

JOHNSON CO. UNKNOWN PAVEMENT - GRADE AND NEW
NHS-380-6(320)2--11-52

LETTING DATE
10-16-18



Highway Division

PLANS OF PROPOSED IMPROVEMENT ON THE

PRIMARY ROAD SYSTEM
JOHNSON COUNTY

UNKNOWN PAVEMENT - GRADE AND NEW

W. Forevergreen Rd from Covered Bridge Blvd. to Jones Blvd.

SCALES: As Noted

Refer to the Proposal Form for list of applicable specifications.

Value Engineering Saves. Refer to Article 1105.14 of the Specifications.



For Project Location Map
Refer to Sheet A.2

REVISIONS

TOTAL

PROJECT IDENTIFICATION NUMBER

16-52-380-010

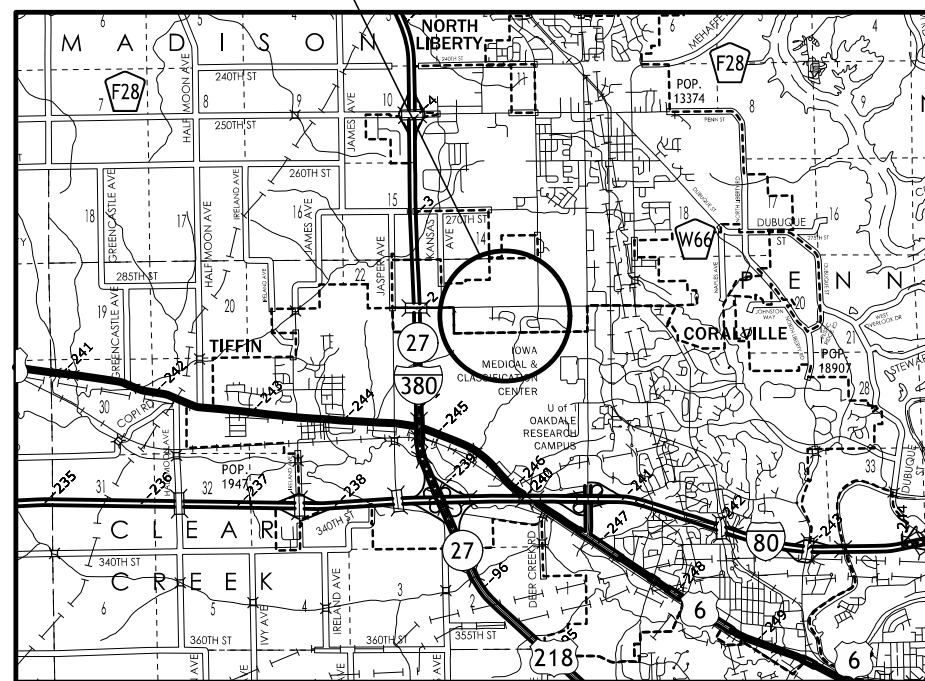
PROJECT NUMBER

NHS-380-6(320)2--11-52

R.O.W. PROJECT NUMBER

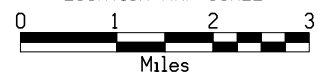
IMN-380-6(324)2--0E-52

Project Location



R-7W

LOCATION MAP SCALE



Project Dates

R1: 11-4-16

D8: 8-7-18

DESIGN DATA URBAN

2020 AADT	1,600	V.P.D.
2040 AADT	5,500	V.P.D.
2040 DHV	567	V.P.H.
TRUCKS	2	%
Total Design ESALs	--	

INDEX OF SEALS

SHEET NO.	NAME	TYPE
A.1	Ryan R. Miller	Primary Signature Block
X	X	X

PRELIMINARY PLANS

Subject to change by final design.

REVISED D5 PLAN - 10-21-16

FILE NO.

ENGLISH

DESIGN TEAM **J1a \ Miller \ Maach**

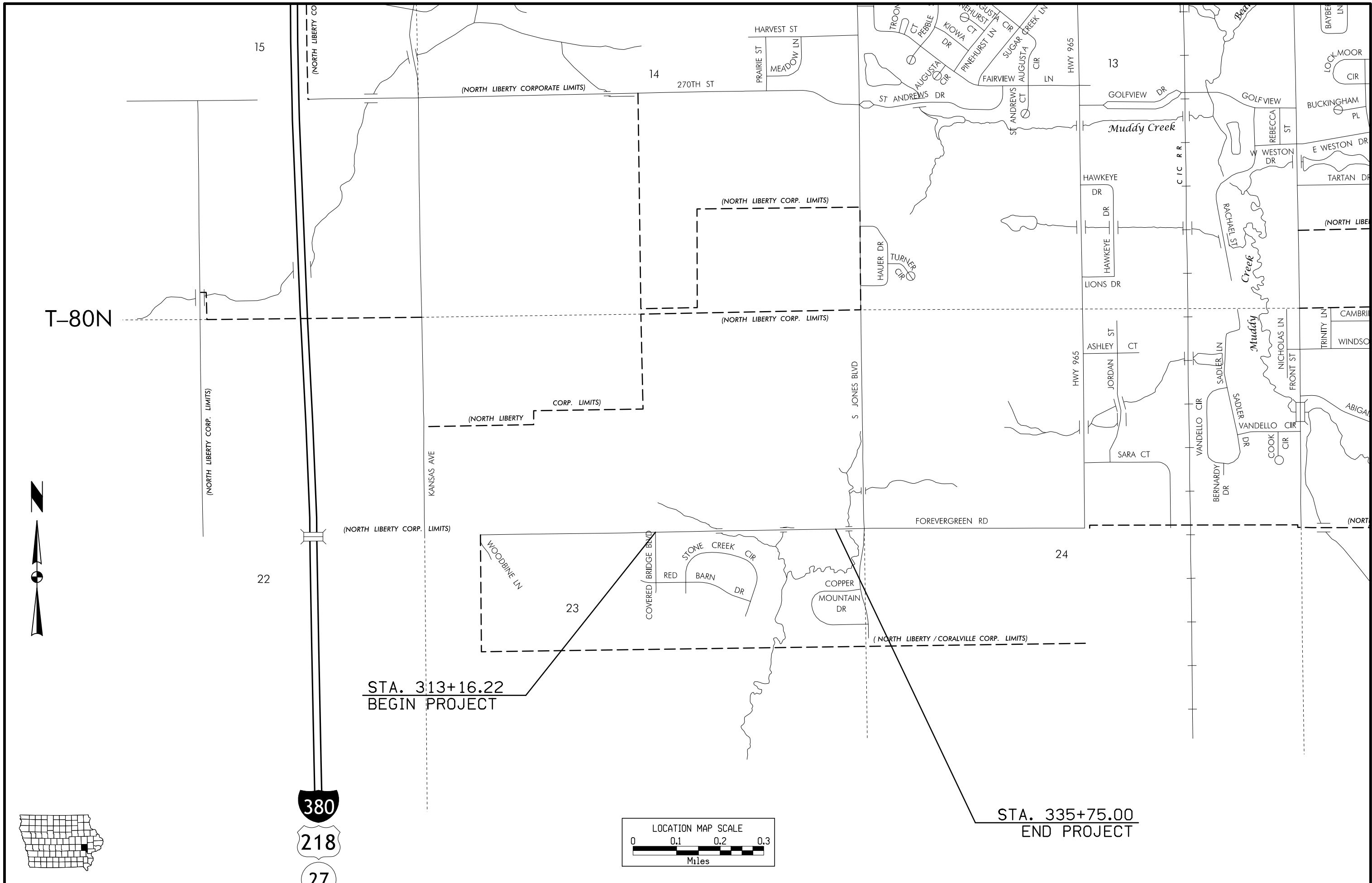
JOHNSON COUNTY

PROJECT NUMBER

NHS-380-6(320)2--11-52

SHEET NUMBER

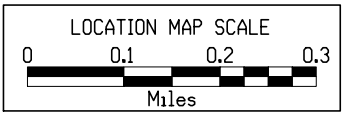
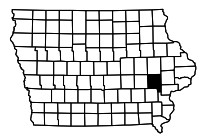
A.1



T-80N

STA. 313+16.22
BEGIN PROJECT

STA. 335+75.00
END PROJECT

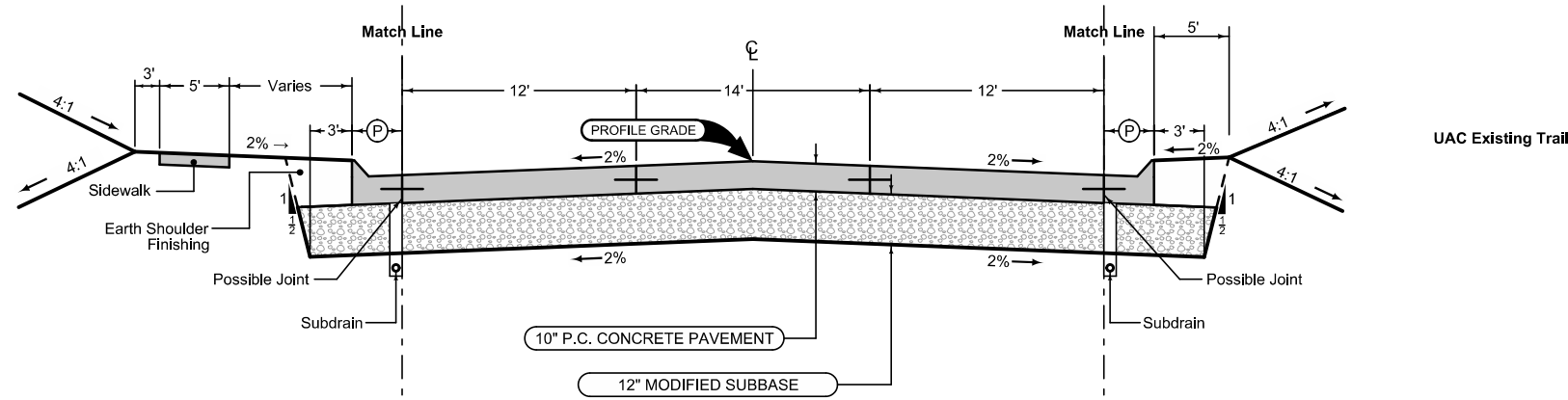


Curbed Shoulder

Shoulder Jointing:
 Longitudinal joint not required when distance from back of
 curb to nearest joint is less than 15':

Single pour: L-2
 Staged : KT-2
 Transverse:C at 20' spacing

STATION TO STATION		(P) Feet	Curb Type See PV-102
313+16.22	331+80	3.5	6" Std.
331+80	331+95	3.5-2.5	6" Std.
331+95	335+75	2.5	6" Std.



