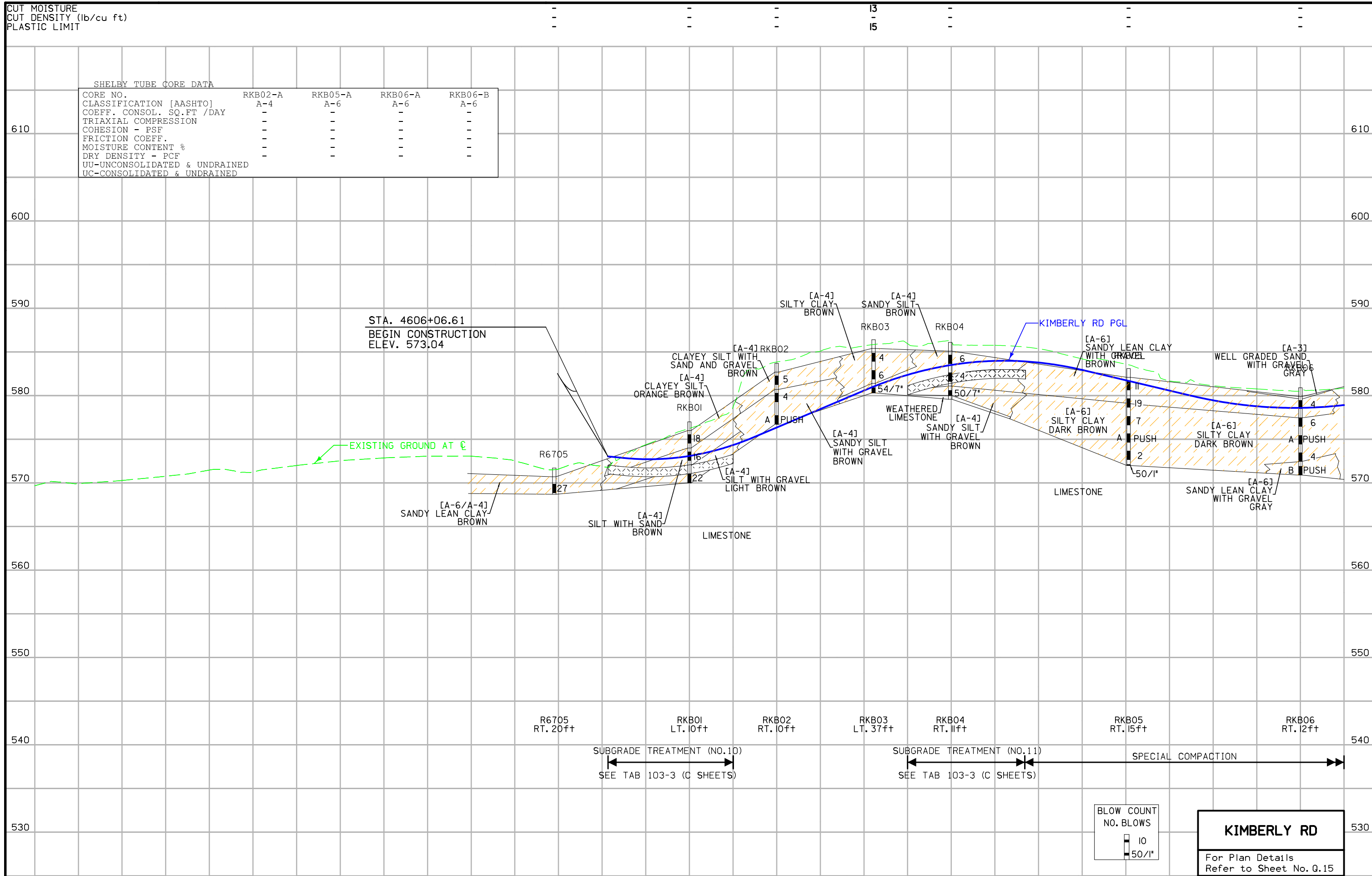


CUT MOISTURE  
 CUT DENSITY (lb/cu ft)  
 PLASTIC LIMIT

---  
 ---  
 ---  
 13  
 ---  
 ---  
 ---  
 15  
 ---  
 ---  
 ---

SHELBY TUBE CORE DATA

CORE NO.	RKB02-A	RKB05-A	RKB06-A	RKB06-B
CLASSIFICATION [AASHTO]	A-4	A-6	A-6	A-6
COEFF. CONSOL. SQ.FT /DAY	-	-	-	-
TRIAxIAL COMPRESSION	-	-	-	-
COHESION - PSF	-	-	-	-
FRICITION COEFF.	-	-	-	-
MOISTURE CONTENT %	-	-	-	-
DRY DENSITY - PCF	-	-	-	-
UU-UNCONSOLIDATED & UNDRAINED	-	-	-	-
UC-CONSOLIDATED & UNDRAINED	-	-	-	-



STA. 4606+06.61  
 BEGIN CONSTRUCTION  
 ELEV. 573.04

KIMBERLY RD PGL

EXISTING GROUND AT Q

R6705  
 RT. 20ft

RKB01  
 LT. 10ft

RKB02  
 RT. 10ft

RKB03  
 LT. 37ft

RKB04  
 RT. 11ft

RKB05  
 RT. 15ft

RKB06  
 RT. 12ft

SUBGRADE TREATMENT (NO.10)  
 SEE TAB 103-3 (C SHEETS)

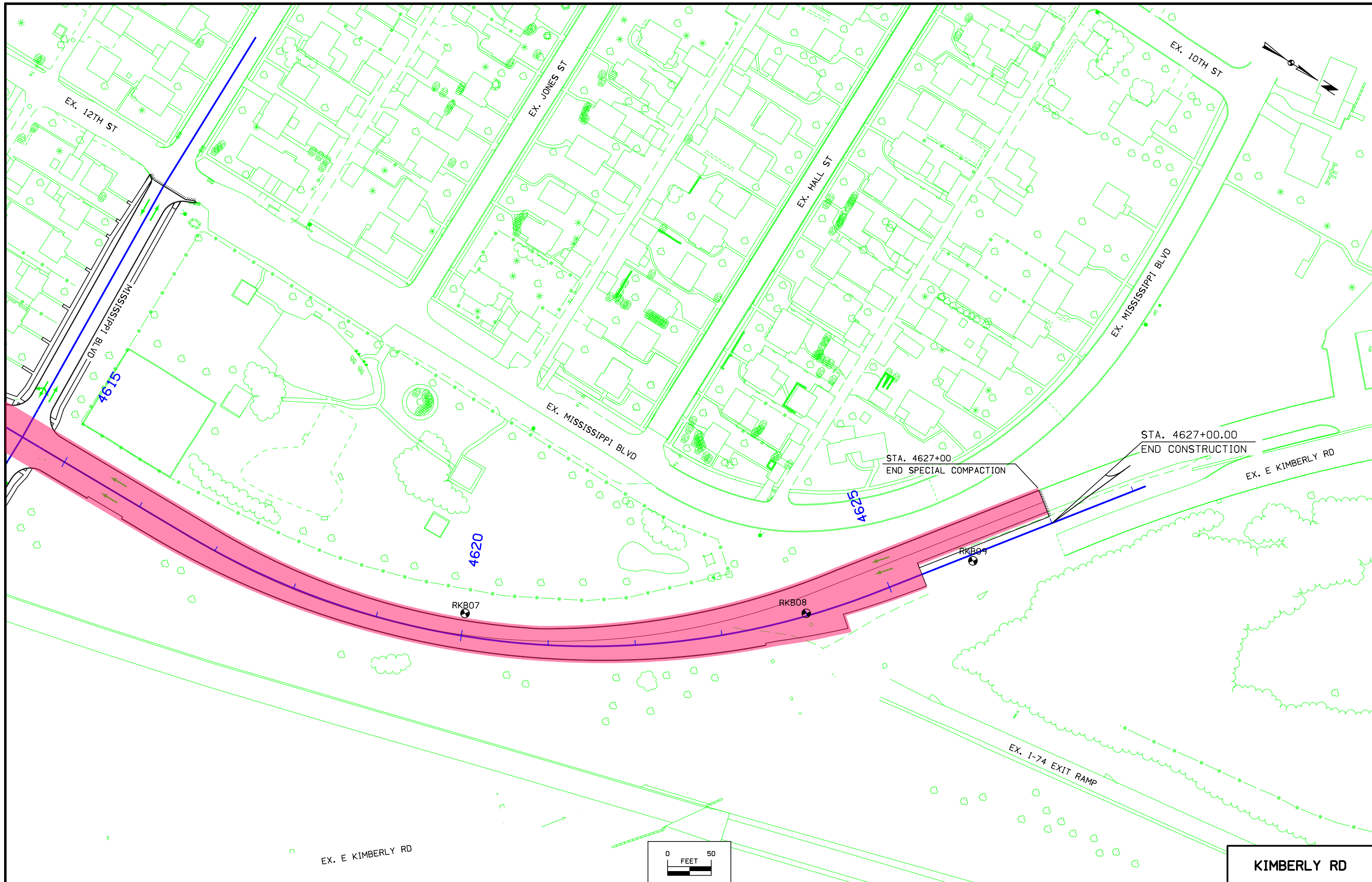
SUBGRADE TREATMENT (NO.11)  
 SEE TAB 103-3 (C SHEETS)

SPECIAL COMPACTION

BLOW COUNT  
 NO. BLOWS  
 10  
 50/1'

**KIMBERLY RD**  
 For Plan Details  
 Refer to Sheet No. Q.15

- UNDESIRABLE
- SUBGRADE TREATMENT
- SHALE
- ROCK
- SELECT SOIL
- SANDY SOIL
- DENS. CORE
- SELECT SAND
- SHELBY
- BLOW
- SAMPLE
- PLUGGED
- WATER
- MOISTURE

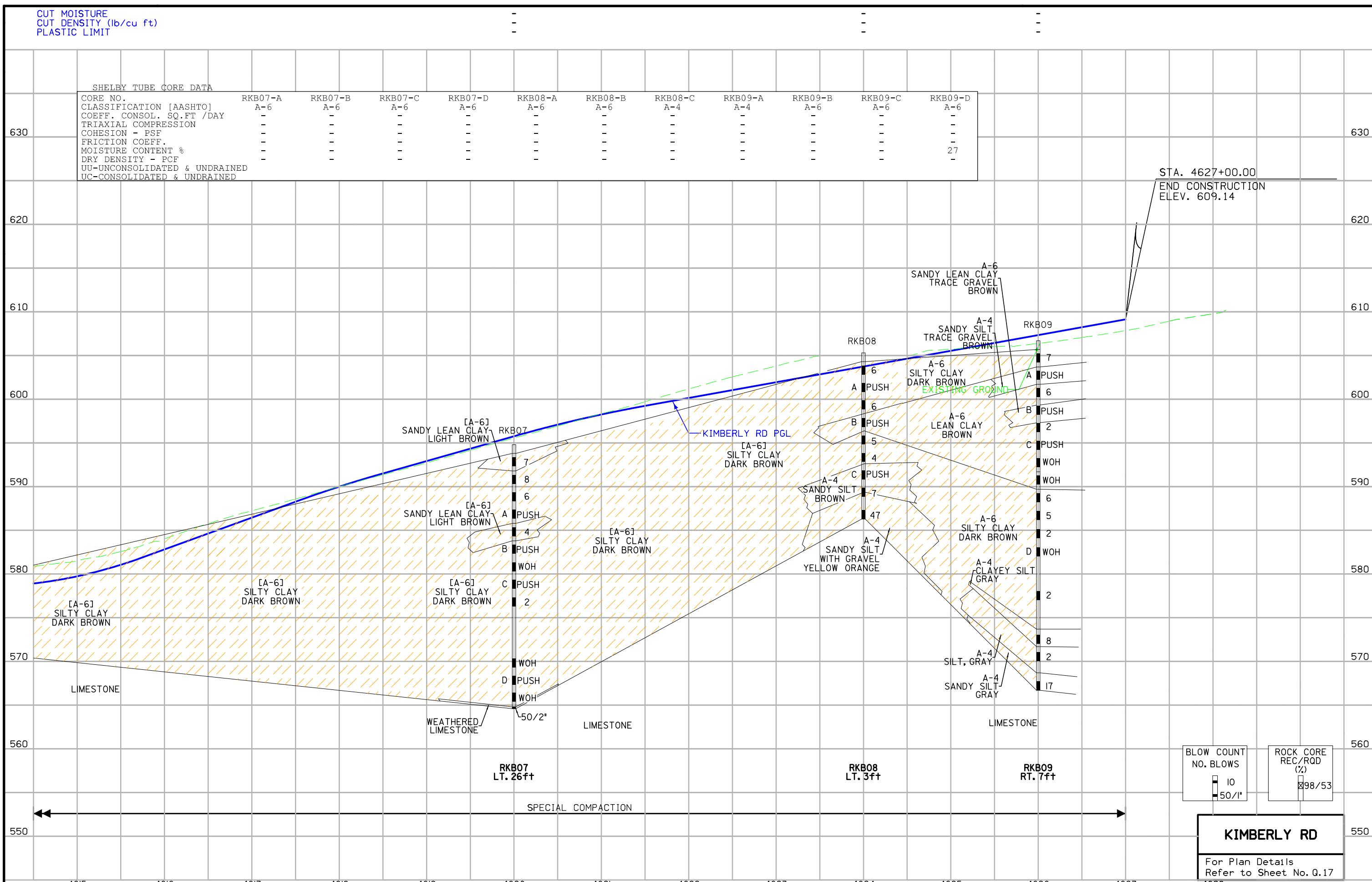




CUT MOISTURE  
CUT DENSITY (lb/cu ft)  
PLASTIC LIMIT

SHELBY TUBE CORE DATA

CORE NO.	RKB07-A	RKB07-B	RKB07-C	RKB07-D	RKB08-A	RKB08-B	RKB08-C	RKB09-A	RKB09-B	RKB09-C	RKB09-D
CLASSIFICATION [AASHTO]	A-6	A-6	A-6	A-6	A-6	A-6	A-4	A-4	A-6	A-6	A-6
COEFF. CONSOL. SQ.FT /DAY	-	-	-	-	-	-	-	-	-	-	-
TRIAxIAL COMPRESSION	-	-	-	-	-	-	-	-	-	-	-
COHESION - PSF	-	-	-	-	-	-	-	-	-	-	-
FRICITION COEFF.	-	-	-	-	-	-	-	-	-	-	-
MOISTURE CONTENT %	-	-	-	-	-	-	-	-	-	-	27
DRY DENSITY - PCF	-	-	-	-	-	-	-	-	-	-	-
UU-UNCONSOLIDATED & UNDRAINED	-	-	-	-	-	-	-	-	-	-	-
UC-CONSOLIDATED & UNDRAINED	-	-	-	-	-	-	-	-	-	-	-



STA. 4627+00.00  
END CONSTRUCTION  
ELEV. 609.14

RKB07  
LT. 26ft

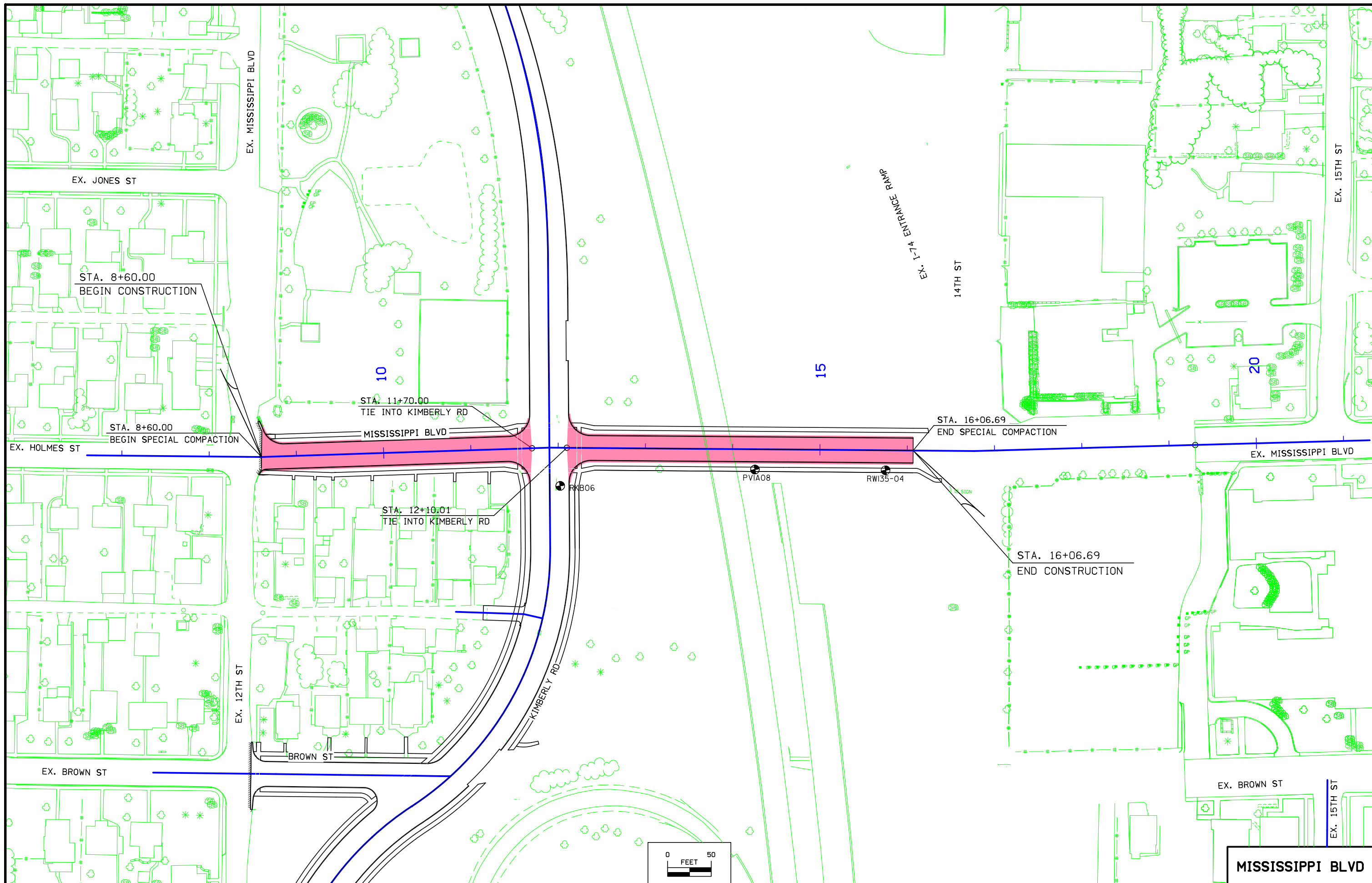
RKB08  
LT. 3ft

RKB09  
RT. 7ft

BLOW COUNT  
NO. BLOWS  
10  
50/1'

ROCK CORE  
REC/RQD (%)  
98/53

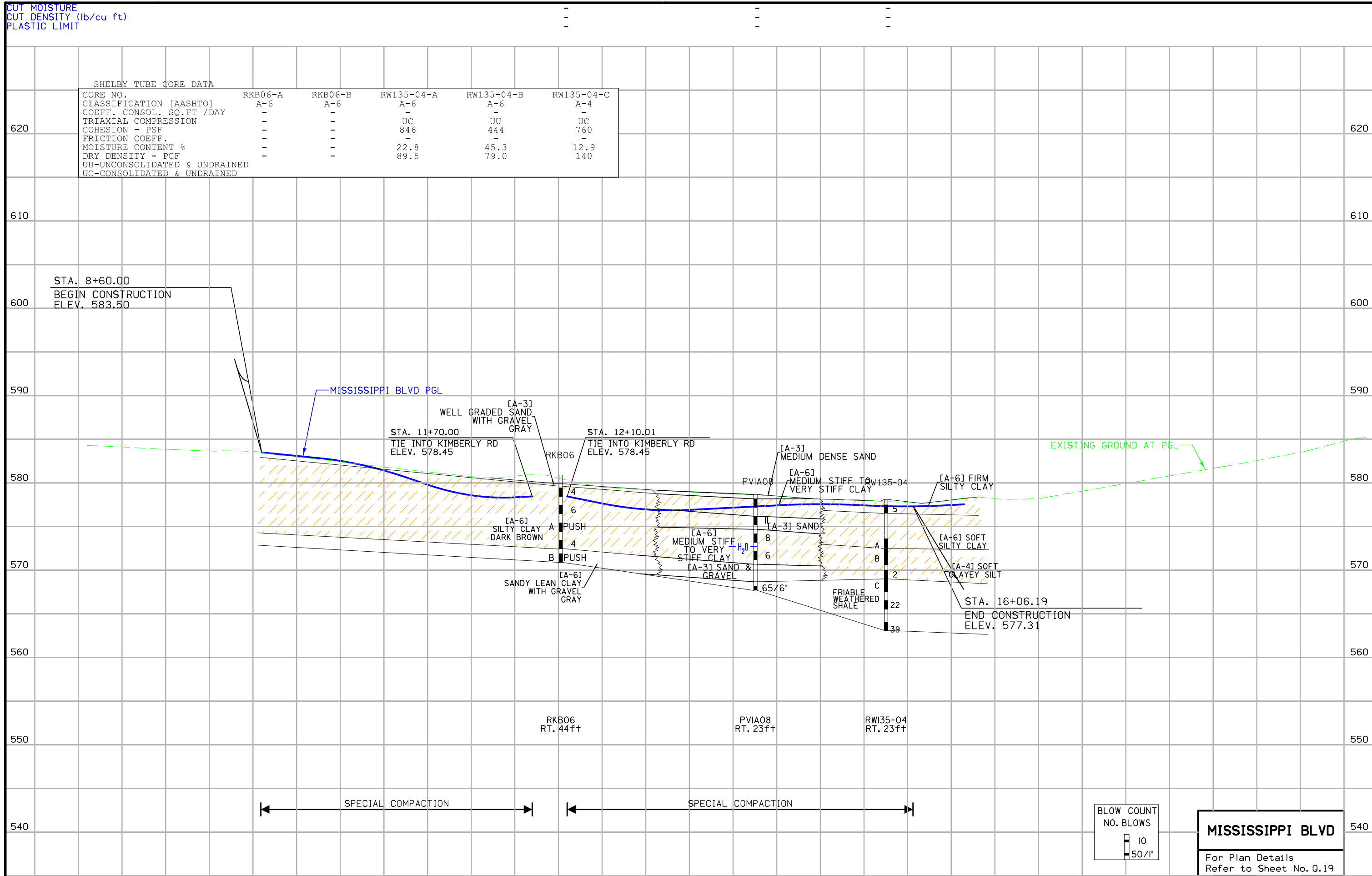
**KIMBERLY RD**  
For Plan Details  
Refer to Sheet No. Q.17



CUT MOISTURE  
 CUT DENSITY (lb/cu ft)  
 PLASTIC LIMIT

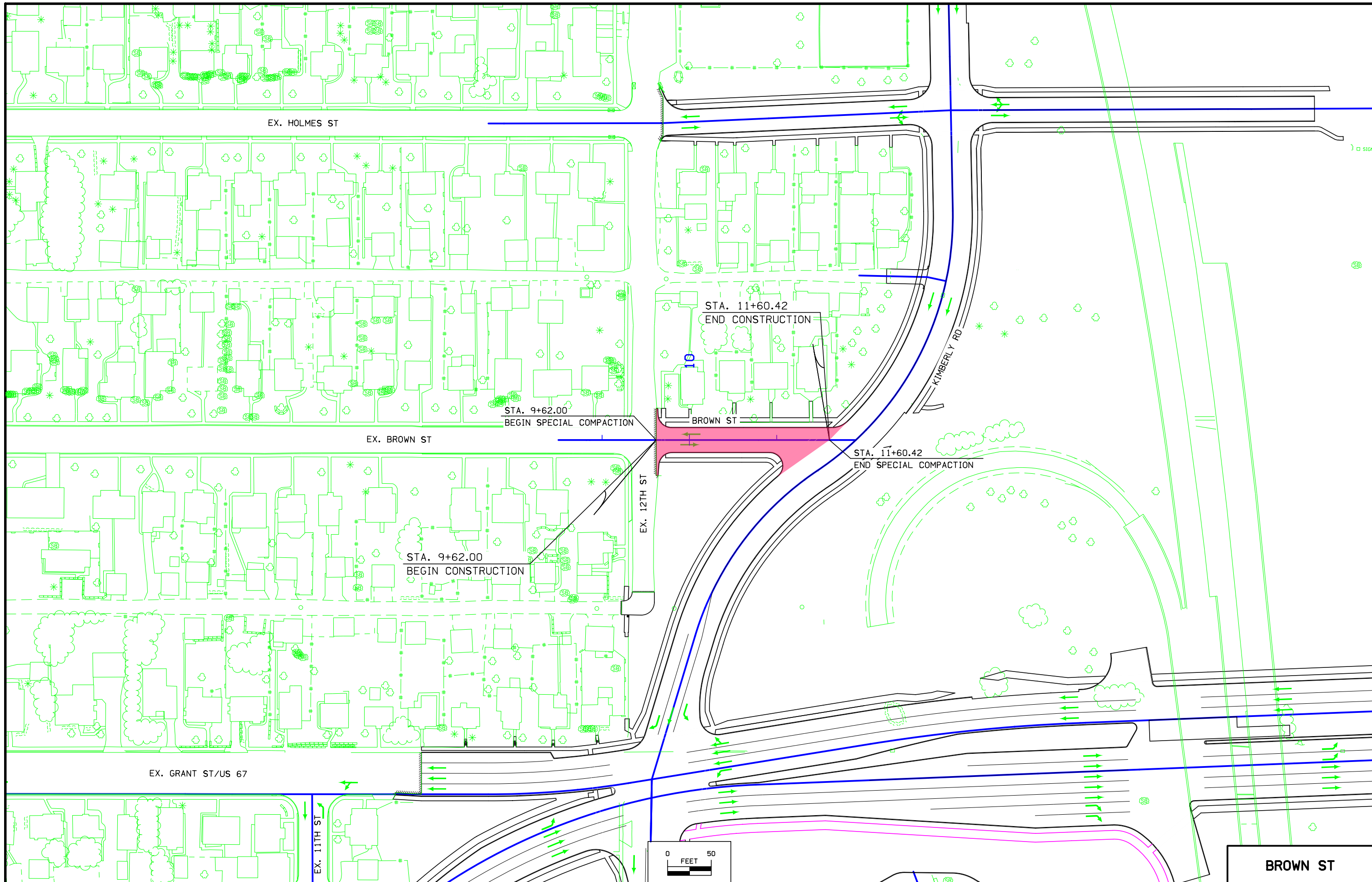
SHELBY TUBE CORE DATA

CORE NO.	RKB06-A	RKB06-B	RW135-04-A	RW135-04-B	RW135-04-C
CLASSIFICATION [AASHTO]	A-6	A-6	A-6	A-6	A-4
COEFF. CONSOL. SQ.FT /DAY	-	-	-	-	-
TRIAxIAL COMPRESSION	-	-	UC	UU	UC
COHESION - PSF	-	-	846	444	760
FRICITION COEFF.	-	-	-	-	-
MOISTURE CONTENT %	-	-	22.8	45.3	12.9
DRY DENSITY - PCF	-	-	89.5	79.0	140
UU-UNCONSOLIDATED & UNDRAINED	-	-	-	-	-
UC-CONSOLIDATED & UNDRAINED	-	-	-	-	-

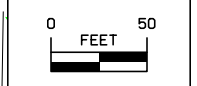


BLOW COUNT  
 NO. BLOWS  
 10  
 50/1'

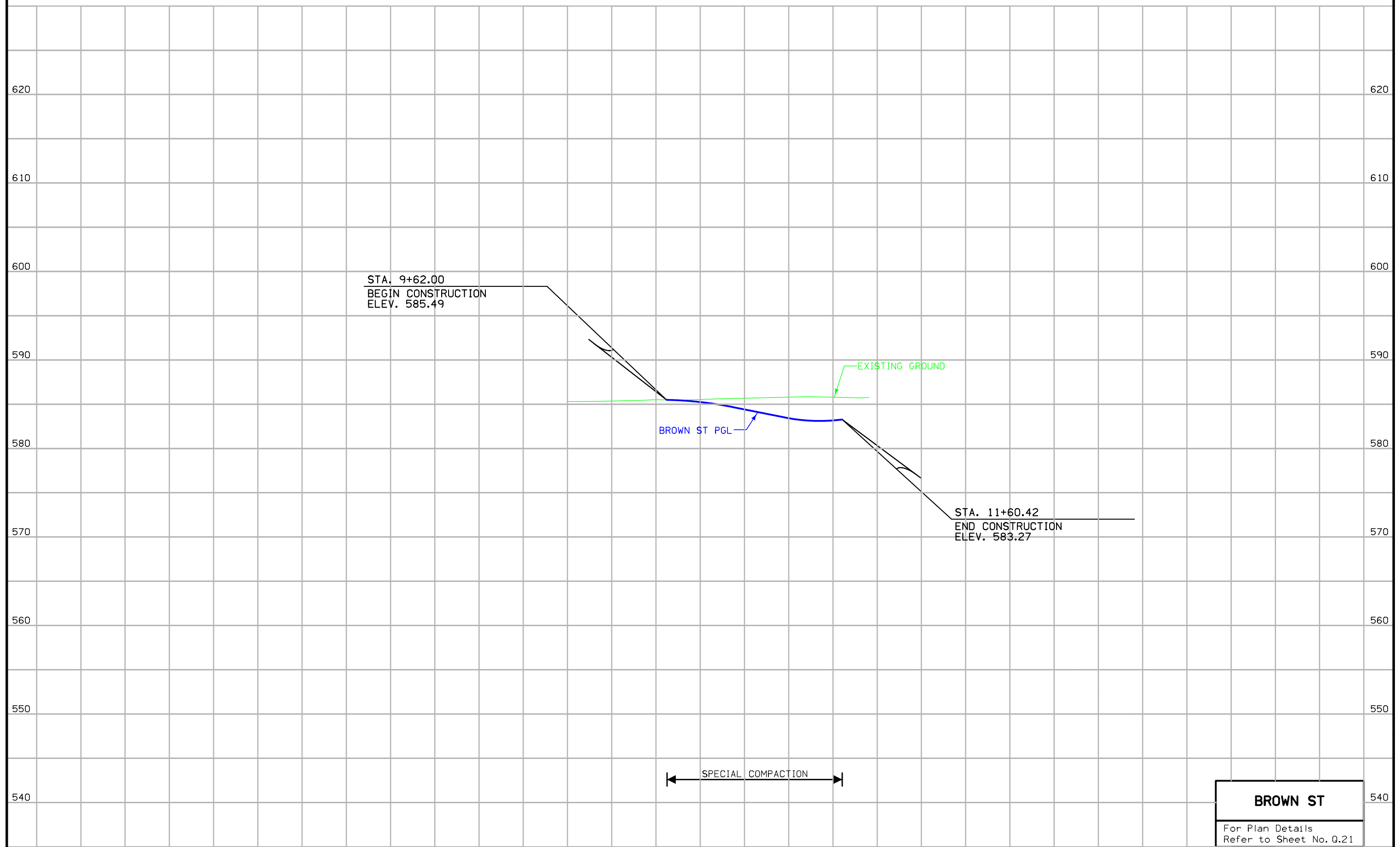
**MISSISSIPPI BLVD**  
 For Plan Details  
 Refer to Sheet No. Q.19



**BROWN ST**



CUT MOISTURE  
CUT DENSITY (lb/cu ft)  
PLASTIC LIMIT



STA. 9+62.00  
BEGIN CONSTRUCTION  
ELEV. 585.49

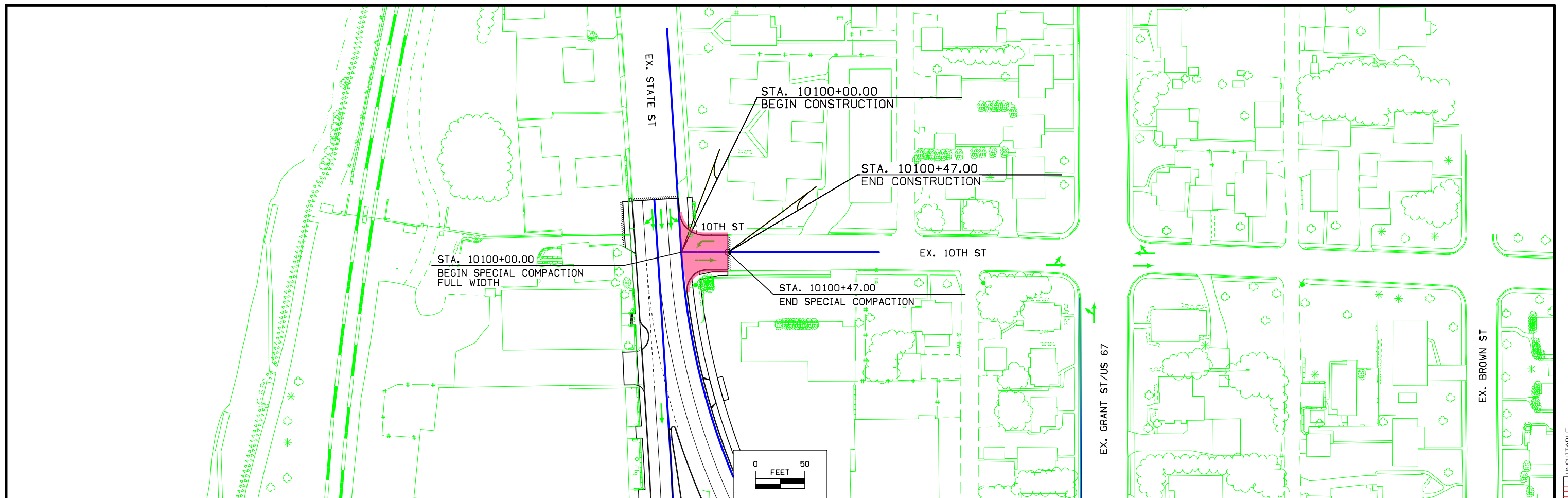
BROWN ST PGL

EXISTING GROUND

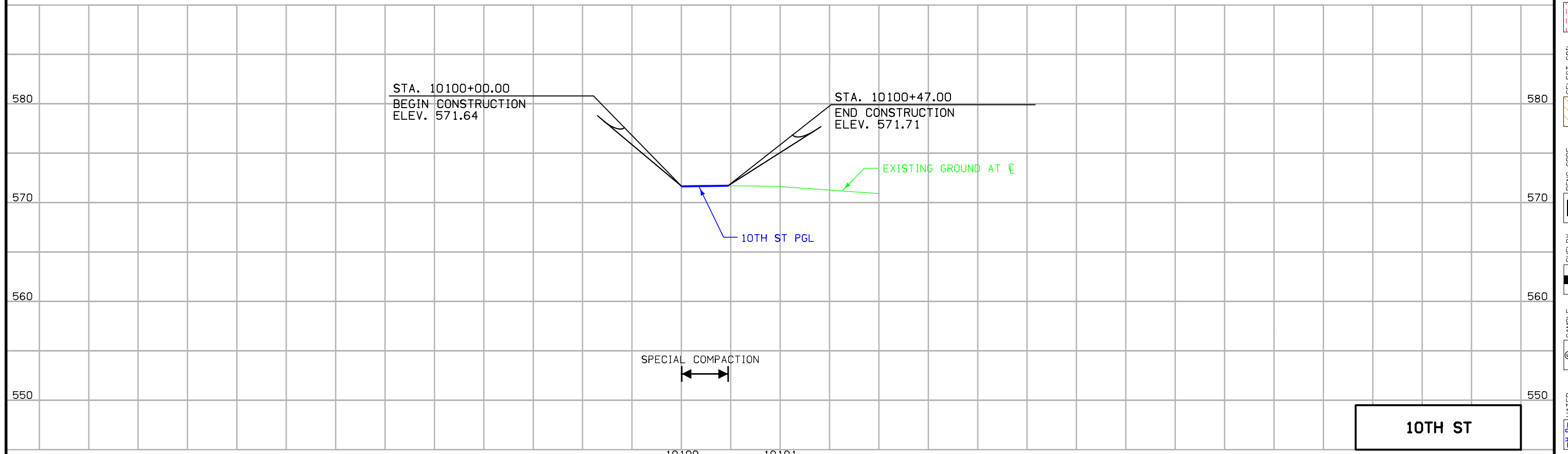
STA. 11+60.42  
END CONSTRUCTION  
ELEV. 583.27

SPECIAL COMPACTION

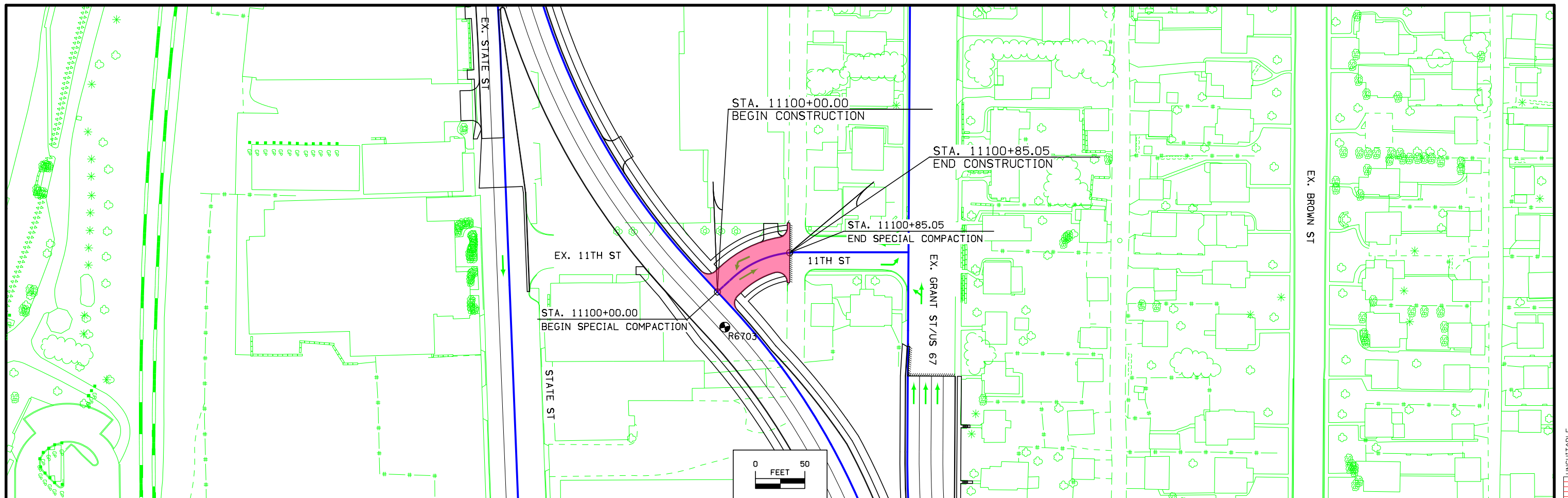
**BROWN ST**  
For Plan Details  
Refer to Sheet No. Q.21



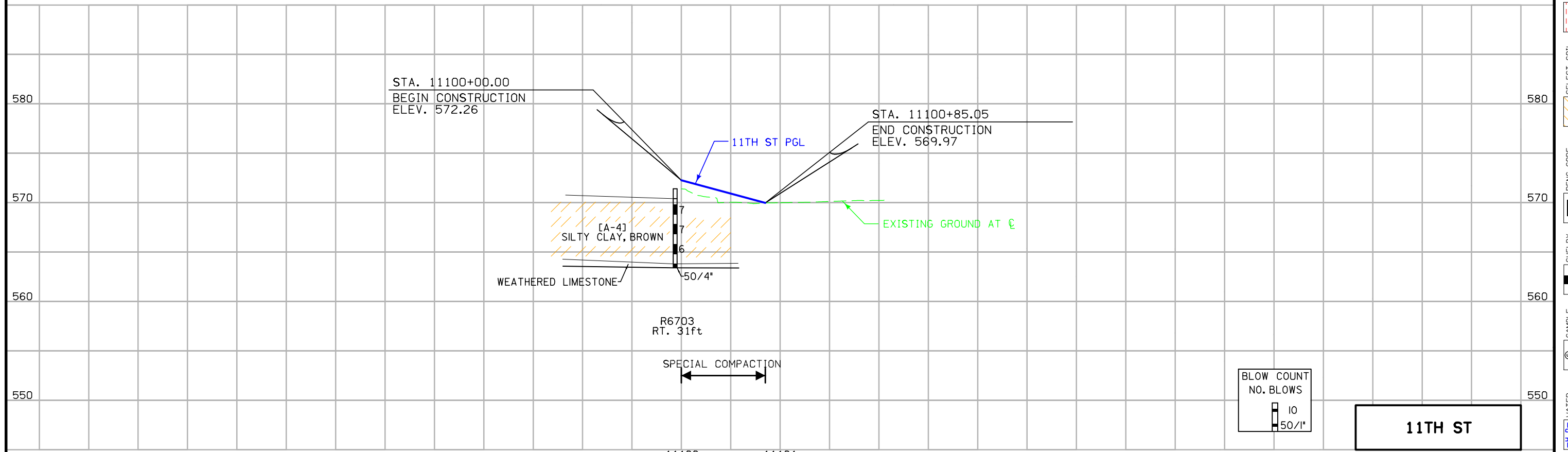
CUT MOISTURE  
 CUT DENSITY (lb/cu ft)  
 PLASTIC LIMIT



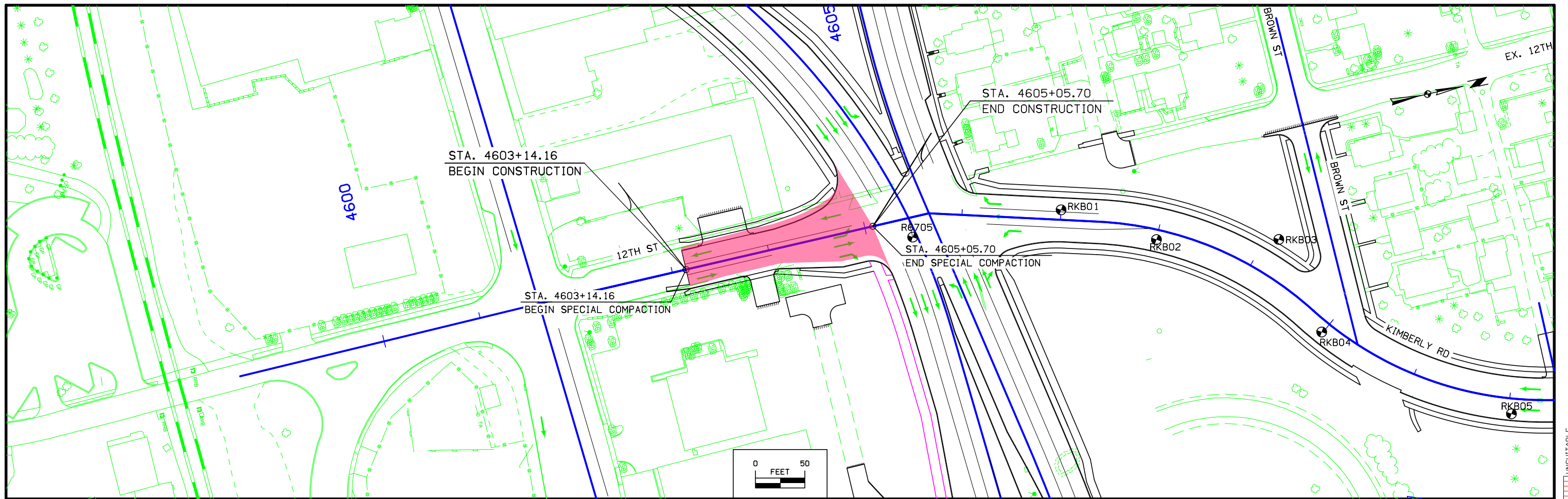
**10TH ST**



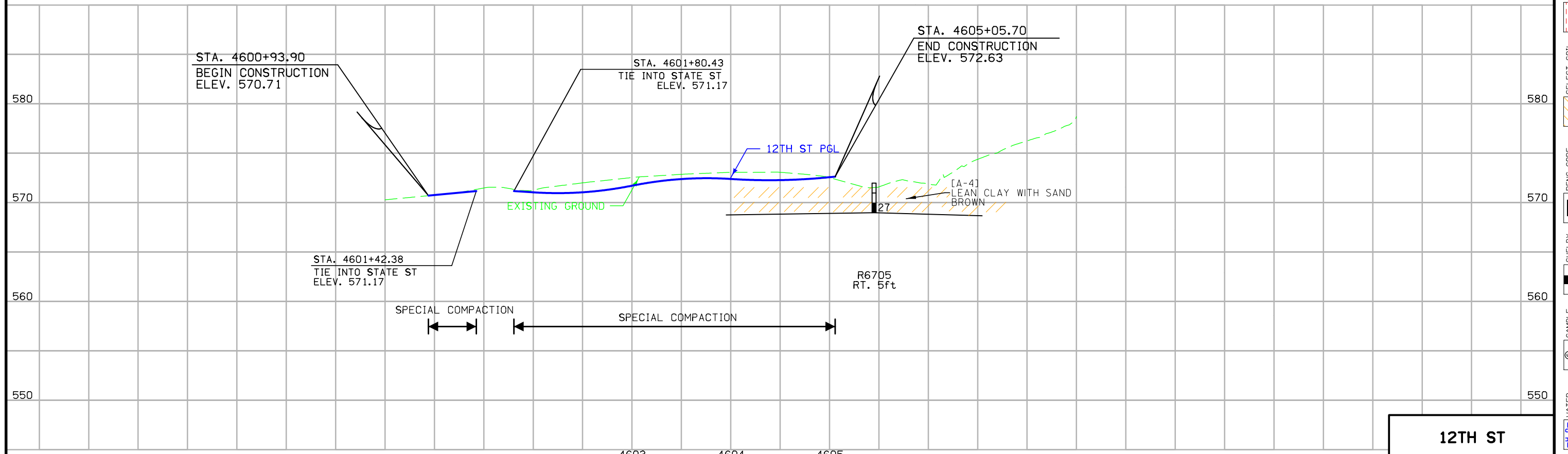
CUT MOISTURE  
 CUT DENSITY (lb/cu ft)  
 PLASTIC LIMIT



- UNSATURABLE
- SUBGRADE TREATMENT
- SHALE
- ROCK
- SELECT SOIL
- SANDY SOIL
- DENS. CORE
- SELECT SAND
- SHELBY
- BLOW
- SAMPLE
- PLUGGED
- WATER
- MOISTURE

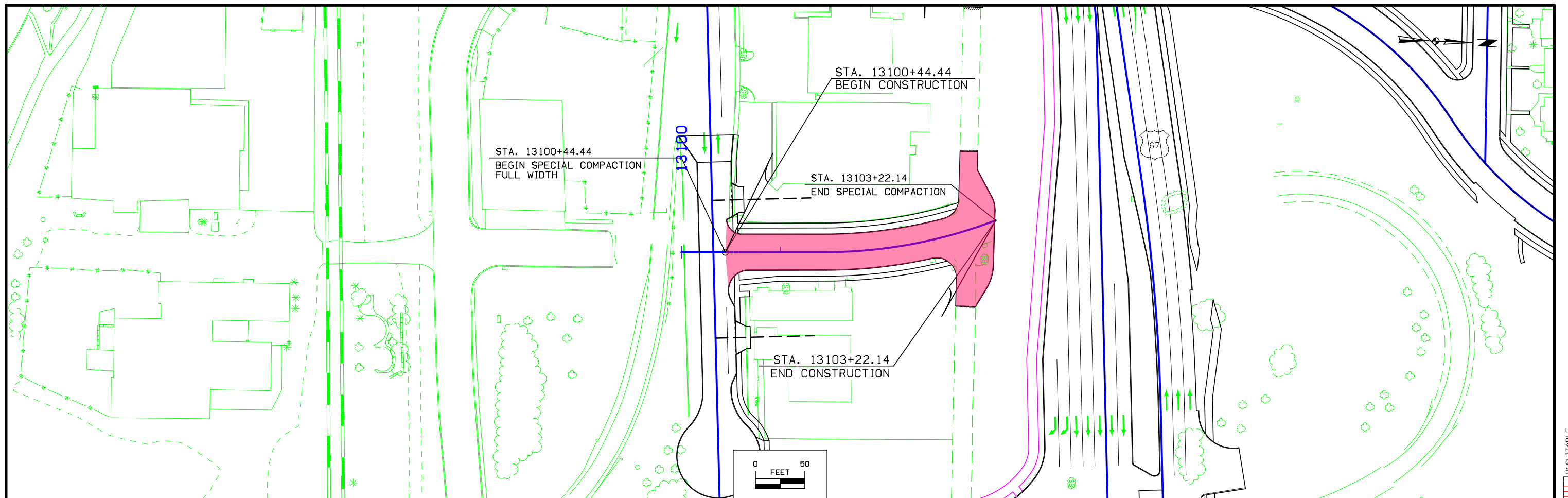


CUT MOISTURE  
 CUT DENSITY (lb/cu ft)  
 PLASTIC LIMIT

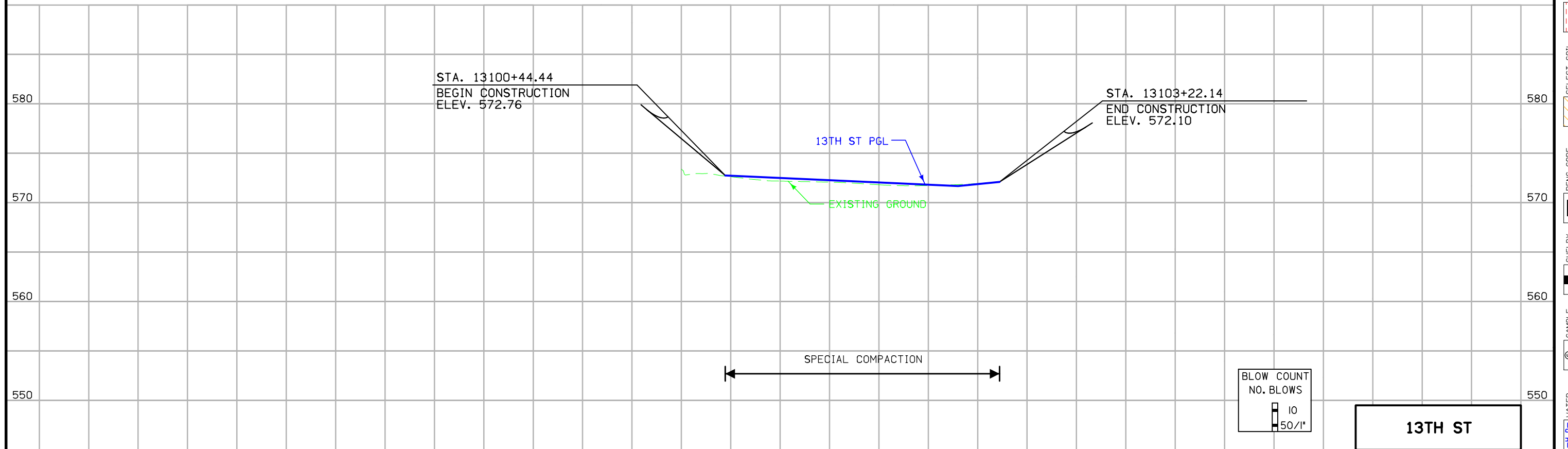


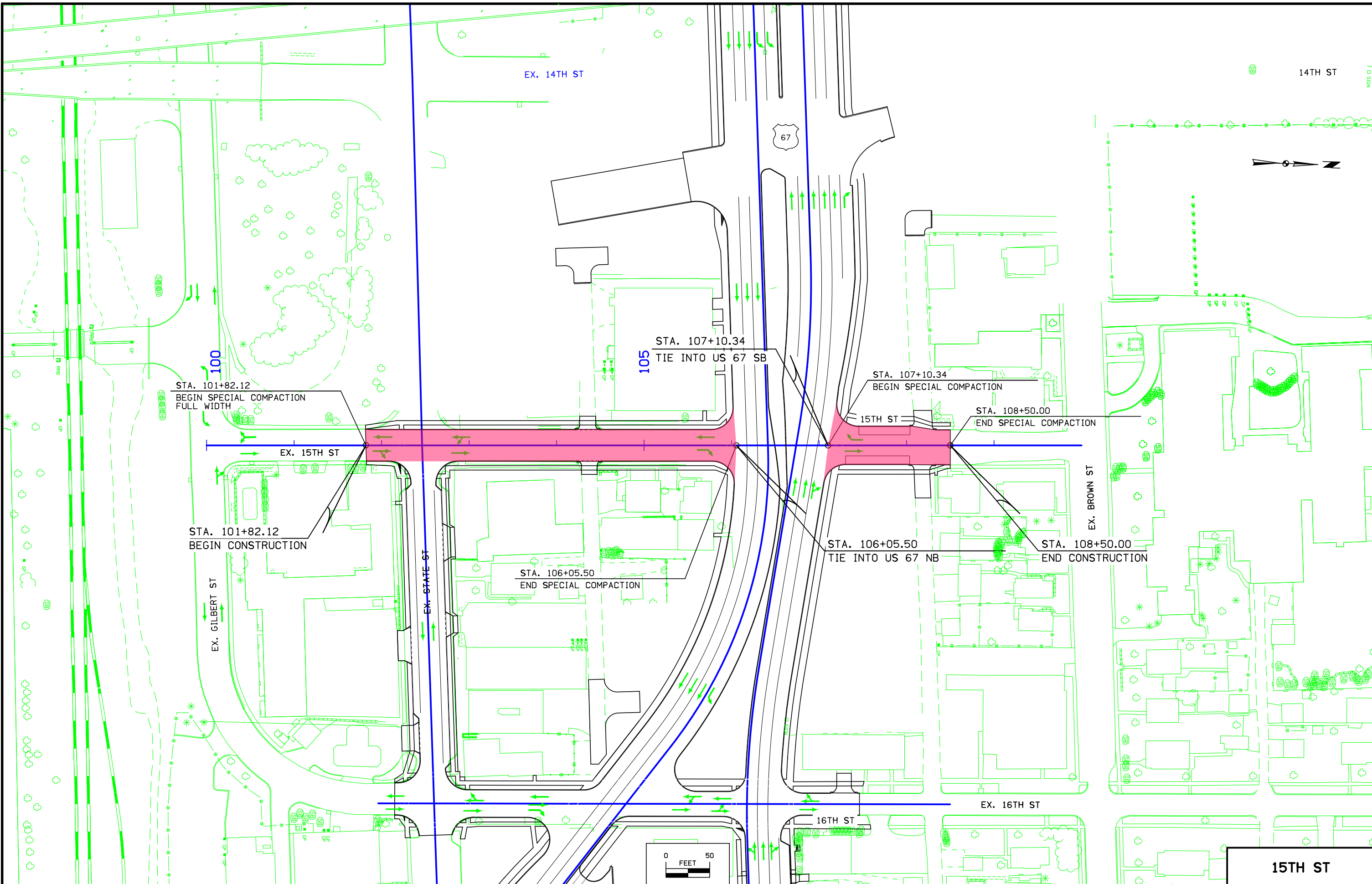
**12TH ST**



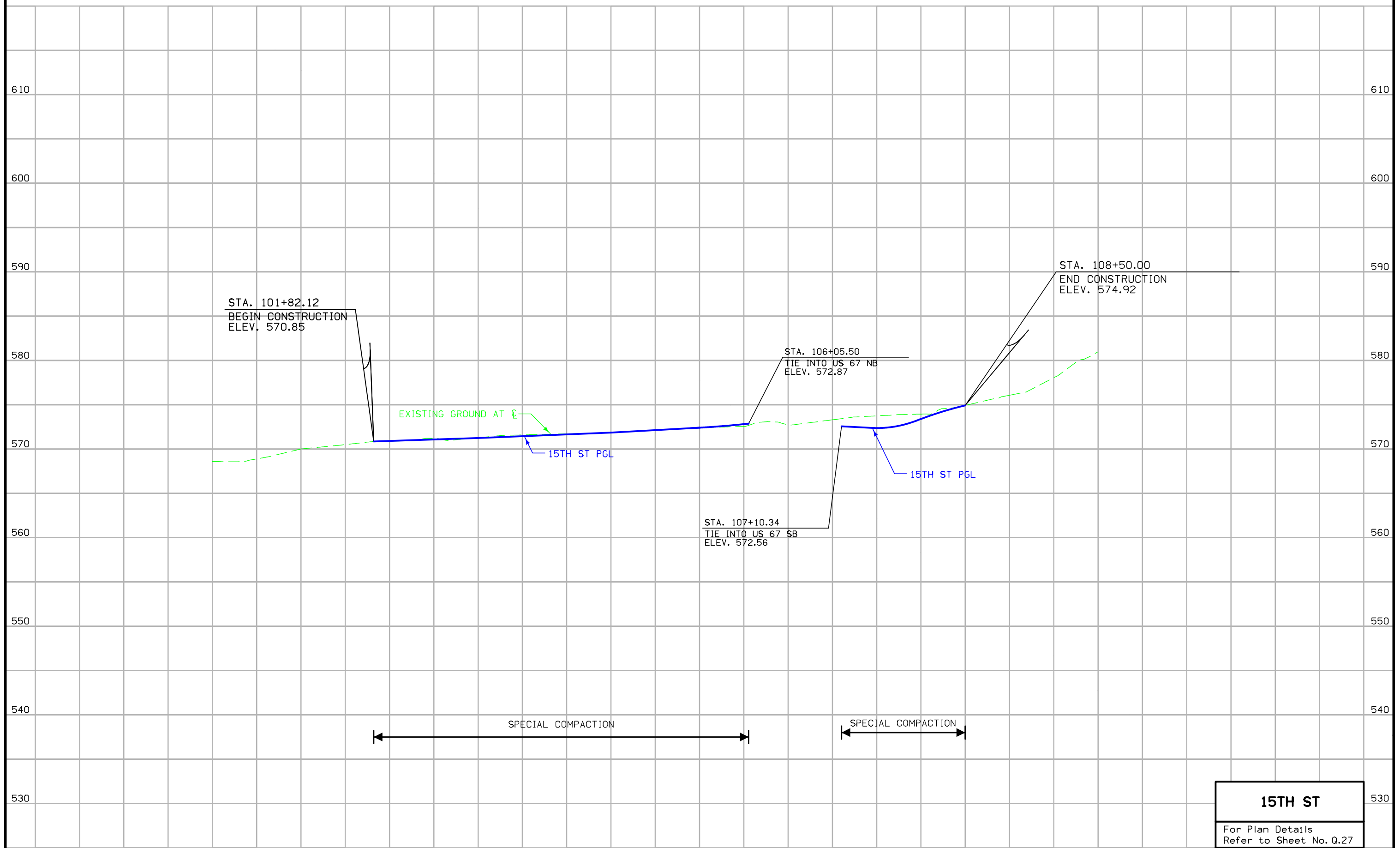


CUT MOISTURE  
 CUT DENSITY (lb/cu ft)  
 PLASTIC LIMIT

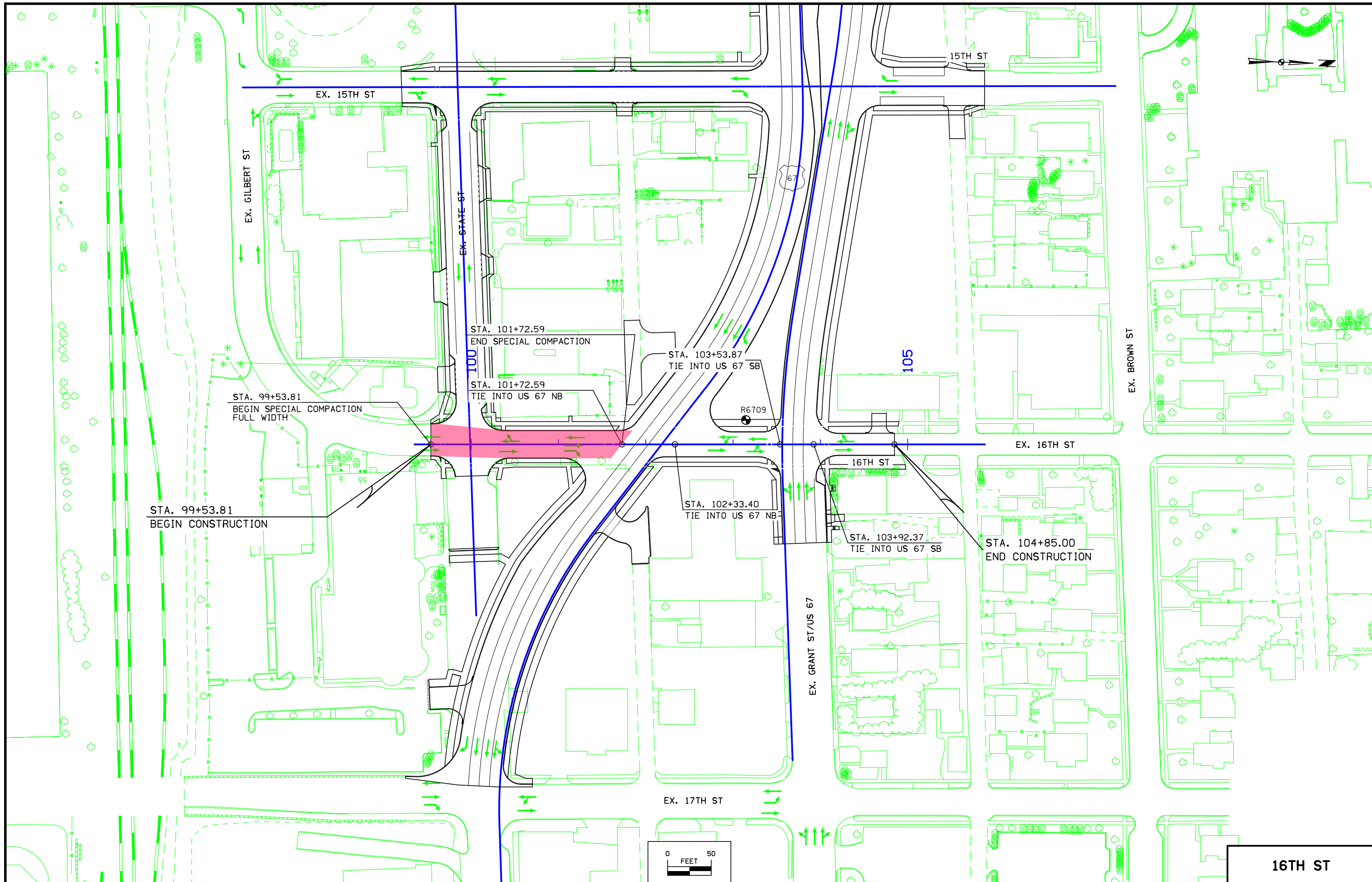




CUT MOISTURE  
 CUT DENSITY (lb/cu ft.)  
 PLASTIC LIMIT

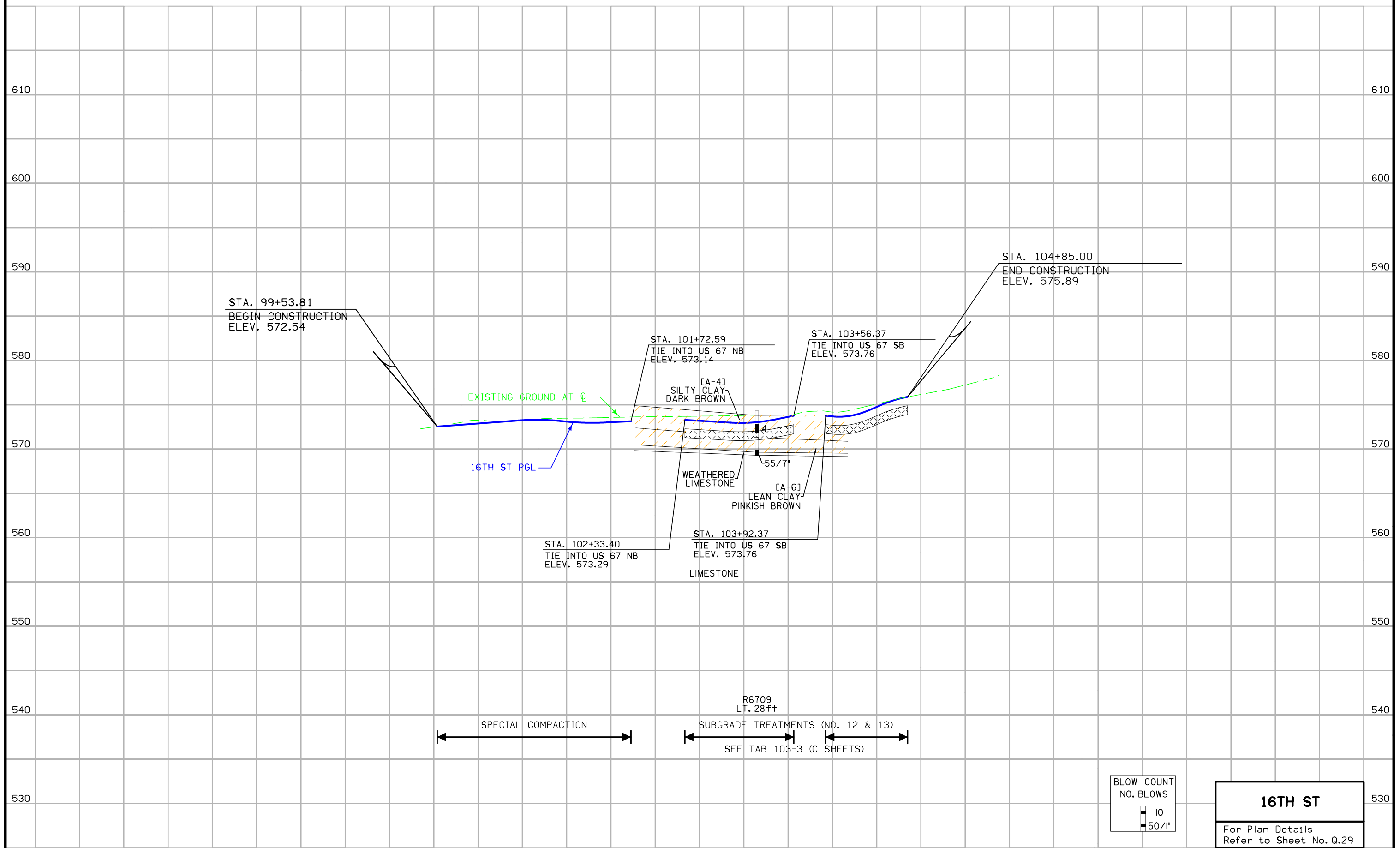


- UNSUITABLE
- SUBGRADE TREATMENT
- SHALE
- ROCK
- SELECT SOIL
- SANDY SOIL
- DENS. CORE
- SELECT SAND
- SHELBY
- BLOW
- SAMPLE
- PLUGGED
- MOISTURE

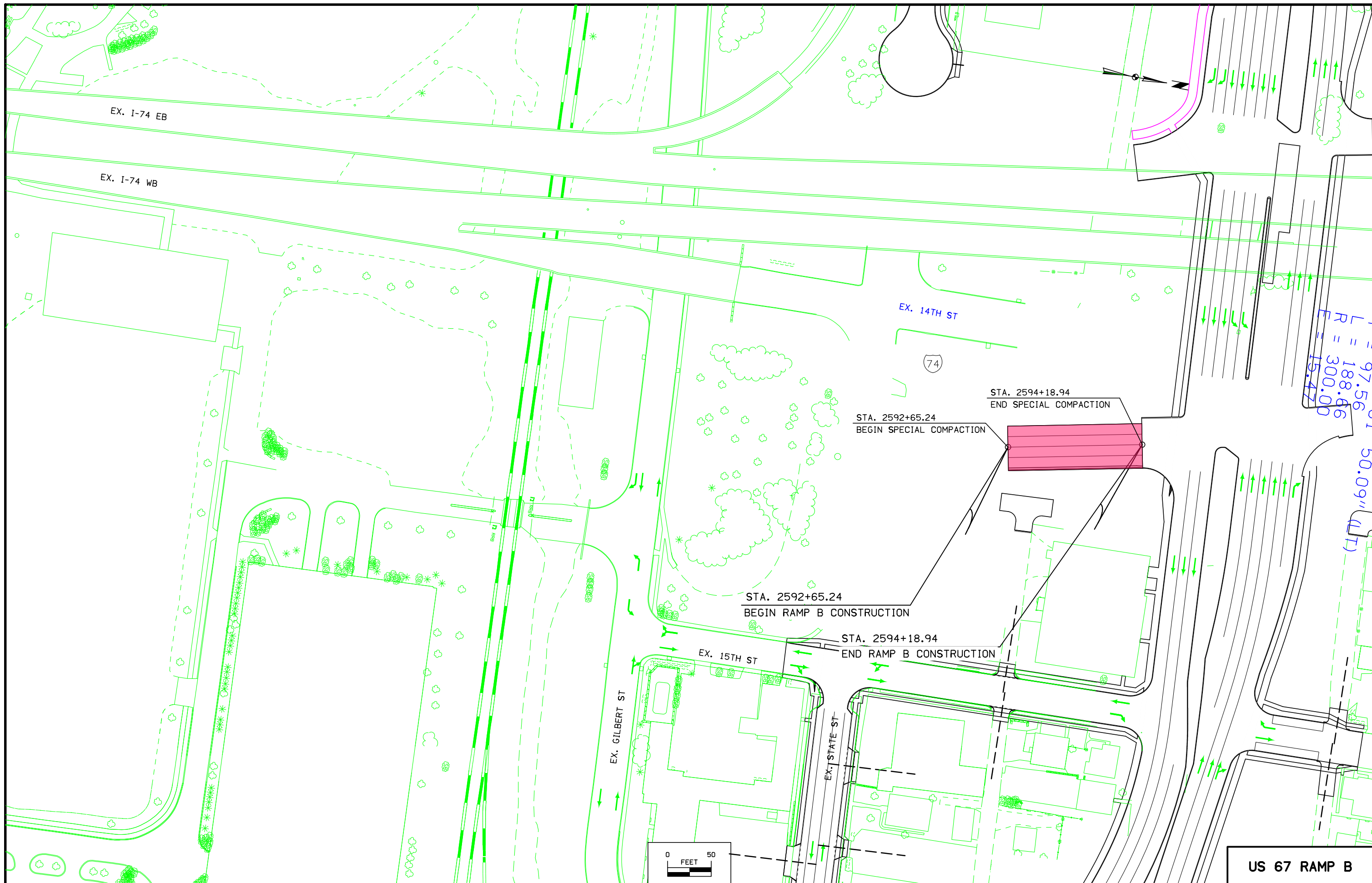


16TH ST

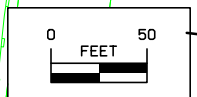
CUT MOISTURE  
 CUT DENSITY (lb/cu ft.)  
 PLASTIC LIMIT



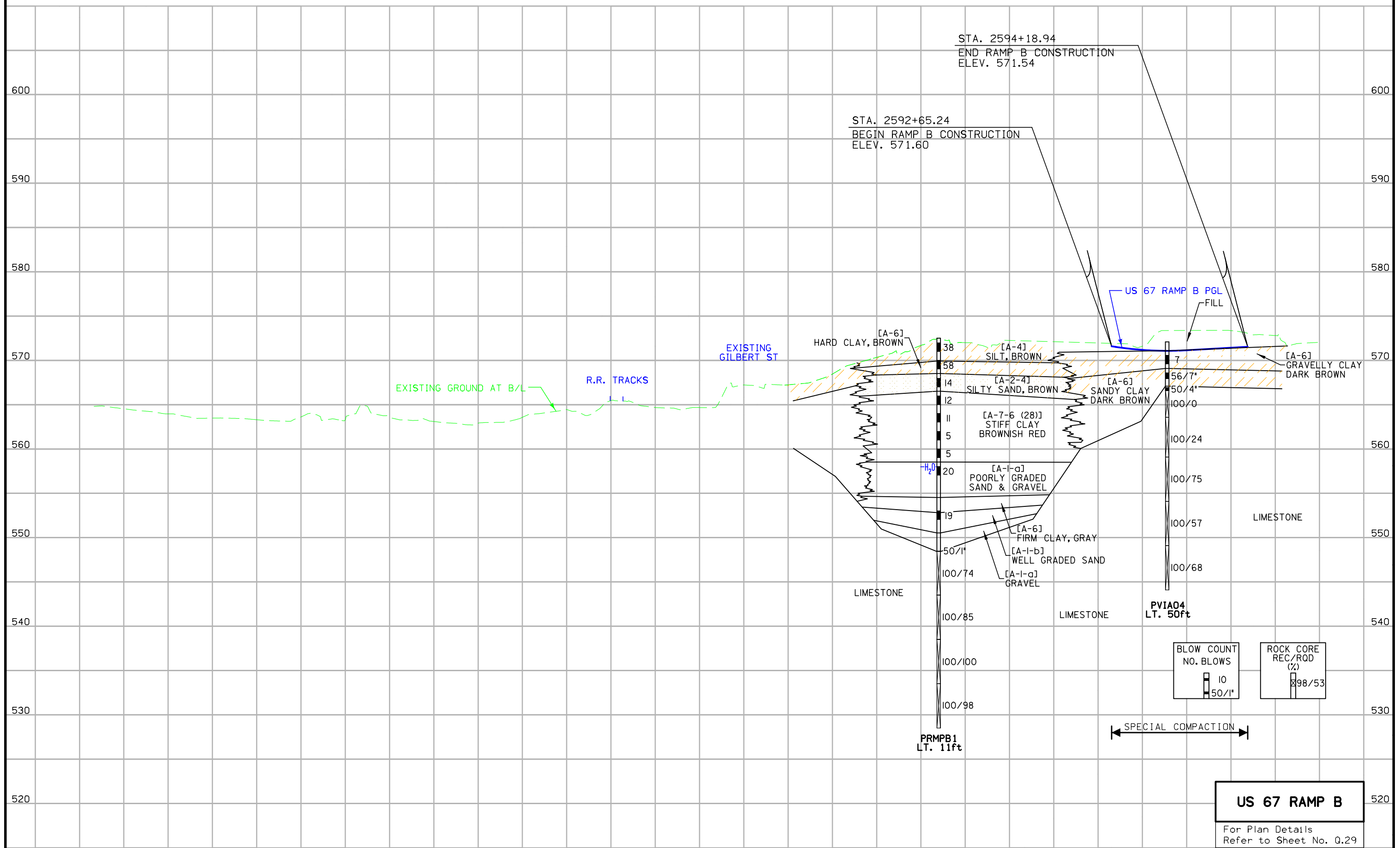
- UNSUITABLE
- SUBGRADE TREATMENT
- SHALE
- ROCK
- SELECT SOIL
- SANDY SOIL
- DENS. CORE
- SELECT SAND
- SHELBY
- BLOW
- SAMPLE
- PLUGGED
- WATER
- MOISTURE



**US 67 RAMP B**



CUT MOISTURE  
 CUT DENSITY (lb/cu ft)  
 PLASTIC LIMIT



- UNSUITABLE
- SUBGRADE TREATMENT
- SHALE
- ROCK
- SELECT SOIL
- SANDY SOIL
- DENS. CORE
- SELECT SAND
- SHELBY
- BLOW
- SAMPLE
- PLUGGED
- WATER
- MOISTURE

**US 67 RAMP B**  
 For Plan Details  
 Refer to Sheet No. Q.29

### SURVEY SYMBOLS

	Interstate Highway Symbol		Septic Tank
	U.S. Highway Symbol		Cistern
	Iowa Highway Symbol		L.P. Gas Tank (No Footing)
	County Road Highway Symbol		Underground Storage Tank
	Evergreen Tree		Latrine
	Deciduous Tree		Luminaire
	Fruit Tree		Traffic Signal
	Shrub (Bushes)		Traffic Signal with Luminaire
	Timber		TP Telephone Pedestal
	Hedge		TVP Television Pedestal
	Stump		Telephone Pole
	Swamp		Telephone Pole (Second Company)
	Rock Outcrop		Telephone Pole (Third Company)
	Broken Concrete		Telephone Pole (Fourth Company)
	Revetment (Rip Rap)		Telephone Pole (Fifth Company)
	Cemetery		Power Pole
	Grave		Power Pole (Second Company)
	Cave		Power Pole (Third Company)
	Sink Hole		Power Pole (Fourth Company)
	Board Fence		Power Pole (Fifth Company)
	Chain Link or Security Fence		Electrical Highline Tower (Metal or Concrete)
	Wire Fence		Telephone Riser Pole
	Terrace		Power Riser Pole
	Earth Dam or Dike (Existing)		Telegraph Pole
	Earth Dam or Dike (Proposed)		Satellite TV Dish
	Tile Outlet		Guardrail (Beam or Cable)
	Edge of Water		GP Guard Post (one or two)
	Existing Drainage		GP Guard Post (over two)
	Proposed Drainage		FP Filler Pipe
	Right of Way Rail or Lot Corner		GV Gas Valve
	Concrete Monument		WV Water Valve
	Well		SL Speed Limit Sign
	Windmill		MM Mile Marker Post
	Beehive Intake		SIGN Sign
	Existing Intake		WHU Water Hook Up
	Proposed Intake		RT Radio Tower
	Existing Utility Access (Manhole)		TA Tower Anchor
	Proposed Utility Access (Manhole)		EB Electric Box
	Fire Hydrant		TCB Traffic Signal Control Box
	Water Hydrant (Rural)		RRB Rail Road Signal Control Box
			TSB Telephone Switch Box

### UTILITY LEGEND

	Existing Fiber Optics (Central Scott)
	Existing Fiber Optics (Mcleod USA)
	Existing Fiber Optics (Qwest)
	Existing Fiber Optics (ATT)
	Existing Fiber Optics (MediaCom)
	Existing Fiber Optics (Bettendorf)
	Existing Fiber Optics (IowaDOT)
	Existing Power Line (MidAmerican)
	Existing Power Line (MidAmerican)
	Existing Power Line (MidAmerican)
	Existing Power Line (MidAmerican)
	Existing Power Line (IowaDOT)
	Existing Gas Line (MidAmerican)
	Existing High Pressure Gas Line (MidAmerican)
	Existing Sanitary Sewer Line (Bettendorf)
	Existing Sanitary Sewer Line (Davenport)
	Existing Telephone Line (Qwest)
	Existing Cable Television Line (MediaCom)
	Existing Cable Television Line (MediaCom)
	Existing Water Line (IA American)

### PLAN VIEW COLOR LEGEND OF PLAN AND PROFILE SHEETS

LINEWORK		Design Color No.	
Green	(2)		Existing Topographic Features and Labels
Blue	(1)		Proposed Alignment, Stationing, Tic Marks, and Alignment Annotation
Magenta	(5)		Existing Utilities
SHADING		Design Color No.	
Tan	(8)		Proposed Sidewalk Shading
Blue, Light	(230)		Proposed Sidewalk Landing Shading
Pink	(11)		Proposed Sidewalk Ramp Shading
Yellow	(4)		Highlight for Critical Notes or Features
Red	(3)		Delineates Restricted Areas
Lavender	(9)		Temporary Pavement Shading
Gray, Light	(48)		Proposed Pavement Shading
Gray, Med	(80)		Proposed Granular Shading
Gray, Dark	(112)		Proposed Grade and Pave Shading
Brown, Light	(236)		Grading Shading

#### RIGHT-OF-WAY LEGEND

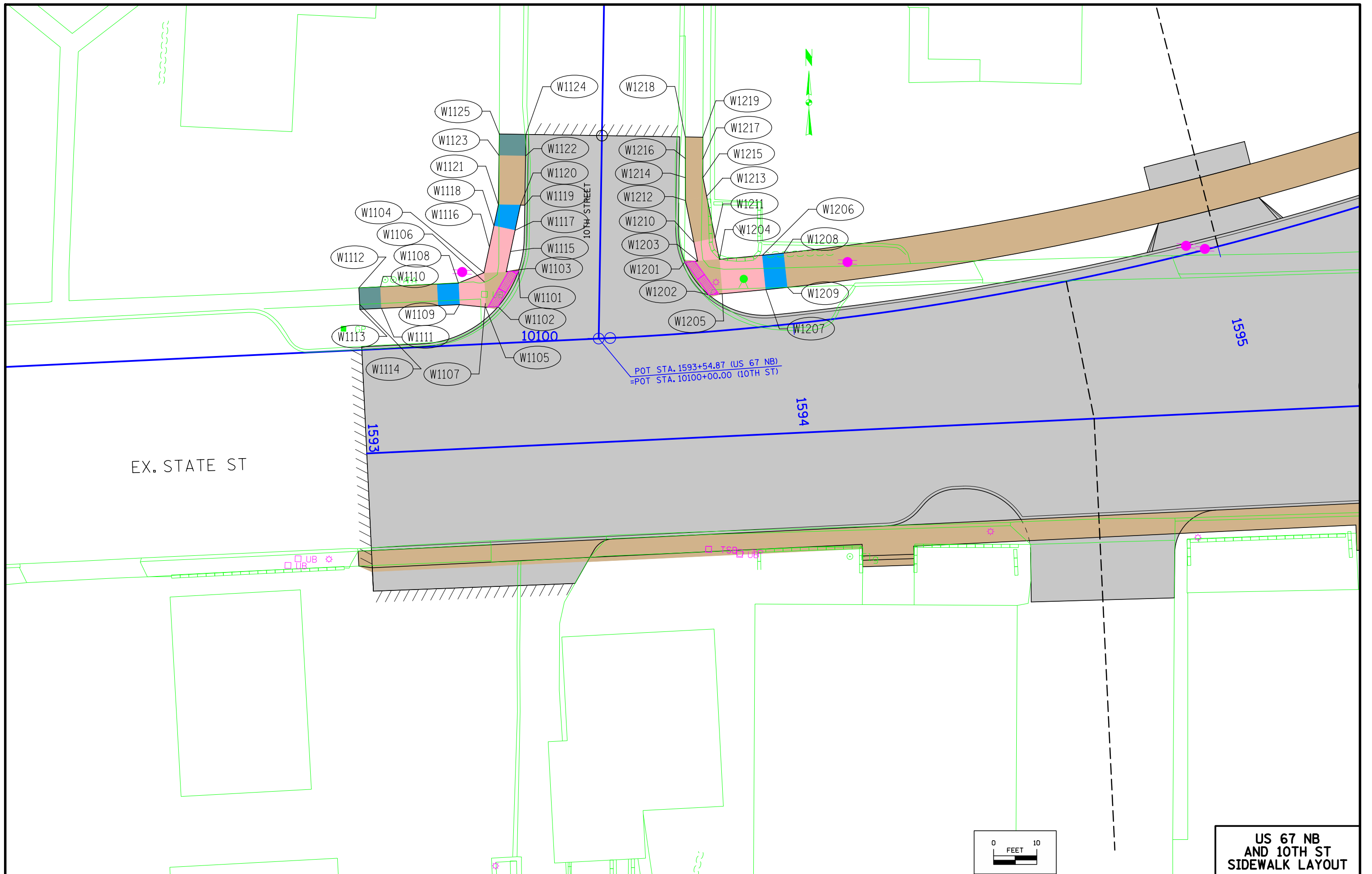
	Reference Point
	Station
	Survey Line
	Section Corner
	Ground Line Intercept
	Saw Cut
	Guardrail
	Clearing & Grubbing Area
	Pavement Removal

	Proposed Right-of-Way
	Existing and Proposed Right-of-Way
	Easement and Existing Right-of-Way
	Borrow
	Easement (Temporary)
	Easement
	Excess
	Access Control

## SIDEWALK LEGEND AND SYMBOL INFORMATION SHEET

(COVERS SHEET SERIES S)





POT STA. 1593+54.87 (US 67 NB)  
 =POT STA. 10100+00.00 (10TH ST)

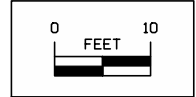
EX. STATE ST

10TH STREET

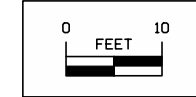
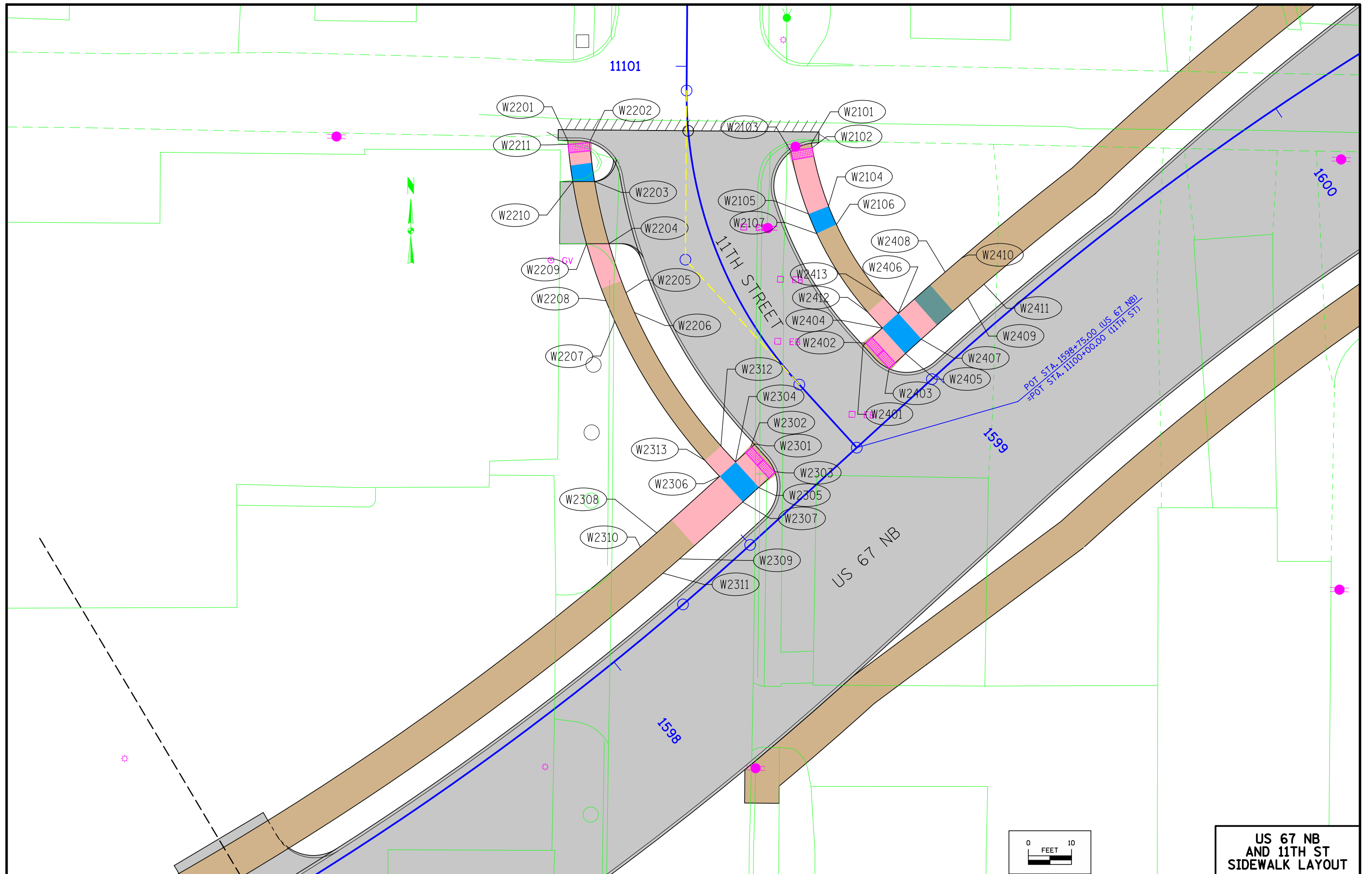
1593

1594

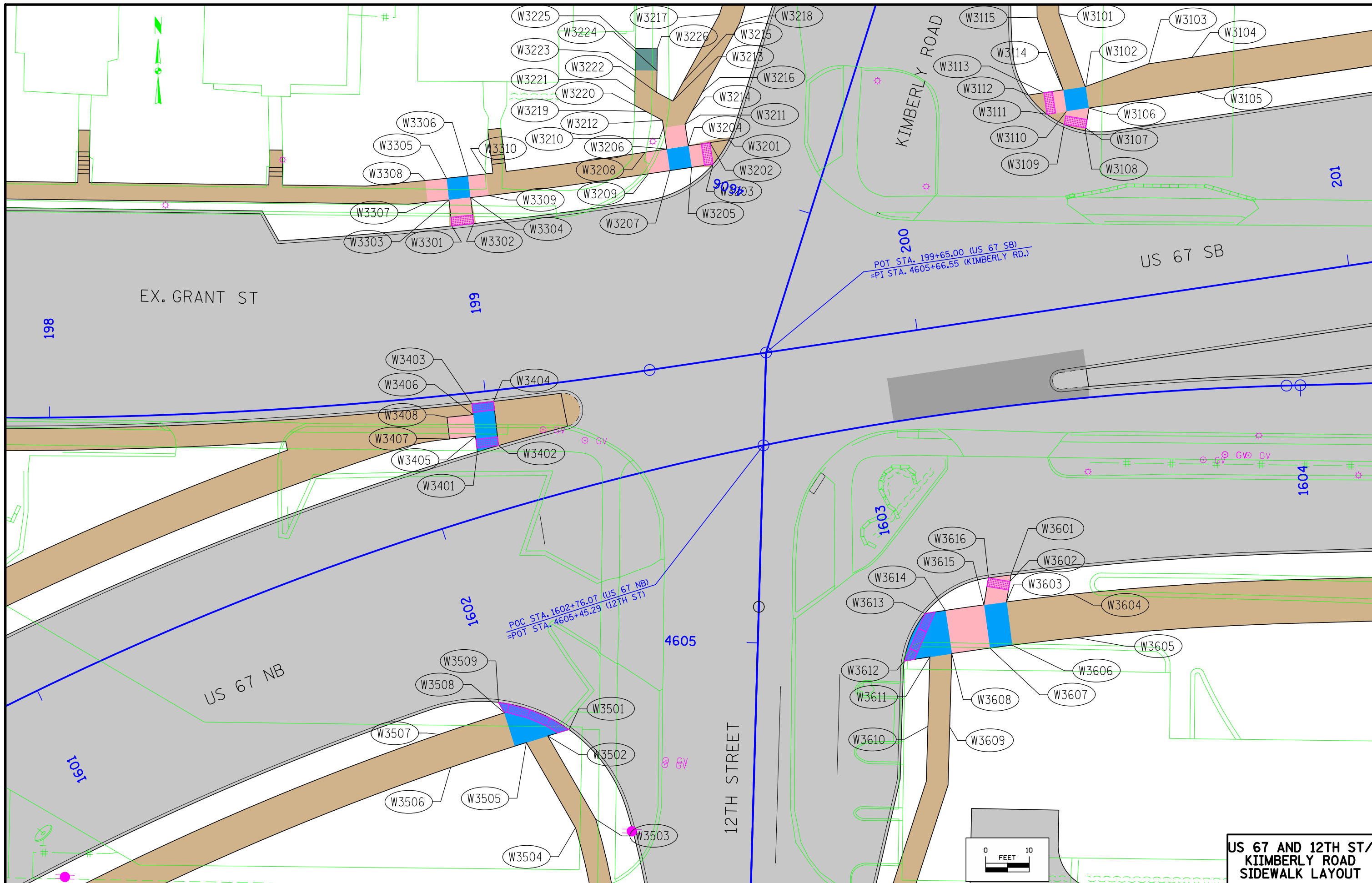
1595

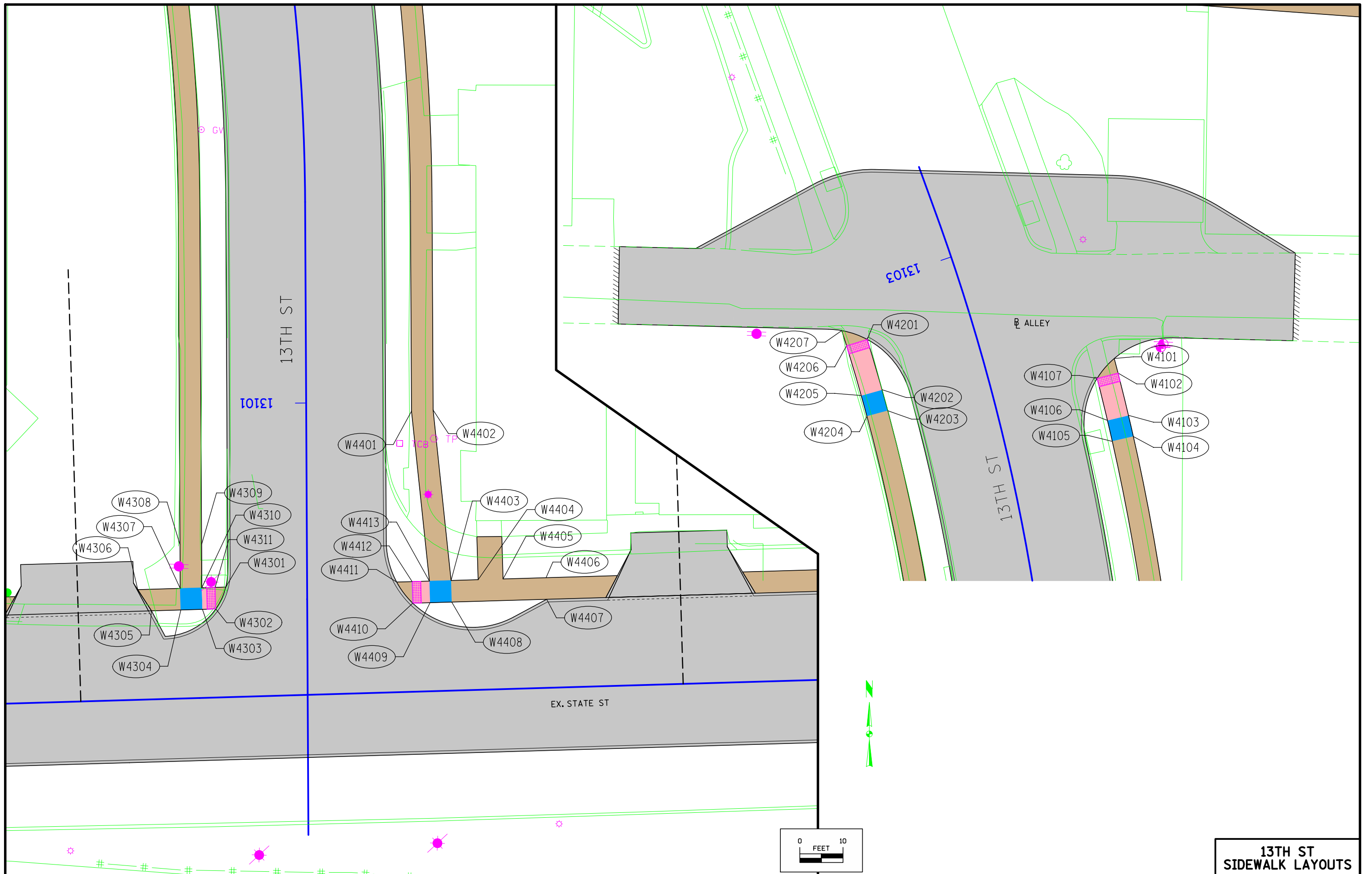


**US 67 NB  
 AND 10TH ST  
 SIDEWALK LAYOUT**

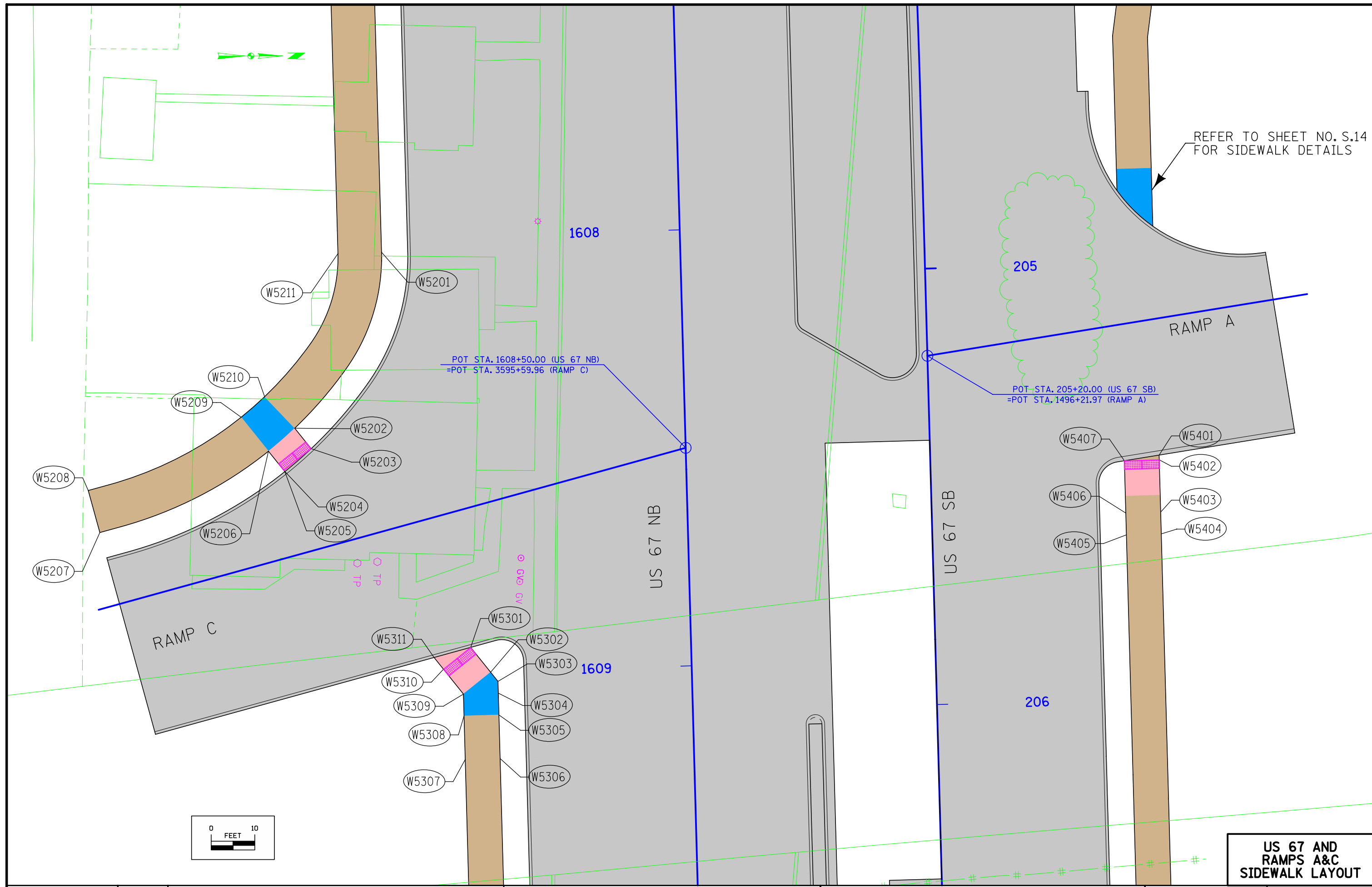


**US 67 NB  
AND 11TH ST  
SIDEWALK LAYOUT**





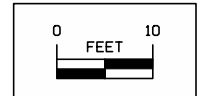
**13TH ST  
SIDEWALK LAYOUTS**



REFER TO SHEET NO. S.14  
FOR SIDEWALK DETAILS

POT STA. 1608+50.00 (US 67 NB)  
=POT STA. 3595+59.96 (RAMP C)