Mainline Jointing:
 Transverse joints: CD at 20' spacing
 Longitudinal joint: L-2

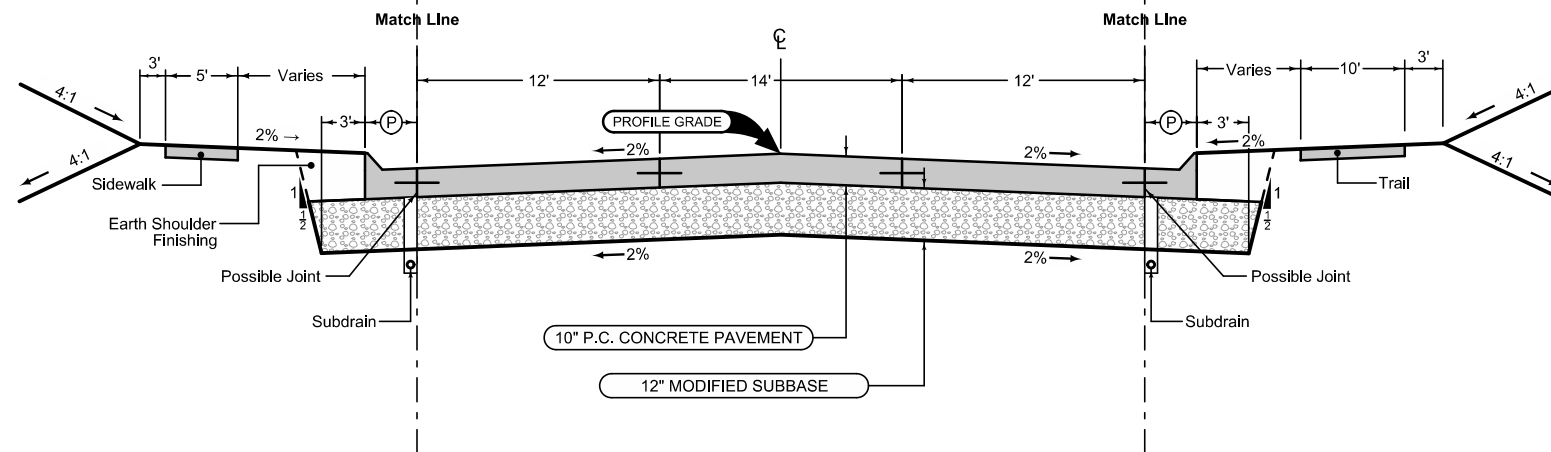
STATION TO STATION	
313+16.22	323+19.94

Curbed Shoulder

Shoulder Jointing:
 Longitudinal joint not required when distance from back of
 curb to nearest joint is less than 15':

Single pour: L-2
 Staged : KT-2
 Transverse:C at 20' spacing

STATION TO STATION		(P) Feet	Curb Type See PV-102
313+16.22	331+80	3.5	6" Std.
331+80	331+95	3.5-2.5	6" Std.
331+95	335+75	2.5	6" Std.

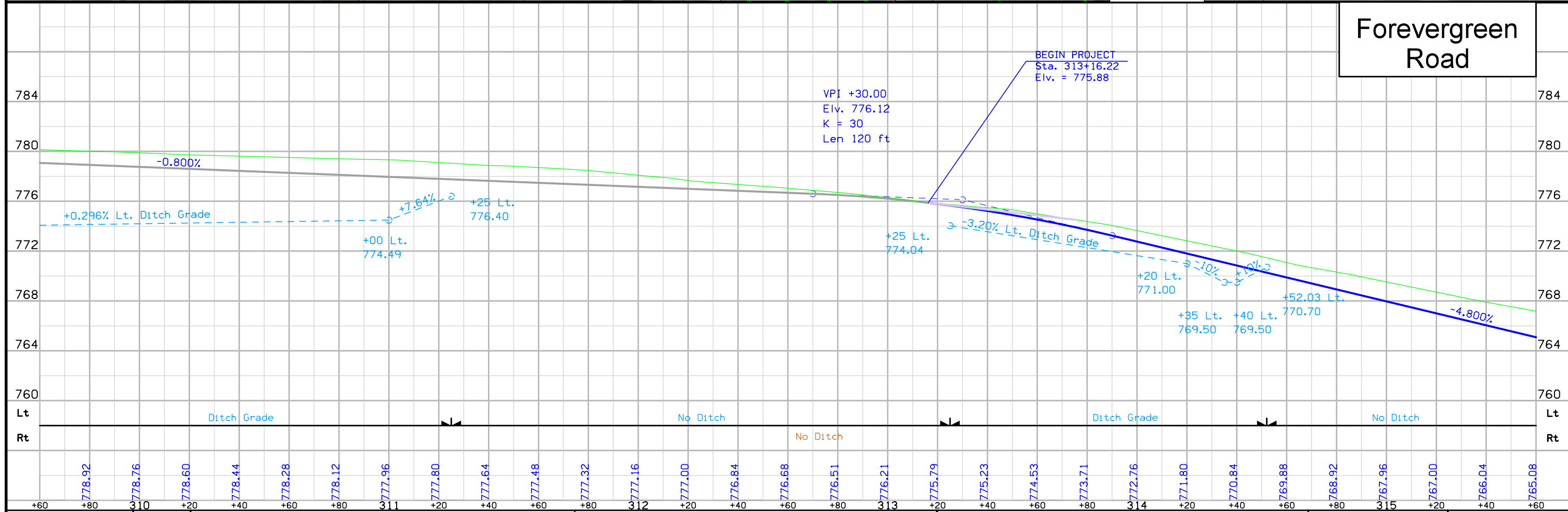
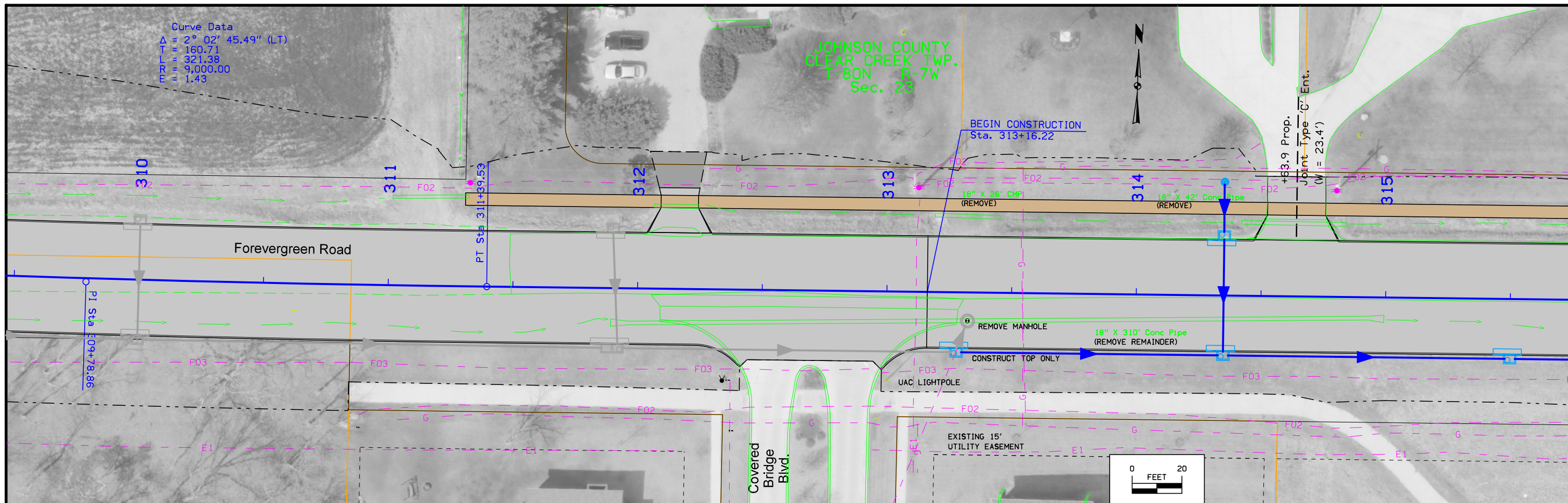


Mainline Jointing:
 Transverse joints: CD at 20' spacing
 Longitudinal joint: L-2

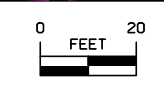
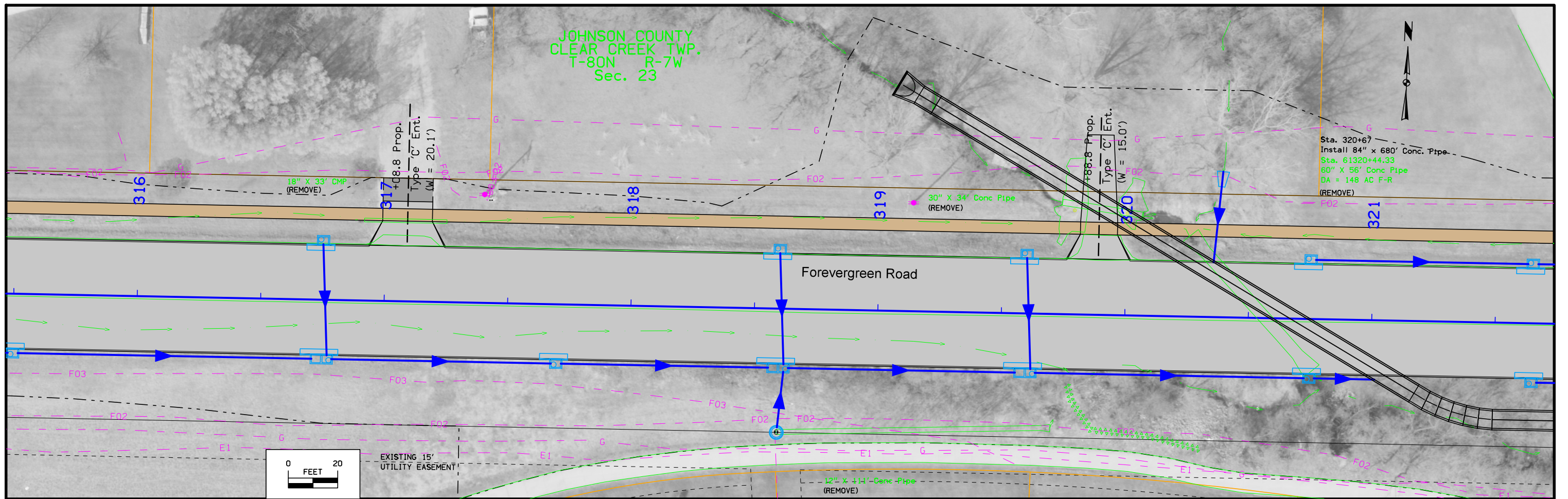
STATION TO STATION	
323+19.94	335+75

See Tab 100-24 or 100-25 for pavement quantities.
 See Tab 112-9 for shoulder quantities.

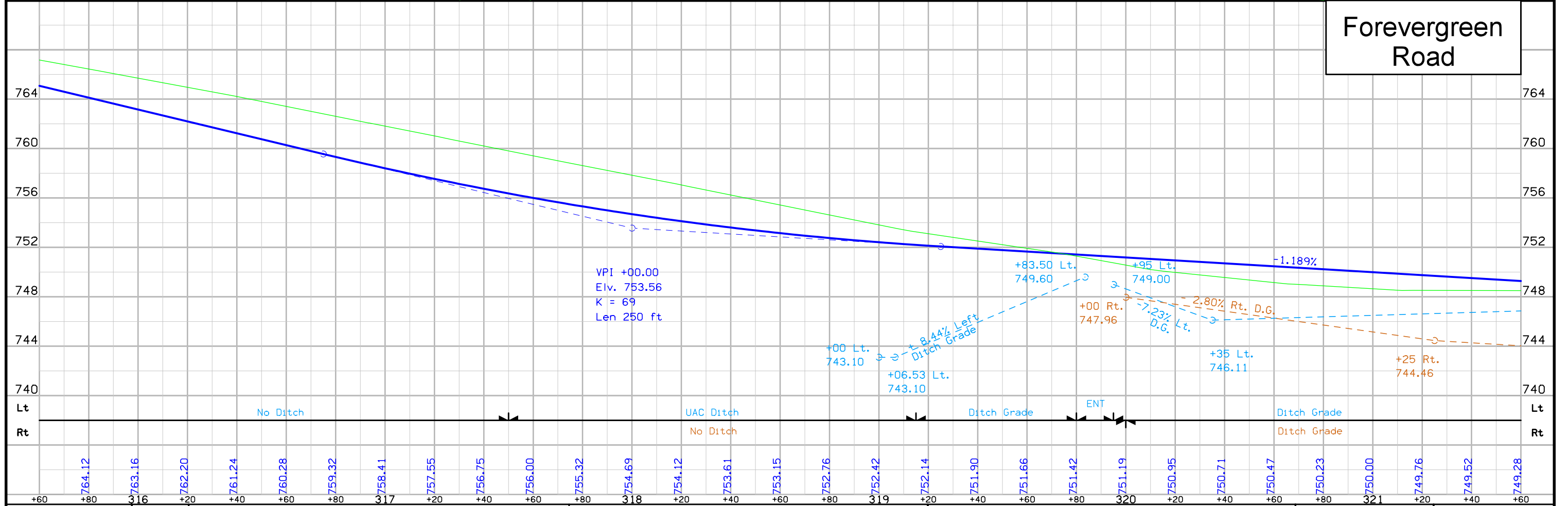
Forevergreen Road



JOHNSON COUNTY
CLEAR CREEK TWP.
T-80N R-7W
Sec. 23



Forevergreen Road



JOHNSON COUNTY
CLEAR CREEK TWP.
T-80N R-7W
Sec. 23

Sta. 61323+31.15
15" X 54' CMP
DA = 2 AC R
(ABANDON)

+95.1 Prop.
Type 'C' Ent.
(W = 21.0')

+21.8 Prop.
Type 'C' Ent.
(W = 17.0')

+22.0 Prop.
Type 'C' Ent.
(W = 15.0')

+30.9 Prop.
Type 'C' Ent.
(W = 16.6')

15" X 30' CMP
(REMOVE)

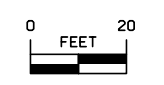
15" X 30' CMP
(REPLACE)

Forevergreen Road

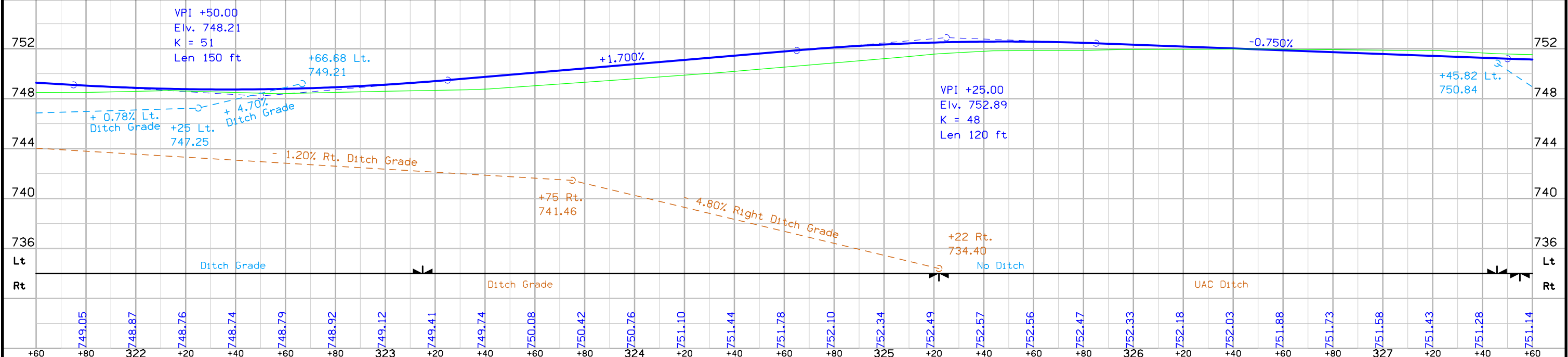
Sta. 320+67
Install 84" x 680' Conc. Pipe

EXISTING 15'
UTILITY EASEMENT

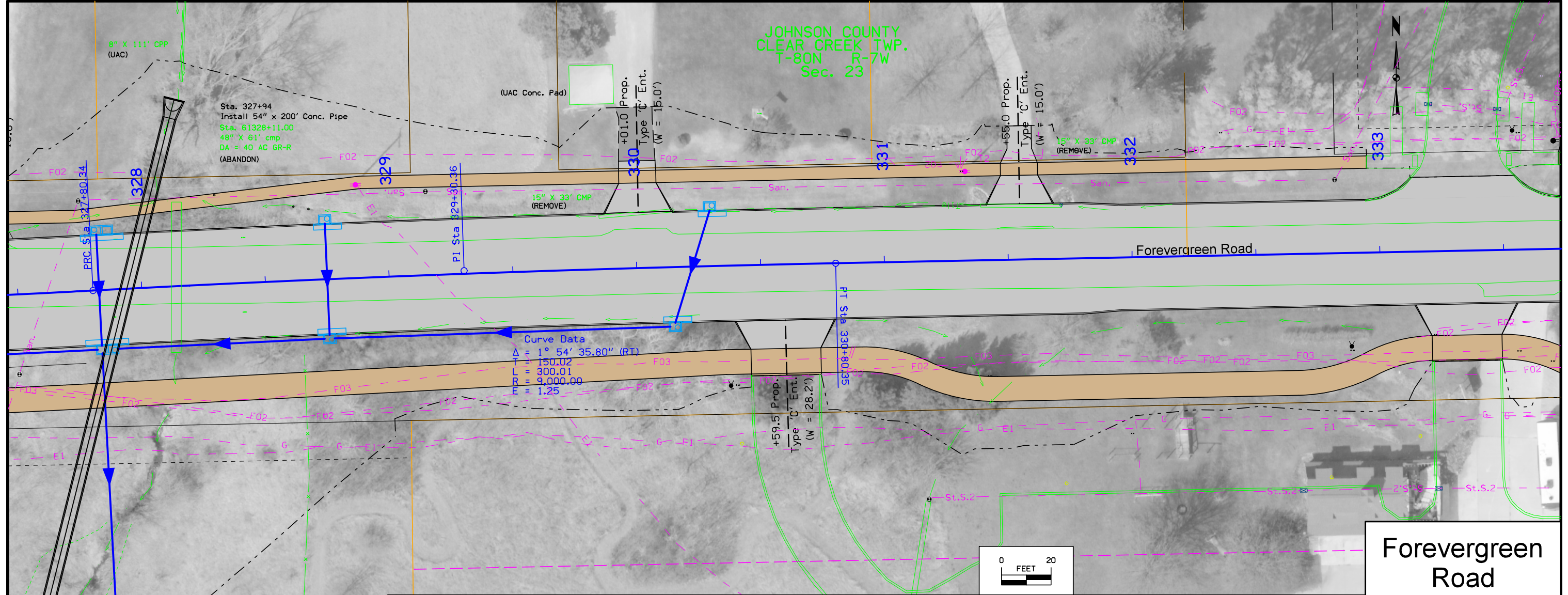
Curve Data
Δ = 1° 54' 35.80" (LT)
T = 150.02
L = 300.01
R = 9,000.00
E = 1.25