POT STA. 205+20.00 (US 67 SB)  
=POT STA. 1496+21.97 (RAMP A)



**US 67 AND  
RAMPS A&C  
SIDEWALK LAYOUT**



1611

208

- W6201
- W6202
- W6203
- W6207
- W6206
- W6205
- W6204

- W6101
- W6108
- W6107
- W6106
- W6102
- W6103
- W6104
- W6105

POT STA. 1612+00.00 (US 67 NB)  
=POT STA. 2595+01.68 (RAMP B)

POT STA. 208+75.00 (US 67 SB)  
=POT STA. 4495+45.77 (RAMP D)

US 67 NB

US 67 SB

RAMP D

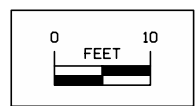
RAMP B

1612

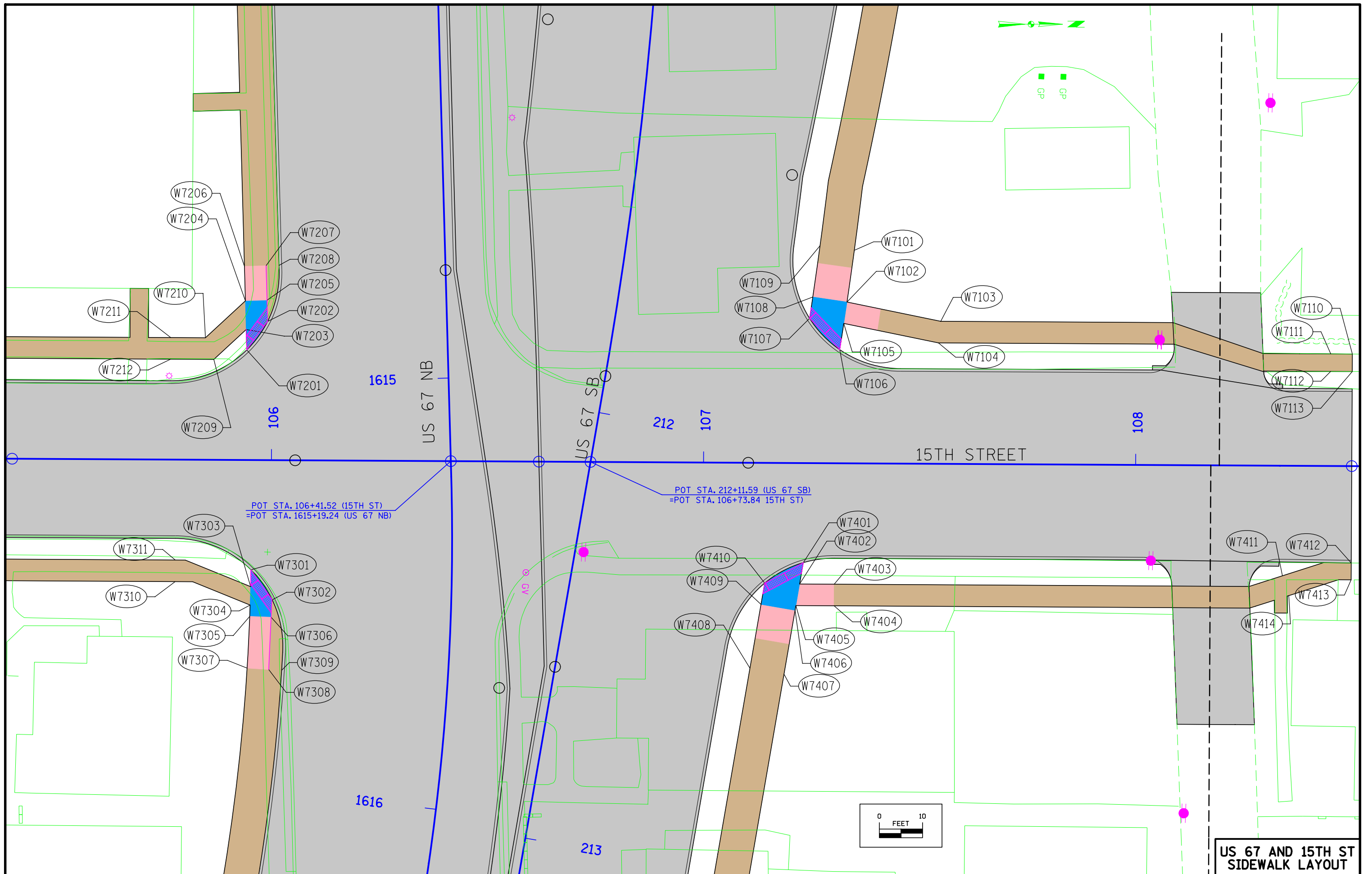
209

- W6301
- W6309
- W6308
- W6307
- W6302
- W6303
- W6304

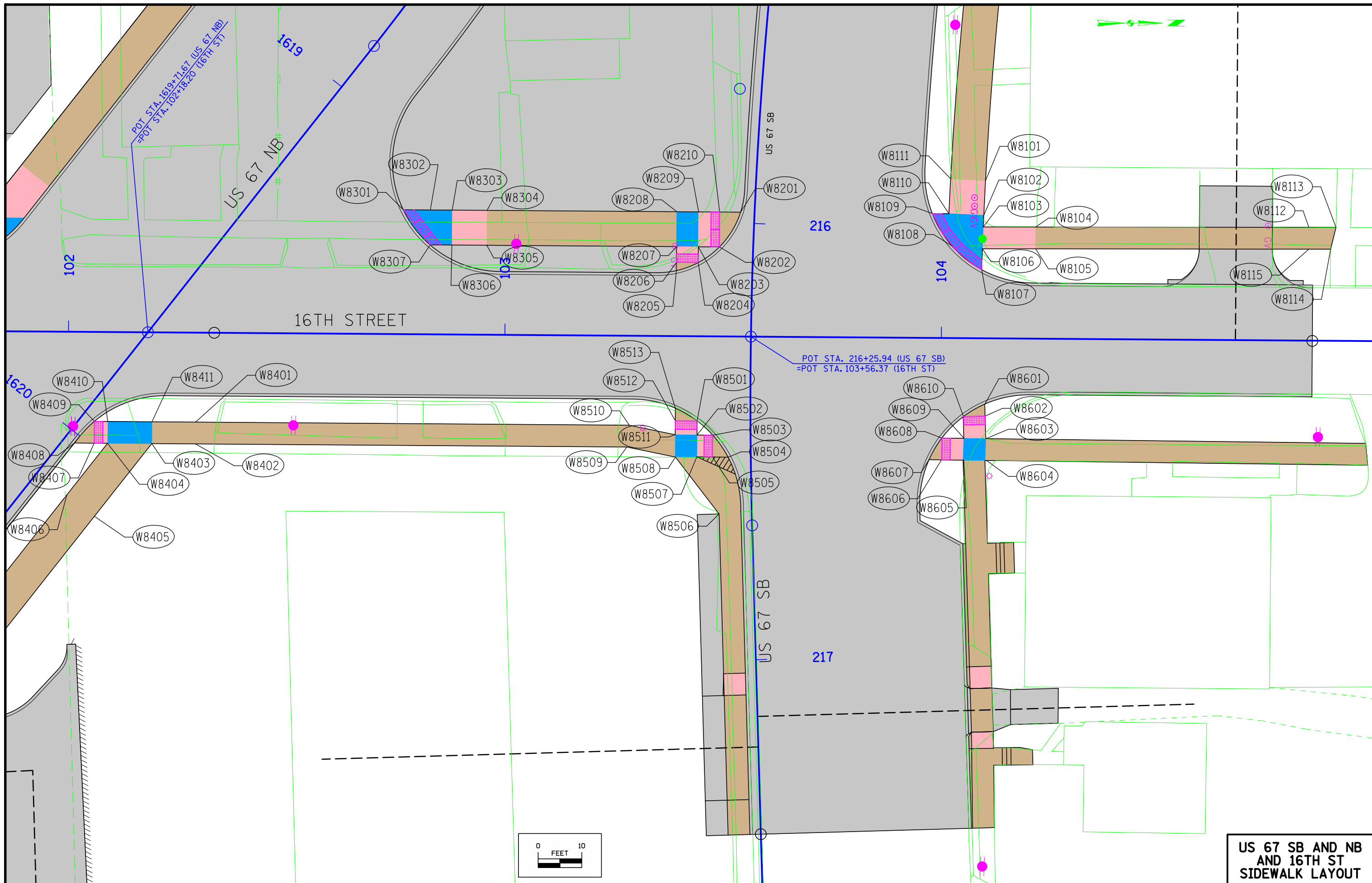
- W6407
- W6406
- W6405
- W6401
- W6402
- W6403
- W6404



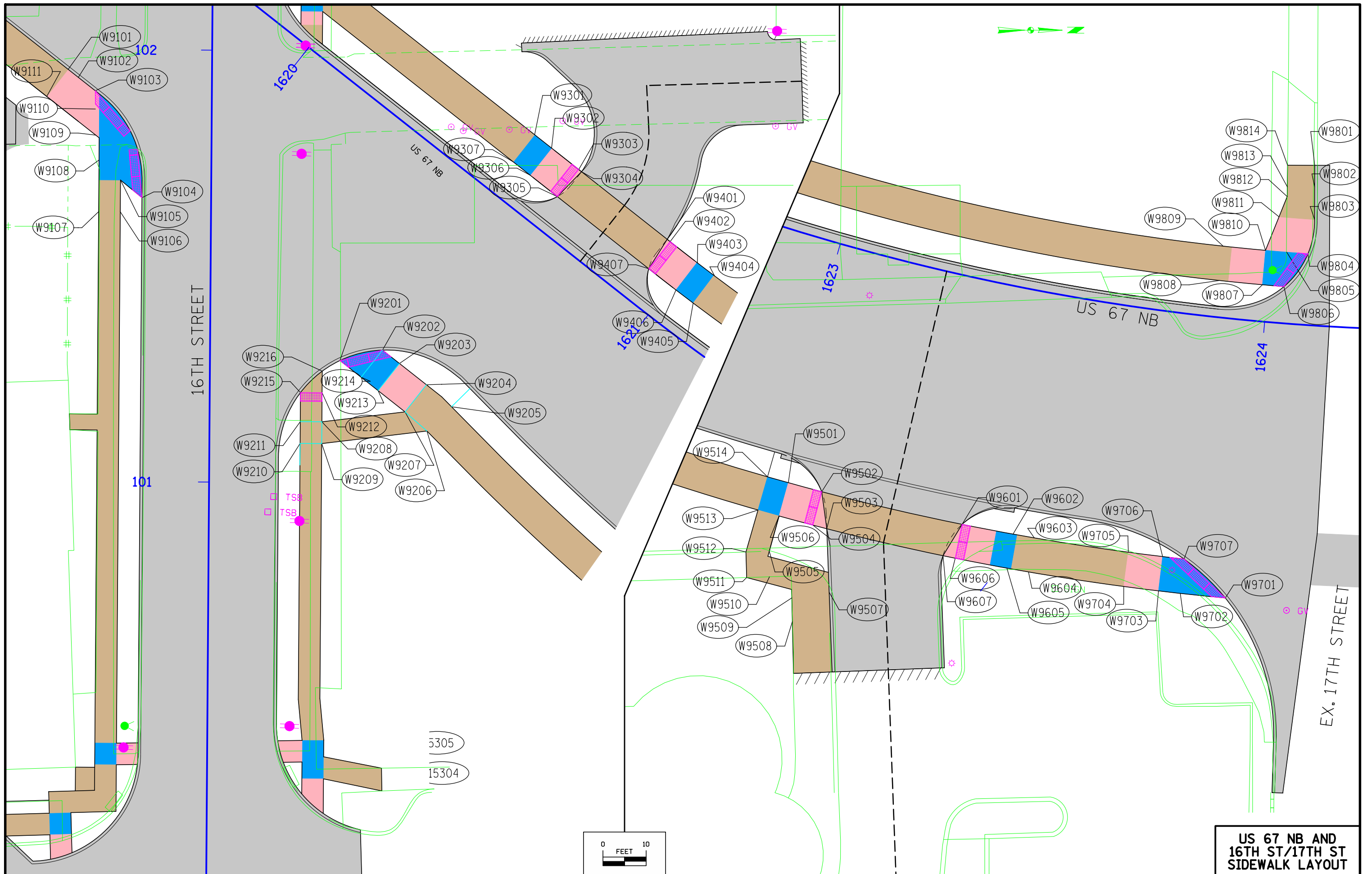
US 67 AND  
RAMPS B&D  
SIDEWALK LAYOUT



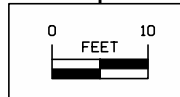
**US 67 AND 15TH ST  
SIDEWALK LAYOUT**

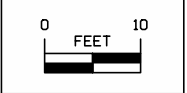
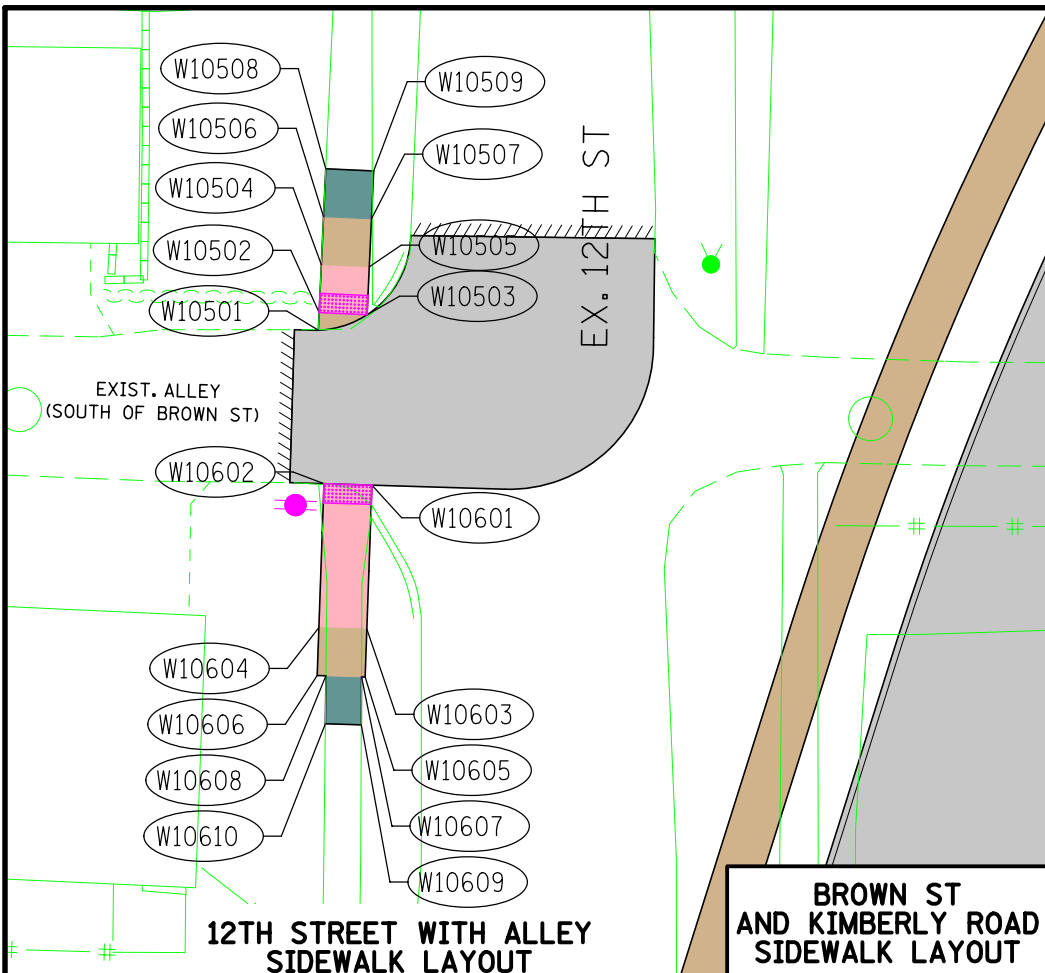
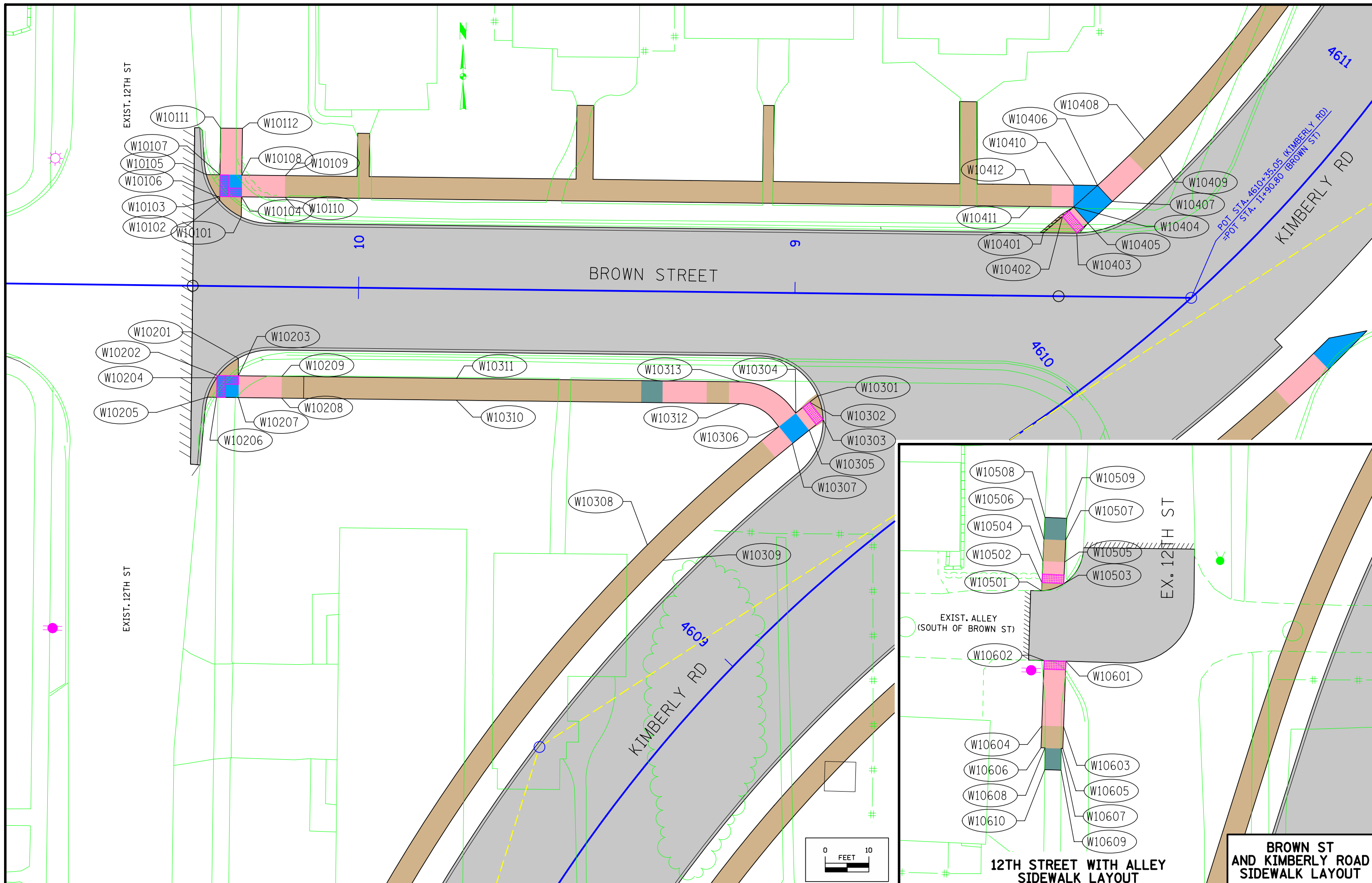




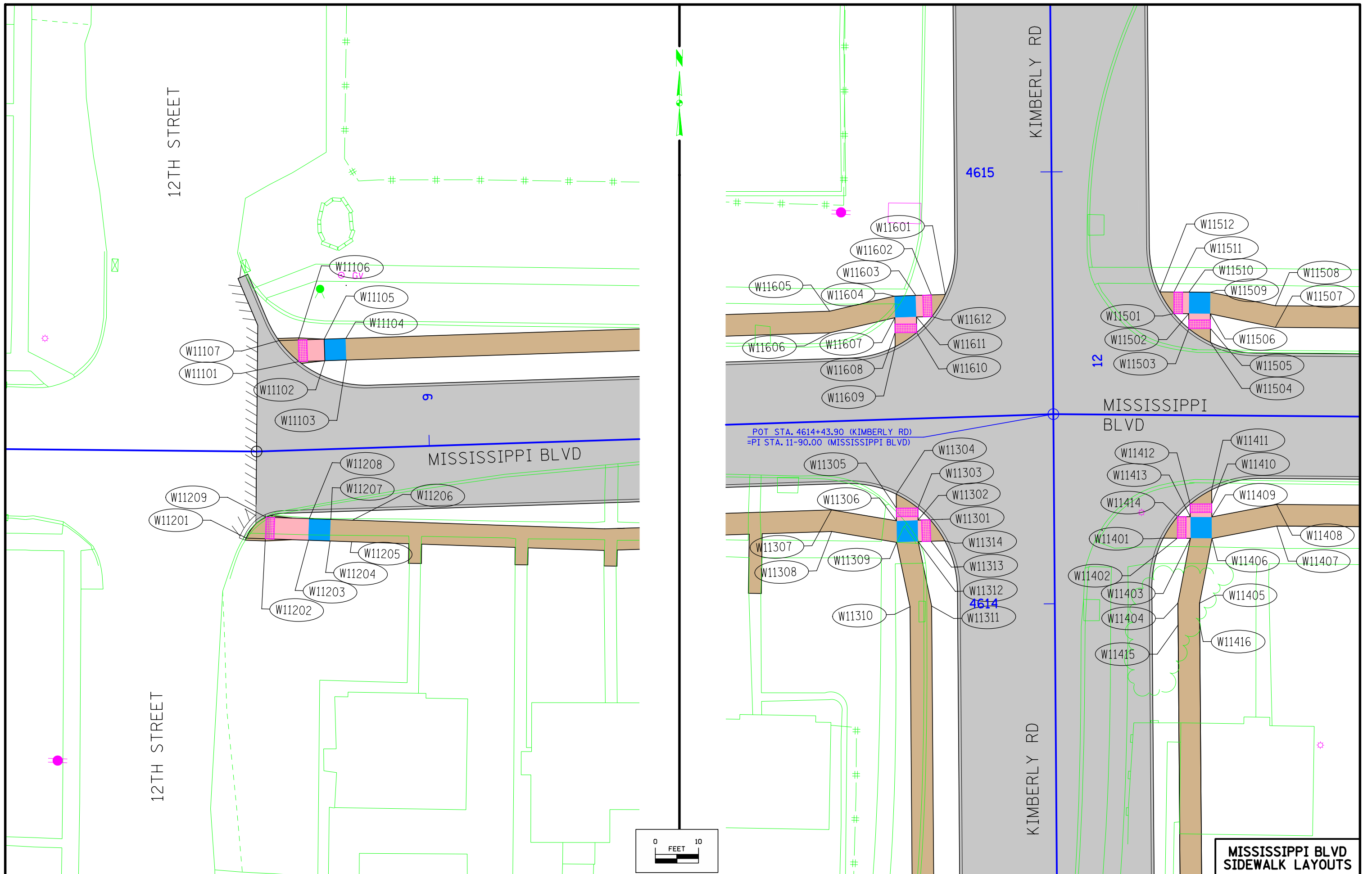


**US 67 NB AND  
16TH ST/17TH ST  
SIDEWALK LAYOUT**

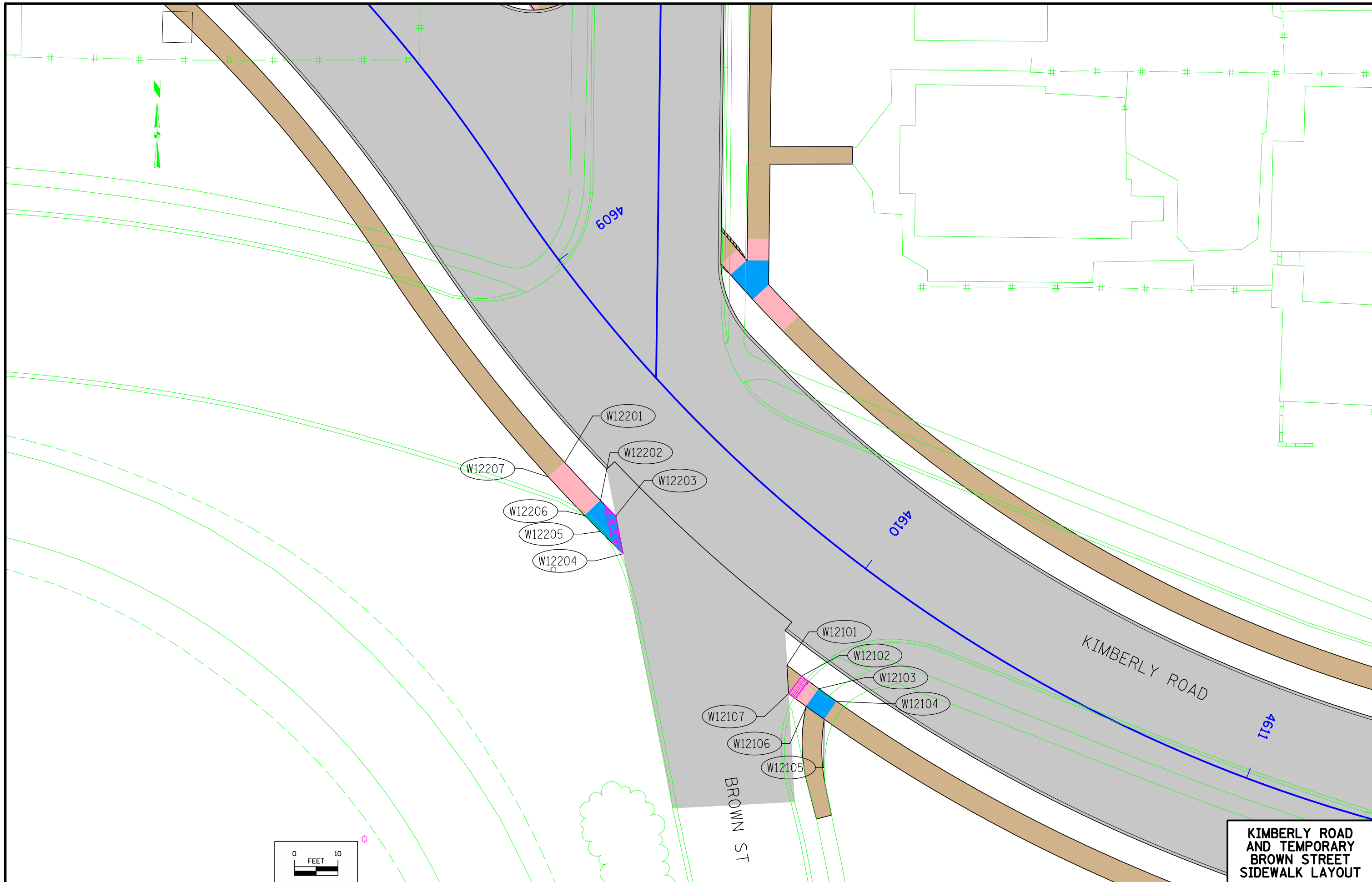




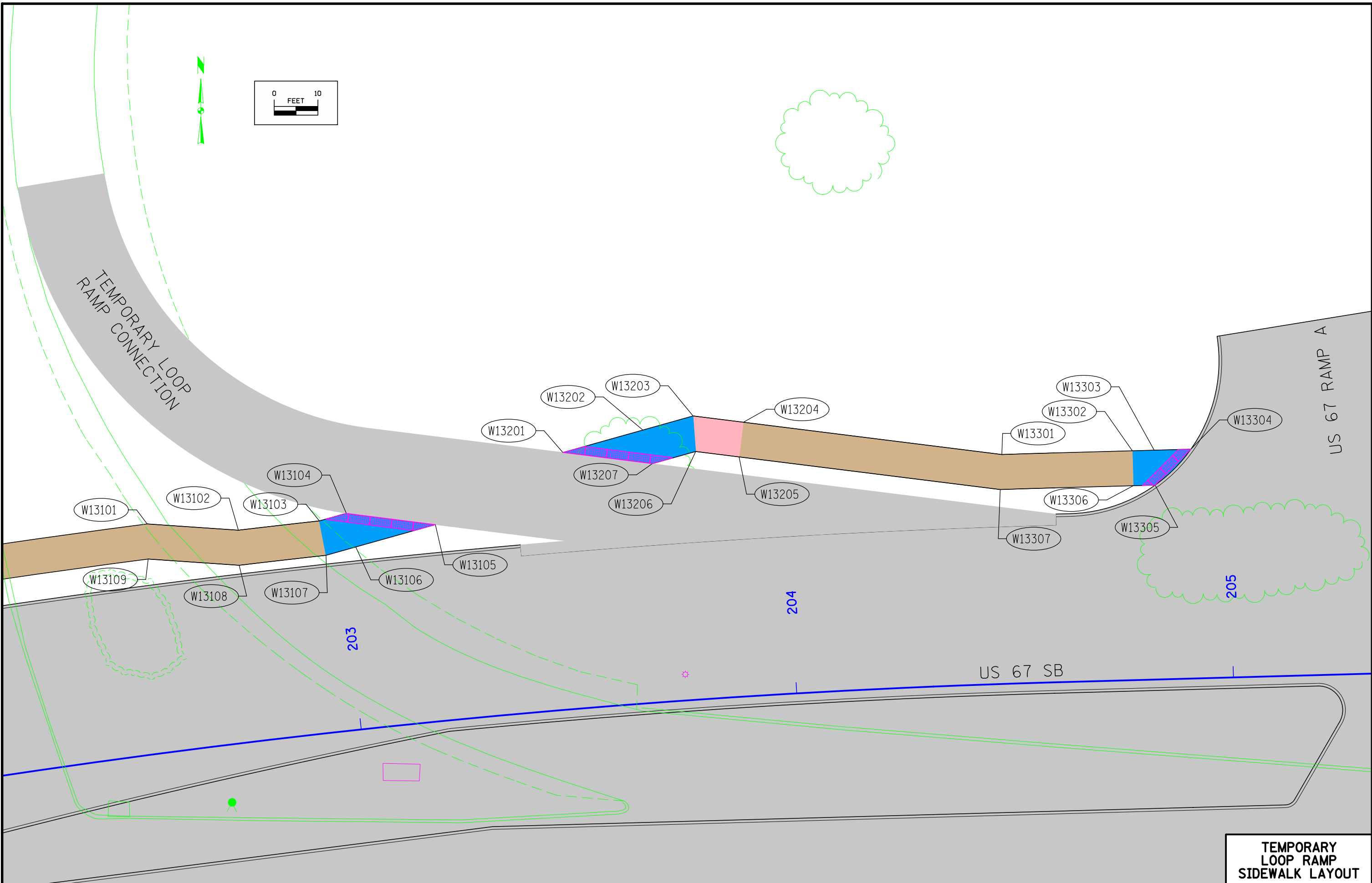
**BROWN ST AND KIMBERLY ROAD SIDEWALK LAYOUT**

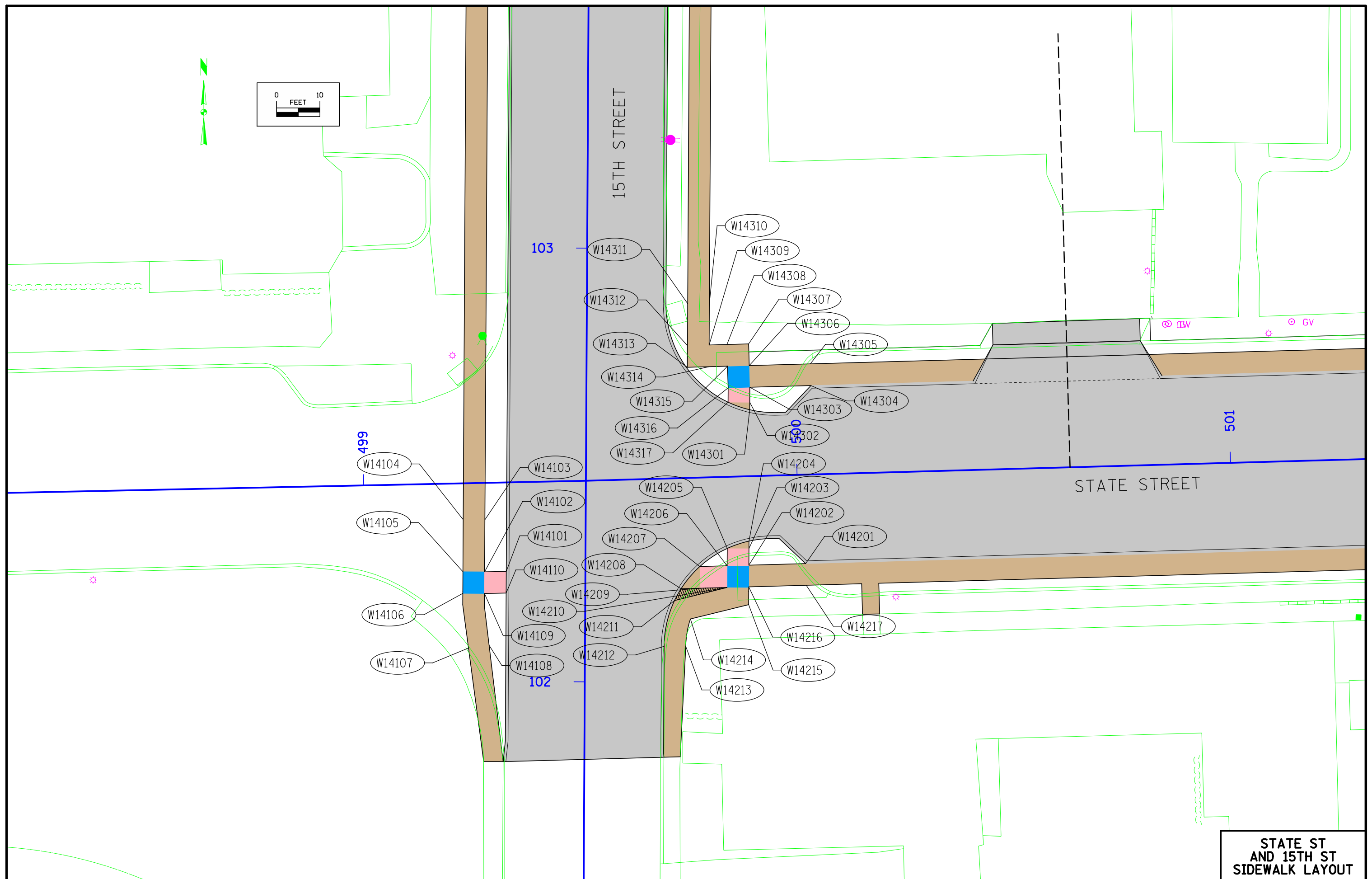
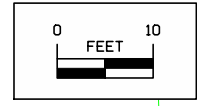


**MISSISSIPPI BLVD  
SIDEWALK LAYOUTS**

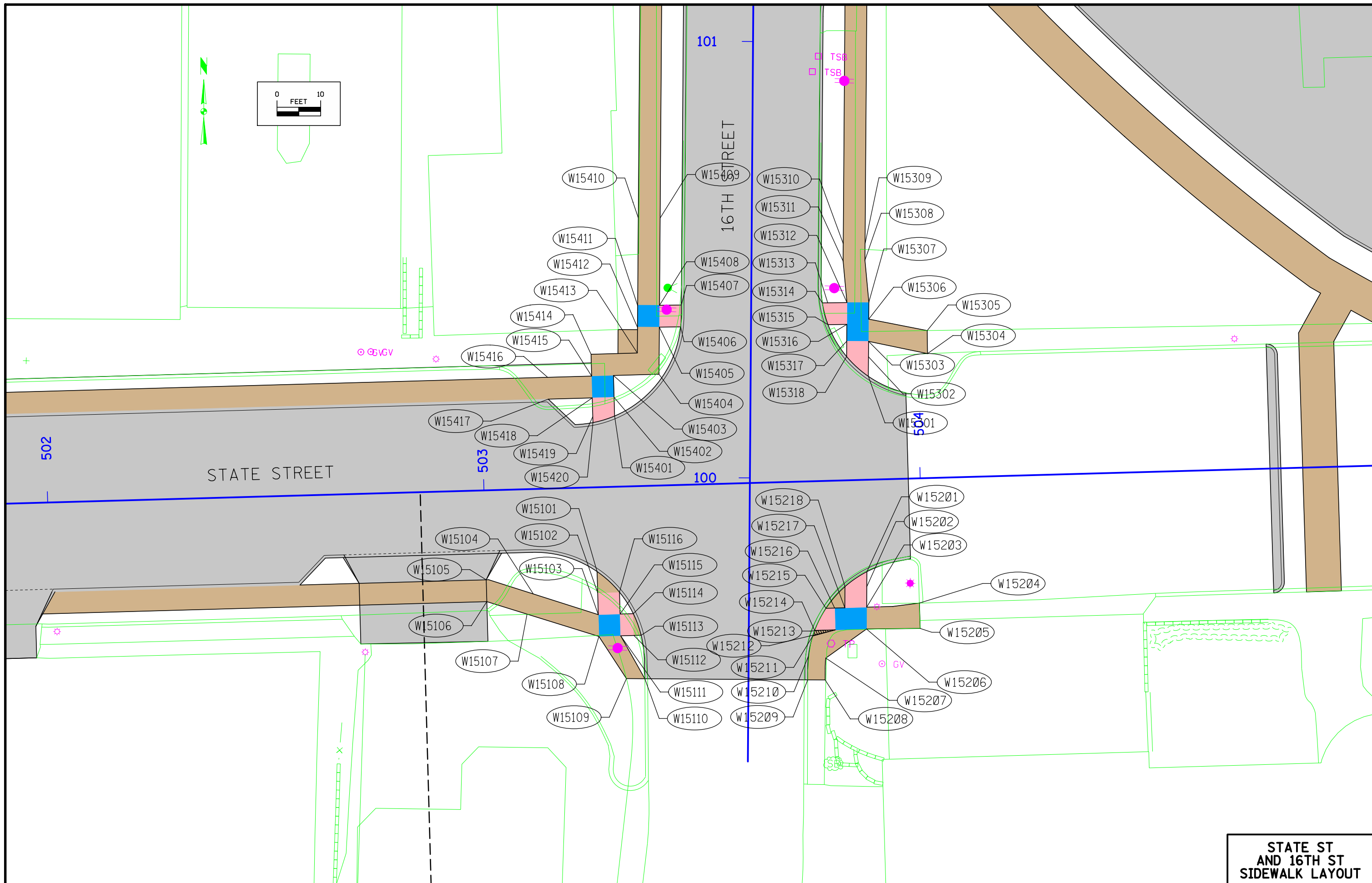


**KIMBERLY ROAD  
AND TEMPORARY  
BROWN STREET  
SIDEWALK LAYOUT**





STATE ST  
AND 15TH ST  
SIDEWALK LAYOUT



**STATE ST  
AND 16TH ST  
SIDEWALK LAYOUT**

### SIDEWALK COMPLIANCE

See S Sheets

\* Does not include curb  
 ① Staking required by Contracting Authority per Article 2511.03 of the Standard Specifications.

Point to Point	Sidewalk Designation	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant? ①	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES			
								Point	Station	Offset	Elevation
								FT	FT	%	Pos. or Neg.
W1101	W1102	Crosswalk Cross Slope - Yield Condition	9.49	0.02	0.2%	0.0% to 2.0%		W1101	10100+15.33	-18.82	571.41
W1101	W1103	Landing/Turning Space	2.83	0.04	1.4%	0.1% to 2.0%		W1102	10100+07.02	-23.41	571.43
W1103	W1104	Ramp Cross Slope	5.17	0.08	1.5%	0.1% to 2.0%		W1103	10100+15.15	-21.64	571.45
W1102	W1105	Landing/Turning Space	2.89	0.04	1.4%	0.1% to 2.0%		W1104	1593+29.07	-16.50	571.53
W1104	W1105	Landing/Turning Space	8.00	-0.06	-0.7%	0.1% to 2.0%		W1105	1593+29.07	-8.50	571.47
W1104	W1108	Ramp Running Slope	6.36	0.42	6.6%	0.5% to 8.3%		W1106	1593+29.07	-14.39	571.51
W1106	W1108	Ramp Running Slope	6.00	0.44	7.3%	0.5% to 8.3%		W1107	1593+29.07	-9.39	571.48
W1107	W1109	Ramp Running Slope	6.00	0.40	6.7%	0.5% to 8.3%		W1108	1593+23.07	-14.39	571.95
W1105	W1109	Ramp Running Slope	6.07	0.41	6.8%	0.5% to 8.3%		W1109	1593+23.07	-9.39	571.88
W1104	W1106	Ramp Cross Slope	2.11	-0.02	-0.9%	0.1% to 2.0%		W1110	1593+05.00	-14.39	571.55
W1106	W1107	Ramp Cross Slope	5.00	-0.03	-0.6%	0.1% to 2.0%		W1111	1593+05.00	-9.39	571.48
W1107	W1105	Ramp Cross Slope	0.89	-0.01	-1.1%	0.1% to 2.0%		W1112	1593+00.00	-14.39	571.52
W1108	W1109	Ramp Cross Slope	5.00	-0.07	-1.4%	0.1% to 2.0%		W1113	1593+00.00	-9.39	571.36
W1108	W1110	Sidewalk Running Slope	18.07	-0.40	-2.2%	0.5% to 5.0%		W1114	1593+00.00	-10.54	571.40
W1109	W1111	Sidewalk Running Slope	18.07	-0.40	-2.2%	0.5% to 5.0%		W1115	10100+19.84	-20.72	571.68
W1110	W1111	Sidewalk Cross Slope	5.00	-0.07	-1.4%	0.5% to 2.0%		W1116	10100+20.80	-25.63	571.75
W1110	W1112	Sidewalk Running Slope	5.00	-0.03	-0.6%	0.5% to 5.0%		W1117	10100+24.74	-19.76	571.91
W1111	W1113	Sidewalk Running Slope	5.00	-0.12	-2.4%	0.5% to 5.0%		W1118	10100+25.71	-24.66	571.98
W1112	W1114	Match Existing Cross Slope	3.85	-0.12	-3.1%	Match Existing	Slope required to match existing conditions	W1119	10100+30.62	-17.60	571.83
W1114	W1113	Match Existing Cross Slope	1.15	-0.04	-3.5%	Match Existing	Slope required to match existing conditions	W1120	10100+30.62	-18.60	571.85
W1103	W1115	Ramp Running Slope	4.78	0.23	4.8%	0.5% to 8.3%		W1121	10100+30.62	-23.70	571.92
W1104	W1116	Ramp Running Slope	6.10	0.22	3.6%	0.5% to 8.3%		W1122	10100+42.00	-17.60	571.73
W1115	W1116	Ramp Cross Slope	5.00	0.07	1.4%	0.1% to 2.0%		W1123	10100+42.00	-23.73	571.82
W1115	W1117	Ramp Running Slope	5.00	0.23	4.6%	0.5% to 8.3%		W1124	10100+47.00	-17.60	571.76
W1116	W1118	Ramp Running Slope	5.00	0.23	4.6%	0.5% to 8.3%		W1125	10100+47.00	-23.75	572.05
W1117	W1118	Ramp Cross Slope	5.00	0.07	1.4%	0.1% to 2.0%					
W1117	W1120	Sidewalk Running Slope	5.98	-0.06	-1.1%	0.5% to 5.0%					
W1118	W1121	Sidewalk Running Slope	5.00	-0.06	-1.2%	0.5% to 5.0%					
W1119	W1120	Sidewalk Cross Slope	1.00	0.01	1.5%	0.5% to 2.0%					
W1120	W1121	Sidewalk Cross Slope	5.10	0.07	1.5%	0.5% to 2.0%					
W1119	W1122	Sidewalk Running Slope	11.38	-0.10	-0.9%	0.5% to 5.0%					
W1121	W1123	Sidewalk Running Slope	11.38	-0.10	-0.9%	0.5% to 5.0%					
W1122	W1123	Sidewalk Cross Slope	6.13	0.09	1.5%	0.5% to 2.0%					
W1122	W1124	Sidewalk Running Slope	5.00	0.03	0.6%	0.5% to 5.0%					
W1123	W1125	Sidewalk Running Slope	5.00	0.23	4.6%	0.5% to 5.0%	Yes				
W1124	W1125	Match Existing Cross Slope	6.15	0.29	4.7%	Match Existing	Slope required to match existing conditions				
W1201	W1202	Crosswalk Cross Slope - Yield Condition	9.40	0.13	1.3%	0.0% to 2.0%		W1201	10100+18.03	19.66	571.53
W1201	W1203	Landing/Turning Space	2.98	0.04	1.3%	0.1% to 2.0%		W1202	10100+10.52	25.31	571.66
W1203	W1204	Ramp Cross Slope	5.04	0.07	1.4%	0.1% to 2.0%		W1203	10100+18.29	22.62	571.57
W1202	W1205	Landing/Turning Space	3.11	0.05	1.4%	0.1% to 2.0%		W1204	1593+84.24	-16.50	571.64
W1204	W1205	Ramp Cross Slope	8.00	0.06	0.8%	0.1% to 2.0%		W1205	1593+84.24	-8.50	571.70
W1204	W1206	Ramp Running Slope	10.00	0.72	7.2%	0.5% to 8.3%		W1206	1593+94.52	-16.50	572.36
W1205	W1207	Ramp Running Slope	10.18	0.74	7.3%	0.5% to 8.3%		W1207	1593+94.52	-8.50	572.44
W1206	W1207	Ramp Cross Slope	8.00	0.08	1.0%	0.1% to 2.0%		W1208	1593+99.64	-16.50	572.72
W1206	W1208	Ramp Running Slope	5.00	0.36	7.2%	0.5% to 8.3%		W1209	1593+99.64	-8.50	572.60
W1207	W1209	Ramp Running Slope	5.09	0.16	3.1%	0.5% to 8.3%		W1210	10100+22.59	21.68	571.78
W1208	W1209	Ramp Cross Slope	8.00	-0.12	-1.5%	0.1% to 2.0%		W1211	10100+23.66	26.57	571.85
W1203	W1210	Ramp Running Slope	4.40	0.21	4.8%	0.5% to 8.3%		W1212	10100+32.27	19.57	571.92
W1204	W1211	Ramp Running Slope	5.00	0.21	4.2%	0.5% to 8.3%		W1213	10100+33.34	24.45	571.99
W1210	W1211	Ramp Cross Slope	5.00	0.07	1.4%	0.1% to 2.0%		W1214	10100+37.56	19.47	571.86
W1210	W1212	Sidewalk Running Slope	5.00	0.14	2.8%	0.5% to 5.0%		W1215	10100+37.63	23.52	571.92
W1211	W1213	Sidewalk Running Slope	5.00	0.14	2.8%	0.5% to 5.0%		W1216	10100+42.00	19.39	571.80
W1212	W1213	Sidewalk Cross Slope	5.00	0.07	1.4%	0.5% to 2.0%		W1217	10100+42.00	23.45	571.86
W1212	W1214	Sidewalk Running Slope	5.29	-0.06	-1.1%	0.5% to 5.0%		W1218	10100+47.00	19.30	571.72
W1213	W1215	Sidewalk Running Slope	5.09	-0.07	-1.4%	0.5% to 5.0%		W1219	10100+47.00	23.36	571.79
W1214	W1215	Sidewalk Cross Slope	4.05	0.06	1.5%	0.5% to 2.0%					
W1214	W1216	Sidewalk Running Slope	4.37	-0.06	-1.4%	0.5% to 5.0%					
W1215	W1217	Sidewalk Running Slope	4.37	-0.06	-1.4%	0.5% to 5.0%					
W1216	W1217	Sidewalk Cross Slope	4.06	0.06	1.5%	0.5% to 2.0%					
W1216	W1218	Sidewalk Running Slope	5.00	-0.08	-1.6%	0.5% to 5.0%					
W1217	W1219	Sidewalk Running Slope	5.00	-0.07	-1.4%	0.5% to 5.0%					
W1218	W1219	Match Existing Cross Slope	4.06	0.07	1.7%	Match Existing	Slope required to match existing conditions				
W2101	W2103	Crosswalk Cross Slope - Yield Condition	5.12	-0.15	-2.9%	0.0% to 3.9%		W2101	11100+77.68	28.00	570.25
W2101	W2102	Ramp Running Slope	1.08	-0.08	-6.9%	0.5% to 8.3%		W2102	11100+76.17	28.00	570.18
W2102	W2103	Ramp Cross Slope	5.00	-0.07	-1.5%	0.1% to 2.0%		W2103	11100+76.17	23.00	570.10
W2102	W2104	Ramp Running Slope	14.03	0.95	6.7%	0.5% to 8.3%		W2104	11100+56.69	28.00	571.12
W2103	W2105	Ramp Running Slope	15.00	0.94	6.3%	0.5% to 8.3%		W2105	11100+56.69	23.00	571.05
W2104	W2105	Ramp Cross Slope	5.00	-0.08	-1.5%	0.1% to 2.0%		W2106	11100+50.20	28.00	571.19
W2104	W2106	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%		W2107	11100+50.20	23.00	571.12
W2105	W2107	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%					
W2106	W2107	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%					
W2201	W2211	Sidewalk Running Slope	0.88	-0.01	-1.7%	0.5% to 5.0%		W2201	11100+85.19	-28.00	569.83
W2202	W2203	Sidewalk Running Slope	9.16	0.30	3.3%	0.5% to 5.0%		W2202	11100+84.50	-23.00	569.74
W2202	W2211	Sidewalk Cross Slope	5.00	0.08	1.5%	0.5% to 2.0%		W2203	11100+77.05	-23.00	570.04



### SIDEWALK COMPLIANCE

See S Sheets

\* Does not include curb  
 ① Staking required by Contracting Authority per Article 2511.03 of the Standard Specifications.

Point to Point	Sidewalk Designation	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant? ①	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES			
								Point	Station	Offset	Elevation
								FT	FT	%	Pos. or Neg.
W2203	W2204	Sidewalk Running Slope	14.77	0.32	2.2%	0.5% to 5.0%		W2204	11100+65.04	-23.00	570.36
W2203	W2210	Sidewalk Cross Slope	5.07	0.08	1.5%	0.5% to 2.0%		W2205	11100+55.29	-23.00	571.24
W2204	W2205	Ramp Running Slope	12.00	0.88	7.3%	0.5% to 8.3%		W2206	11100+51.22	-23.00	571.31
W2204	W2209	Ramp Cross Slope	5.20	0.08	1.5%	0.1% to 2.0%		W2207	11100+51.22	-28.00	571.39
W2205	W2206	Landing/Turning Space	5.00	0.07	1.5%	0.1% to 2.0%		W2208	11100+55.29	-28.00	571.31
W2205	W2208	Landing/Turning Space	5.00	0.07	1.5%	0.1% to 2.0%		W2209	11100+66.19	-28.00	570.44
W2206	W2207	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%		W2210	11100+77.70	-28.00	570.12
W2207	W2208	Landing/Turning Space	5.20	-0.08	-1.4%	0.1% to 2.0%		W2211	11100+84.50	-28.00	569.82
W2208	W2209	Ramp Running Slope	13.95	-0.87	-6.3%	0.5% to 8.3%					
W2209	W2210	Sidewalk Running Slope	14.74	-0.32	-2.2%	0.5% to 5.0%					
W2210	W2211	Sidewalk Running Slope	8.70	-0.30	-3.5%	0.5% to 5.0%					
W2301	W2303	Crosswalk Cross Slope - No Yield Condition	8.04	0.19	2.4%	0.0% to 5.0%		W2301	11100+16.70	-17.00	571.85
W2301	W2302	Ramp Running Slope	0.98	0.07	7.1%	0.5% to 8.3%		W2302	11100+16.69	-17.98	571.92
W2302	W2303	Ramp Cross Slope	8.00	0.12	1.5%	0.1% to 2.0%		W2303	11100+08.68	-17.77	572.04
W2302	W2304	Ramp Running Slope	5.15	0.29	5.5%	0.5% to 8.3%		W2304	11100+16.50	-23.00	572.21
W2303	W2305	Ramp Running Slope	5.15	0.29	5.5%	0.5% to 8.3%		W2305	11100+08.50	-23.00	572.33
W2304	W2305	Ramp Cross Slope	8.00	0.12	1.5%	0.1% to 2.0%		W2306	11100+16.50	-28.00	572.28
W2304	W2306	Ramp Cross Slope	5.00	0.07	1.5%	0.1% to 2.0%		W2307	11100+08.50	-28.00	572.40
W2305	W2307	Landing/Turning Space	5.00	0.07	1.5%	0.1% to 2.0%		W2308	1598+27.12	-16.50	573.07
W2306	W2307	Ramp Cross Slope	8.00	0.12	1.5%	0.1% to 2.0%		W2309	1598+27.12	-8.50	572.96
W2306	W2308	Sidewalk Running Slope	19.58	0.79	4.0%	0.5% to 5.0%		W2310	1598+31.88	-16.50	573.27
W2307	W2309	Sidewalk Running Slope	19.76	0.56	2.8%	0.5% to 5.0%		W2311	1598+31.88	-8.50	573.16
W2308	W2309	Sidewalk Cross Slope	8.00	-0.11	-1.4%	0.5% to 2.0%		W2312	11100+21.18	-23.00	572.35
W2308	W2310	Sidewalk Running Slope	5.00	0.20	4.0%	0.5% to 5.0%		W2313	11100+21.18	-28.00	572.42
W2309	W2311	Sidewalk Running Slope	5.00	0.20	4.0%	0.5% to 5.0%					
W2310	W2311	Sidewalk Cross Slope	8.00	-0.11	-1.4%	0.5% to 2.0%					
W2312	W2313	Sidewalk Cross Slope	5.00	0.07	1.5%	0.5% to 2.0%					
W2312	W2304	Sidewalk Running Slope	4.92	-0.14	-2.8%	0.5% to 5.0%					
W2401	W2403	Crosswalk Cross Slope - Yield Condition	8.04	0.18	2.2%	0.0% to 3.2%		W2401	11100+16.50	17.00	571.70
W2401	W2402	Ramp Running Slope	0.83	0.06	7.2%	0.5% to 8.3%		W2402	11100+16.50	17.83	571.76
W2402	W2403	Ramp Cross Slope	8.00	0.12	1.5%	0.1% to 2.0%		W2403	11100+08.50	17.83	571.88
W2402	W2404	Ramp Running Slope	5.24	0.33	6.2%	0.5% to 8.3%		W2404	11100+16.50	23.00	572.09
W2403	W2405	Ramp Running Slope	5.24	0.32	6.1%	0.5% to 8.3%		W2405	11100+08.50	23.00	572.20
W2404	W2405	Ramp Cross Slope	8.00	0.12	1.4%	0.1% to 2.0%		W2406	1599+02.90	-16.50	572.16
W2404	W2406	Ramp Cross Slope	5.00	0.07	1.5%	0.1% to 2.0%		W2407	1599+02.90	-8.50	572.28
W2405	W2407	Landing/Turning Space	4.95	0.07	1.5%	0.1% to 2.0%		W2408	1599+11.02	-16.50	572.70
W2406	W2407	Ramp Cross Slope	8.00	0.12	1.4%	0.1% to 2.0%		W2409	1599+11.02	-8.50	572.58
W2406	W2408	Sidewalk Running Slope	14.45	0.54	3.7%	0.5% to 5.0%		W2410	1599+15.90	-16.50	572.88
W2407	W2409	Sidewalk Running Slope	14.36	0.30	2.1%	0.5% to 5.0%		W2411	1599+15.90	-8.50	572.76
W2408	W2409	Sidewalk Cross Slope	8.00	-0.12	-1.5%	0.5% to 2.0%		W2412	11100+22.02	23.00	572.20
W2408	W2410	Sidewalk Running Slope	5.00	0.18	3.7%	0.5% to 5.0%		W2413	11100+22.02	28.00	572.27
W2409	W2411	Sidewalk Running Slope	4.94	0.18	3.7%	0.5% to 5.0%					
W2410	W2411	Sidewalk Cross Slope	8.00	-0.12	-1.5%	0.5% to 2.0%					
W2404	W2412	Sidewalk Running Slope	5.00	0.11	2.2%	0.5% to 5.0%					
W2406	W2413	Sidewalk Running Slope	4.90	0.11	2.2%	0.5% to 5.0%					
W2412	W2413	Sidewalk Cross Slope	5.00	0.07	1.5%	0.5% to 2.0%					
W3101	W3102	Sidewalk Running Slope	17.74	0.47	2.6%	0.5% to 5.0%		W3101	200+42.83	-66.89	573.22
W3101	W3115	Sidewalk Cross Slope	5.07	-0.07	-1.4%	0.5% to 2.0%		W3102	200+46.47	-49.50	573.69
W3102	W3103	Sidewalk Running Slope	15.30	0.59	3.9%	0.5% to 5.0%		W3103	200+61.47	-52.50	574.28
W3102	W3106	Landing/Turning Space	5.00	-0.07	-1.4%	0.1% to 2.0%		W3104	200+71.47	-52.50	574.67
W3102	W3114	Landing/Turning Space	5.26	-0.08	-1.5%	0.1% to 2.0%		W3105	200+71.47	-44.50	574.55
W3103	W3104	Sidewalk Running Slope	10.00	0.39	3.9%	0.5% to 5.0%		W3106	200+46.47	-44.50	573.62
W3104	W3105	Sidewalk Cross Slope	8.00	-0.12	-1.5%	0.5% to 2.0%		W3107	200+45.03	-40.08	573.47
W3105	W3106	Sidewalk Running Slope	25.00	-0.93	-3.7%	0.5% to 5.0%		W3108	200+44.84	-39.47	573.50
W3106	W3107	Ramp Running Slope	4.65	-0.15	-3.2%	0.5% to 8.3%		W3109	200+40.28	-41.62	573.40
W3106	W3110	Landing/Turning Space	5.26	-0.08	-1.5%	0.1% to 2.0%		W3110	200+41.21	-44.50	573.54
W3107	W3108	Ramp Running Slope	0.63	0.03	4.8%	0.5% to 8.3%		W3111	200+36.72	-44.50	573.28
W3107	W3109	Ramp Cross Slope	5.00	-0.07	-1.4%	0.1% to 2.0%		W3112	200+33.14	-49.50	573.11
W3109	W3110	Ramp Running Slope	3.03	0.14	4.6%	0.5% to 8.3%		W3113	200+36.72	-49.50	573.30
W3110	W3111	Ramp Running Slope	4.49	-0.26	-5.8%	0.5% to 8.3%		W3114	200+41.21	-49.50	573.61
W3110	W3114	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%		W3115	200+37.77	-66.72	573.15
W3111	W3113	Ramp Cross Slope	5.00	0.02	0.4%	0.1% to 2.0%					
W3112	W3113	Ramp Running Slope	3.58	0.19	5.3%	0.5% to 8.3%					
W3113	W3114	Ramp Running Slope	4.49	0.31	6.9%	0.5% to 8.3%					
W3114	W3115	Sidewalk Running Slope	17.56	-0.46	-2.6%	0.5% to 5.0%					
W3201	W3203	Crosswalk Cross Slope - Yield Condition	6.70	0.10	1.5%	0.0% to 2.0%		W3201	199+63.83	-49.50	572.58
W3201	W3202	Sidewalk Running Slope	4.46	0.13	2.9%	0.5% to 5.0%		W3202	199+59.37	-49.50	572.71
W3202	W3203	Sidewalk Cross Slope	5.00	-0.03	-0.6%	0.5% to 2.0%		W3203	199+59.37	-44.50	572.68
W3202	W3204	Ramp Running Slope	5.00	0.35	7.0%	0.5% to 8.3%		W3204	199+54.37	-49.50	573.06
W3203	W3205	Ramp Running Slope	5.00	0.31	6.2%	0.5% to 8.3%		W3205	199+54.37	-44.50	572.99
W3204	W3205	Ramp Cross Slope	5.00	-0.07	-1.4%	0.1% to 2.0%		W3206	199+49.37	-49.50	573.13
W3204	W3206	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%		W3207	199+49.37	-44.50	573.06
W3205	W3207	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%		W3208	199+44.37	-49.50	573.37

**SIDEWALK COMPLIANCE**

See S Sheets

\* Does not include curb  
① Staking required by Contracting Authority per Article 2511.03 of the Standard Specifications.

Point to Point	Sidewalk Designation	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant? ①	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES				
								Point	Station	Offset	Elevation	
		FT	FT	%	Pos. or Neg.							
W3206	W3207	Ramp Cross Slope	5.00	-0.07	-1.4%	0.1% to 2.0%		W3209	199+44.37	-44.50	573.30	
W3206	W3208	Ramp Running Slope	5.00	0.24	4.8%	0.5% to 8.3%		W3210	4606+08.69	-37.94	573.43	
W3207	W3209	Ramp Running Slope	5.00	0.24	4.8%	0.5% to 8.3%		W3211	4606+10.88	-33.44	573.38	
W3208	W3209	Ramp Cross Slope	5.00	-0.07	-1.4%	0.1% to 2.0%		W3212	4606+10.18	-38.67	573.45	
W3206	W3210	Ramp Running Slope	5.00	0.30	6.0%	0.5% to 8.3%		W3213	4606+15.09	-37.73	573.50	
W3204	W3211	Ramp Running Slope	5.00	0.32	6.4%	0.5% to 8.3%		W3214	4606+14.15	-32.82	573.43	
W3210	W3211	Ramp Cross Slope	5.00	-0.05	-1.0%	0.1% to 2.0%		W3215	4606+20.00	-36.79	573.41	
W3210	W3212	Sidewalk Running Slope	1.66	0.02	1.2%	0.5% to 5.0%		W3216	4606+19.07	-31.88	573.34	
W3212	W3213	Sidewalk Cross Slope	5.00	0.05	1.0%	0.5% to 2.0%		W3217	4606+34.21	-33.50	573.13	
W3211	W3214	Sidewalk Running Slope	3.34	0.05	1.5%	0.5% to 5.0%		W3218	4606+36.73	-28.50	573.06	
W3213	W3214	Sidewalk Cross Slope	5.00	-0.07	-1.4%	0.5% to 2.0%		W3219	4606+10.97	-42.77	573.53	
W3213	W3215	Sidewalk Running Slope	5.00	-0.09	-1.8%	0.5% to 5.0%		W3220	4606+11.54	-45.74	573.59	
W3214	W3216	Sidewalk Running Slope	5.00	-0.09	-1.8%	0.5% to 5.0%		W3221	4606+14.43	-46.62	573.65	
W3215	W3216	Sidewalk Cross Slope	5.00	-0.07	-1.4%	0.5% to 2.0%		W3222	4606+15.88	-41.83	573.58	
W3215	W3217	Sidewalk Running Slope	17.54	-0.28	-1.6%	0.5% to 5.0%		W3223	4606+19.23	-48.36	573.72	
W3216	W3218	Sidewalk Running Slope	17.99	-0.28	-1.6%	0.5% to 5.0%		W3224	4606+20.66	-43.28	573.65	
W3217	W3218	Sidewalk Cross Slope	5.02	-0.07	-1.4%	0.5% to 2.0%		W3225	4606+24.01	-49.51	573.83	
W3212	W3219	Sidewalk Running Slope	4.18	0.08	1.9%	0.5% to 5.0%		W3226	4606+25.45	-44.72	573.70	
W3213	W3222	Sidewalk Running Slope	4.18	0.08	1.9%	0.5% to 5.0%						
W3219	W3222	Sidewalk Cross Slope	5.00	0.05	1.0%	0.5% to 2.0%						
W3219	W3220	Sidewalk Running Slope	3.03	0.06	2.0%	0.5% to 5.0%						
W3220	W3221	Sidewalk Running Slope	3.03	0.06	2.0%	0.5% to 5.0%						
W3221	W3222	Sidewalk Cross Slope	5.00	-0.07	-1.4%	0.5% to 2.0%						
W3221	W3223	Sidewalk Running Slope	5.00	0.07	1.4%	0.5% to 5.0%						
W3222	W3224	Sidewalk Running Slope	5.00	0.07	1.4%	0.5% to 5.0%						
W3223	W3224	Sidewalk Cross Slope	5.00	-0.07	-1.4%	0.5% to 2.0%						
W3223	W3225	Sidewalk Running Slope	5.00	0.11	2.2%	0.5% to 5.0%						
W3224	W3226	Sidewalk Running Slope	5.00	0.05	1.0%	0.5% to 5.0%						
W3225	W3226	Match Existing Cross Slope	5.00	-0.13	-2.6%	Match Existing						
W3301	W3302	Crosswalk Cross Slope - Yield Condition	5.00	0.05	1.0%	0.0% to 2.0%		W3301	198+96.47	-38.50	572.01	
W3301	W3303	Ramp Running Slope	6.00	0.44	7.3%	0.5% to 8.3%		W3302	199+01.67	-38.50	572.06	
W3302	W3304	Ramp Running Slope	6.00	0.44	7.3%	0.5% to 8.3%		W3303	198+96.44	-44.50	572.45	
W3303	W3304	Ramp Cross Slope	5.00	0.05	1.0%	0.1% to 2.0%		W3304	199+01.67	-44.50	572.50	
W3303	W3305	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%		W3305	198+96.44	-49.50	572.52	
W3304	W3306	Ramp Cross Slope	5.00	0.07	1.4%	0.1% to 2.0%		W3306	199+01.67	-49.50	572.57	
W3305	W3306	Landing/Turning Space	4.97	0.05	1.0%	0.1% to 2.0%		W3307	198+91.21	-44.50	572.69	
W3303	W3307	Ramp Running Slope	5.00	0.24	4.8%	0.5% to 8.3%		W3308	198+91.21	-49.50	572.76	
W3305	W3308	Ramp Running Slope	4.97	0.24	4.8%	0.5% to 8.3%		W3309	199+06.07	-44.50	572.85	
W3307	W3308	Sidewalk Cross Slope	5.00	0.07	1.4%	0.5% to 2.0%		W3310	199+06.07	-49.50	572.92	
W3304	W3309	Ramp Running Slope	5.00	0.35	7.0%	0.5% to 8.3%						
W3306	W3310	Ramp Running Slope	4.97	0.35	7.0%	0.5% to 8.3%						
W3309	W3310	Sidewalk Cross Slope	5.00	0.07	1.4%	0.5% to 2.0%						
W3401	W3402	Crosswalk Cross Slope - Yield Condition	5.08	0.04	0.8%	0.0% to 2.0%		W3401	1602+12.74	-14.50	572.18	
W3401	W3403	Sidewalk Running Slope	11.07	0.31	2.8%	0.5% to 5.0%		W3402	1602+17.71	-14.50	572.22	
W3402	W3404	Sidewalk Running Slope	10.18	0.33	3.2%	0.5% to 5.0%		W3403	198+96.68	2.50	572.49	
W3403	W3404	Crosswalk Cross Slope - Yield Condition	5.00	0.06	1.2%	0.0% to 2.0%		W3404	199+01.67	2.50	572.55	
W3401	W3405	Sidewalk Running Slope	3.57	0.10	2.8%	0.5% to 5.0%		W3405	1602+13.36	-18.02	572.28	
W3405	W3406	Sidewalk Running Slope	5.00	0.14	2.8%	0.5% to 5.0%		W3406	1602+07.62	-19.15	572.42	
W3406	W3403	Sidewalk Running Slope	2.50	0.07	2.8%	0.5% to 5.0%		W3407	198+96.70	5.00	572.72	
W3405	W3407	Ramp Running Slope	6.00	0.44	7.3%	0.5% to 8.3%		W3408	198+90.73	5.00	572.79	
W3406	W3408	Ramp Running Slope	6.00	0.37	6.2%	0.5% to 8.3%						
W3407	W3408	Ramp Cross Slope	5.00	0.07	1.4%	0.1% to 2.0%						
W3501	W3502	Landing/Turning Space	6.17	0.08	1.3%	0.1% to 2.0%		W3501	1602+14.79	52.50	571.86	
W3502	W3503	Sidewalk Running Slope	22.81	0.57	2.5%	0.5% to 5.0%		W3502	1602+09.30	52.50	571.94	
W3502	W3505	Landing/Turning Space	5.12	0.07	1.4%	0.1% to 2.0%		W3503	4604+58.58	-35.99	572.51	
W3503	W3504	Sidewalk Cross Slope	5.04	0.07	1.4%	0.5% to 2.0%		W3504	4604+56.45	-40.55	572.58	
W3504	W3505	Sidewalk Running Slope	23.32	-0.57	-2.4%	0.5% to 5.0%		W3505	1602+03.75	52.50	572.01	
W3505	W3506	Ramp Running Slope	15.00	0.80	5.3%	0.5% to 8.3%		W3506	1601+84.02	52.50	572.81	
W3505	W3508	Landing/Turning Space	8.03	0.02	0.2%	0.1% to 2.0%		W3507	1601+84.02	44.50	572.69	
W3506	W3507	Ramp Cross Slope	8.00	-0.12	-1.5%	0.1% to 2.0%		W3508	1602+00.81	44.50	572.03	
W3507	W3508	Ramp Running Slope	14.47	-0.66	-4.6%	0.5% to 8.3%		W3509	1602+00.18	41.80	571.99	
W3508	W3509	Landing/Turning Space	3.37	-0.04	-1.2%	0.1% to 2.0%						
W3601	W3602	Sidewalk Running Slope	1.52	0.02	1.6%	0.5% to 5.0%		W3601	1603+28.71	38.50	573.20	
W3602	W3603	Ramp Running Slope	4.75	0.20	4.3%	0.5% to 8.3%		W3602	1603+28.25	39.95	573.23	
W3602	W3616	Ramp Cross Slope	5.00	-0.08	-1.5%	0.1% to 2.0%		W3603	1603+26.79	44.50	573.43	
W3603	W3604	Sidewalk Running Slope	15.23	0.59	3.9%	0.5% to 5.0%		W3604	1603+43.06	44.50	574.02	
W3603	W3606	Sidewalk Cross Slope	10.00	0.15	1.5%	0.5% to 2.0%		W3605	1603+43.06	54.50	574.17	
W3603	W3615	Landing/Turning Space	5.23	-0.08	-1.5%	0.1% to 2.0%		W3606	1603+26.79	54.50	573.58	
W3604	W3605	Sidewalk Cross Slope	10.00	0.15	1.5%	0.5% to 2.0%		W3607	1603+21.21	54.50	573.50	
W3605	W3606	Sidewalk Running Slope	15.00	-0.59	-4.0%	0.5% to 5.0%		W3608	1603+11.68	54.50	572.87	
W3606	W3607	Landing/Turning Space	5.15	-0.08	-1.5%	0.1% to 2.0%		W3609	4604+82.00	44.50	573.29	
W3607	W3608	Ramp Running Slope	14.89	-0.63	-4.3%	0.5% to 8.3%		W3610	4604+82.00	39.50	573.21	
W3607	W3615	Landing/Turning Space	10.00	-0.15	-1.5%	0.1% to 2.0%		W3611	1603+06.16	54.50	572.79	

### SIDEWALK COMPLIANCE

See S Sheets

\* Does not include curb  
① Staking required by Contracting Authority per Article 2511.03 of the Standard Specifications.

Point to Point	Sidewalk Designation	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant? ①	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES					
								Point	Station	Offset	Elevation		
		FT	FT	%	Pos. or Neg.								
W3608	W3609	Sidewalk Running Slope	15.97	0.42	2.6%	0.5% to 5.0%				W3612	1602+99.70	54.50	572.70
W3608	W3611	Landing/Turning Space	5.09	-0.08	-1.5%	0.1% to 2.0%				W3613	1603+06.64	44.50	572.87
W3608	W3614	Landing/Turning Space	10.00	0.05	0.5%	0.1% to 2.0%				W3614	1603+11.68	44.50	572.92
W3609	W3610	Sidewalk Cross Slope	5.00	-0.07	-1.5%	0.5% to 2.0%				W3615	1603+21.21	44.50	573.35
W3610	W3611	Sidewalk Running Slope	15.00	-0.42	-2.8%	0.5% to 5.0%				W3616	1603+23.18	38.50	573.15
W3611	W3612	Landing/Turning Space	5.99	-0.09	-1.5%	0.1% to 2.0%							
W3613	W3614	Landing/Turning Space	4.77	0.05	1.0%	0.1% to 2.0%							
W3614	W3615	Ramp Running Slope	15.12	0.43	2.9%	0.5% to 8.3%							
W3615	W3616	Ramp Running Slope	6.28	-0.20	-3.2%	0.5% to 8.3%							
W4101	W4102	Sidewalk Running Slope	3.49	0.14	4.0%	0.5% to 5.0%				W4101	13102+67.60	29.37	571.41
W4102	W4103	Ramp Running Slope	10.09	0.63	6.2%	0.5% to 8.3%				W4102	13102+64.31	29.37	571.55
W4102	W4107	Ramp Cross Slope	5.00	-0.06	-1.2%	0.1% to 2.0%				W4103	13102+54.83	29.35	572.18
W4103	W4104	Landing/Turning Space	5.05	0.07	1.5%	0.1% to 2.0%				W4104	13102+50.07	29.37	572.25
W4103	W4106	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%				W4105	13102+50.05	24.37	572.18
W4104	W4105	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%				W4106	13102+54.81	24.35	572.10
W4105	W4106	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%				W4107	13102+64.32	24.37	571.49
W4106	W4107	Ramp Running Slope	10.00	-0.61	-6.1%	0.5% to 8.3%							
W4201	W4202	Ramp Running Slope	12.13	0.83	6.8%	0.5% to 8.3%				W4201	13102+87.51	-25.37	571.20
W4201	W4206	Ramp Cross Slope	5.00	0.07	1.4%	0.1% to 2.0%				W4202	13102+74.73	-24.37	572.03
W4202	W4203	Landing/Turning Space	5.06	0.07	1.4%	0.1% to 2.0%				W4203	13102+69.40	-24.37	572.10
W4202	W4205	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%				W4204	13102+69.40	-29.37	572.18
W4203	W4204	Landing/Turning Space	5.00	0.07	1.5%	0.1% to 2.0%				W4205	13102+74.73	-29.37	572.10
W4204	W4205	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%				W4206	13102+87.51	-29.37	571.27
W4205	W4206	Ramp Running Slope	12.00	-0.83	-6.9%	0.5% to 8.3%				W4207	13102+91.45	-29.37	571.13
W4206	W4207	Sidewalk Running Slope	3.69	-0.14	-3.8%	0.5% to 5.0%							
W4301	W4311	Ramp Running Slope	2.46	0.15	6.0%	0.5% to 8.3%				W4301	490+25.42	-25.50	572.39
W4302	W4303	Ramp Running Slope	3.09	0.19	6.0%	0.5% to 8.3%				W4302	490+22.96	-20.50	572.46
W4302	W4311	Ramp Cross Slope	5.00	0.08	1.5%	0.1% to 2.0%				W4303	490+19.87	-20.50	572.64
W4303	W4304	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%				W4304	490+14.87	-20.50	572.72
W4303	W4310	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%				W4305	490+08.07	-20.50	572.59
W4304	W4305	Sidewalk Running Slope	6.79	-0.13	-1.9%	0.5% to 5.0%				W4306	490+05.04	-25.50	572.67
W4304	W4307	Landing/Turning Space	5.00	0.07	1.5%	0.1% to 2.0%				W4307	490+14.87	-25.50	572.79
W4306	W4307	Sidewalk Running Slope	9.83	0.13	1.3%	0.5% to 5.0%				W4308	13100+67.35	-29.37	573.10
W4307	W4308	Sidewalk Running Slope	10.00	0.30	3.0%	0.5% to 5.0%				W4309	13100+67.35	-24.37	573.02
W4307	W4310	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%				W4310	490+19.87	-25.50	572.72
W4308	W4309	Sidewalk Cross Slope	5.00	-0.07	-1.5%	0.5% to 2.0%				W4311	490+22.96	-25.50	572.53
W4309	W4310	Sidewalk Running Slope	10.00	-0.30	-3.0%	0.5% to 5.0%							
W4310	W4311	Ramp Running Slope	3.09	-0.19	-6.0%	0.5% to 8.3%							
W4401	W4402	Sidewalk Cross Slope	5.00	0.07	1.5%	0.5% to 2.0%				W4401	13100+97.98	24.37	572.88
W4401	W4413	Sidewalk Running Slope	39.56	0.20	0.5%	0.5% to 5.0%	Drainage issues were considered			W4402	13100+98.23	29.37	572.96
W4402	W4403	Sidewalk Running Slope	39.69	0.20	0.5%	0.5% to 5.0%	Drainage issues were considered			W4403	490+77.58	-25.50	573.16
W4403	W4404	Sidewalk Running Slope	6.27	-0.15	-2.3%	0.5% to 5.0%				W4404	490+83.84	-25.50	573.01
W4403	W4408	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%				W4405	490+89.61	-25.50	572.87
W4403	W4413	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%				W4406	490+99.57	-25.50	572.64
W4404	W4405	Sidewalk Running Slope	5.77	-0.13	-2.3%	0.5% to 5.0%				W4407	490+99.57	-20.50	572.57
W4405	W4406	Sidewalk Running Slope	9.96	-0.23	-2.3%	0.5% to 5.0%				W4408	490+77.58	-20.50	573.08
W4406	W4407	Sidewalk Cross Slope	5.00	-0.07	-1.5%	0.5% to 2.0%				W4409	490+72.56	-20.50	573.01
W4407	W4408	Sidewalk Running Slope	21.99	0.51	2.3%	0.5% to 5.0%				W4410	490+68.56	-20.50	572.77
W4408	W4409	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%				W4411	490+64.98	-25.50	572.78
W4409	W4410	Ramp Running Slope	4.00	-0.23	-5.8%	0.5% to 8.3%				W4412	490+68.56	-25.50	572.83
W4409	W4413	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%				W4413	490+72.56	-25.50	573.08
W4410	W4412	Landing/Turning Space	5.00	0.06	1.1%	0.1% to 2.0%							
W4411	W4412	Ramp Running Slope	3.58	0.05	1.5%	0.5% to 8.3%							
W4412	W4413	Ramp Running Slope	4.00	0.25	6.3%	0.5% to 8.3%							
W5201	W5202	Sidewalk Running Slope	46.48	-0.24	-0.5%	0.5% to 5.0%	Drainage issues were considered			W5201	1608+03.24	68.50	571.61
W5201	W5211	Sidewalk Cross Slope	10.00	0.15	1.5%	0.5% to 2.0%				W5202	1608+43.04	89.51	571.37
W5202	W5203	Ramp Running Slope	6.02	-0.44	-7.3%	0.5% to 8.3%				W5203	1608+47.83	85.87	570.93
W5202	W5206	Landing/Turning Space	8.01	0.12	1.5%	0.1% to 2.0%				W5204	1608+52.67	92.24	571.08
W5202	W5210	Landing/Turning Space	10.00	0.15	1.5%	0.1% to 2.0%				W5205	1608+52.93	92.05	571.09
W5203	W5204	Ramp Cross Slope	8.00	0.15	1.9%	0.1% to 2.0%	Yes			W5206	1608+48.15	95.68	571.49
W5204	W5205	Ramp Running Slope	0.32	0.01	3.1%	0.5% to 8.3%				W5207	1608+65.73	134.85	573.17
W5204	W5206	Ramp Running Slope	5.68	0.41	7.3%	0.5% to 8.3%				W5208	1608+56.02	137.24	573.32
W5206	W5207	Sidewalk Running Slope	43.29	1.68	3.9%	0.5% to 5.0%				W5209	1608+40.18	101.72	571.63
W5206	W5209	Landing/Turning Space	10.00	0.14	1.4%	0.1% to 2.0%				W5210	1608+35.62	96.21	571.52
W5207	W5208	Sidewalk Cross Slope	10.00	0.15	1.5%	0.5% to 2.0%				W5211	1608+03.29	78.50	571.76
W5208	W5209	Sidewalk Running Slope	39.21	-1.69	-4.3%	0.5% to 5.0%	Yes						
W5209	W5210	Landing/Turning Space	7.16	-0.11	-1.5%	0.1% to 2.0%							
W5210	W5211	Sidewalk Running Slope	38.07	0.24	0.6%	0.5% to 5.0%							
W5301	W5302	Ramp Running Slope	7.31	0.53	7.3%	0.5% to 8.3%				W5301	1608+94.39	50.55	571.90
W5301	W5310	Ramp Cross Slope	8.00	0.12	1.5%	0.1% to 2.0%				W5302	1609+00.21	46.13	572.43
W5302	W5303	Landing/Turning Space	2.69	0.04	1.5%	0.1% to 2.0%				W5303	1609+02.35	44.50	572.47

## SIDEWALK COMPLIANCE

See S Sheets

\* Does not include curb

① Staking required by Contracting Authority per Article 2511.03 of the Standard Specifications.

Point to Point	Sidewalk Designation	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant? ①	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES			
								Point	Station	Offset	Elevation
								FT	FT	%	Pos. or Neg.
W5301	W5310	Ramp Cross Slope	8.00	0.12	1.5%	0.1% to 2.0%		W5302	1609+00.21	46.13	572.43
W5302	W5303	Landing/Turning Space	2.69	0.04	1.5%	0.1% to 2.0%		W5303	1609+02.35	44.50	572.47
W5302	W5309	Landing/Turning Space	8.00	0.12	1.5%	0.1% to 2.0%		W5304	1605+05.04	44.50	572.43
W5303	W5304	Landing/Turning Space	2.69	-0.04	-1.5%	0.1% to 2.0%		W5305	1609+10.04	44.50	572.50
W5304	W5305	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%		W5306	1609+20.04	44.50	572.77
W5304	W5309	Landing/Turning Space	8.00	0.12	1.5%	0.1% to 2.0%		W5307	1609+20.04	52.50	572.89
W5305	W5306	Sidewalk Running Slope	10.00	0.26	2.6%	0.5% to 5.0%		W5308	1609+10.04	52.50	572.62
W5305	W5308	Landing/Turning Space	8.00	0.12	1.5%	0.1% to 2.0%		W5309	1609+05.04	52.50	572.55
W5306	W5307	Sidewalk Cross Slope	8.00	0.12	1.5%	0.5% to 2.0%		W5310	1608+99.22	56.92	572.02
W5307	W5308	Sidewalk Running Slope	10.00	-0.26	-2.6%	0.5% to 5.0%		W5311	1608+95.47	59.01	571.86
W5308	W5309	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%					
W5309	W5310	Ramp Running Slope	7.31	-0.53	-7.3%	0.5% to 8.3%					
W5310	W5311	Ramp Running Slope	3.45	-0.15	-4.5%	0.5% to 8.3%					
W5401	W5402	Sidewalk Running Slope	1.08	0.02	1.9%	0.5% to 5.0%		W5401	205+44.22	-52.50	571.87
W5402	W5403	Ramp Running Slope	12.00	0.86	7.2%	0.5% to 8.3%		W5402	205+45.30	-52.50	571.89
W5402	W5407	Ramp Cross Slope	8.00	0.03	0.4%	0.1% to 2.0%		W5403	205+57.30	-52.50	572.75
W5403	W5404	Landing/Turning Space	5.00	0.07	1.3%	0.1% to 2.0%		W5404	205+62.30	-52.50	572.82
W5403	W5406	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%		W5405	205+62.30	-44.50	572.70
W5404	W5405	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%		W5406	205+57.30	-44.50	572.63
W5405	W5406	Landing/Turning Space	5.00	-0.07	-1.4%	0.1% to 2.0%		W5407	205+45.30	-44.50	571.92
W5406	W5407	Ramp Running Slope	12.00	-0.71	-5.9%	0.5% to 8.3%					
W6101	W6102	Landing/Turning Space	8.00	0.12	1.5%	0.1% to 2.0%		W6101	208+13.38	-44.50	572.81
W6101	W6108	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%		W6102	208+13.38	-52.50	572.93
W6102	W6103	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%		W6103	208+18.38	-52.50	572.85
W6103	W6104	Ramp Running Slope	8.00	-0.58	-7.3%	0.5% to 8.3%		W6104	208+26.38	-52.50	572.27
W6103	W6108	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%		W6105	208+34.27	-53.86	571.69
W6104	W6105	Ramp Running Slope	8.00	-0.58	-7.3%	0.5% to 8.3%		W6106	208+35.89	-46.02	571.76
W6104	W6107	Ramp Cross Slope	8.00	-0.02	-0.3%	0.1% to 2.0%		W6107	208+27.17	-44.50	572.25
W6105	W6106	Ramp Cross Slope	8.00	0.07	0.9%	0.1% to 2.0%		W6108	208+18.38	-44.50	572.73
W6106	W6107	Ramp Running Slope	8.85	0.49	5.5%	0.5% to 8.3%					
W6107	W6108	Ramp Running Slope	8.79	0.49	5.5%	0.5% to 8.3%					
W6201	W6202	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%		W6201	1611+57.95	52.50	572.78
W6201	W6207	Landing/Turning Space	5.00	-0.07	-1.4%	0.1% to 2.0%		W6202	1611+57.95	44.50	572.66
W6202	W6203	Landing/Turning Space	8.00	-0.07	-0.9%	0.1% to 2.0%		W6203	1611+62.95	44.50	572.59
W6203	W6204	Ramp Running Slope	12.00	-0.76	-6.3%	0.5% to 8.3%		W6204	1611+74.95	44.50	571.83
W6203	W6207	Landing/Turning Space	8.00	0.12	1.5%	0.1% to 2.0%		W6205	1611+76.07	52.50	571.85
W6204	W6206	Ramp Cross Slope	8.00	0.07	0.9%	0.1% to 2.0%		W6206	1611+74.95	52.50	571.90
W6205	W6206	Ramp Running Slope	1.12	0.05	4.5%	0.5% to 8.3%		W6207	1611+62.95	52.50	572.71
W6206	W6207	Ramp Running Slope	12.00	0.81	6.8%	0.5% to 8.3%					
W6301	W6309	Landing/Turning Space	8.40	0.13	1.5%	0.1% to 2.0%		W6301	1612+44.94	52.50	571.25
W6302	W6303	Landing/Turning Space	5.00	0.04	0.8%	0.1% to 2.0%		W6302	1612+53.34	44.50	571.39
W6302	W6309	Landing/Turning Space	8.00	-0.01	-0.2%	0.1% to 2.0%		W6303	1612+58.34	44.50	571.43
W6303	W6304	Ramp Running Slope	8.00	0.59	7.3%	0.5% to 8.3%		W6304	1612+66.34	44.50	572.02
W6303	W6308	Landing/Turning Space	5.00	0.02	0.4%	0.1% to 2.0%		W6305	1612+96.34	38.50	571.89
W6304	W6307	Ramp Cross Slope	8.00	0.02	0.2%	0.1% to 2.0%		W6306	1612+96.34	46.50	572.01
W6305	W6306	Sidewalk Cross Slope	8.00	0.12	1.5%	0.5% to 2.0%		W6307	1612+66.34	52.50	572.04
W6306	W6307	Sidewalk Running Slope	30.59	0.03	0.1%	0.5% to 5.0%	Drainage issues were considered	W6308	1612+58.34	52.50	571.45
W6307	W6308	Ramp Running Slope	8.00	-0.59	-7.3%	0.5% to 8.3%		W6309	1612+53.34	52.50	571.38
W6308	W6309	Landing/Turning Space	5.00	-0.07	-1.4%	0.1% to 2.0%					
W6401	W6402	Landing/Turning Space	5.25	0.08	1.5%	0.1% to 2.0%		W6401	209+01.51	-63.20	571.43
W6402	W6403	Landing/Turning Space	6.09	0.09	1.4%	0.1% to 2.0%		W6402	209+06.72	-63.83	571.51
W6402	W6407	Landing/Turning Space	8.00	-0.11	-1.4%	0.1% to 2.0%		W6403	209+12.78	-64.50	571.60
W6403	W6404	Ramp Running Slope	8.00	0.51	6.4%	0.5% to 8.3%		W6404	209+20.78	-64.50	572.11
W6403	W6406	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%		W6405	209+20.78	-56.50	571.99
W6404	W6405	Ramp Cross Slope	8.00	-0.12	-1.5%	0.1% to 2.0%		W6406	209+12.78	-56.50	571.48
W6405	W6406	Ramp Running Slope	8.00	-0.51	-6.4%	0.5% to 8.3%		W6407	209+07.81	-55.90	571.40
W6406	W6407	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%					
W7101	W7102	Ramp Running Slope	12.10	-0.74	-6.1%	0.5% to 8.3%		W7101	107+34.35	-47.31	572.87
W7101	W7109	Sidewalk Cross Slope	8.00	-0.12	-1.5%	0.5% to 2.0%		W7102	107+32.99	-37.34	572.13
W7102	W7103	Sidewalk Running Slope	22.14	0.63	2.8%	0.5% to 5.0%		W7103	107+54.70	-33.00	572.75
W7102	W7105	Landing/Turning Space	5.01	-0.07	-1.5%	0.1% to 2.0%		W7104	107+54.21	-28.00	572.68
W7102	W7108	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%		W7105	107+32.27	-32.39	572.05
W7103	W7104	Sidewalk Cross Slope	5.00	-0.07	-1.5%	0.5% to 2.0%		W7106	107+31.37	-26.32	571.96
W7104	W7105	Sidewalk Running Slope	22.37	-0.63	-2.8%	0.5% to 5.0%		W7107	107+24.35	-33.52	572.00
W7105	W7106	Landing/Turning Space	6.14	-0.09	-1.5%	0.1% to 2.0%		W7108	107+25.07	-38.47	572.01
W7107	W7108	Landing/Turning Space	5.00	0.01	0.2%	0.1% to 2.0%		W7109	107+26.42	-48.37	572.75
W7108	W7109	Ramp Running Slope	12.00	0.74	6.2%	0.5% to 8.3%		W7110	108+50.00	-25.95	574.65
W7110	W7111	Sidewalk Running Slope	5.00	0.20	4.0%	0.5% to 5.0%		W7111	108+45.00	-25.95	574.85
W7110	W7113	Match Existing Cross Slope	3.90	0.03	0.8%	Match Existing		W7112	108+45.00	-21.89	574.79
W7111	W7112	Sidewalk Cross Slope	3.95	-0.06	-1.5%	0.5% to 2.0%		W7113	108+50.00	-21.89	574.68
W7112	W7113	Sidewalk Running Slope	5.00	-0.11	-2.2%	0.5% to 5.0%					

### SIDEWALK COMPLIANCE

See S Sheets

\* Does not include curb  
① Staking required by Contracting Authority per Article 2511.03 of the Standard Specifications.

Point to Point	Sidewalk Designation	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant? ①	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES					
								Point	Station	Offset	Elevation		
		FT	FT	%	Pos. or Neg.								
W7201	W7202	Crosswalk Cross Slope - Yield Condition	8.17	0.04	0.5%	0.0% to 2.0%			W7201	1614+92.20	46.50	572.35	
W7201	W7203	Landing/Turning Space	4.72	0.07	1.5%	0.1% to 2.0%			W7202	1614+85.75	41.50	572.39	
W7203	W7204	Landing/Turning Space	6.64	0.10	1.5%	0.1% to 2.0%			W7203	1614+87.48	46.50	572.42	
W7203	W7209	Sidewalk Running Slope	10.19	0.27	2.6%	0.5% to 5.0%			W7204	1614+80.84	46.50	572.52	
W7204	W7205	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W7205	1614+80.84	41.50	572.45	
W7204	W7206	Ramp Running Slope	8.00	0.54	6.7%	0.5% to 8.3%			W7206	1614+72.84	46.50	573.06	
W7204	W7210	Sidewalk Running Slope	12.59	0.24	1.9%	0.5% to 5.0%			W7207	1614+72.84	41.50	572.99	
W7205	W7207	Ramp Running Slope	8.00	0.54	6.7%	0.5% to 8.3%			W7208	1614+72.84	38.50	572.94	
W7206	W7207	Ramp Cross Slope	5.00	-0.07	-1.5%	0.1% to 2.0%			W7209	105+86.54	-23.25	572.69	
W7207	W7208	Sidewalk Cross Slope	3.00	-0.04	-1.5%	0.5% to 2.0%			W7210	105+84.57	-28.25	572.77	
W7209	W7210	Sidewalk Cross Slope	5.37	0.07	1.4%	0.5% to 2.0%			W7211	105+76.56	-28.25	573.04	
W7209	W7212	Sidewalk Running Slope	9.99	0.27	2.7%	0.5% to 5.0%			W7212	105+76.56	-23.25	572.96	
W7210	W7211	Sidewalk Running Slope	8.02	0.27	3.4%	0.5% to 5.0%							
W7211	W7212	Sidewalk Cross Slope	5.00	-0.07	-1.5%	0.5% to 2.0%							
W7301	W7302	Crosswalk Cross Slope - Yield Condition	8.32	0.12	1.4%	0.0% to 2.0%			W7301	1615+45.10	46.50	572.87	
W7301	W7303	Landing/Turning Space	3.89	0.04	1.0%	0.1% to 2.0%			W7302	1615+52.61	41.50	572.99	
W7303	W7304	Sidewalk Cross Slope	4.27	0.05	1.2%	0.5% to 2.0%			W7303	1615+49.37	16.50	572.91	
W7304	W7305	Landing/Turning Space	2.62	0.01	0.6%	0.1% to 2.0%			W7304	1615+54.06	46.50	572.96	
W7302	W7306	Landing/Turning Space	4.00	0.06	1.5%	0.1% to 2.0%			W7305	1615+56.93	46.50	572.98	
W7305	W7306	Ramp Cross Slope	5.00	0.07	1.5%	0.1% to 2.0%			W7306	1615+56.93	41.55	573.05	
W7305	W7307	Ramp Running Slope	12.00	0.87	7.2%	0.5% to 8.3%			W7307	1615+70.10	46.50	573.84	
W7306	W7308	Ramp Running Slope	12.12	0.72	5.9%	0.5% to 8.3%			W7308	1615+70.10	41.50	573.77	
W7307	W7308	Sidewalk Cross Slope	5.00	-0.08	-1.5%	0.5% to 2.0%			W7309	1615+70.10	38.50	573.72	
W7308	W7309	Sidewalk Cross Slope	3.00	-0.04	-1.5%	0.5% to 2.0%			W7310	105+77.88	28.25	573.33	
W7303	W7311	Sidewalk Running Slope	18.63	0.34	1.8%	0.5% to 5.0%			W7311	105+77.88	23.25	573.25	
W7304	W7310	Sidewalk Running Slope	18.40	0.37	2.0%	0.5% to 5.0%							
W7310	W7311	Sidewalk Cross Slope	5.00	-0.08	-1.5%	0.5% to 2.0%							
W7401	W7402	Landing/Turning Space	5.03	0.08	1.5%	0.1% to 2.0%			W7401	107+23.22	23.04	572.64	
W7402	W7403	Ramp Running Slope	8.00	0.54	6.8%	0.5% to 8.3%			W7402	107+22.39	28.00	572.72	
W7402	W7410	Landing/Turning Space	8.00	0.03	0.4%	0.1% to 2.0%			W7403	107+30.39	28.00	573.26	
W7402	W7405	Landing/Turning Space	5.06	0.08	1.5%	0.1% to 2.0%			W7404	107+30.39	33.00	573.34	
W7403	W7404	Ramp Cross Slope	5.00	0.08	1.5%	0.1% to 2.0%			W7405	107+21.55	33.00	572.79	
W7404	W7405	Ramp Running Slope	8.84	-0.54	-6.1%	0.5% to 8.3%			W7406	107+21.37	34.09	572.81	
W7405	W7406	Landing/Turning Space	1.10	0.02	1.5%	0.1% to 2.0%			W7407	107+18.89	48.88	573.78	
W7406	W7407	Ramp Running Slope	15.00	0.98	6.5%	0.5% to 8.3%			W7408	107+11.00	47.56	573.66	
W7406	W7409	Landing/Turning Space	8.00	-0.04	-0.4%	0.1% to 2.0%			W7409	107+13.48	32.77	572.77	
W7407	W7408	Ramp Cross Slope	8.00	-0.12	-1.5%	0.1% to 2.0%			W7410	107+14.30	27.84	572.75	
W7408	W7409	Ramp Running Slope	15.00	-0.89	-5.9%	0.5% to 8.3%			W7411	108+34.10	25.49	574.61	
W7409	W7410	Landing/Turning Space	5.00	-0.02	-0.4%	0.1% to 2.0%			W7412	108+50.00	22.43	574.90	
W7411	W7412	Sidewalk Running Slope	15.00	0.29	1.9%	0.5% to 5.0%			W7413	108+50.00	26.33	575.40	
W7411	W7414	Sidewalk Cross Slope	3.95	0.06	1.5%	0.5% to 2.0%			W7414	108+35.59	30.07	574.67	
W7412	W7413	Match Existing Cross Slope	3.90	0.50	12.8%	Match Existing							
W7413	W7414	Sidewalk Running Slope	15.00	-0.73	-4.9%	0.5% to 5.0%	Yes						
W8101	W8102	Ramp Running Slope	8.00	-0.55	-6.9%	0.5% to 8.3%			W8101	104+09.86	-36.12	574.04	
W8101	W8111	Ramp Cross Slope	8.00	-0.12	-1.5%	0.1% to 2.0%			W8102	104+09.53	-28.20	573.49	
W8102	W8103	Landing/Turning Space	2.69	-0.01	-0.3%	0.1% to 2.0%			W8103	104+09.43	-25.51	573.48	
W8102	W8110	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%			W8104	104+21.43	-25.58	574.36	
W8103	W8104	Ramp Running Slope	12.00	0.88	7.3%	0.5% to 8.3%			W8105	104+21.46	-20.58	574.33	
W8103	W8106	Landing/Turning Space	5.00	-0.01	-0.3%	0.1% to 2.0%			W8106	104+09.27	-20.51	573.46	
W8104	W8105	Ramp Cross Slope	5.00	-0.03	-0.6%	0.1% to 2.0%			W8107	104+09.13	-15.72	573.45	
W8105	W8106	Ramp Running Slope	12.00	-0.86	-7.2%	0.5% to 8.3%			W8108	104+02.30	-21.10	573.38	
W8106	W8107	Landing/Turning Space	4.79	-0.01	-0.3%	0.1% to 2.0%			W8109	103+97.73	-28.50	573.31	
W8109	W8110	Landing/Turning Space	3.81	0.06	1.5%	0.1% to 2.0%			W8110	104+01.53	-28.50	573.37	
W8110	W8111	Ramp Running Slope	8.00	0.55	6.9%	0.5% to 8.3%			W8111	104+01.87	-36.49	573.92	
W8112	W8113	Sidewalk Running Slope	5.00	0.15	3.1%	0.5% to 5.0%			W8112	104+84.02	-25.97	576.10	
W8112	W8115	Sidewalk Cross Slope	5.00	-0.08	-1.5%	0.5% to 2.0%			W8113	104+90.24	-26.01	576.25	
W8113	W8114	Sidewalk Cross Slope	5.15	-0.46	-8.9%	0.5% to 9.9%		Slope required to match existing conditions	W8114	104+89.05	-21.00	575.79	
W8114	W8115	Sidewalk Running Slope	5.00	0.23	4.6%	0.5% to 5.0%	Yes		W8115	104+84.05	-20.97	576.02	
W8201	W8210	Sidewalk Running Slope	4.80	-0.03	-0.6%	0.5% to 5.0%			W8201	103+53.80	-28.50	573.48	
W8202	W8203	Ramp Running Slope	4.83	0.04	0.9%	0.5% to 8.3%			W8202	103+49.00	-20.50	573.38	
W8203	W8204	Ramp Running Slope	3.62	-0.15	-4.3%	0.5% to 8.3%			W8203	103+44.18	-20.50	573.43	
W8203	W8207	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%			W8204	103+44.18	-16.88	573.27	
W8203	W8209	Landing/Turning Space	8.00	0.12	1.5%	0.1% to 2.0%			W8205	103+39.18	-15.00	573.17	
W8204	W8206	Ramp Cross Slope	5.00	0.01	0.2%	0.1% to 2.0%			W8206	103+39.18	-16.88	573.28	
W8205	W8206	Ramp Running Slope	1.87	0.11	6.0%	0.5% to 8.3%			W8207	103+39.18	-20.50	573.50	
W8206	W8207	Ramp Running Slope	3.62	0.22	6.0%	0.5% to 8.3%			W8208	103+39.18	-28.50	573.62	
W8207	W8208	Landing/Turning Space	8.00	0.12	1.5%	0.1% to 2.0%			W8209	103+44.18	-28.50	573.55	
W8208	W8209	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W8210	103+49.00	-28.20	573.45	
W8209	W8210	Ramp Running Slope	4.83	-0.09	-2.0%	0.5% to 8.3%							
W8301	W8302	Landing/Turning Space	6.20	-0.11	-1.8%	0.1% to 2.0%	Yes		W8301	102+76.51	-28.50	573.06	
W8302	W8303	Landing/Turning Space	5.00	0.07	1.5%	0.1% to 2.0%			W8302	102+82.71	-28.50	572.95	

**SIDEWALK COMPLIANCE**

See S Sheets

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Point to Point	Sidewalk Designation	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant? ①	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES					
								Point	Station	Offset	Elevation		
		FT	FT	%	Pos. or Neg.								
W8302	W8307	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%			W8303	102+87.71	-28.50	573.03	
W8303	W8304	Ramp Running Slope	8.00	0.50	6.2%	0.5% to 8.3%			W8304	102+95.71	-28.50	573.52	
W8303	W8306	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%			W8305	102+95.71	-20.50	573.40	
W8304	W8305	Ramp Cross Slope	8.00	-0.12	-1.5%	0.1% to 2.0%			W8306	102+87.71	-20.50	572.91	
W8305	W8306	Ramp Running Slope	8.00	-0.50	-6.2%	0.5% to 8.3%			W8307	102+82.71	-20.50	572.83	
W8306	W8307	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%							
W8401	W8402	Sidewalk Cross Slope	5.00	0.07	1.5%	0.5% to 2.0%			W8401	102+29.27	20.50	573.83	
W8401	W8411	Sidewalk Running Slope	10.00	-0.06	-0.6%	0.5% to 5.0%			W8402	102+29.27	25.50	573.91	
W8402	W8403	Sidewalk Running Slope	10.00	-0.06	-0.6%	0.5% to 5.0%			W8403	102+19.27	25.50	573.85	
W8403	W8404	Landing/Turning Space	10.13	-0.15	-1.5%	0.1% to 2.0%			W8404	102+09.13	25.50	573.70	
W8403	W8405	Sidewalk Running Slope	21.22	0.41	2.0%	0.5% to 5.0%			W8405	102+06.24	42.25	574.26	
W8403	W8411	Landing/Turning Space	5.00	-0.07	-1.4%	0.1% to 2.0%			W8406	101+99.93	37.34	574.14	
W8404	W8406	Sidewalk Running Slope	15.00	0.44	3.0%	0.5% to 5.0%			W8407	102+06.07	25.50	573.48	
W8404	W8407	Ramp Running Slope	3.07	-0.22	-7.3%	0.5% to 8.3%			W8408	102+01.53	25.50	573.35	
W8404	W8410	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%			W8409	102+06.07	20.50	573.40	
W8405	W8406	Sidewalk Cross Slope	8.00	-0.12	-1.5%	0.5% to 2.0%			W8410	102+09.13	20.50	573.62	
W8407	W8408	Ramp Running Slope	4.53	-0.13	-2.8%	0.5% to 8.3%			W8411	102+19.27	20.50	573.78	
W8407	W8409	Ramp Cross Slope	5.00	-0.08	-1.5%	0.1% to 2.0%							
W8409	W8410	Ramp Running Slope	3.07	0.22	7.3%	0.5% to 8.3%							
W8410	W8411	Landing/Turning Space	10.13	0.15	1.5%	0.1% to 2.0%							
W8501	W8502	Ramp Running Slope	3.24	0.18	5.6%	0.5% to 8.3%			W8501	103+44.18	19.39	573.38	
W8501	W8512	Ramp Cross Slope	5.00	-0.01	-0.2%	0.1% to 2.0%			W8502	103+44.18	22.63	573.56	
W8502	W8503	Ramp Running Slope	3.62	-0.06	-1.7%	0.5% to 8.3%			W8503	103+47.79	22.65	573.50	
W8502	W8507	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%			W8504	103+51.35	27.67	573.64	
W8502	W8511	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%			W8505	103+47.77	27.65	573.58	
W8503	W8505	Ramp Cross Slope	5.00	0.08	1.5%	0.1% to 2.0%			W8506	103+49.37	40.69	574.45	
W8504	W8505	Sidewalk Running Slope	3.58	-0.06	-1.8%	0.5% to 5.0%			W8507	103+44.18	27.64	573.63	
W8505	W8507	Ramp Running Slope	3.59	0.05	1.5%	0.5% to 8.3%			W8508	103+39.18	27.61	573.71	
W8506	W8508	Sidewalk Running Slope	16.58	-0.75	-4.5%	0.5% to 5.0%	Yes		W8509	103+28.14	25.50	573.75	
W8507	W8508	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%			W8510	103+28.64	20.50	573.67	
W8508	W8509	Sidewalk Running Slope	11.24	0.04	0.4%	0.5% to 5.0%		Drainage issues were considered	W8511	103+39.18	22.61	573.63	
W8508	W8511	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W8512	103+39.18	19.39	573.37	
W8509	W8510	Sidewalk Cross Slope	5.02	-0.08	-1.5%	0.5% to 2.0%			W8513	103+39.18	16.52	573.24	
W8510	W8511	Sidewalk Running Slope	10.75	-0.04	-0.4%	0.5% to 5.0%		Drainage issues were considered					
W8511	W8512	Ramp Running Slope	3.22	-0.26	-8.1%	0.5% to 8.3%	Yes						
W8512	W8513	Ramp Running Slope	2.87	-0.13	-4.5%	0.5% to 8.3%							
W8601	W8602	Sidewalk Running Slope	2.75	0.08	2.9%	0.5% to 5.0%			W8601	104+10.12	15.05	573.65	
W8602	W8603	Ramp Running Slope	5.20	0.31	6.0%	0.5% to 8.3%			W8602	104+10.21	17.79	573.73	
W8602	W8610	Sidewalk Cross Slope	5.00	-0.03	-0.6%	0.5% to 2.0%			W8603	104+10.38	22.99	574.04	
W8603	W8604	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%			W8604	104+10.54	27.99	574.12	
W8603	W8609	Landing/Turning Space	5.19	-0.08	-1.5%	0.1% to 2.0%			W8605	104+05.35	27.96	574.04	
W8604	W8605	Landing/Turning Space	5.19	-0.08	-1.5%	0.1% to 2.0%			W8606	104+00.24	27.94	573.74	
W8605	W8606	Ramp Running Slope	5.10	-0.29	-5.8%	0.5% to 8.3%			W8607	103+97.38	27.92	573.65	
W8605	W8609	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%			W8608	104+00.27	22.94	573.71	
W8606	W8607	Sidewalk Running Slope	2.86	-0.09	-3.1%	0.5% to 5.0%			W8609	104+05.37	22.96	573.96	
W8606	W8608	Sidewalk Cross Slope	5.00	-0.03	-0.6%	0.5% to 2.0%			W8610	104+05.21	17.95	573.70	
W8608	W8609	Ramp Running Slope	5.10	0.25	4.9%	0.5% to 8.3%							
W8609	W8610	Ramp Running Slope	5.01	-0.26	-5.2%	0.5% to 8.3%							
W9101	W9102	Ramp Running Slope	3.00	-0.02	-0.6%	0.5% to 8.3%		Drainage issues were considered	W9101	1619+59.43	38.50	573.73	
W9101	W9111	Ramp Cross Slope	8.00	0.12	1.5%	0.1% to 2.0%			W9102	1619+62.43	38.50	573.71	
W9103	W9110	Landing/Turning Space	4.23	0.04	0.9%	0.1% to 2.0%			W9103	1619+67.43	38.50	573.25	
W9104	W9105	Landing/Turning Space	7.49	0.11	1.5%	0.1% to 2.0%			W9104	101+65.73	-16.00	573.21	
W9105	W9106	Ramp Running Slope	8.00	0.48	6.0%	0.5% to 8.3%			W9105	101+69.78	-21.00	573.32	
W9105	W9108	Landing/Turning Space	6.30	0.09	1.5%	0.1% to 2.0%			W9106	101+62.43	-21.00	573.80	
W9106	W9107	Ramp Cross Slope	5.00	0.08	1.5%	0.1% to 2.0%			W9107	101+62.43	-26.00	573.88	
W9107	W9108	Ramp Running Slope	11.83	-0.46	-3.9%	0.5% to 8.3%			W9108	1619+77.49	50.97	573.41	
W9108	W9109	Landing/Turning Space	5.75	-0.05	-0.9%	0.1% to 2.0%			W9109	1619+73.88	46.50	573.36	
W9109	W9110	Landing/Turning Space	6.04	-0.07	-1.2%	0.1% to 2.0%			W9110	1619+70.09	41.80	573.29	
W9109	W9111	Ramp Running Slope	14.45	0.49	3.4%	0.5% to 8.3%			W9111	1619+59.43	46.50	573.85	
W9201	W9214	Landing/Turning Space	6.40	0.09	1.5%	0.1% to 2.0%			W9201	1620+50.57	52.45	572.87	
W9202	W9203	Landing/Turning Space	5.00	0.06	1.3%	0.1% to 2.0%			W9202	1620+56.80	44.45	572.86	
W9202	W9214	Landing/Turning Space	8.00	0.11	1.3%	0.1% to 2.0%			W9203	1620+61.80	44.45	572.92	
W9203	W9204	Ramp Running Slope	8.00	0.50	6.2%	0.5% to 8.3%			W9204	1620+69.76	44.45	573.42	
W9203	W9213	Landing/Turning Space	8.00	0.12	1.5%	0.1% to 2.0%			W9205	1620+77.47	44.86	573.66	
W9204	W9205	Sidewalk Running Slope	7.74	0.25	3.2%	0.5% to 5.0%			W9206	1620+76.48	52.79	573.78	
W9204	W9207	Landing/Turning Space	8.00	0.12	1.5%	0.1% to 2.0%			W9207	1620+69.76	52.45	573.54	
W9205	W9206	Landing/Turning Space	8.00	0.12	1.5%	0.1% to 2.0%			W9208	101+14.14	26.00	573.32	
W9206	W9207	Sidewalk Running Slope	6.74	-0.25	-3.7%	0.5% to 5.0%			W9209	101+09.10	26.00	573.40	
W9206	W9209	Sidewalk Running Slope	23.67	-0.38	-1.6%	0.5% to 5.0%			W9210	101+09.10	21.00	573.32	
W9207	W9208	Sidewalk Running Slope	18.54	-0.21	-1.1%	0.5% to 5.0%			W9211	101+14.14	21.00	573.25	
W9207	W9213	Ramp Running Slope	8.00	-0.50	-6.2%	0.5% to 8.3%			W9212	101+20.87	26.00	572.90	
W9208	W9209	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%			W9213	1620+61.80	54.45	573.04	

**SIDEWALK COMPLIANCE**

See S Sheets

\* Does not include curb

① Staking required by Contracting Authority per Article 2511.03 of the Standard Specifications.

Point to Point	Sidewalk Designation	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant? ①	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES			
								Point	Station	Offset	Elevation
								FT	FT	%	Pos. or Neg.
W9208 W9211	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%			W9214	1620+56.80	52.45	572.96
W9208 W9212	Ramp Running Slope	8.00	-0.42	-5.3%	0.5% to 8.3%			W9215	101+20.87	21.00	572.83
W9209 W9210	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W9216	101+25.87	26.00	572.85
W9210 W9211	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%						
W9211 W9215	Ramp Running Slope	8.00	-0.42	-5.3%	0.5% to 8.3%						
W9212 W9215	Ramp Cross Slope	5.00	-0.08	-1.5%	0.1% to 2.0%						
W9212 W9216	Sidewalk Running Slope	4.46	-0.05	-1.1%	0.5% to 5.0%						
W9213 W9214	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%						
W9301 W9302	Landing/Turning Space	5.00	-0.07	-1.4%	0.1% to 2.0%			W9301	1620+53.54	-16.50	574.10
W9301 W9307	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%			W9302	1620+58.57	-16.50	574.03
W9302 W9303	Ramp Running Slope	8.00	-0.41	-5.1%	0.5% to 8.3%			W9303	1620+66.54	-16.50	573.62
W9302 W9306	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%			W9304	1620+67.37	-16.50	573.61
W9303 W9304	Sidewalk Running Slope	0.83	-0.01	-1.2%	0.5% to 5.0%			W9305	1620+66.54	-8.50	573.50
W9303 W9305	Ramp Cross Slope	8.00	-0.12	-1.5%	0.1% to 2.0%			W9306	1620+58.54	-8.50	573.91
W9305 W9306	Ramp Running Slope	8.00	0.41	5.1%	0.5% to 8.3%			W9307	1620+53.54	-8.50	573.98
W9306 W9307	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%						
W9401 W9402	Sidewalk Running Slope	0.87	0.01	1.1%	0.5% to 5.0%			W9401	1620+92.62	-16.50	573.51
W9402 W9403	Ramp Running Slope	8.00	0.30	3.7%	0.5% to 8.3%			W9402	1620+93.49	-16.50	573.52
W9402 W9407	Ramp Cross Slope	8.00	-0.12	-1.5%	0.1% to 2.0%			W9403	1621+01.71	-16.50	573.82
W9403 W9404	Landing/Turning Space	5.00	0.07	1.5%	0.1% to 2.0%			W9404	1621+06.87	-16.50	573.89
W9403 W9406	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%			W9405	1621+06.68	-8.50	573.77
W9404 W9405	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%			W9406	1621+01.60	-8.50	573.70
W9405 W9406	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%			W9407	1620+93.49	-8.50	573.40
W9406 W9407	Ramp Running Slope	8.00	-0.30	-3.7%	0.5% to 8.3%						
W9501 W9502	Ramp Running Slope	8.01	-0.61	-7.6%	0.5% to 8.3%	Yes		W9501	1623+03.22	56.50	573.37
W9501 W9506	Landing/Turning Space	8.00	0.12	1.5%	0.1% to 2.0%			W9502	1623+10.44	56.50	572.76
W9501 W9514	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%			W9503	1623+13.39	64.50	572.81
W9502 W9504	Ramp Cross Slope	8.00	0.12	1.5%	0.1% to 2.0%			W9504	1623+10.44	64.50	572.88
W9503 W9504	Sidewalk Running Slope	3.31	0.07	2.1%	0.5% to 5.0%			W9505	1623+03.22	72.96	573.85
W9504 W9506	Ramp Running Slope	8.11	0.60	7.4%	0.5% to 8.3%	Yes		W9506	1623+03.22	64.50	573.48
W9505 W9506	Sidewalk Running Slope	8.46	-0.37	-4.3%	0.5% to 5.0%	Yes		W9507	1623+17.04	78.62	573.51
W9505 W9510	Sidewalk Cross Slope	5.93	0.09	1.5%	0.5% to 2.0%			W9508	1623+11.37	87.33	573.73
W9505 W9512	Sidewalk Cross Slope	5.41	0.08	1.5%	0.5% to 2.0%			W9509	1623+10.06	82.04	573.64
W9506 W9513	Landing/Turning Space	5.07	0.08	1.5%	0.1% to 2.0%			W9510	1623+04.64	78.66	573.94
W9507 W9509	Sidewalk Cross Slope	8.68	0.13	1.5%	0.5% to 2.0%			W9511	1623+00.26	80.25	574.16
W9508 W9509	Sidewalk Running Slope	5.50	-0.09	-1.6%	0.5% to 5.0%			W9512	1622+98.71	74.61	573.93
W9509 W9510	Sidewalk Running Slope	7.90	0.30	3.8%	0.5% to 5.0%			W9513	1622+98.71	64.50	573.56
W9510 W9511	Sidewalk Running Slope	5.29	0.22	4.2%	0.5% to 5.0%	Yes	Slope required to match existing conditions	W9514	1622+98.71	56.50	573.44
W9511 W9512	Sidewalk Cross Slope	5.91	-0.23	-3.9%	0.5% to 4.9%						
W9512 W9513	Sidewalk Running Slope	10.11	-0.37	-3.7%	0.5% to 5.0%						
W9513 W9514	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%						
W9601 W9602	Ramp Running Slope	12.00	0.78	6.5%	0.5% to 8.3%			W9601	1623+40.56	56.50	572.81
W9601 W9606	Ramp Cross Slope	8.00	0.12	1.5%	0.1% to 2.0%			W9602	1623+51.40	56.50	573.59
W9602 W9603	Landing/Turning Space	5.00	0.07	1.5%	0.1% to 2.0%			W9603	1623+55.91	56.50	573.67
W9602 W9605	Landing/Turning Space	8.00	0.12	1.5%	0.1% to 2.0%			W9604	1623+55.93	64.50	573.79
W9603 W9604	Landing/Turning Space	8.00	0.12	1.5%	0.1% to 2.0%			W9605	1623+51.40	64.50	573.71
W9604 W9605	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W9606	1623+40.56	64.50	572.93
W9605 W9606	Ramp Running Slope	12.17	-0.78	-6.4%	0.5% to 8.3%			W9607	1623+38.00	64.50	572.91
W9606 W9607	Ramp Running Slope	2.88	-0.02	-0.7%	0.5% to 8.3%						
W9701 W9702	Landing/Turning Space	10.48	-0.08	-0.8%	0.1% to 2.0%			W9701	1623+96.85	64.50	573.94
W9702 W9703	Landing/Turning Space	5.07	0.08	1.5%	0.1% to 2.0%			W9702	1623+87.52	64.50	573.86
W9702 W9707	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%			W9703	1623+83.00	64.50	573.94
W9703 W9704	Ramp Running Slope	8.11	0.14	1.8%	0.5% to 8.3%			W9704	1623+75.78	64.50	574.08
W9703 W9706	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%			W9705	1623+75.78	56.50	573.96
W9704 W9705	Ramp Cross Slope	8.00	-0.12	-1.5%	0.1% to 2.0%			W9706	1623+83.00	56.50	573.82
W9705 W9706	Ramp Running Slope	8.00	-0.14	-1.8%	0.5% to 8.3%			W9707	1623+87.52	56.50	573.74
W9706 W9707	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%						
W9801 W9802	Sidewalk Running Slope	5.00	0.18	3.6%	0.5% to 5.0%			W9801	1624+09.05	-37.02	573.92
W9801 W9814	Sidewalk Cross Slope	6.28	0.29	4.6%	0.5% to 5.6%		Slope required to match existing conditions	W9802	1624+09.40	-32.03	574.10
W9802 W9803	Sidewalk Running Slope	7.55	0.10	1.3%	0.5% to 5.0%			W9803	1623+09.93	-24.50	574.20
W9802 W9813	Sidewalk Cross Slope	5.00	0.07	1.5%	0.5% to 2.0%			W9804	1624+08.91	-16.50	573.86
W9803 W9811	Ramp Cross Slope	8.25	0.12	1.5%	0.1% to 2.0%			W9805	1624+03.81	-16.50	573.93
W9804 W9805	Landing/Turning Space	4.94	0.07	1.5%	0.1% to 2.0%			W9806	1624+03.81	-8.50	573.96
W9805 W9806	Landing/Turning Space	8.00	0.03	0.3%	0.1% to 2.0%			W9807	1623+98.73	-8.50	574.00
W9806 W9807	Landing/Turning Space	5.00	0.04	0.8%	0.1% to 2.0%			W9808	1623+88.56	-8.50	574.54
W9807 W9808	Ramp Running Slope	10.00	0.54	5.4%	0.5% to 8.3%			W9809	1623+88.56	-16.50	574.66
W9807 W9810	Ramp Cross Slope	8.00	0.04	0.5%	0.1% to 2.0%			W9810	1623+98.73	-16.50	574.04
W9808 W9809	Ramp Cross Slope	8.00	0.12	1.5%	0.1% to 2.0%			W9811	1624+01.27	-24.50	574.32
W9809 W9810	Ramp Running Slope	9.85	-0.62	-6.3%	0.5% to 8.3%			W9812	1624+02.85	-29.33	574.22
W9810 W9811	Ramp Running Slope	8.37	0.28	3.3%	0.5% to 8.3%			W9813	1624+02.68	-31.57	574.18
W9811 W9812	Sidewalk Running Slope	5.05	-0.10	-2.0%	0.5% to 5.0%			W9814	1624+02.31	-36.56	574.21





### SIDEWALK COMPLIANCE

See S Sheets

\* Does not include curb  
 ① Staking required by Contracting Authority per Article 2511.03 of the Standard Specifications.

Point to Point	Sidewalk Designation	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant? ①	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES					
								Point	Station	Offset	Elevation		
		FT	FT	%	Pos. or Neg.								
W10504	W10506	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%			W10507	4607+60.26	-88.79	583.06	
W10505	W10507	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%			W10508	4607+62.74	-95.12	583.12	
W10505	W10507	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%			W10509	4607+64.03	-90.40	583.29	
W10506	W10508	Sidewalk Running Slope	5.00	0.13	2.6%	0.5% to 5.0%							
W10507	W10509	Sidewalk Running Slope	5.00	0.23	4.6%	0.5% to 5.0%	Yes						
W10508	W10509	Sidewalk Cross Slope	5.00	0.17	3.4%	0.5% to 4.4%		Slope required to match existing conditions					
W10601	W10602	Crosswalk Cross Slope - No Yield Condition	5.00	0.15	3.0%	0.0% to 5.0%			W10601	4607+39.17	-79.53	582.21	
W10601	W10603	Ramp Running Slope	15.00	-1.84	-12.3%	0.5% to 13.3%		Length constructed must exceed 15 feet at a uniform running slope	W10602	4607+38.02	-84.34	582.36	
W10602	W10604	Ramp Running Slope	15.00	-1.91	-12.8%	0.5% to 13.8%		Length constructed must exceed 15 feet at a uniform running slope	W10603	4607+24.92	-75.53	580.37	
W10603	W10604	Ramp Cross Slope	5.00	0.08	1.5%	0.1% to 2.0%			W10604	4607+23.56	-80.34	580.45	
W10603	W10605	Landing/Turning Space	5.00	-0.10	-2.0%	0.1% to 2.0%	Yes	Proposed slope is maximum allowed	W10605	4607+20.10	-74.19	580.27	
W10604	W10606	Landing/Turning Space	5.00	-0.10	-2.0%	0.1% to 2.0%	Yes	Proposed slope is maximum allowed	W10606	4607+18.74	-79.00	580.35	
W10605	W10607	Landing/Turning Space	0.38	0.00	1.3%	0.1% to 2.0%			W10607	4607+20.00	-74.56	580.28	
W10606	W10608	Landing/Turning Space	0.91	-0.01	-1.5%	0.1% to 2.0%			W10608	4607+18.99	-78.13	580.33	
W10607	W10608	Sidewalk Cross Slope	3.71	0.06	1.5%	0.5% to 2.0%			W10609	4607+15.22	-73.07	580.04	
W10607	W10609	Sidewalk Running Slope	5.00	-0.24	-4.7%	0.5% to 5.0%	Yes		W10610	4607+14.21	-76.68	580.18	
W10608	W10610	Sidewalk Running Slope	5.00	-0.15	-3.0%	0.5% to 5.0%							
W10609	W10610	Sidewalk Cross Slope	3.75	0.14	3.7%	0.5% to 4.7%		Slope required to match existing conditions					
W11101	W11102	Ramp Running Slope	6.00	0.43	7.2%	0.5% to 8.3%			W11101	8+70.48	-20.50	583.32	
W11101	W11107	Sidewalk Cross Slope	6.70	0.09	1.3%	0.5% to 2.0%			W11102	8+76.48	-20.50	583.75	
W11101	W11106	Ramp Cross Slope	5.00	0.06	1.3%	0.1% to 2.0%			W11103	8+81.48	-20.50	583.81	
W11102	W11103	Landing/Turning Space	5.00	0.06	1.2%	0.1% to 2.0%			W11104	8+81.48	-25.50	583.88	
W11102	W11105	Ramp Cross Slope	5.00	0.07	1.4%	0.1% to 2.0%			W11105	8+76.48	-25.50	583.82	
W11103	W11104	Sidewalk Cross Slope	5.00	0.07	1.4%	0.5% to 2.0%			W11106	8+70.48	-25.50	583.39	
W11105	W11104	Landing/Turning Space	5.00	0.06	1.2%	0.1% to 2.0%			W11107	8+66.01	-25.50	583.41	
W11106	W11105	Ramp Running Slope	6.00	0.44	7.3%	0.5% to 8.3%							
W11107	W11106	Sidewalk Running Slope	4.46	-0.02	-0.6%	0.5% to 5.0%							
W11201	W11202	Sidewalk Running Slope	4.45	0.01	0.3%	0.5% to 5.0%		Drainage issues were considered	W11201	8+57.82	20.07	583.10	
W11202	W11203	Ramp Running Slope	10.00	0.64	6.4%	0.5% to 8.3%			W11202	8+61.37	20.25	583.12	
W11202	W11209	Ramp Cross Slope	5.00	-0.08	-1.5%	0.1% to 2.0%			W11203	8+71.35	20.92	583.76	
W11203	W11204	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W11204	8+76.34	21.26	583.68	
W11203	W11208	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W11205	8+81.33	21.59	583.65	
W11204	W11205	Sidewalk Running Slope	5.00	-0.03	-0.6%	0.5% to 5.0%			W11206	8+81.66	16.60	583.58	
W11204	W11207	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%			W11207	8+76.68	16.27	583.61	
W11205	W11206	Sidewalk Cross Slope	5.00	-0.07	-1.5%	0.5% to 2.0%			W11208	8+71.69	15.93	583.68	
W11206	W11207	Sidewalk Running Slope	5.00	0.03	0.6%	0.5% to 5.0%			W11209	8+61.71	15.27	583.04	
W11207	W11208	Landing/Turning Space	5.00	0.07	1.5%	0.1% to 2.0%							
W11208	W11209	Ramp Running Slope	10.00	-0.64	-6.4%	0.5% to 8.3%							
W11301	W11314	Crosswalk Cross Slope - Yield Condition	6.00	0.02	0.3%	0.0% to 2.0%			W11301	11+60.72	23.50	578.10	
W11314	W11313	Sidewalk Running Slope	3.24	0.03	0.9%	0.5% to 5.0%			W11302	11+57.91	23.50	578.28	
W11301	W11313	Ramp Cross Slope	5.00	0.05	1.0%	0.1% to 2.0%			W11303	11+57.98	20.69	578.08	
W11301	W11302	Ramp Running Slope	2.81	0.18	6.4%	0.5% to 8.3%			W11304	11+53.06	17.33	578.07	
W11313	W11312	Ramp Running Slope	2.93	0.20	6.7%	0.5% to 8.3%			W11305	11+52.98	20.57	578.11	
W11302	W11312	Ramp Cross Slope	5.00	0.07	1.3%	0.1% to 2.0%			W11306	11+52.91	23.50	578.33	
W11304	W11303	Crosswalk Cross Slope - Yield Condition	6.00	0.01	0.2%	0.0% to 2.0%			W11307	11+37.98	20.50	578.77	
W11304	W11305	Sidewalk Running Slope	3.24	0.04	1.2%	0.5% to 5.0%			W11308	11+37.98	25.50	578.84	
W11305	W11303	Ramp Cross Slope	5.00	-0.03	-0.6%	0.1% to 2.0%			W11309	11+52.79	58.50	578.40	
W11305	W11306	Ramp Running Slope	2.93	0.22	7.3%	0.5% to 8.3%			W11310	4613+99.68	-33.50	578.96	
W11306	W11302	Ramp Cross Slope	5.00	-0.05	-0.9%	0.1% to 2.0%			W11311	4613+99.68	-28.50	578.89	
W11303	W11302	Ramp Running Slope	2.81	0.20	7.1%	0.5% to 8.3%			W11312	11+57.79	28.50	578.35	
W11306	W11309	Landing/Turning Space	5.00	0.07	1.4%	0.1% to 2.0%			W11313	11+60.72	28.50	578.15	
W11306	W11307	Sidewalk Running Slope	15.23	0.44	2.9%	0.5% to 5.0%			W11314	11+63.96	28.50	578.12	
W11309	W11308	Sidewalk Running Slope	15.23	0.45	2.9%	0.5% to 5.0%							
W11307	W11308	Sidewalk Cross Slope	5.00	0.07	1.4%	0.5% to 2.0%							
W11309	W11312	Landing/Turning Space	5.00	-0.05	-1.0%	0.1% to 2.0%							
W11309	W11310	Sidewalk Running Slope	15.11	0.57	3.7%	0.5% to 5.0%							
W11312	W11311	Sidewalk Running Slope	15.23	0.54	3.6%	0.5% to 5.0%							
W11310	W11311	Sidewalk Cross Slope	5.00	-0.07	-1.4%	0.5% to 2.0%							
W11401	W11414	Crosswalk Cross Slope - Yield Condition	6.00	-0.08	-1.3%	0.0% to 2.0%			W11401	12+15.67	28.50	577.99	
W11401	W11402	Sidewalk Running Slope	3.24	-0.02	-0.6%	0.5% to 5.0%			W11402	12+18.91	28.50	577.97	
W11402	W11414	Ramp Cross Slope	5.00	-0.06	-1.2%	0.1% to 2.0%			W11403	12+21.95	28.50	578.13	
W11402	W11403	Ramp Running Slope	3.04	0.16	5.3%	0.5% to 8.3%			W11404	4613+99.96	28.50	578.70	
W11414	W11413	Ramp Running Slope	2.96	0.15	5.0%	0.5% to 8.3%			W11405	4613+99.96	33.50	578.77	
W11403	W11413	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%			W11406	12+26.95	28.50	578.21	
W11403	W11404	Sidewalk Running Slope	15.23	0.56	3.7%	0.5% to 5.0%			W11407	12+41.83	25.50	578.34	
W11406	W11405	Sidewalk Running Slope	15.18	0.57	3.7%	0.5% to 5.0%			W11408	12+41.83	20.50	578.26	
W11404	W11405	Sidewalk Cross Slope	5.00	0.08	1.5%	0.5% to 2.0%			W11409	12+26.87	23.50	578.13	
W11403	W11406	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%			W11410	12+26.83	20.46	577.92	
W11406	W11409	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W11411	12+26.78	17.22	577.75	
W11406	W11407	Sidewalk Running Slope	15.18	0.13	0.9%	0.5% to 5.0%			W11412	12+21.83	20.54	577.84	

### SIDEWALK COMPLIANCE

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Point to Point	Sidewalk Designation	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant? ①	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES			
								Point	Station	Offset	Elevation
								FT	FT	%	Pos. or Neg.
W11409	W11408	Sidewalk Running Slope	15.23	0.13	0.9%	0.5% to 5.0%		W11413	12+21.87	23.50	578.06
W11407	W11408	Sidewalk Cross Slope	5.00	-0.08	-1.5%	0.5% to 2.0%		W11414	12+18.91	23.50	577.91
W11411	W11412	Sidewalk Cross Slope	6.00	0.09	1.5%	0.5% to 2.0%		W11415	4613+94.88	28.50	578.88
W11411	W11410	Ramp Running Slope	3.24	0.16	5.1%	0.5% to 8.3%		W11416	4613+94.88	33.50	578.96
W11410	W11412	Ramp Cross Slope	5.00	-0.07	-1.5%	0.1% to 2.0%					
W11410	W11409	Ramp Running Slope	3.40	0.22	6.4%	0.5% to 8.3%					
W11412	W11413	Ramp Running Slope	3.00	0.22	7.2%	0.5% to 8.3%					
W11409	W11413	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%					
W11404	W11415	Sidewalk Running Slope	5.00	0.19	3.8%	0.5% to 5.0%					
W11405	W11416	Sidewalk Running Slope	5.00	0.19	3.7%	0.5% to 5.0%					
W11415	W11416	Sidewalk Cross Slope	5.00	0.07	1.5%	0.5% to 2.0%					
W11501	W11512	Crosswalk Cross Slope - No Yield Condition	6.00	0.12	1.9%	0.0% to 5.0%		W11501	12+17.69	-23.50	578.51
W11501	W11511	Ramp Cross Slope	5.00	0.08	1.5%	0.1% to 2.0%		W11502	12+21.14	-23.50	578.73
W11512	W11511	Sidewalk Running Slope	3.24	-0.04	-1.2%	0.5% to 5.0%		W11503	12+21.20	-20.04	578.48
W11501	W11502	Ramp Running Slope	3.46	0.22	6.3%	0.5% to 8.3%		W11504	12+26.25	-16.88	578.29
W11511	W11510	Ramp Running Slope	3.46	0.22	6.3%	0.5% to 8.3%		W11505	12+26.20	-20.12	578.41
W11502	W11510	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%		W11506	12+26.14	-23.50	578.65
W11503	W11502	Ramp Running Slope	3.46	0.25	7.2%	0.5% to 8.3%		W11507	12+41.22	-20.50	578.54
W11504	W11505	Sidewalk Running Slope	3.24	0.11	3.5%	0.5% to 5.0%		W11508	12+41.22	-25.50	578.61
W11503	W11505	Ramp Cross Slope	5.00	-0.08	-1.5%	0.1% to 2.0%		W11509	12+26.14	-28.50	578.73
W11505	W11506	Ramp Running Slope	3.38	0.25	7.3%	0.5% to 8.3%		W11510	12+21.14	-28.50	578.80
W11502	W11506	Ramp Cross Slope	5.00	-0.07	-1.5%	0.1% to 2.0%		W11511	12+17.69	-28.50	578.59
W11506	W11509	Landing/Turning Space	5.00	0.07	1.5%	0.1% to 2.0%		W11512	12+14.45	-28.50	578.63
W11510	W11509	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%					
W11509	W11508	Sidewalk Running Slope	15.37	-0.11	-0.7%	0.5% to 5.0%					
W11506	W11507	Sidewalk Running Slope	15.37	-0.12	-0.7%	0.5% to 5.0%					
W11507	W11508	Sidewalk Cross Slope	5.00	0.08	1.5%	0.5% to 2.0%					
W11601	W11612	Sidewalk Cross Slope	6.00	-0.08	-1.3%	0.5% to 2.0%		W11601	11+65.92	-28.50	578.76
W11601	W11602	Sidewalk Running Slope	3.24	-0.03	-0.8%	0.5% to 5.0%		W11602	11+62.68	-28.50	578.74
W11602	W11612	Ramp Cross Slope	5.00	-0.05	-1.0%	0.1% to 2.0%		W11603	11+59.08	-28.50	578.99
W11602	W11603	Ramp Running Slope	3.60	0.26	7.1%	0.5% to 8.3%		W11604	11+54.08	-28.50	578.92
W11612	W11611	Ramp Running Slope	3.60	0.23	6.4%	0.5% to 8.3%		W11605	11+38.96	-25.50	579.08
W11603	W11611	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%		W11606	11+38.96	-20.50	579.00
W11603	W11604	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%		W11607	11+54.08	-23.50	578.84
W11604	W11607	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%		W11608	11+53.99	-20.02	578.59
W11604	W11605	Sidewalk Running Slope	15.41	0.16	1.0%	0.5% to 5.0%		W11609	11+53.91	-16.78	578.55
W11607	W11606	Sidewalk Running Slope	15.41	0.16	1.0%	0.5% to 5.0%		W11610	11+58.99	-19.90	578.66
W11605	W11606	Sidewalk Cross Slope	5.00	-0.08	-1.5%	0.5% to 2.0%		W11611	11+59.08	-23.50	578.92
W11609	W11610	Crosswalk Cross Slope - Yield Condition	6.00	0.11	1.9%	0.0% to 2.0%		W11612	11+62.68	-23.50	578.69
W11609	W11608	Sidewalk Running Slope	3.24	0.04	1.2%	0.5% to 5.0%					
W11608	W11610	Ramp Cross Slope	5.00	0.07	1.5%	0.1% to 2.0%					
W11608	W11607	Ramp Running Slope	3.48	0.25	7.3%	0.5% to 8.3%					
W11610	W11611	Ramp Running Slope	3.60	0.26	7.1%	0.5% to 8.3%					
W11607	W11611	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%					
W12101	W12102	Sidewalk Running Slope	4.17	0.06	1.4%	0.5% to 5.0%		W12101	4610+99.02	28.50	583.76
W12102	W12103	Ramp Running Slope	4.93	0.18	3.8%	0.5% to 8.3%		W12102	4611+02.88	28.50	583.82
W12102	W12107	Ramp Cross Slope	5.00	0.07	1.4%	0.1% to 2.0%		W12103	4611+07.44	28.50	584.01
W12103	W12104	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%		W12104	4611+12.07	28.50	583.93
W12103	W12106	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%		W12105	4611+12.04	33.50	584.01
W12104	W12105	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%		W12106	4611+07.44	33.50	584.08
W12105	W12106	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%		W12107	4611+20.88	33.50	583.89
W12106	W12107	Ramp Running Slope	5.00	-0.19	-3.8%	0.5% to 8.3%					
W12201	W12202	Ramp Running Slope	12.00	-0.74	-6.1%	0.5% to 8.3%		W12201	4610+35.16	28.50	584.20
W12201	W12207	Ramp Cross Slope	5.00	0.07	1.5%	0.1% to 2.0%		W12202	4610+46.26	28.50	583.47
W12202	W12203	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%		W12203	4610+50.88	28.50	583.39
W12202	W12206	Landing/Turning Space	5.00	-0.01	-0.2%	0.1% to 2.0%		W12204	4610+57.56	33.50	583.27
W12203	W12205	Landing/Turning Space	5.00	-0.01	-0.2%	0.1% to 2.0%		W12205	4610+50.88	33.50	583.38
W12204	W12205	Landing/Turning Space	7.32	0.11	1.5%	0.1% to 2.0%		W12206	4610+46.26	33.50	583.46
W12205	W12206	Landing/Turning Space	5.07	0.08	1.5%	0.1% to 2.0%		W12207	4610+35.16	33.50	584.28
W12206	W12207	Ramp Running Slope	12.16	0.82	6.7%	0.5% to 8.3%					
W13101	W13102	Sidewalk Running Slope	20.92	-0.37	-1.8%	0.5% to 5.0%		W13101	202+57.47	-52.50	573.52
W13101	W13109	Sidewalk Cross Slope	8.00	-0.12	-1.5%	0.5% to 2.0%		W13102	202+77.49	-48.50	573.15
W13102	W13103	Sidewalk Running Slope	18.64	0.48	2.5%	0.5% to 5.0%		W13103	202+95.70	-48.50	573.62
W13102	W13108	Sidewalk Cross Slope	8.00	-0.12	-1.5%	0.5% to 2.0%		W13104	203+01.87	-49.56	573.72
W13103	W13104	Landing/Turning Space	6.41	0.10	1.5%	0.1% to 2.0%		W13105	203+21.42	-44.92	573.51
W13103	W13107	Landing/Turning Space	8.00	-0.12	-1.5%	0.1% to 2.0%		W13106	203+03.17	-41.67	573.61
W13104	W13106	Landing/Turning Space	8.00	-0.11	-1.4%	0.1% to 2.0%		W13107	202+96.34	-40.50	573.50
W13105	W13106	Landing/Turning Space	18.92	0.10	0.5%	0.1% to 2.0%		W13108	202+76.74	-40.50	573.03
W13106	W13107	Landing/Turning Space	7.07	-0.11	-1.5%	0.1% to 2.0%		W13109	202+56.68	-44.50	573.40
W13107	W13108	Sidewalk Running Slope	20.00	-0.47	-2.4%	0.5% to 5.0%					
W13108	W13109	Sidewalk Running Slope	20.87	0.37	1.8%	0.5% to 5.0%					

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\* Does not include curb  
① Staking required by Contracting Authority per Article 2511.03 of the Standard Specifications.

Point to Point	Sidewalk Designation	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant? ①	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES			
								Point	Station	Offset	Elevation
								FT	FT	%	Pos. or Neg.
W13201	W13202	Landing/Turning Space	19.01	-0.04	-0.2%	0.1% to 2.0%		W13201	203+51.18	-58.75	571.19
W13202	W13203	Landing/Turning Space	11.93	0.18	1.5%	0.1% to 2.0%		W13202	203+69.28	-62.46	571.16
W13202	W13207	Landing/Turning Space	8.00	-0.08	-1.0%	0.1% to 2.0%		W13203	203+80.60	-64.88	571.34
W13203	W13204	Ramp Running Slope	11.61	0.48	4.1%	0.5% to 8.3%		W13204	203+91.66	-62.69	571.81
W13203	W13206	Landing/Turning Space	8.00	-0.10	-1.3%	0.1% to 2.0%		W13205	203+90.22	-54.83	571.69
W13204	W13205	Ramp Cross Slope	8.00	-0.12	-1.5%	0.1% to 2.0%		W13206	203+80.66	-56.71	571.23
W13205	W13206	Ramp Running Slope	10.00	-0.46	-4.6%	0.5% to 8.3%		W13207	203+70.83	-54.62	571.08
W13206	W13207	Landing/Turning Space	10.31	-0.15	-1.5%	0.1% to 2.0%					
W13301	W13302	Sidewalk Running Slope	29.87	-0.60	-2.0%	0.5% to 5.0%		W13301	204+48.73	-52.50	572.08
W13301	W13307	Sidewalk Cross Slope	8.00	-0.12	-1.4%	0.5% to 2.0%		W13302	204+78.30	-52.50	571.48
W13302	W13303	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%		W13303	204+83.30	-52.50	571.41
W13302	W13306	Landing/Turning Space	8.00	0.04	0.6%	0.1% to 2.0%		W13304	204+91.71	-52.50	571.30
W13303	W13304	Landing/Turning Space	8.40	-0.11	-1.3%	0.1% to 2.0%		W13305	204+83.30	-44.50	571.45
W13303	W13305	Landing/Turning Space	8.00	0.05	0.6%	0.1% to 2.0%		W13306	204+78.30	-44.50	571.53
W13305	W13306	Landing/Turning Space	5.00	0.07	1.5%	0.1% to 2.0%		W13307	204+48.11	-44.50	571.97
W13306	W13307	Sidewalk Running Slope	30.46	0.44	1.4%	0.5% to 5.0%					
W14101	W14102	Ramp Running Slope	5.00	0.31	6.3%	0.5% to 8.3%		W14101	102+25.37	-18.25	570.69
W14101	W14110	Ramp Cross Slope	5.00	-0.01	-0.2%	0.1% to 2.0%		W14102	102+25.24	-23.25	571.01
W14102	W14103	Sidewalk Running Slope	12.00	0.37	3.1%	0.5% to 5.0%		W14103	102+37.24	-23.25	571.38
W14102	W14105	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%		W14104	102+37.24	-28.25	571.45
W14102	W14109	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%		W14105	102+25.24	-28.25	571.08
W14103	W14104	Sidewalk Cross Slope	5.00	0.08	1.5%	0.5% to 2.0%		W14106	102+20.24	-28.25	571.01
W14104	W14105	Sidewalk Running Slope	12.00	-0.37	-3.1%	0.5% to 5.0%		W14107	102+07.65	-26.86	571.35
W14105	W14106	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%		W14108	102+08.31	-22.12	571.28
W14106	W14107	Sidewalk Running Slope	12.69	0.35	2.7%	0.5% to 5.0%		W14109	102+20.24	-23.25	570.93
W14106	W14109	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%		W14110	102+20.37	-18.25	570.68
W14107	W14108	Sidewalk Cross Slope	5.00	-0.08	-1.5%	0.5% to 2.0%					
W14108	W14109	Sidewalk Running Slope	12.00	-0.35	-2.9%	0.5% to 5.0%					
W14109	W14110	Ramp Running Slope	5.00	-0.25	-5.0%	0.5% to 8.3%					
W14201	W14202	Sidewalk Running Slope	13.11	-0.18	-1.4%	0.5% to 5.0%		W14201	102+27.54	50.79	571.27
W14201	W14217	Sidewalk Cross Slope	5.00	0.08	1.5%	0.5% to 2.0%		W14202	102+27.11	37.69	571.09
W14202	W14203	Ramp Running Slope	3.84	-0.25	-6.5%	0.5% to 8.3%		W14203	102+30.94	37.69	570.84
W14202	W14206	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%		W14204	102+32.66	37.69	570.81
W14202	W14216	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%		W14205	102+30.94	32.69	570.76
W14203	W14204	Ramp Running Slope	1.71	-0.03	-1.6%	0.5% to 8.3%		W14206	102+26.94	32.69	571.01
W14203	W14205	Ramp Cross Slope	5.00	-0.08	-1.5%	0.1% to 2.0%		W14207	102+26.73	26.38	570.66
W14205	W14206	Ramp Running Slope	4.00	0.25	6.2%	0.5% to 8.3%		W14208	102+21.59	22.08	570.58
W14206	W14207	Ramp Running Slope	6.32	-0.35	-5.5%	0.5% to 8.3%		W14209	102+21.73	26.54	570.65
W14206	W14210	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%		W14210	102+21.94	32.69	571.09
W14207	W14209	Ramp Cross Slope	5.00	-0.01	-0.2%	0.1% to 2.0%		W14211	102+18.86	20.60	571.01
W14208	W14209	Ramp Running Slope	4.46	0.07	1.6%	0.5% to 8.3%		W14212	102+08.28	18.25	570.99
W14209	W14210	Ramp Running Slope	6.15	0.44	7.1%	0.5% to 8.3%		W14213	102+08.28	23.25	571.07
W14210	W14211	Sidewalk Running Slope	12.48	-0.08	-0.6%	0.5% to 5.0%		W14214	102+14.63	24.28	571.08
W14210	W14216	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%		W14215	102+18.01	37.69	571.10
W14211	W14212	Sidewalk Running Slope	10.92	-0.02	-0.2%	0.5% to 5.0%	Drainage issues were considered	W14216	102+22.11	37.69	571.16
W14211	W14214	Sidewalk Cross Slope	5.61	0.07	1.2%	0.5% to 2.0%		W14217	102+22.55	50.96	571.35
W14212	W14213	Sidewalk Cross Slope	5.00	0.08	1.5%	0.5% to 2.0%					
W14213	W14214	Sidewalk Running Slope	6.46	0.01	0.1%	0.5% to 5.0%	Drainage issues were considered				
W14214	W14215	Sidewalk Running Slope	13.83	0.02	0.2%	0.5% to 5.0%	Drainage issues were considered				
W14215	W14216	Sidewalk Running Slope	4.10	0.06	1.6%	0.5% to 5.0%					
W14216	W14217	Sidewalk Running Slope	13.27	0.18	1.4%	0.5% to 5.0%					
W14301	W14302	Ramp Running Slope	1.71	0.06	3.2%	0.5% to 8.3%		W14301	102+62.95	37.69	570.88
W14302	W14303	Ramp Running Slope	3.47	0.24	6.9%	0.5% to 8.3%		W14302	102+64.67	37.69	570.94
W14302	W14317	Ramp Cross Slope	5.00	-0.08	-1.5%	0.1% to 2.0%		W14303	102+68.13	37.69	571.18
W14303	W14304	Sidewalk Running Slope	14.09	0.10	0.7%	0.5% to 5.0%		W14304	102+68.60	51.78	571.27
W14303	W14306	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%		W14305	102+73.60	51.61	571.35
W14303	W14316	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%		W14306	102+73.13	37.53	571.25
W14304	W14305	Sidewalk Cross Slope	5.00	0.08	1.5%	0.5% to 2.0%		W14307	102+78.13	37.36	571.33
W14305	W14306	Sidewalk Running Slope	14.09	-0.10	-0.7%	0.5% to 5.0%		W14308	102+77.96	32.36	571.25
W14306	W14307	Sidewalk Running Slope	5.00	0.08	1.5%	0.5% to 5.0%		W14309	102+77.82	28.25	571.31
W14306	W14315	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%		W14310	102+87.33	28.25	571.62
W14307	W14308	Sidewalk Running Slope	5.00	-0.08	-1.5%	0.5% to 5.0%		W14311	102+87.33	23.25	571.54
W14308	W14309	Sidewalk Running Slope	4.11	0.05	1.3%	0.5% to 5.0%		W14312	102+77.66	23.25	571.38
W14308	W14315	Sidewalk Cross Slope	5.00	-0.08	-1.5%	0.5% to 2.0%		W14313	102+72.65	23.25	571.30
W14309	W14310	Sidewalk Running Slope	9.50	0.31	3.3%	0.5% to 5.0%		W14314	102+72.82	28.25	571.23
W14309	W14312	Sidewalk Cross Slope	5.00	0.08	1.5%	0.5% to 2.0%		W14315	102+72.96	32.53	571.18
W14309	W14314	Sidewalk Cross Slope	5.00	-0.07	-1.5%	0.5% to 2.0%		W14316	102+67.96	32.69	571.10
W14310	W14311	Sidewalk Cross Slope	5.00	-0.08	-1.5%	0.5% to 2.0%		W14317	102+64.67	32.69	570.86
W14311	W14312	Sidewalk Running Slope	9.67	-0.16	-1.7%	0.5% to 5.0%					
W14312	W14313	Sidewalk Running Slope	5.00	-0.08	-1.6%	0.5% to 5.0%					
W14313	W14314	Sidewalk Running Slope	5.00	-0.07	-1.4%	0.5% to 5.0%					
W14314	W14315	Sidewalk Running Slope	4.28	-0.06	-1.3%	0.5% to 5.0%					
W14315	W14316	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%					

SIDEWALK COMPLIANCE

See S Sheets

\* Does not include curb  
① Staking required by Contracting Authority per Article 2511.03 of the Standard Specifications.

Point to Point	Sidewalk Designation	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant? ①	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES					
								Point	Station	Offset	Elevation		
		FT	FT	%	Pos. or Neg.								
W14316	W14317	Ramp Running Slope	3.30	-0.24	-7.2%	0.5% to 8.3%							
W15101	W15102	Ramp Running Slope	4.86	0.04	0.9%	0.5% to 8.3%							
W15102	W15103	Ramp Running Slope	5.02	0.19	3.8%	0.5% to 8.3%			W15101	99+78.28	-34.86	572.43	
W15102	W15116	Ramp Cross Slope	5.00	-0.07	-1.5%	0.1% to 2.0%			W15102	99+73.43	-34.69	572.48	
W15103	W15104	Sidewalk Running Slope	15.62	0.34	2.1%	0.5% to 5.0%			W15103	99+68.42	-34.52	572.67	
W15103	W15108	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%			W15104	99+73.05	-49.43	573.00	
W15103	W15115	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W15105	99+76.21	-60.25	572.62	
W15104	W15105	Sidewalk Running Slope	11.30	-0.38	-3.4%	0.5% to 5.0%			W15106	99+71.21	-60.07	572.69	
W15104	W15107	Sidewalk Cross Slope	4.84	0.07	1.5%	0.5% to 2.0%			W15107	99+68.41	-50.84	573.08	
W15105	W15106	Sidewalk Cross Slope	5.00	0.07	1.4%	0.5% to 2.0%			W15108	99+63.42	-34.34	572.74	
W15106	W15107	Sidewalk Running Slope	9.65	0.39	4.0%	0.5% to 5.0%			W15109	99+53.81	-27.96	572.86	
W15107	W15108	Sidewalk Running Slope	17.24	-0.33	-1.9%	0.5% to 5.0%			W15110	99+53.81	-23.75	572.80	
W15108	W15109	Sidewalk Running Slope	11.54	0.12	1.0%	0.5% to 5.0%			W15111	99+63.59	-29.34	572.67	
W15108	W15111	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W15112	99+63.70	-26.32	572.45	
W15109	W15110	Sidewalk Cross Slope	4.22	-0.06	-1.5%	0.5% to 2.0%			W15113	99+63.76	-24.67	572.36	
W15110	W15111	Sidewalk Running Slope	11.28	-0.13	-1.2%	0.5% to 5.0%			W15114	99+68.70	-26.49	572.37	
W15111	W15112	Ramp Running Slope	3.03	-0.22	-7.3%	0.5% to 8.3%			W15115	99+68.59	-29.52	572.59	
W15111	W15115	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W15116	99+73.61	-29.69	572.40	
W15112	W15113	Ramp Running Slope	1.66	-0.09	-5.1%	0.5% to 8.3%							
W15112	W15114	Ramp Cross Slope	5.00	-0.08	-1.5%	0.1% to 2.0%							
W15114	W15115	Ramp Running Slope	3.03	0.22	7.3%	0.5% to 8.3%							
W15115	W15116	Ramp Running Slope	5.02	-0.19	-3.8%	0.5% to 8.3%							
W15201	W15202	Ramp Running Slope	3.45	-0.04	-1.3%	0.5% to 8.3%							
W15202	W15203	Sidewalk Running Slope	4.69	0.07	1.4%	0.5% to 5.0%			W15201	98+78.71	26.96	572.81	
W15202	W15218	Ramp Cross Slope	5.00	-0.08	-1.5%	0.1% to 2.0%			W15202	99+75.26	26.96	572.77	
W15203	W15204	Sidewalk Running Slope	12.14	0.37	3.1%	0.5% to 5.0%			W15203	99+70.57	26.96	572.84	
W15203	W15206	Landing/Turning Space	5.00	0.07	1.5%	0.1% to 2.0%			W15204	99+71.60	39.04	573.21	
W15203	W15217	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W15205	99+65.78	39.27	573.19	
W15204	W15205	Match Existing Cross Slope	5.81	-0.02	-0.3%	Match Existing			W15206	99+65.56	26.96	572.91	
W15205	W15206	Sidewalk Running Slope	12.31	-0.28	-2.3%	0.5% to 5.0%			W15207	99+58.83	17.68	573.02	
W15206	W15207	Sidewalk Running Slope	11.46	0.11	0.9%	0.5% to 5.0%			W15208	99+53.82	17.57	572.93	
W15206	W15212	Landing/Turning Space	7.16	-0.11	-1.5%	0.1% to 2.0%			W15209	99+53.81	13.57	572.91	
W15207	W15208	Sidewalk Running Slope	5.01	-0.09	-1.8%	0.5% to 5.0%			W15210	99+59.26	13.71	572.96	
W15207	W15210	Sidewalk Cross Slope	4.00	-0.06	-1.5%	0.5% to 2.0%			W15211	99+63.87	14.65	572.99	
W15208	W15209	Match Existing Cross Slope	4.00	-0.02	-0.4%	Match Existing			W15212	99+65.31	19.81	572.80	
W15209	W15210	Sidewalk Running Slope	5.46	0.05	0.8%	0.5% to 5.0%			W15213	99+65.24	17.81	572.66	
W15210	W15211	Sidewalk Running Slope	4.71	0.04	0.8%	0.5% to 5.0%			W15214	99+65.15	15.08	572.54	
W15211	W15212	Sidewalk Running Slope	4.71	-0.19	-4.0%	0.5% to 5.0%			W15215	99+70.24	17.63	572.58	
W15212	W15213	Ramp Running Slope	2.00	-0.15	-7.3%	0.5% to 8.3%			W15216	99+70.31	19.63	572.73	
W15212	W15216	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W15217	99+70.39	21.96	572.76	
W15213	W15214	Ramp Running Slope	2.73	-0.12	-4.2%	0.5% to 8.3%			W15218	99+75.26	21.96	572.69	
W15213	W15215	Ramp Cross Slope	5.00	-0.08	-1.5%	0.1% to 2.0%							
W15215	W15216	Ramp Running Slope	2.00	0.15	7.3%	0.5% to 8.3%							
W15216	W15217	Landing/Turning Space	2.33	0.03	1.4%	0.1% to 2.0%							
W15217	W15218	Ramp Running Slope	4.69	-0.07	-1.4%	0.5% to 8.3%							
									#REF!	#REF!	#REF!	#REF!	
W15301	W15302	Ramp Running Slope	4.48	0.00	-0.1%	0.5% to 8.3%	Drainage issues were considered						
W15302	W15303	Ramp Running Slope	3.52	0.26	7.3%	0.5% to 8.3%			W15301	100+23.48	26.96	572.83	
W15302	W15318	Ramp Cross Slope	5.00	-0.08	-1.5%	0.1% to 2.0%			W15302	100+27.96	26.96	572.83	
W15303	W15304	Sidewalk Running Slope	13.80	0.22	1.6%	0.5% to 5.0%			W15303	100+31.48	26.96	573.08	
W15303	W15306	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%			W15304	100+28.78	40.49	573.30	
W15303	W15317	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%			W15305	100+34.05	40.37	573.26	
W15304	W15305	Match Existing Cross Slope	5.28	-0.04	-0.8%	Match Existing			W15306	100+36.58	26.96	573.16	
W15305	W15306	Sidewalk Running Slope	13.65	-0.10	-0.7%	0.5% to 5.0%			W15307	100+40.45	26.96	573.22	
W15306	W15307	Landing/Turning Space	3.87	0.06	1.5%	0.1% to 2.0%			W15308	100+50.07	26.00	573.60	
W15307	W15308	Sidewalk Running Slope	9.67	0.38	3.9%	0.5% to 5.0%			W15309	100+53.82	26.00	573.74	
W15307	W15312	Landing/Turning Space	5.00	-0.07	-1.5%	0.1% to 2.0%			W15310	100+53.82	21.00	573.67	
W15308	W15309	Sidewalk Running Slope	4.00	0.14	3.6%	0.5% to 5.0%			W15311	100+49.82	21.00	573.52	
W15308	W15311	Sidewalk Cross Slope	5.00	-0.07	-1.5%	0.5% to 2.0%			W15312	100+40.34	21.95	573.14	
W15309	W15310	Sidewalk Cross Slope	5.00	-0.07	-1.5%	0.5% to 2.0%			W15313	100+40.24	17.54	572.88	
W15310	W15311	Sidewalk Running Slope	4.00	-0.14	-3.6%	0.5% to 5.0%			W15314	100+40.22	16.31	572.90	
W15311	W15312	Sidewalk Running Slope	9.53	-0.38	-4.0%	0.5% to 5.0%			W15315	100+35.24	17.65	572.80	
W15312	W15313	Ramp Running Slope	4.41	-0.27	-6.0%	0.5% to 8.3%			W15316	100+35.34	21.96	573.07	
W15312	W15316	Landing/Turning Space	5.00	-0.08	-1.5%	0.1% to 2.0%			W15317	100+31.48	21.96	573.01	
W15313	W15314	Ramp Running Slope	1.22	0.03	2.0%	0.5% to 8.3%			W15318	100+27.96	21.96	572.75	
W15313	W15315	Ramp Cross Slope	5.00	-0.07	-1.5%	0.1% to 2.0%							
W15315	W15316	Ramp Running Slope	4.41	0.26	6.0%	0.5% to 8.3%							
W15316	W15317	Landing/Turning Space	3.86	-0.06	-1.5%	0.1% to 2.0%							
W15317	W15318	Ramp Running Slope	3.52	-0.26	-7.3%	0.5% to 8.3%							
W15401	W15402	Ramp Running Slope	5.66	0.13	2.2%	0.5% to 8.3%							
W15401	W15419	Ramp Cross Slope	5.00	0.04	0.8%	0.1% to 2.0%			W15401	100+13.96	-31.11	572.63	
W15402	W15403	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%			W15402	100+18.25	-31.26	572.76	
W15402	W15418	Landing/Turning Space	5.00	0.08	1.5%	0.1% to 2.0%			W15403	100+23.24	-31.43	572.83	
W15403	W15404	Sidewalk Running Slope	10.44	-0.34	-3.2%	0.5% to 5.0%			W15404	100+23.61	-21.00	572.50	

### SIDEWALK COMPLIANCE

See S Sheets

\* Does not include curb  
 ① Staking required by Contracting Authority per Article 2511.03 of the Standard Specifications.

Point to Point	Sidewalk Designation	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant? ①	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES				
								Point	Station	Offset	Elevation	
		FT	FT	%	Pos. or Neg.							
W15403	W15415	5.00	0.07	1.5%	0.1% to 2.0%			W15405	100+34.40	-21.00	572.93	
W15404	W15405	10.78	0.43	4.0%	0.5% to 5.0%			W15406	100+34.50	-16.12	572.86	
W15405	W15406	4.88	-0.07	-1.3%	0.5% to 8.3%			W15407	100+39.51	-16.00	572.90	
W15405	W15408	5.00	0.08	1.5%	0.1% to 2.0%			W15408	100+39.40	-21.00	573.00	
W15405	W15412	5.00	0.01	0.2%	0.1% to 2.0%			W15409	100+59.40	-21.00	573.71	
W15406	W15407	5.00	0.04	0.8%	0.1% to 2.0%			W15410	100+59.40	-26.00	573.78	
W15407	W15408	5.00	0.10	2.0%	0.5% to 8.3%			W15411	100+39.29	-26.00	573.01	
W15408	W15409	20.00	0.71	3.5%	0.5% to 5.0%			W15412	100+34.29	-26.00	572.94	
W15408	W15411	5.00	0.01	0.2%	0.1% to 2.0%			W15413	100+28.44	-26.00	572.51	
W15409	W15410	5.00	0.08	1.5%	0.5% to 2.0%			W15414	100+28.07	-36.60	573.01	
W15410	W15411	20.11	-0.77	-3.8%	0.5% to 5.0%			W15415	100+23.07	-36.43	572.91	
W15411	W15412	5.00	-0.08	-1.5%	0.1% to 2.0%			W15416	100+22.72	-46.51	572.97	
W15412	W15413	5.85	-0.43	-7.3%	Match Existing			W15417	100+17.72	-46.33	572.89	
W15413	W15414	10.61	0.50	4.7%	Match Existing			W15418	100+18.07	-36.25	572.83	
W15414	W15415	5.00	-0.10	-2.0%	Match Existing			W15419	100+13.78	-36.10	572.67	
W15415	W15416	10.09	0.06	0.6%	Match Existing			W15420	100+12.41	-36.05	572.60	
W15415	W15418	5.00	-0.07	-1.5%	0.1% to 2.0%							
W15416	W15417	5.00	-0.08	-1.5%	0.5% to 2.0%							
W15417	W15418	10.09	-0.06	-0.6%	0.5% to 5.0%							
W15418	W15419	4.29	-0.16	-3.8%	0.5% to 8.3%							
W15419	W15420	1.37	-0.07	-5.0%	0.5% to 8.3%							

### TABULATION OF TEMPLATE QUANTITIES AND ADJUSTMENTS

Refer to Standard Plan EW-101 and RL-1B.

STATION	TOTAL CUT	CLASS 10 SUITABLE CUT	ADJUSTED CLASS 10 TOTAL	TOTAL FILL	CLASS 10 SUITABLE + 30% SHRINK	TOTAL FILL WITH SHRINK																STATION	TOTAL CUT	CLASS 10 SUITABLE CUT	ADJUSTED CLASS 10 TOTAL	TOTAL FILL	CLASS 10 SUITABLE + 30% SHRINK	TOTAL FILL WITH SHRINK											
US 67 SB																							210+00.00	217	217	217													
197+00.01	76	76	76																			210+20.00	54	54	54														
197+25.00	84	84	84																			210+25.00	276	276	276														
197+50.00	87	87	87																			210+50.00	279	279	279														
197+75.00	90	90	90																			210+75.00	265	265	265														
198+00.00	74	74	74																			211+00.00	102	102	102														
198+25.00	56	56	56																			211+10.00	147	147	147														
198+50.00	34	34	34	1	1	1																211+25.00	142	142	142														
198+75.00	20	20	20	2	3	3																211+40.00	137	137	137														
199+00.00	16	16	16	2	3	3																211+55.00	188	188	188														
199+21.00	9	9	9	6	8	8																211+80.00	85	85	85														
199+43.00	3	3	3	4	5	5																212+00.00	45	45	45														
199+50.00	8	8	8	5	7	7																212+23.00	99	99	99														
199+66.00	25	25	25																			212+49.00	68	68	68														
199+75.00	9	9	9																			212+60.00	93	93	93														
200+03.00	2	2	2																			212+75.00	159	159	159														
200+25.00	14	14	14	1	1	1																213+00.00	163	163	163														
200+31.00	32	32	32	2	3	3																213+25.00	158	158	158														
200+50.00	36	36	36	1	1	1																213+50.00	152	152	152														
200+75.00	48	48	48																			213+75.00	132	132	132														
201+00.00	104	104	104																			214+00.00	111	111	111														
201+25.00	162	162	162																			214+25.00	101	101	101														
201+50.00	173	173	173																			214+50.00	94	94	94														
201+75.00	196	196	196																			214+75.00	90	90	90														
202+00.00	251	251	251																			215+00.00	88	88	88	1	1	1											
202+25.00	258	258	258																			215+25.00	82	82	82	2	3	3											
202+50.00	219	219	219																			215+50.00	76	76	76														
202+75.00	150	150	150																			215+75.00	30	30	30														
203+00.00	35	35	35																			215+85.00	21	21	21														
203+20.00	181	181	181																			215+92.00	53	53	53														
203+25.00	201	201	201																			216+13.00	25	25	25														
203+50.00	204	204	204																			216+25.00	113	113	113														
203+75.00	195	195	195																			216+39.00	91	91	91	1	1	1											
204+00.00	186	186	186																			216+50.00	37	37	37	1	1	1											
204+25.00	91	91	91																			216+65.00	31	31	31														
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204+75.00	145	145	145																			217+10.00	21	21	21														
205+00.00	25	25	25																			217+20.00	12	12	12														
205+25.00	116	116	116																			217+25.00	34	34	34														
205+45.00	231	231	231																			217+40.00																	
205+50.00	261	261	261																			US 67 NB																	
205+75.00	188	188	188																			1593+00.00	64	64	64	9	12	12											
206+03.00	222	222	222																			1593+25.00	85	85	85	7	9	9											
206+25.00	215	215	215																			1593+50.00	98	98	98	7	9	9											
206+50.00	194	194	194																			1593+75.00	90	90	90	9	12	12											
206+75.00	169	169	169																			1594+00.00	74	74	74	8	10	10											
207+00.00	149	149	149																			1594+25.00	61	61	61	6	8	8											
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



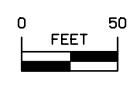






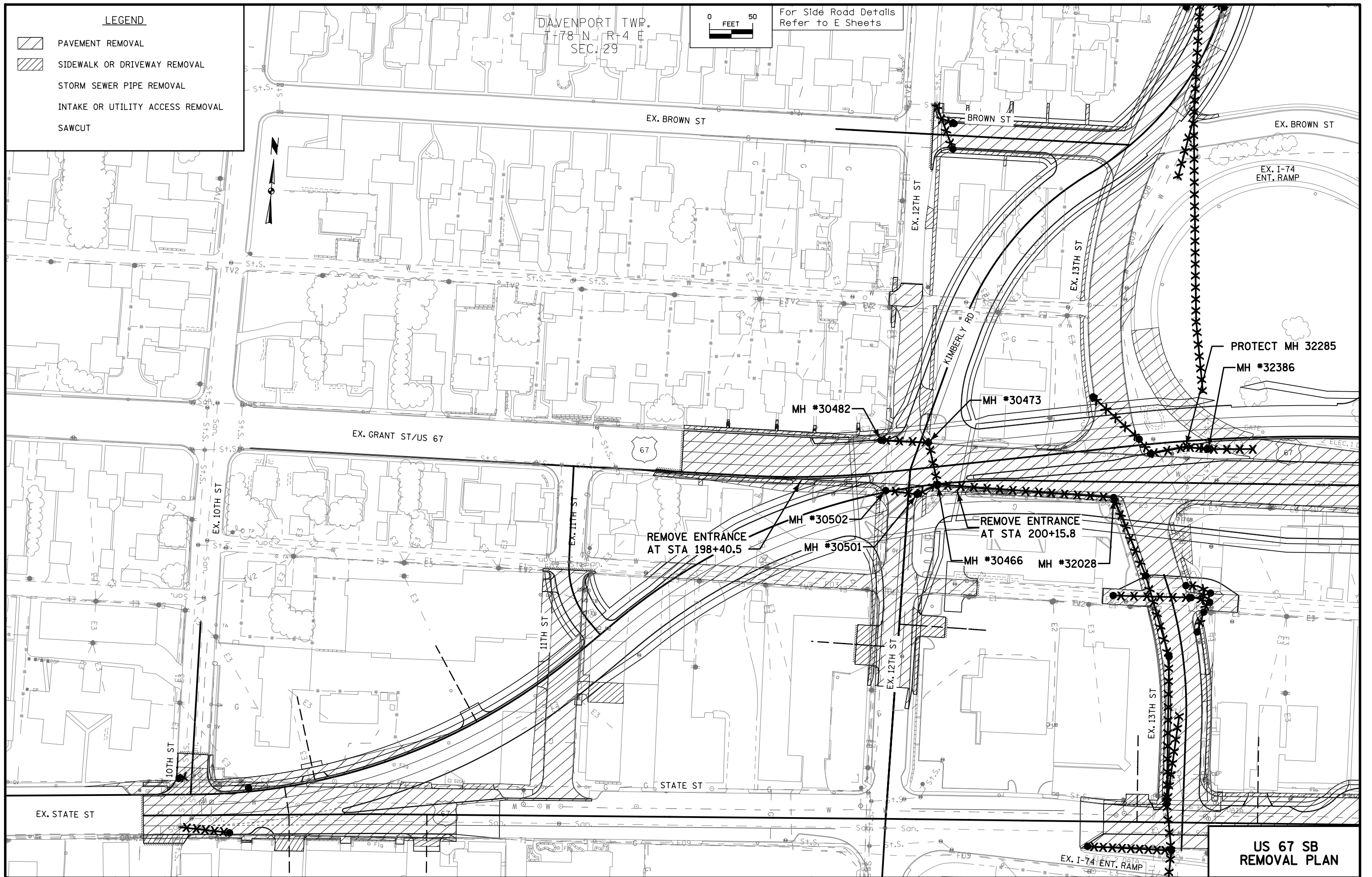
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-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
- STORM SEWER PIPE REMOVAL
- INTAKE OR UTILITY ACCESS REMOVAL
- SAWCUT



For Side Road Details Refer to E Sheets

DAVENPORT TWP.  
T-78 N. R-4 E  
SEC. 29





PROTECT MH 32285  
MH #32386

REMOVE ENTRANCE  
AT STA 198+40.5

REMOVE ENTRANCE  
AT STA 200+15.8

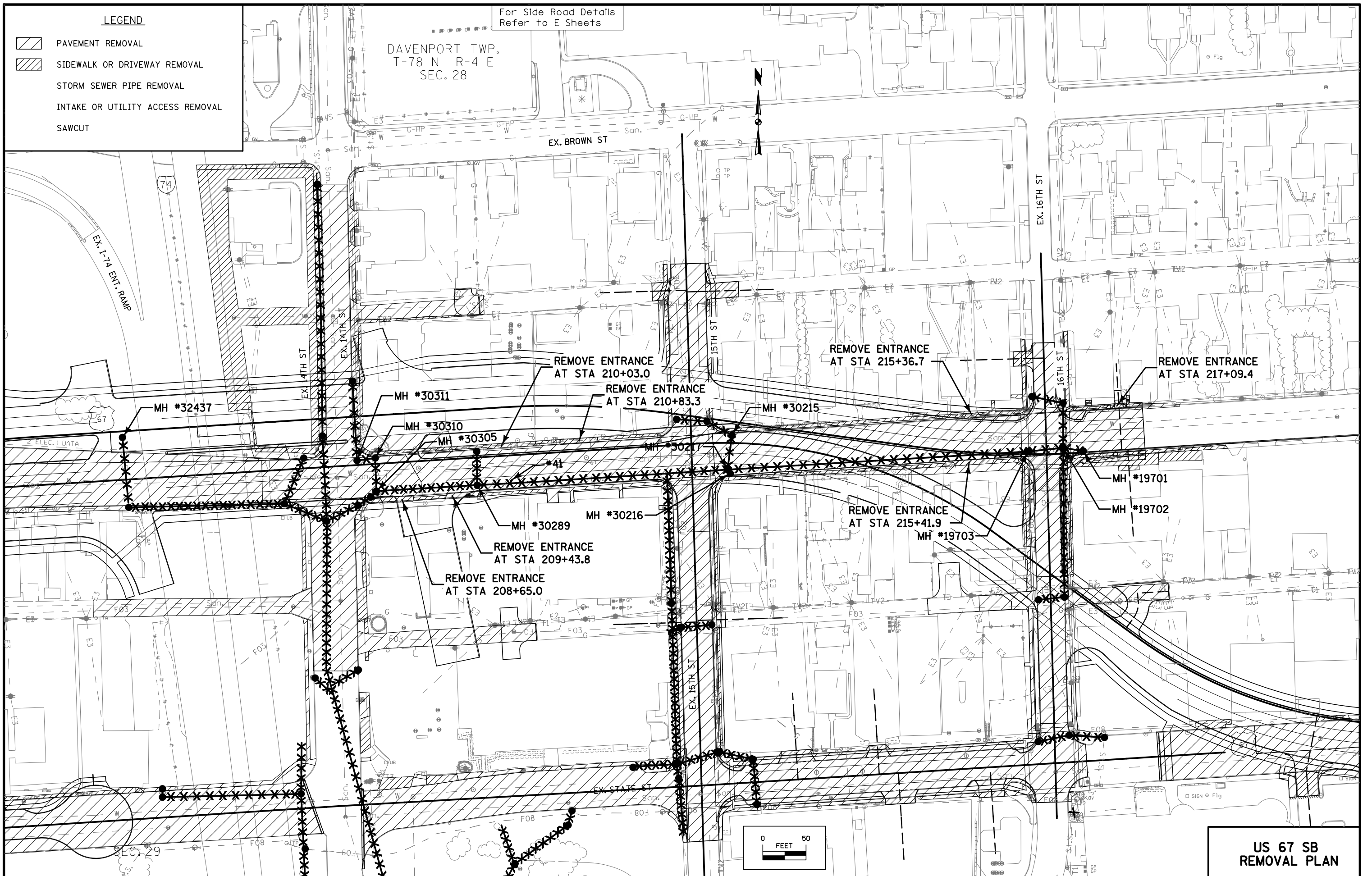
**US 67 SB  
REMOVAL PLAN**

**LEGEND**

-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
- STORM SEWER PIPE REMOVAL
- INTAKE OR UTILITY ACCESS REMOVAL
- SAWCUT



For Side Road Details  
Refer to E Sheets

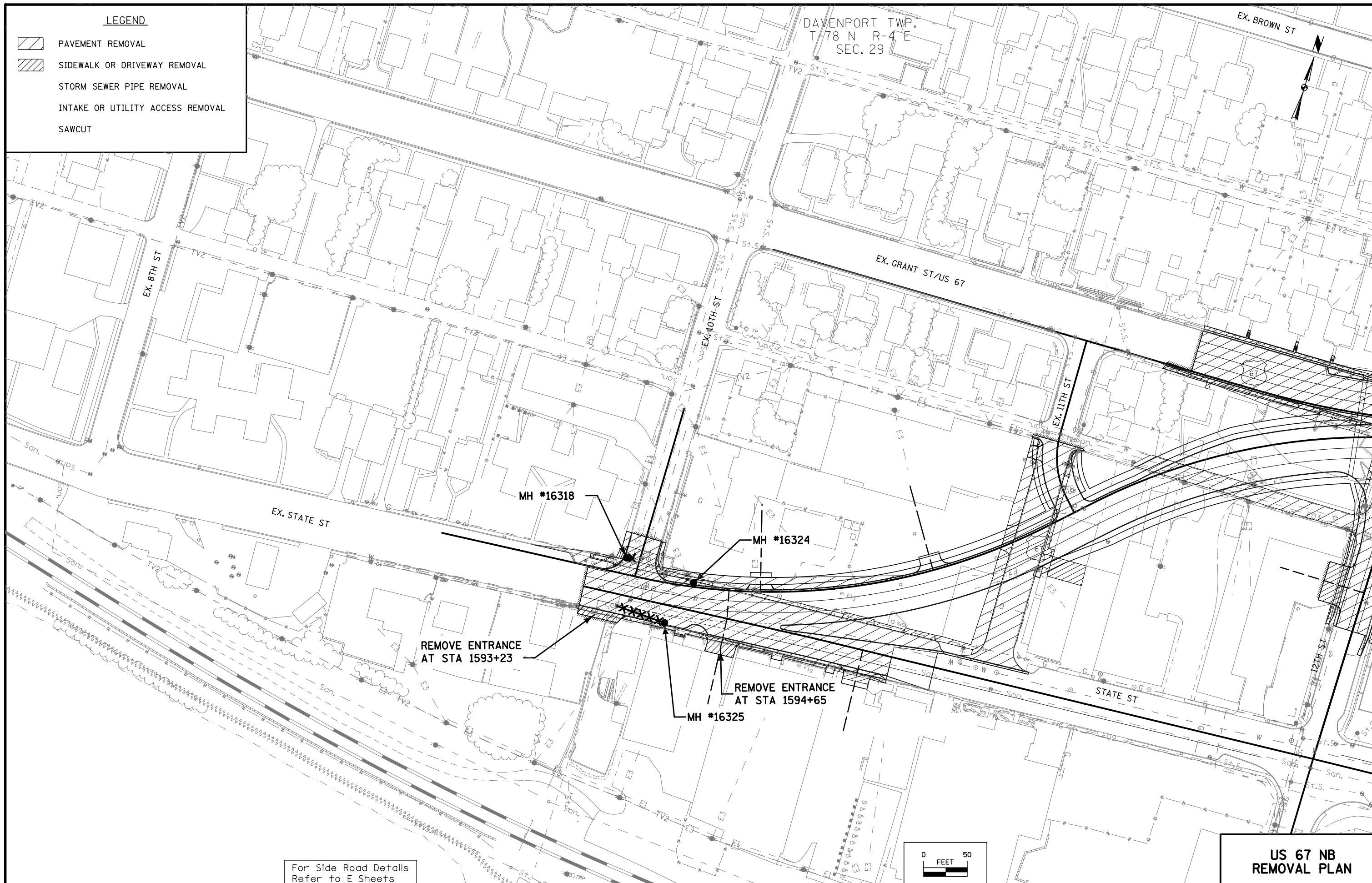
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T-78 N R-4 E  
SEC. 28



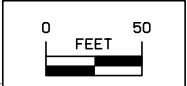
**US 67 SB  
REMOVAL PLAN**

**LEGEND**

-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
- STORM SEWER PIPE REMOVAL
- INTAKE OR UTILITY ACCESS REMOVAL
- SAWCUT





For Side Road Details  
Refer to E Sheets



**US 67 NB  
REMOVAL PLAN**

LEGEND

-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
- STORM SEWER PIPE REMOVAL
- INTAKE OR UTILITY ACCESS REMOVAL
- SAWCUT



DAVENPORT TWP.  
T-78 N R-4 E  
SEC. 29

For Side Road Details  
Refer to E Sheets

US 67 NB  
REMOVAL PLAN

ENGLISH

IOWA DOT






DESIGN TEAM **WHKS & Co.**

SCOTT COUNTY

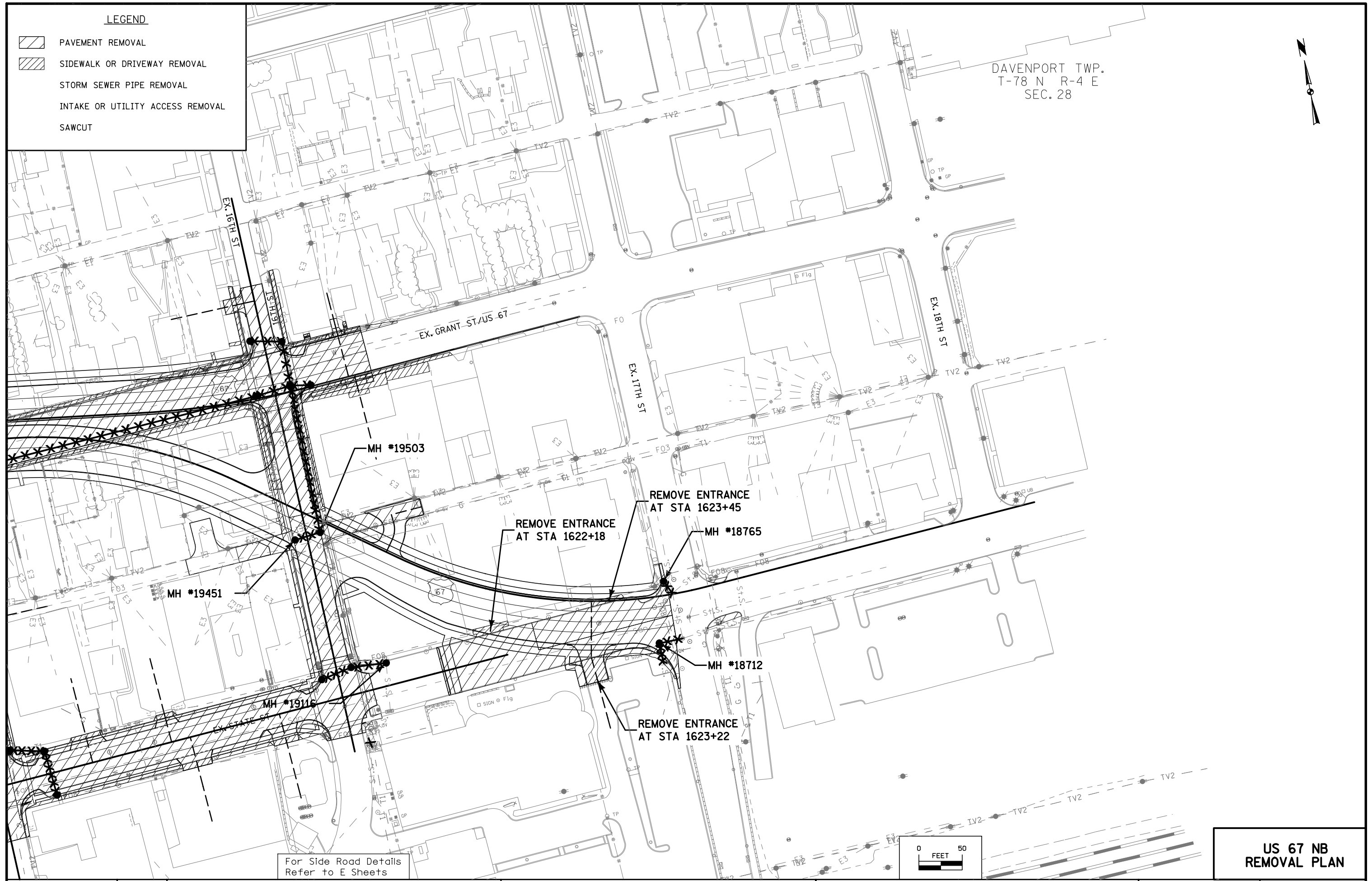
PROJECT NUMBER **IM-74-1(226)4--13-82**

SHEET NUMBER **U.4**

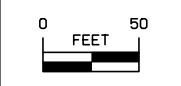
**LEGEND**

-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
-  STORM SEWER PIPE REMOVAL
-  INTAKE OR UTILITY ACCESS REMOVAL
-  SAWCUT