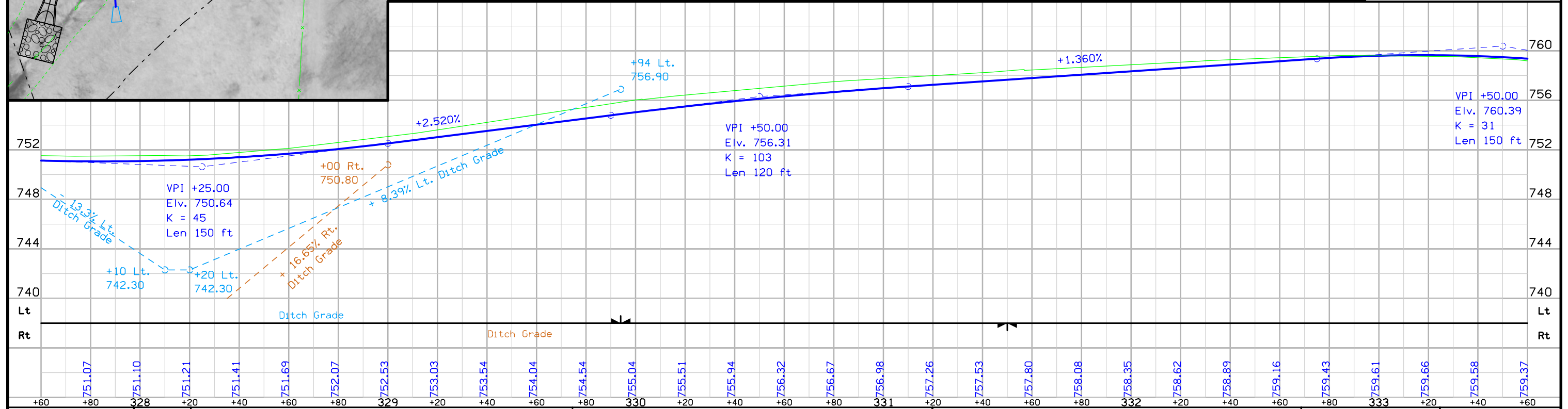
Forevergreen Road

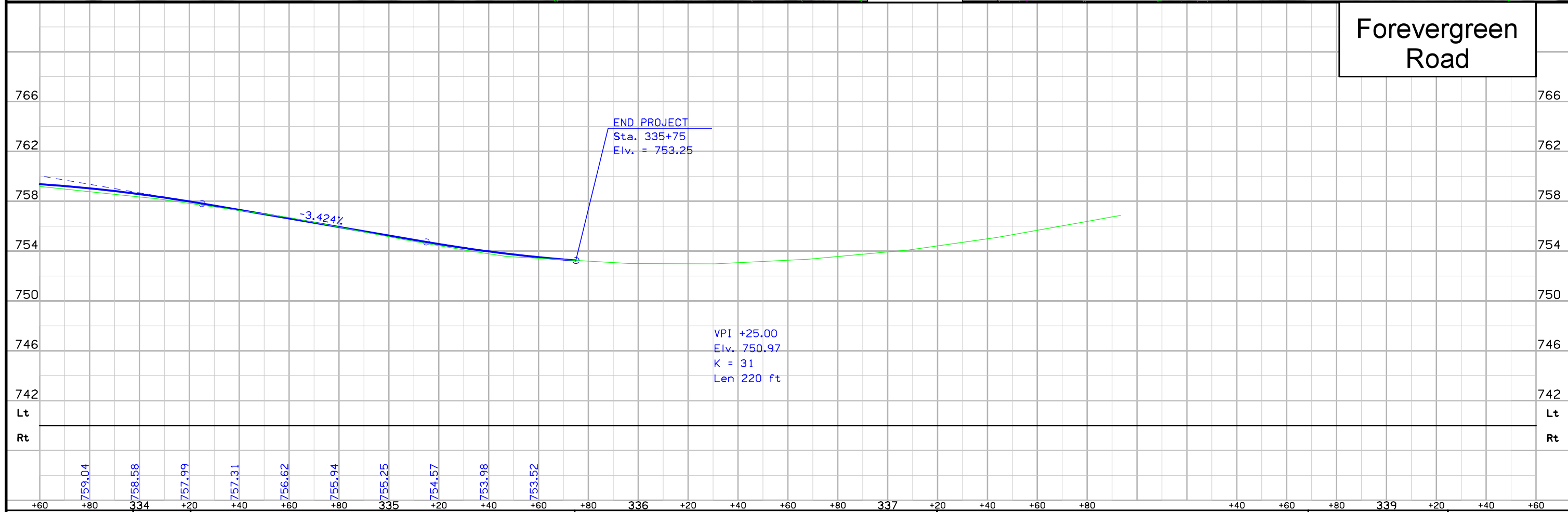
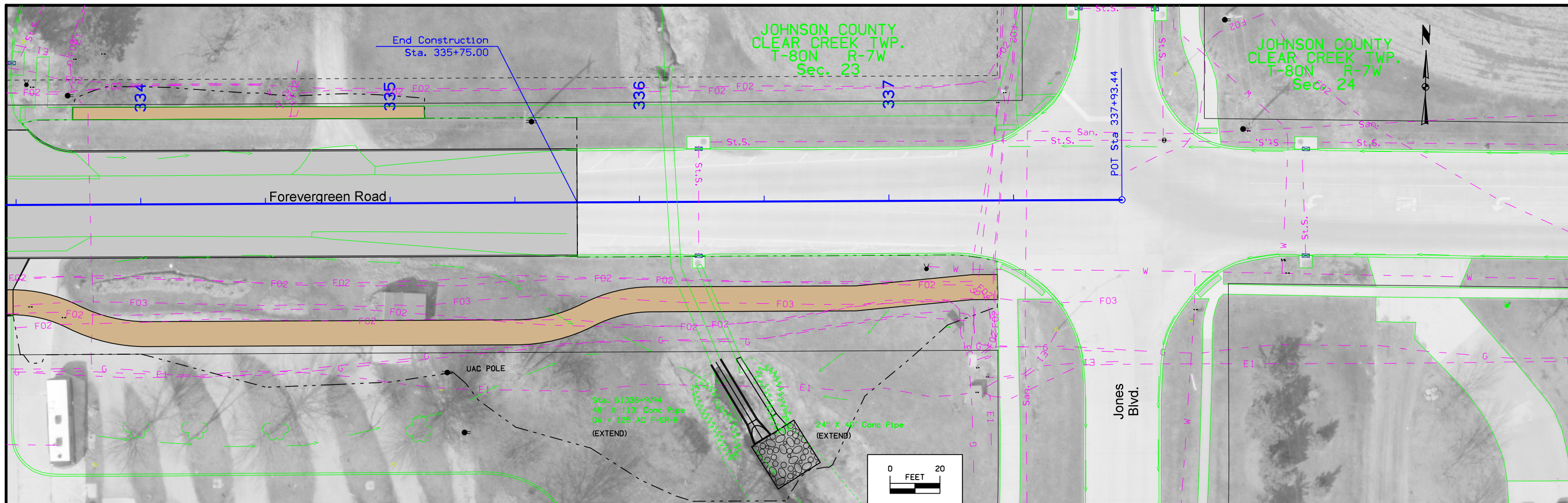


JOHNSON COUNTY
CLEAR CREEK TWP.
T-80N R-7W
Sec. 23



Forevergreen Road





FILE NO.	ENGLISH	DESIGN TEAM	J1a \ Miller \ Maach	JOHNSON COUNTY	PROJECT NUMBER	NHS-380-6(320)2--11-52	SHEET NUMBER	D.6
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General Information

The GPS Network along this project was collected by IDOT Preliminary Survey Crews. Information about that network can be found in the 0411gpspoints.doc file included with this survey in NAD83(1996) Project Coordinates.

This survey completes the Design DTM event of the "Can Do Process." Twelve section corners were found and included in this survey. None of the section corners coded as SCR in this survey have been certified by District 6 office. This survey was measured in English Units.

The mainline alignment of this survey is a retrace of the as-built plans # F-518-4(12)-20-52 1986 AB plans (centerline of median).

This survey intersects a 2000 Preliminary Survey along I-80. The 2000 survey data used Sap 0321. A revised GPS network was observed in 2002 for this project that includes all 2000 network control with approximately fourteen additional points added along the I-380 corridor north and south of I-80. The project control for this project is identical to the 2000 network control survey. Station equations to all as-built PI points are in the Horizontal datum information included below.

Vertical Datum

This survey is relative to NAVD88 vertical datum. Three wire bench level loops were run throughout this project. All bench loops originated and closed on one project benchmark #566 a 3rd order USGS mark called 26FDR 1964 682. Note the vertical datum difference between NGVD 88 and NAVD 29 is 0.10 feet in this area. The 29 datum is 0.10 higher than 88.