DAVENPORT TWP.  
T-78 N R-4 E  
SEC. 28





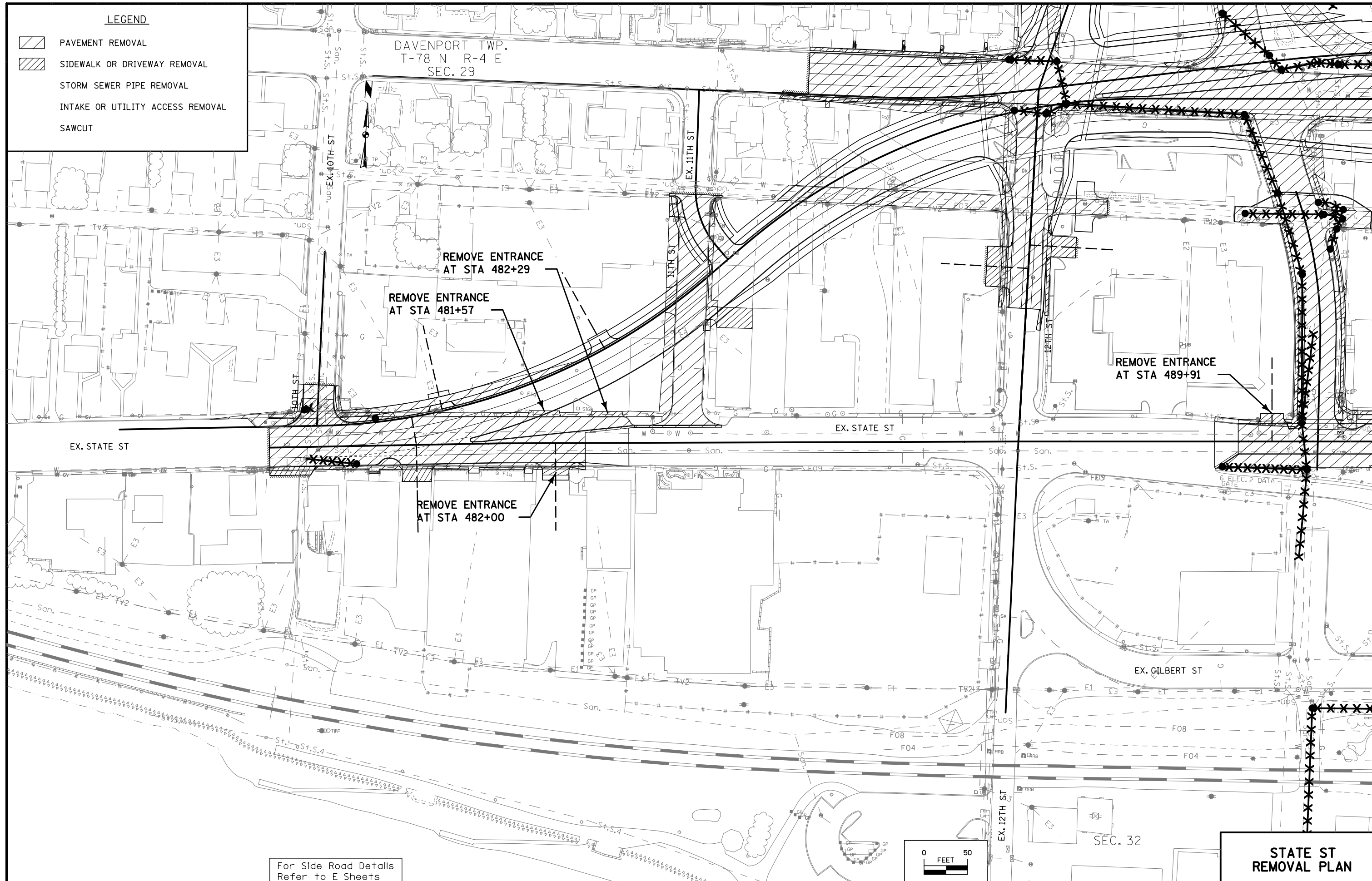
For Side Road Details  
Refer to E Sheets



**US 67 NB  
REMOVAL PLAN**

LEGEND

-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
- STORM SEWER PIPE REMOVAL
- INTAKE OR UTILITY ACCESS REMOVAL
- SAWCUT








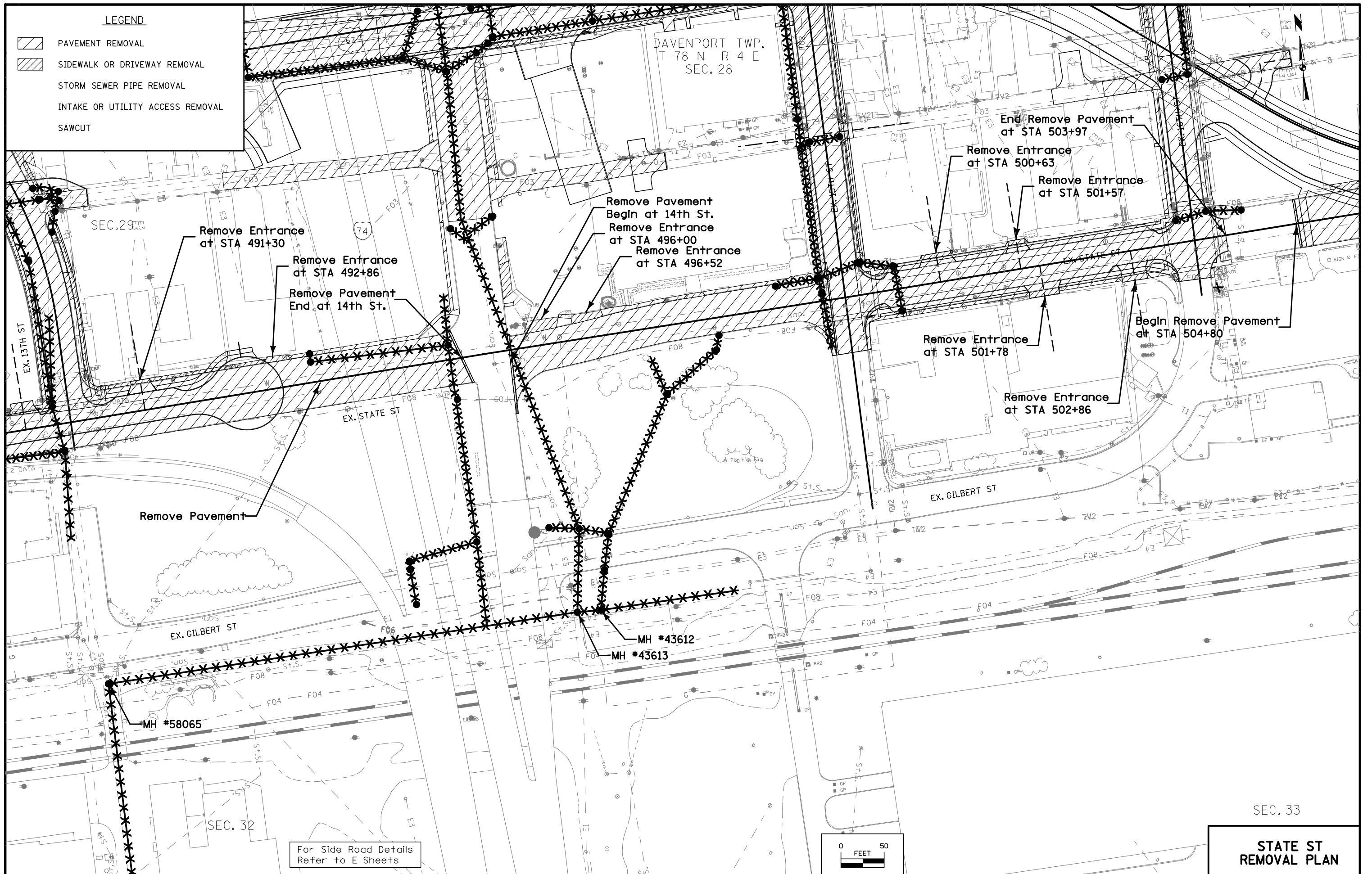
For Side Road Details Refer to E Sheets



STATE ST REMOVAL PLAN



**LEGEND**

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-  SIDEWALK OR DRIVEWAY REMOVAL
-  STORM SEWER PIPE REMOVAL
-  INTAKE OR UTILITY ACCESS REMOVAL
-  SAWCUT

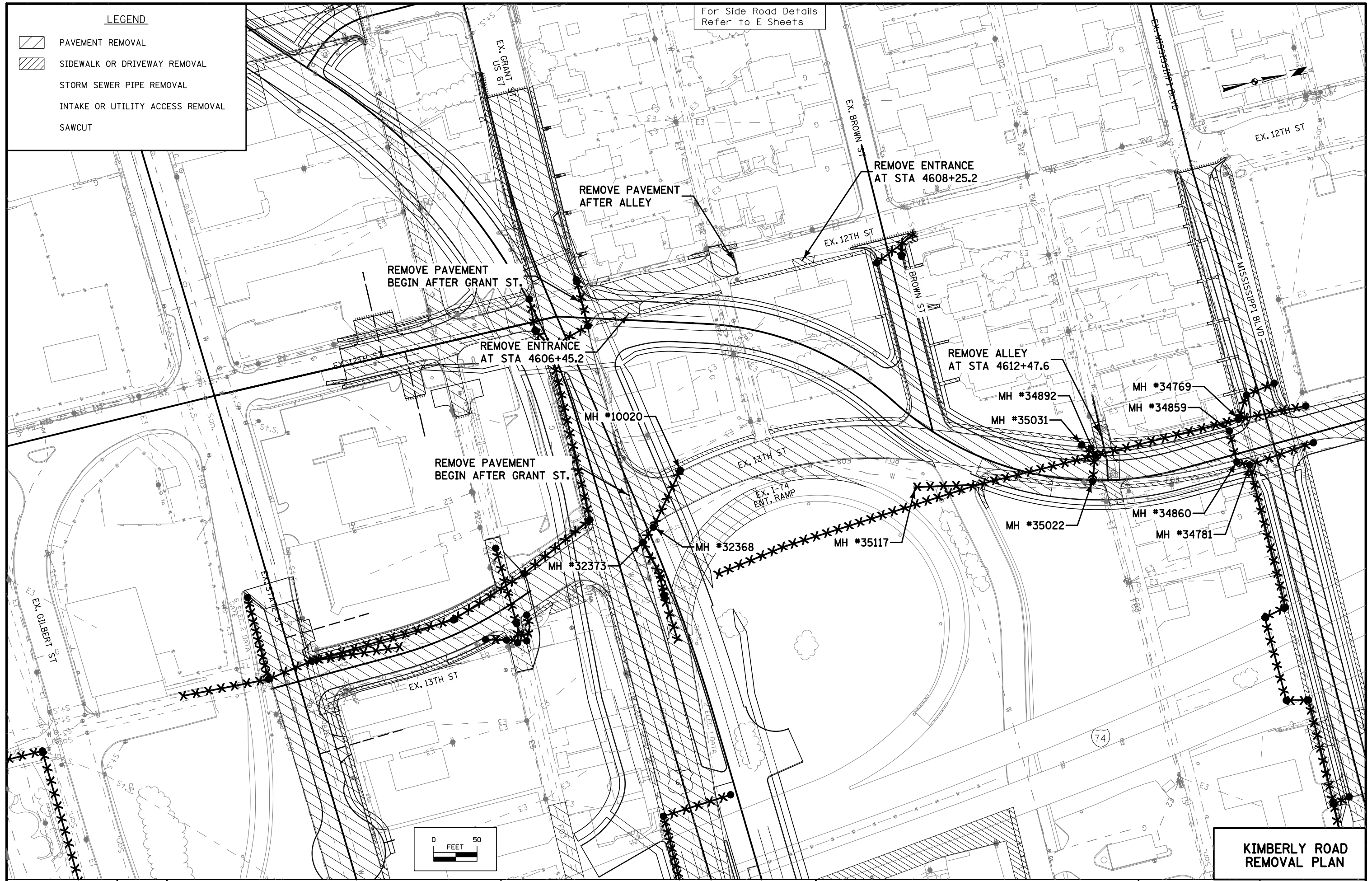




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

-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
- STORM SEWER PIPE REMOVAL
- INTAKE OR UTILITY ACCESS REMOVAL
- SAWCUT

For Side Road Details  
Refer to E Sheets



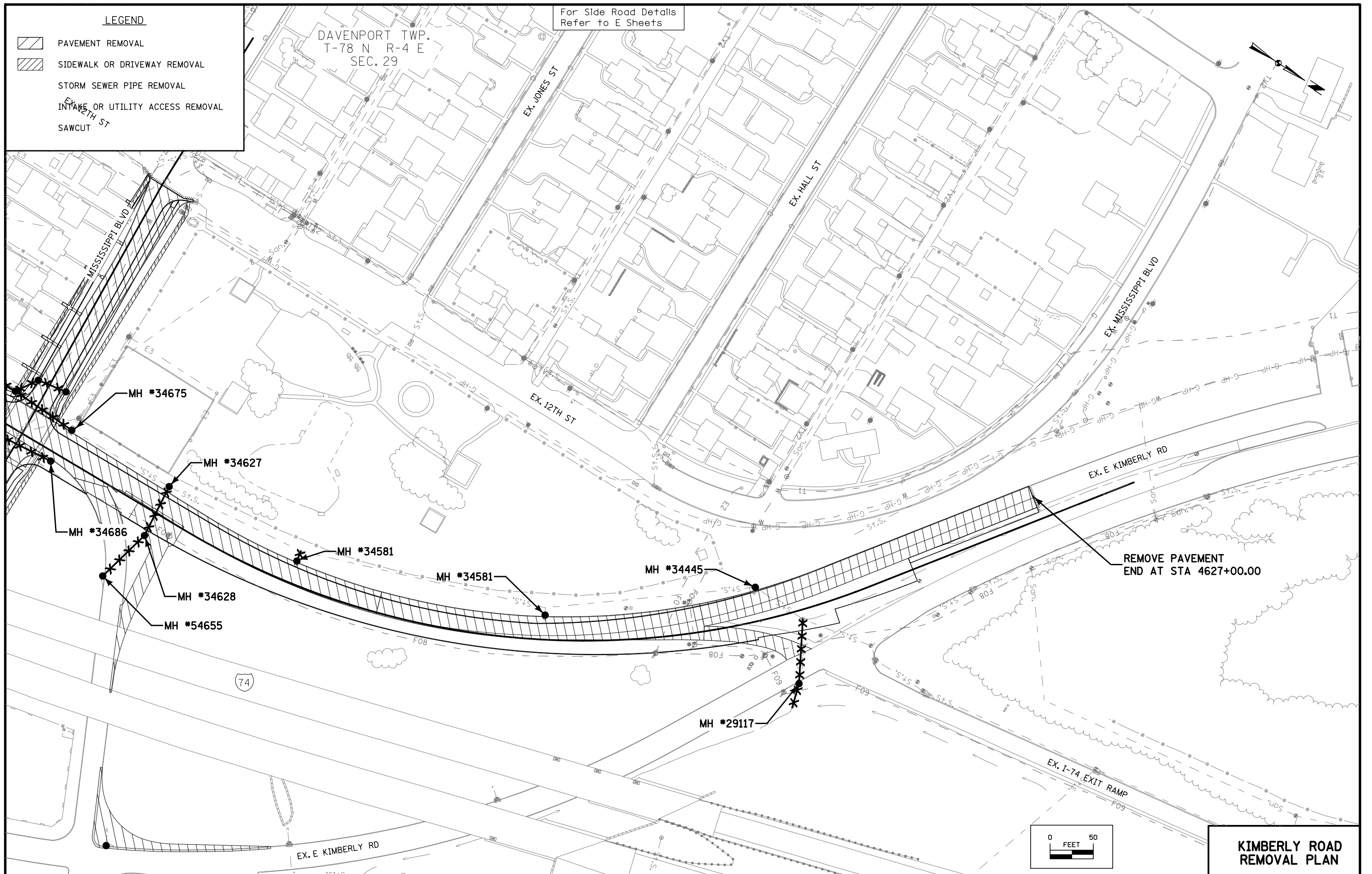
**KIMBERLY ROAD  
REMOVAL PLAN**

**LEGEND**

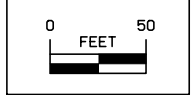
-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
- STORM SEWER PIPE REMOVAL
- INTAKE OR UTILITY ACCESS REMOVAL
- SAWCUT

For Side Road Details Refer to E Sheets

DAVENPORT TWP.  
T-78 N R-4 E  
SEC. 29








REMOVE PAVEMENT  
END AT STA 4627+00.00



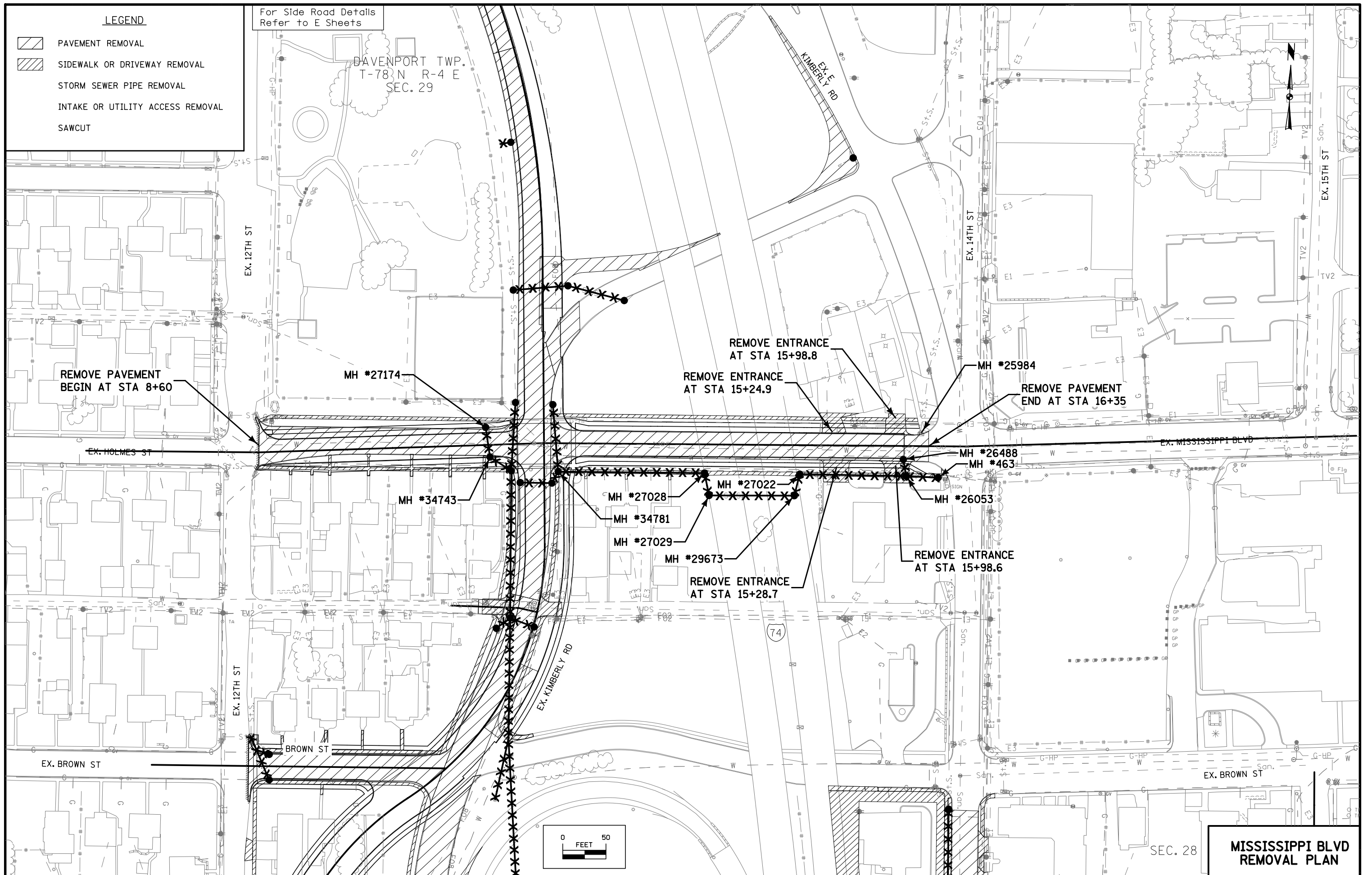
**KIMBERLY ROAD  
REMOVAL PLAN**

**LEGEND**

-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
-  STORM SEWER PIPE REMOVAL
-  INTAKE OR UTILITY ACCESS REMOVAL
-  SAWCUT



For Side Road Details Refer to E Sheets

DAVENPORT TWP.  
T-78 N R-4 E  
SEC. 29



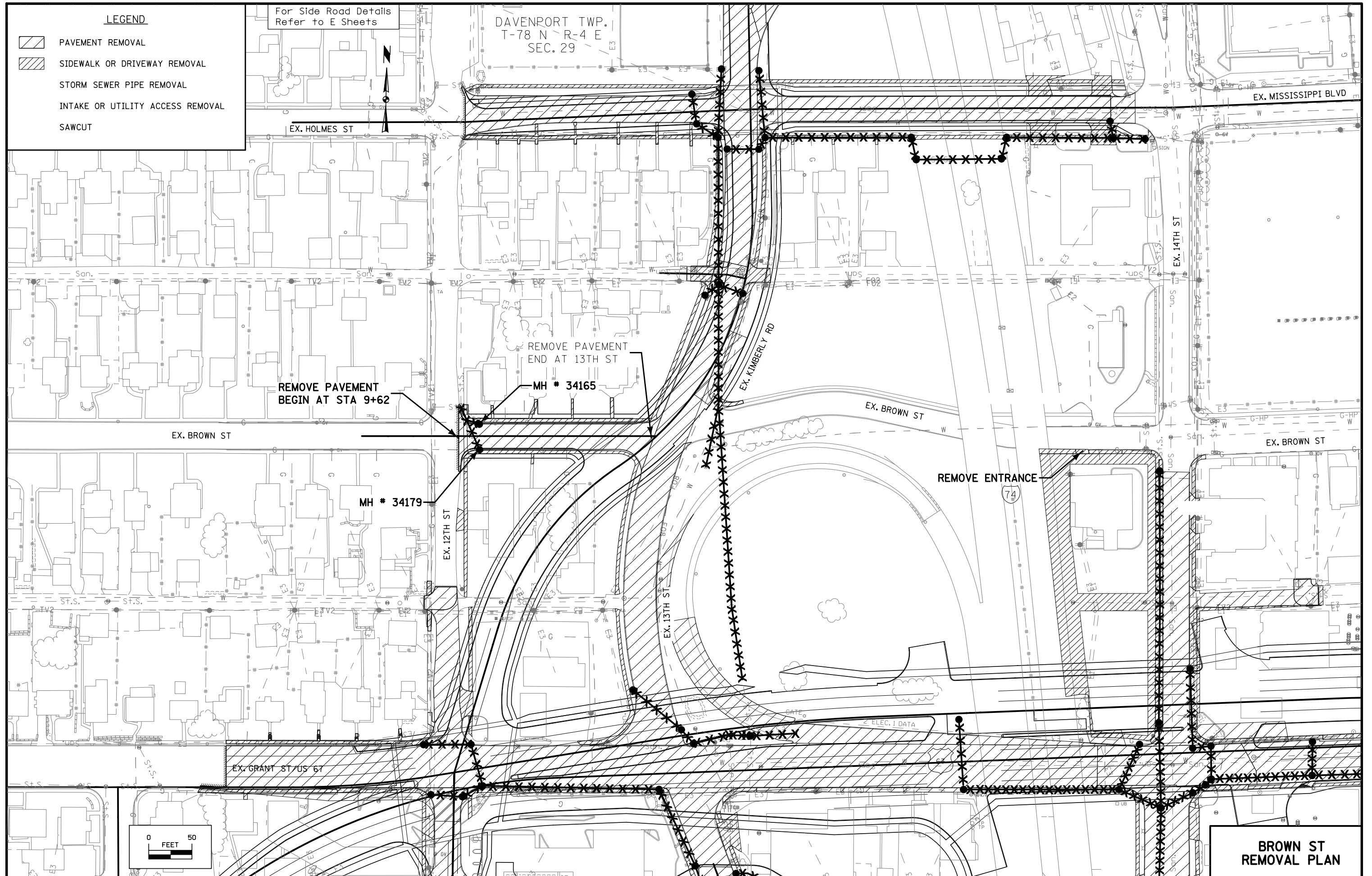
**MISSISSIPPI BLVD  
REMOVAL PLAN**

**LEGEND**

-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
- STORM SEWER PIPE REMOVAL
- INTAKE OR UTILITY ACCESS REMOVAL
- SAWCUT

For Side Road Details Refer to E Sheets

DAVENPORT TWP.  
T-78 N R-4 E  
SEC. 29



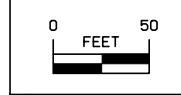
REMOVE PAVEMENT  
BEGIN AT STA 9+62

MH # 34165

REMOVE PAVEMENT  
END AT 13TH ST



MH # 34179

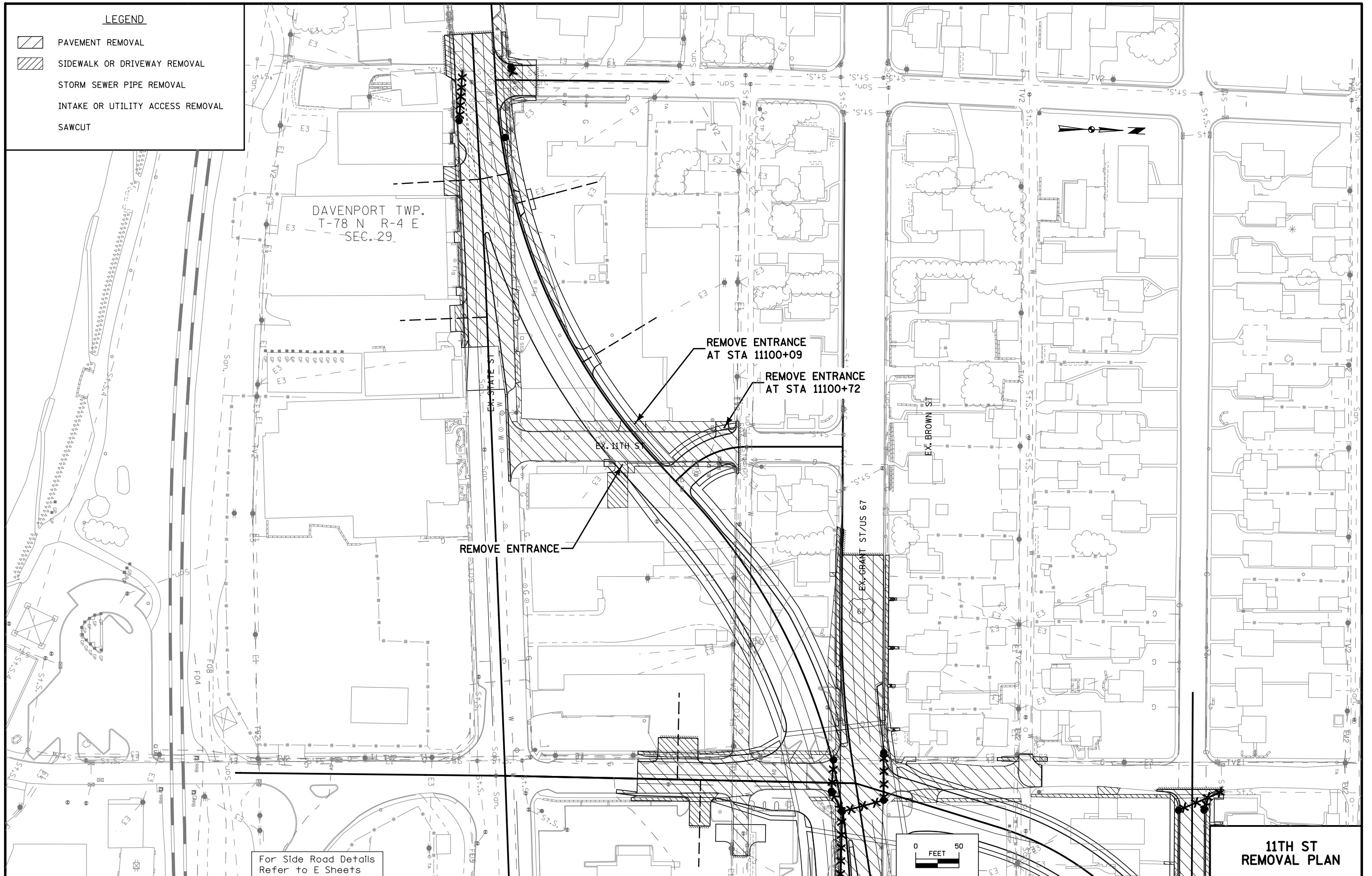
REMOVE ENTRANCE



**BROWN ST  
REMOVAL PLAN**

**LEGEND**

-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
- STORM SEWER PIPE REMOVAL
- INTAKE OR UTILITY ACCESS REMOVAL
- SAWCUT



For Side Road Details Refer to E Sheets



**11TH ST  
REMOVAL PLAN**

ENGLISH IOWA DOT



DESIGN TEAM **WHKS & Co.**

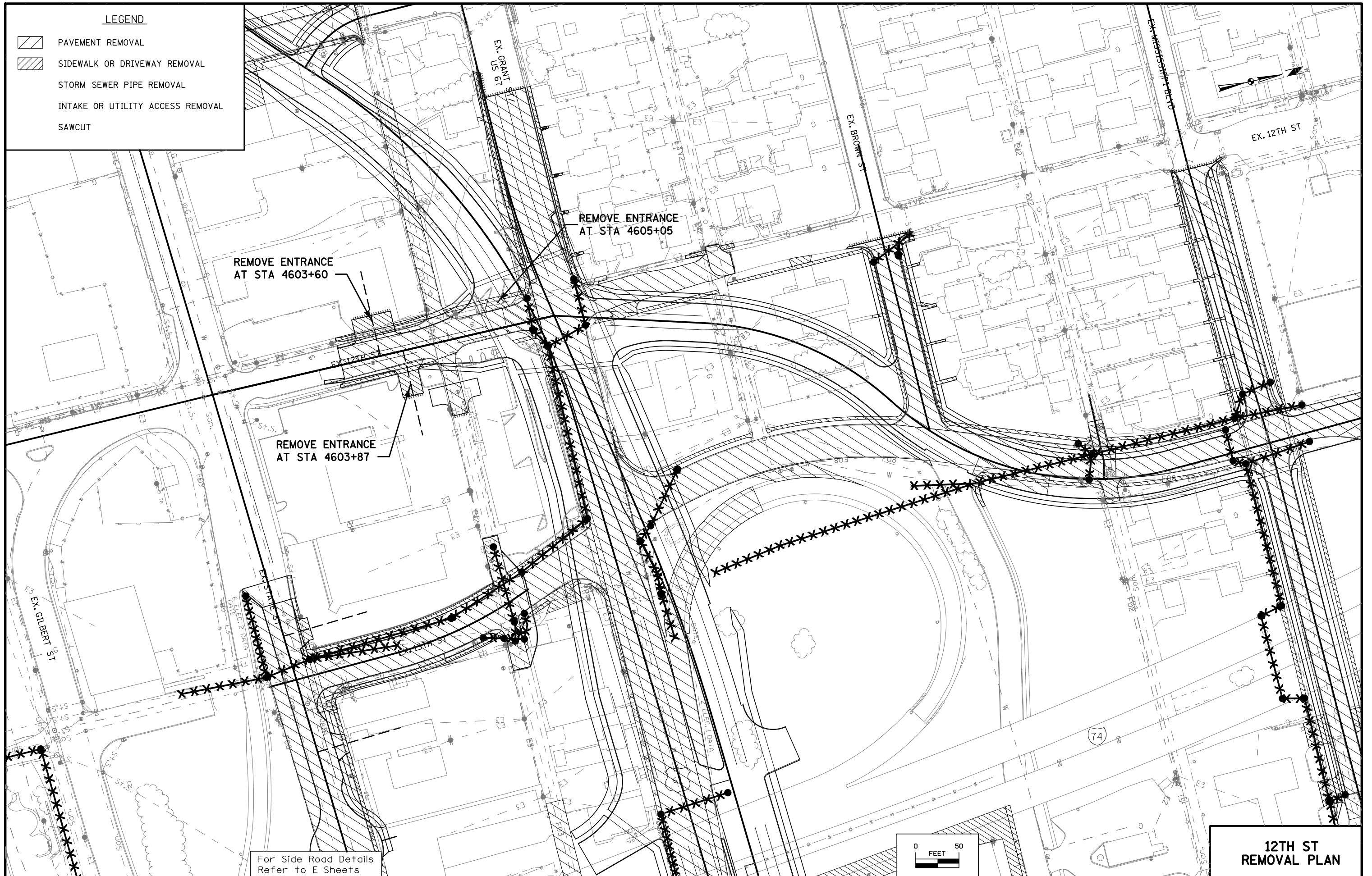
SCOTT COUNTY

PROJECT NUMBER **IM-74-1(226)4--13-82**

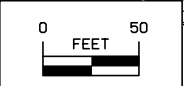
SHEET NUMBER **U.12**

LEGEND

-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
- STORM SEWER PIPE REMOVAL
- INTAKE OR UTILITY ACCESS REMOVAL
- SAWCUT








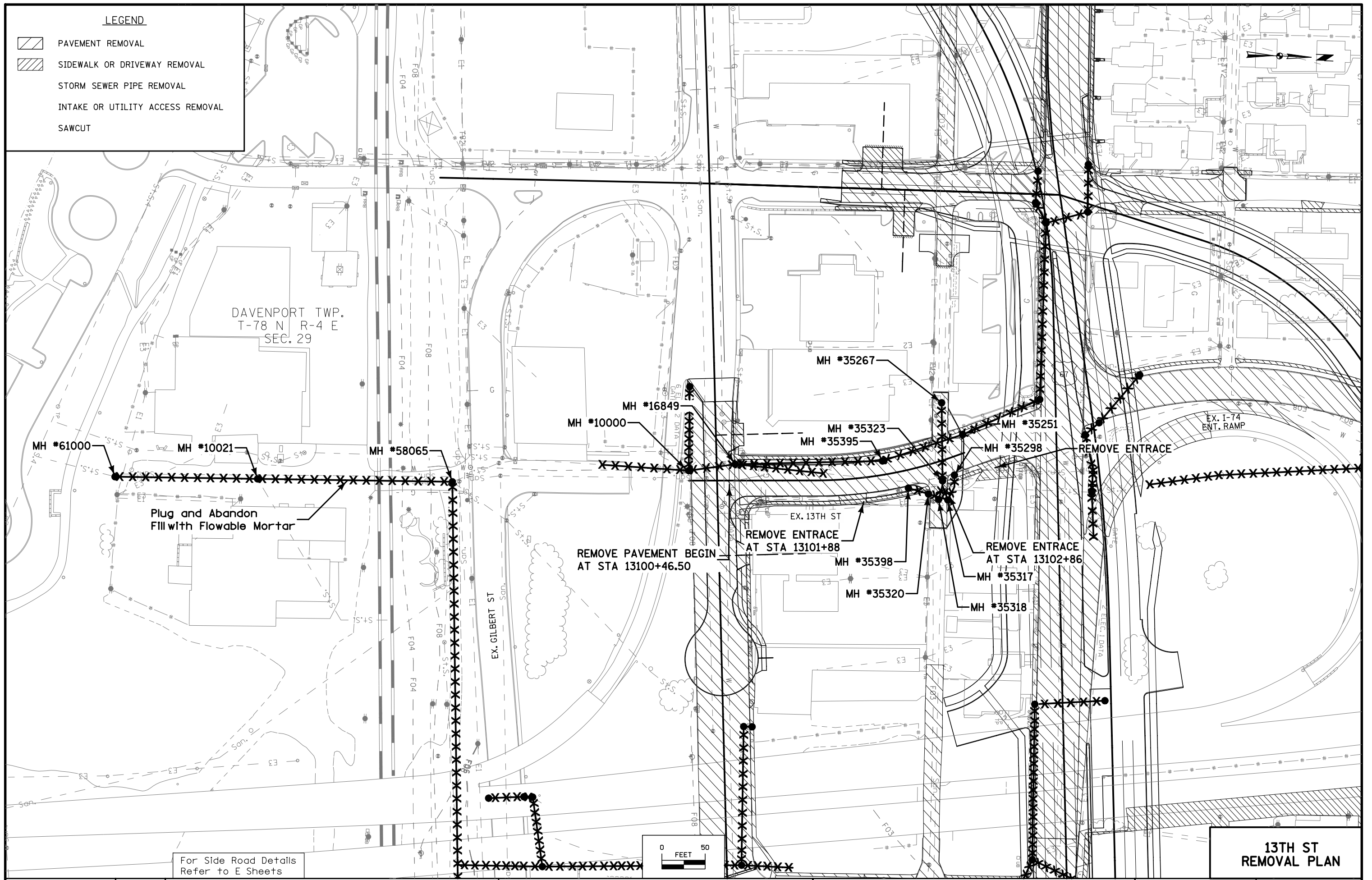
12TH ST  
REMOVAL PLAN



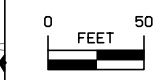
For Side Road Details  
Refer to E Sheets

**LEGEND**

-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
-  STORM SEWER PIPE REMOVAL
-  INTAKE OR UTILITY ACCESS REMOVAL
-  SAWCUT








For Side Road Details Refer to E Sheets



**13TH ST  
REMOVAL PLAN**

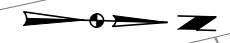
**LEGEND**

-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
-  STORM SEWER PIPE REMOVAL
-  INTAKE OR UTILITY ACCESS REMOVAL
-  SAWCUT

DAVENPORT TWP.  
T-78 N R-4 E  
SEC. 29

EX. I-74  
ENT. RAMP

74



REMOVE ALLEY  
NORTH OF GRANT ST

REMOVE PARKING  
LOT PAVEMENT

MH #28289

REMOVE TEMPORARY  
PAVEMENT

REMOVE PAVEMENT  
END AT GRANT ST

REMOVE ALLEY  
AT STA 18+12.7

REMOVE PAVEMENT  
BEGIN AT GRANT ST

MH #24600  
REMOVE ENTRANCE

REMOVE PAVEMENT  
BEGIN AT TEMPORARY RAMP

MH #32492

MH #26052

EX. 14TH ST

EX. 14TH ST

REMOVE ENTRANCE

REMOVE ENTRANCE

MH #55256

REMOVE ENTRANCE

REMOVE ALLEY SOUTH  
OF BROWN ST

REMOVE ALLEY SOUTH  
OF GRANT ST

EX. MISSISSIPPI BLVD

EX. 15TH ST

EX. 15TH ST

EX. BROWN ST

EX. MISSISSIPPI BLVD








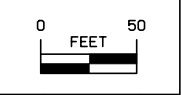
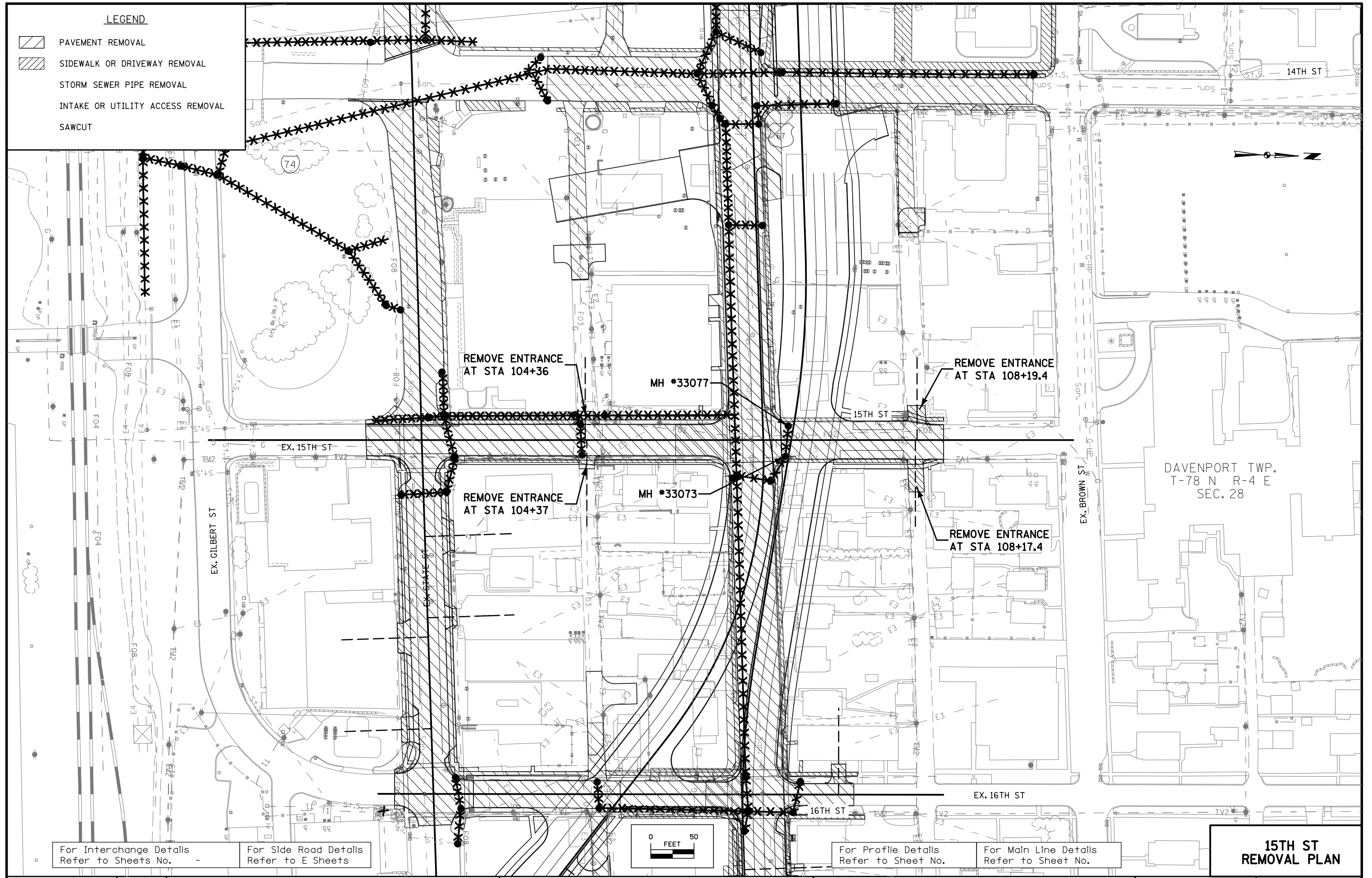
For Side Road Details  
Refer to E Sheets

**14TH ST  
REMOVAL PLAN**



**LEGEND**

-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
-  STORM SEWER PIPE REMOVAL
-  INTAKE OR UTILITY ACCESS REMOVAL
-  SAWCUT



For Interchange Details  
Refer to Sheets No. -






For Side Road Details  
Refer to E Sheets

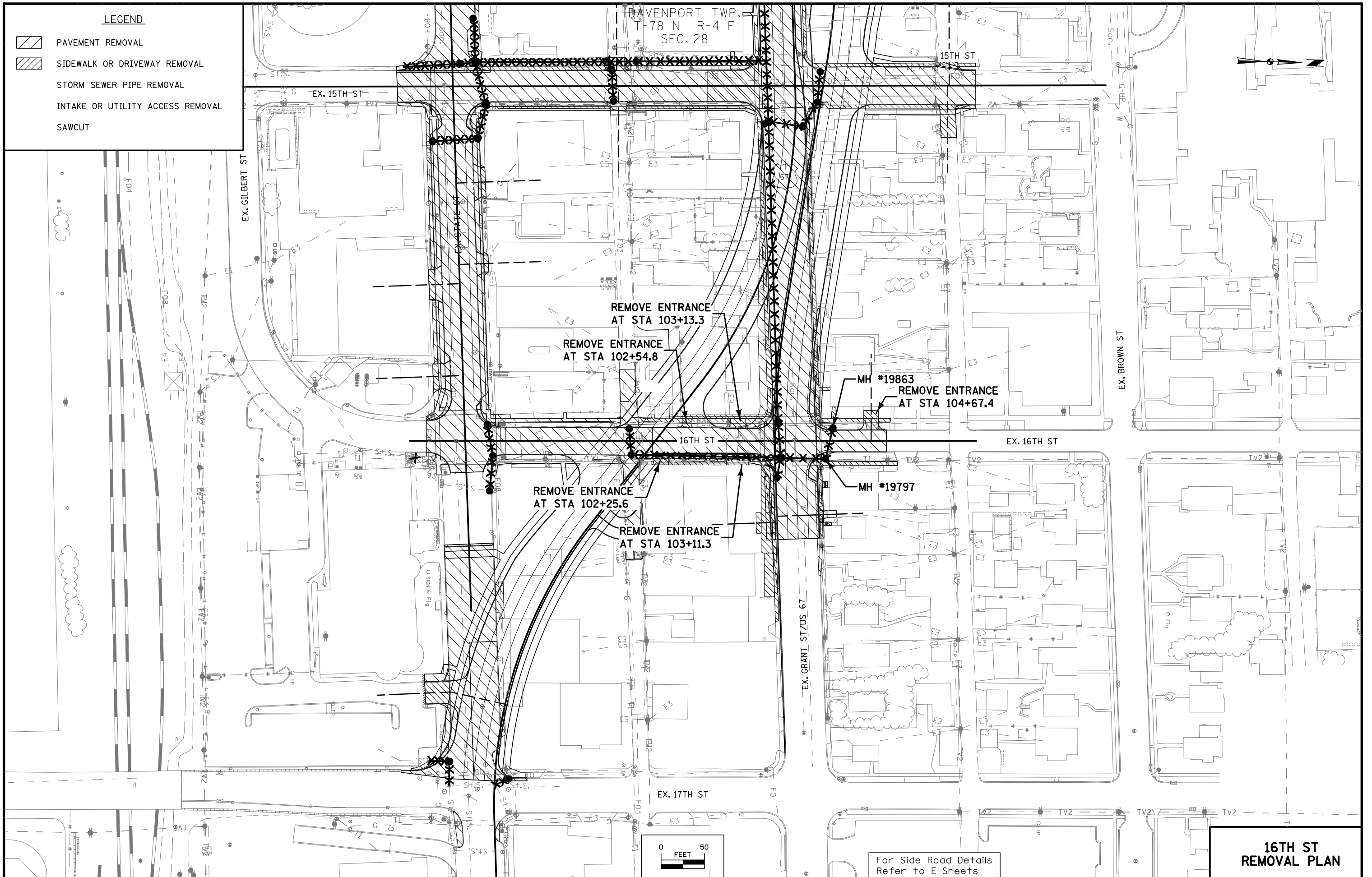
For Profile Details  
Refer to Sheet No.

For Main Line Details  
Refer to Sheet No.

**15TH ST  
REMOVAL PLAN**

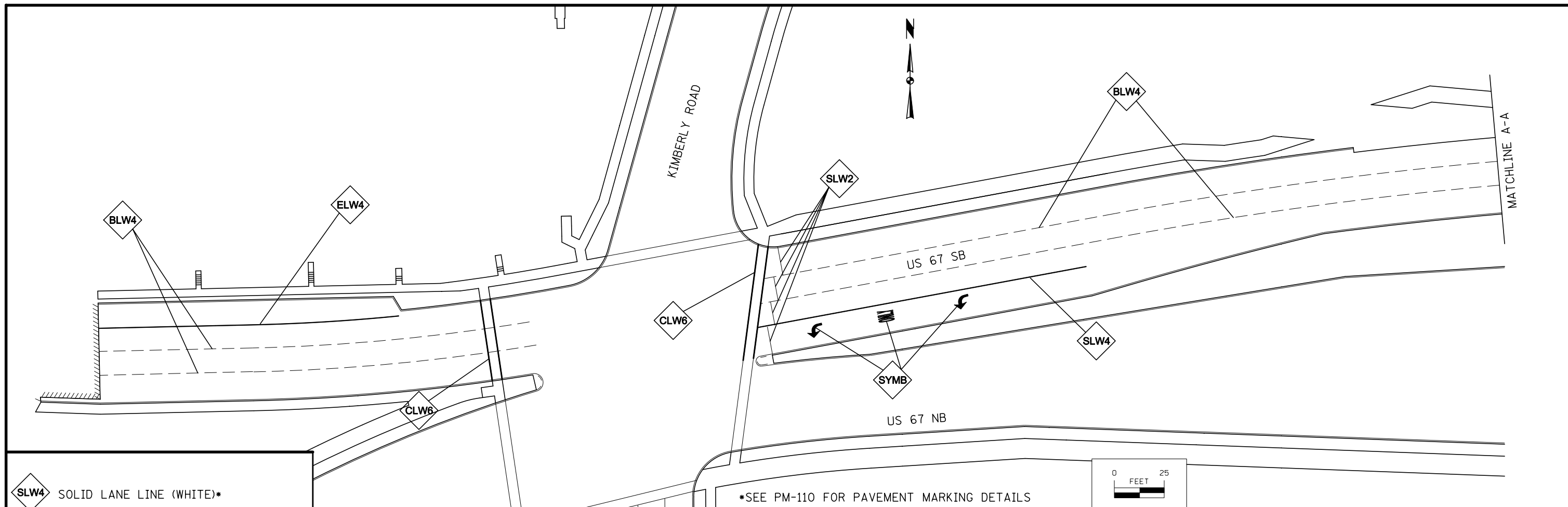
**LEGEND**






-  PAVEMENT REMOVAL
-  SIDEWALK OR DRIVEWAY REMOVAL
-  STORM SEWER PIPE REMOVAL
-  INTAKE OR UTILITY ACCESS REMOVAL
-  SAWCUT

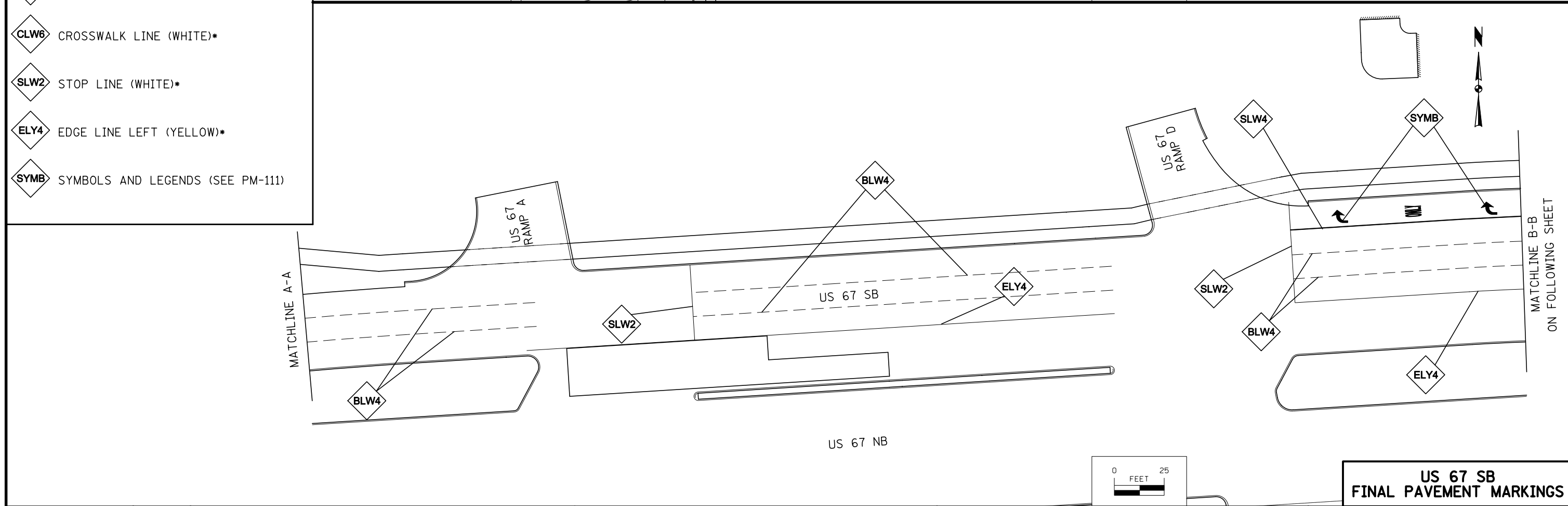


DAVENPORT TWP.  
78 N R-4 E  
SEC. 28

**16TH ST  
REMOVAL PLAN**



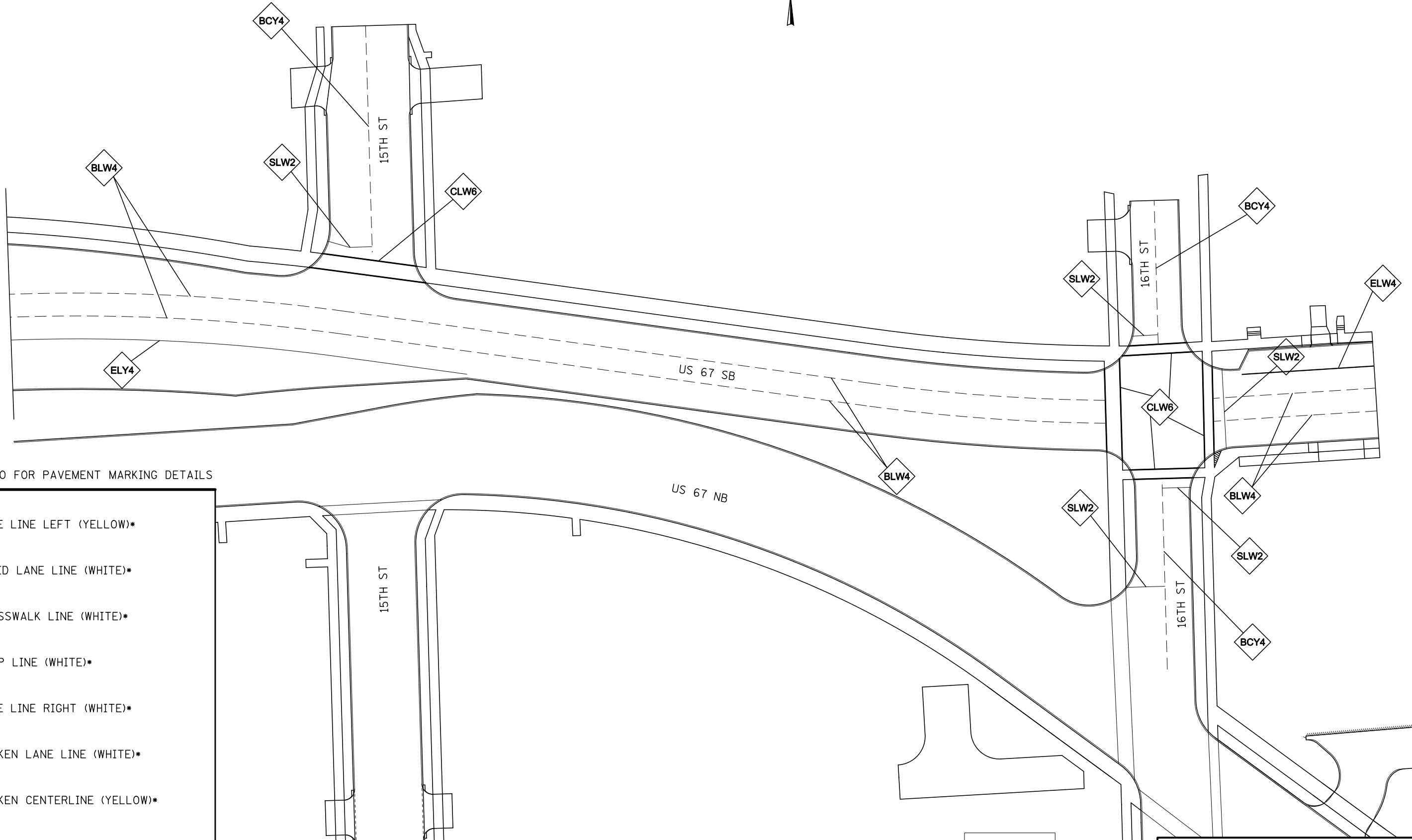
-  SOLID LANE LINE (WHITE)\*
-  CROSSWALK LINE (WHITE)\*
-  STOP LINE (WHITE)\*
-  EDGE LINE LEFT (YELLOW)\*
-  SYMBOLS AND LEGENDS (SEE PM-111)











**US 67 SB  
FINAL PAVEMENT MARKINGS**

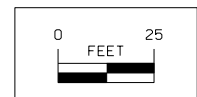


MATCHLINE B-B  
ON PREVIOUS SHEET











\*SEE PM-110 FOR PAVEMENT MARKING DETAILS

-  ELY4 EDGE LINE LEFT (YELLOW)\*
-  SLW4 SOLID LANE LINE (WHITE)\*
-  CLW6 CROSSWALK LINE (WHITE)\*
-  SLW2 STOP LINE (WHITE)\*
-  ELW4 EDGE LINE RIGHT (WHITE)\*
-  BLW4 BROKEN LANE LINE (WHITE)\*
-  BCY4 BROKEN CENTERLINE (YELLOW)\*
-  SYMB SYMBOLS AND LEGENDS (SEE PM-111)

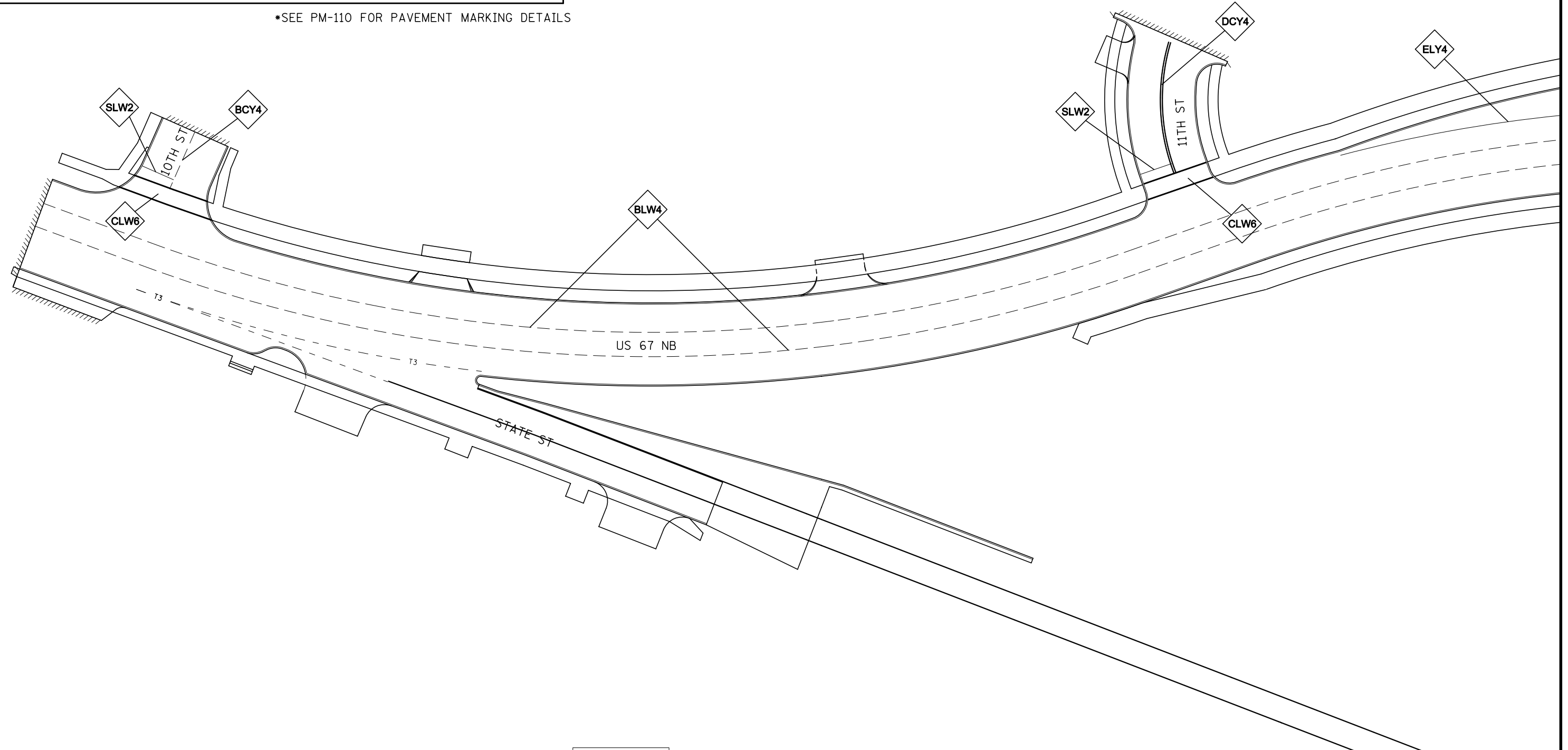


**US 67 SB, 15TH ST, 16TH ST  
FINAL PAVEMENT MARKINGS**

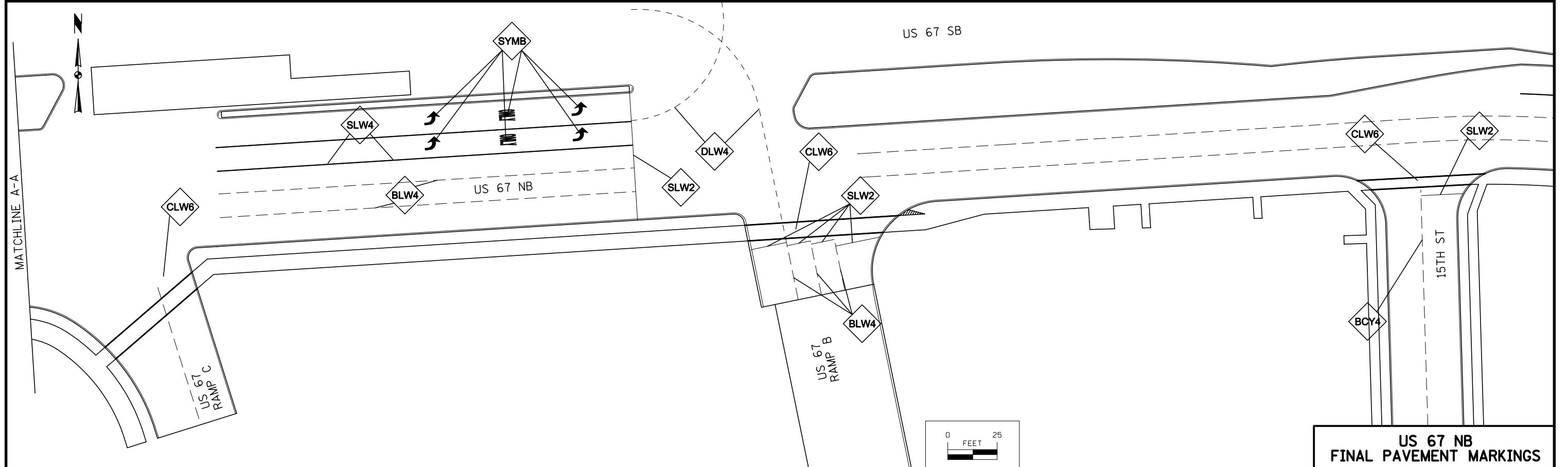
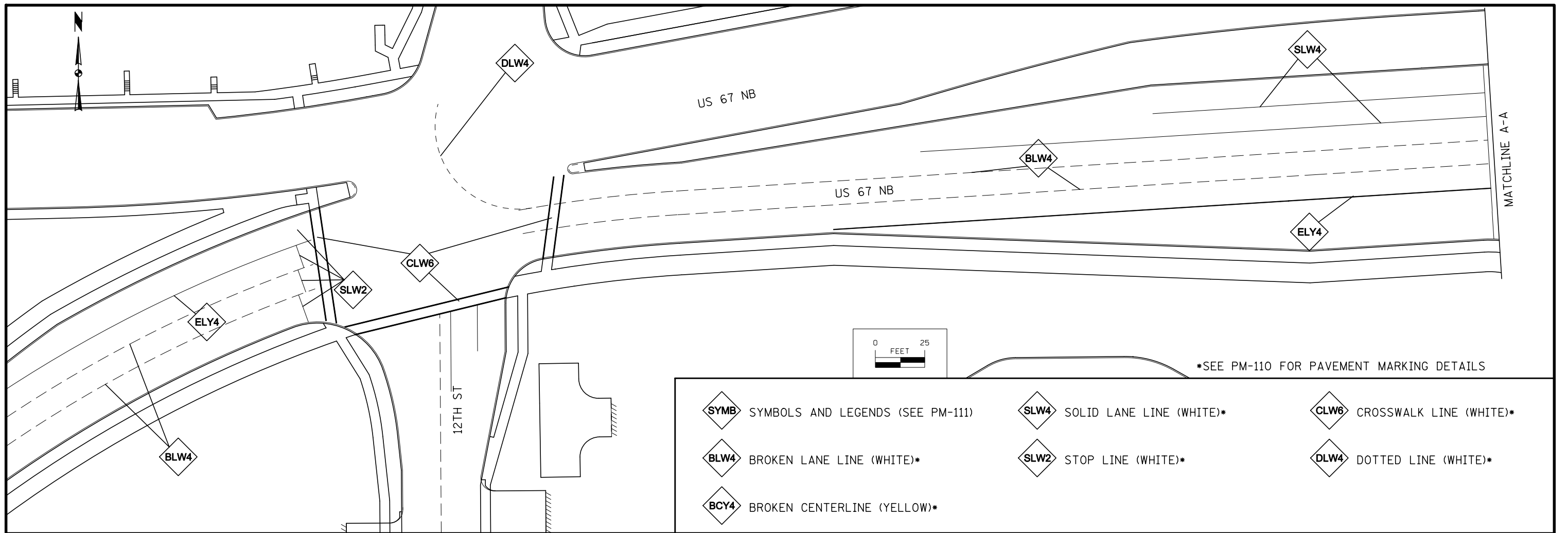
-  SOLID LANE LINE (WHITE)\*
-  CHANNELIZING LINE (YELLOW)\*
-  CROSSWALK LINE (WHITE)\*
-  EDGE LINE LEFT (YELLOW)\*
-  EDGE LINE RIGHT (WHITE)\*
-  NO PASSING ZONE LINE (YELLOW)\*
-  STOP LINE (WHITE)\*
-  SYMBOLS AND LEGENDS (SEE PM-111)

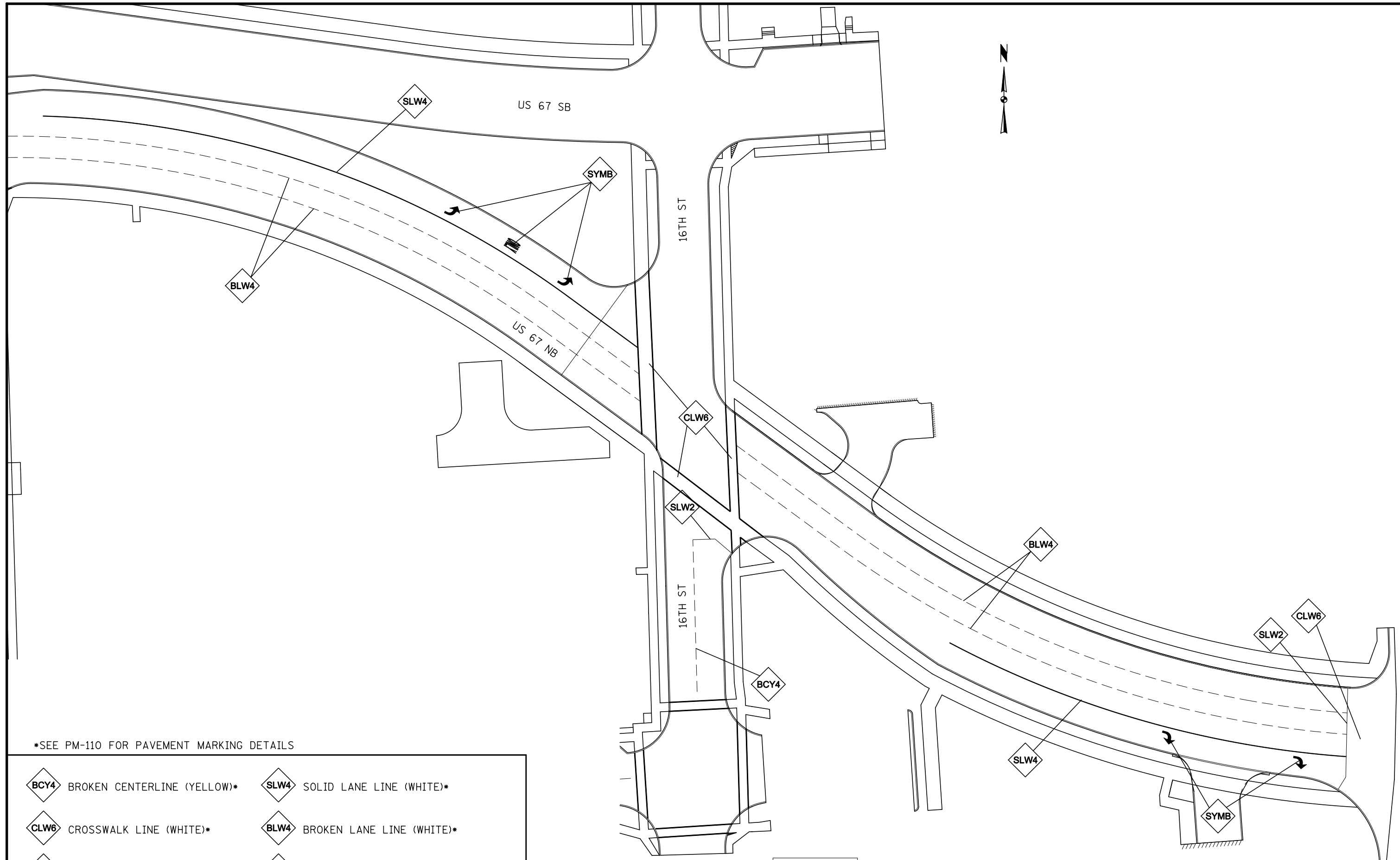


\*SEE PM-110 FOR PAVEMENT MARKING DETAILS









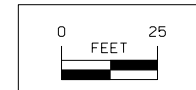
**US 67 NB, 10TH ST, 11TH ST  
FINAL PAVEMENT MARKINGS**



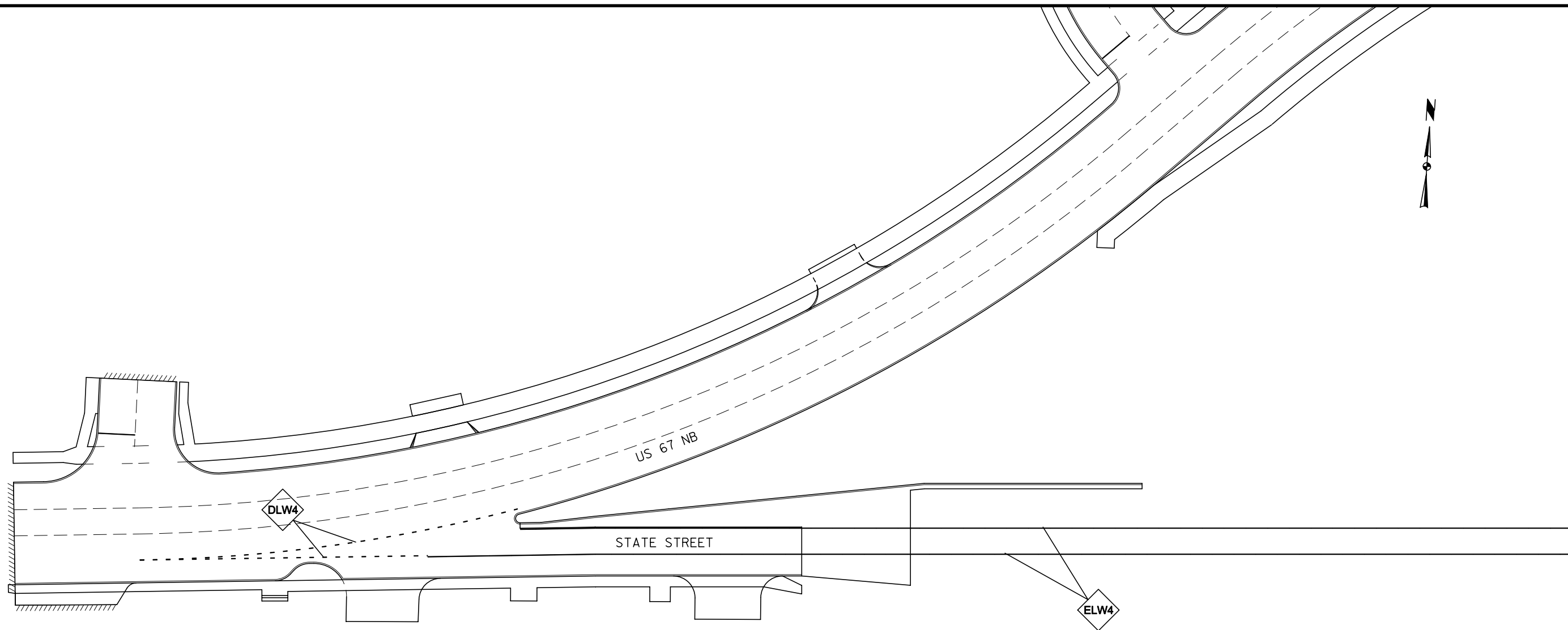


\*SEE PM-110 FOR PAVEMENT MARKING DETAILS



- |                                                                                                                 |                                                                                                                      |
|-----------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------|
|  BROKEN CENTERLINE (YELLOW)* |  SOLID LANE LINE (WHITE)*         |
|  CROSSWALK LINE (WHITE)*     |  BROKEN LANE LINE (WHITE)*        |
|  STOP LINE (WHITE)*          |  SYMBOLS AND LEGENDS (SEE PM-111) |

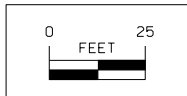


**US 67 NB, 16TH ST  
FINAL PAVEMENT MARKINGS**



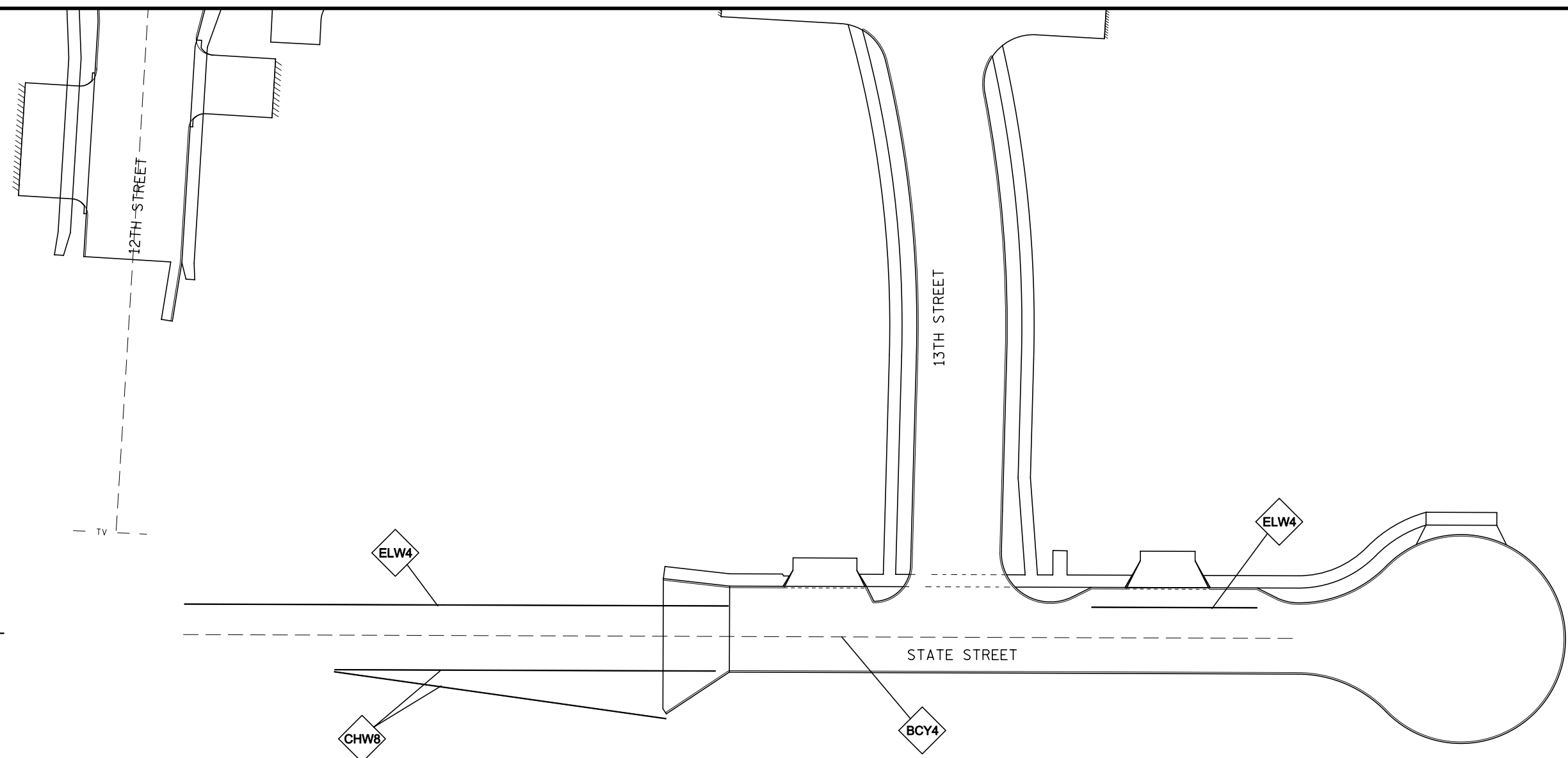
\*SEE PM-110 FOR PAVEMENT MARKING DETAILS

	SOLID LANE LINE (WHITE)*
	DOTTED LINE (WHITE)*






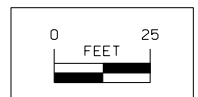
**STATE STREET  
FINAL PAVEMENT MARKINGS**



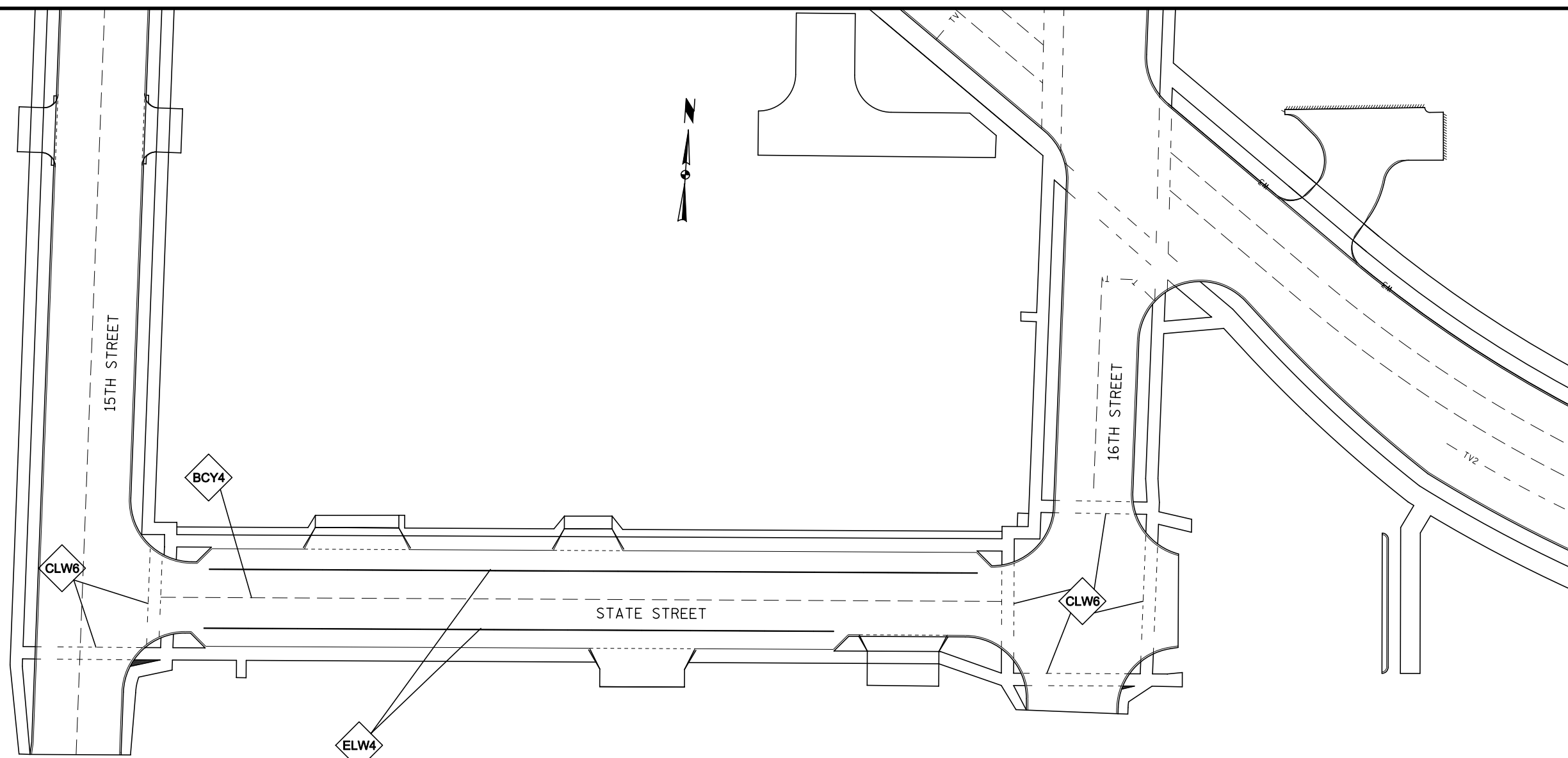


\*SEE PM-110 FOR PAVEMENT MARKING DETAILS




-  BROKEN CENTERLINE (YELLOW)\*
-  EDGE LINE RIGHT (WHITE)\*
-  CHANNELIZING LINE (WHITE)\*

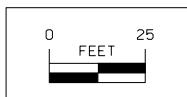


**STATE STREET  
FINAL PAVEMENT MARKINGS**

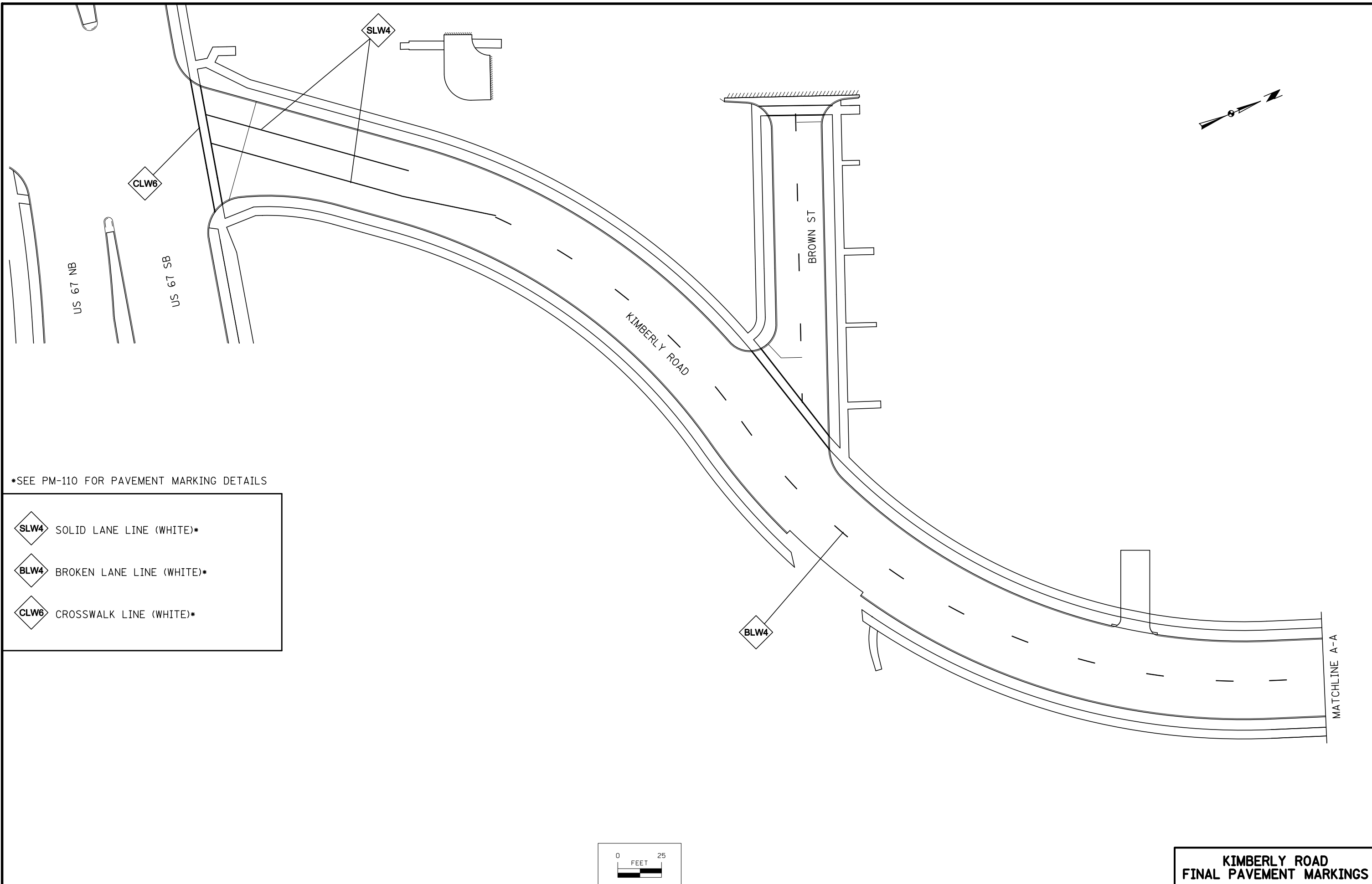


\*SEE PM-110 FOR PAVEMENT MARKING DETAILS

-  **BCY4** BROKEN CENTERLINE (YELLOW)\*
-  **ELW4** EDGE LINE RIGHT (WHITE)\*
-  **CLW6** CROSSWALK LINE (WHITE)\*

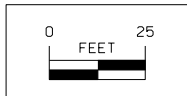


**STATE STREET  
FINAL PAVEMENT MARKINGS**

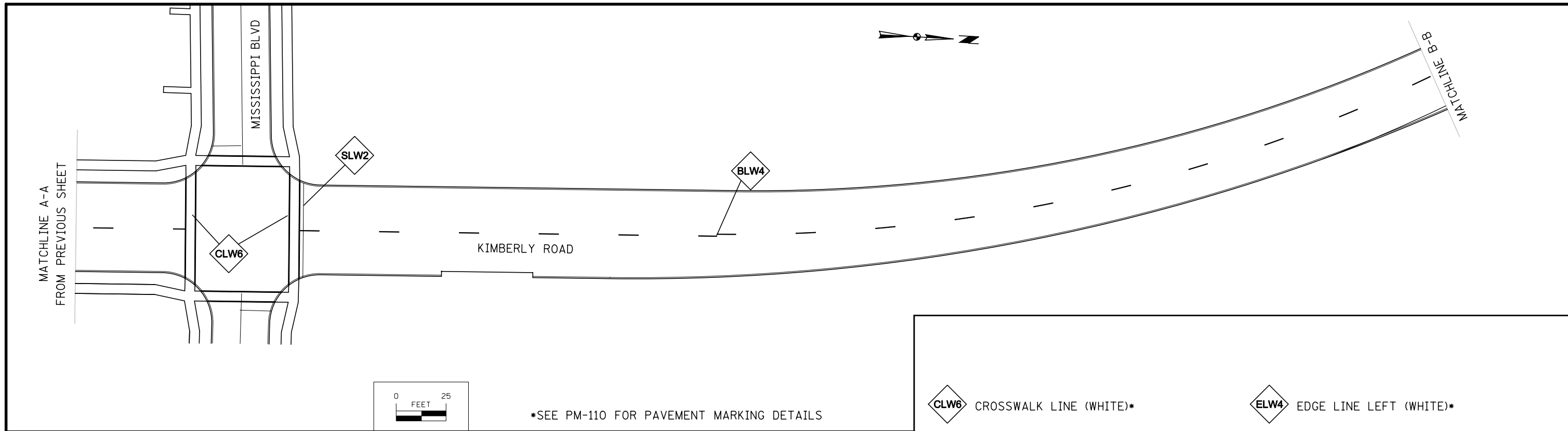






\*SEE PM-110 FOR PAVEMENT MARKING DETAILS

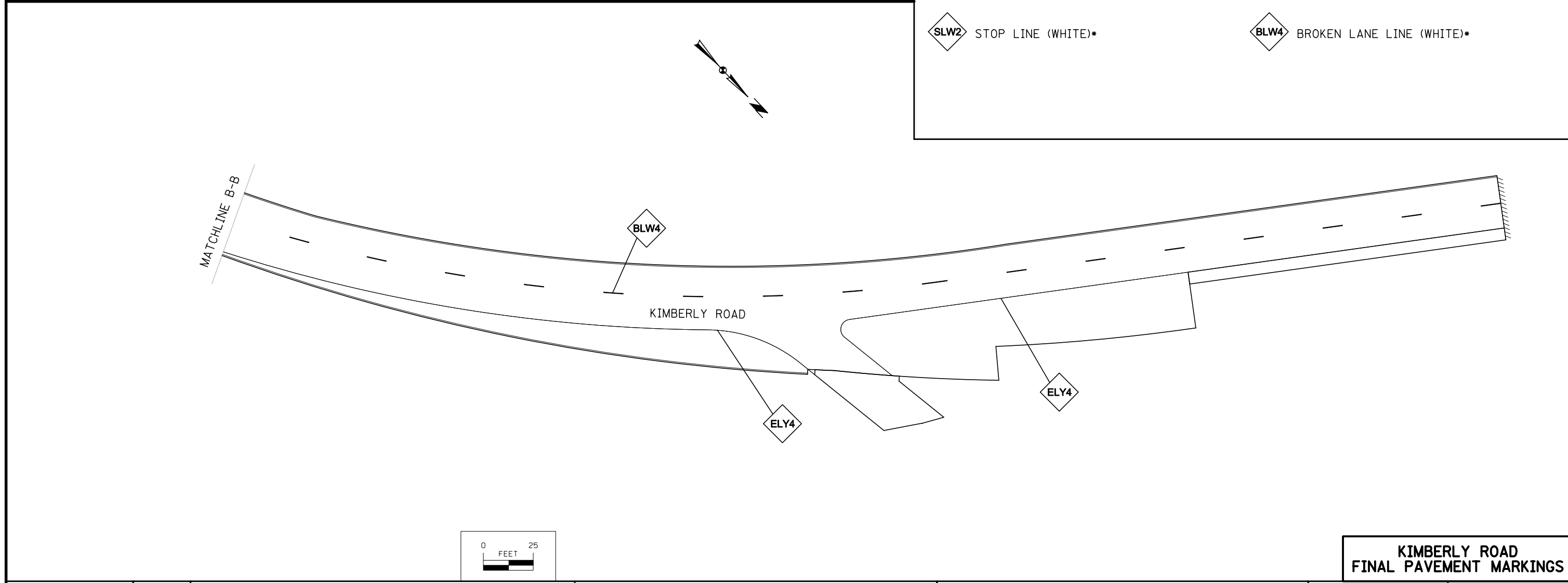
	SOLID LANE LINE (WHITE)*
	BROKEN LANE LINE (WHITE)*
	CROSSWALK LINE (WHITE)*






**KIMBERLY ROAD  
FINAL PAVEMENT MARKINGS**



 <b>CLW6</b> CROSSWALK LINE (WHITE)*	 <b>ELW4</b> EDGE LINE LEFT (WHITE)*
 <b>SLW2</b> STOP LINE (WHITE)*	 <b>BLW4</b> BROKEN LANE LINE (WHITE)*