Vertical equations to the project datum Bench Marks and other benches along this survey are as follows:

BM # 566	This survey	EL=682.046
= BM # 566	Johnson County 2000 survey	EL=682.046
= USGS BM # 26	26FDR 1964 682	EL=682.046
BM # 625	This survey	EL=685.519
= BM # 14	1986 AB plan F-518-4(26)--20-52	EL=685.56
= BM # 1	Paving plan I-IG-380-6(19)243--04-52	EL=685.56
BM # 517	This survey	EL=737.314
= BM # 3	Paving plan I-IG-380-6(19)243--04-52	EL=737.37
BM # 520	This survey	EL=699.144
= BM # 19	Paving plan I-IG-380-6(19)243--04-52	EL=699.26
BM # 536	This survey	EL=691.494
= BM # 22	Paving plan I-IG-380-6(19)243--04-52	EL=691.61
BM # 636	This survey	EL=691.493
= BM # 521	2000 Carlson survey	EL=691.493
	IMN-80-6(21)240--00-52 (88 Datum)	EL=691.493
= BM # 22	Paving plan	EL=691.61
	I-IG-380-6(19)243--04-52 (29 Datum)	EL=691.61
= BM # 22	1986 AB Plan	EL=691.61
	F-518-4(12)--20-52 (29 Datum)	EL=691.61
BM # 608	This survey	EL=804.915
= BM # 37	1986 AB Plan	EL=804.85
	F-518-4(12)--20-52 (29 Datum)	EL=804.85
BM # 582	This survey	EL=758.068
= BM # 39	1986 AB Plan	EL=758.03
	F-518-4(12)--20-52 (29 Datum)	EL=758.03
BM # 502	This survey	EL=738.113
= BM # 502	2000 Carlson survey	EL=738.113
	IMN-80-6(21)240--00-52 (88 Datum)	EL=738.113
= BM # 51A	I-80-6(12)238 Grading Plan (29 Datum)	EL=738.36
BM # 512	This survey	EL=789.582
= BM # 512	2000 Carlson survey\	EL=789.582
	IMN-80-6(21)240--00-52 (88 Datum)	EL=789.582
= BM # 60A	I-80-6(12)238 Grading Plan (29 Datum)	EL=789.74
= BM # 500	IM-80-6(171)240--13-52 Plan (29 Datum)	EL=789.96
BM # 633	This survey	EL=684.211
= BM # 21A	F-289(6) 1970 AB PLAN (Datum unknown)	EL=703.62

BM # 634 This survey EL=682.904
 = BM # 21B F-289(6) 1970 AB PLAN (Datum unknown) EL=702.26

Horizontal Datum

The mainline horizontal alignment for this survey is a retrace of the I-380 plans. Stationing for I-380 was backed up and carried forward from TS Sta 11183+81.20 on the as built plans. Two sets of I-380 plans were used on this project are as follows:

I-380 F-518-4(12)--20-52 As-Built I-380 Plans from south of I-80 to just North of I-80 and I-IG-380-6(19)243-04-52 AS-Built I-380 Plans from just south of I-80 to 1.5 miles North of Co. Rd. F-28

Equations are as follows:

BOP POT Sta.11082+95.29 this survey =
 POT Sta. 1684+00.22 F-518-4(12)--20-52 As-Built Plans

CP Point 11097+51.08, 0.14 feet right this survey =
 = PC Sta. 1698+56.76 F-518-4(12)--20-52 As-Built Plans Back
 = PC Sta. 1698+60.00 F-518-4(12)--20-52 As-Built Plans Ahead

PI Sta. 11109+54.89 this survey =
 PI Sta. 1710+60.76 F-518-4(12)--20-52 As-Built Plans

PI Sta.11127+45.33 this survey =
 = POT Sta. 1127+45.33 IMN-80-6(211)240-0E-52 2000 Preliminary Survey
 = POT Sta. 1728+54.9 F-518-4(12)--20-52 As-Built Plans Back
 = POT Sta. 1127+44.85 F-518-4(12)--20-52 As-Built Plans Ahead

POT Sta.11163+54.20 This Survey I-380 Stationing (Not Set in Field)
 = POT Sta. 644+59.06 This Survey I-80 Stationing

= POT Sta. 644+50.24 IM-80-6(167)240-13-52 Feb 1996 Grading Plan I-80 Stationing
 = POT Sta. 638+56.24 F-518-4(12)--20-52 As-Built Plans I-80 Stationing
 = POT Sta. 638+56.24 I-IG-380-6(19)243-04-52 As-Built Plans I-80 Stationing
 = POT Sta. 1163+53.95 F-518-4(12)--20-52 As-Built Plans I-380 Stationing
 = POT Sta. 1163+53.95 I-IG-380-6(19)243-04-52 As-Built Plans I-380 Stationing
 = POT Sta. 644+59.06 IMN-80-6(211)240-0E-52 2000 Preliminary Survey I-80 Stationing

POT Sta. 11183+81.20 This survey
 = TS Sta. 1183+81.20 F-518-4(12)--20-52 As-Built Plans
 = TS Sta. 1183+81.20 I-IG-380-6(19)243-04-52 As-Built Plans
 = TS Sta. 1183+81.20 IMN-80-6(211)240-0E-52 2000 Preliminary Survey

PI Sta. 11191+13.01 this survey =
 PI Sta. 1191+12.08 I-IG-380-6(19)243-04-52 As-Built Plans

PI Sta. 11271+13.95 this survey =
 PI Sta. 1271+13.52 I-IG-380-6(19)243-04-52 As-Built Plans

PI Sta. 11324+10.95 this survey =
 PI Sta. 1324+11.21 I-IG-380-6(19)243-04-52 As-Built Plans

POT Sta. 11404+97.20 this survey =
 PC Sta. 1404+96.91 I-IG-380-6(19)243-04-52 As-Built Plans

BENCHMARKS		ELEVATION	
No.	Sta.	Horizontal	Vertical
No. 624	Sta.11111+94.255	79.87 Lt.	714.060
No. 625	Sta.11118+31.923	151.22 Lt.	685.519
			685.519
No. 626	Sta.11128+02.043	110.86 Rt.	730.221
No. 516	Sta.11138+40.798	85.63 Rt.	741.143

No. 517	Sta.11149+45.044	108.49 Rt.	737.314
No. 518	Sta.11161+33.669	75.84 Rt.	746.360
No. 515	Sta.11163+24.440	152.20 Lt.	717.715
No. 519	Sta.11165+48.102	77.23 Lt.	737.910
No. 520	Sta.11176+43.987	109.01 Lt.	699.144
No. 567	Sta.11187+84.421	65.27 Rt.	690.972
No. 636	Sta.11188+16.120	22.87 Rt.	691.494
No. 521	Sta.11188+16.200	22.77 Rt.	691.493
No. 568	Sta.11188+39.041	64.91 Lt.	695.024
No. 569	Sta.11189+63.986	64.82 Rt.	694.617
No. 570	Sta.11198+34.501	65.02 Rt.	714.922
No. 632	Sta.11199+30.780	142.78 Lt.	680.741
No. 572	Sta.11200+25.396	23.34 Lt.	716.012
No. 571	Sta.11200+34.418	65.35 Lt.	715.783
No. 573	Sta.11204+64.485	22.95 Lt.	713.595
No. 574	Sta.11204+75.377	65.13 Lt.	713.436
No. 575	Sta.11206+88.793	64.63 Rt.	712.079
No. 627	Sta.11206+90.470	216.20 Lt.	685.435
No. 576	Sta.11218+97.892	85.21 Rt.	732.351
No. 577	Sta.11235+00.075	16.06 Lt.	752.765
No. 578	Sta.11248+47.438	194.26 Rt.	745.111
No. 579	Sta.11261+23.666	152.27 Rt.	783.326