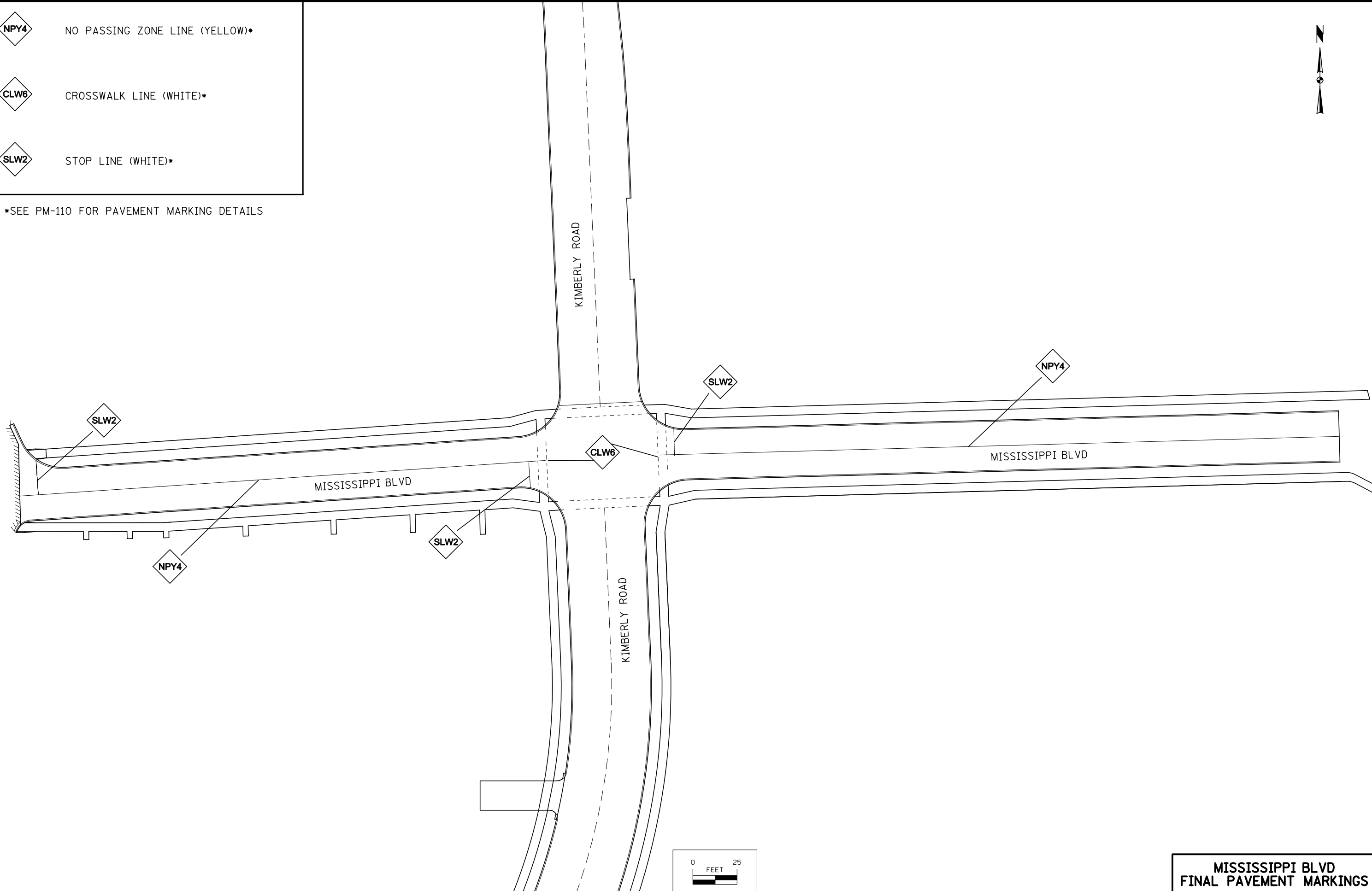


**KIMBERLY ROAD  
FINAL PAVEMENT MARKINGS**





-  NO PASSING ZONE LINE (YELLOW)\*
-  CROSSWALK LINE (WHITE)\*
-  STOP LINE (WHITE)\*



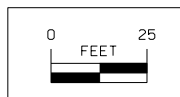
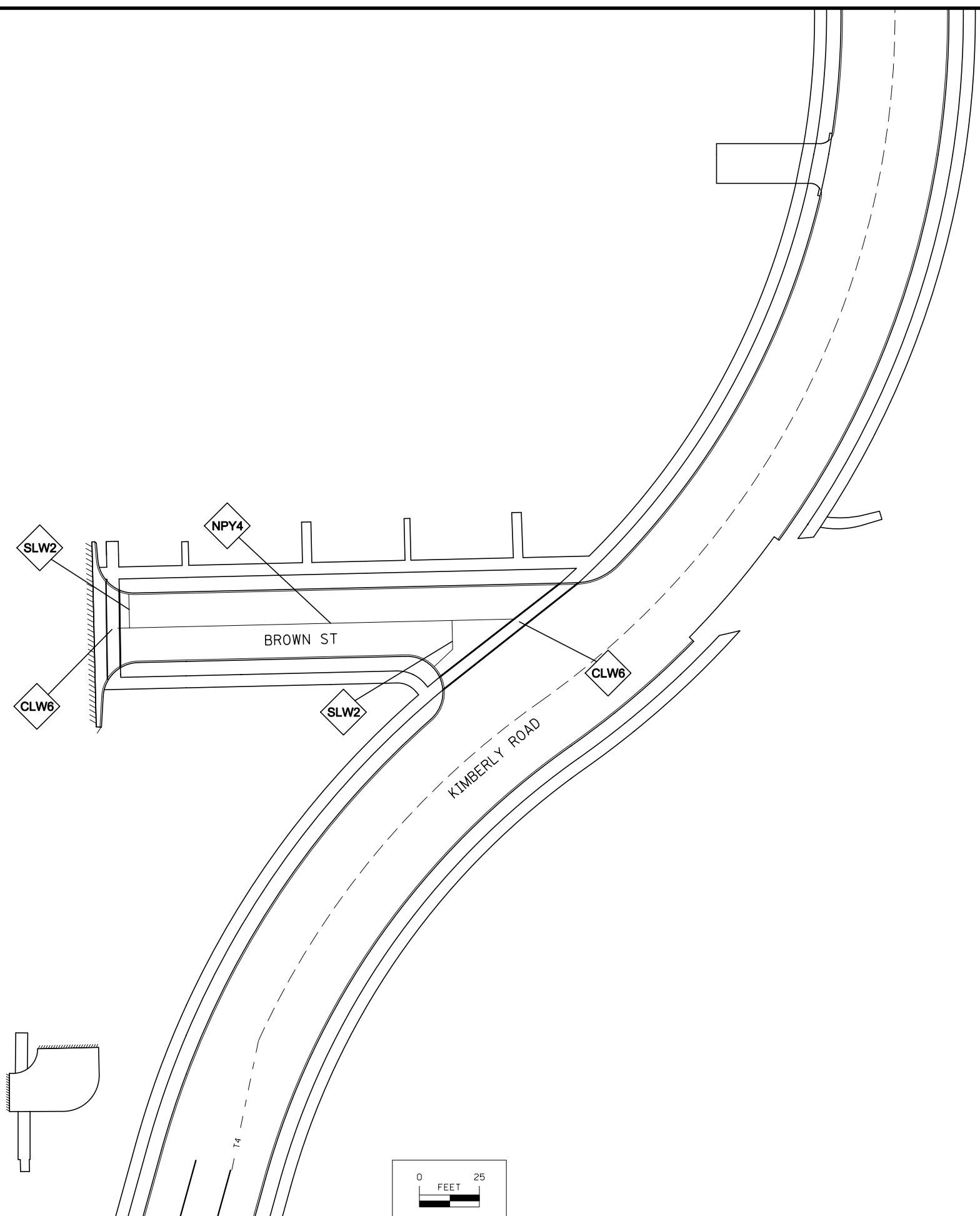
\*SEE PM-110 FOR PAVEMENT MARKING DETAILS



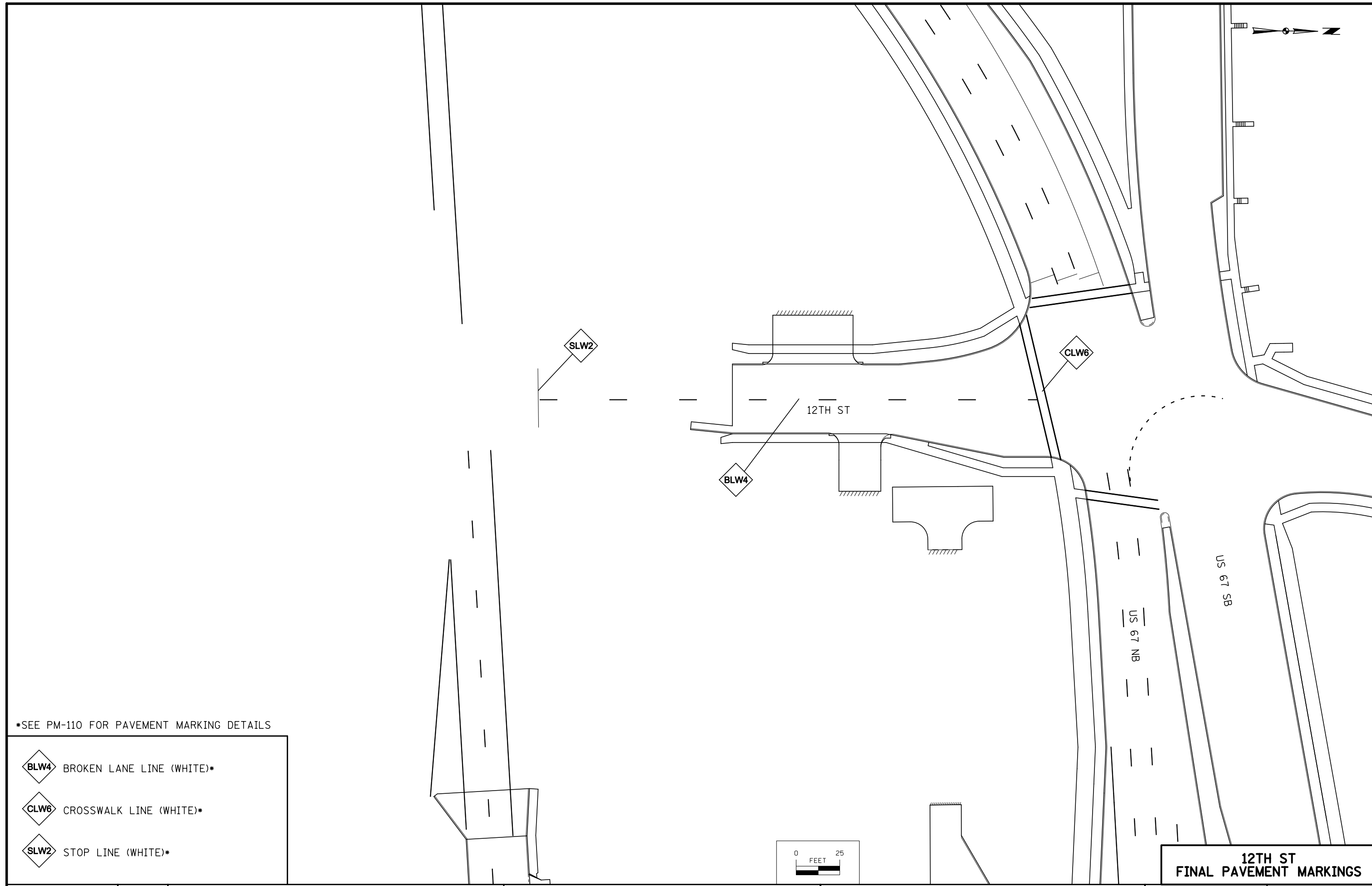
**MISSISSIPPI BLVD  
FINAL PAVEMENT MARKINGS**

-  NO PASSING ZONE LINE (YELLOW)\*
-  STOP LINE (WHITE)\*
-  EDGE LINE RIGHT (WHITE)\*
-  CROSSWALK LINE (WHITE)\*




\*SEE PM-110 FOR PAVEMENT MARKING DETAILS






**BROWN ST  
FINAL PAVEMENT MARKINGS**



\*SEE PM-110 FOR PAVEMENT MARKING DETAILS

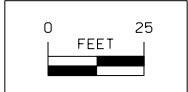
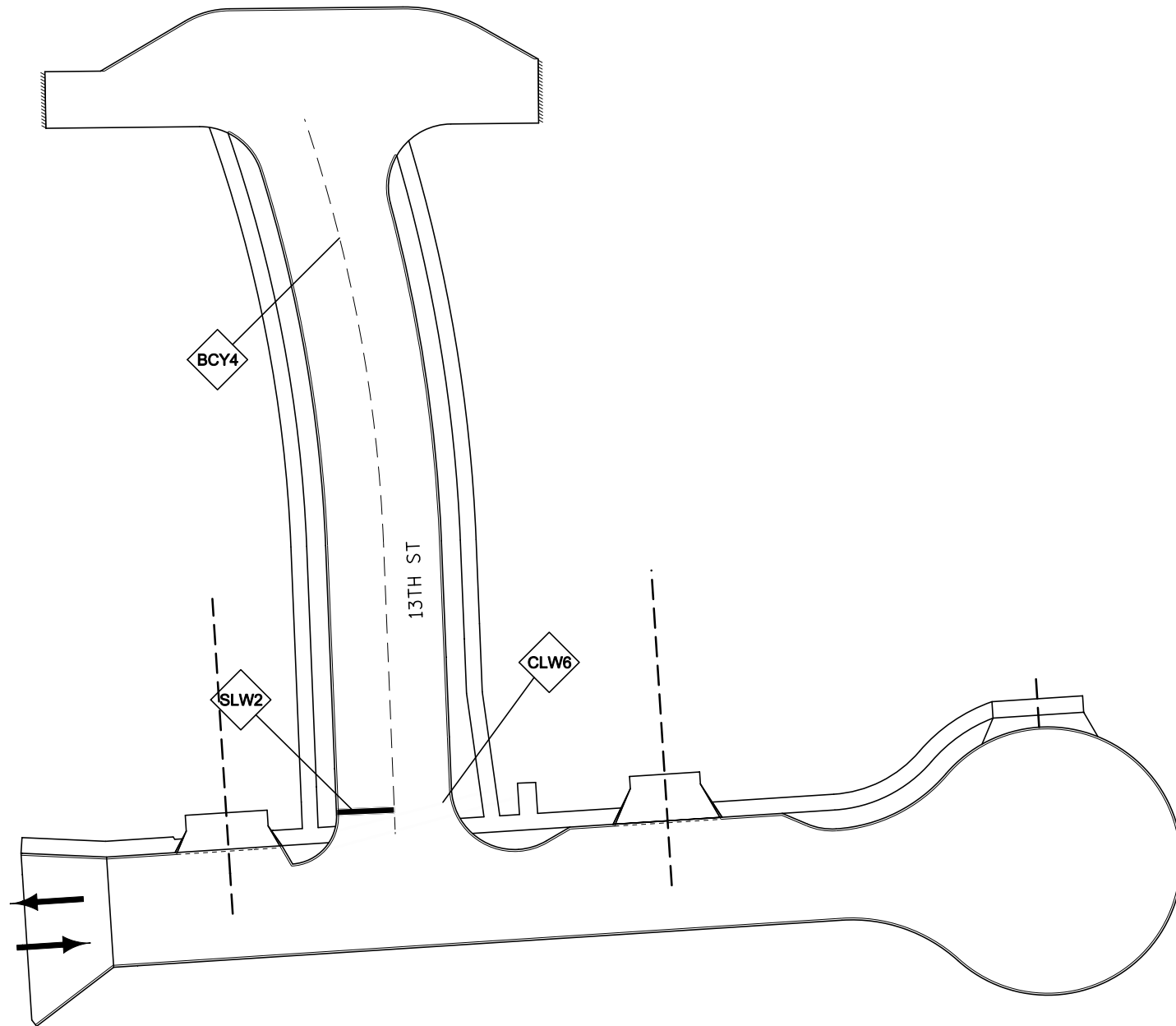
-  **BLW4** BROKEN LANE LINE (WHITE)\*
-  **CLW6** CROSSWALK LINE (WHITE)\*
-  **SLW2** STOP LINE (WHITE)\*

**12TH ST  
FINAL PAVEMENT MARKINGS**

-  BROKEN CENTERLINE (YELLOW)\*
-  STOP LINE (WHITE)\*
-  CROSSWALK LINE (WHITE)\*

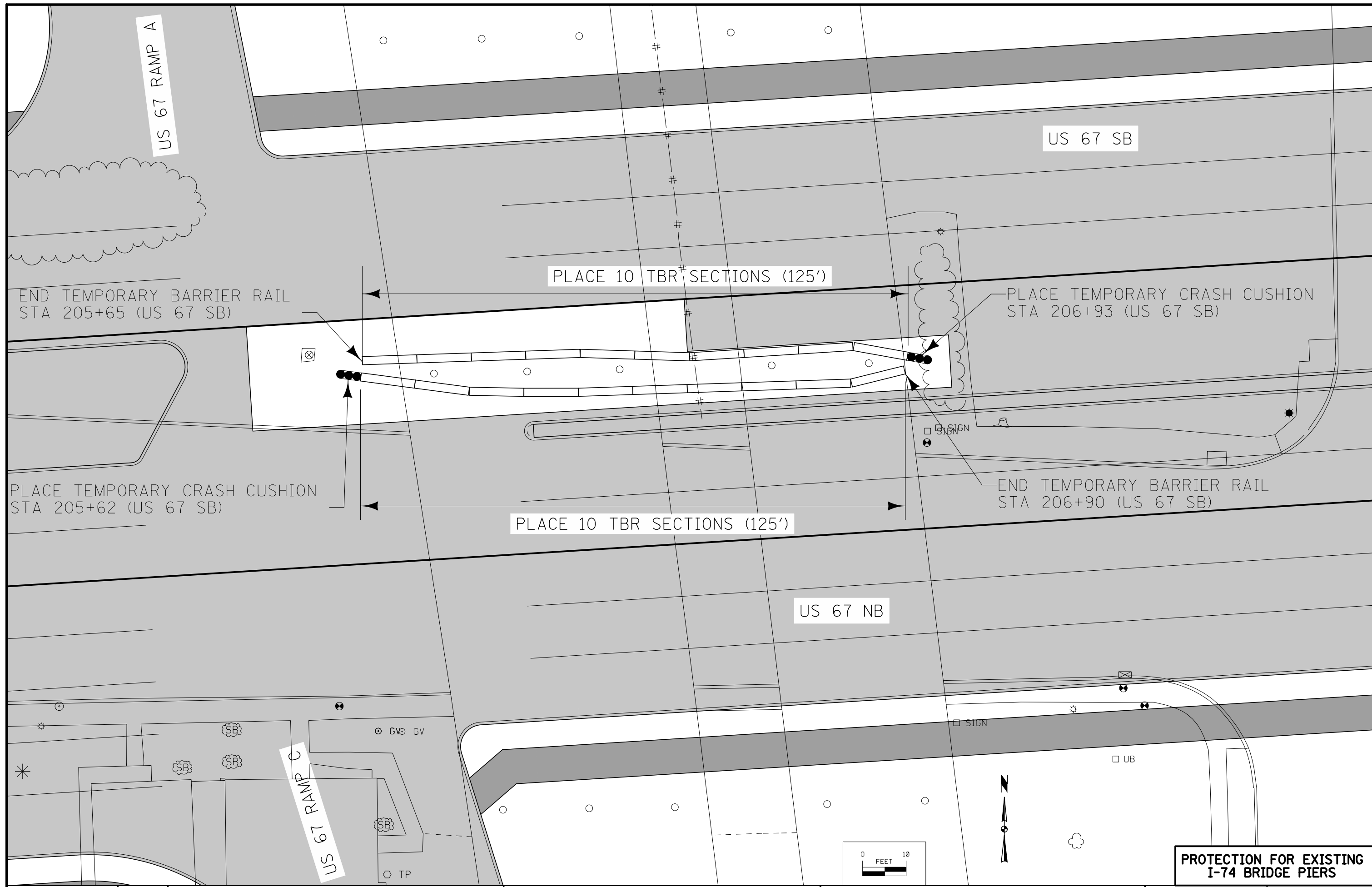


\*SEE PM-110 FOR PAVEMENT MARKING DETAILS



**13TH ST  
FINAL PAVEMENT MARKINGS**





US 67 RAMP A

US 67 SB

PLACE 10 TBR# SECTIONS (125')

END TEMPORARY BARRIER RAIL  
STA 205+65 (US 67 SB)

PLACE TEMPORARY CRASH CUSHION  
STA 206+93 (US 67 SB)

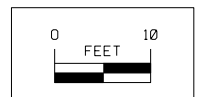
PLACE TEMPORARY CRASH CUSHION  
STA 205+62 (US 67 SB)

PLACE 10 TBR SECTIONS (125')

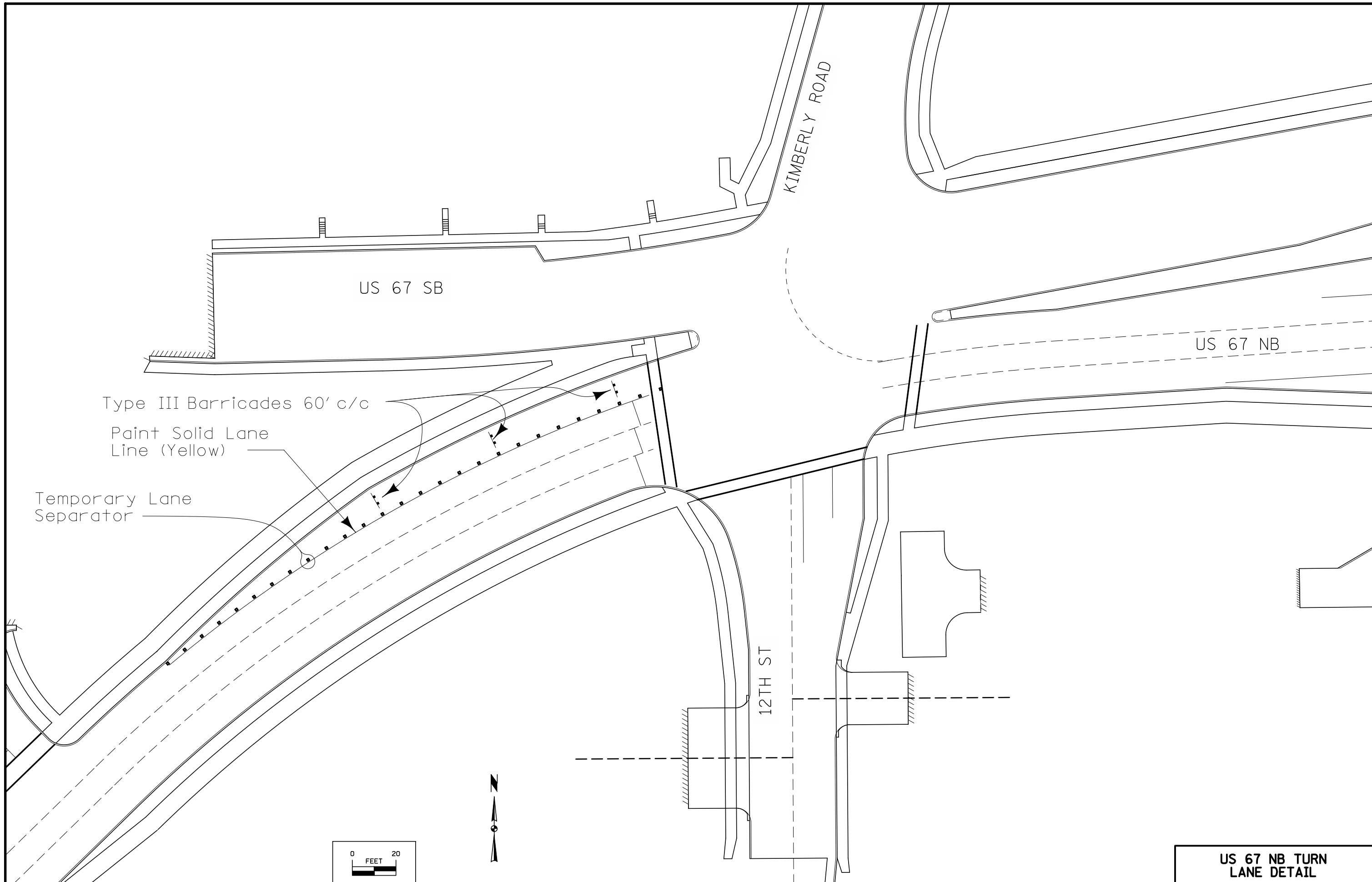
END TEMPORARY BARRIER RAIL  
STA 206+90 (US 67 SB)

US 67 NB

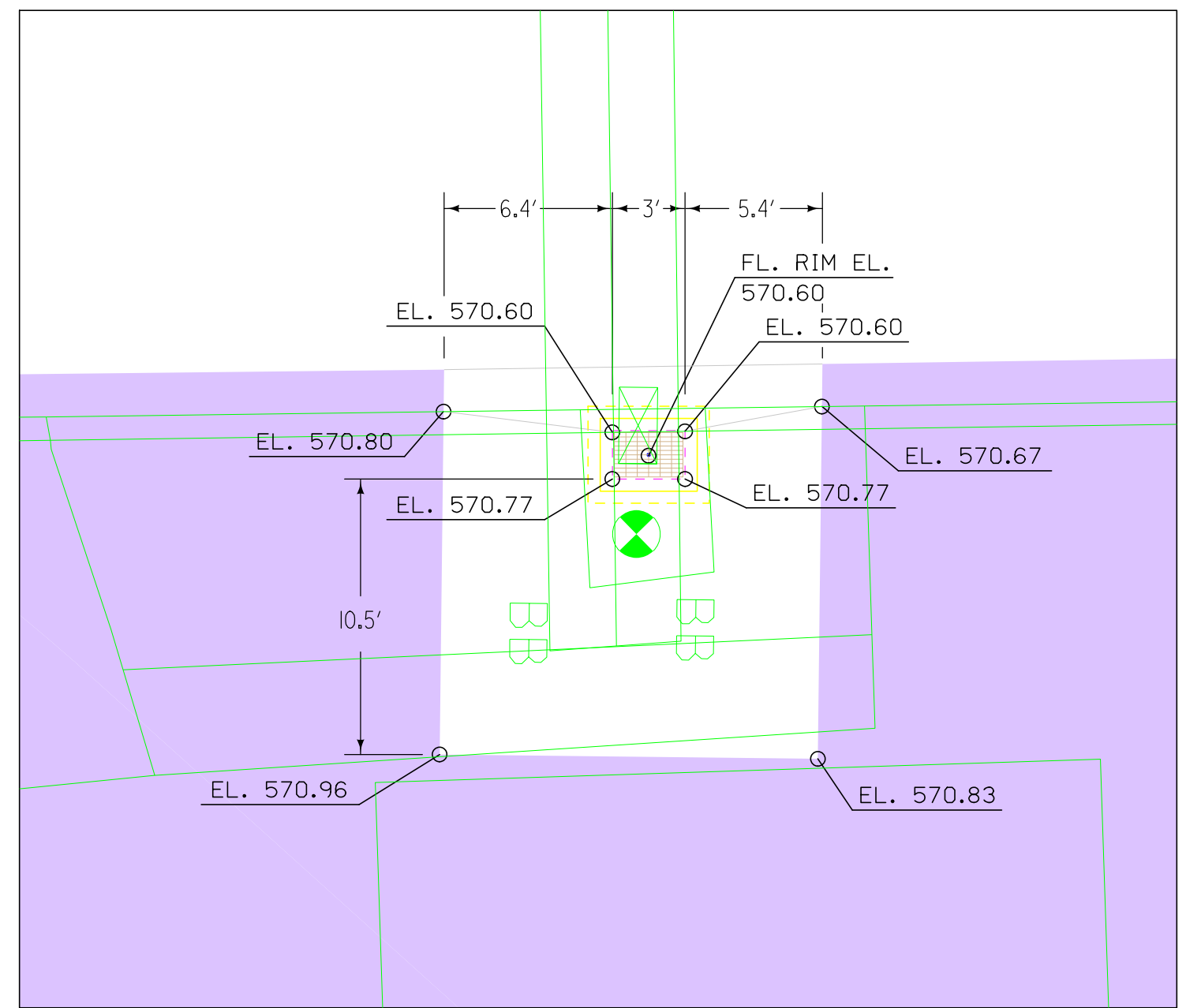
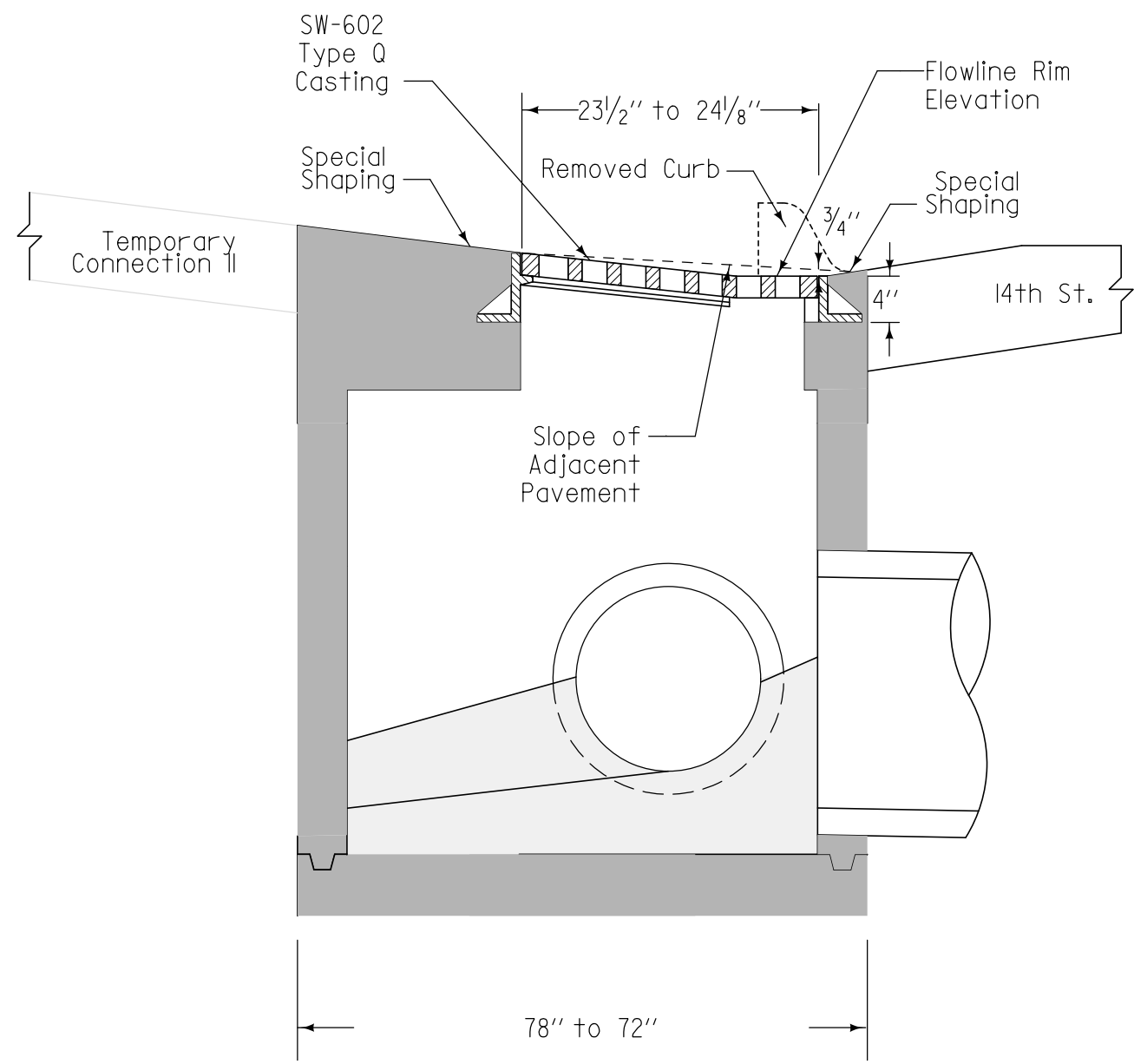
US 67 RAMP C



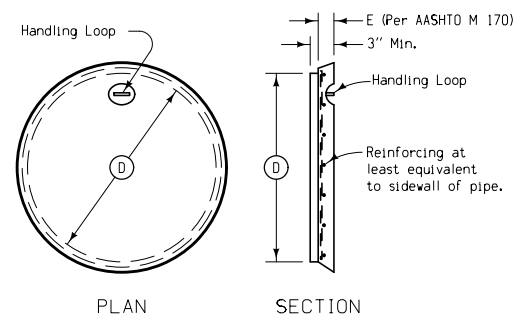
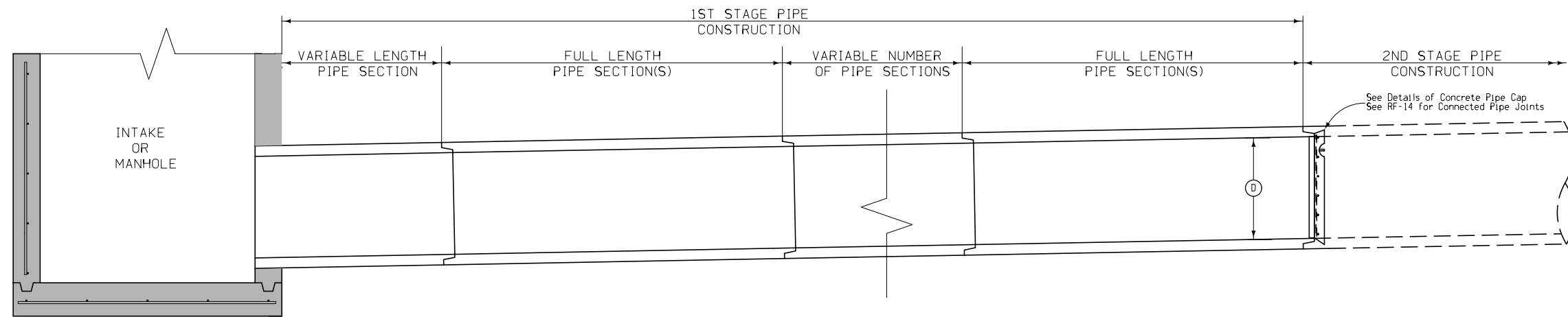
PROTECTION FOR EXISTING  
I-74 BRIDGE PIERS



**US 67 NB TURN  
LANE DETAIL**



DETAIL FOR TEMPORARY INTAKE  
FOR TEMPORARY CONNECTION 11



DETAILS OF CONCRETE PIPE CAP

**CONCRETE PIPE CAP:**

The use of an approved pipe cap is required when so indicated on the detail project plans. The dimensions of the pipe cap shall be such as to neatly fit the groove end of the appropriate size of pipe.

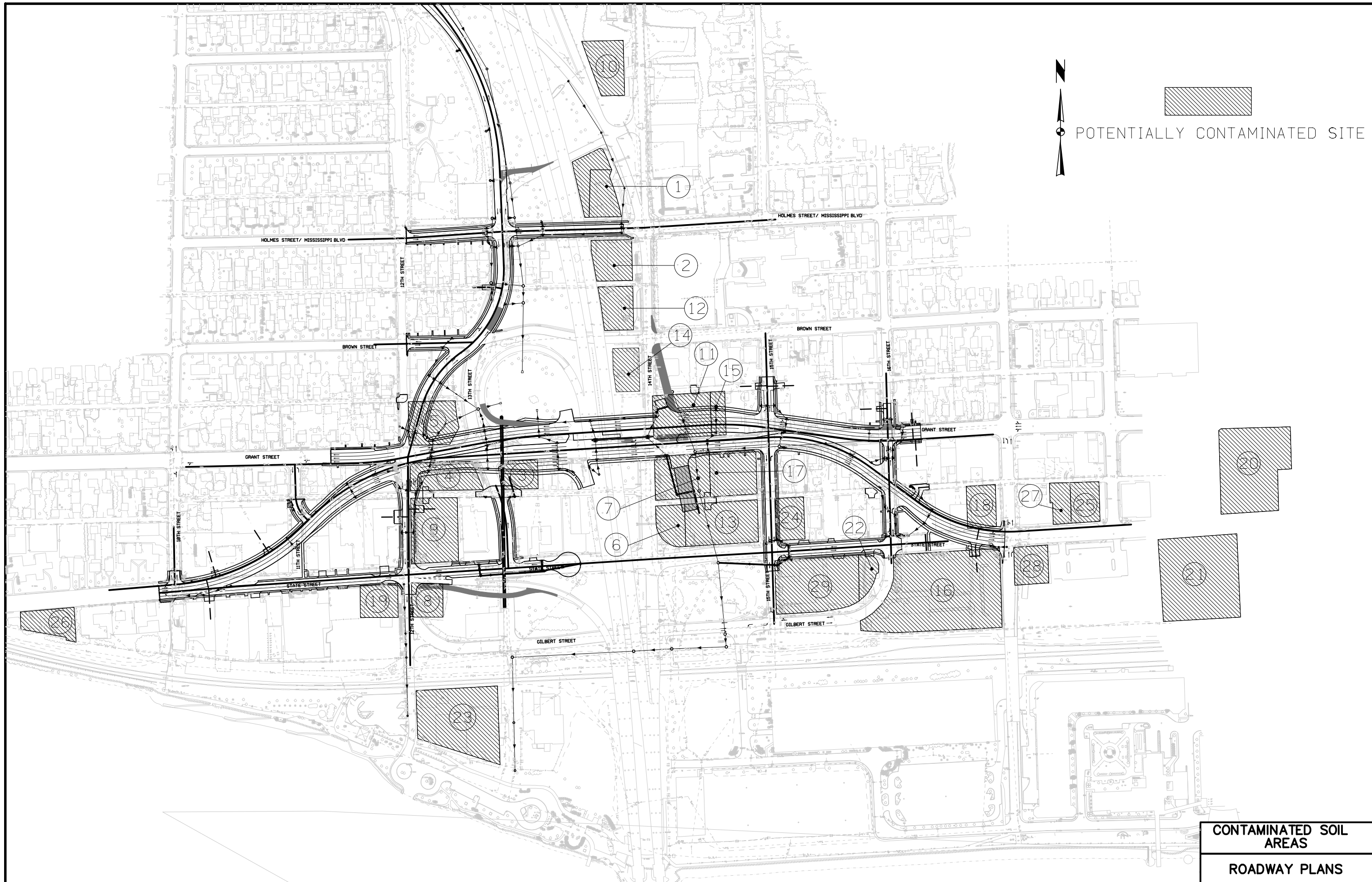
The cap must be precast and an approved bituminous joint material shall be placed between the cap and the pipe.

The Pipe Cap placement or removal shall not be paid for directly, but when specified, shall be considered to be incidental to other pipe work on the project.

1st Stage: Install Concrete Pipe Cap where specified.

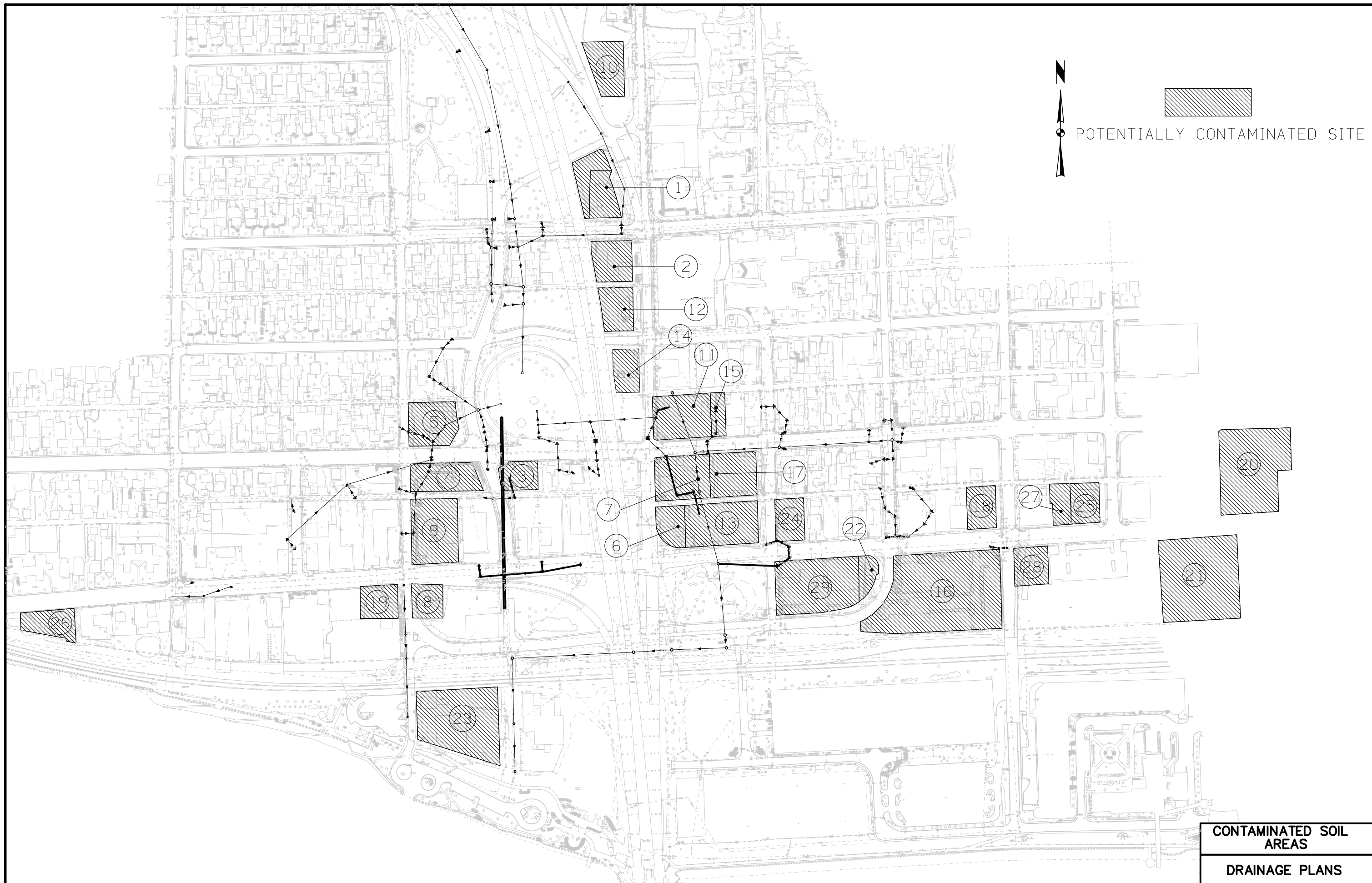
2nd Stage: Remove Concrete Pipe Cap prior to connecting to existing pipes.

**DETAIL OF STAGED STORM SEWER  
PIPE CONSTRUCTION  
AND CONCRETE PIPE CAP**



**CONTAMINATED SOIL  
AREAS**

**ROADWAY PLANS**



**CONTAMINATED SOIL AREAS**  
**DRAINAGE PLANS**

Contaminated Soil Summary				PCSS-1
				SPECIAL
Location	Parcel # See HE Sheets	Description and Address	Site Impacted by this Contract Yes/No	Remarks
1	380	H & H Car Care Center	Yes	4 UST removed 1990.
		612 14th Street		DNR - "No Action Req'd" Feb. 2005.
				Petroleum contamination in soil boring.
2	374	Dale Snapp Co.	Yes	2 UST removed 1998.
		536 14th Street		Petroleum contamination in recent soil boring.
3	345	Crescent Economy Inc. 1303 Grant Street	Yes	No tanks. Dry cleaning chemicals present. DNR statewide standards exceeded (Oct. 2005).
4	346	Showboat Car Wash 1215 Grant Street	Yes	3 UST removed 1992. DNR - "No Action Req'd" Aug. 2003.
5	353	Hoyt & Son Automotive 1210 Grant Street	Yes	5 UST removed 1996. Previous soil removal project DNR - "No Action Req'd" July 2003.
6	341	Johnny's Amoco BP/QC Mart 1402 State Street	Yes	Total of 9 UST: 5 removed 1989 and 4 active; DOT to request owners to remove tanks. DNR - "No Action Req'd" Sep. 2004. Petroleum contamination in soil boring.
7	341A	Twin Bridges 66/Shell Oil 333 14th Street	Yes	Total of 4 UST: 1 removed 1993 and 3 active; DOT to request owners to remove tanks; if not removed then Iowa DOT OLE to remove prior to letting. Petroleum contamination in recent soil boring.
8	312	Adel Parking Lot 1207 State Street	Yes	Former gas station. Now part of QCA Spa. Petroleum contamination in ground water from monitoring wells.
9	324	Village Inn 1210 State Street	Yes	Petroleum contamination in recent soil borings.
10	386	Great American Window Co 710 14th Street	No	Petroleum contamination in ground water from monitoring wells.
11	355	Dart Mart/Big 10 Mart	Yes	Total of 5 UST: 1 removed 1990 and 4 active; DOT to request owners to remove tanks; if not removed then Iowa DOT OLE to remove prior to letting.
		411 14th Street		Contamination documented in monitoring wells.

Contaminated Soil Summary				PCSS-1
				SPECIAL
Location	Parcel # See HE Sheets	Description and Address	Site Impacted by this Contract Yes/No	Remarks
12	372	Ross' Drive Through 512 14th Street	No	No action necessary. No contamination identified.
13	320	Knox Corporation 1416 State Street	Yes	No action necessary. No contamination identified.
14	339	Ross' Restaurant Inc 430 14th Street	Yes	Contamination documented in monitoring wells.
15	357	Handy Shop 1430 Grant Street	Yes	3 UST removed 1992, 2005. Increasing contamination levels in monitoring wells. DNR "No Action Req'd" March 2001.
16	311	City Hall 1609 State Street	Yes	Total of 5 UST: 3 UST removed 1988 and one active. Petroleum contamination in recent soil boring.
17	339	US West 1437 Grant Street	Yes	1 UST removed 1993. No contamination identified.
18	331	Car Quest 312 17th Street	Yes	Contamination documented in monitoring wells.
19	NA	Adel Parking Lot 1159 State Street	No	Owner denied access to property. Potential UST.
20	NA	Lindquist Ford 1910 State Street	No	8 UST removed 1997. DNR "No Action Req'd" Nov. 1998.
21	NA	Plaza Building 1823 State Street	No	Petroleum contamination identified.
22	NA	Kelley's Gas 1543 State Street	No	Total of 5 UST: 2 removed 2000 and 3 active (2 - 6000 gal and 1- 8200 gal); Contamination documented in monitoring wells.
23	NA	Twin Bridges Truck City 131 12th Street	No	2 UST removed 1990. DNR "No Action Req'd" Jan. 1996.
24	NA	Nextel Phone 1504 State Street	No	Former gas station. No documented information.
25	NA	Rapid Lube and Oil 1740 State Street	No	Former gas station. 6 UST removed 1981 to 1987.
26	NA	US Petro Mart 845 State Street	No	Operating gas station identified as LUST site. 4 UST (3-10,000 gal and 1-8,000)
27	NA	Hans Body Shop 1720 State Street	No	Former gas station. No documented information.
28	NA	Bettendorf Auto 1705-1719 State Street	No	No contamination identified.
29	NA	Twin Bridges Motor Inn 221 15th Street	No	No contamination identified.

### LINE STYLE LEGEND OF CROSS SECTION SHEETS (ROAD)

- Existing Ground Line
- ===== Proposed Template
- ===== Proposed Topsoil Placement
- Additional Topsoil Removal
- Subgrade Treatment
- Granular Shoulder
- ===== Pavement
- Existing Pipe\RCB
- ===== Proposed Pipe\RCB
- ===== Proposed Dike
- ===== All Elements Associated with Proposed Entrances

### LINE STYLE LEGEND OF CROSS SECTION SHEETS (SOILS)

- TS----- Topsoil (Class 10)
- TS A----- Topsoil (Type A Disposal)
- TS B----- Topsoil (Type B Disposal)
- TS C----- Topsoil (Type C Disposal)
- CL 10----- Class 10 Materials
- SEL L0----- Select Loams And Clay-Loams
- SEL SA----- Select Sand
- UNS A----- Unsuitable Type A Disposal
- UNS B----- Unsuitable Type B Disposal
- UNS C----- Unsuitable Type C Disposal
- SHALE----- Shale
- WASTE----- Waste
- B&W LS----- Broken and Weathered Rock
- ROCK----- Solid Rock
- BLDRS----- Boulders

Note: All layer lines and descriptions identify layers above the line.

Note: Vertical or near vertical lines connecting soil layers at edges of cross sections are only for the purpose of calculating template quantities and do not depict soil stratification.

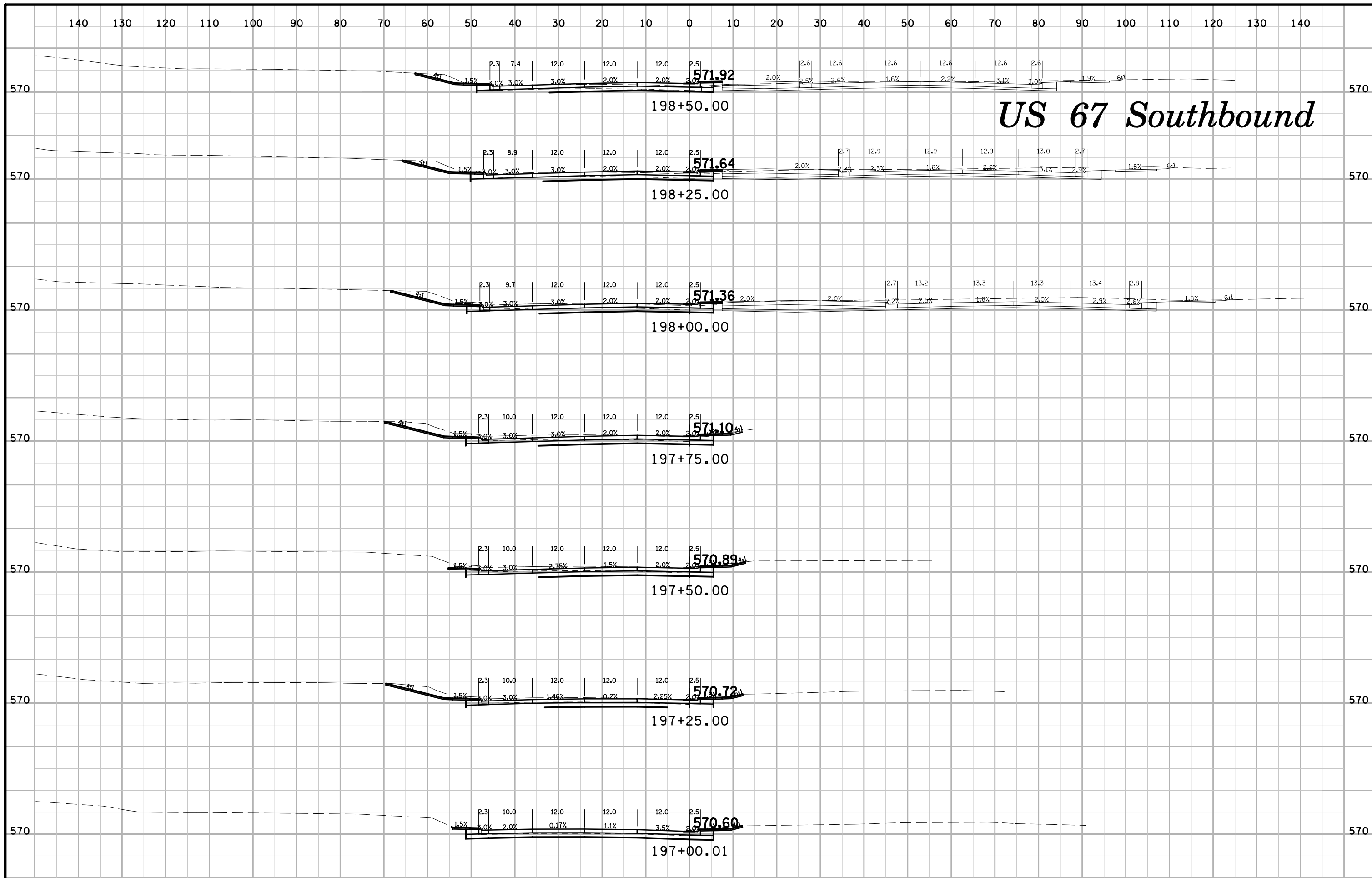
### SYMBOL LEGEND OF CROSS SECTION SHEETS

- Existing ROW  
|  
-----  
Existing Right-of-Way Limit
- Proposed ROW  
|  
-----  
Proposed Right-of-Way Limit
- Temporary ROW  
|  
-----  
Temporary Right-of-Way Limit

## CROSS SECTION LEGEND AND SYMBOL INFORMATION SHEET

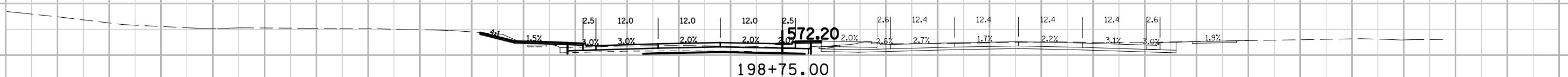
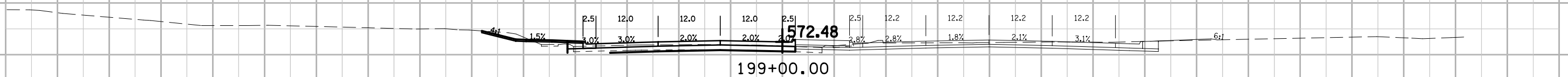
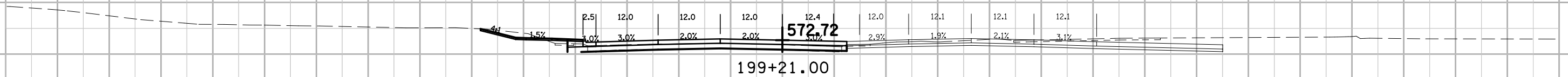
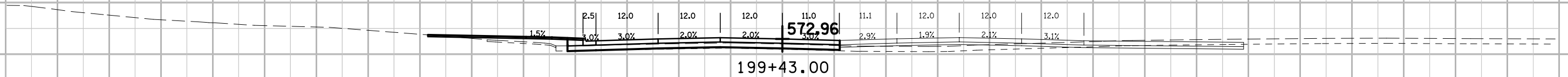
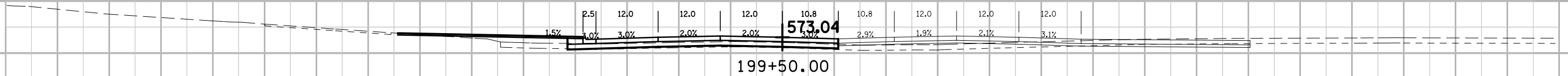
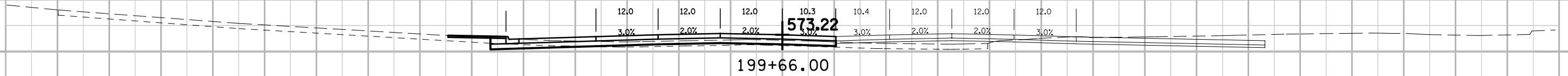
(COVERS SHEET SERIES W, X, Y, & Z)





140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

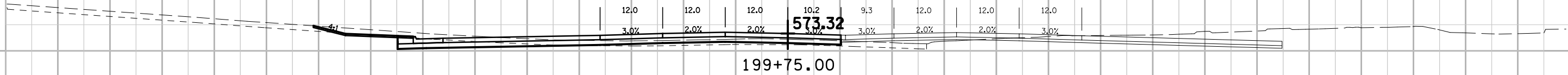
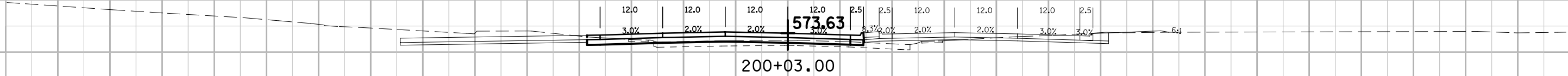
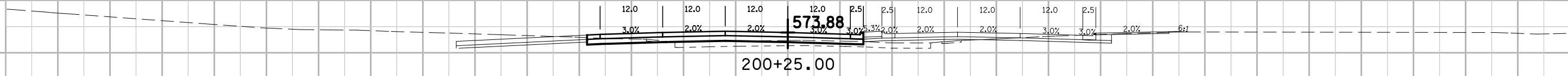
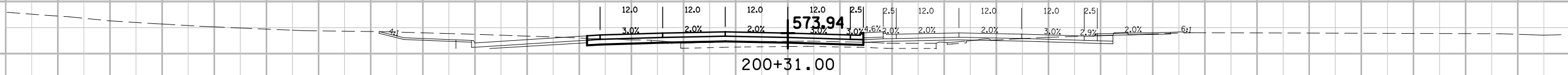
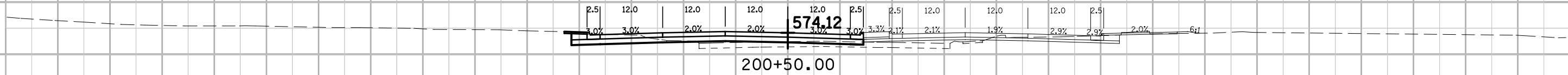
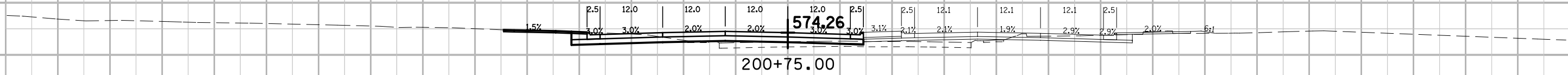
# US 67 Southbound



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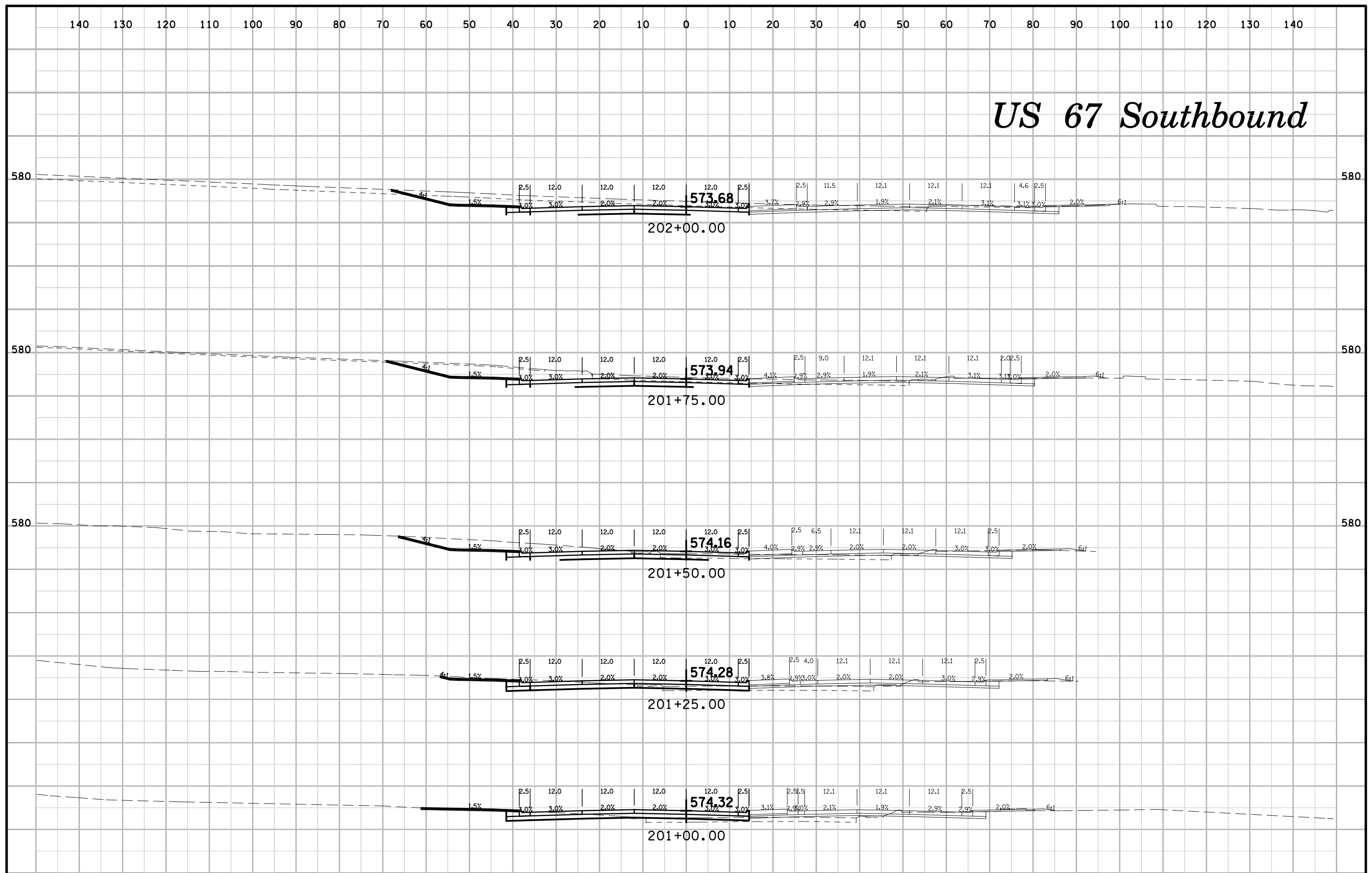
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# US 67 Southbound



140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

# US 67 Southbound



140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

# US 67 Southbound

572.38  
203+25.00

572.43  
203+20.00

572.64  
203+00.00

572.90  
202+75.00

573.16  
202+50.00

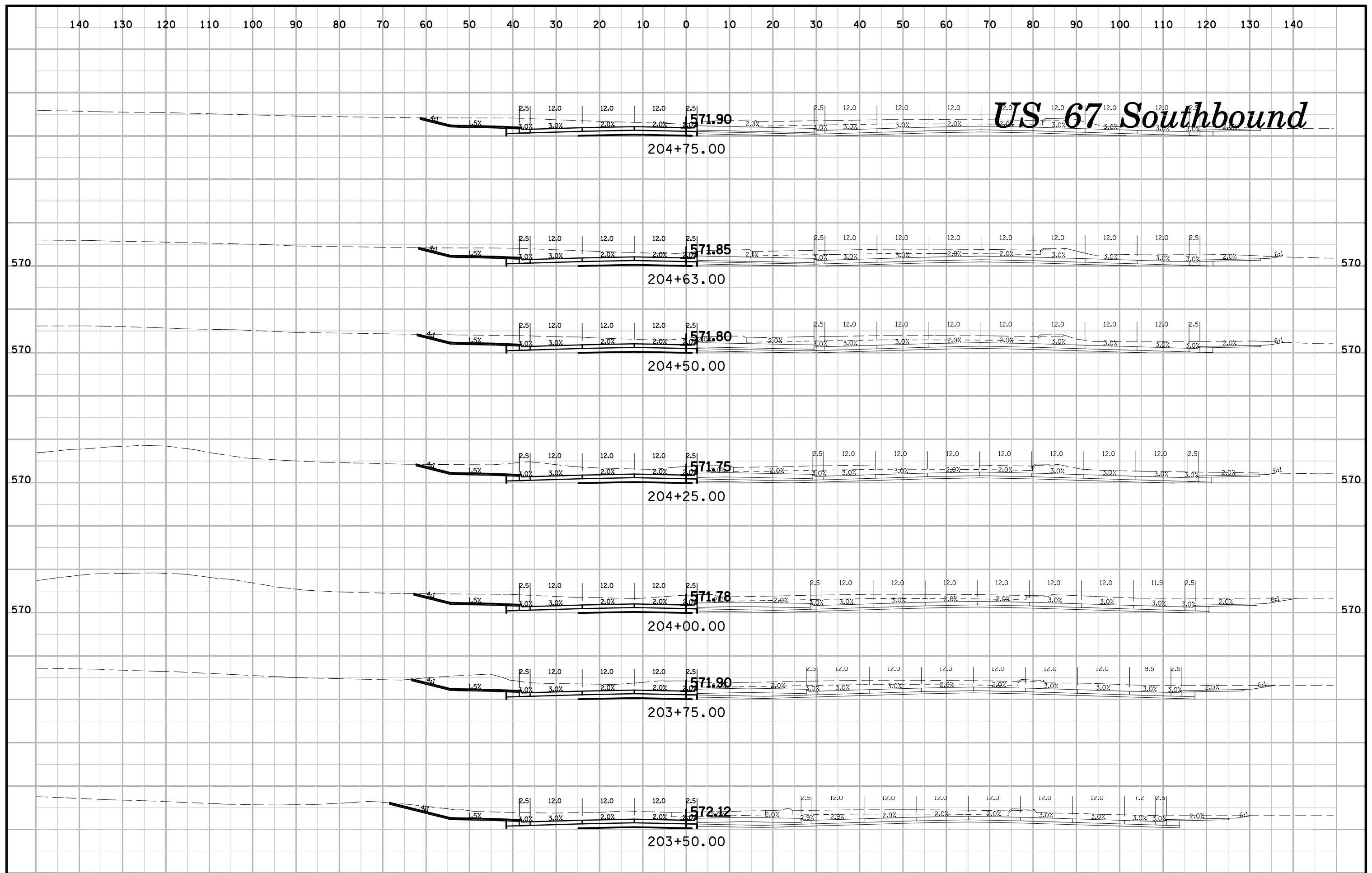
573.42  
202+25.00

580

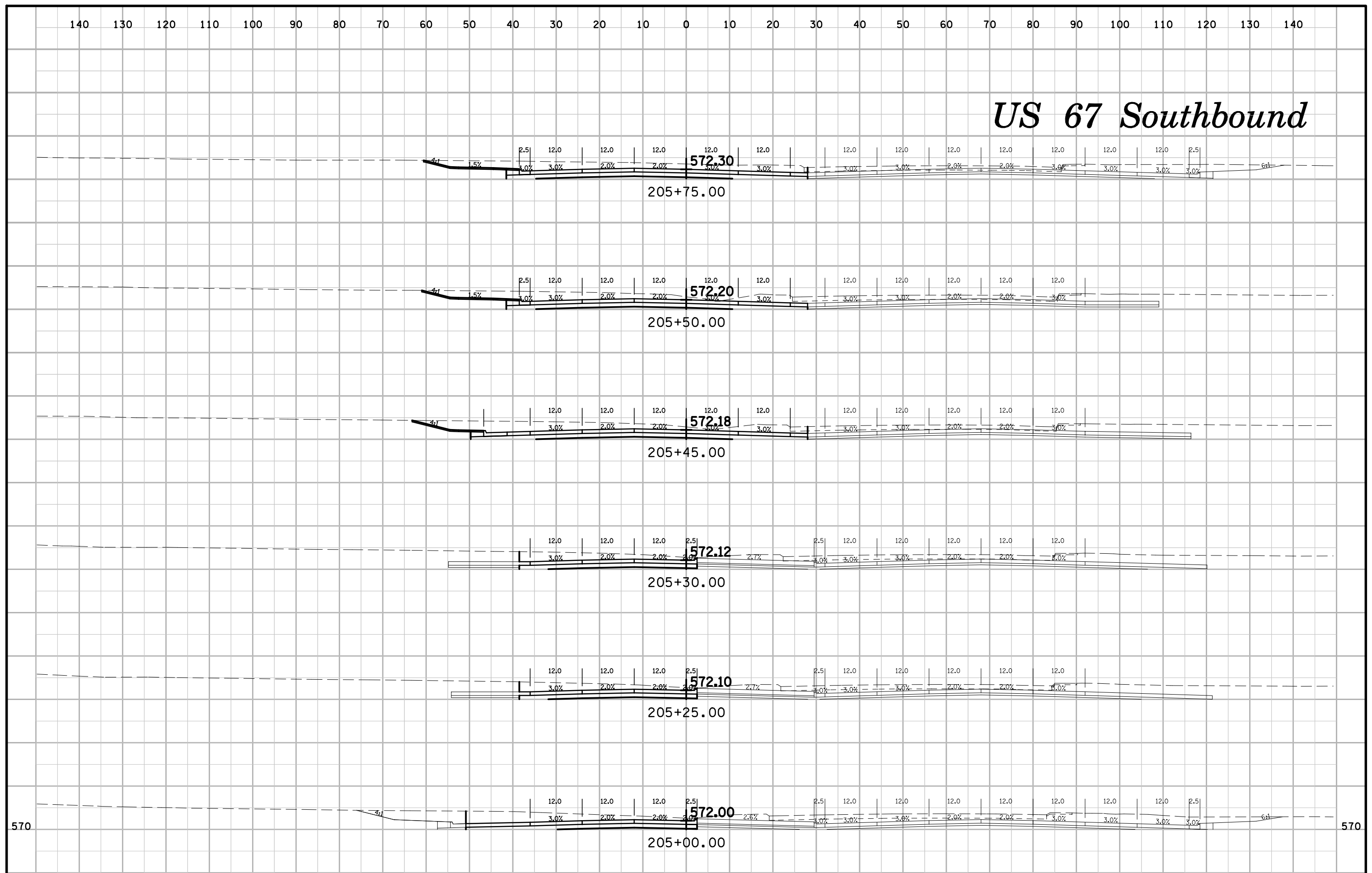
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# US 67 Southbound

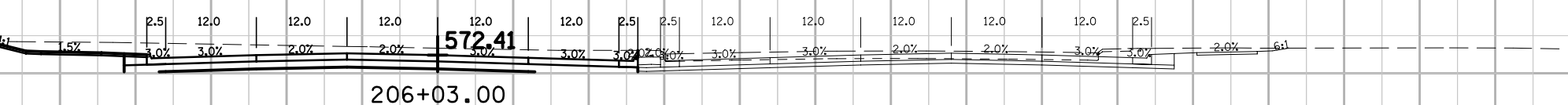
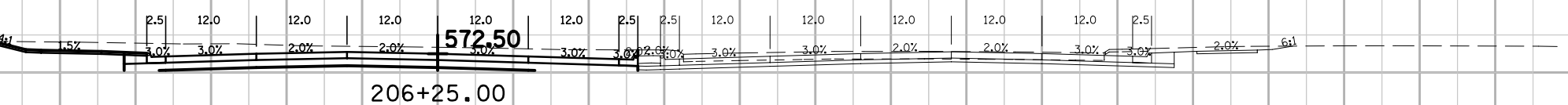
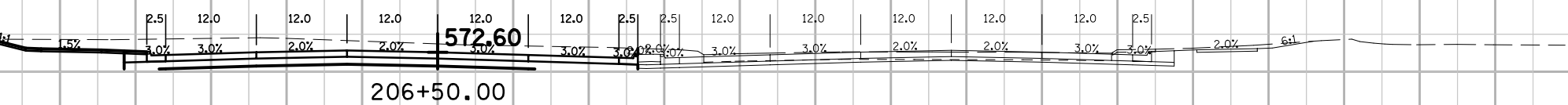
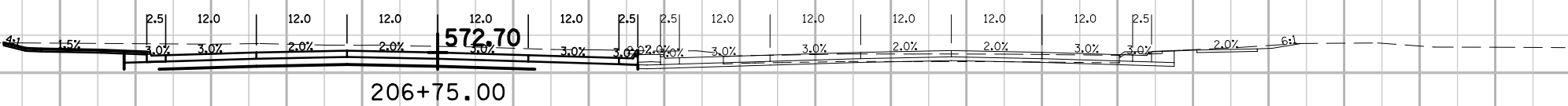
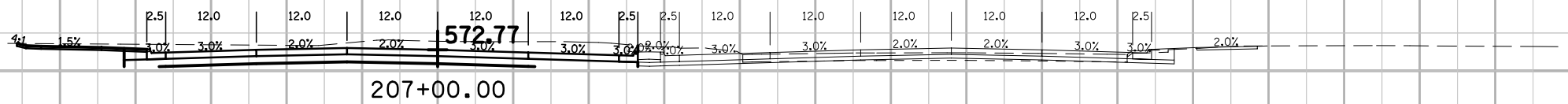
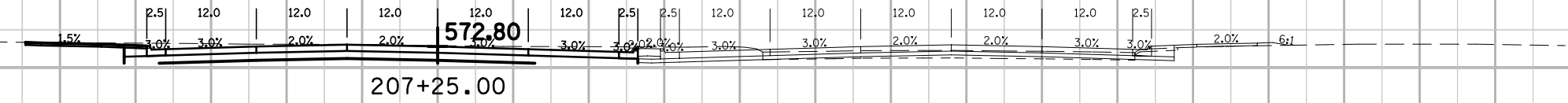


# US 67 Southbound



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# US 67 Southbound

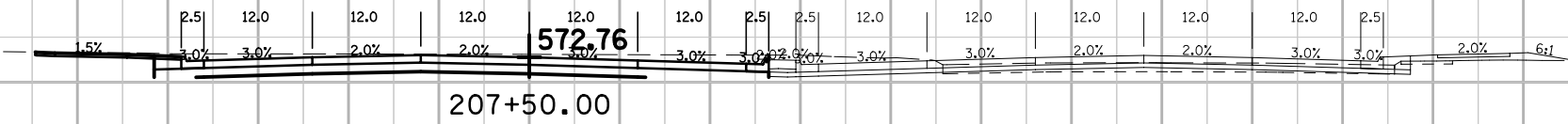
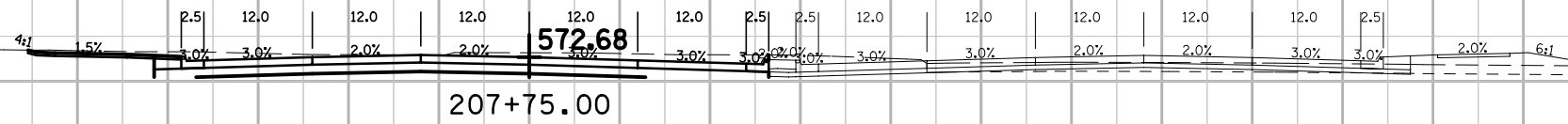
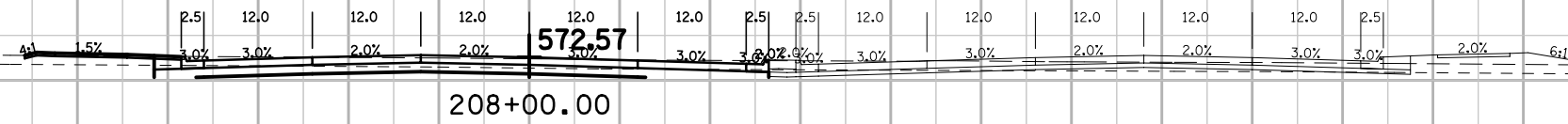
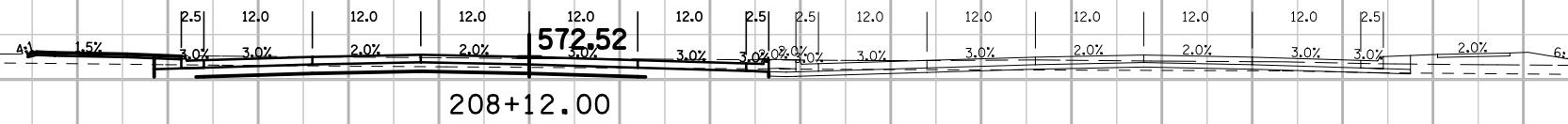
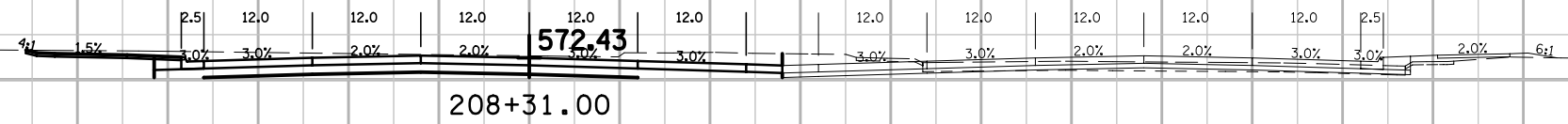
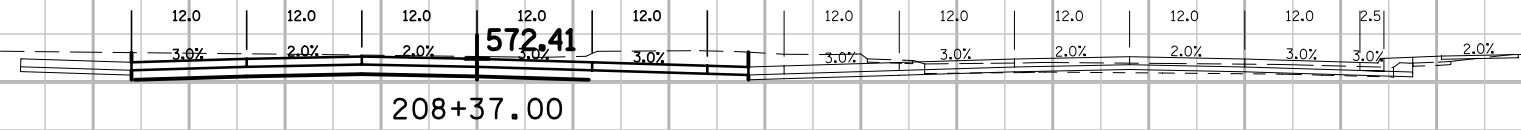


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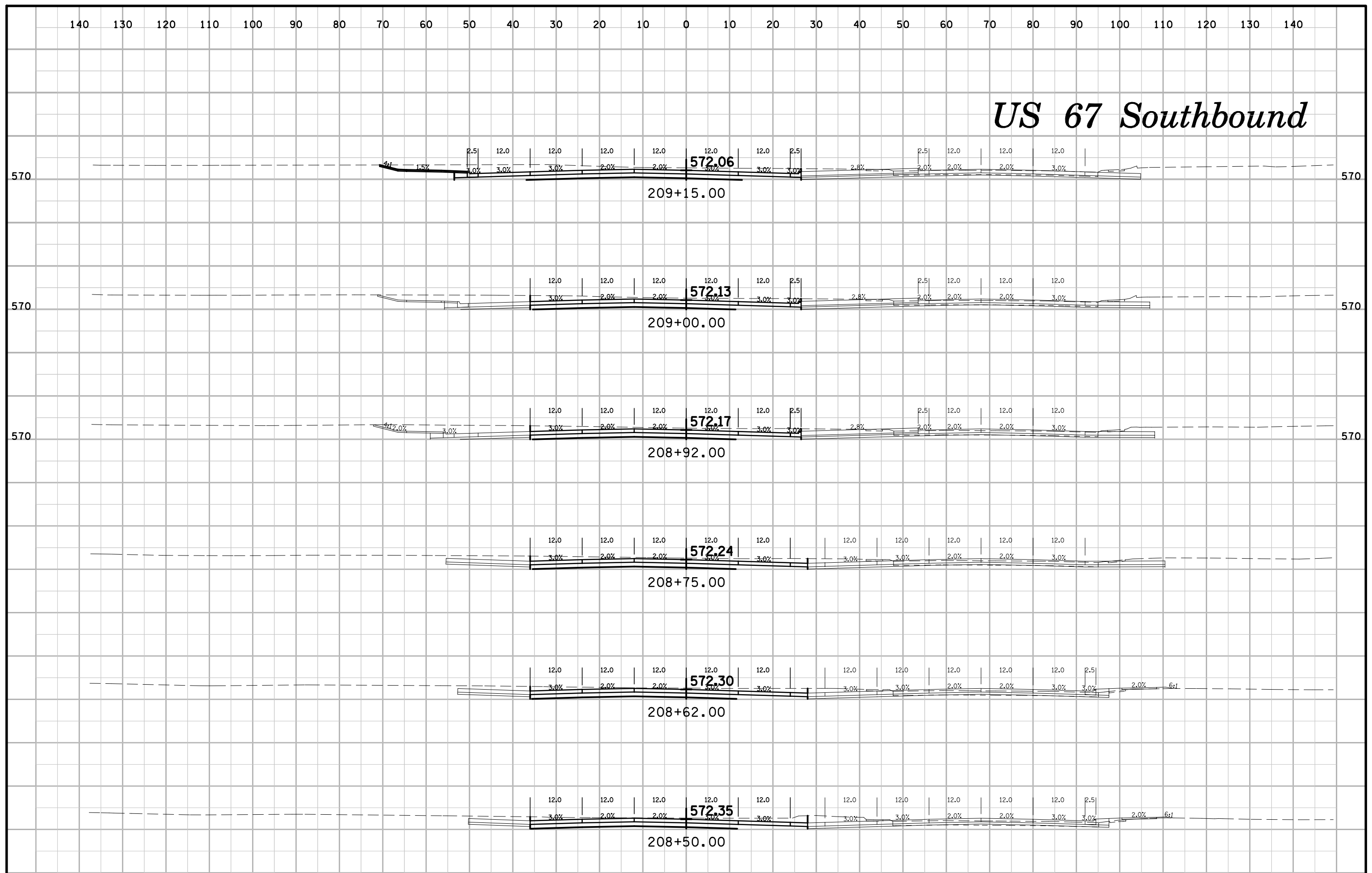
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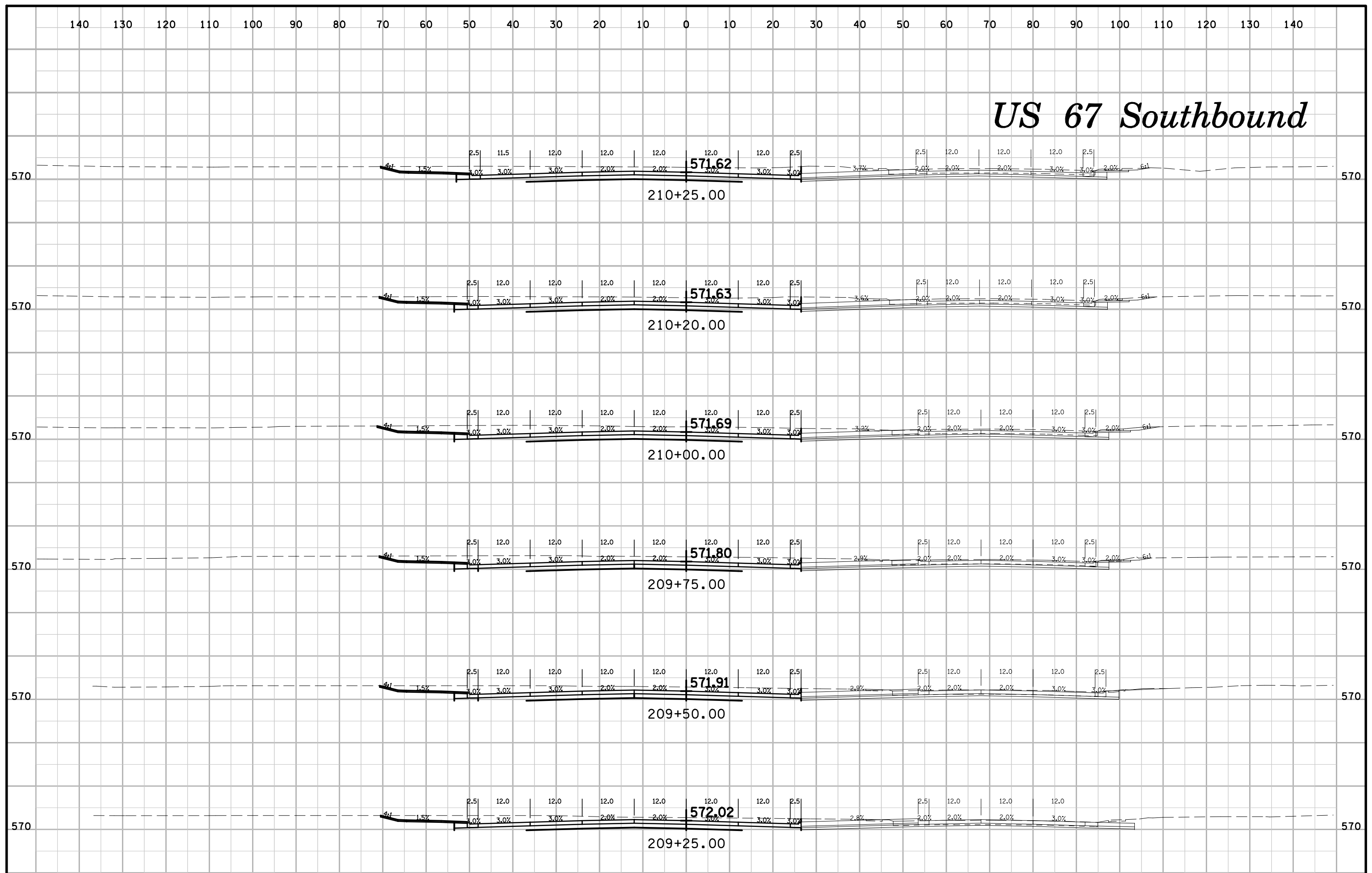


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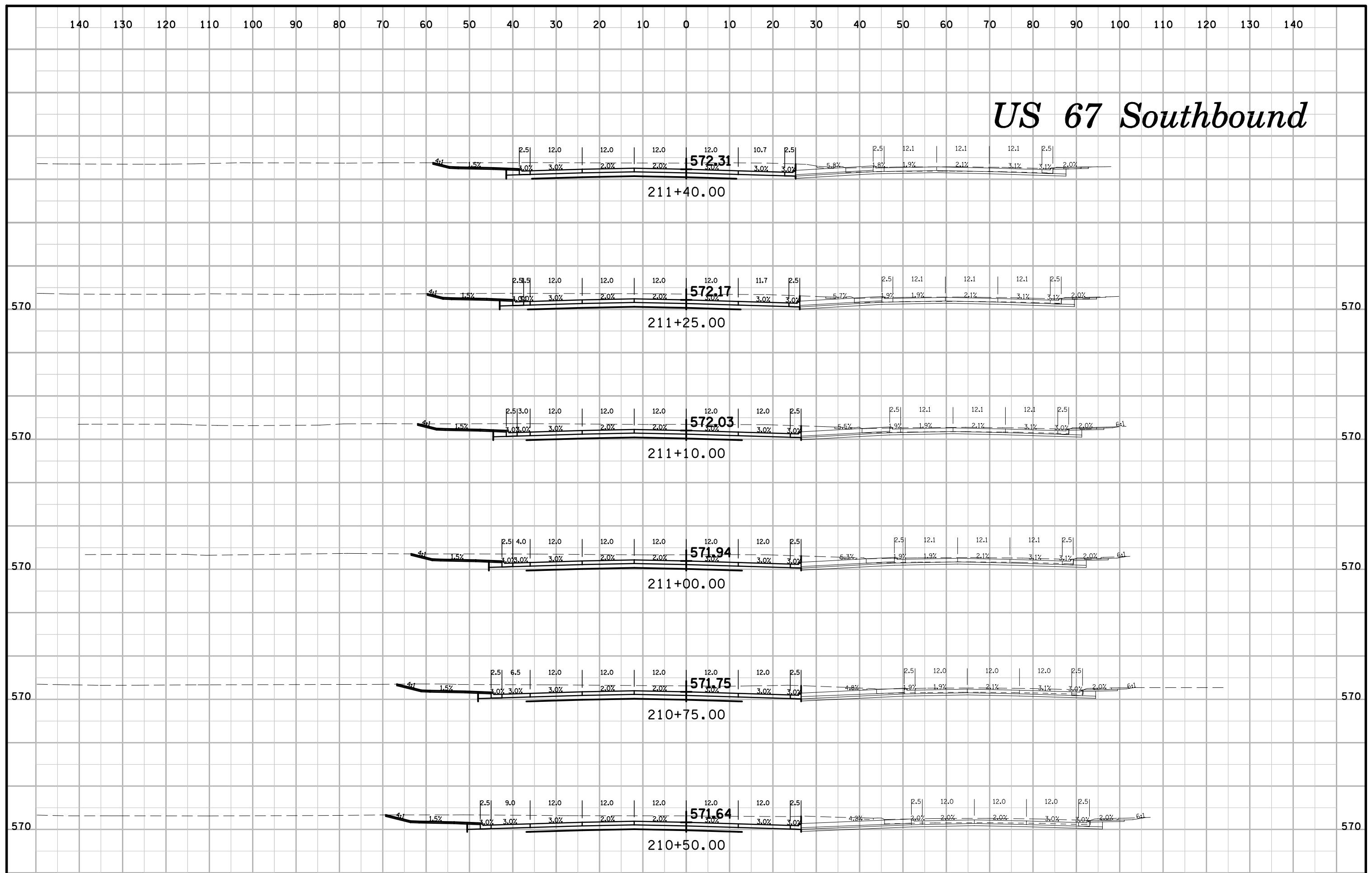
# US 67 Southbound



# US 67 Southbound

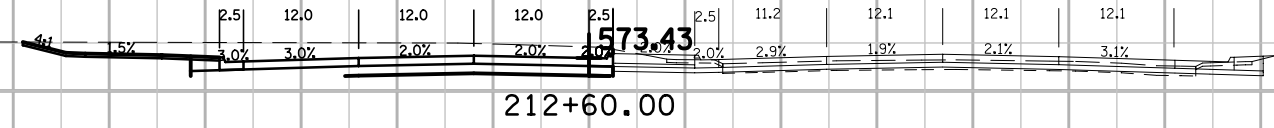


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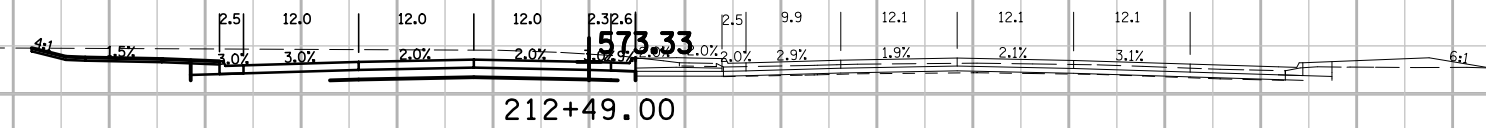


# US 67 Southbound

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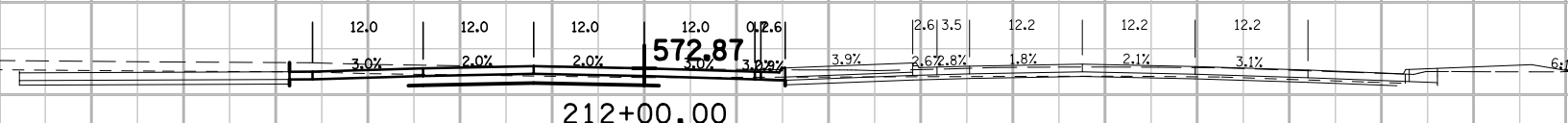
212+60.00



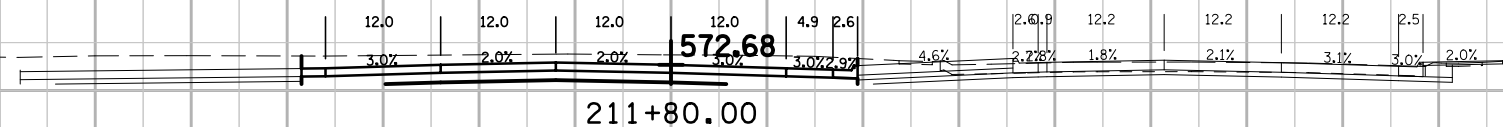
212+49.00



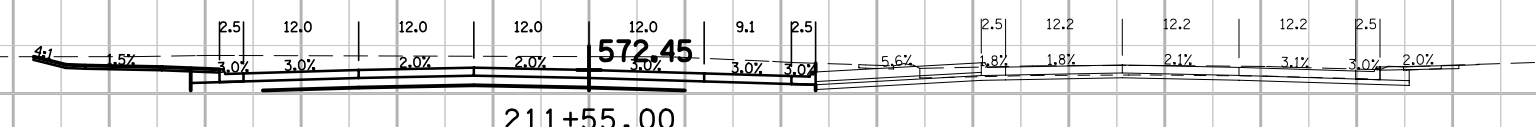
212+23.00



212+00.00



211+80.00

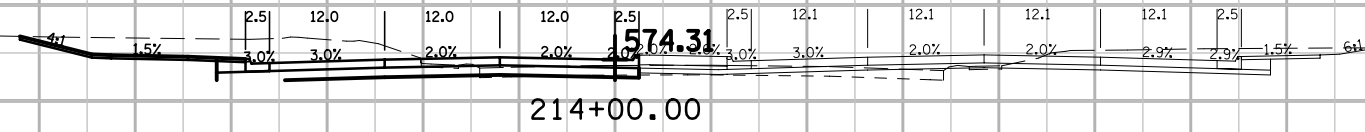


211+55.00

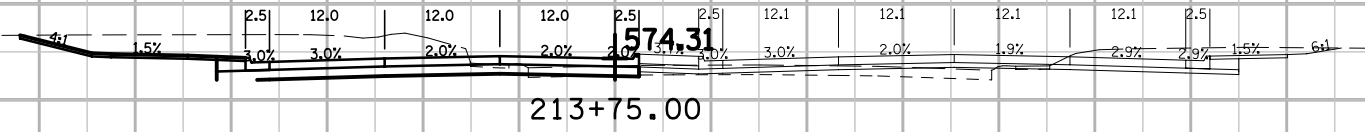
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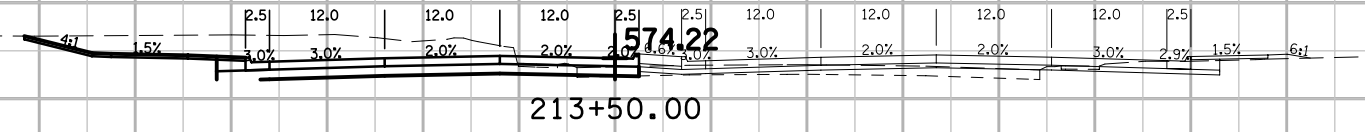
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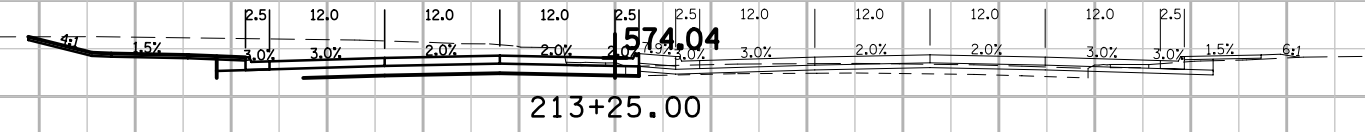
214+00.00



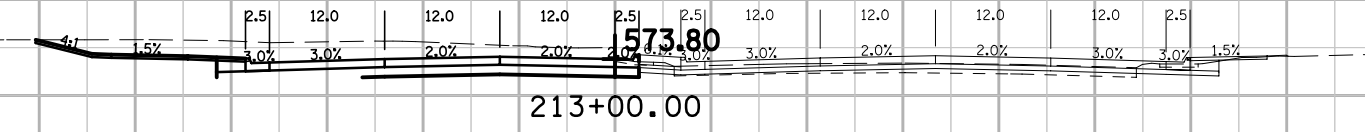
213+75.00



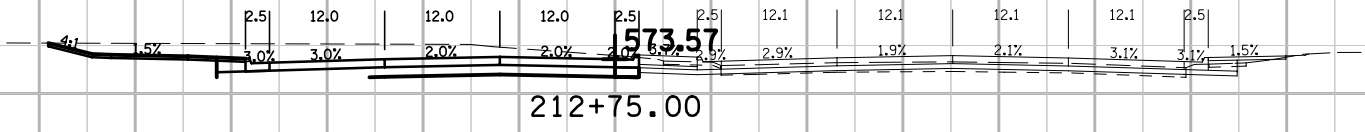
213+50.00



213+25.00



213+00.00

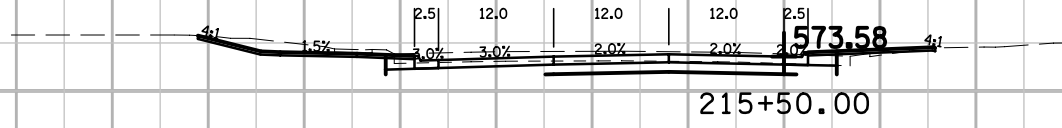


212+75.00

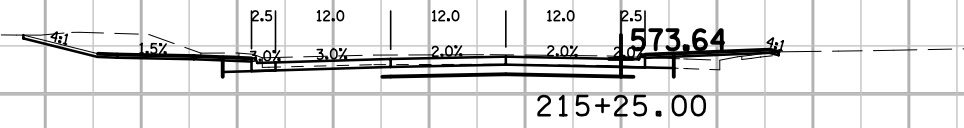
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# US 67 Southbound

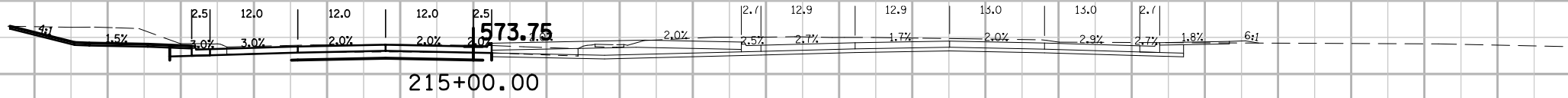
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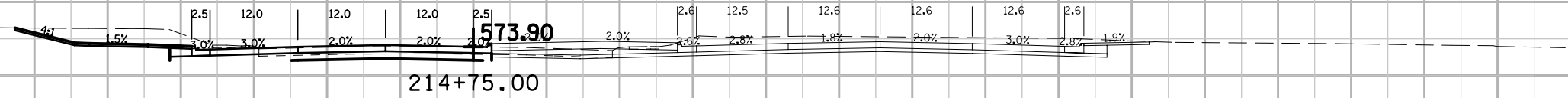
215+50.00



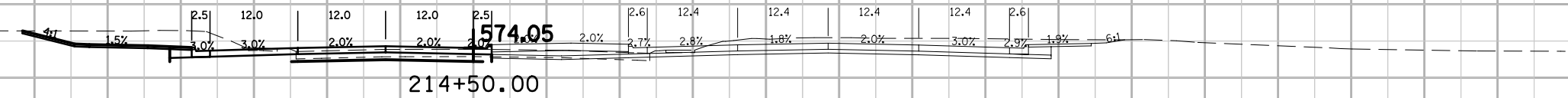
215+25.00



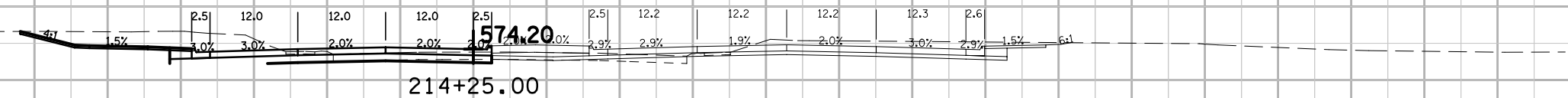
215+00.00



214+75.00



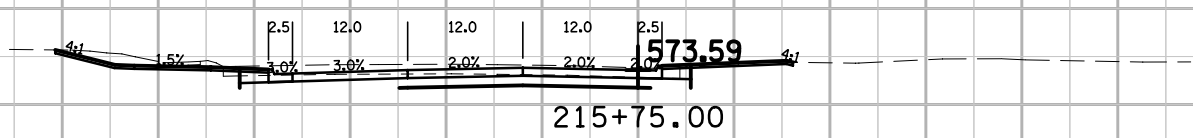
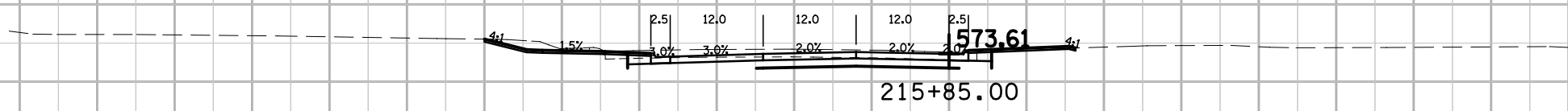
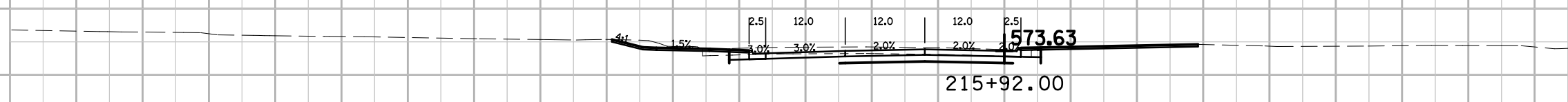
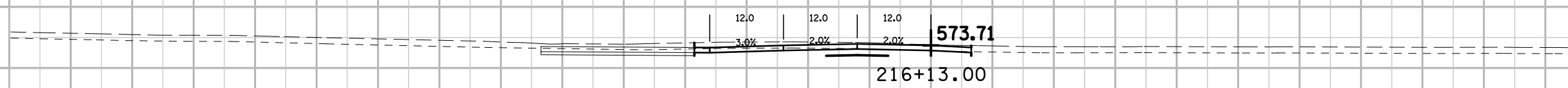
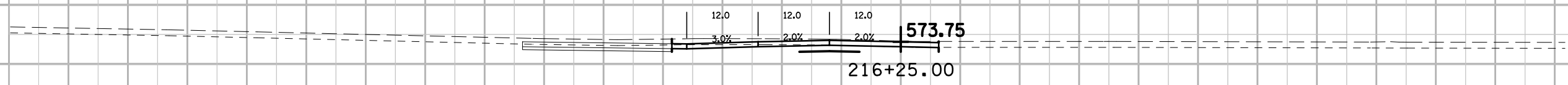
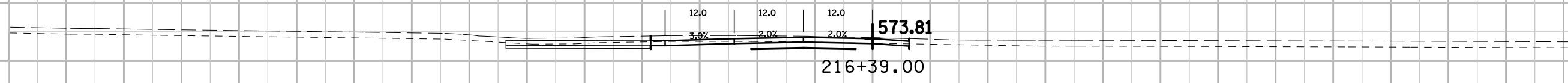
214+50.00



214+25.00

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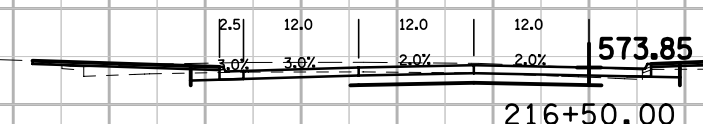
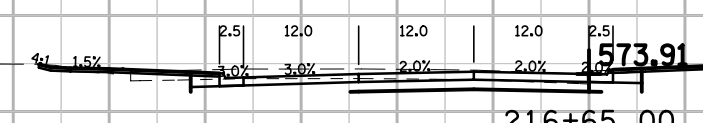
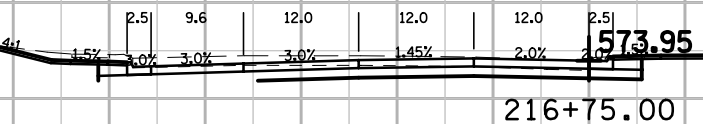
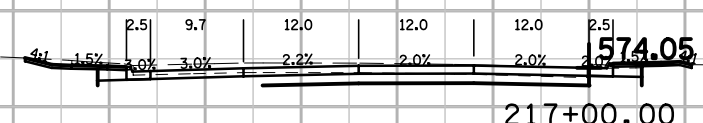
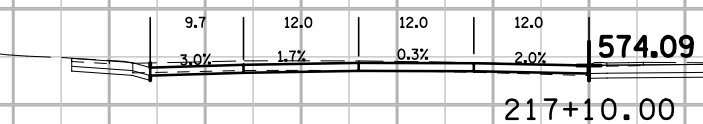
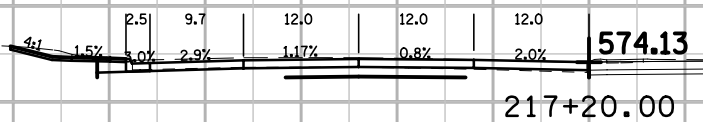
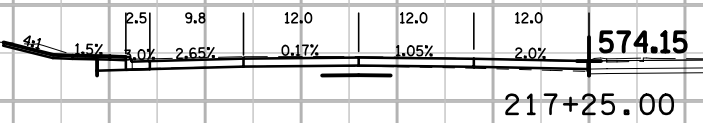
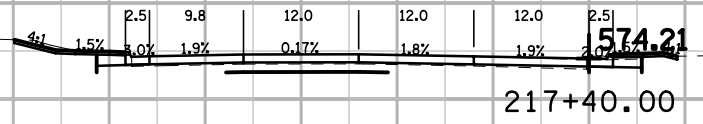
# US 67 Southbound





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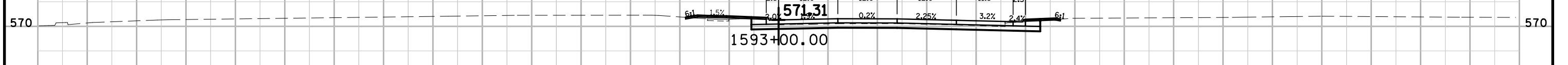
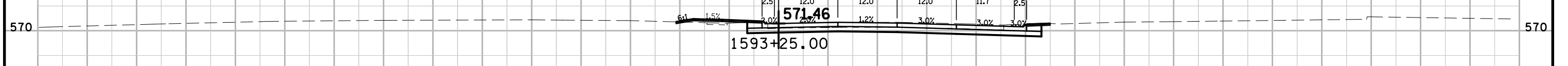
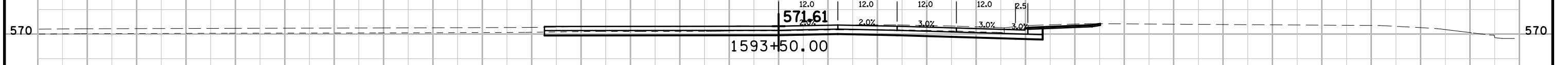
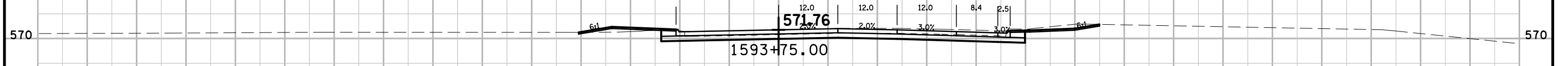
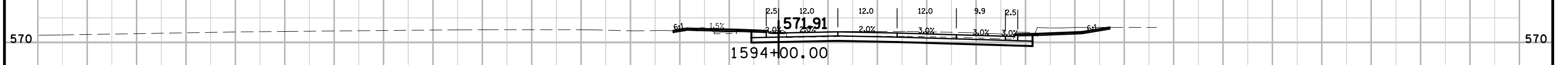
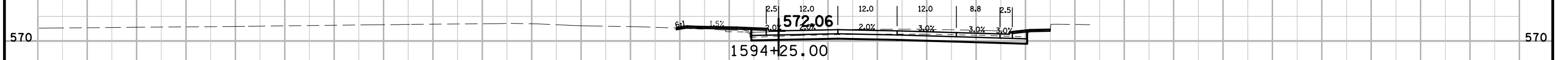
# US 67 Southbound



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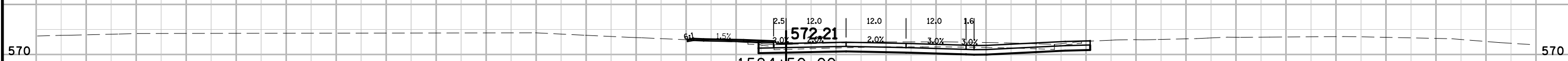
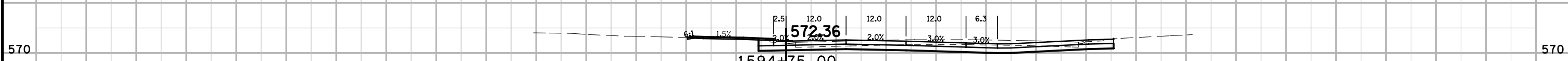
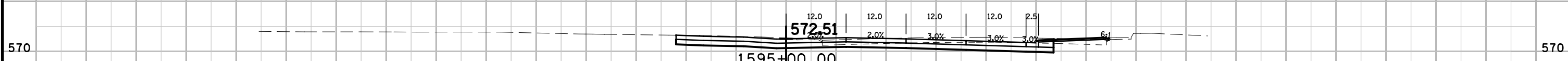
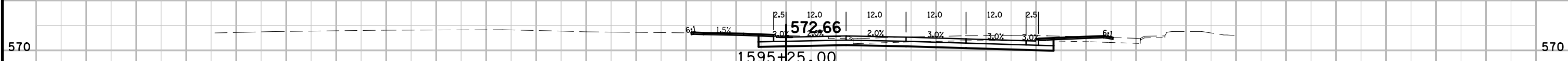
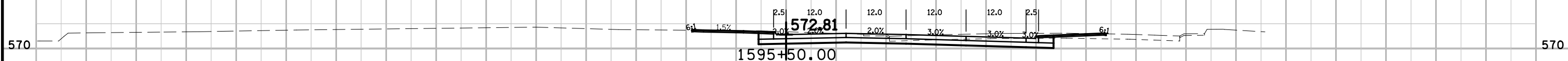
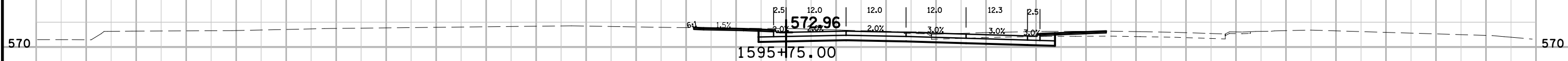
# US 67 Northbound



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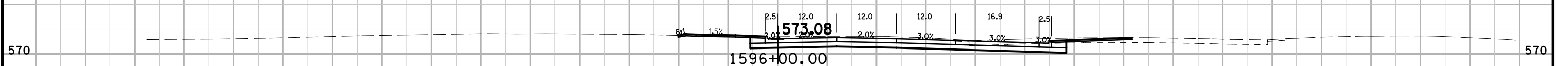
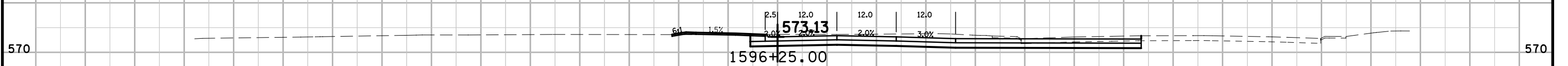
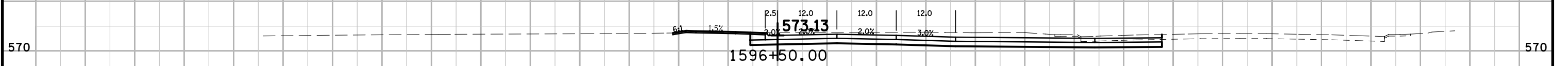
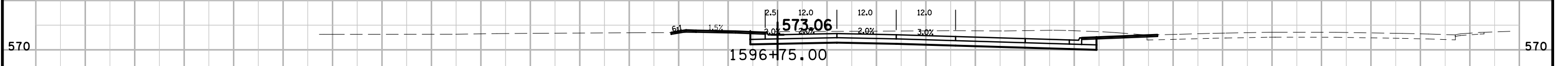
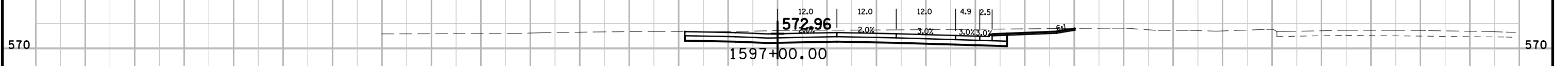
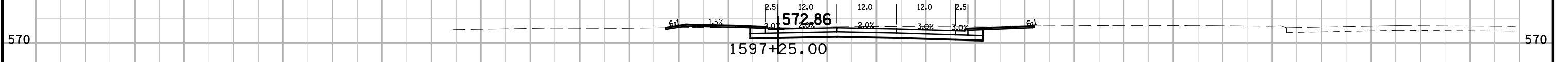
# US 67 Northbound



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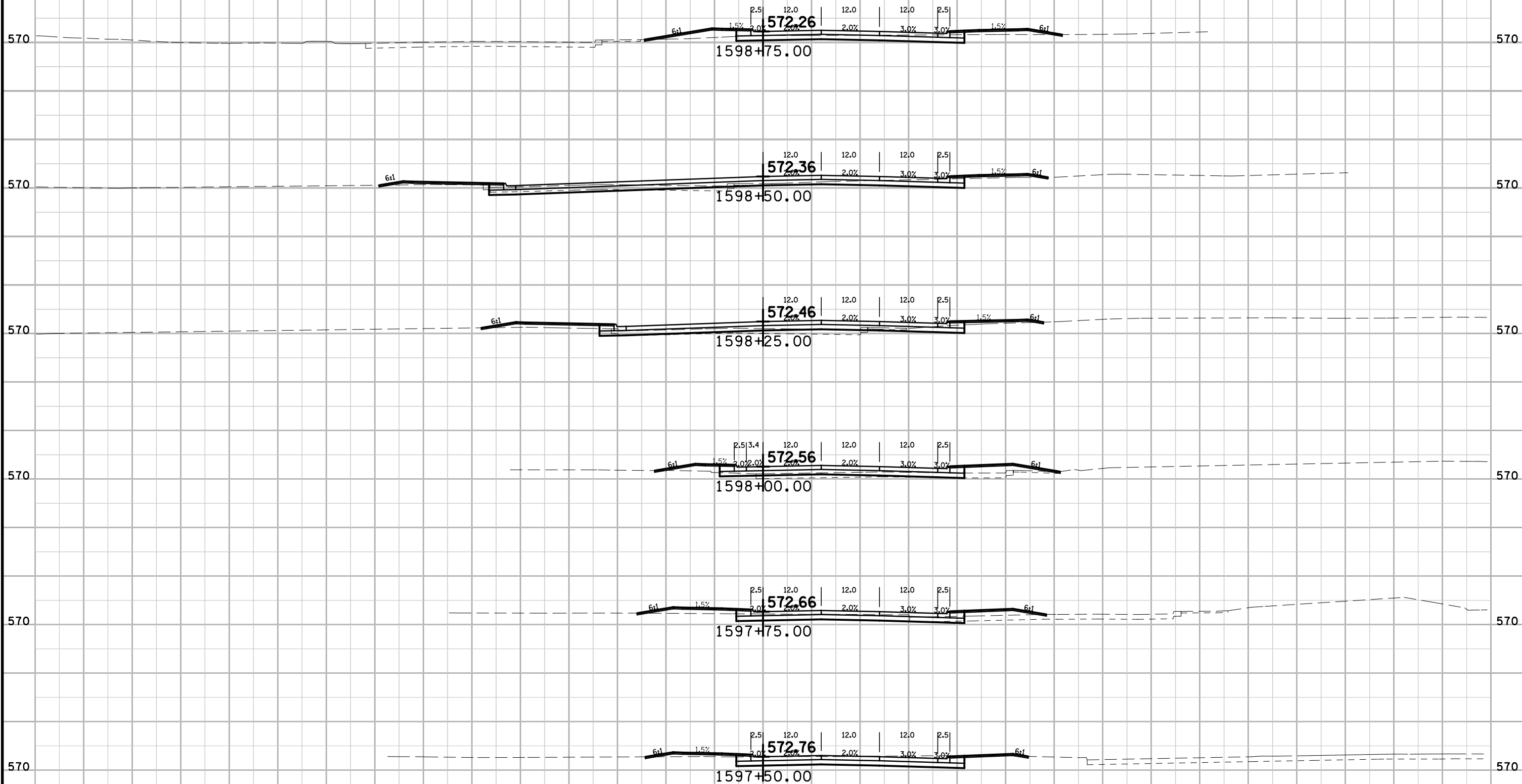
# US 67 Northbound



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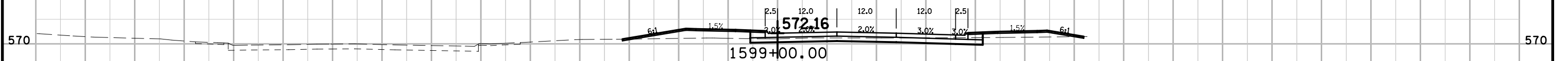
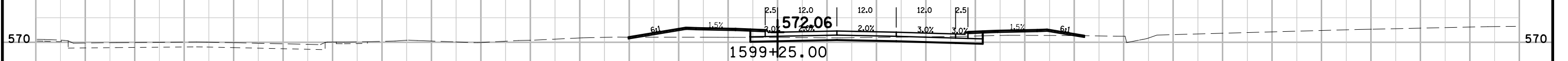
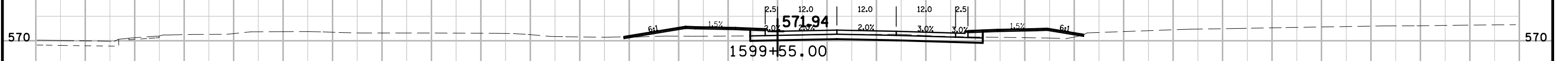
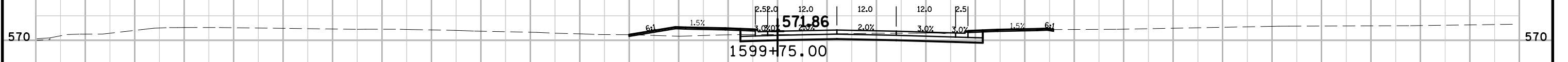
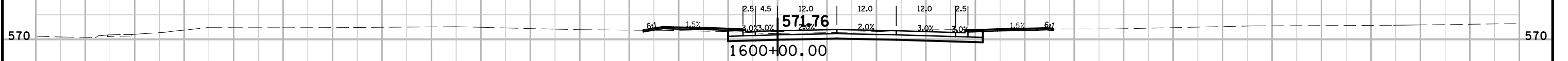
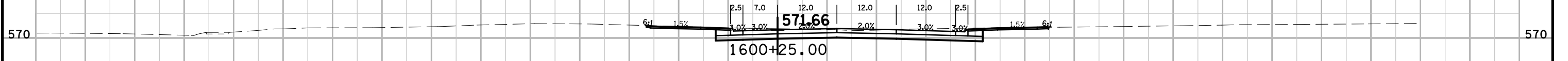
# US 67 Northbound



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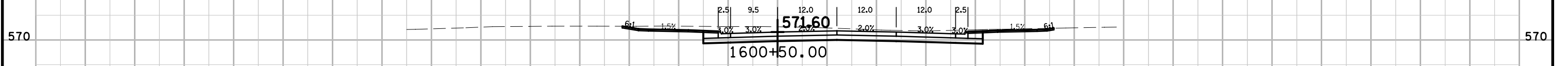
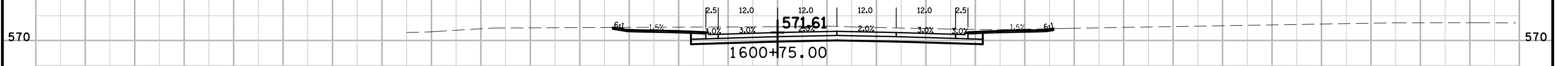
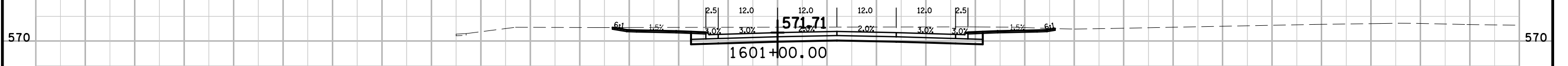
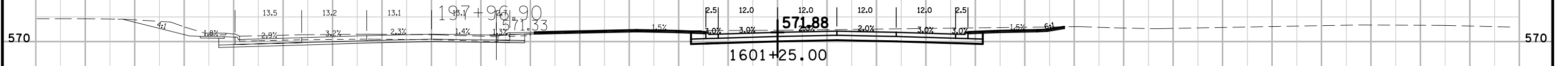
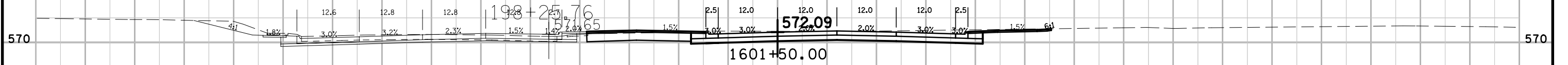
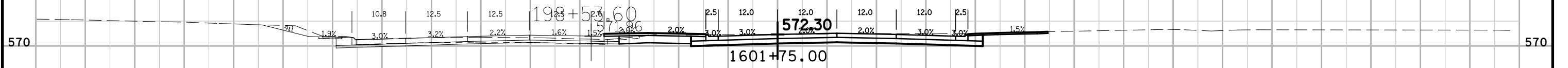
# US 67 Northbound



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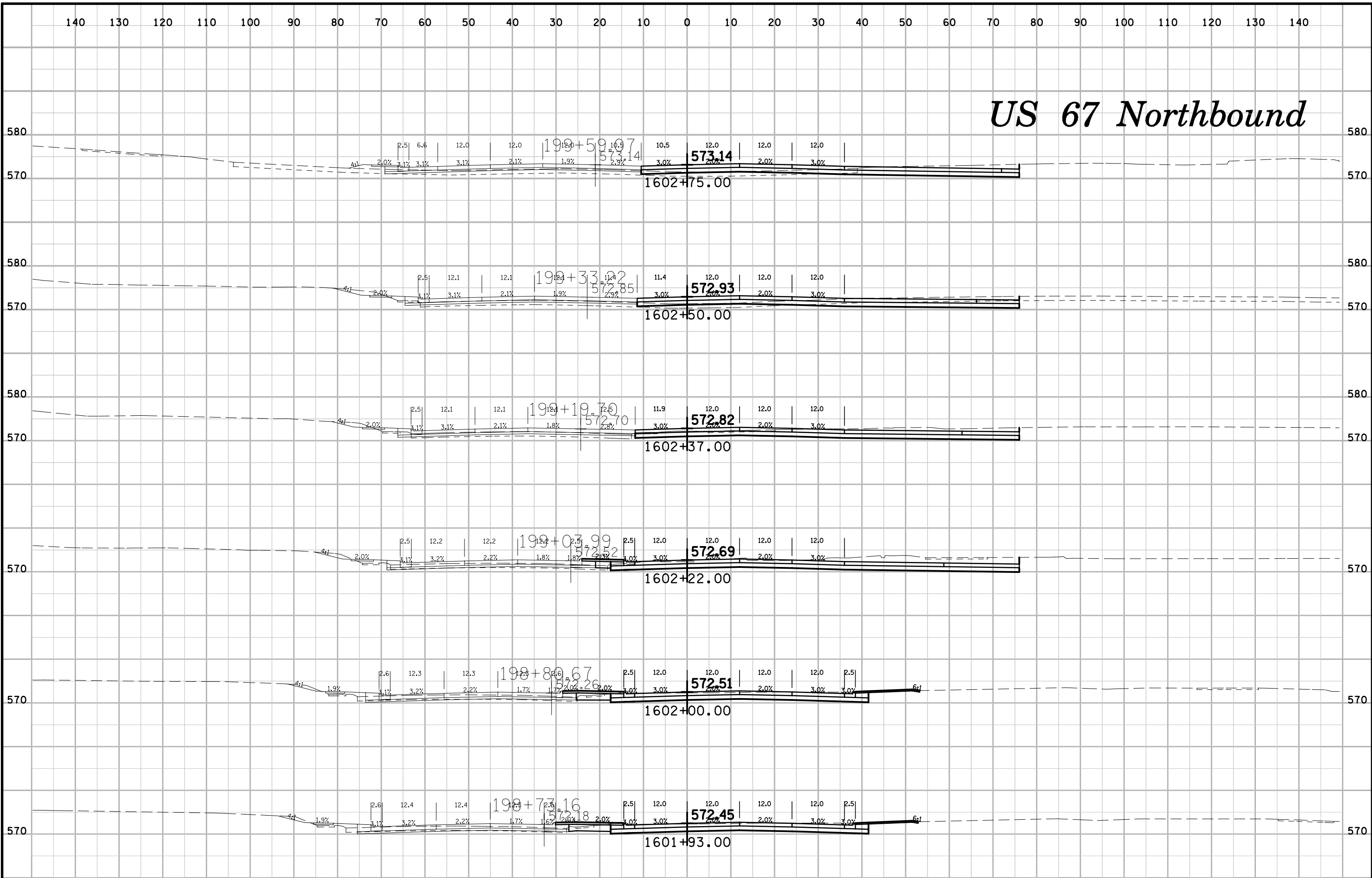
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# US 67 Northbound



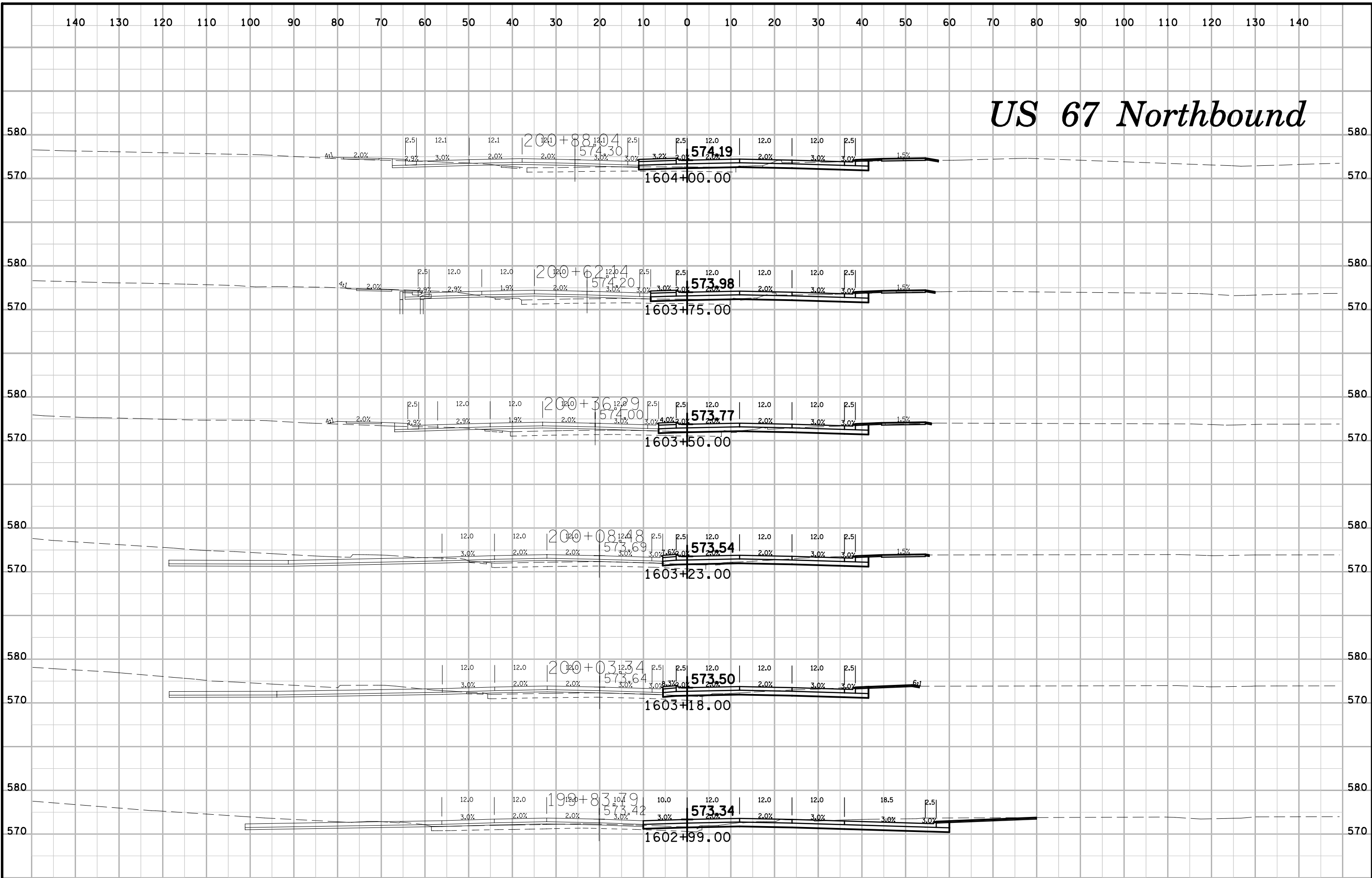
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# US 67 Northbound

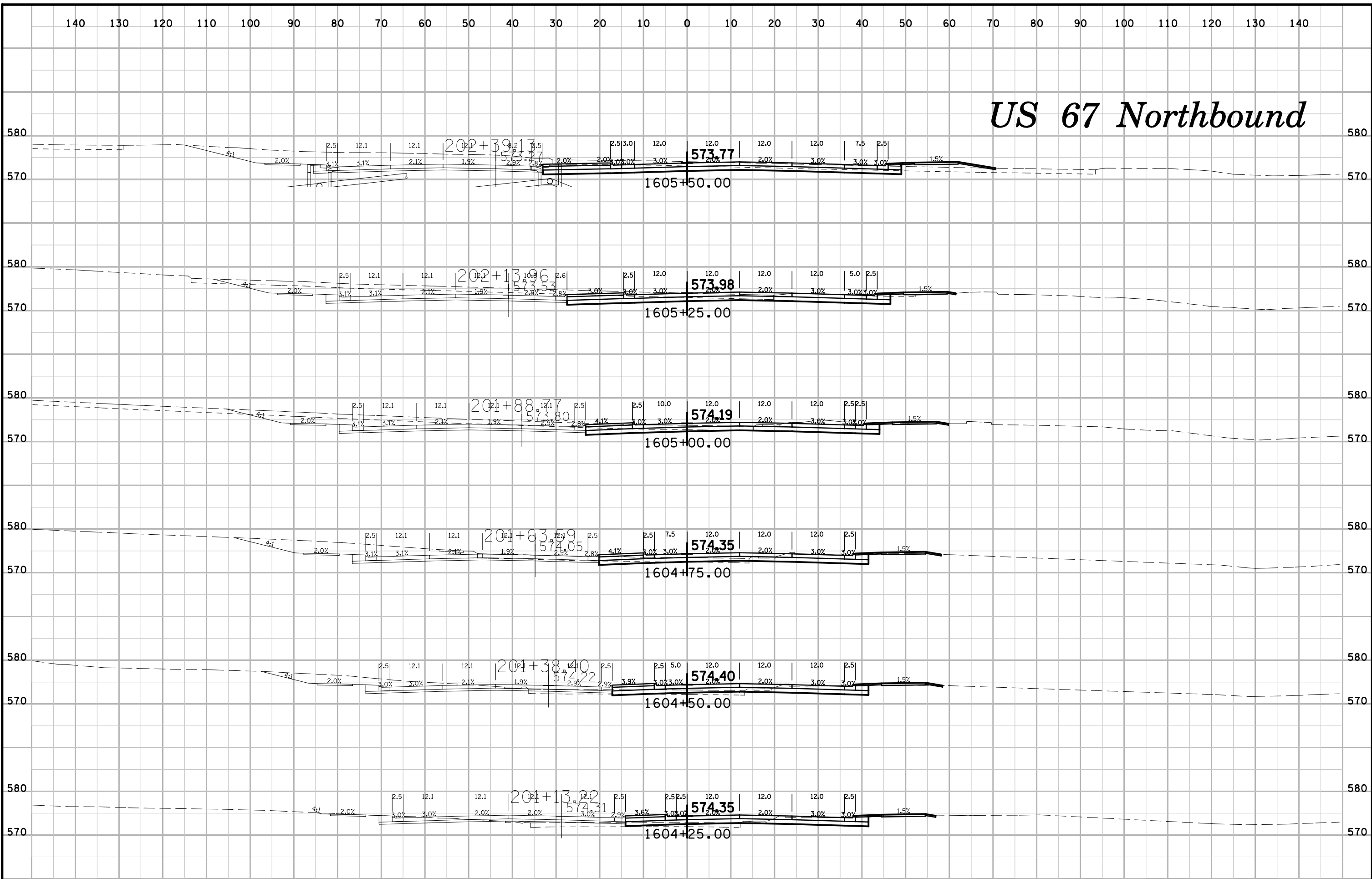




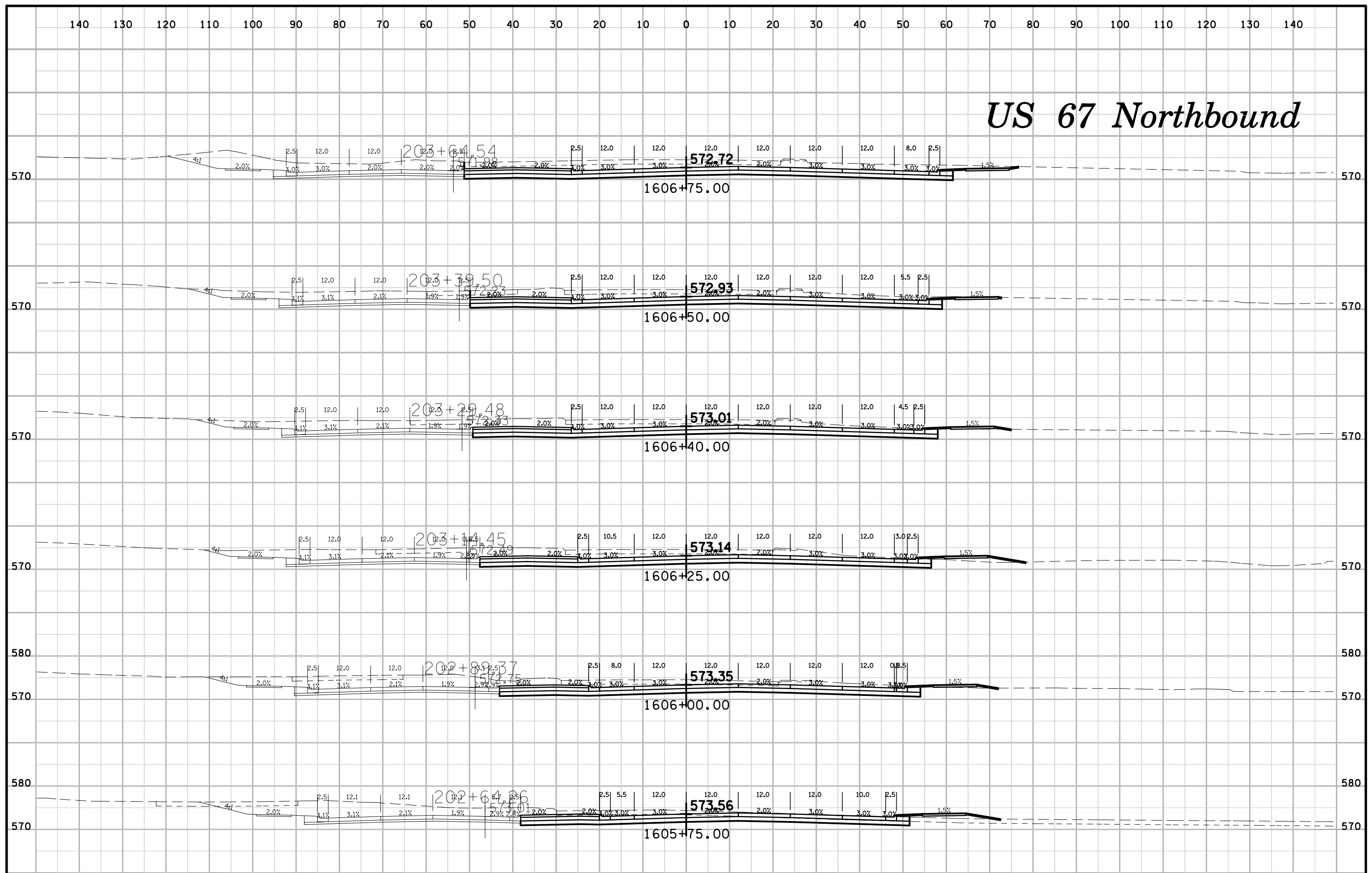
# US 67 Northbound



# US 67 Northbound

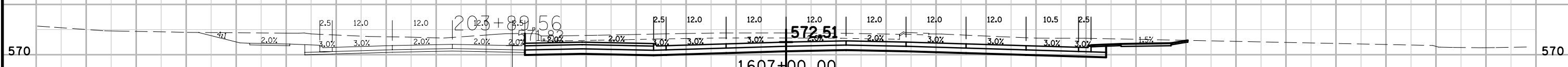
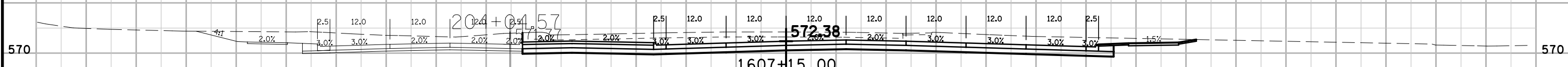
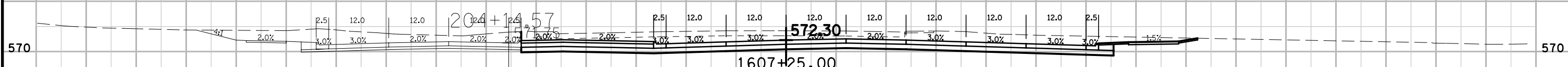
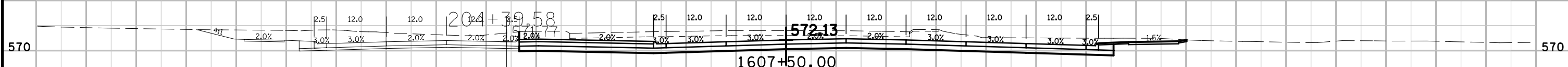
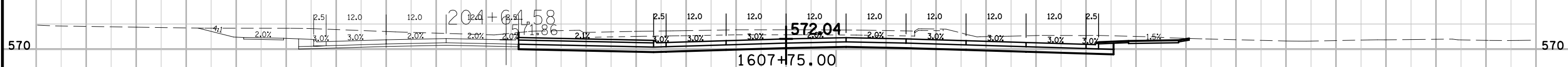
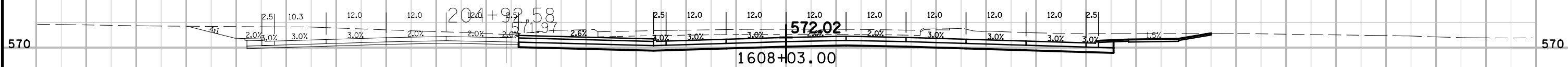


# US 67 Northbound



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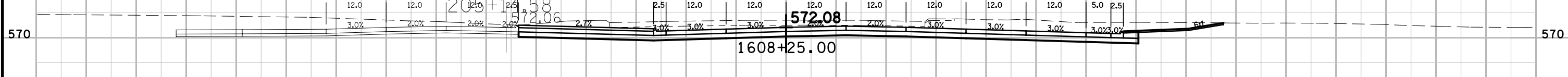
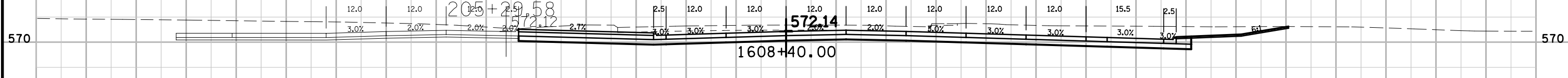
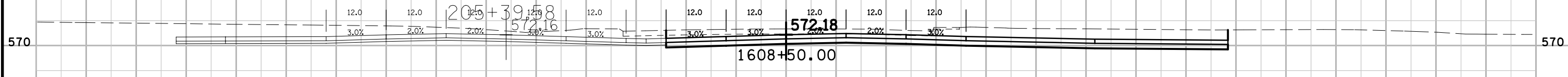
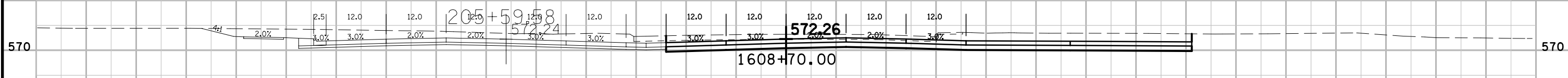
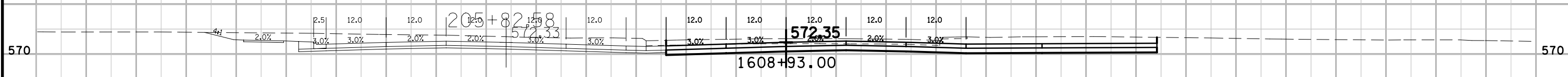
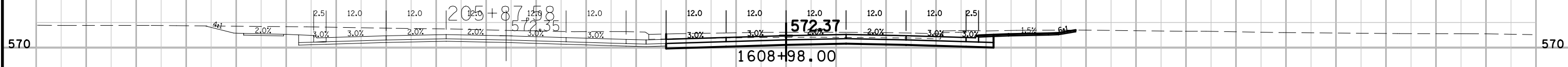
# US 67 Northbound



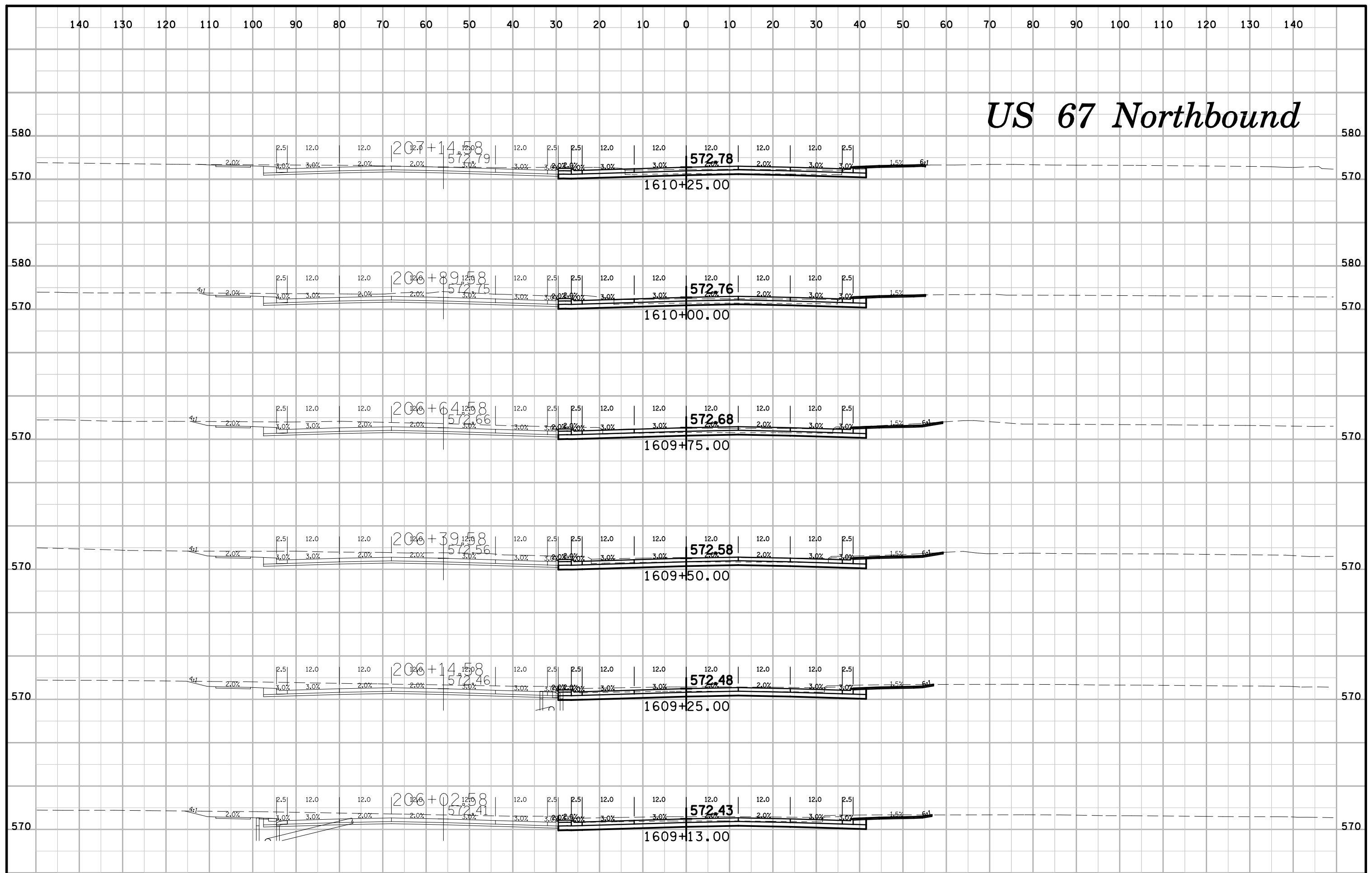
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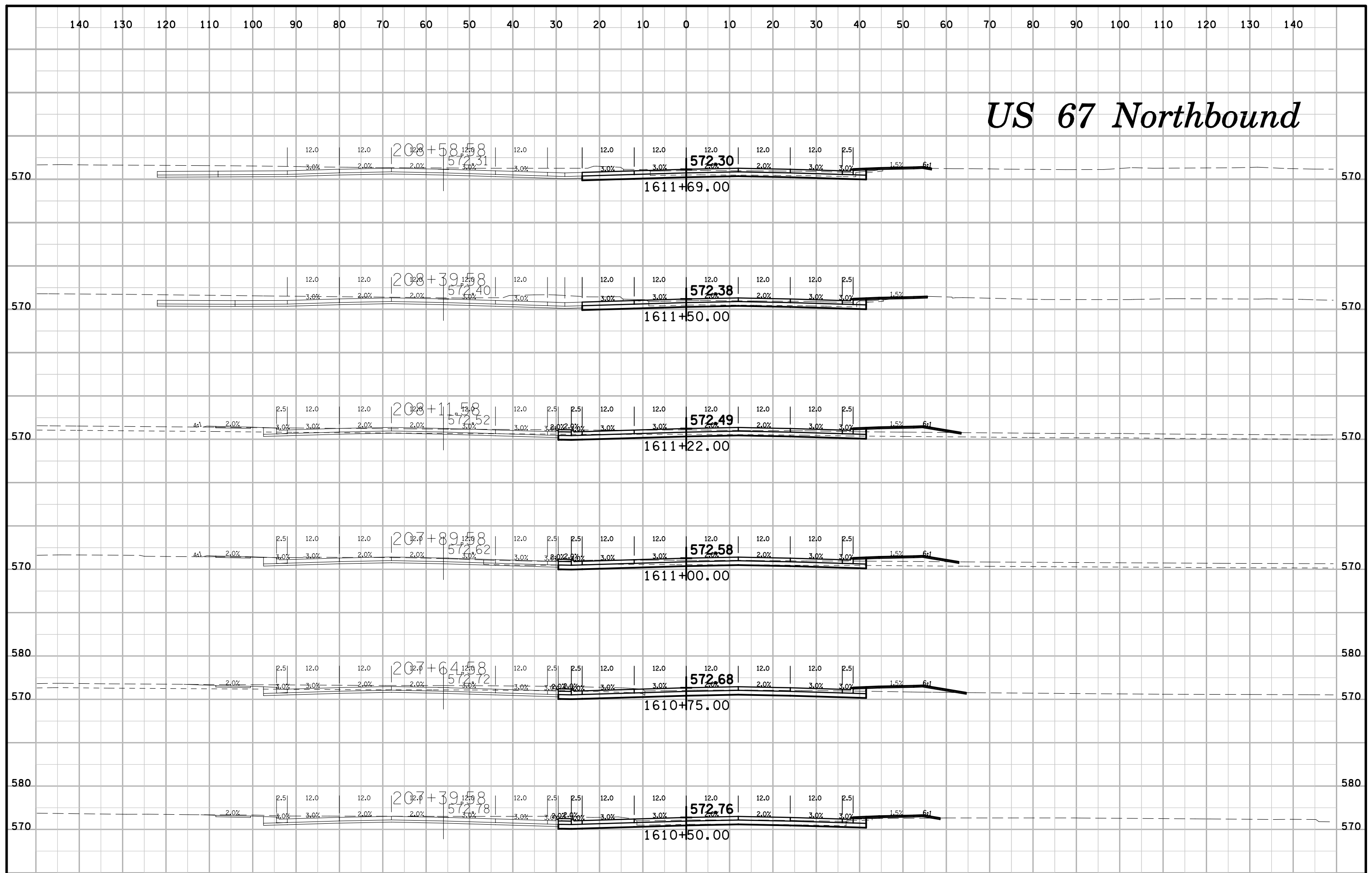
# US 67 Northbound



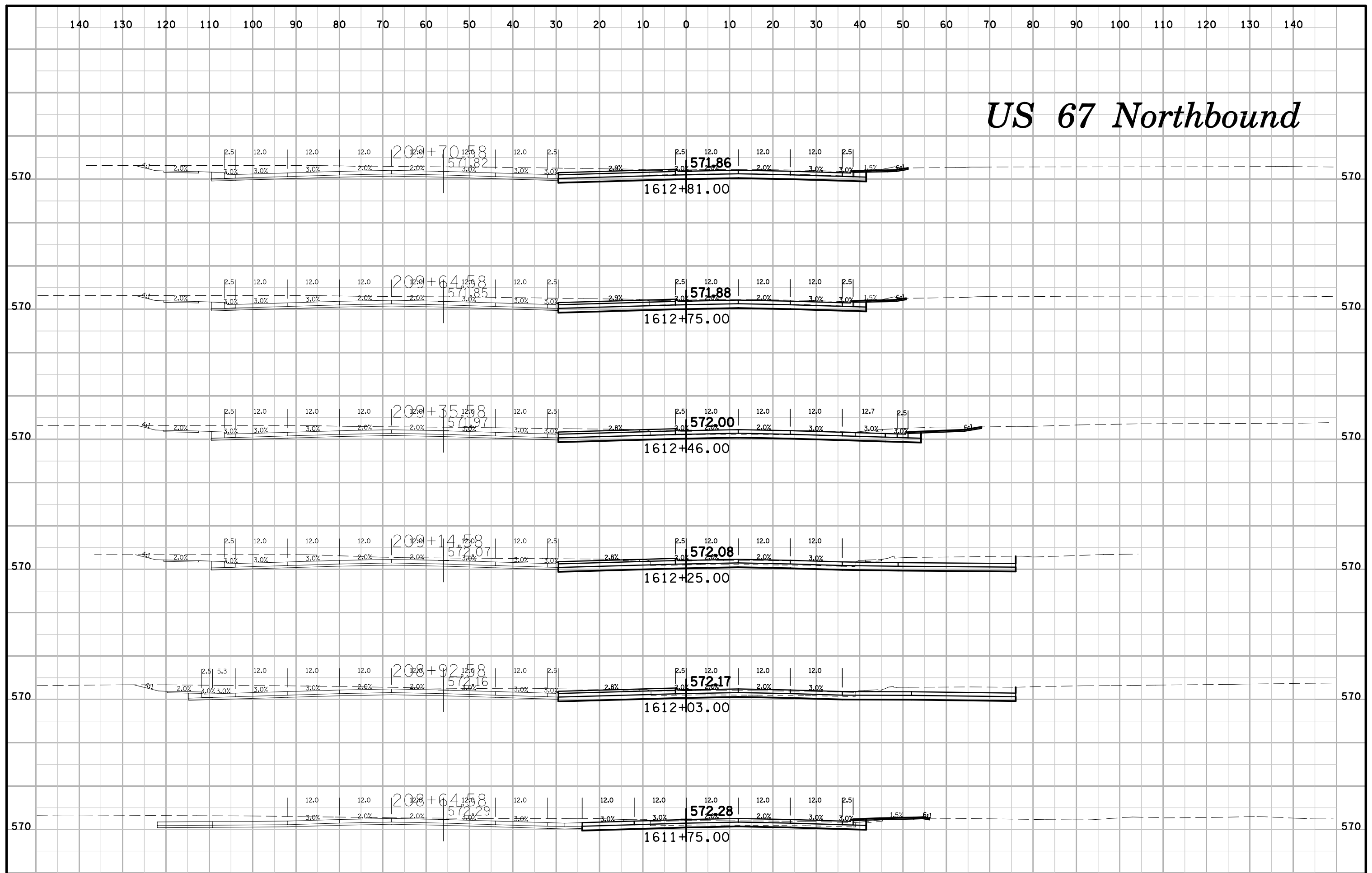
# US 67 Northbound



# US 67 Northbound

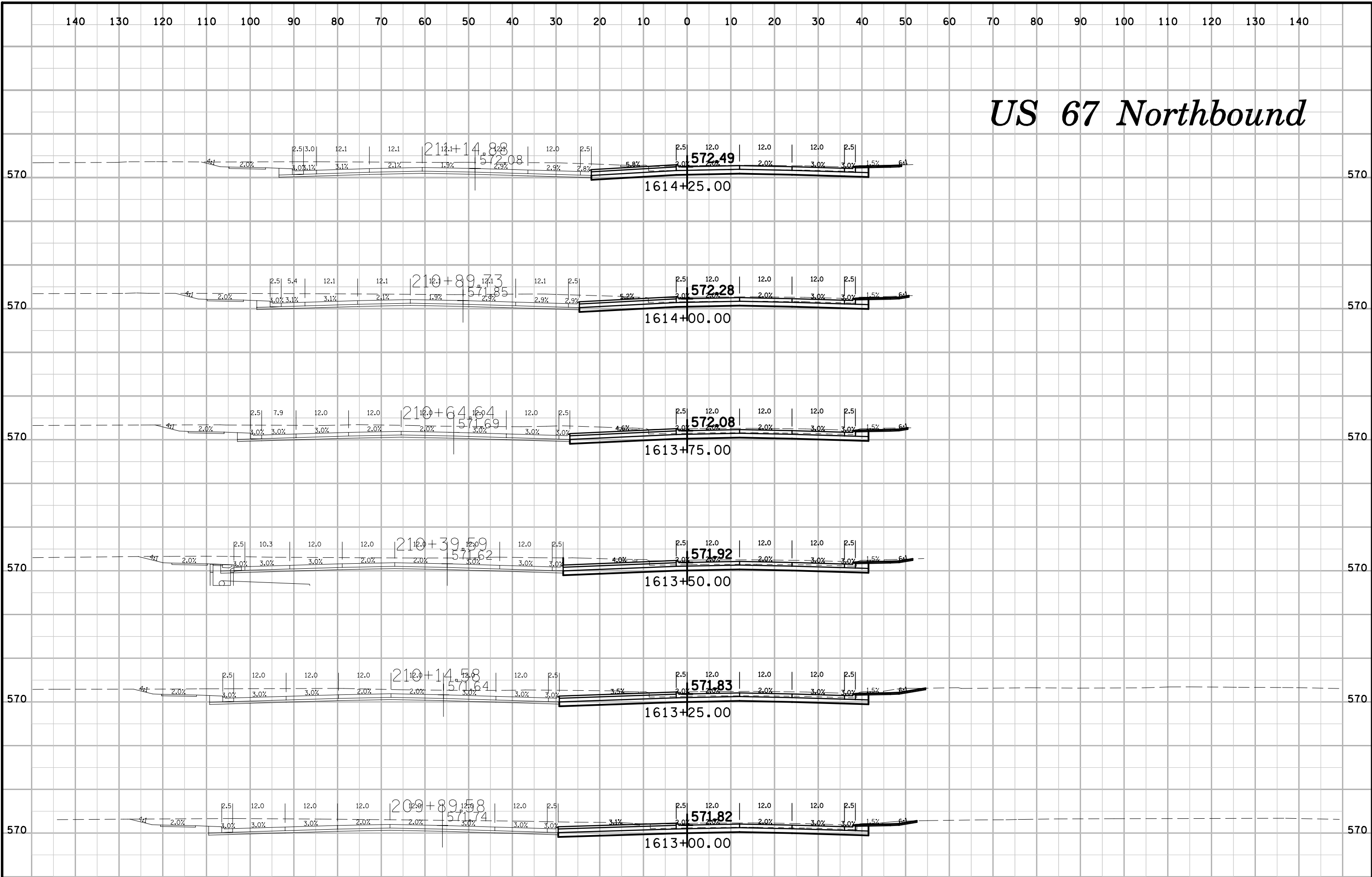


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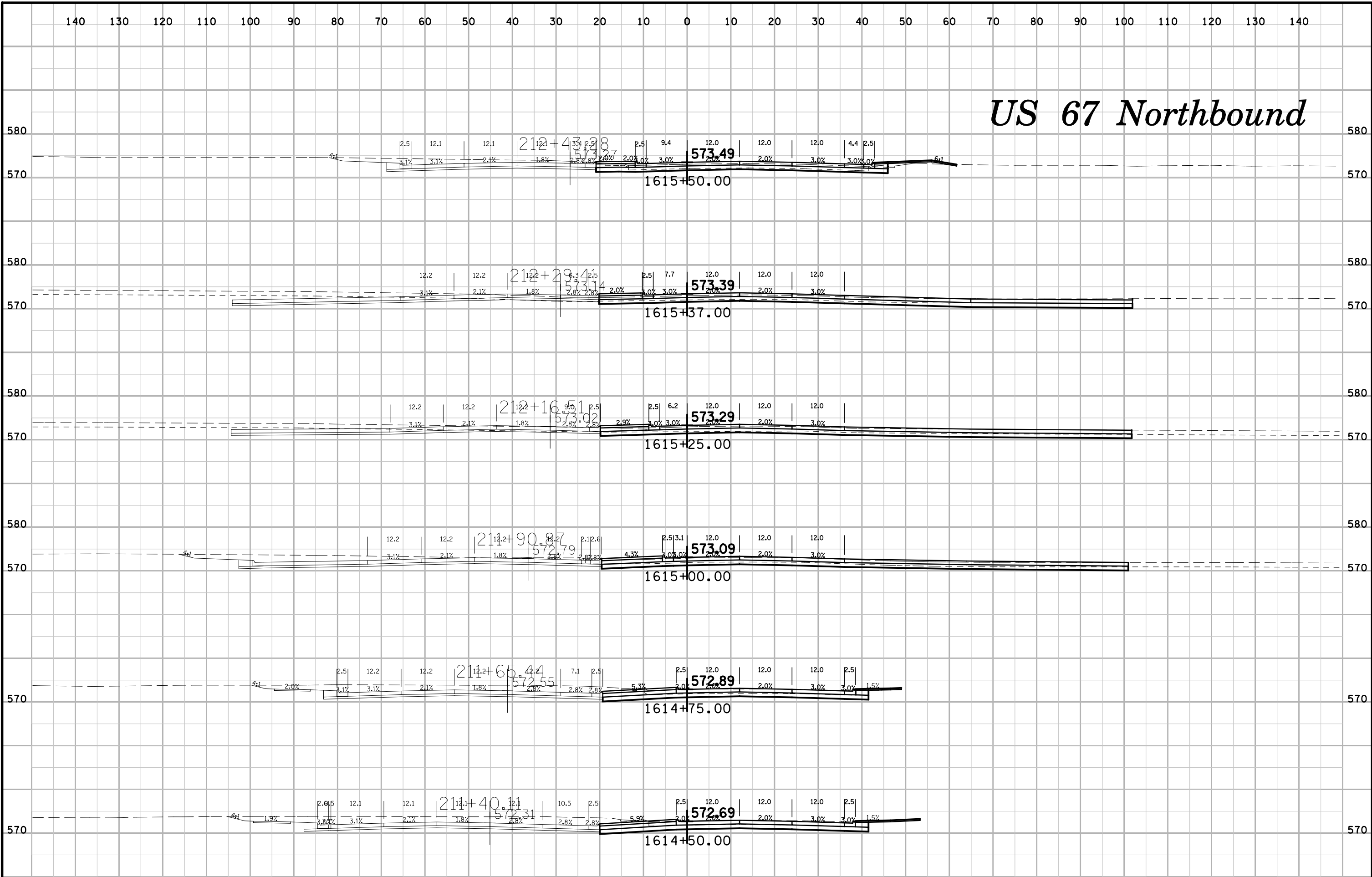




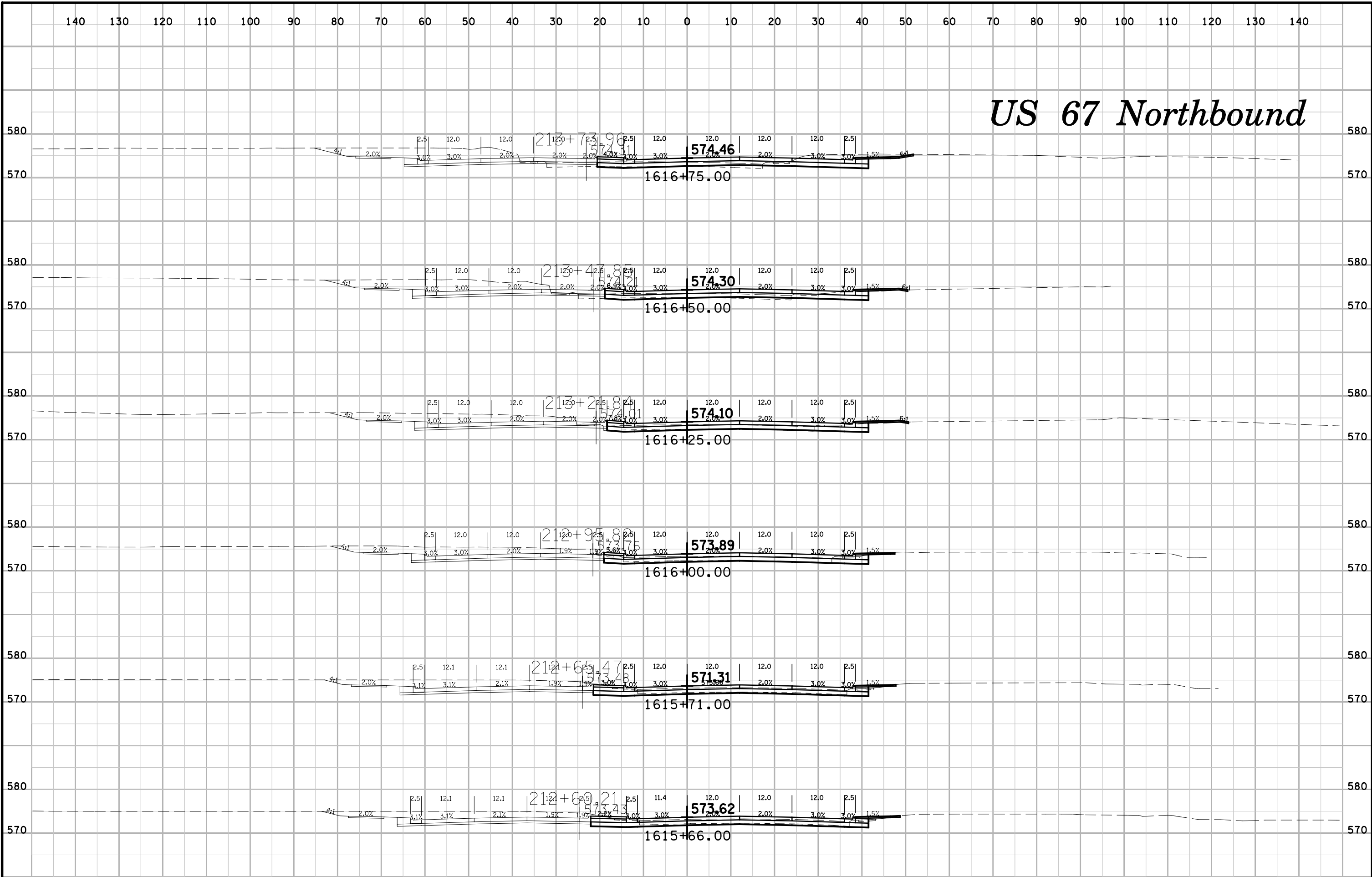
# US 67 Northbound



# US 67 Northbound

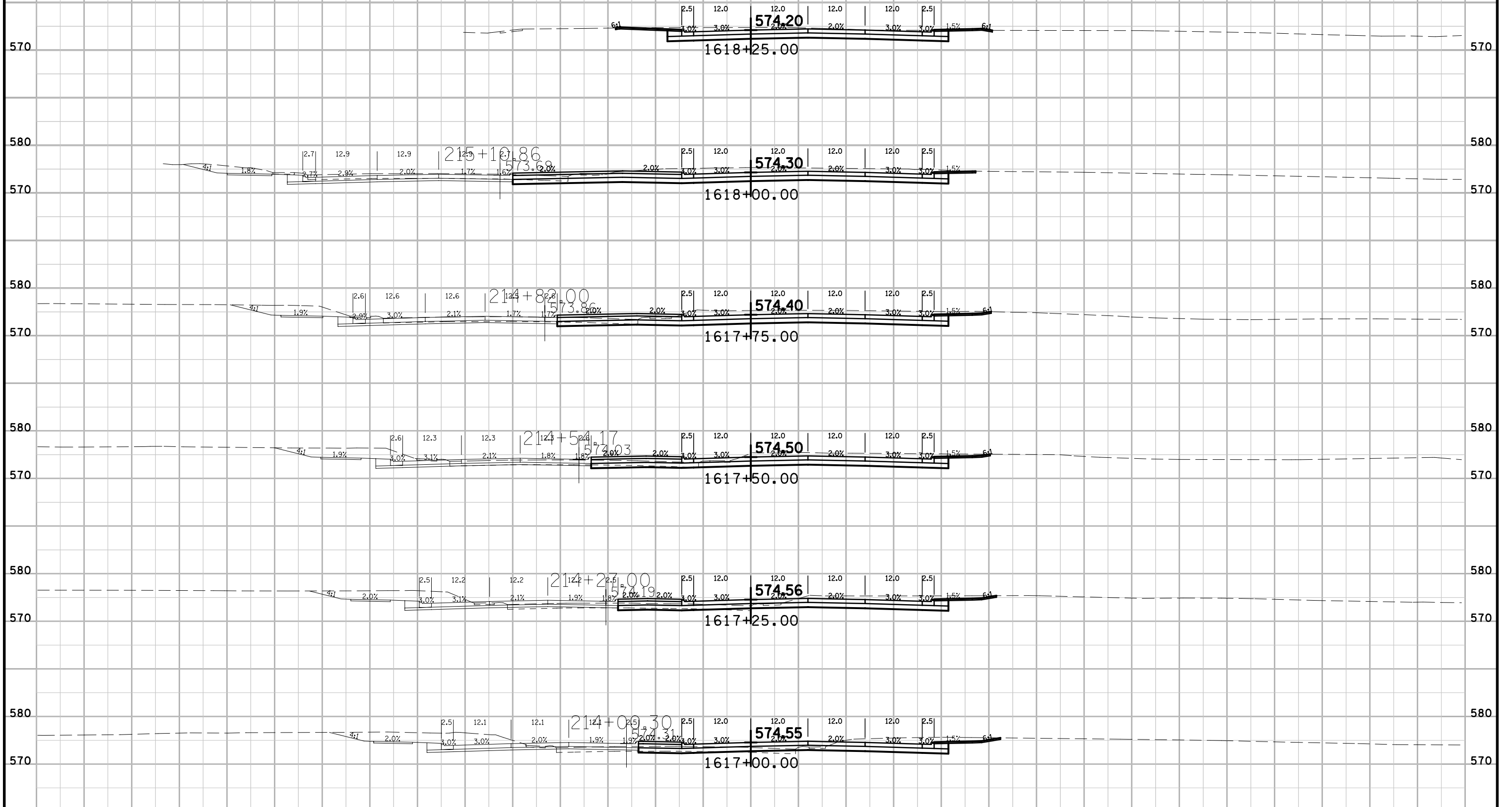


# US 67 Northbound



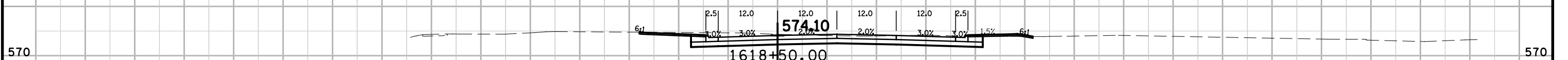
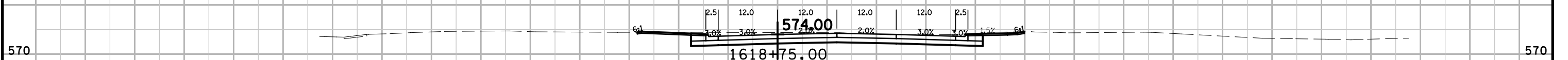
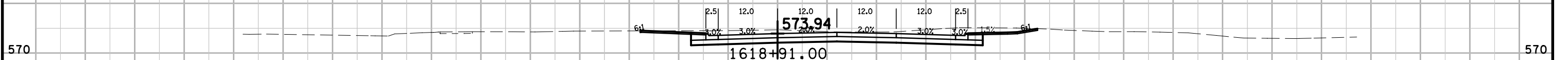
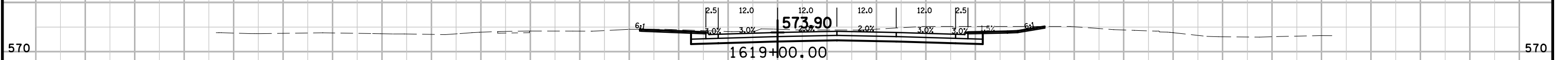
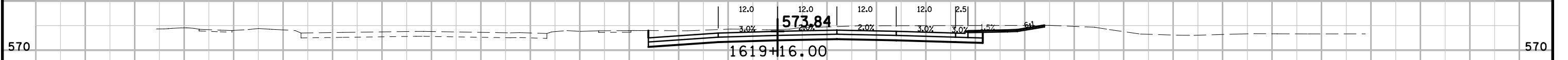
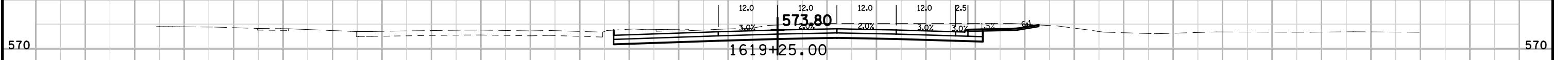
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# US 67 Northbound



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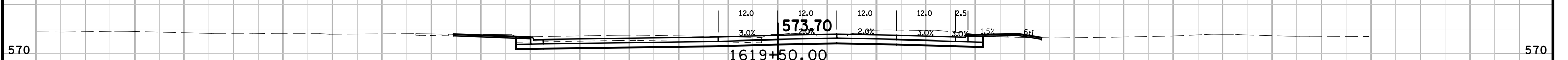
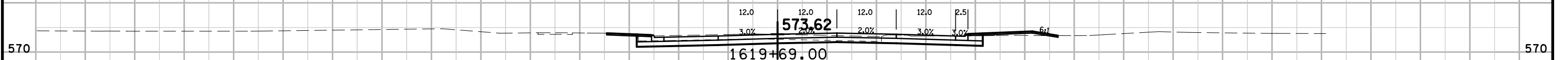
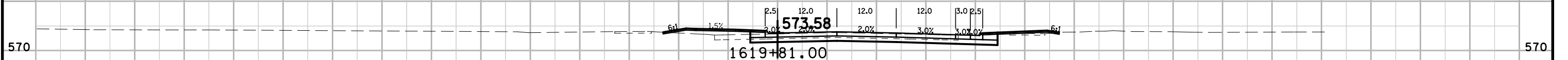
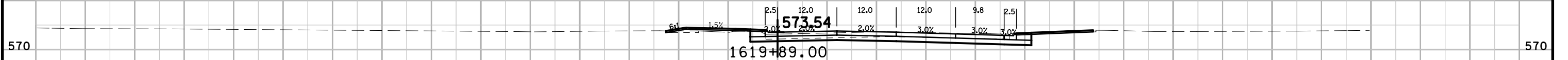
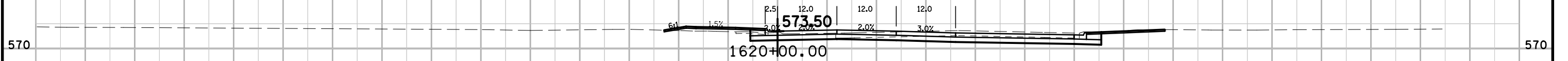
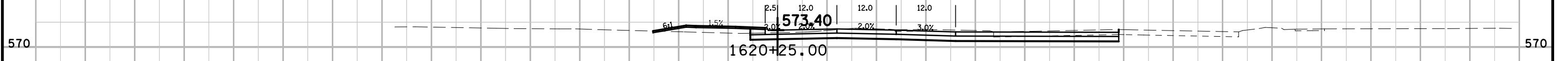
# US 67 Northbound



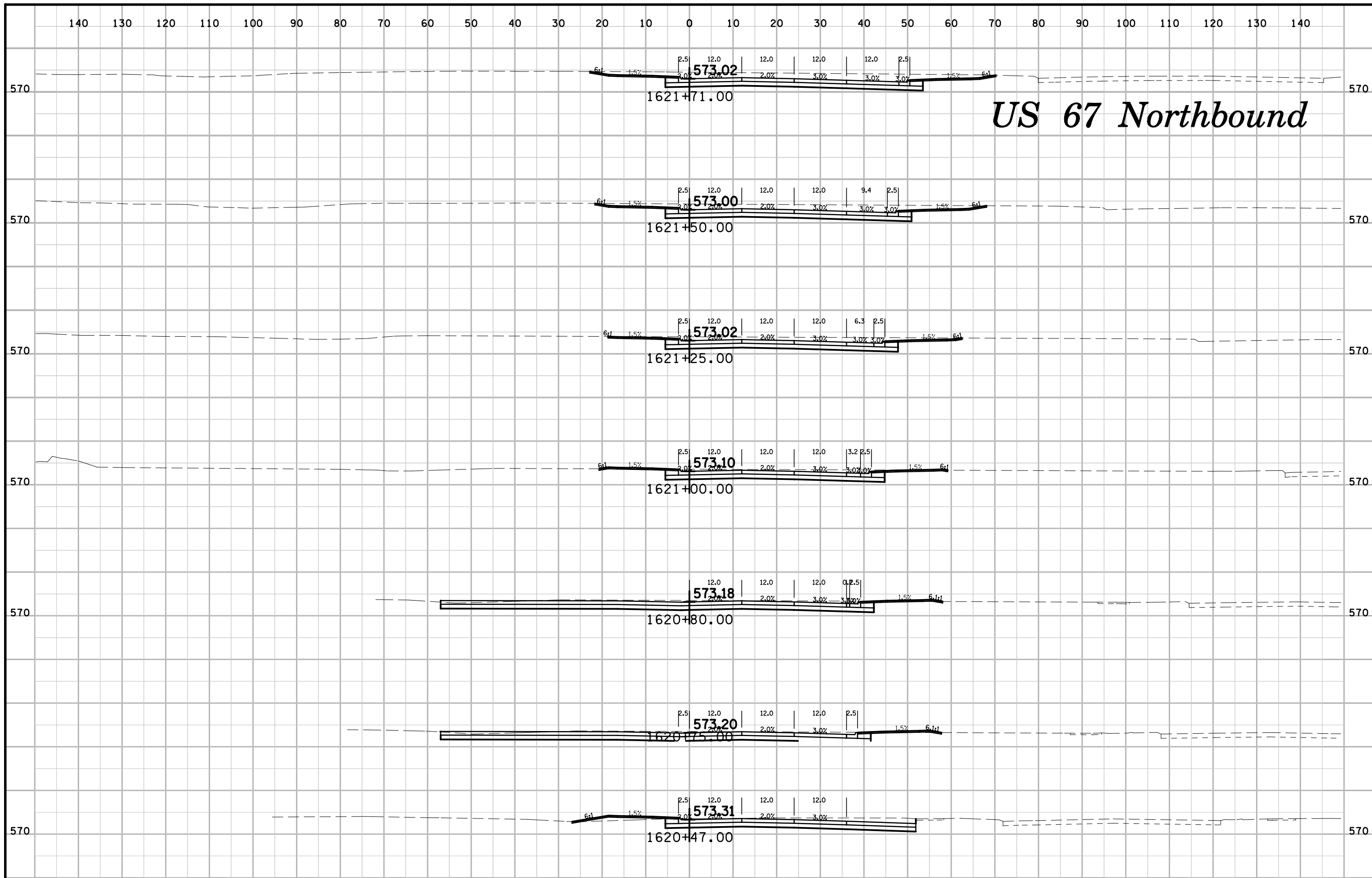
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140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

# US 67 Northbound

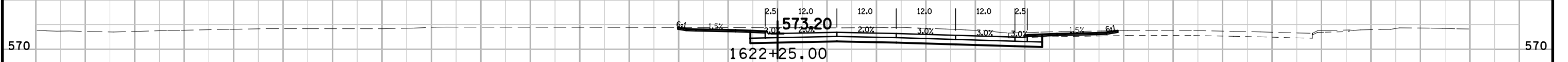
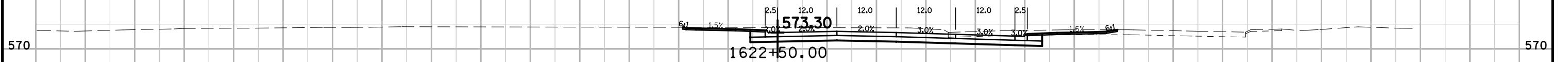
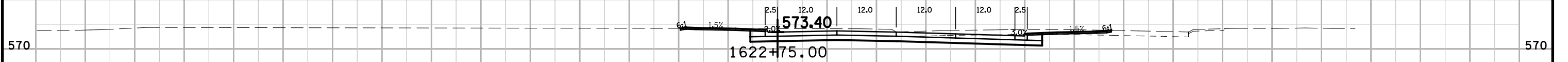
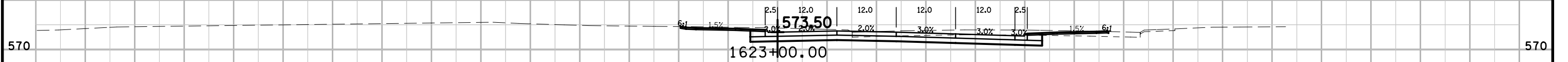


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140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

# US 67 Northbound

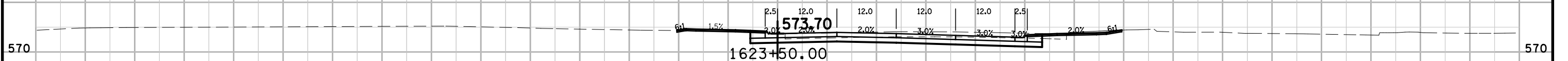
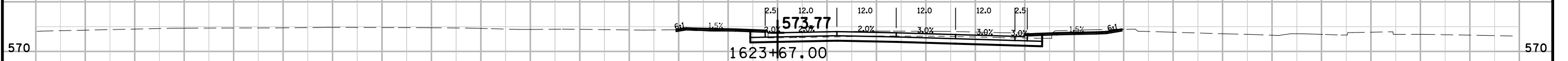
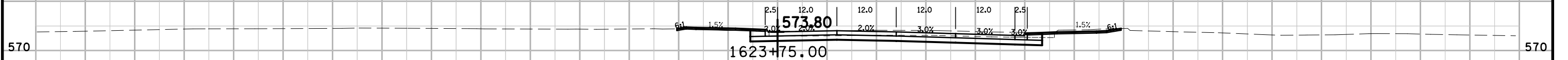
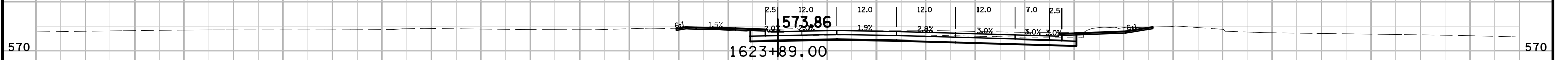
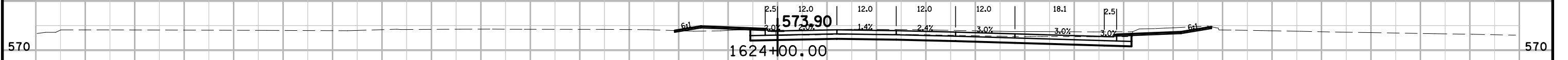
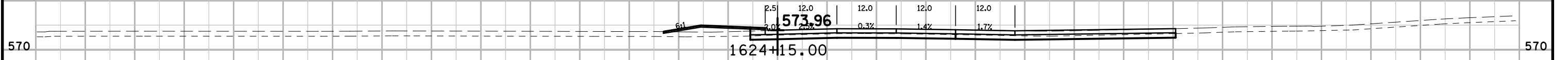


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140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

# US 67 Northbound

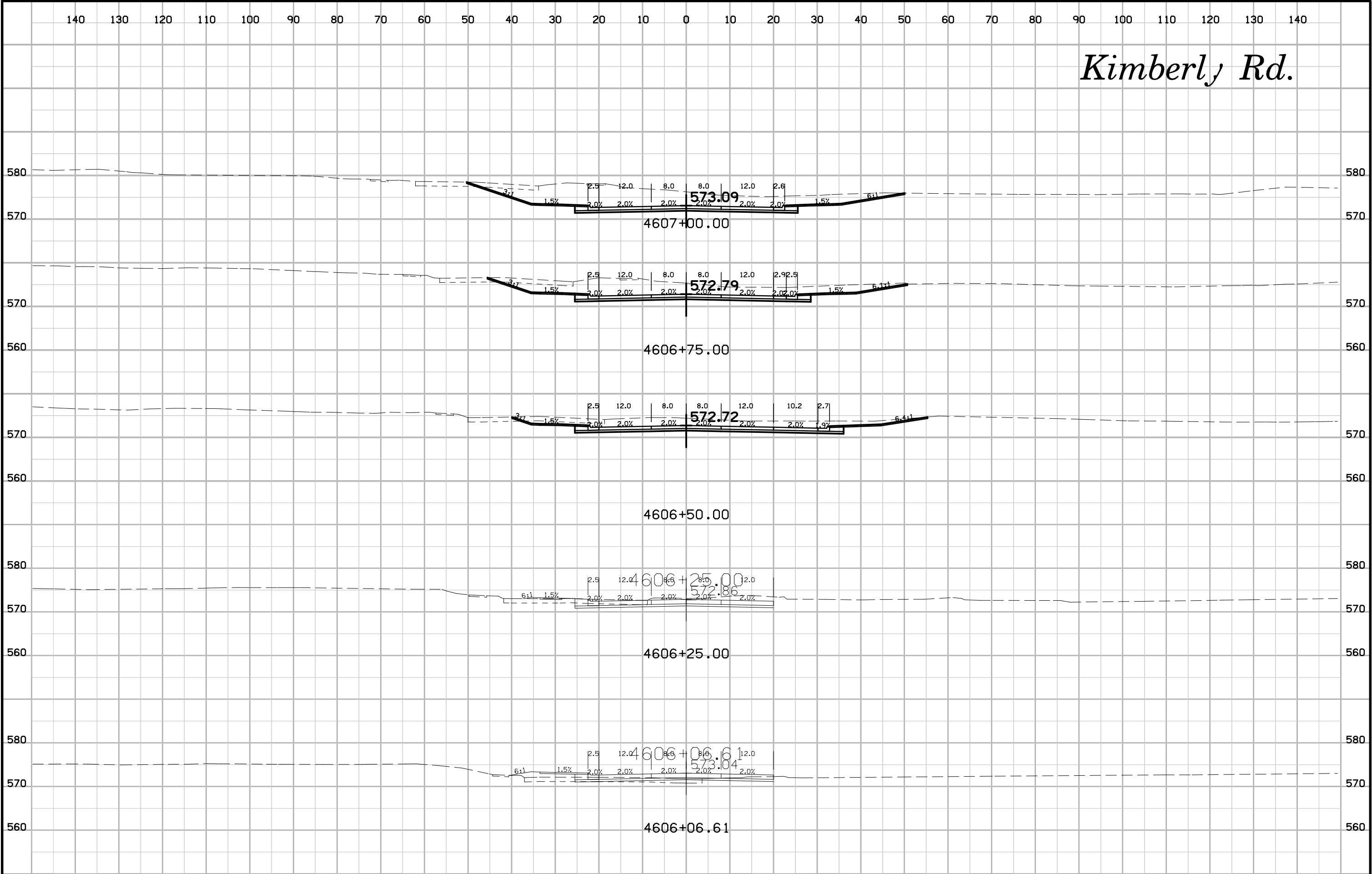


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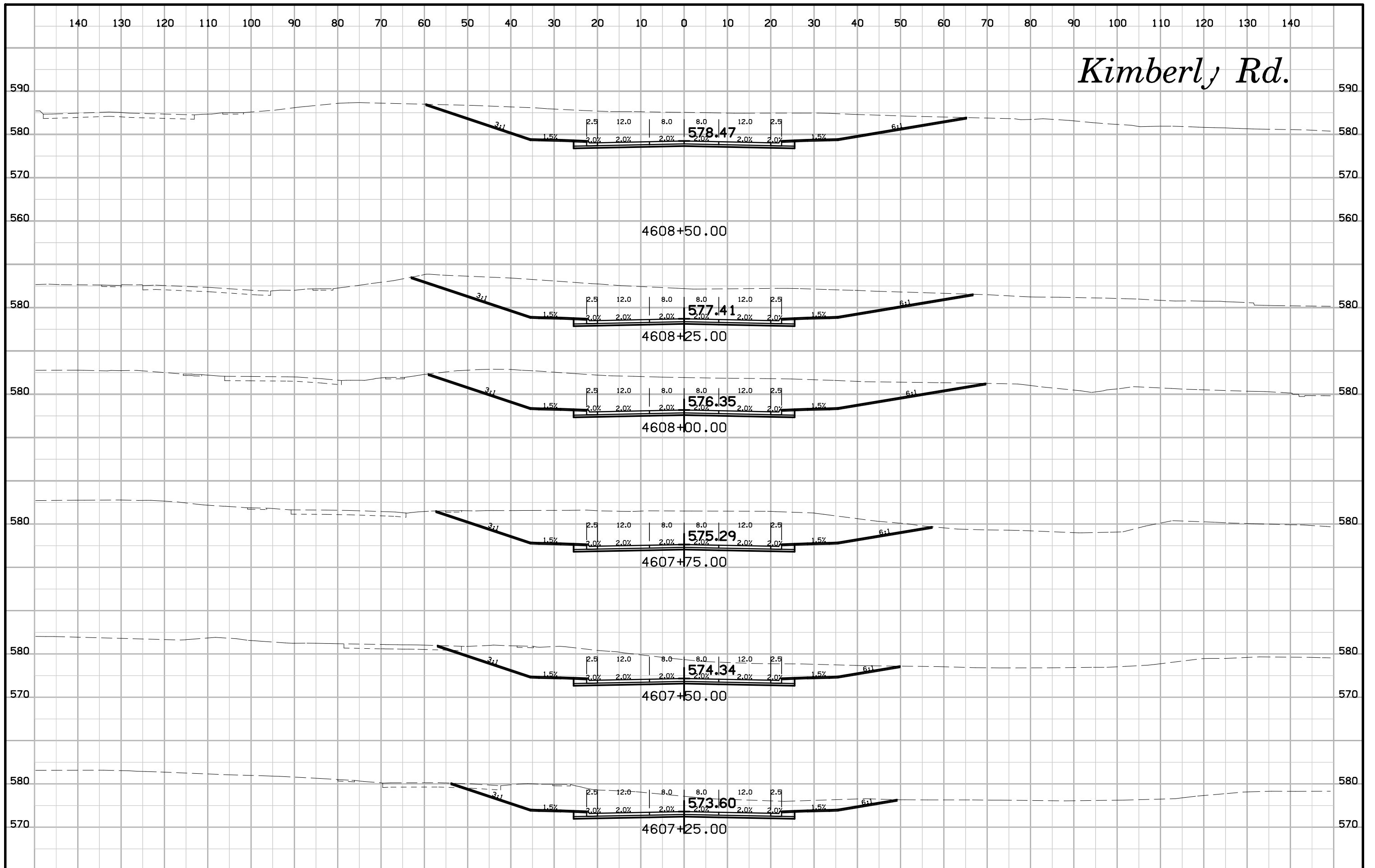
RESERVED FOR STATE  
STREET CROSS SECTIONS

RESERVED FOR STATE  
STREET CROSS SECTIONS

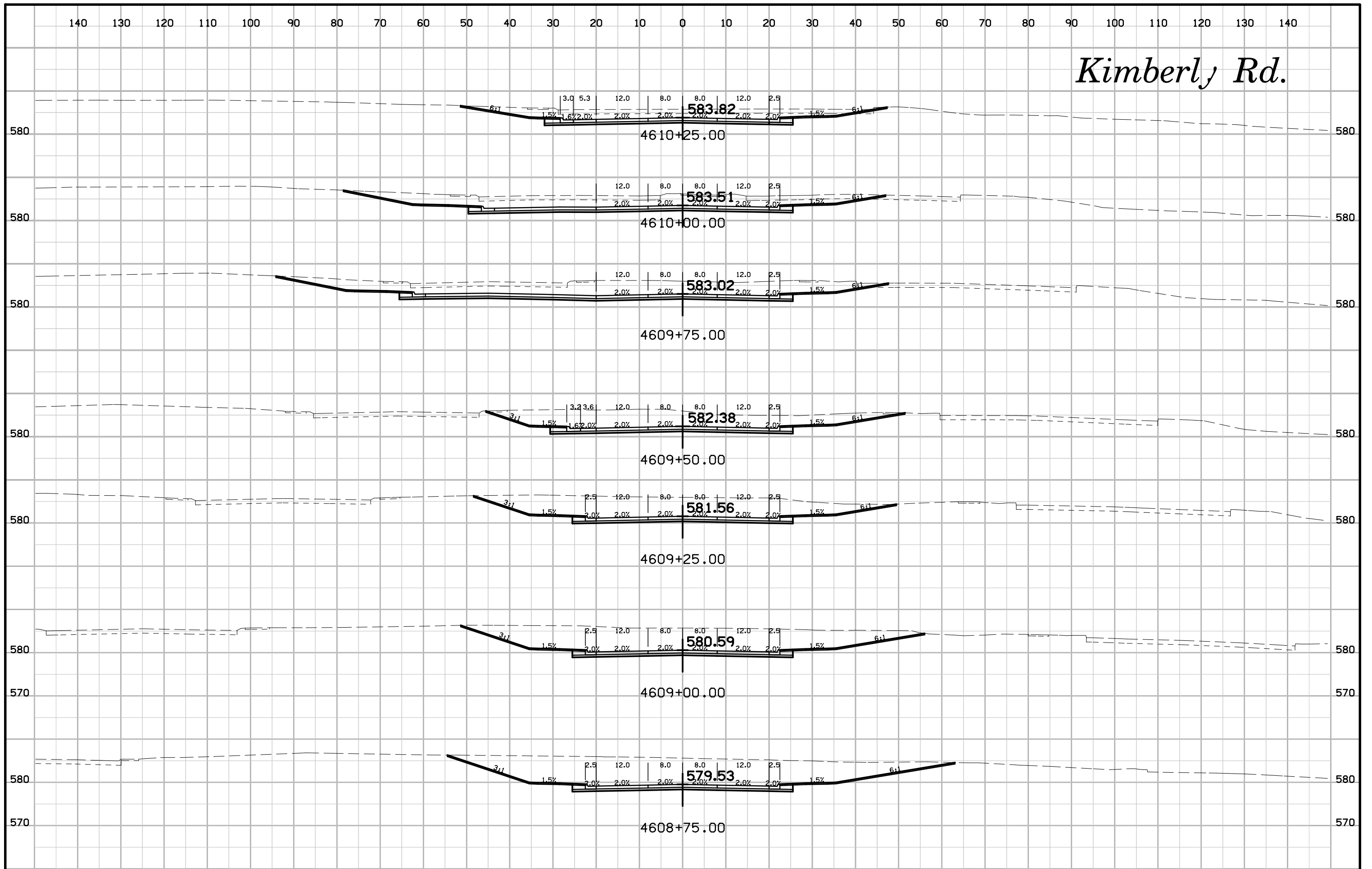
Kimberly Rd.