No. 608	Sta.11270+93.677	134.86 Rt.	FD.IHC SE.COR.WHLGD.BRIDG BM#608 ELEV.= 804.915(E) THIS SURVEY -BM#37 ELEV.= 804.85(E) F-518-4(12)--20-52 1986 AB PLAN----- 804.915 CUT"X"NW.COR.WHLGD.BRIDG----- 804.763	No. 522	Sta. 659+46.078	96.58 Lt.	FD\IHC-BM-SW-WING-RAMP BRG FROM I-80 W.BOUND TO I-380 N.BOUND----- 683.510	No. 628	Sta.51220+41.072	38.97 Rt.	FD.DOT BUTTON OUTLET HDWL 12.0 X 6.0 RCB,S.SIDE RD.----- 692.107
No. 607	Sta.11271+31.826	130.49 Lt.	CUT"X"NW.COR.WHLGD.BRIDG----- 804.763	No. 523	Sta. 661+62.644	113.94 Rt.	FD\X-CONC-WHEELGUARD-SE END OF RAMP BRIDGE FROM I-380 N.BOUND TO I-80 EAST BOUND----- 678.334	No. 601	Sta.61258+19.677	73.21 Lt.	SET RR.SPK.SW.SIDE P.POLE----- 797.156
No. 580	Sta.11272+23.924	148.34 Rt.	SET RR.SPK.W.SIDE FE.POST----- 779.049	No. 524	Sta. 678+73.492	137.92 Rt.	SET\RR-SPK-N-SIDE-WOOD FENCE POST----- 668.615	No. 607	Sta.61269+81.203	16.29 Lt.	CUT"X"NW.COR.WHLGD.BRIDG----- 804.763
No. 581	Sta.11287+68.335	98.68 Rt.	CUT"X"INLET 42"RCP S.10F2----- 764.010	No. 622	Sta.20572+00.606	33.65 Rt.	SET RR.SPK.SW.SIDE P.POLE----- 764.688	No. 608	Sta.61272+47.430	15.13 Rt.	FD.IHC SE.COR.WHLGD.BRIDG BM#608 ELEV.= 804.915(E) THIS SURVEY -BM#37 ELEV.= 804.85(E) F-518-4(12)--20-52 1986 AB PLAN----- 804.915
No. 582	Sta.11303+75.126	105.25 Rt.	FD.IHC INLET HDWL.TWIN8X8 BM#582 ELEV.= 758.068(E) THIS SURVEY -BM#39 ELEV.= 758.03(E) F-518-4(12)--20-52 1986 AB PLAN----- 758.068	No. 621	Sta.20578+73.257	31.20 Rt.	SET RR.SPK.W.SIDE P.POLE----- 763.973	No. 580	Sta.61272+57.419	115.97 Lt.	SET RR.SPK.W.SIDE FE.POST----- 779.049
No. 583	Sta.11319+99.239	100.54 Rt.	CUT"X"NW.W.CONC.SIGN BASE----- 767.172	No. 620	Sta.20591+97.150	53.93 Rt.	SET RR.SPK.SW.SIDE FE.PST----- 740.677	No. 609	Sta.71285+24.573	36.52 Lt.	SET RR.SPK.W.SIDE P.POLE----- 782.709
No. 606	Sta.11324+44.101	150.98 Lt.	SET RR.SPK.SE.SIDE P.POLE----- 780.252	No. 619	Sta.20602+54.671	63.25 Lt.	SET RR.SPK.N.SIDE P.POLE----- 786.486	No. 610	Sta.71300+33.158	43.78 Rt.	SET RR.SPK.E.SIDE P.POLE----- 777.704
No. 584	Sta.11332+00.705	107.52 Rt.	CUT"X"E.SIDEW.CONC.SGN.BA----- 775.672	No. 513	Sta.20608+06.706	35.16 Lt.	SET\RR-SPK-N-SIDE-P-POLE----- 799.211	No. 611	Sta.71313+91.962	33.55 Lt.	SET RR.SPK.W.SIDE P.POLE----- 794.997
No. 585	Sta.11343+50.252	109.94 Rt.	CUT"X"NE.SIDEW.CONC.SGN.B----- 788.481	No. 510	Sta.20622+90.521	33.39 Lt.	FD\RR-SPK-E-SIDE-P-POLE----- 759.074	No. 612	Sta.71324+43.867	33.77 Lt.	SET RR.SPK.E.SIDE P.POLE----- 777.635
No. 586	Sta.11356+00.495	116.53 Rt.	CUT"X"W.SIDEW.CONC.SGN.BA----- 795.758	No. 511	Sta.30620+64.760	67.37 Lt.	FD\X-SOUTH-CONC-BASE-OF OVERHEAD SIGN= BM # 501 PROJECT NUMBER IM-80-6(171)240--13-52 ELEVATION=768.539----- 768.176	No. 605	Sta.81312+32.522	26.41 Lt.	SET RR.SPK.S.SIDE P.POLE----- 778.045
No. 587	Sta.11375+75.327	149.09 Rt.	SET RR.SPK.W.SIDE P.POLE----- 798.873	No. 512	Sta.30622+33.605	14.88 Lt.	FD\IHC-BM-NW-WING-BRIDG = BM 60A ELEV = 789.74 GRADING PLANS PROJECT NO.I-80-6(12)238=BM # 500 PROJECT NUMBER IM-80-6(171)240--13-52 ELEVATION=789.96----- 789.582	No. 606	Sta.81322+59.298	30.49 Lt.	SET RR.SPK.SE.SIDE P.POLE----- 780.252
No. 592	Sta.11377+19.336	135.71 Lt.	CUT"X"SW.WING CO.RD.F28 B----- 816.985	No. 617	Sta.30634+79.709	15.80 Rt.	SET RR.SPK.W.SIDE TEL.POL ON JASPER AVE.----- 778.994	No. 613	Sta.81339+01.184	31.48 Lt.	SET RR.SPK.S.SIDE P.POLE----- 764.142
No. 593	Sta.11377+53.473	133.56 Rt.	FD.DOT BUTTON NE.WING BR.----- 817.034	No. 616	Sta.30639+48.147	43.04 Rt.	SET RR.SPK.W.SIDE P.POLE ON JASPER AVE.----- 758.301	No. 590	Sta.91359+26.297	34.25 Rt.	ARROWHEAD ON SW.SIDE FHD----- 788.696
No. 588	Sta.11389+34.201	160.47 Rt.	SET RR.SPK.W.SIDE FE.POST----- 789.830	No. 615	Sta.30653+45.242	17.13 Rt.	FD.SQ.SE.COR.HNDRL.BRIDG OVER CLEAR CREEK,JASPER A----- 686.896	No. 591	Sta.91365+45.283	23.04 Rt.	ARROWHEAD ON NE.SIDE FHD----- 792.288
No. 589	Sta.11403+67.224	180.83 Rt.	SET RR.SPK.NW.SIDE P.POLE----- 762.153	No. 614	Sta.30655+23.641	16.15 Lt.	CUT"X"NW.COR.HNDRL.BRG.OV ER CLEAR CREEK ON JASPER AVE. JUST S.RR.TRACKS----- 687.721	No. 592	Sta.91376+00.766	16.91 Rt.	CUT"X"SW.WING CO.RD.F28 B----- 816.985
No. 500	Sta. 524+24.824	275.01 Rt.	CUT-X-NW-CORNER-CONC-SLAB OF THE EAST MOST HISTORICAL MONUMENT----- 725.532	No. 633	Sta.30664+43.666	29.41 Rt.	FD.IHC INLET HDWL 4X2 RCB BM# 633 ELEV.= 684.221(E) THIS SURVEY -BM#21A ELEV.=703.62 F-289(6) 1970 AB PLAN----- 684.221	No. 593	Sta.91378+70.104	16.70 Lt.	FD.DOT BUTTON NE.WING BR.----- 817.034
No. 501	Sta. 541+26.761	95.72 Rt.	FD\IHC-BM-ON-INLET-HDWL 6.0 X 6.0 RCB----- 716.211	No. 596	Sta.30677+73.777	27.86 Lt.	SET RR.SPK.E.SIDE P.POLE----- 717.410	No. 587	Sta.91378+85.282	161.48 Rt.	SET RR.SPK.W.SIDE P.POLE----- 798.873
No. 502	Sta. 557+55.028	100.54 Lt.	FD\IHC-BM-ON-INLET-HDWL 4.0 X 4.0 RCB = BM 51A ELEV = 738.36 GRADING PLANS PROJECT NO. I-80-6(12)238----- 738.113	No. 597	Sta.30686+97.621	16.19 Rt.	FD.BOLT INLET HDWL.8X6RCB----- 709.220	No. 594	Sta.91393+35.495	28.53 Rt.	ARROWHEAD ON W.SIDE FHD----- 788.677
No. 503	Sta. 567+54.008	112.27 Rt.	FD\IHC-BM-SE-WING-BRIDG = BM 51C ELEV = 768.83 GRADING PLANS PROJECT NO. I-80-6(12)238----- 768.519	No. 598	Sta.30698+38.006	33.23 Lt.	SET RR.SPK.W.SIDE P.POLE----- 733.159	No. 595	Sta.91400+06.045	28.37 Rt.	ARROWHEAD ON W.SIDE FHD.----- 785.191
No. 505	Sta. 572+43.123	232.39 Lt.	FD\IHC-BM-ON-INLET-HDWL 5.0 X 5.0 RCB = BM 56B ELEV = 736.39 GRADING PLANS PROJECT NO. I-80-6(12)238----- 736.150	No. 599	Sta.30711+77.883	32.92 Lt.	SET RR.SPK.W.SIDE P.POLE----- 780.071	MISCELLANEOUS LOCATIONS			
No. 506	Sta. 576+04.565	174.78 Lt.	FD\IHC-BM-ON-OUTLET-HDWL 42" CIR RCB W/ FLUME = BM 56A ELEV = 736.54 GRADING PLANS PROJECT NO. I-80-6(12)238----- 736.266	No. 600	Sta.30719+26.238	33.12 Lt.	SET RR.SPK.W.SIDE P.POLE----- 808.144	No. 43	*****	GPS# 043-GPS ZC= 751.45 ADJUSTED LOCAL PROJECT PLANE COORDINATES ELEVATION IS ESTIMATED GPS DERIVED ORTHO HEIGHT----- 751.478	
No. 507	Sta. 582+71.852	157.29 Rt.	SET\RR-SPK-N-SIDE-FE-POST----- 741.725	No. 601	Sta.30728+10.453	42.91 Rt.	SET RR.SPK.SW.SIDE P.POLE----- 797.156	No. 44	*****	GPS# 044 -GPS ZC= 783.95 ADJUSTED LOCAL PROJECT PLANE COORDINATES ELEVATION IS ESTIMATED GPS DERIVED ORTHO HEIGHT----- 783.937	
No. 508	Sta. 593+92.172	141.09 Lt.	SET\RR-SPK-S-SIDE-P-POLE----- 741.525	No. 602	Sta.30740+44.218	32.72 Rt.	SET RR.SPK.W.SIDE GATEPOS----- 760.080	No. 45	*****	GPS# 045 -GPS ZC= 767.37 ADJUSTED LOCAL PROJECT PLANE COORDINATES ELEVATION IS ESTIMATED GPS DERIVED ORTHO HEIGHT----- 767.378	
No. 509	Sta. 607+05.870	79.46 Rt.	FD\IHC-BM-ON-INLET-HDWL 5.0 X 5.0 RCB = BM 58A ELEV = 738.31 GRADING PLANS PROJECT NO. I-80-6(12)238----- 738.221	No. 603	Sta.30753+47.152	26.22 Rt.	SET RR.SPK.W.SIDE P.POLE----- 757.608	No. 46	*****	GPS# 046 -GPS ZC= 777.31 ADJUSTED LOCAL PROJECT PLANE COORDINATES ELEVATION IS ESTIMATED GPS DERIVED ORTHO HEIGHT----- 777.325	
No. 511	Sta. 620+62.574	65.33 Rt.	FD\X-SOUTH-CONC-BASE-OF OVERHEAD SIGN= BM # 501 PROJECT NUMBER IM-80-6(171)240--13-52 ELEVATION=768.539----- 768.176	No. 604	Sta.30766+58.868	30.99 Rt.	SET RR.SPK.W.SIDE P.POLE----- 768.523	No. 47	*****	GPS# 047 -GPS ZC= 798.04 ADJUSTED LOCAL PROJECT PLANE COORDINATES ELEVATION IS ESTIMATED GPS DERIVED ORTHO HEIGHT----- 798.026	
No. 512	Sta. 621+03.405	106.71 Lt.	FD\IHC-BM-NW-WING-BRIDG = BM 60A ELEV = 789.74 GRADING PLANS PROJECT NO.I-80-6(12)238=BM # 500 PROJECT NUMBER IM-80-6(171)240--13-52 ELEVATION=789.96----- 789.582	No. 632	Sta.41197+93.824	27.47 Rt.	CUT"X"N.SIDE 48"CONC.P.PO -LE BASE S.RR.TRACKS JUST W. 380 OVERPASS----- 680.741	No. 51	*****	GPS# 051 -GPS ZC= 764.45 ADJUSTED LOCAL PROJECT PLANE COORDINATES ELEVATION IS ESTIMATED GPS DERIVED ORTHO HEIGHT----- 764.528	
No. 514	Sta. 628+04.839	138.29 Rt.	FD-X-NORTH-SIDE-CONC-BASE LIGHT POLE = BM # 502 PROJECT NUMBER IM-80-6(171)240--13-52 ELEVATION=759.83----- 759.429	No. 571	Sta.41198+53.244	87.28 Lt.	CUT"X"NW.WING SB.BRIDG----- 715.783	No. 566	*****	FD USGS DISK OUTLET HDWL 4.0 X 2.0 RCB NAVD 88 EL=682.046----- 682.046	
No. 519	Sta. 643+13.570	149.65 Lt.	FD\IHC-BM-NW-WING-S-BOUND I-380 BRIDGE OVER I-80----- 737.910	No. 572	Sta.41198+95.539	85.52 Lt.	CUT"X"NE.WING SB.BRIDG----- 716.012				
No. 515	Sta. 643+29.799	85.69 Rt.	CUT-X-S-SIDE-OF-THE-SOUTH CONC BASE OVERHEAD SIGN----- 717.715	No. 570	Sta.41200+17.490	85.89 Rt.	FD.DOT BUTTON SE.WING BR.----- 714.922				
No. 518	Sta. 646+13.448	174.79 Rt.	FD\IHC-BM-SE-WING-N-BOUND I-380 BRIDGE OVER I-80----- 746.360	No. 631	Sta.41210+26.902	27.22 Rt.	CUT"X"N.SIDE 48"CONC.P.PO -LE BASE,OVERHEAD POWER LINE,S.RR.TRACKS,JUST W. FS FEEDS BUILDING----- 684.629				
				No. 630	Sta.41219+67.035	28.89 Rt.	CUT"X"N.SIDE 48"CONC.P.PO -LE BASE S.RR.TRACKS E. END FS FEEDS FENCE----- 688.824				
				No. 633	Sta.51192+29.509	33.19 Lt.	FD.IHC INLET HDWL 4X2 RCB BM# 633 ELEV.= 684.221(E) THIS SURVEY -BM#21A ELEV.=703.62 F-289(6) 1970 AB PLAN----- 684.221				
				No. 634	Sta.51200+72.371	24.25 Lt.	FD.IHC INLET HDWL 4X2 RCB BM# 634 ELEV.= 682.904(E) THIS SURVEY -BM#21B ELEV.=702.26 F-289(6) 1970 AB PLAN----- 682.904				
				No. 627	Sta.51203+52.358	60.83 Lt.	SET RR.SPK.S.SIDE P.POLE N.SIDE HWY 6,JUST W.OF 380 OVERPASS----- 685.435				
				No. 574	Sta.51205+48.360	121.48 Rt.	FD.DOT BUTTON SW.WING BR.----- 713.436				
				No. 573	Sta.51205+96.470	120.17 Rt.	CUT"X" SE.WING SB.BR.HWY6----- 713.595				
				No. 575	Sta.51206+11.957	120.14 Lt.	FD.DOT BUTTON NE.WING BR.----- 712.079				