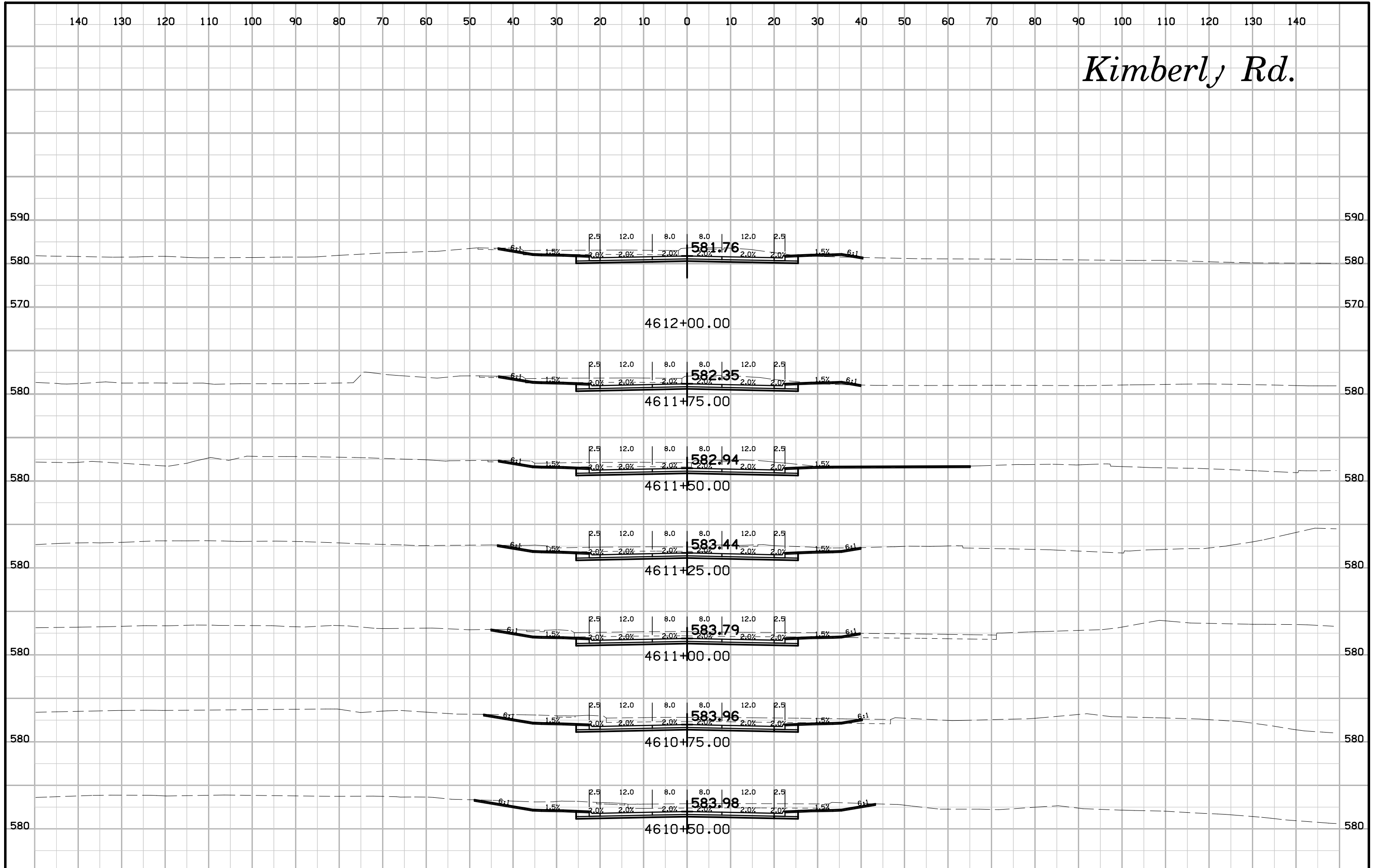
# Kimberly Rd.



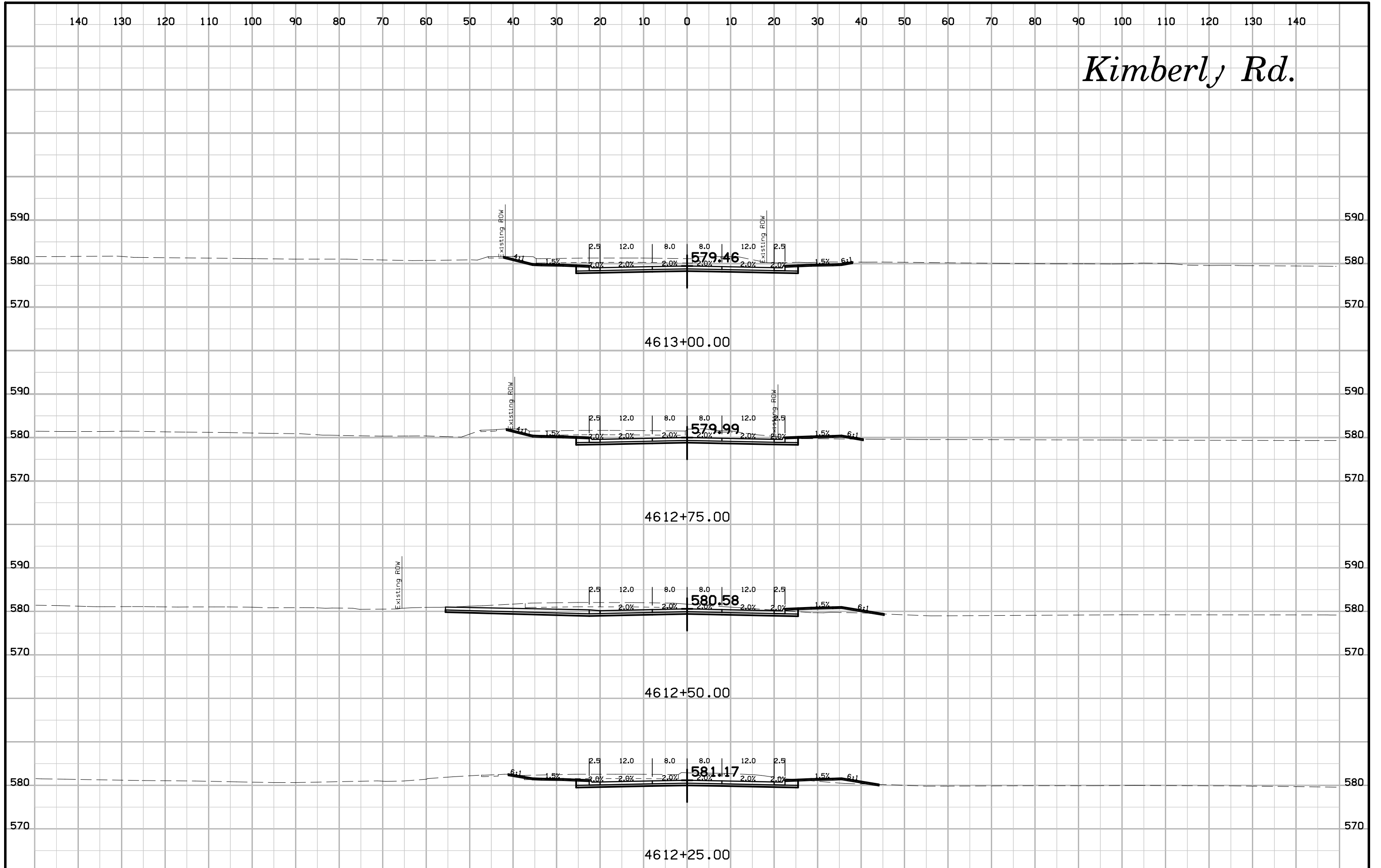
# Kimberly Rd.



# Kimberly Rd.

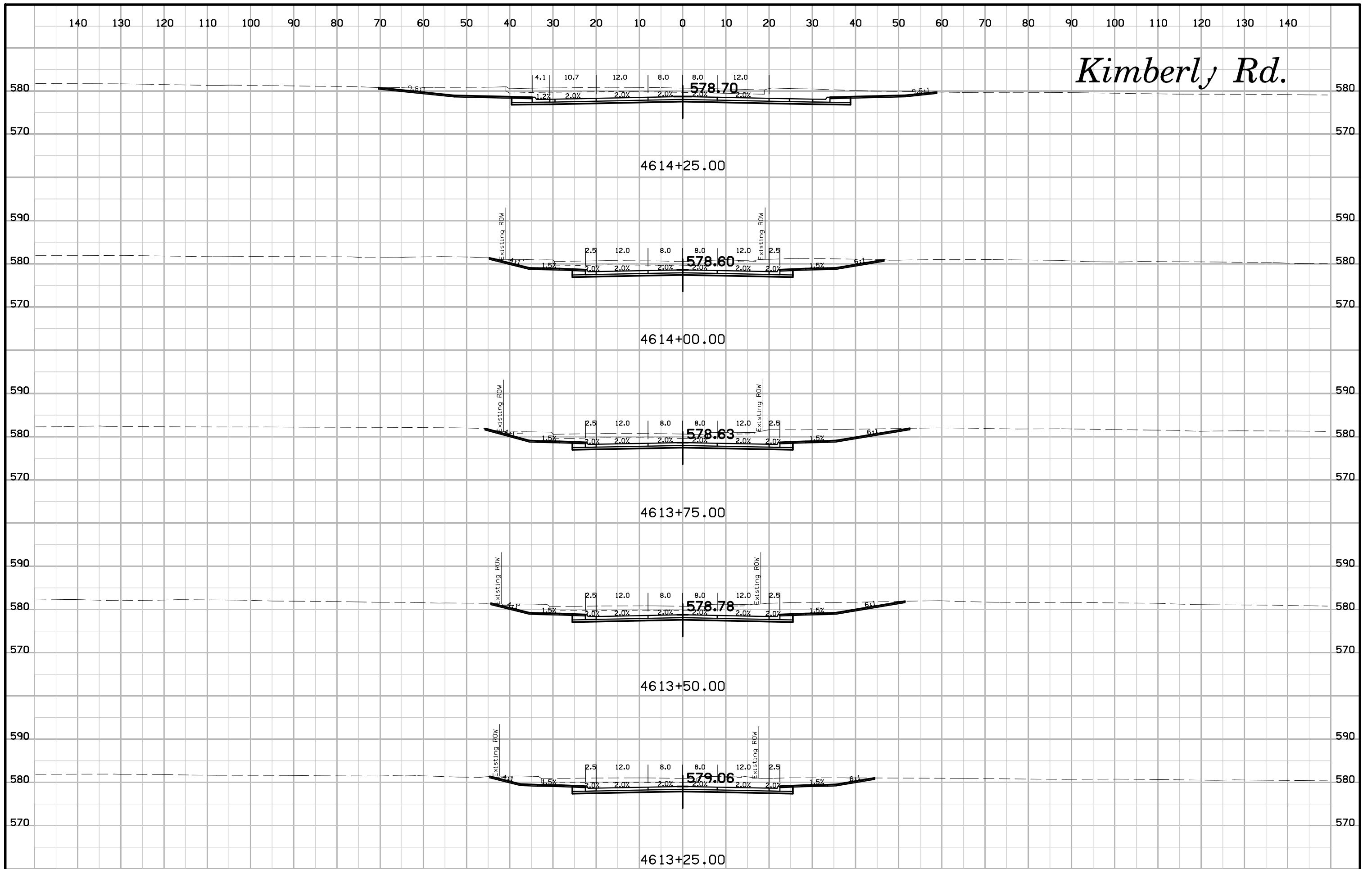


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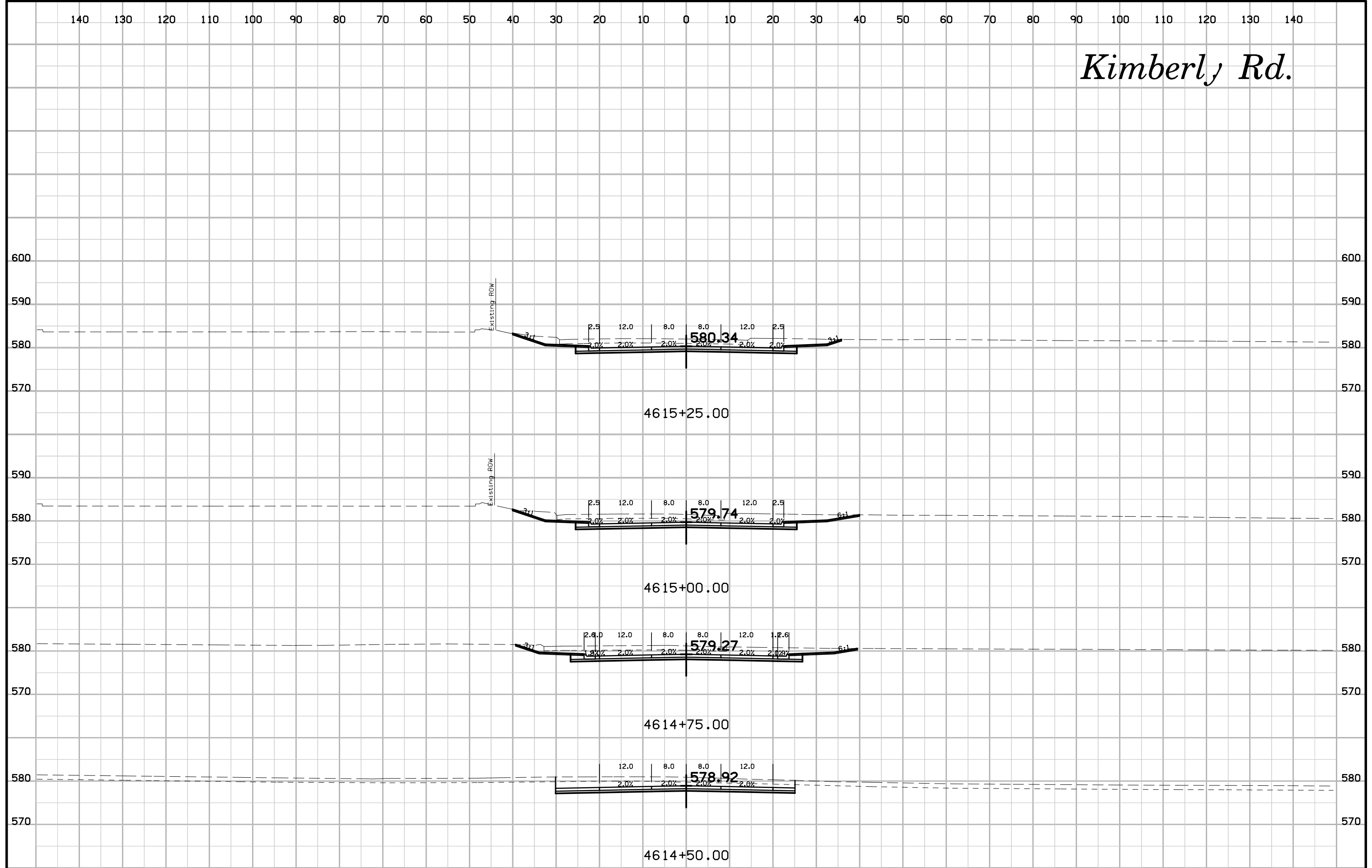




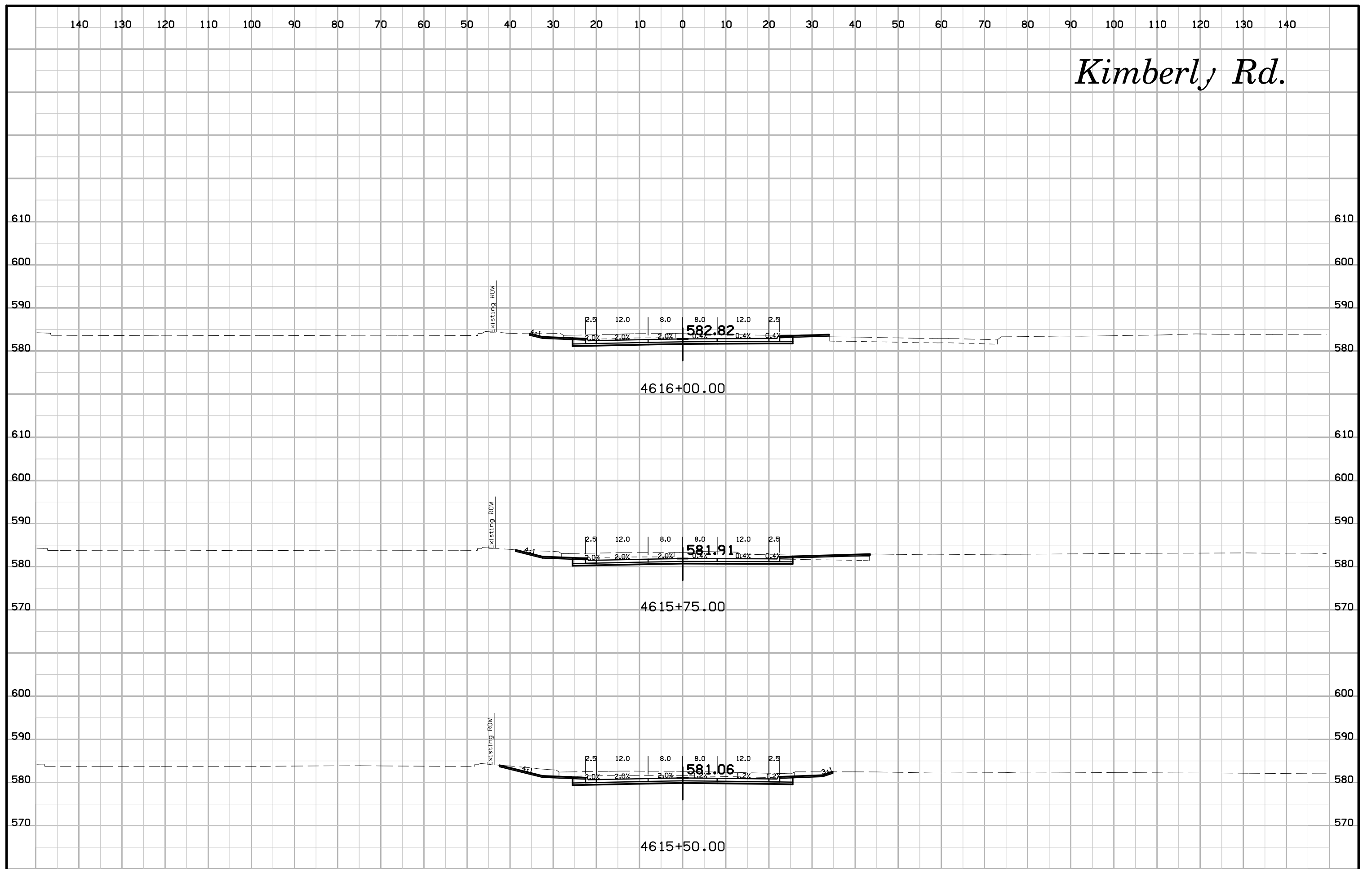
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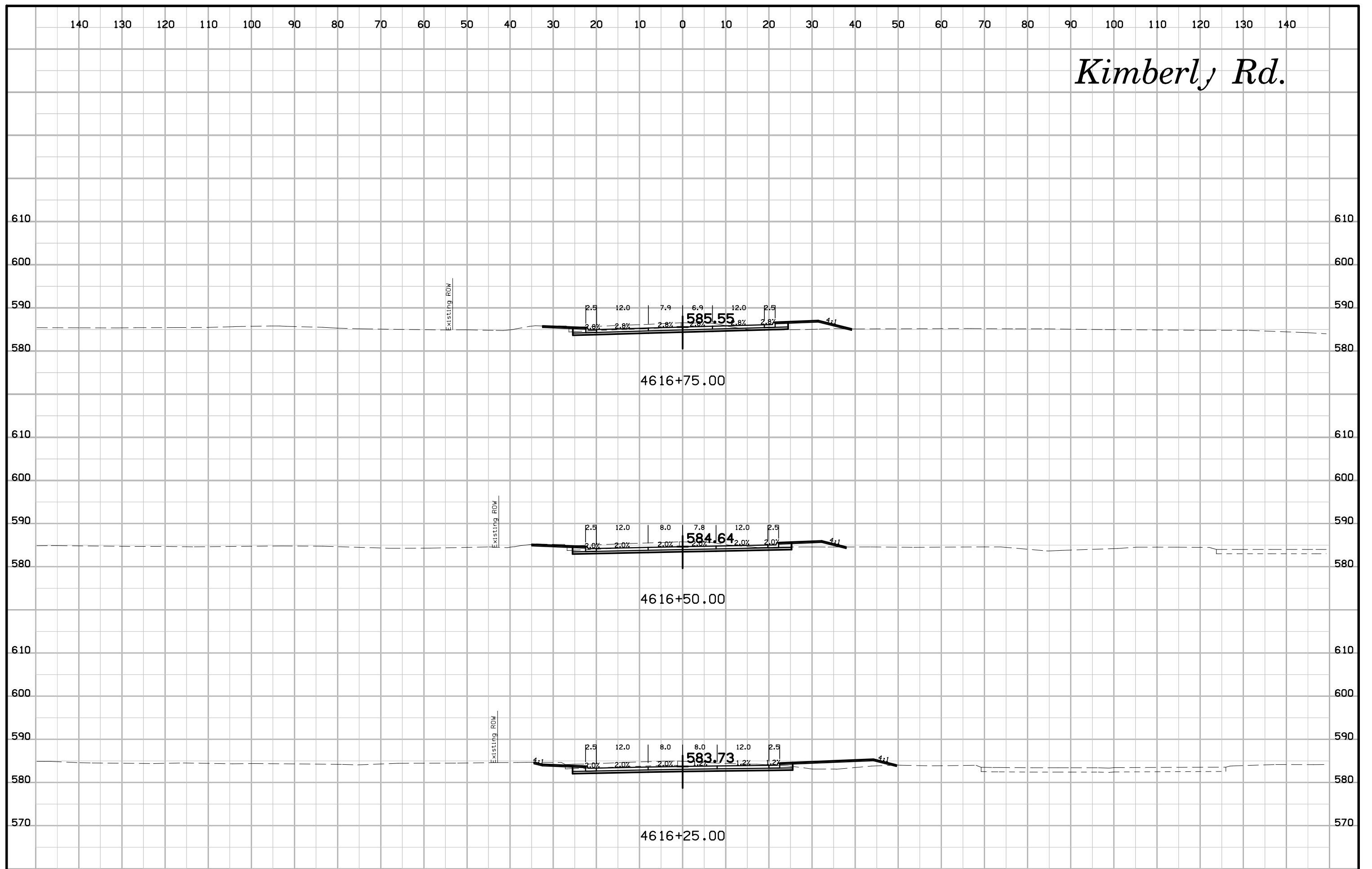
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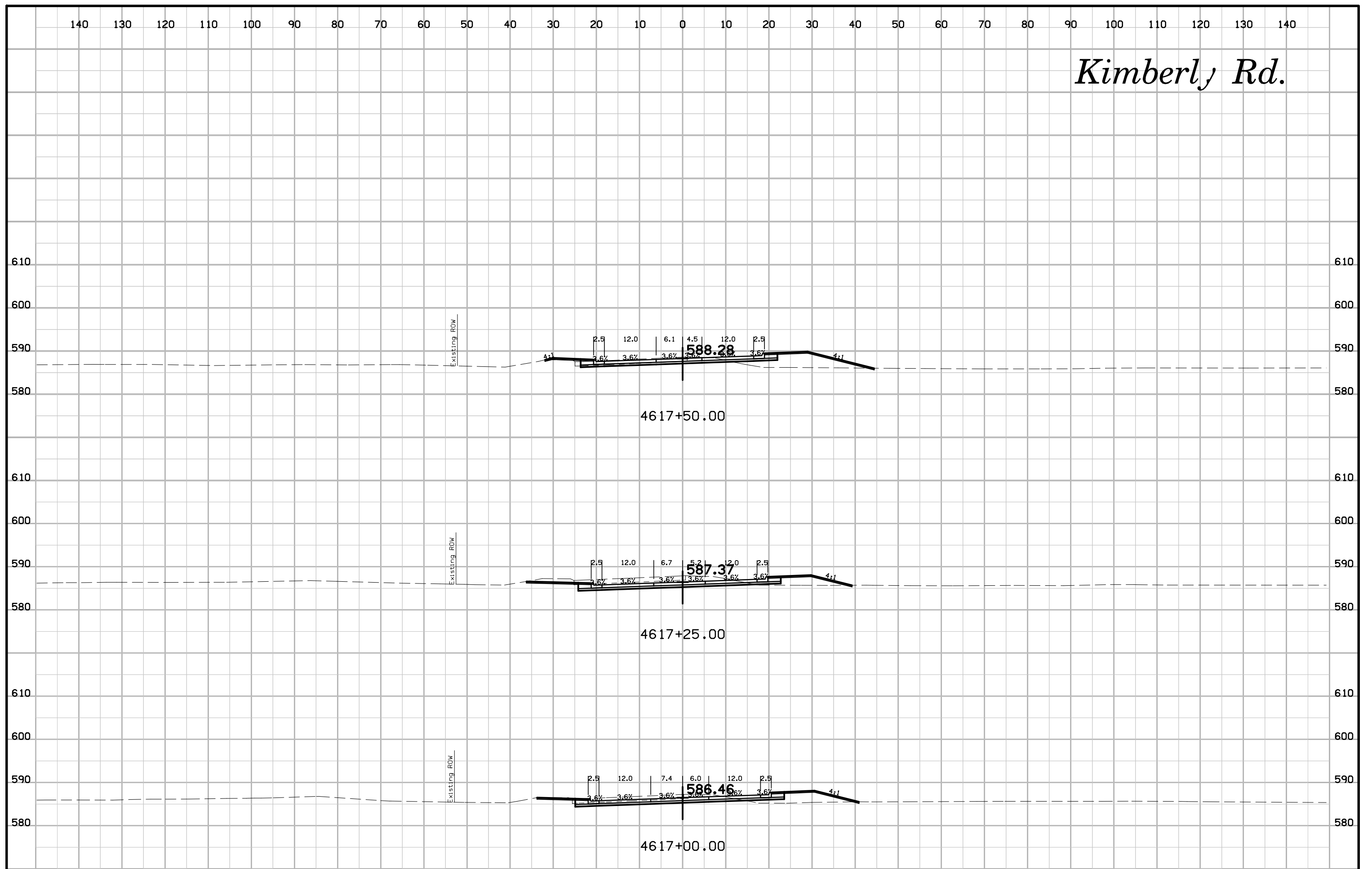
# Kimberly Rd.



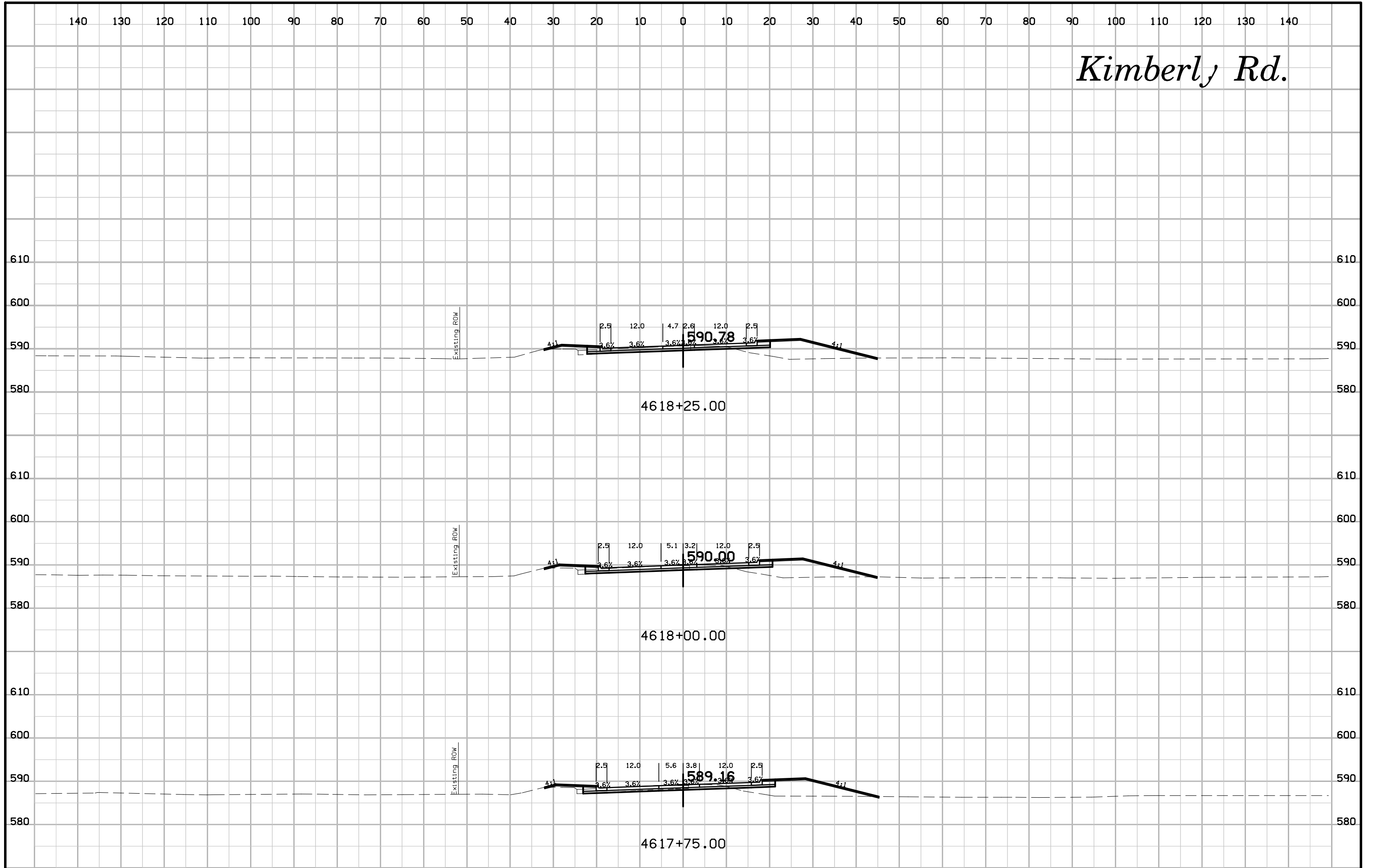
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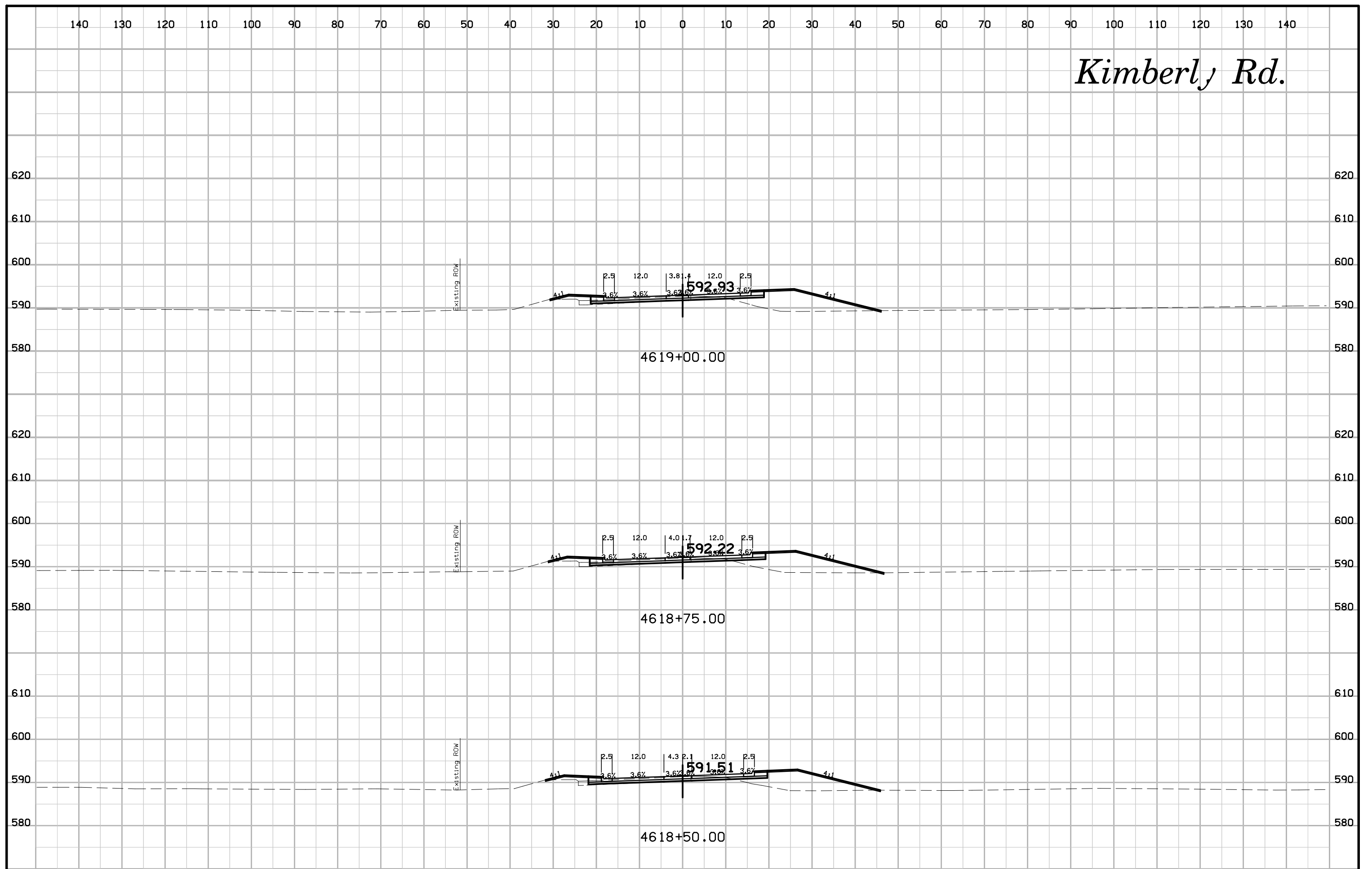
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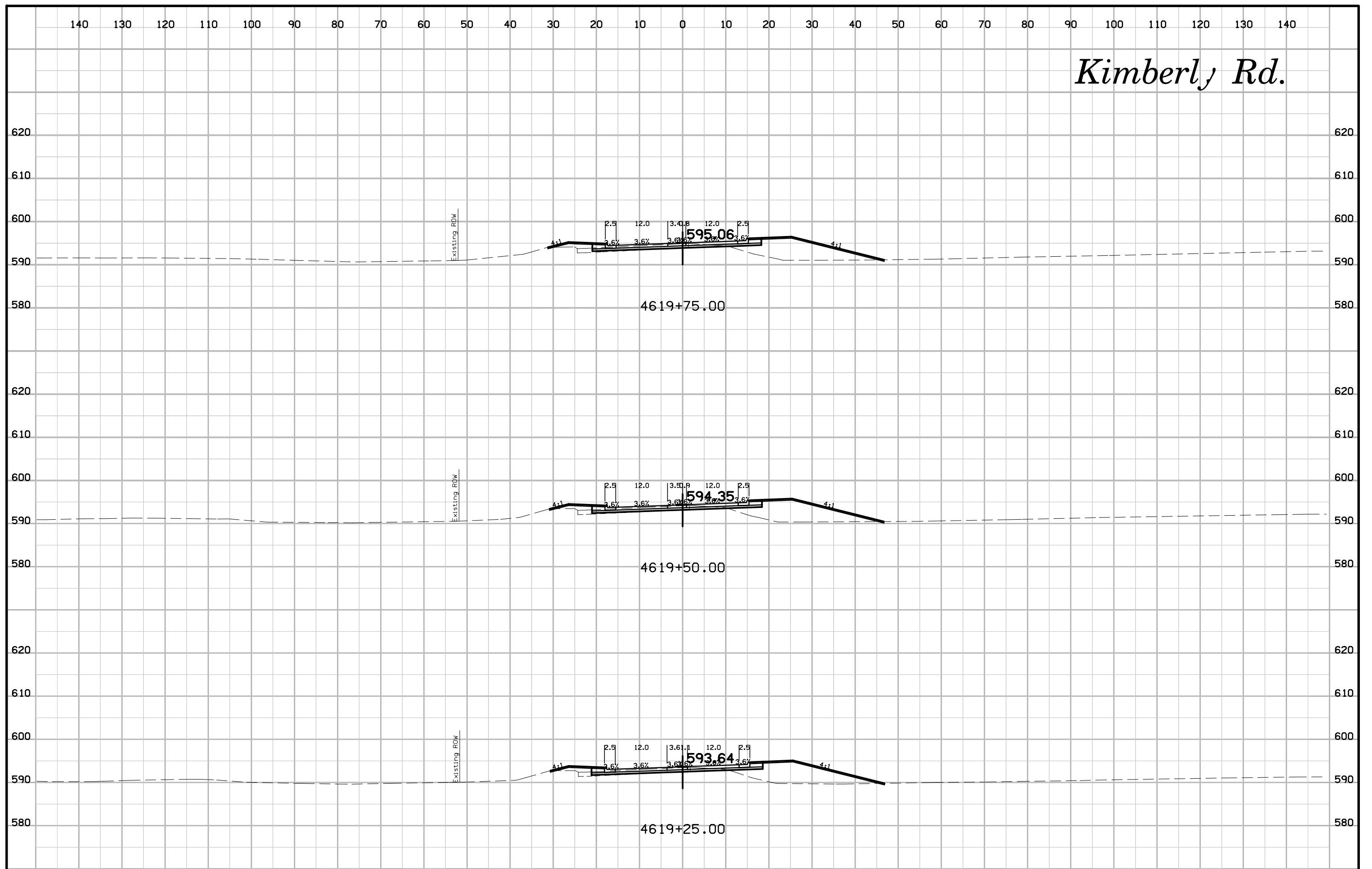
# Kimberly Rd.



# Kimberly Rd.

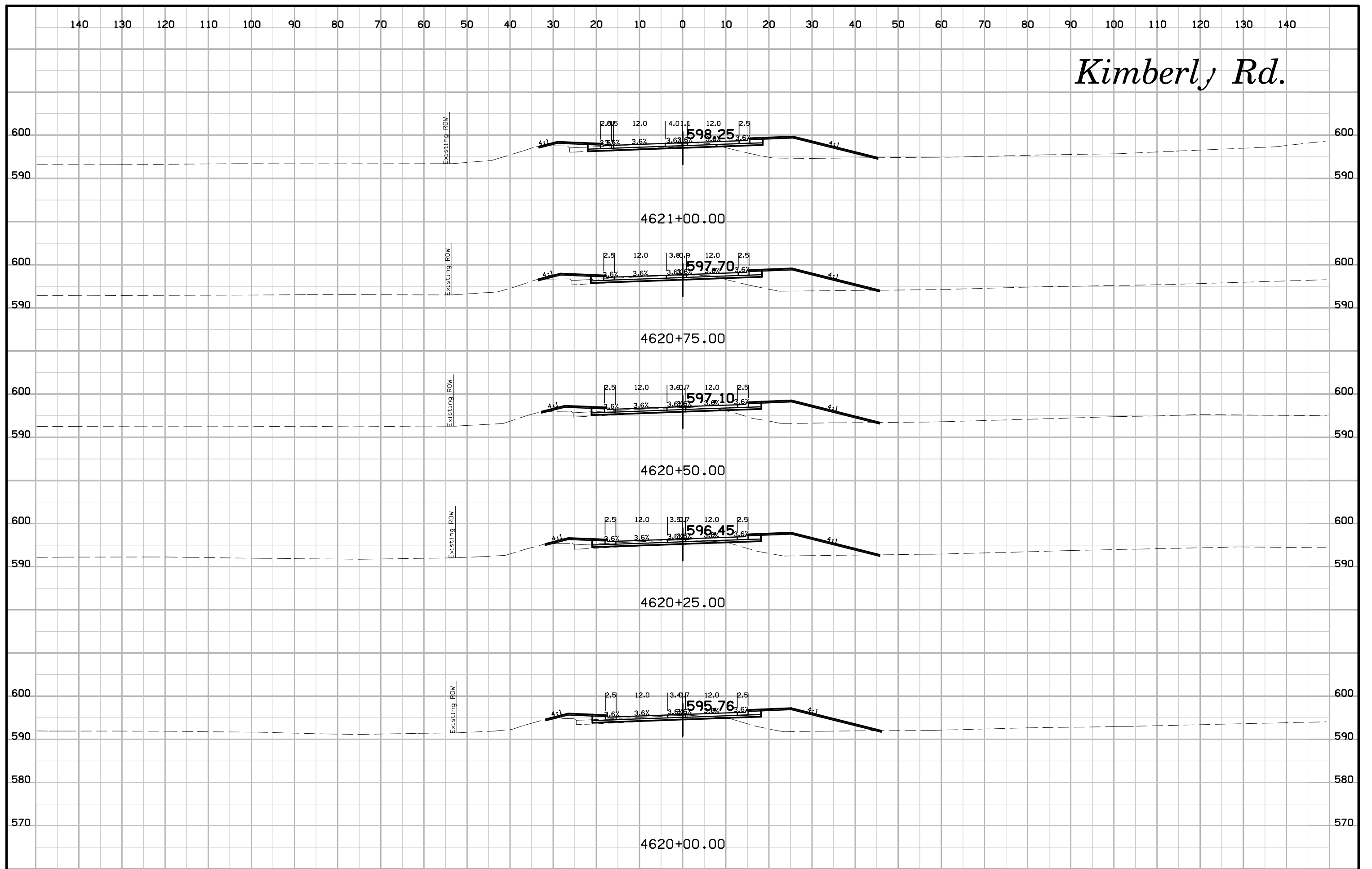


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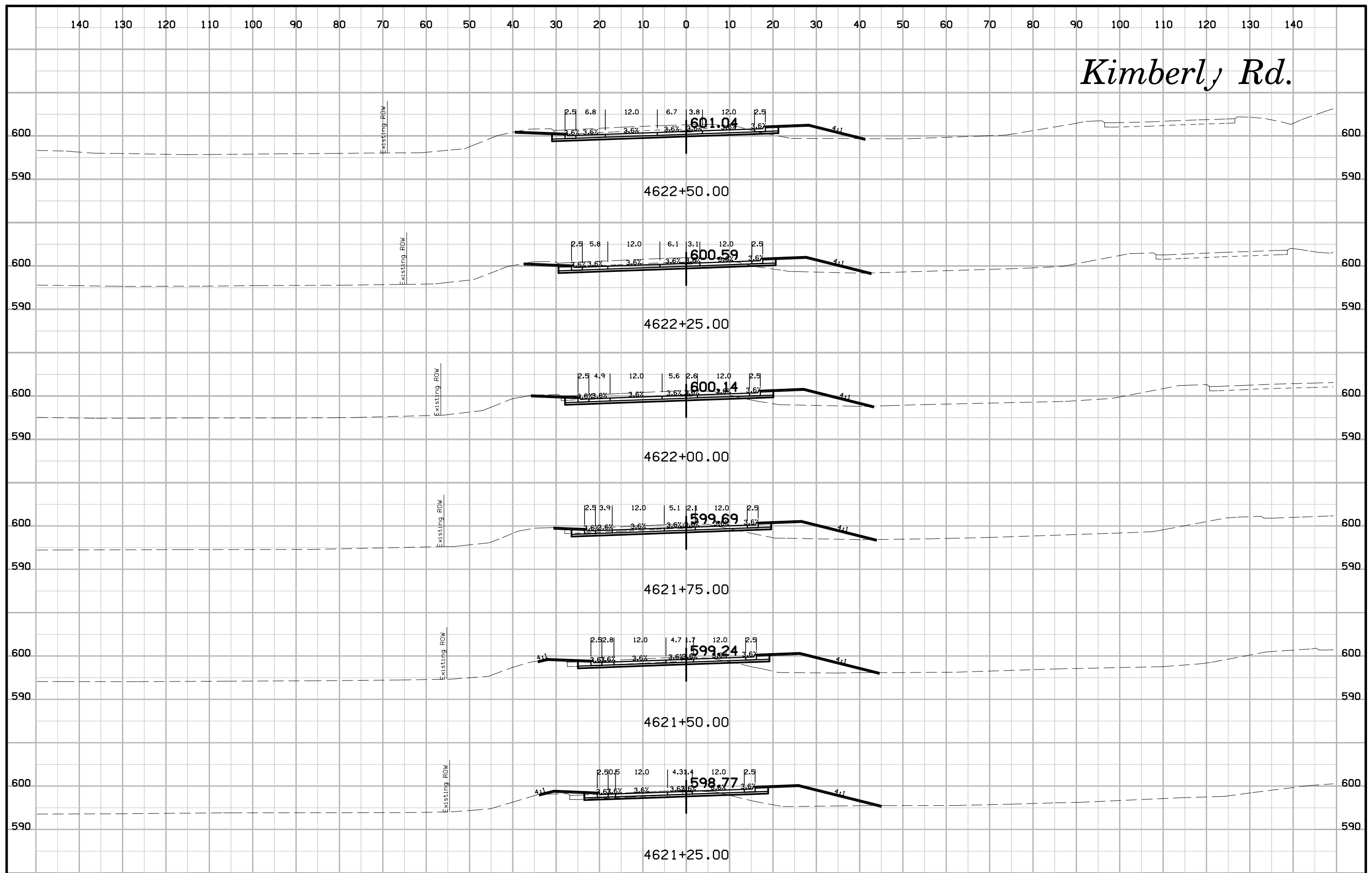




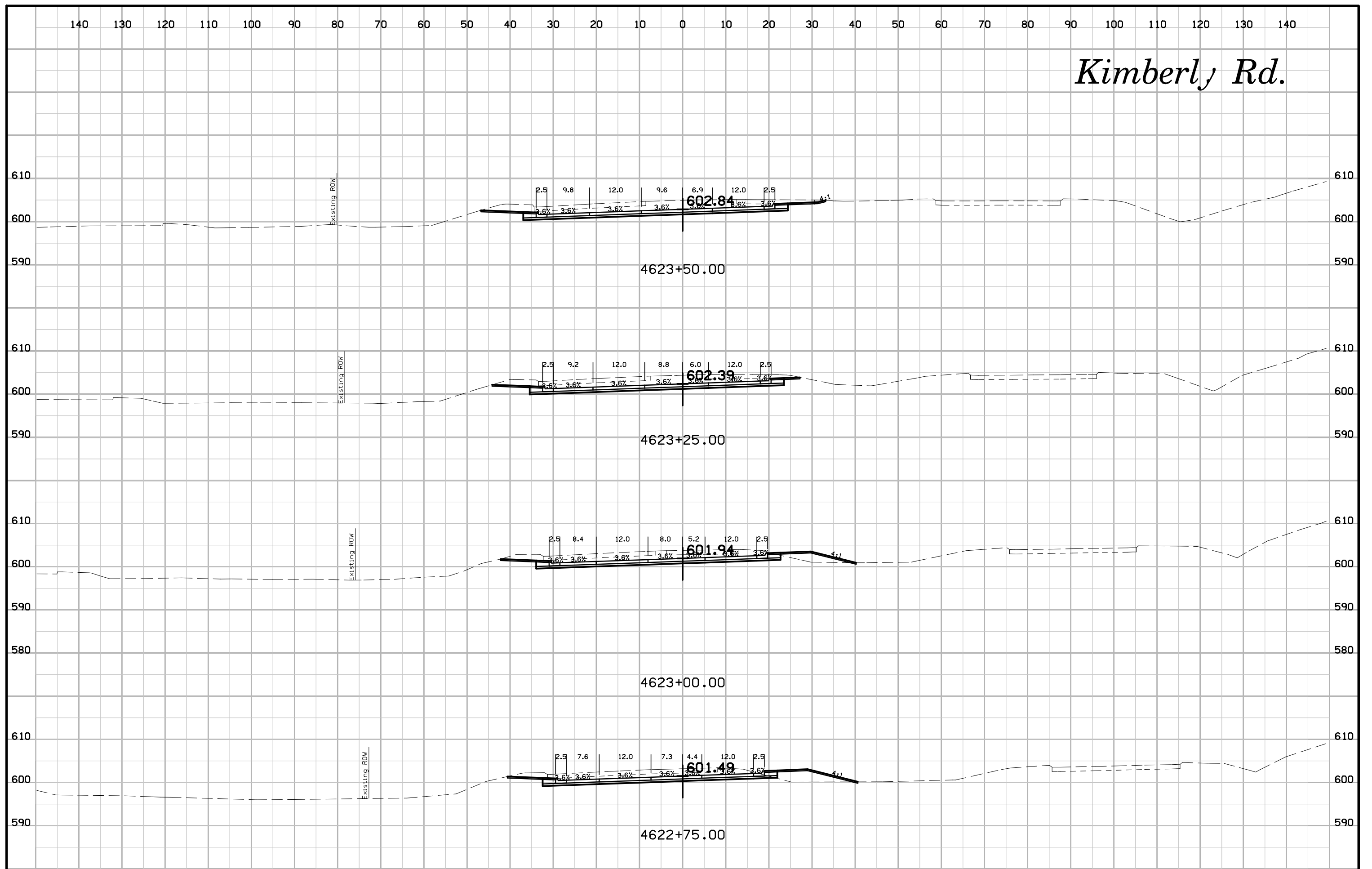
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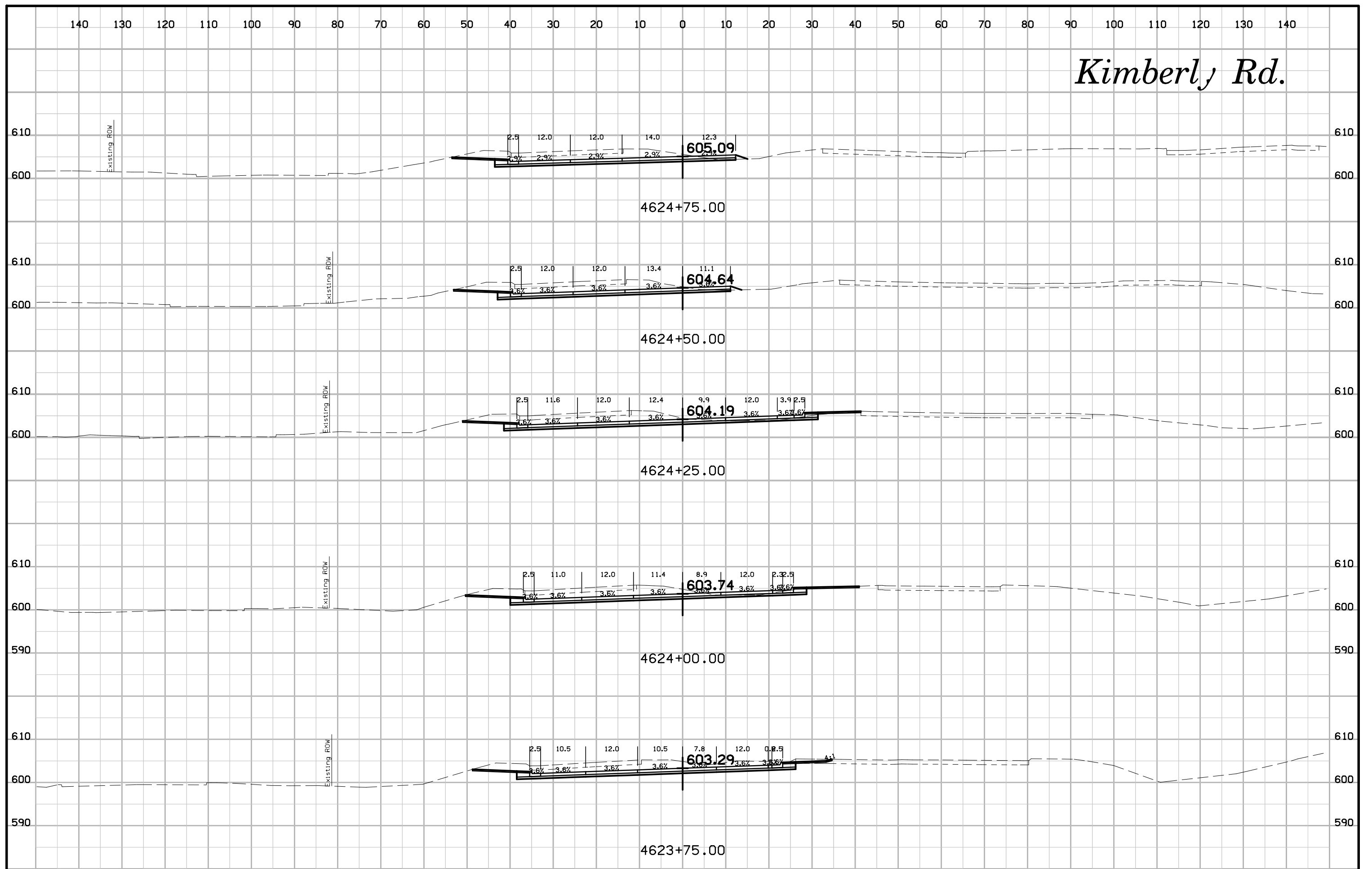
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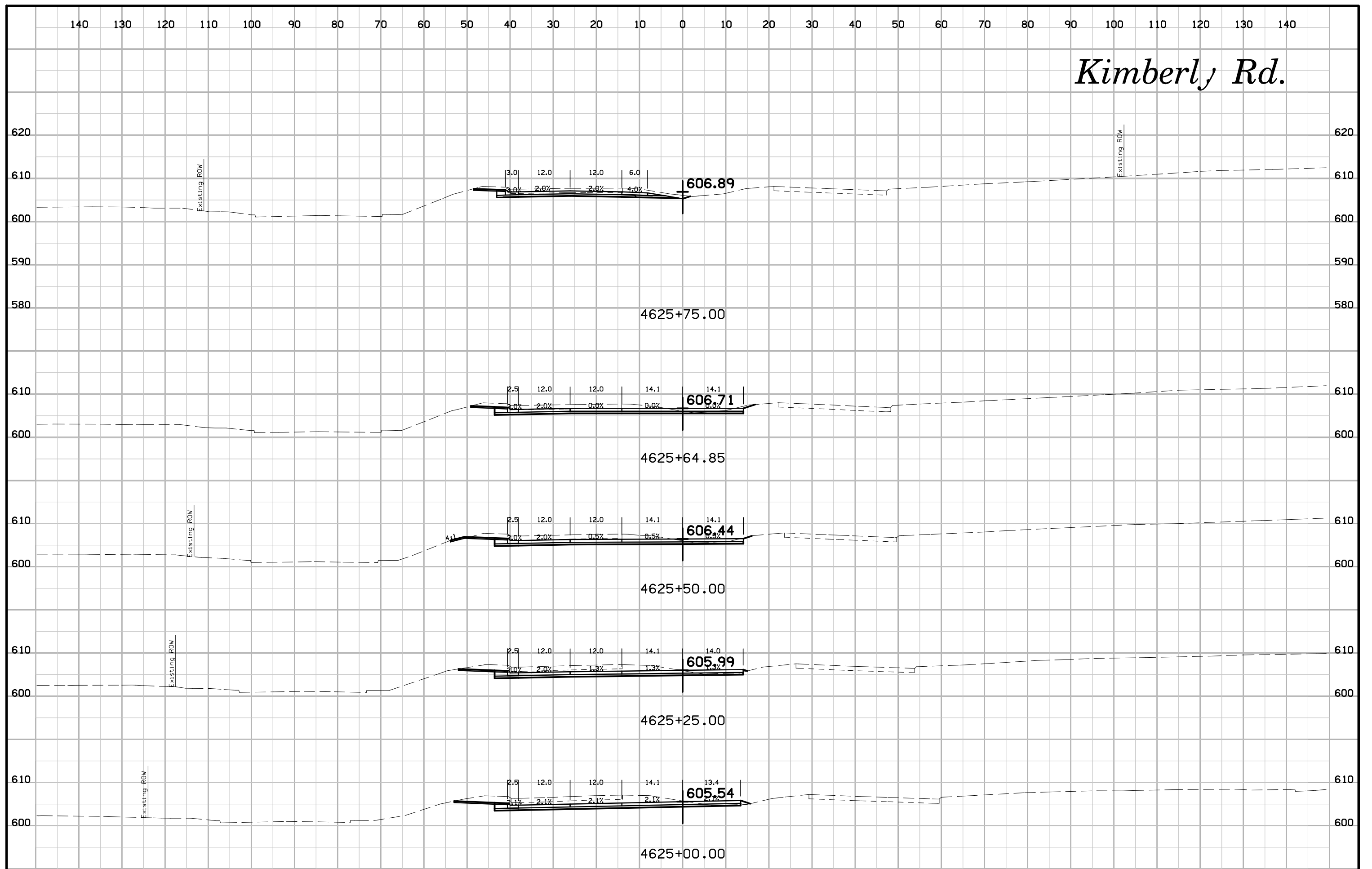
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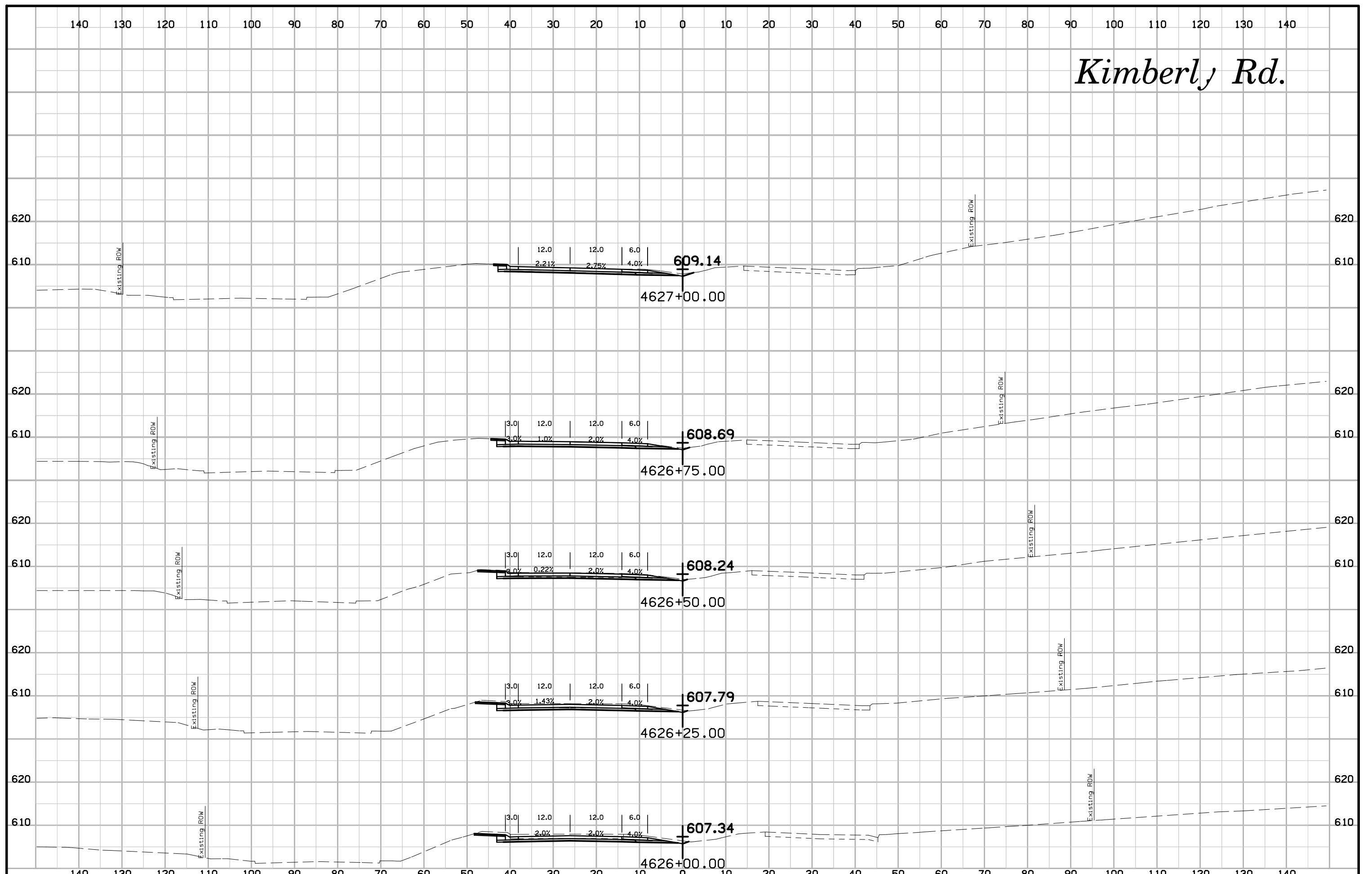
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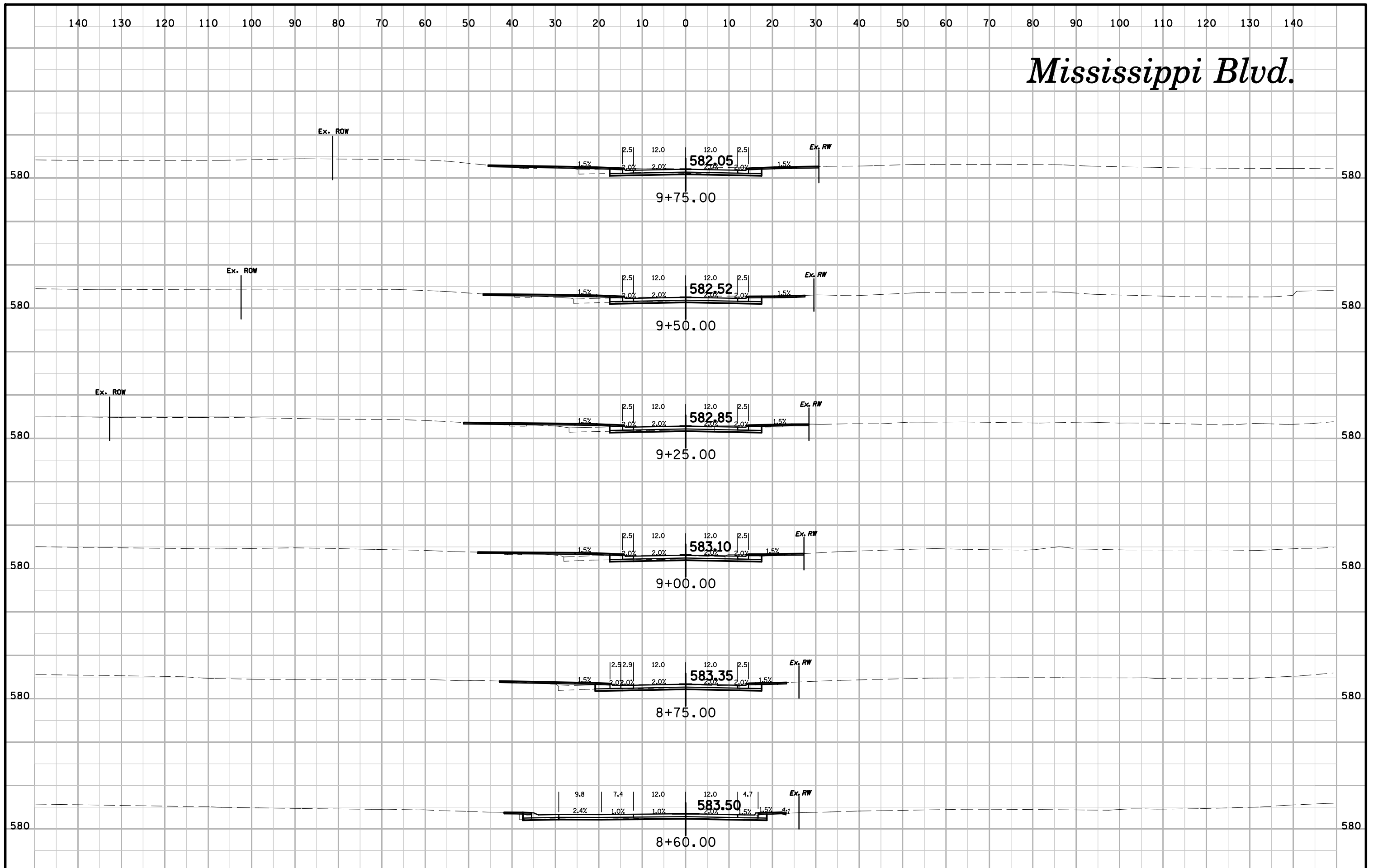
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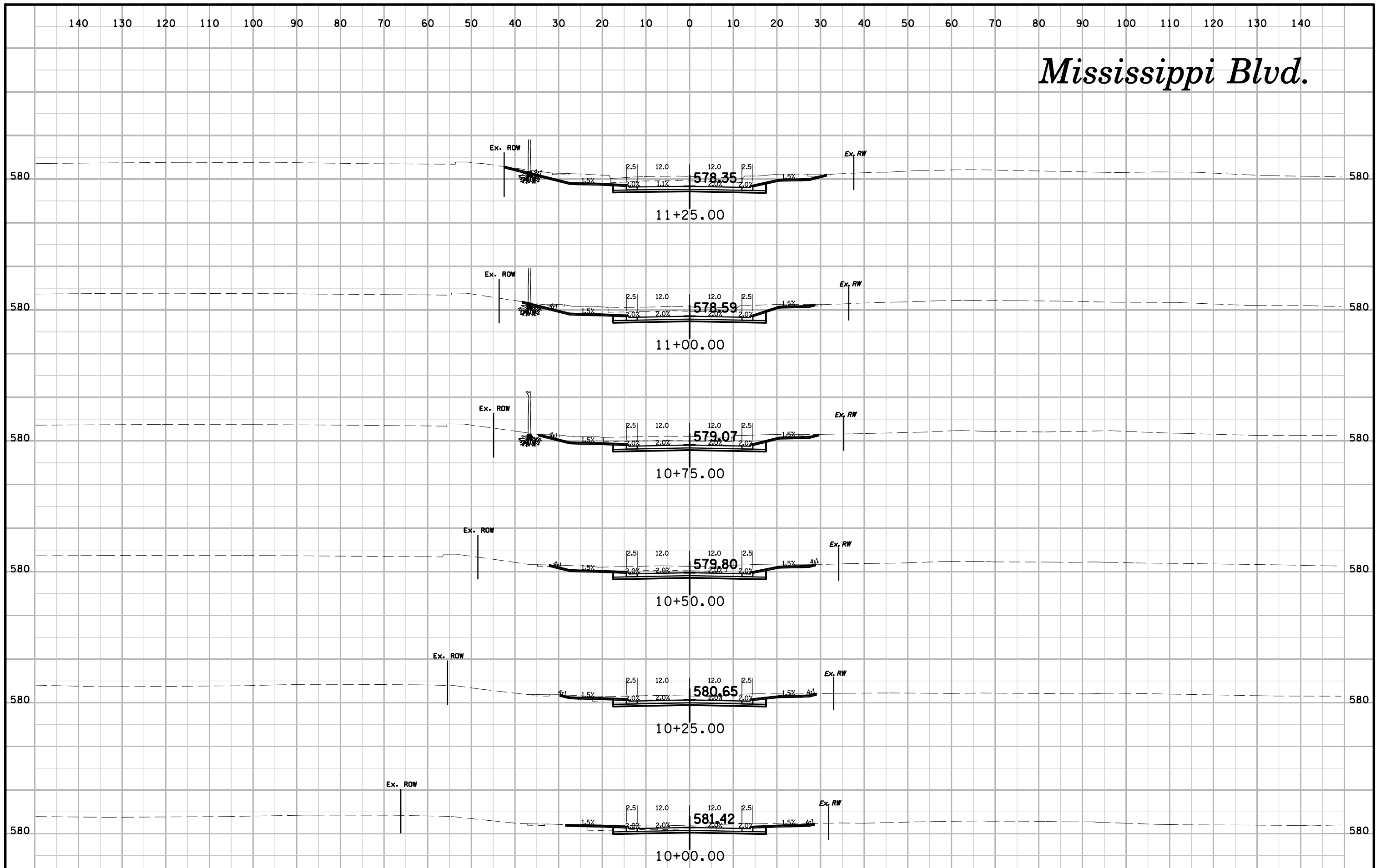
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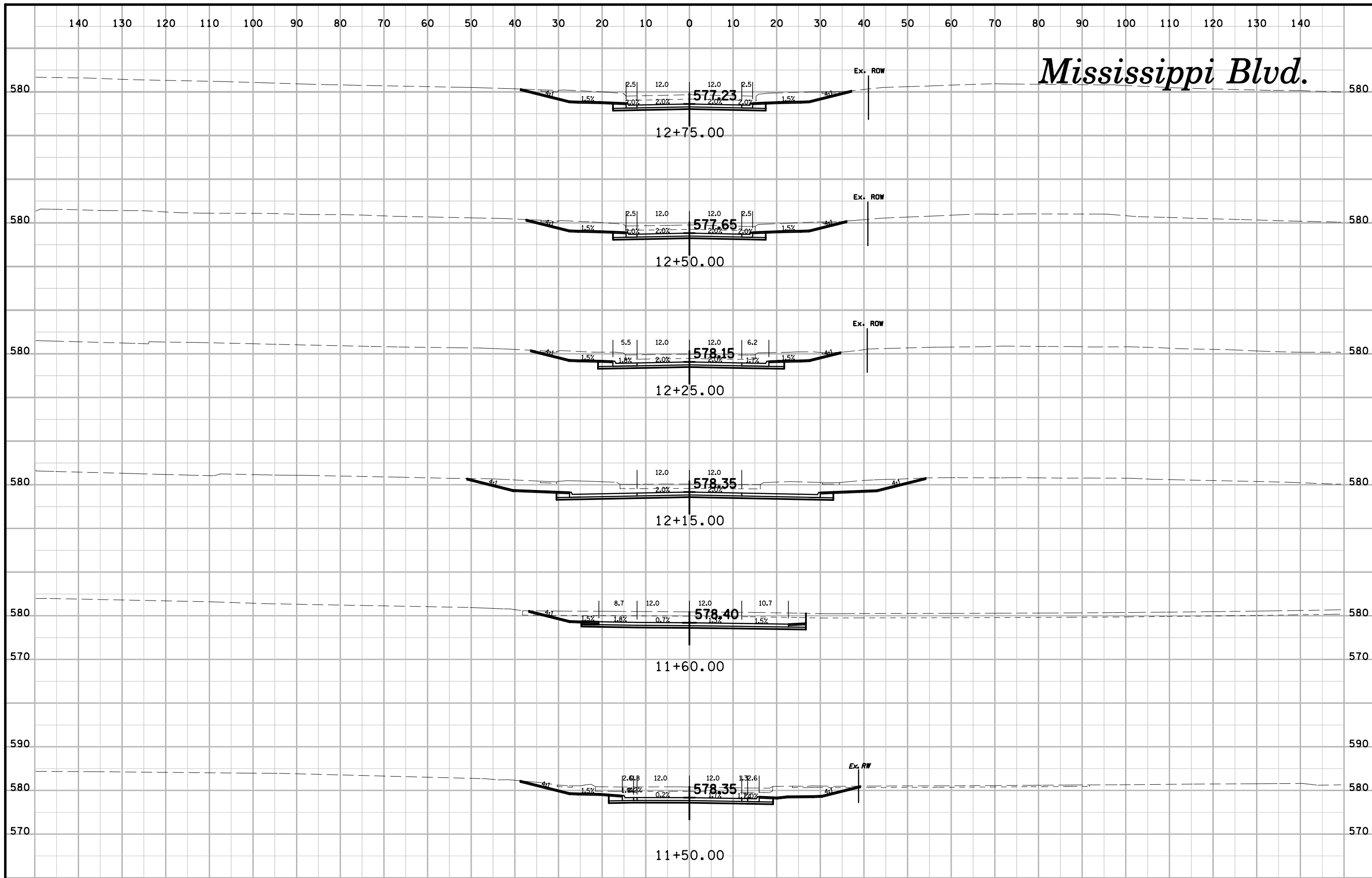
# Mississippi Blvd.



# Mississippi Blvd.

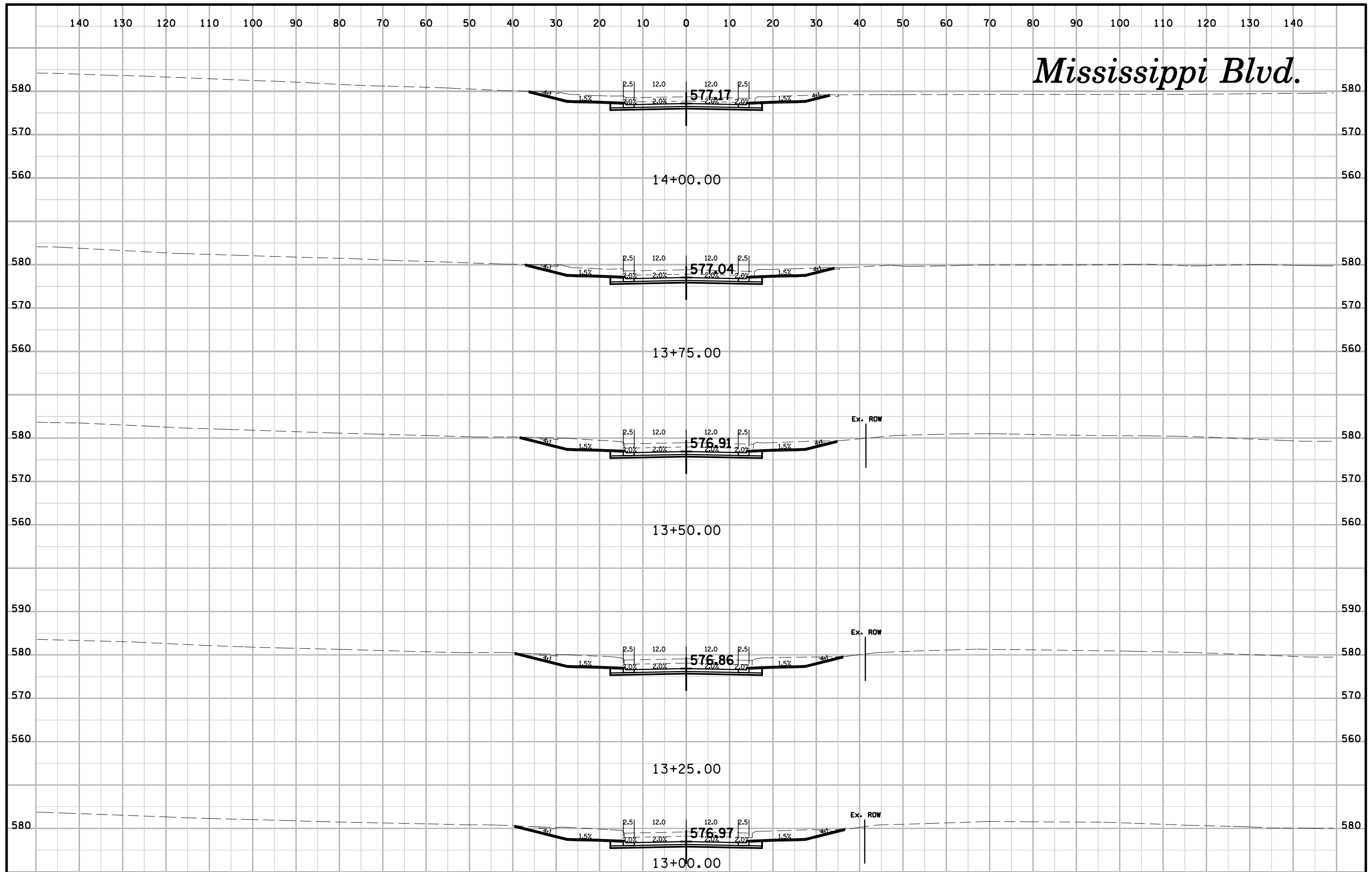




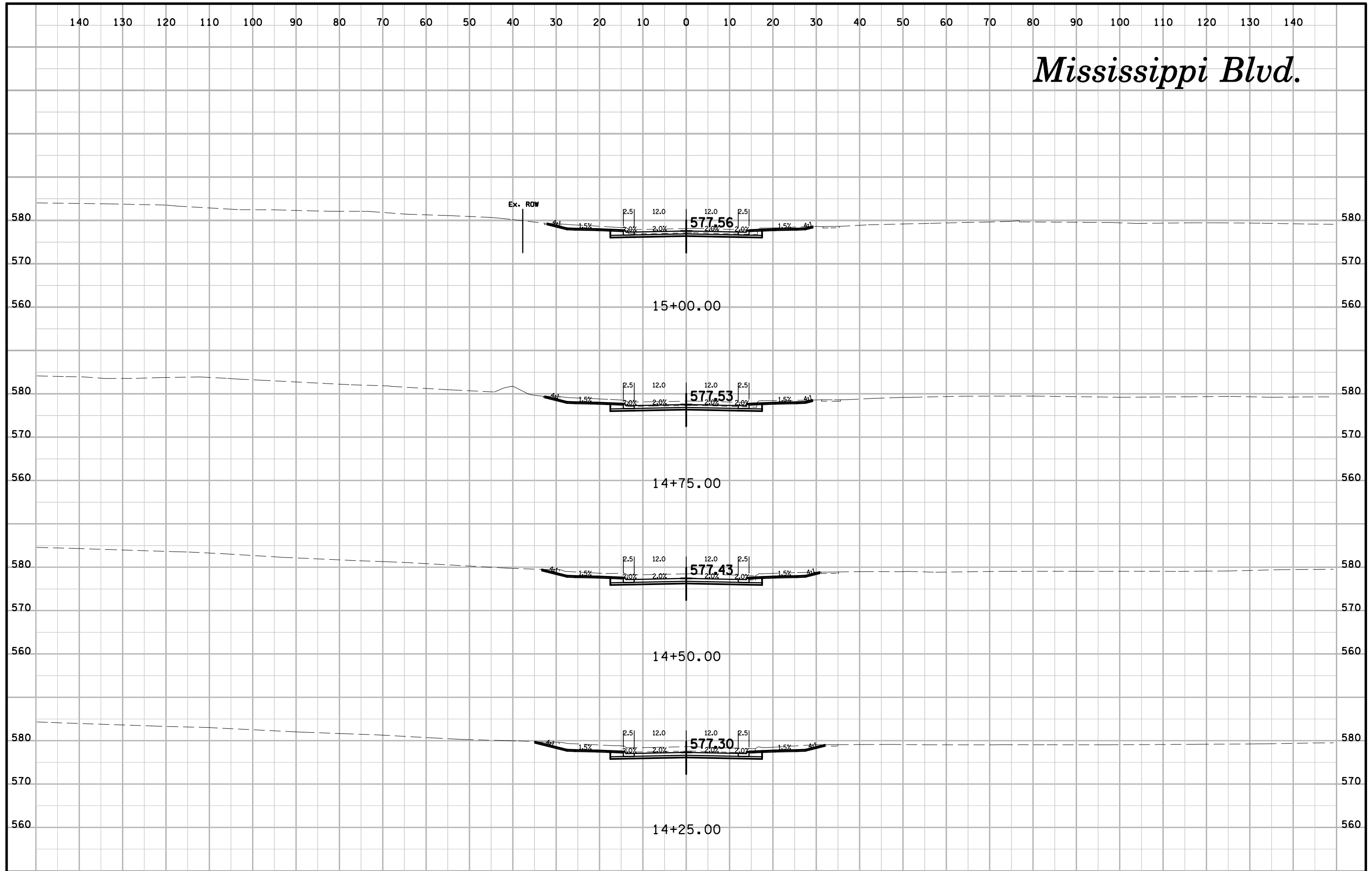


*Mississippi Blvd.*

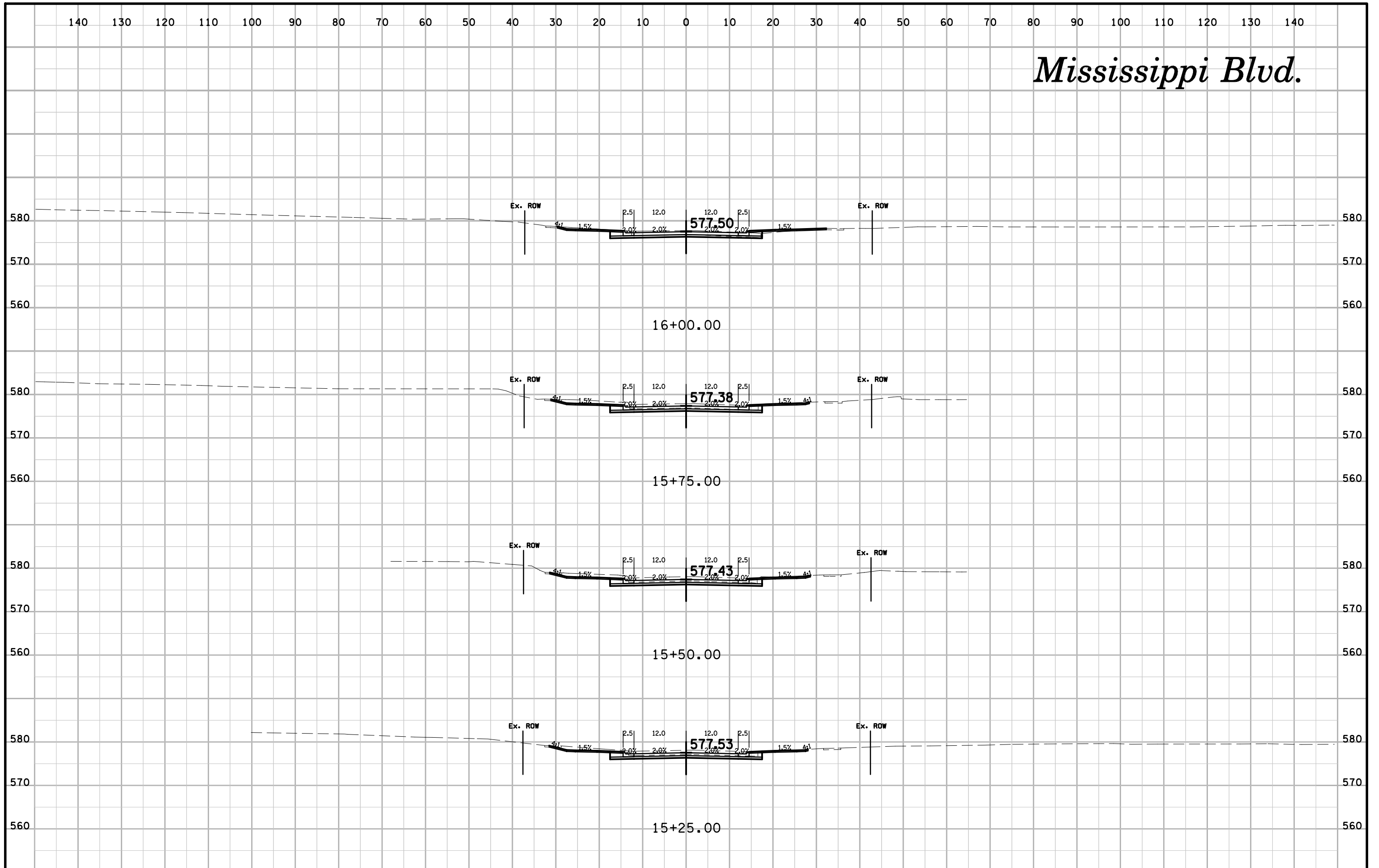
# Mississippi Blvd.



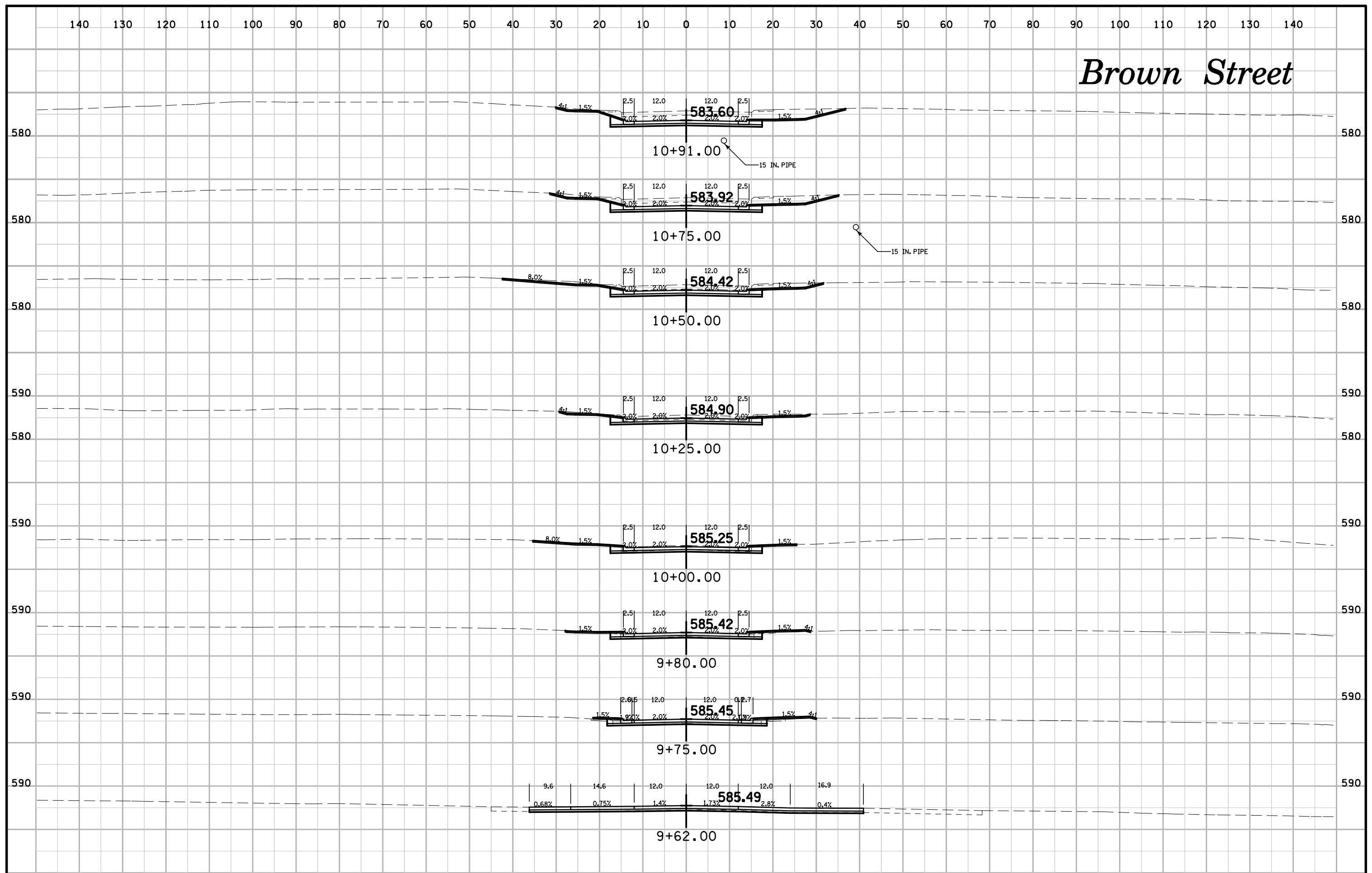
# Mississippi Blvd.



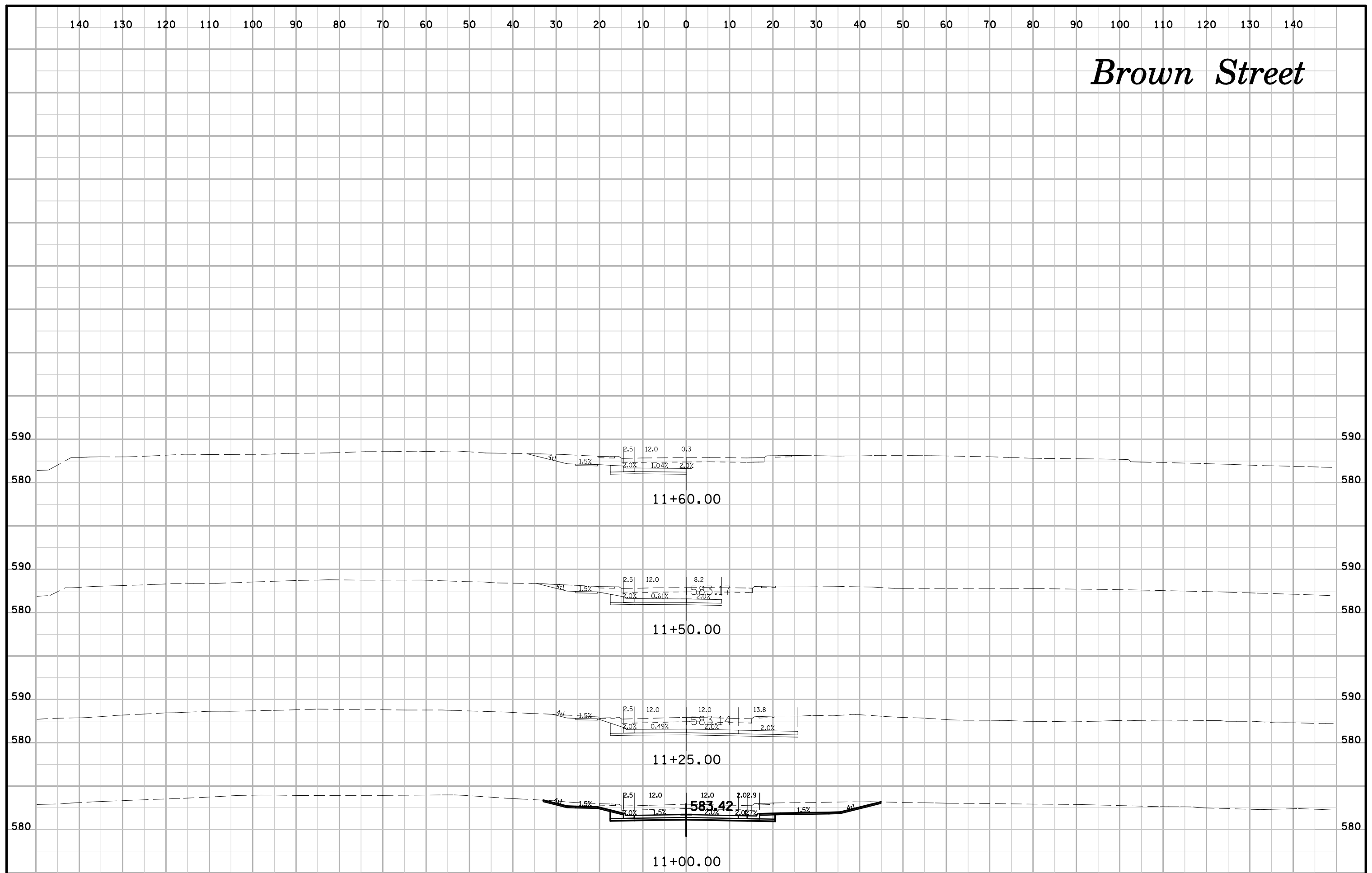
# Mississippi Blvd.