No. 48 ***** GPS# 048-GPS ZC= 786.78
ADJUSTED LOCAL PROJECT
PLANE COORDINATES
ELEVATION IS ESTIMATED
GPS DERIVED ORTHO HEIGHT----- 786.758

No. 49 ***** GPS# 049 -GPS ZC= 813.21
ADJUSTED LOCAL PROJECT
PLANE COORDINATES
ELEVATION IS ESTIMATED
GPS DERIVED ORTHO HEIGHT----- 813.212

No. 50 ***** GPS# 050-GPS ZC= 784.71
ADJUSTED LOCAL PROJECT
PLANE COORDINATES
ELEVATION IS ESTIMATED
GPS DERIVED ORTHO HEIGHT----- 784.725

No. 629 ***** SET RR,SPK,SW,SIDE L.POLE
IN ENT.TO FS FEEDS----- 702.775

No. 623 ***** FD.BOLT SW,COR,HDWL,TWIN
12 X 12 RCB----- 688.940

No. 38 ***** GPS# 038 -GPS ZC= 717.88
ADJUSTED LOCAL PROJECT
PLANE COORDINATES
ELEVATION IS ESTIMATED
GPS DERIVED ORTHO HEIGHT
GPS POINT G038----- 717.875

No. 39 ***** GPS POINT G039
GPS# 39 -GPS ZC= 733.36
ADJUSTED LOCAL PROJECT
PLANE COORDINATES
ELEVATION IS ESTIMATED
GPS DERIVED ORTHO HEIGHT----- 733.310

No. 635 ***** SET RR SPK N. SIDE P.POLE
AT ENT TO 911 # 3050
ON N. SIDE 355 ST. S.W.----- 775.965

No. 504 ***** FD\IHC-BM-ON-INLET-HDWL
6.0 X 4.0 RCB
= BM 51B ELEV = 760.19
GRADING PLANS PROJECT
NO. I-80-6(12)238----- 759.878

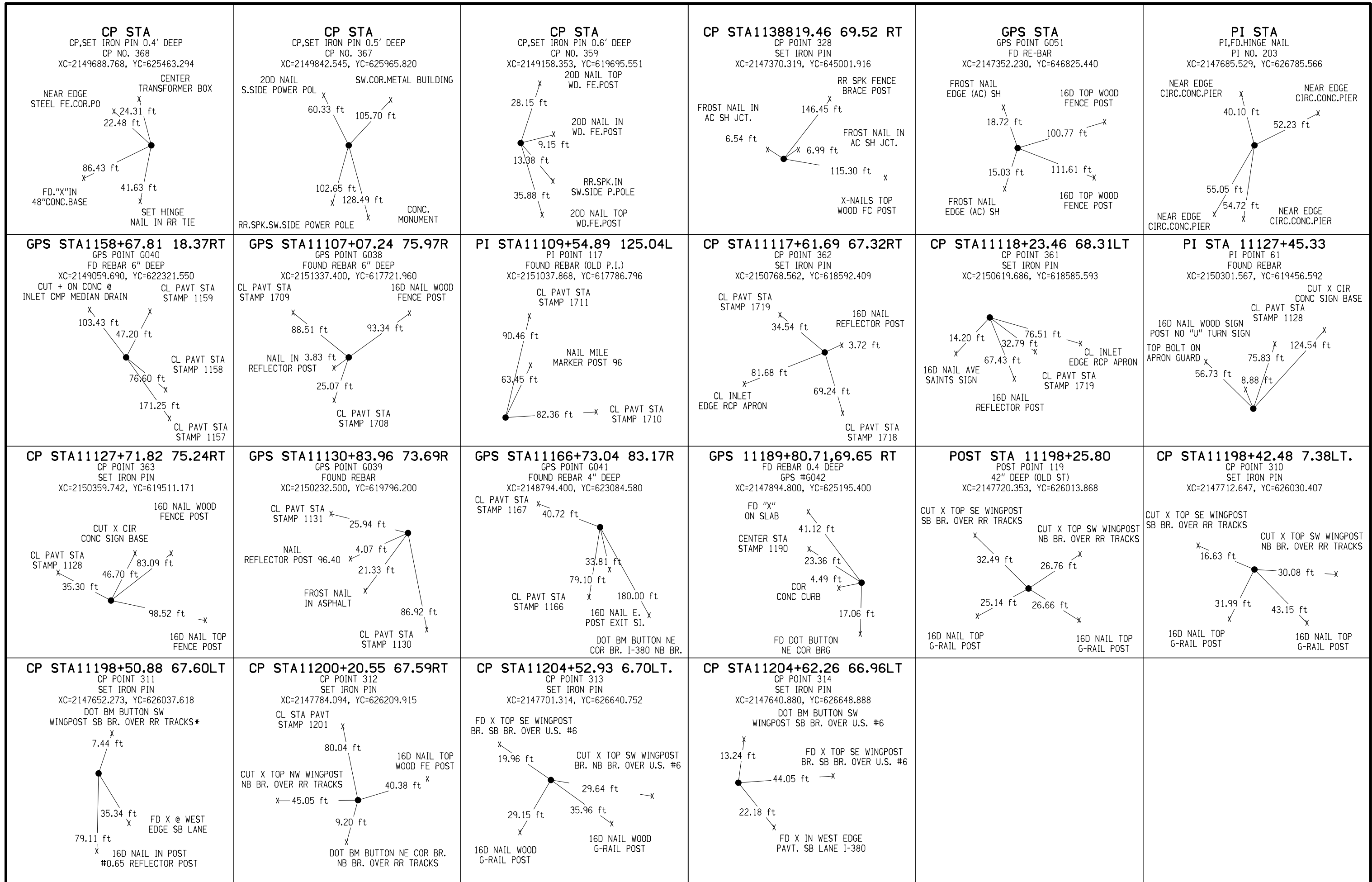
No. 11 ***** GPS-POINT-G011
FENO MONUMENT
ORTHO ELVATION =722.71----- 722.698