# Brown Street

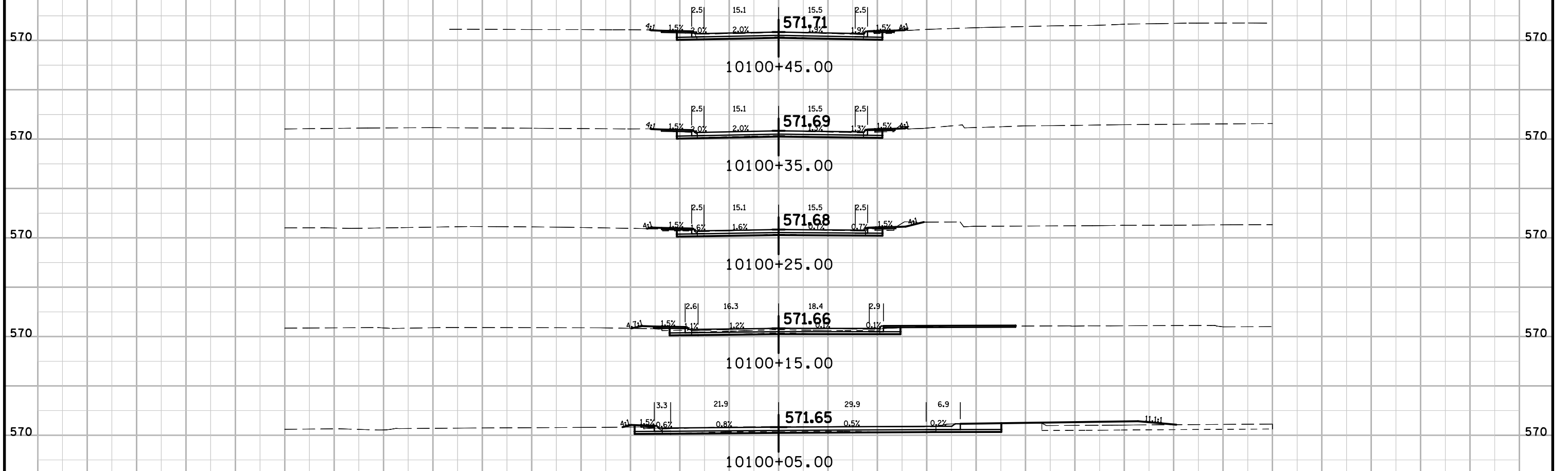


# Brown Street



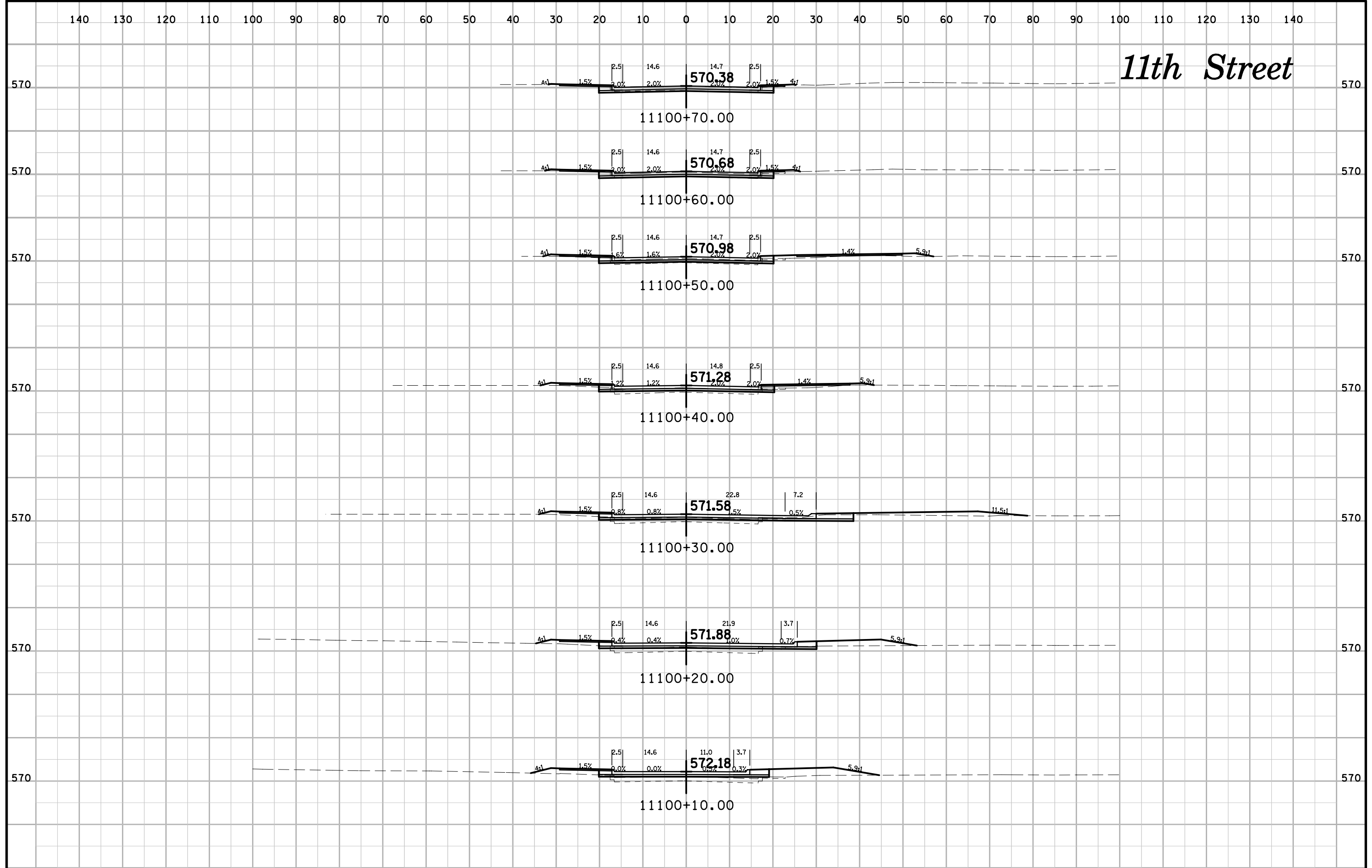
140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

# 10th Street



140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

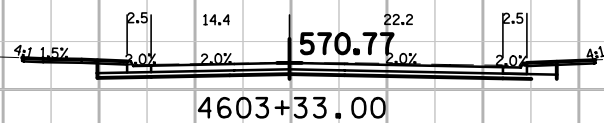
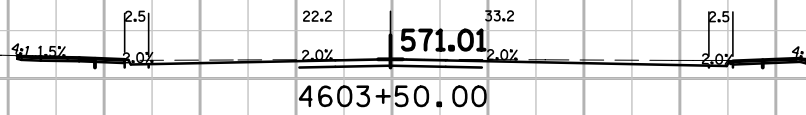
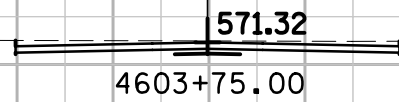
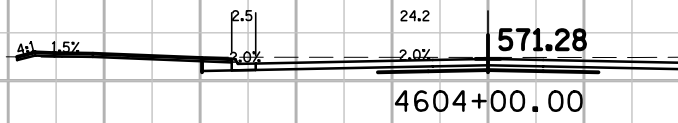
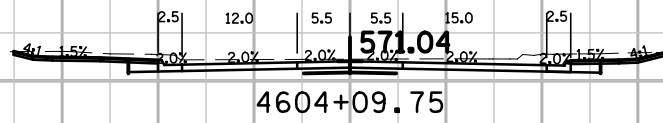
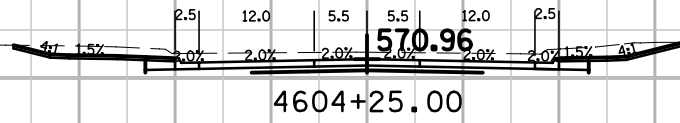
# 11th Street





140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

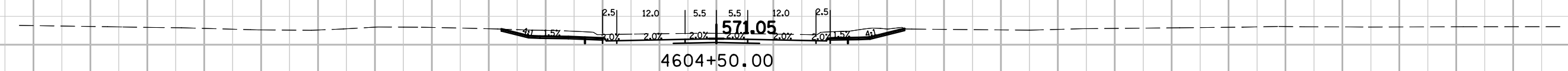
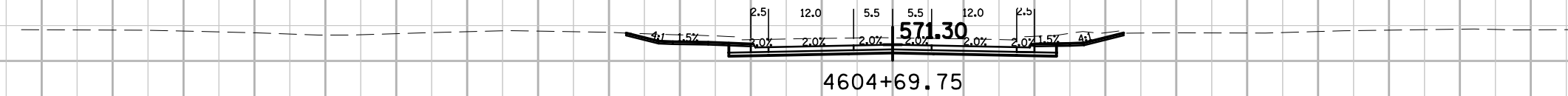
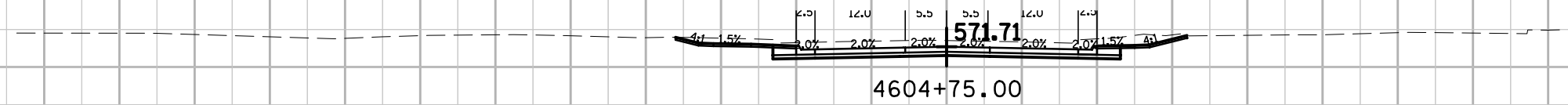
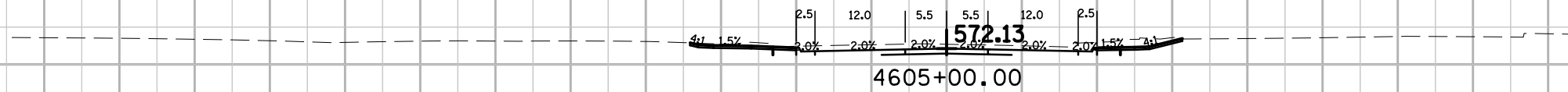
# 12th Street



140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

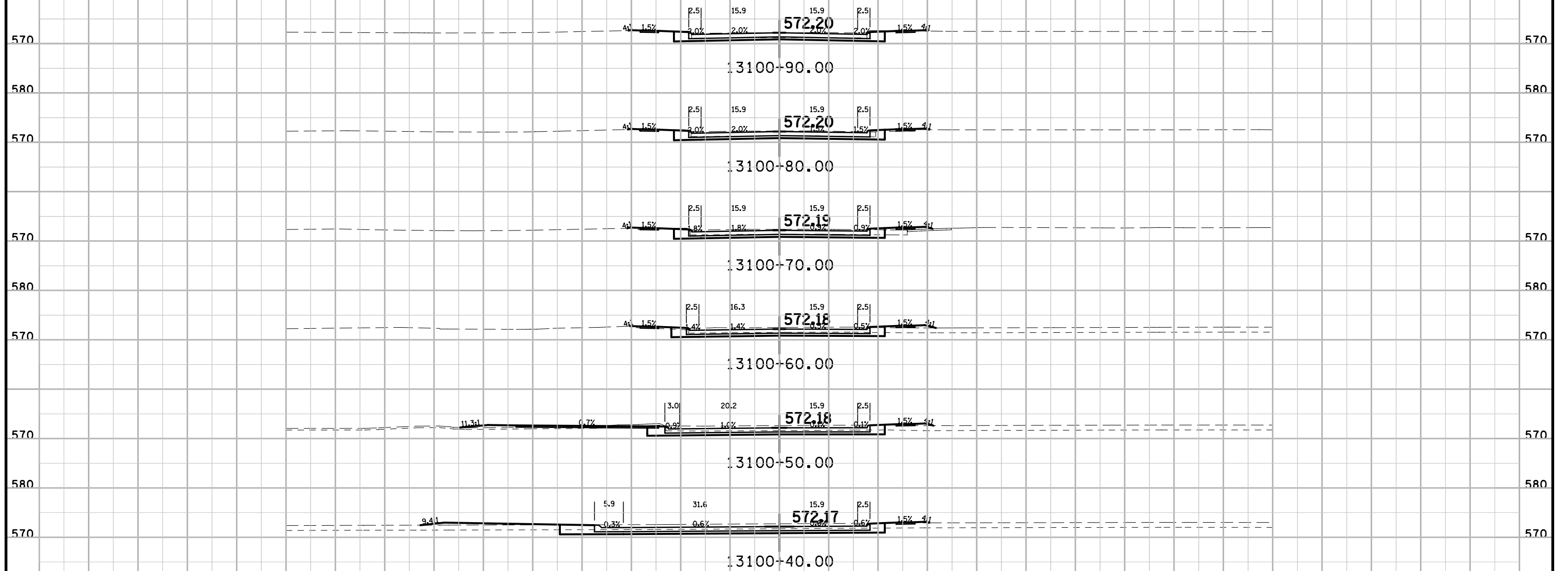
# 12th Street



140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

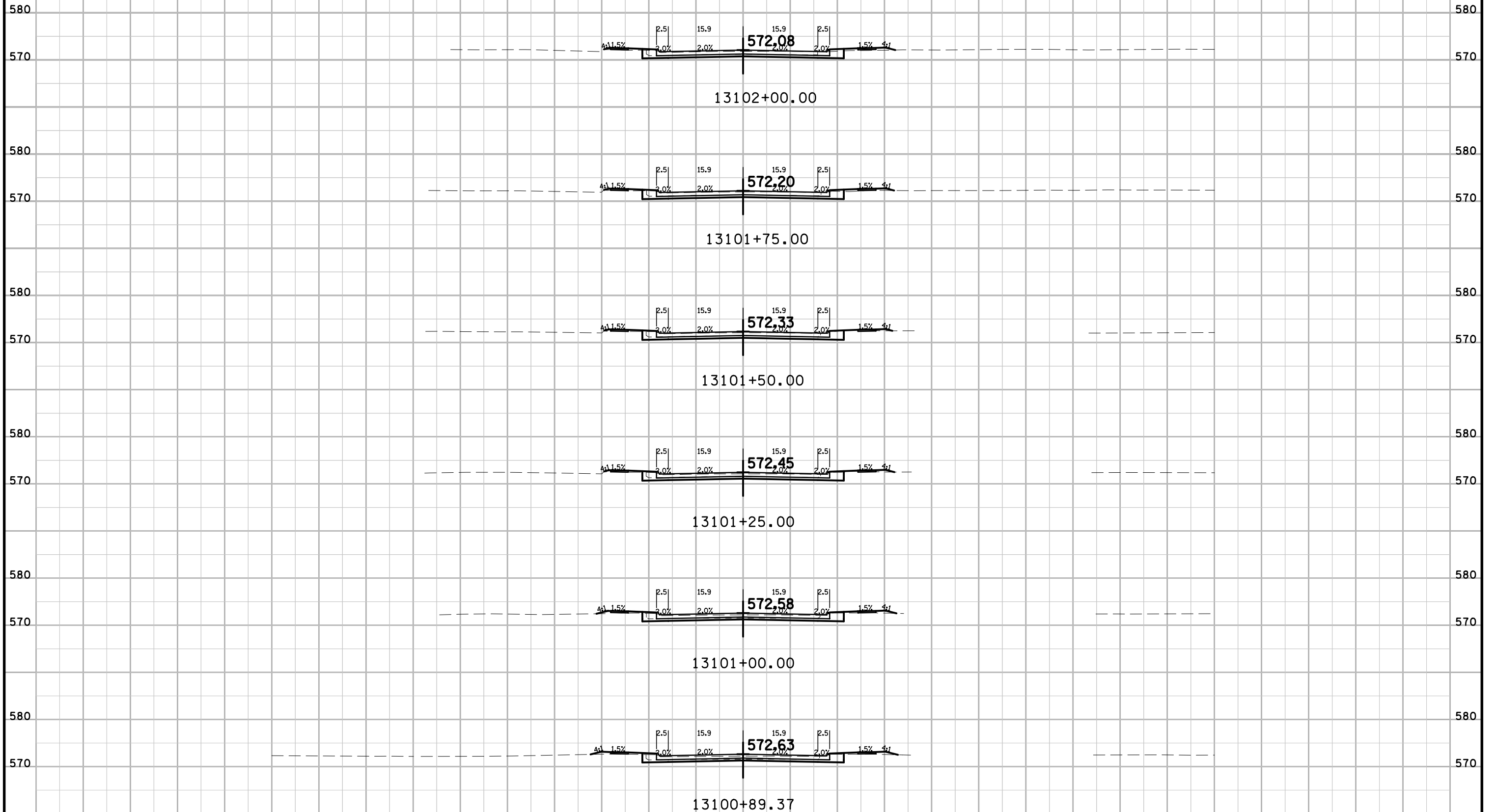
# 13th Street



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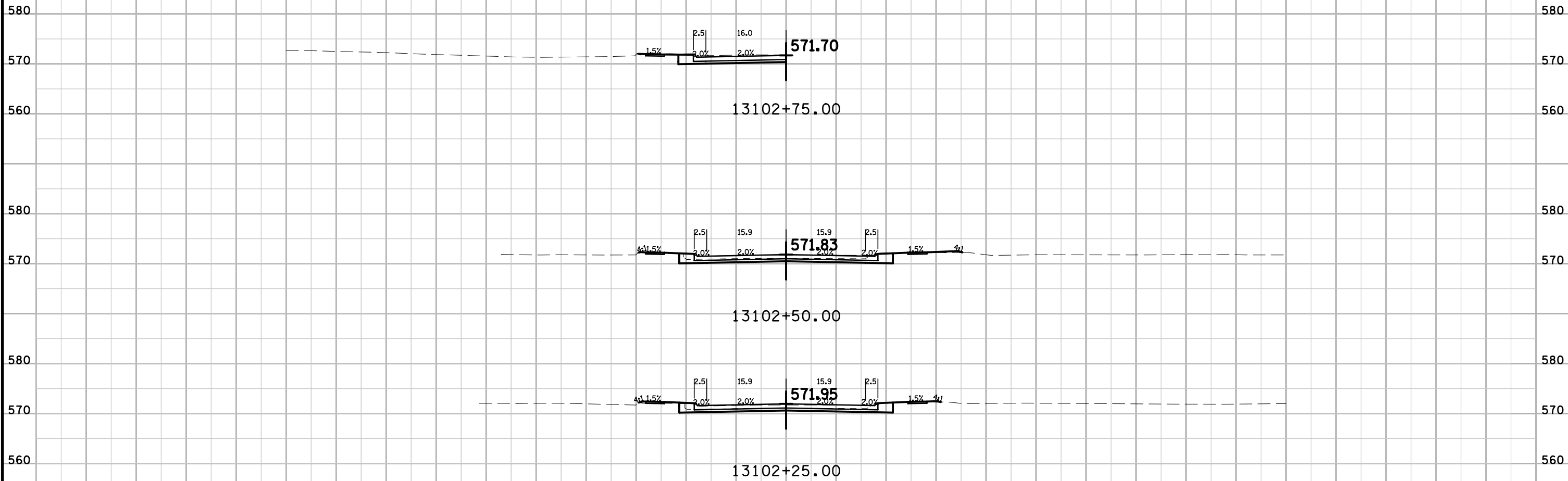
# 13th Street



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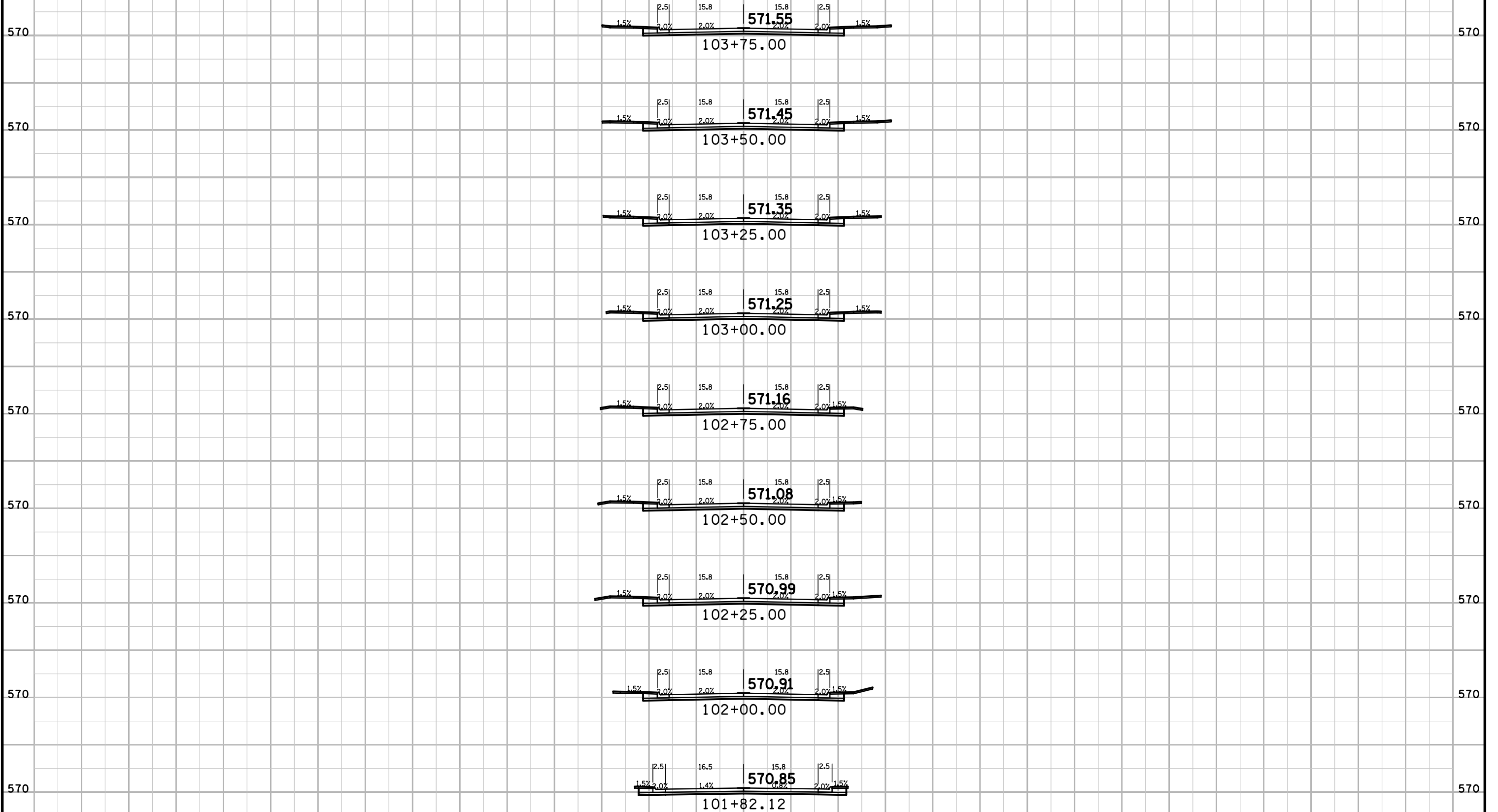
# 13th Street



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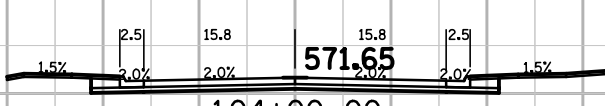
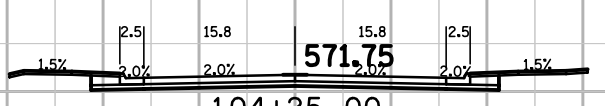
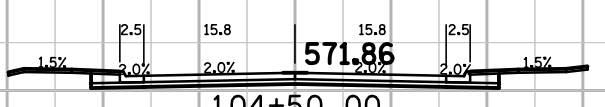
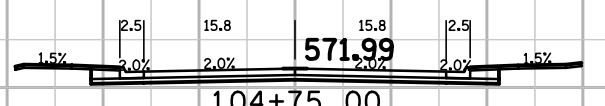
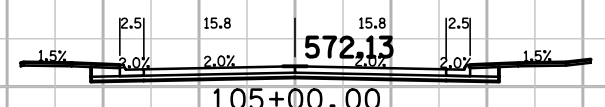
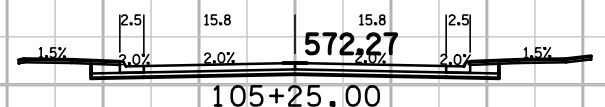
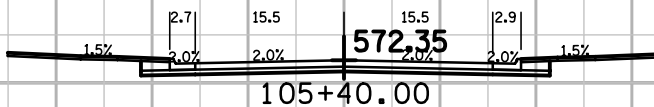
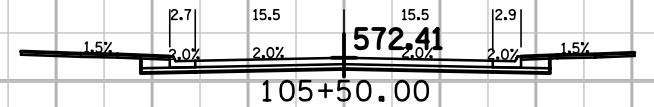
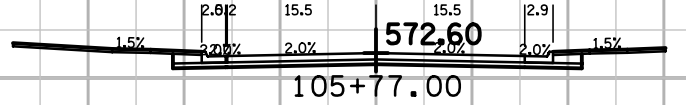
# 15th Street



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# 15th Street



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140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

ENGLISH

IOWA DOT

DESIGN TEAM **WHKS & Co.**

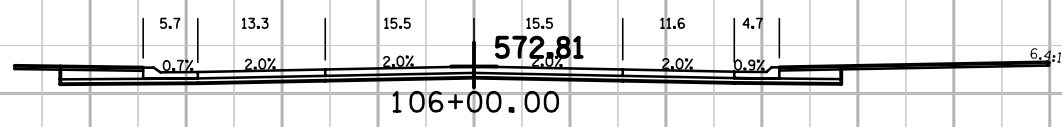
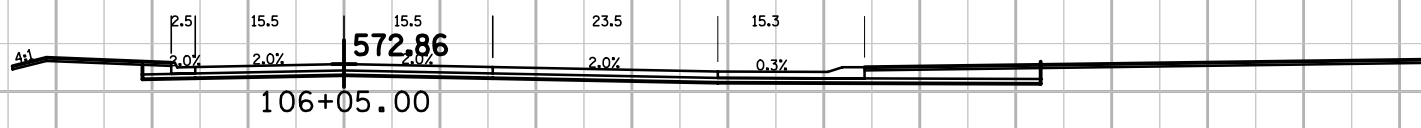
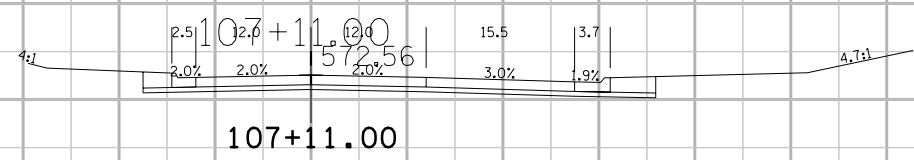
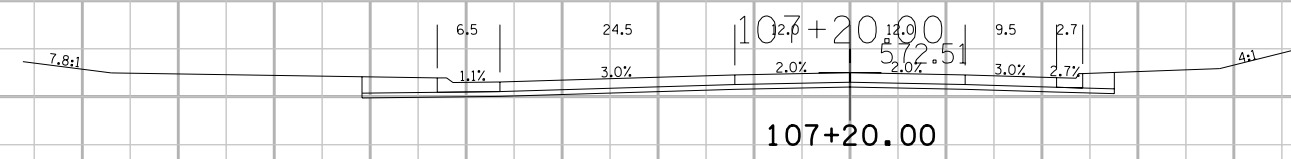
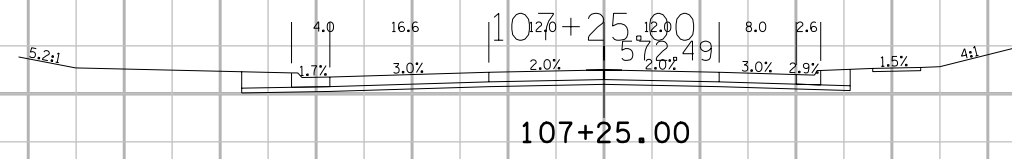
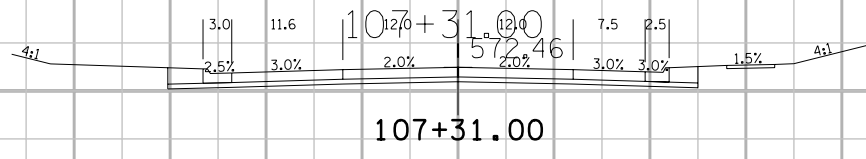
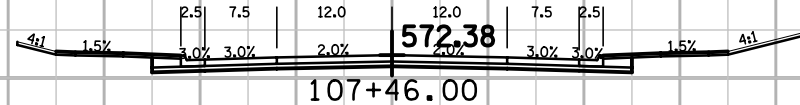
SCOTT COUNTY

PROJECT NUMBER **IM-74-1(226)4--13-82**

SHEET NUMBER

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# 15th Street



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140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

ENGLISH

IOWA DOT

DESIGN TEAM **WHKS & Co.**

SCOTT COUNTY

PROJECT NUMBER **IM-74-1(226)4--13-82**

SHEET NUMBER

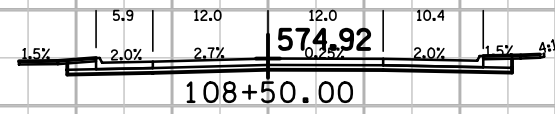


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# 15th Street

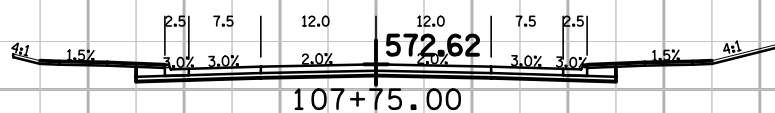
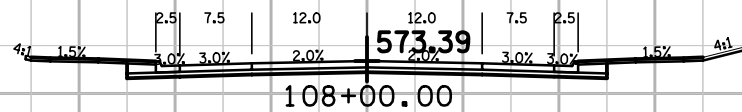
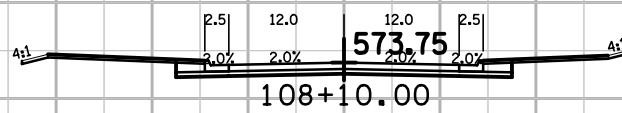
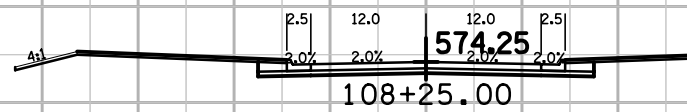
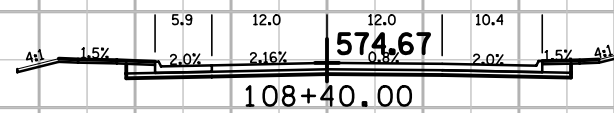
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ENGLISH

IOWA DOT

DESIGN TEAM **WHKS & Co.**

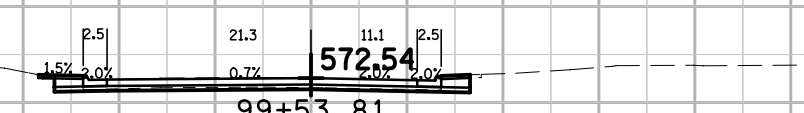
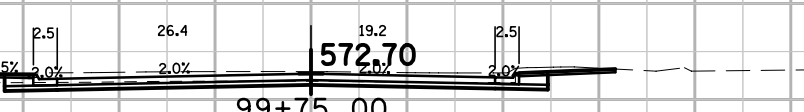
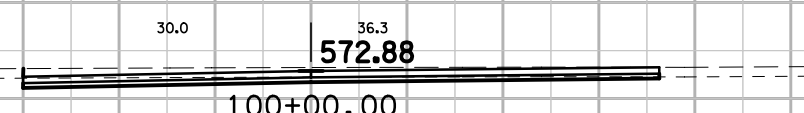
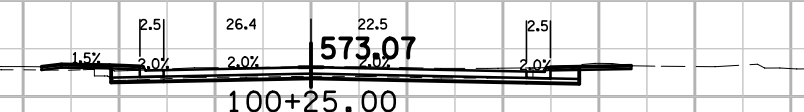
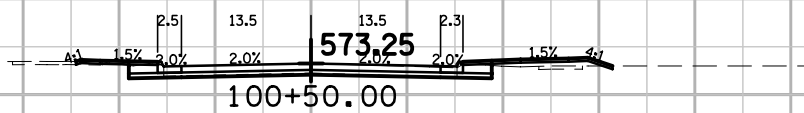
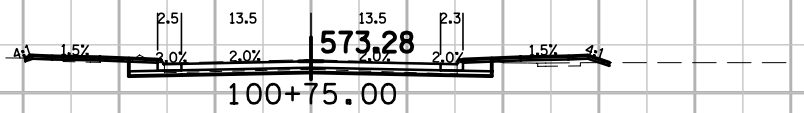
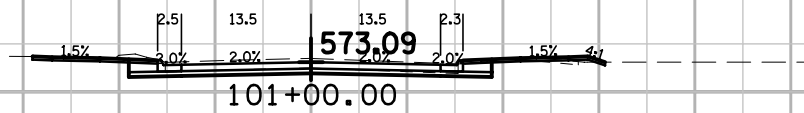
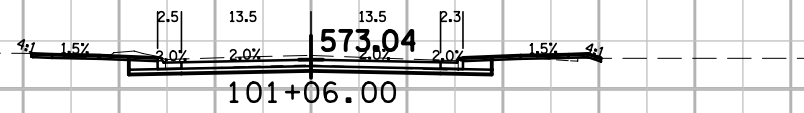
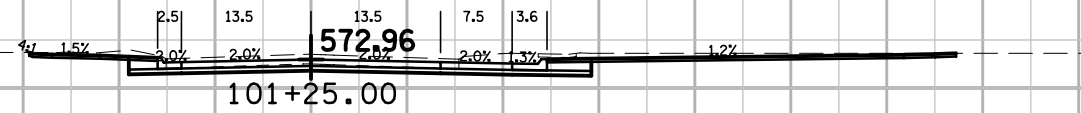
SCOTT COUNTY

PROJECT NUMBER **IM-74-1(226)4--13-82**

SHEET NUMBER

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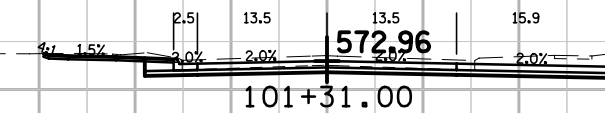
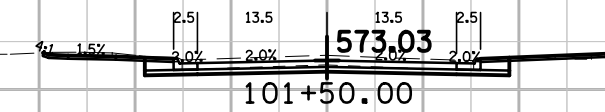
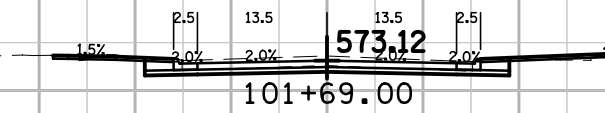
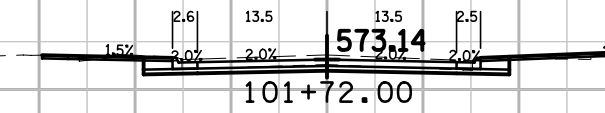
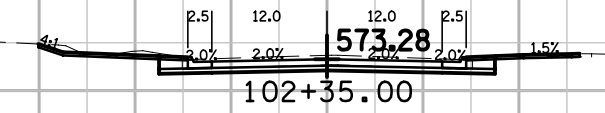
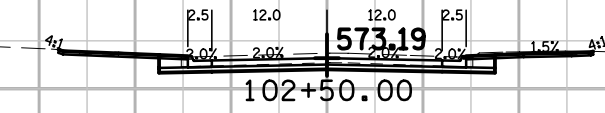
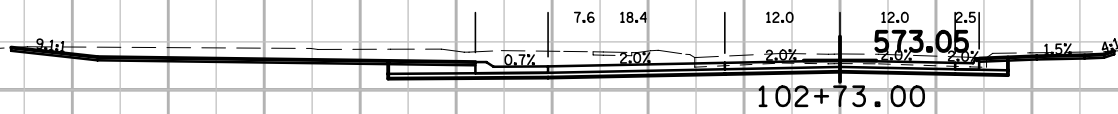
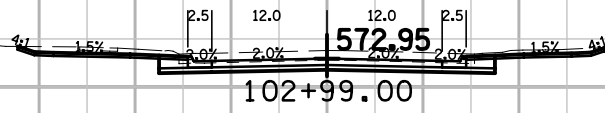
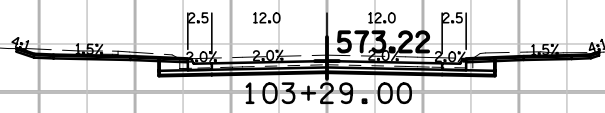
# 16th Street



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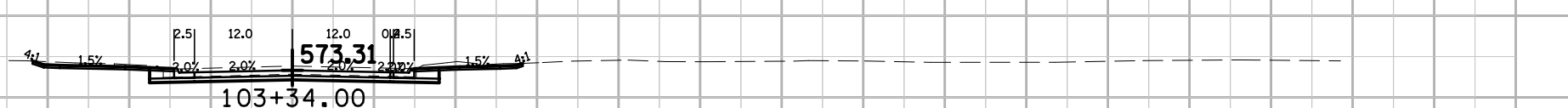
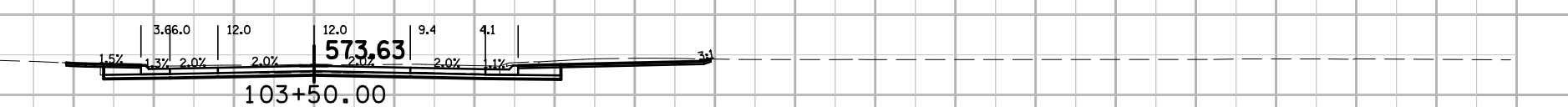
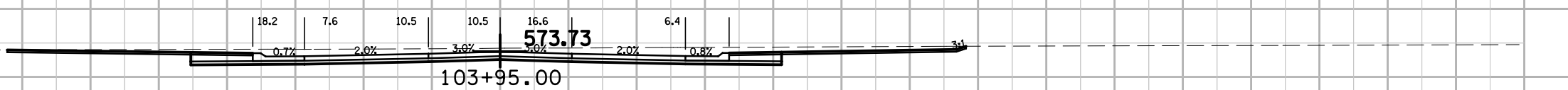
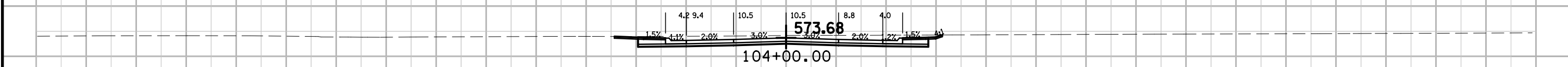
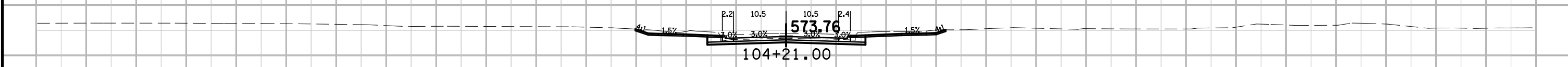
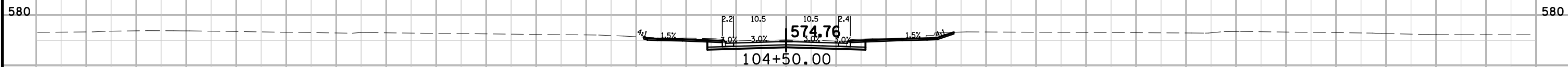
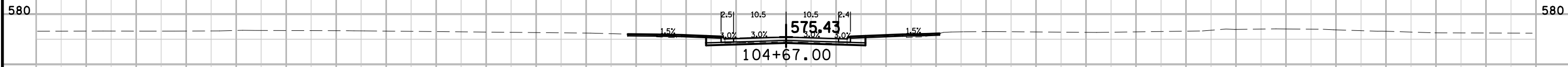
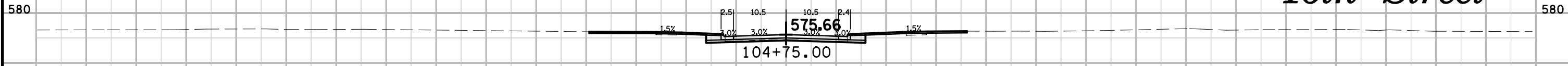
# 16th Street



140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

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# 16th Street



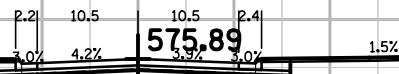
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# 16th Street

580

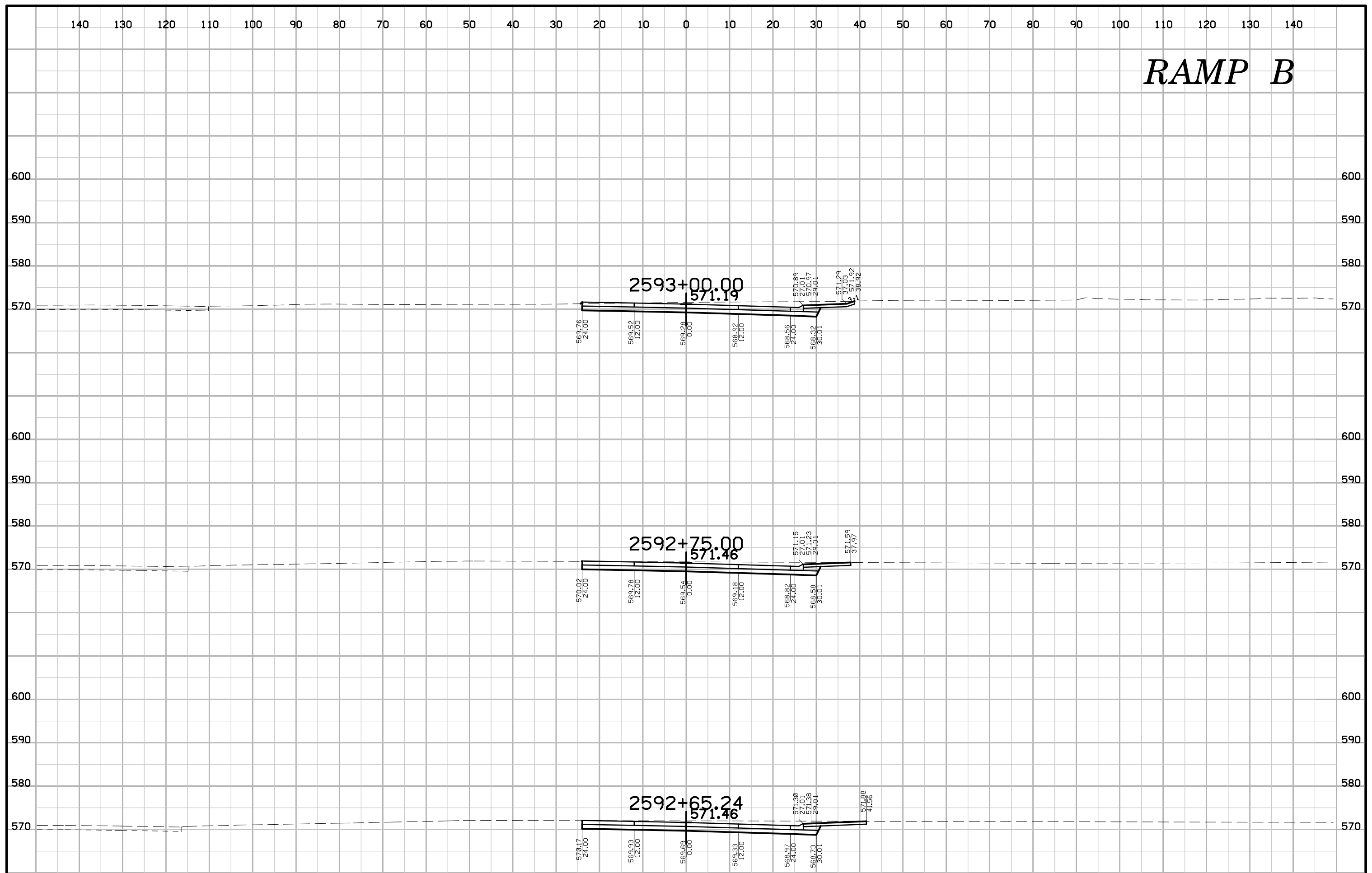
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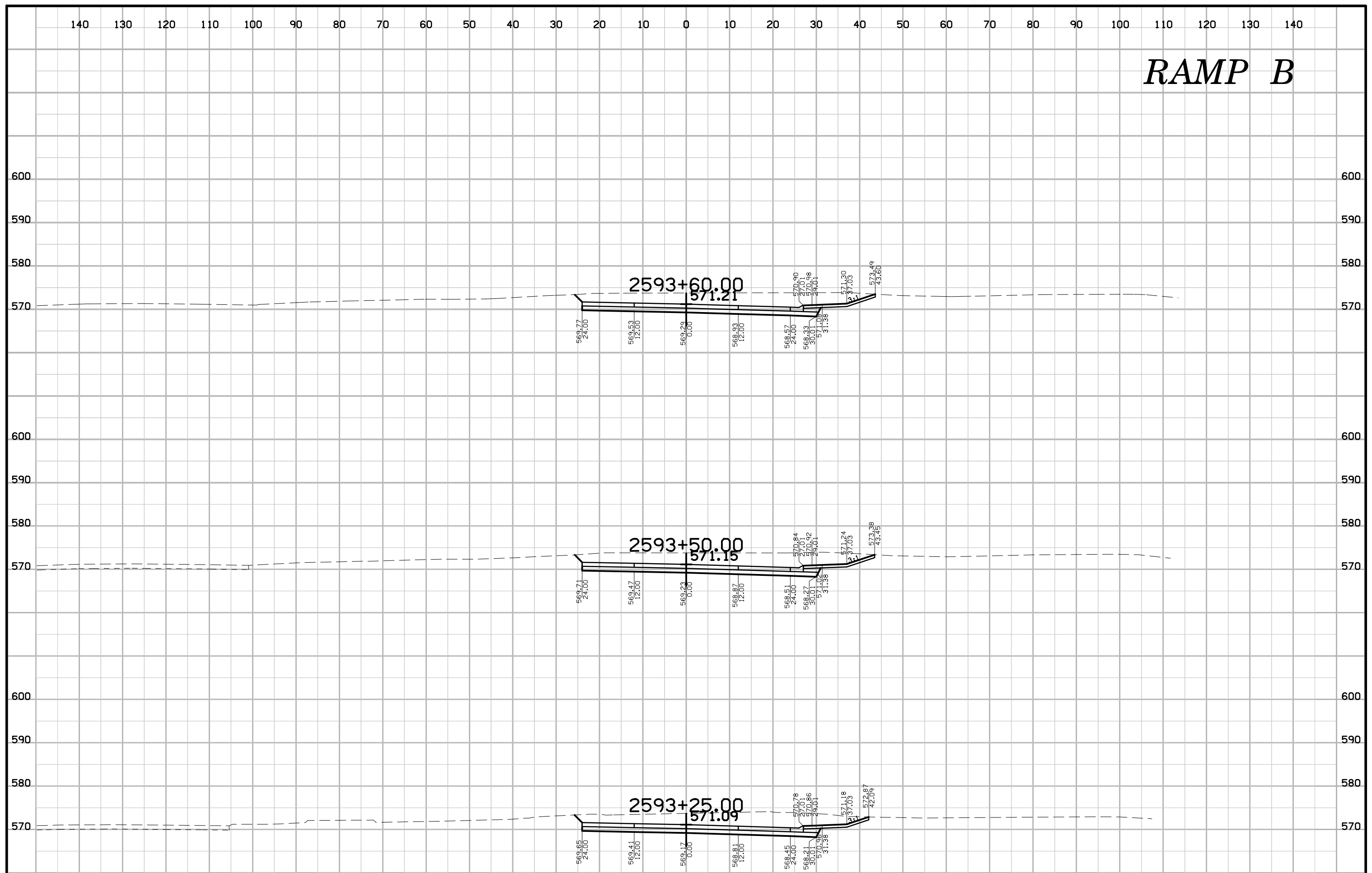
104+85.00

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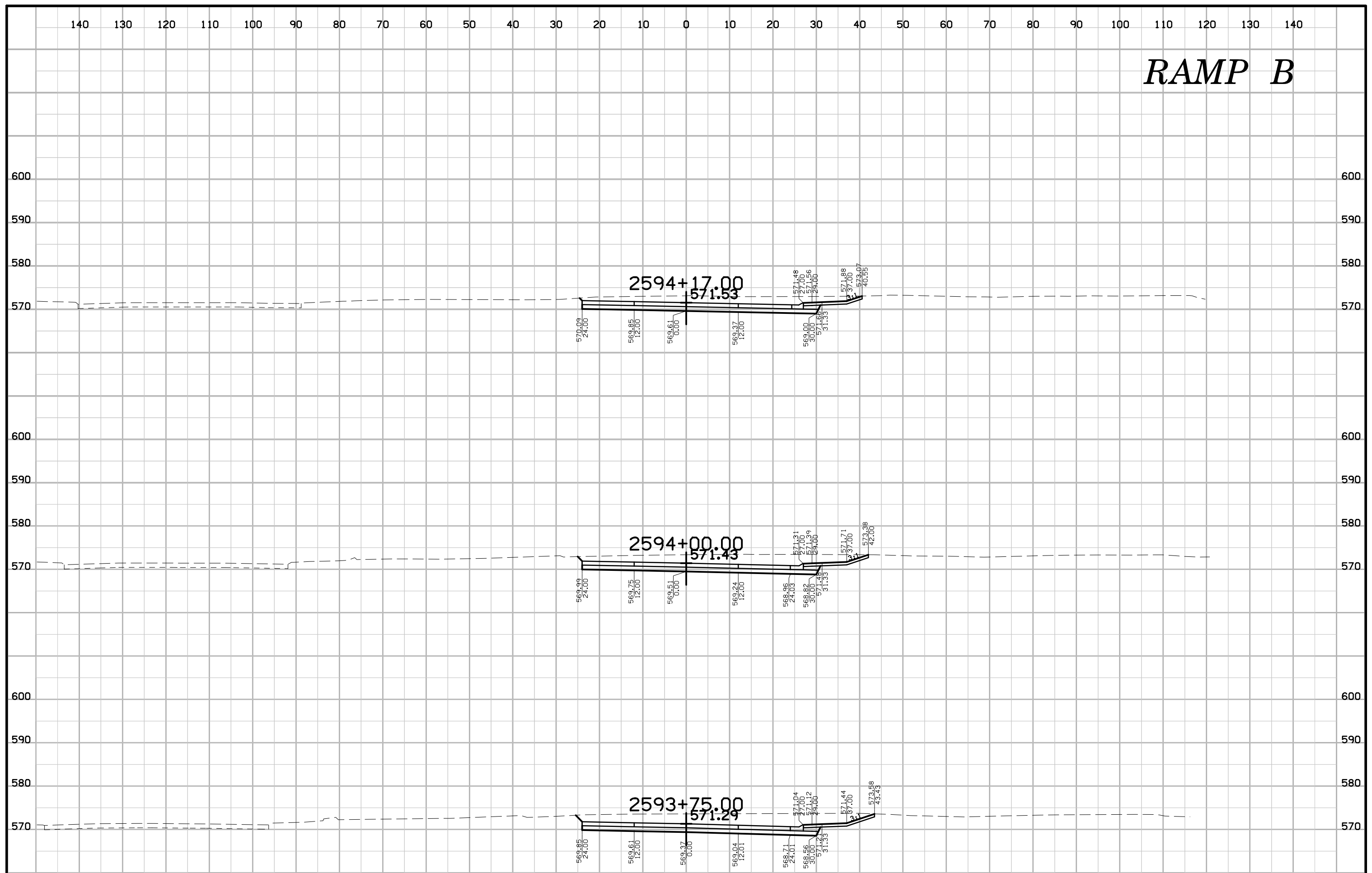
# RAMP B



# RAMP B

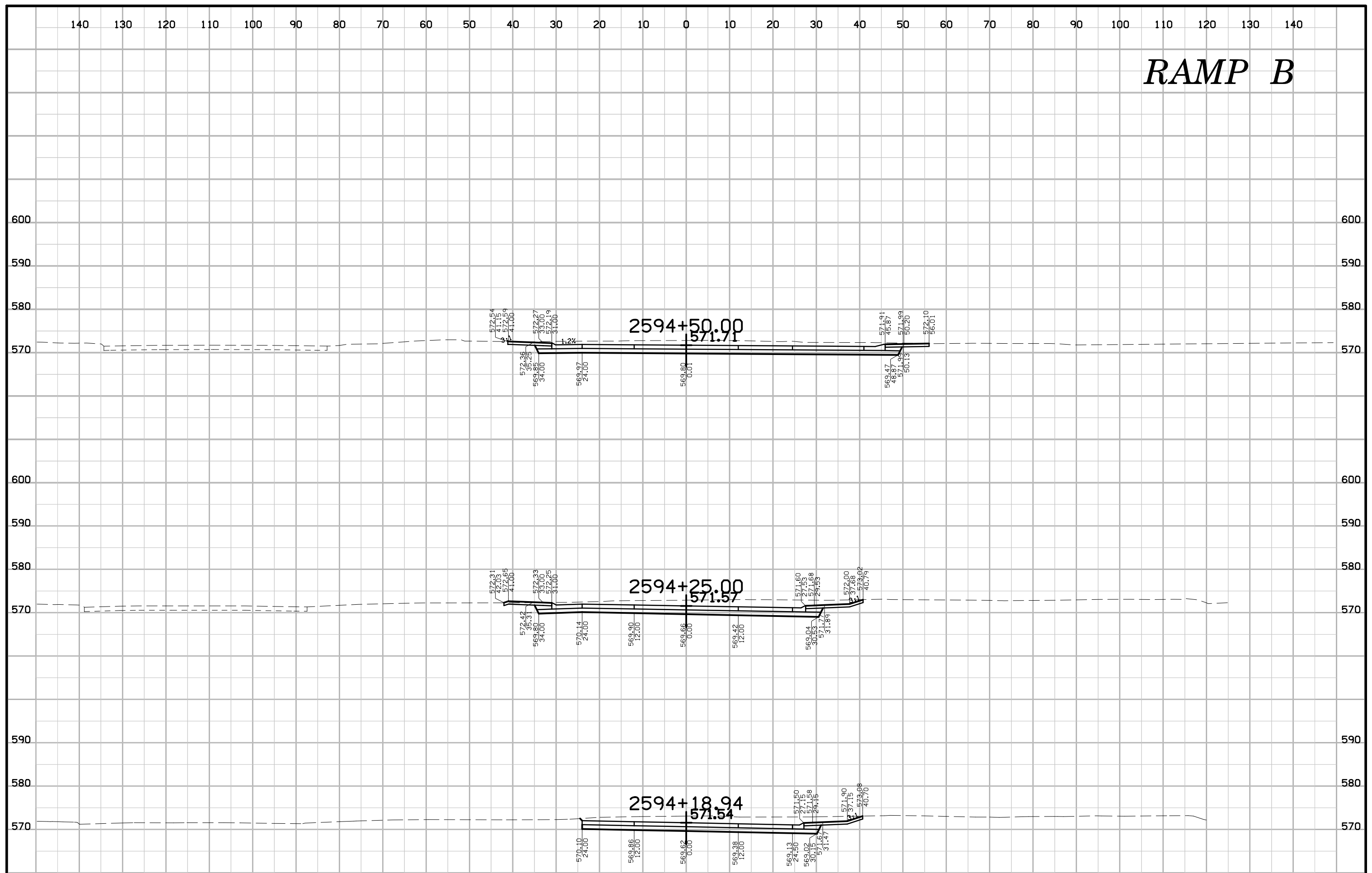


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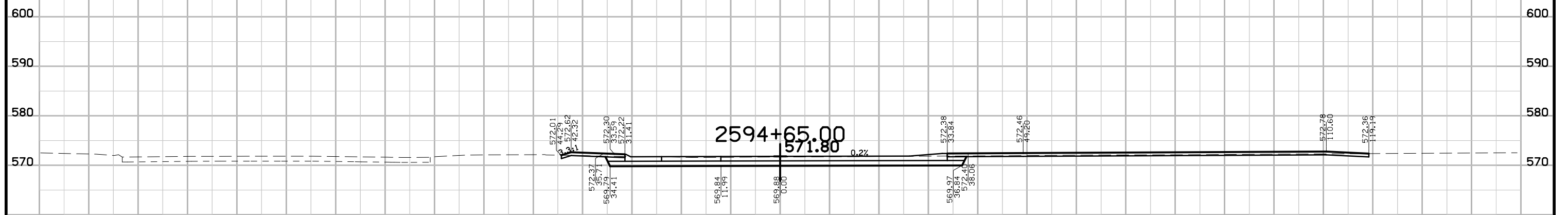


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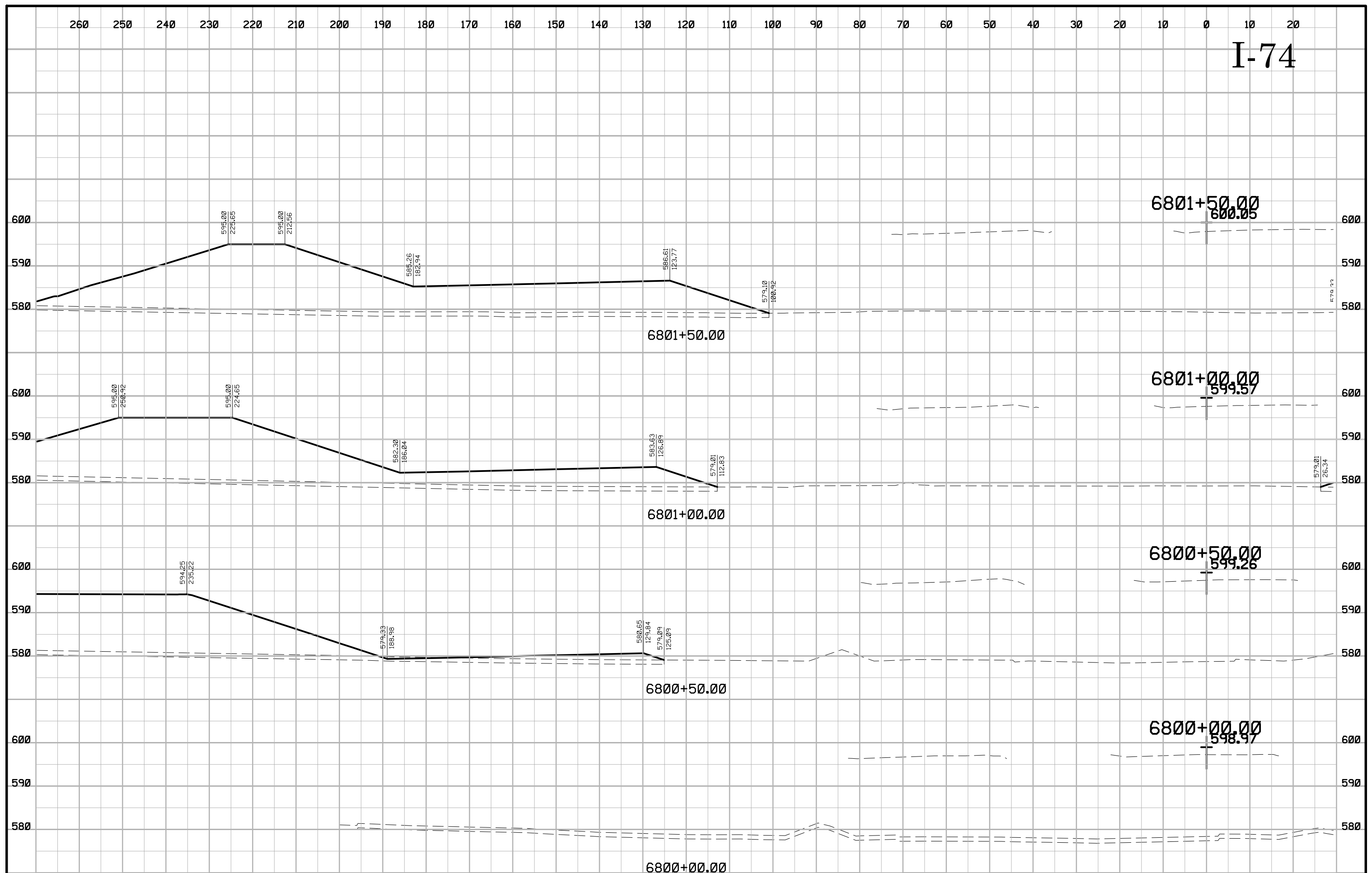


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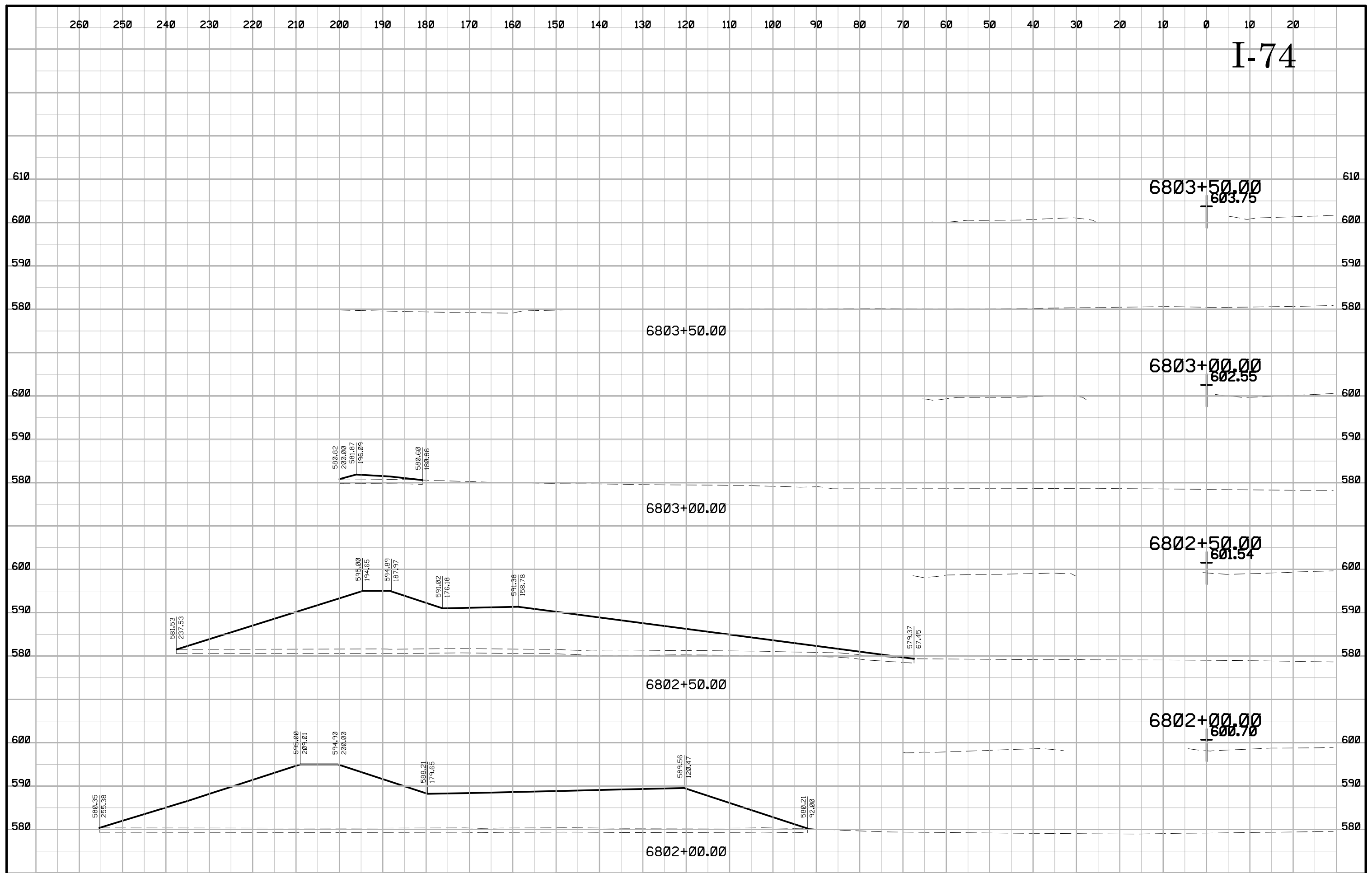
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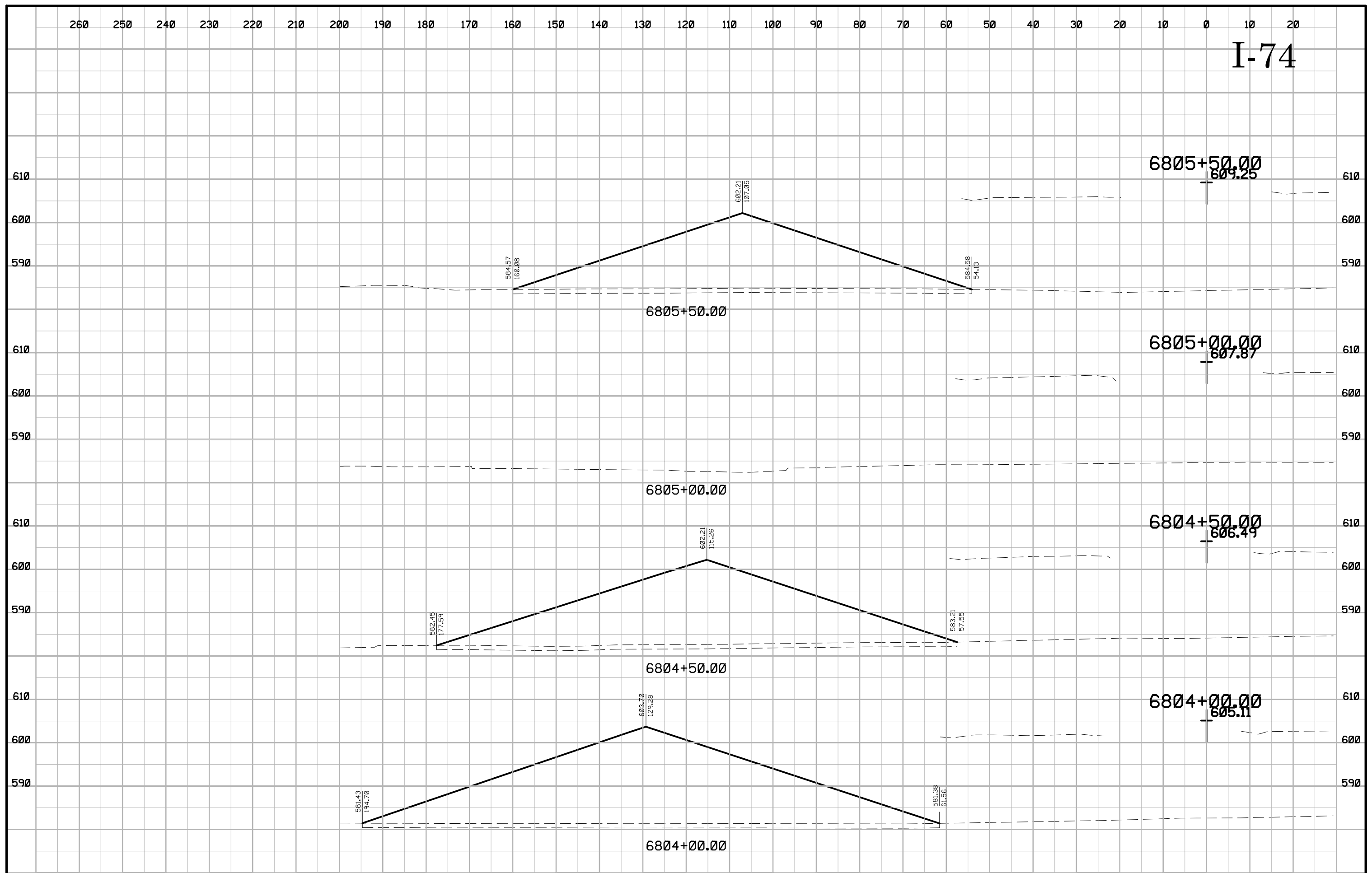
# I-74



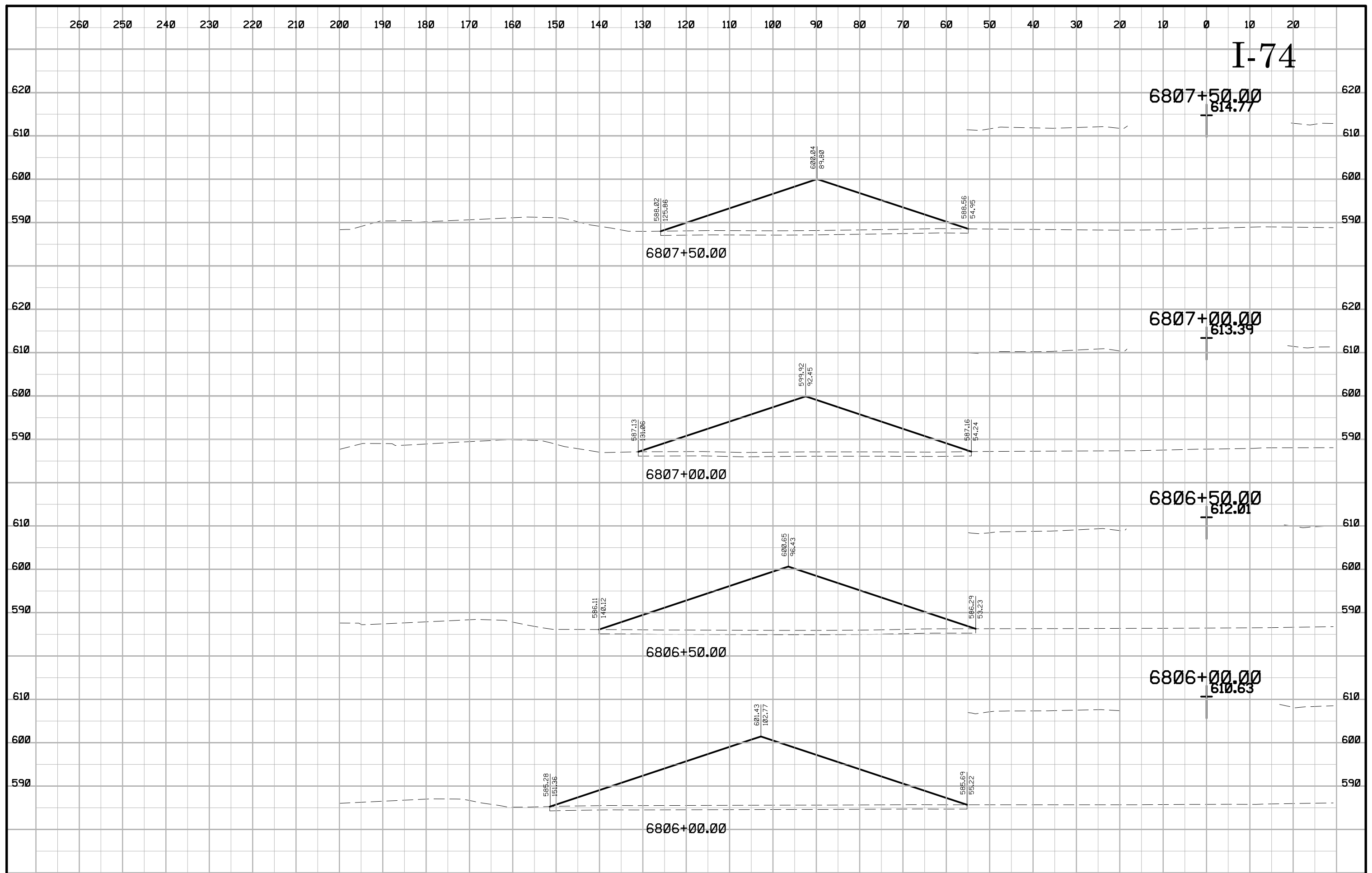
# I-74



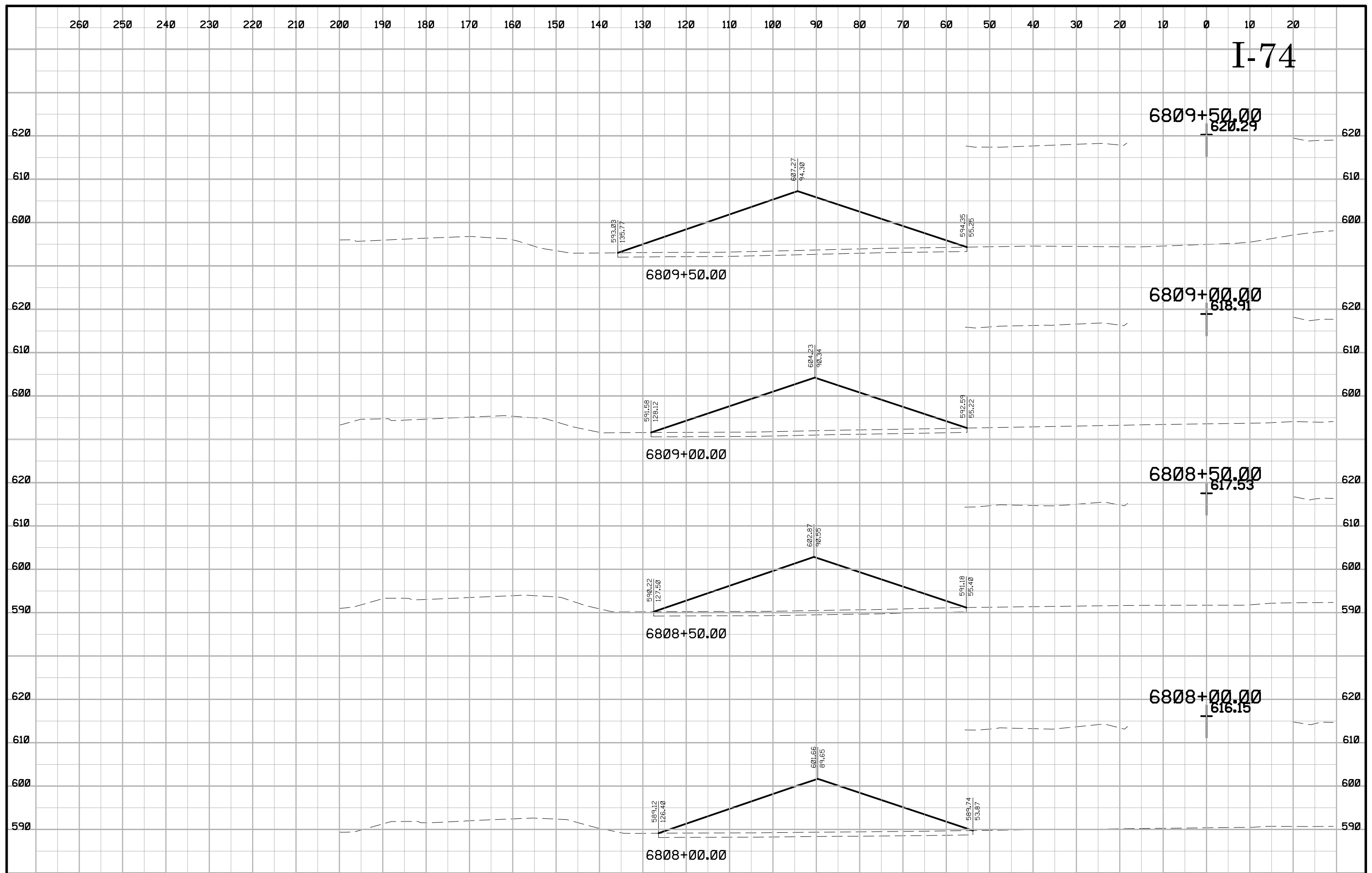
# I-74



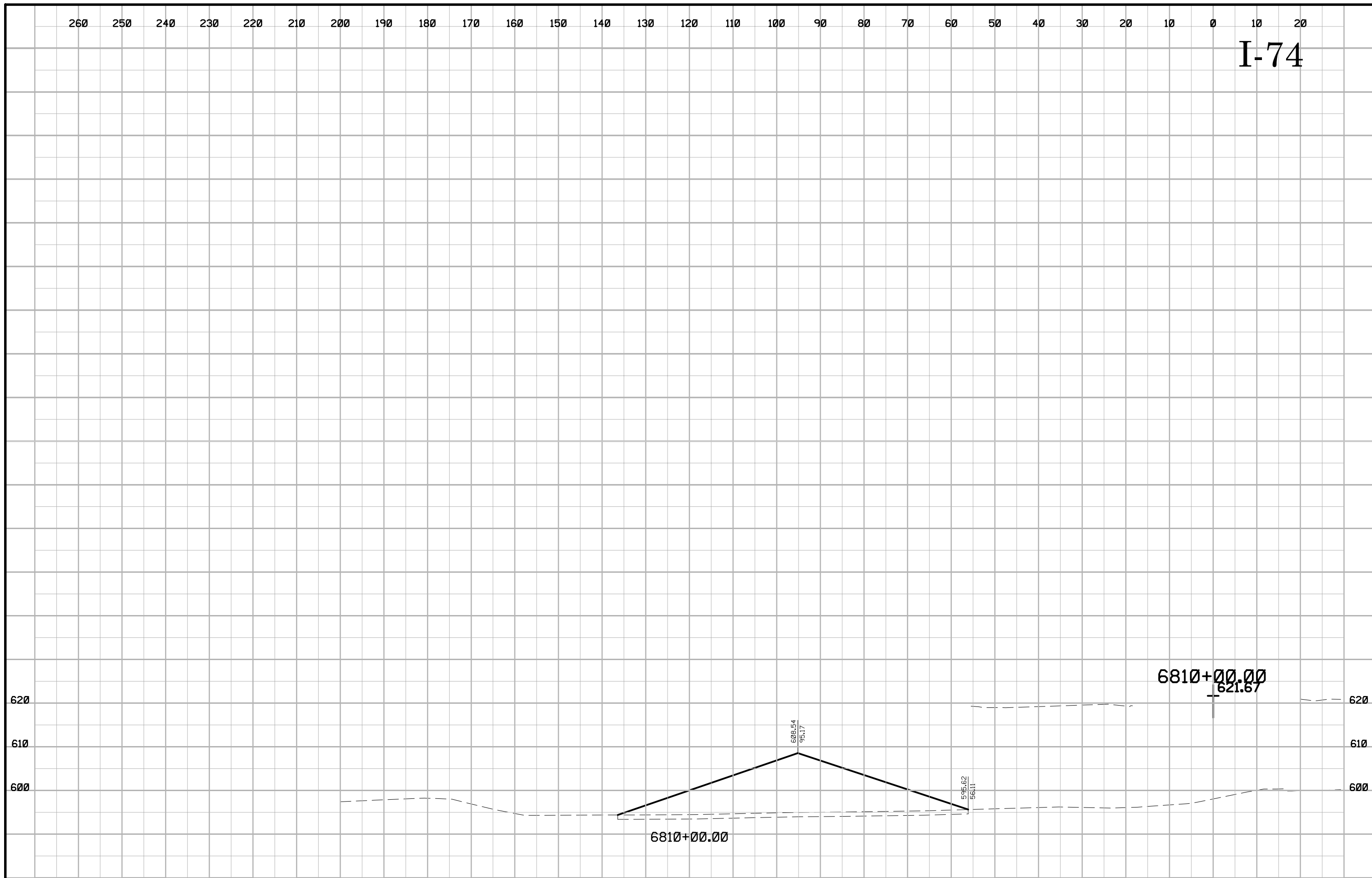
# I-74



# I-74

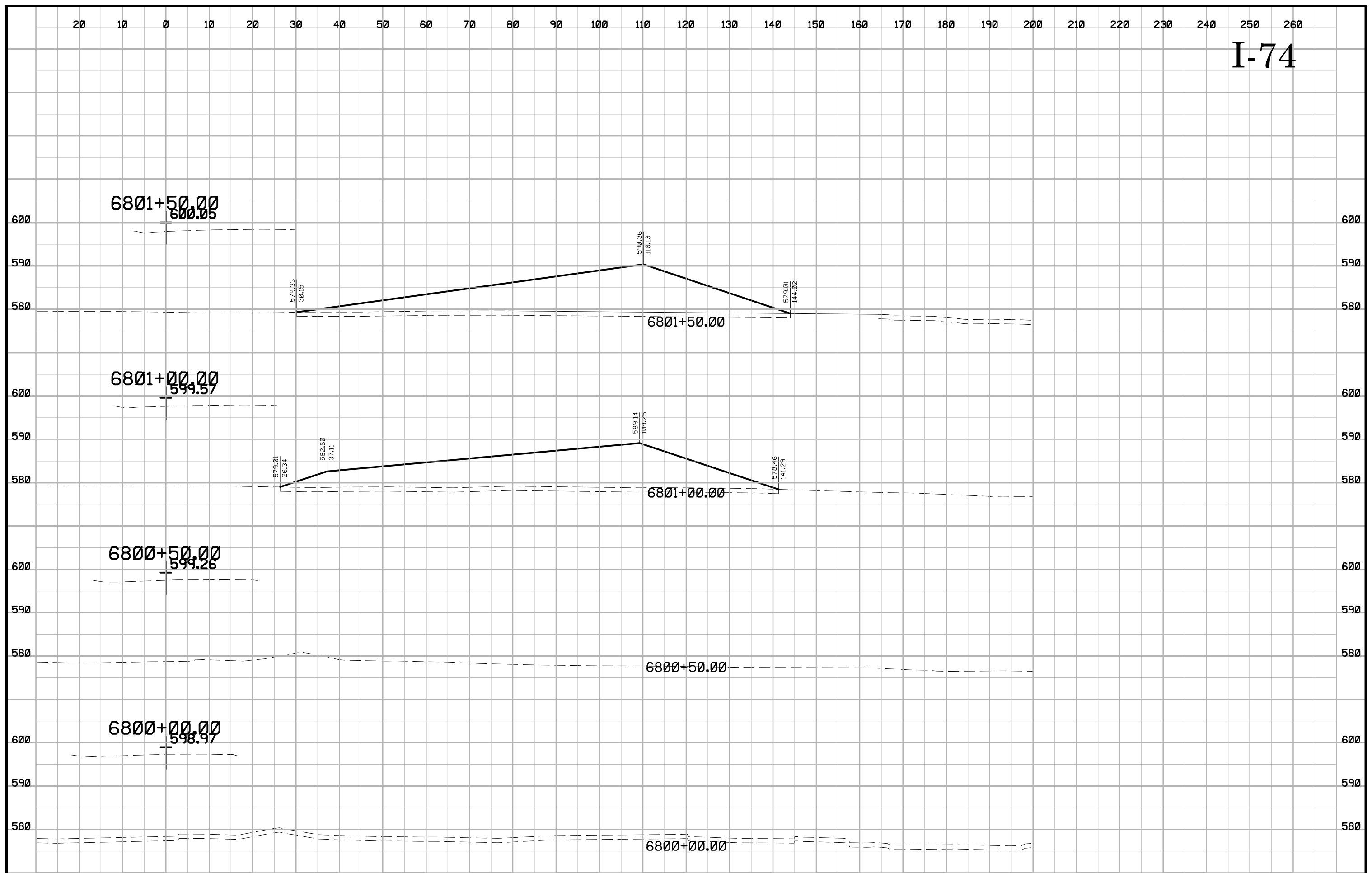


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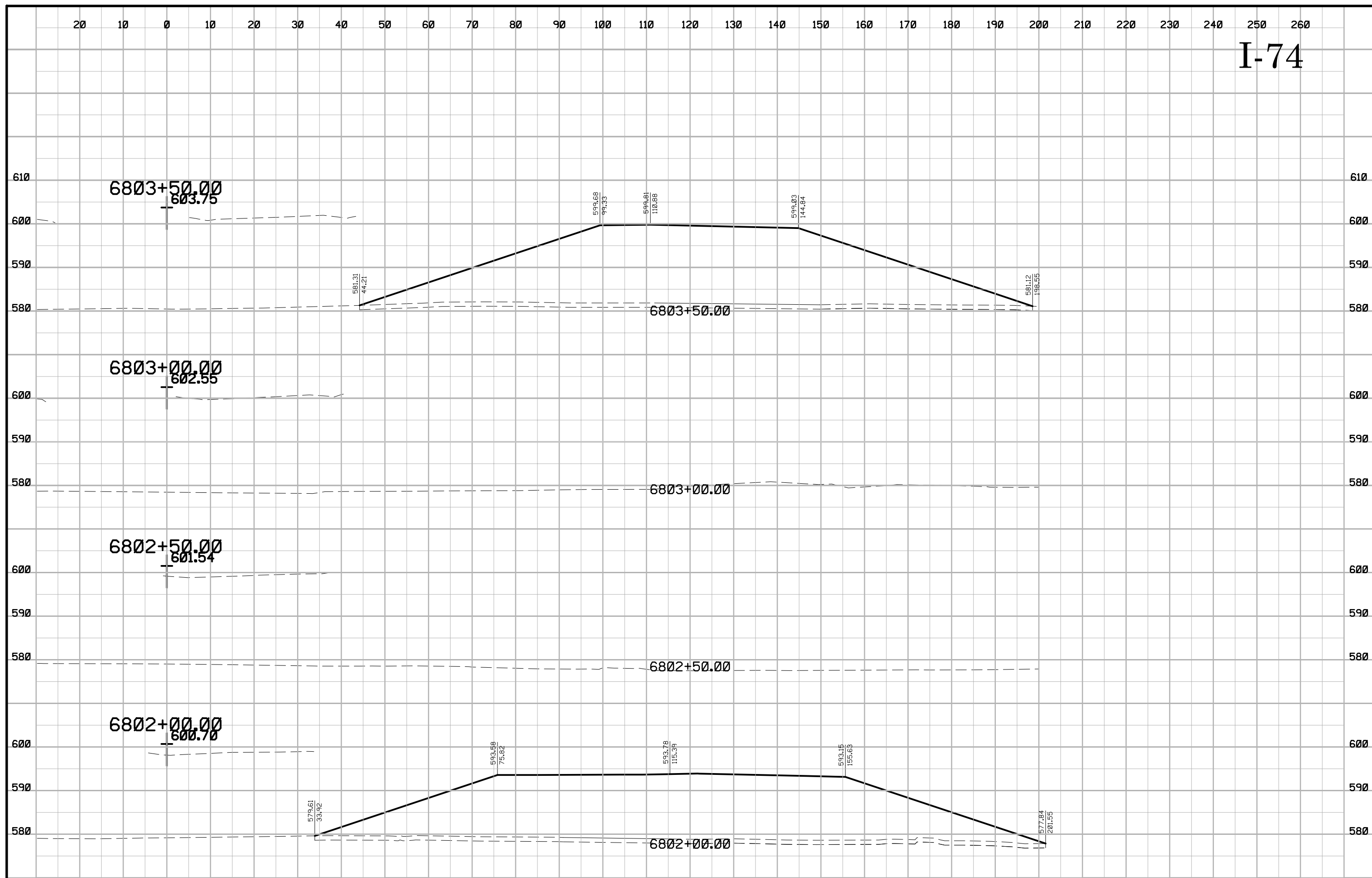




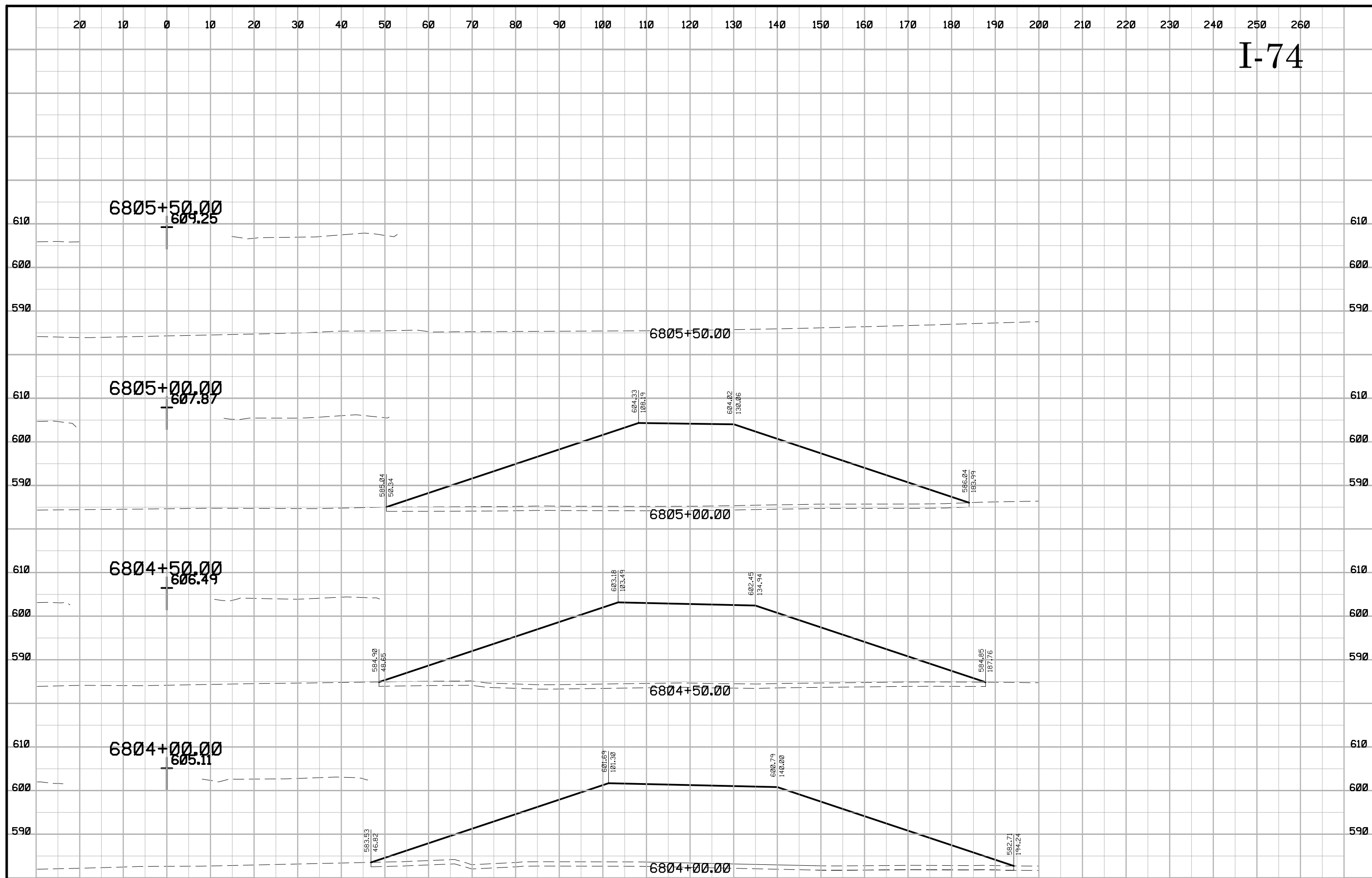
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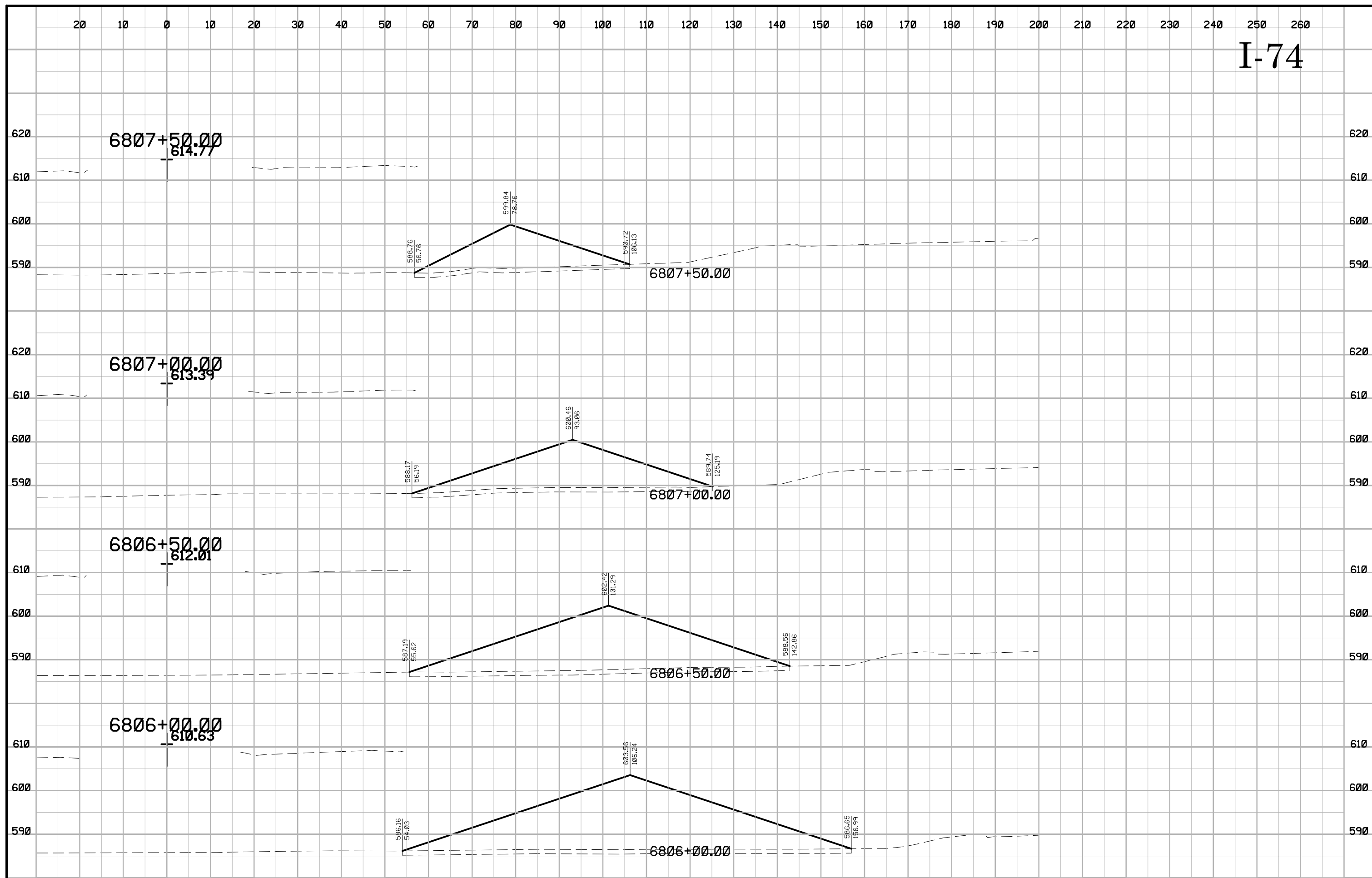
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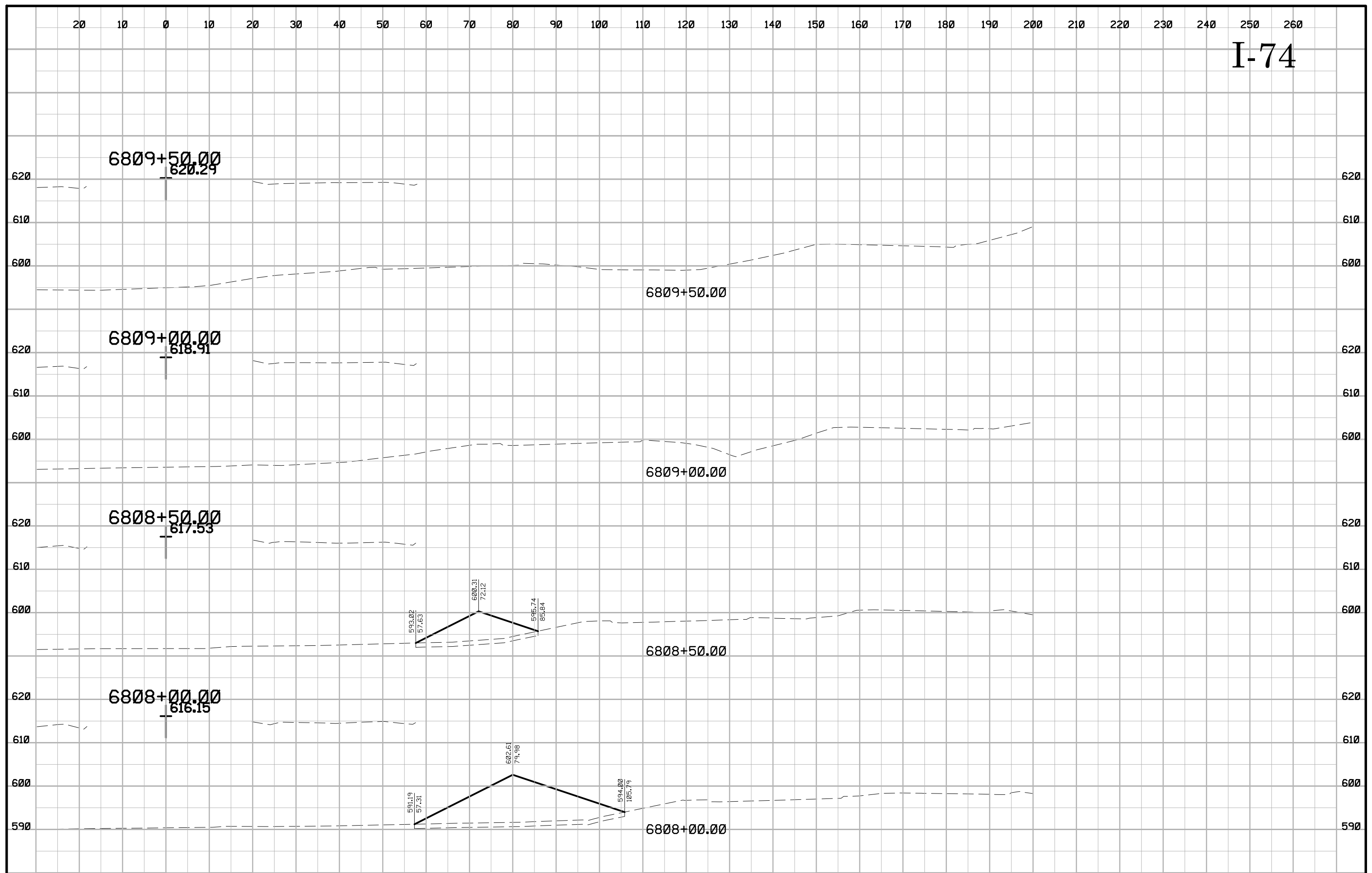
# I-74



# I-74



# I-74



# I-74