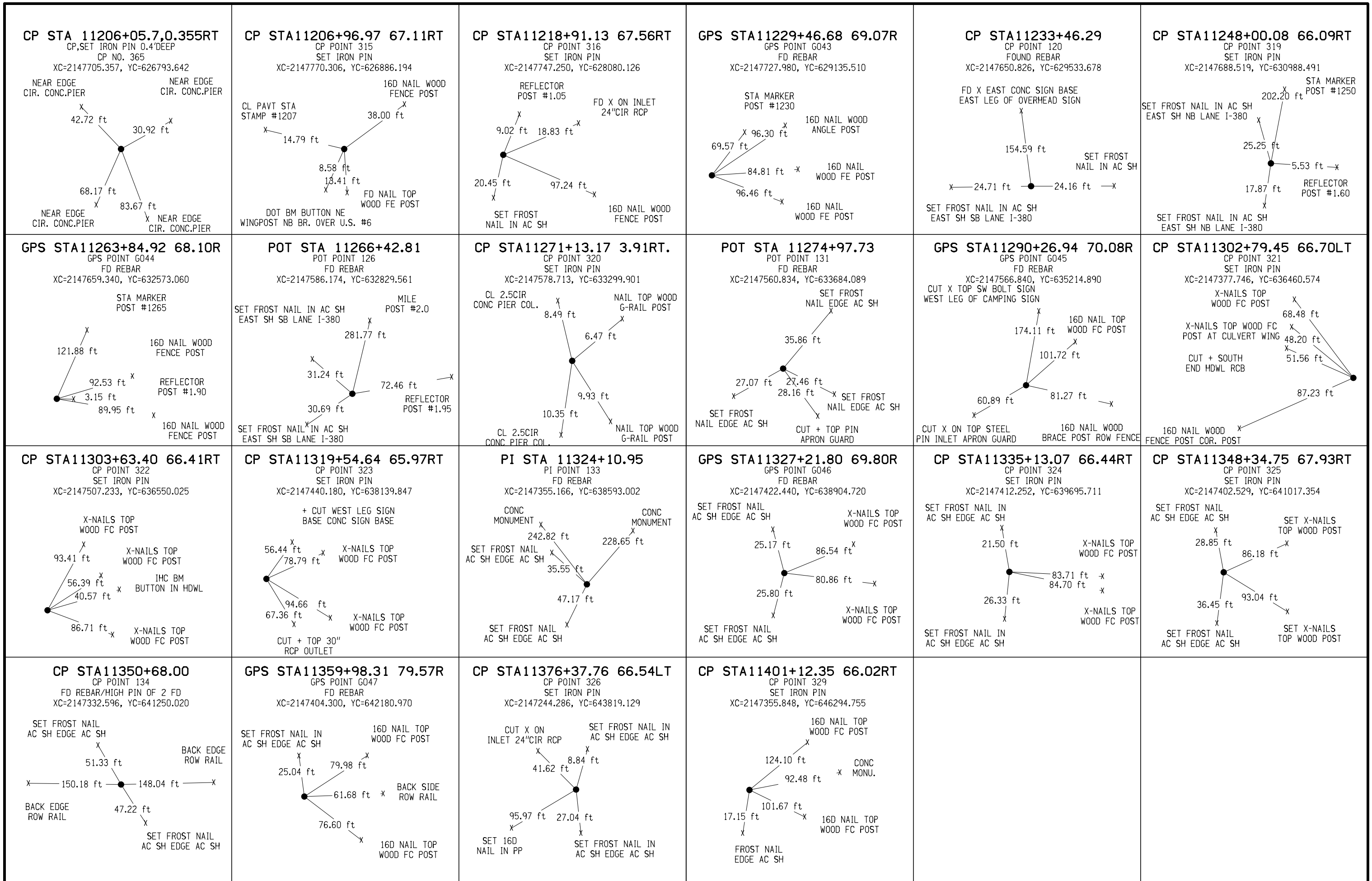
No. 13 ***** GPS-POINT-G013
ORTHO ELEVATION=709.82
FD\ REBAR----- 709.794

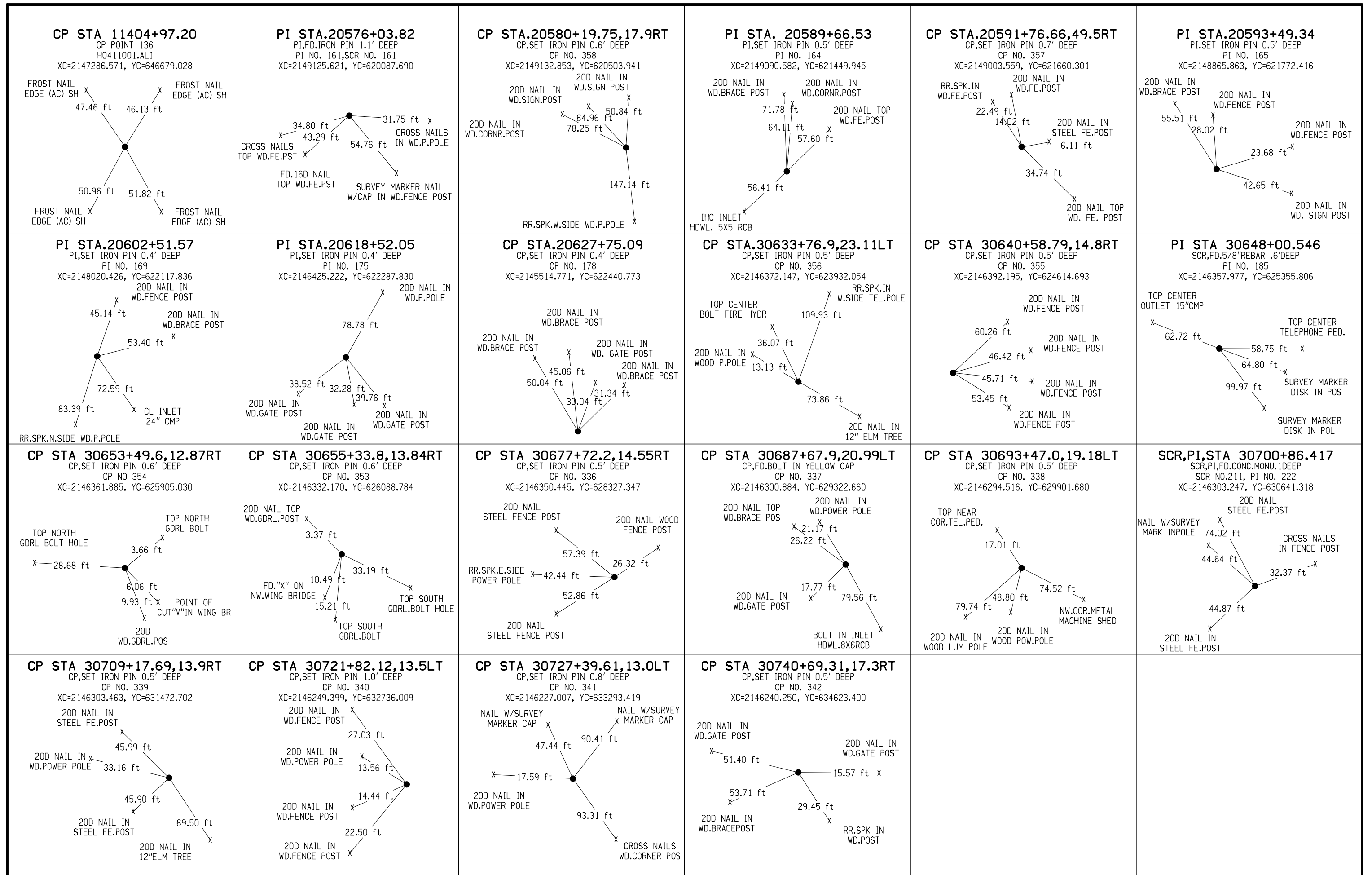
No. 525 ***** FD\IHC-BM-SW-WING-I-80
E.BOUND BR. OVER HWY 6----- 713.964

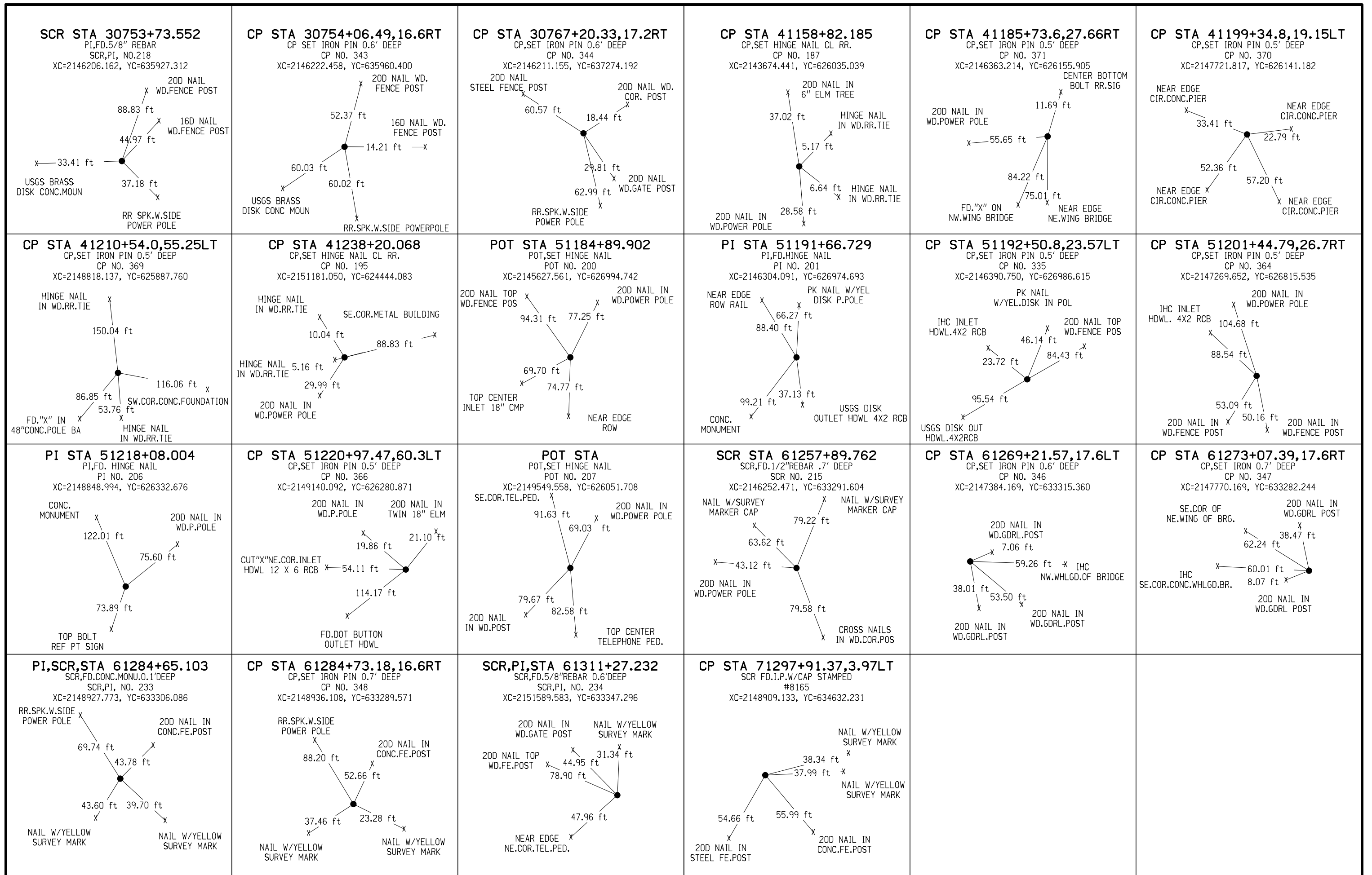
No. 1095 ***** TARGET-#-1095----- 679.243

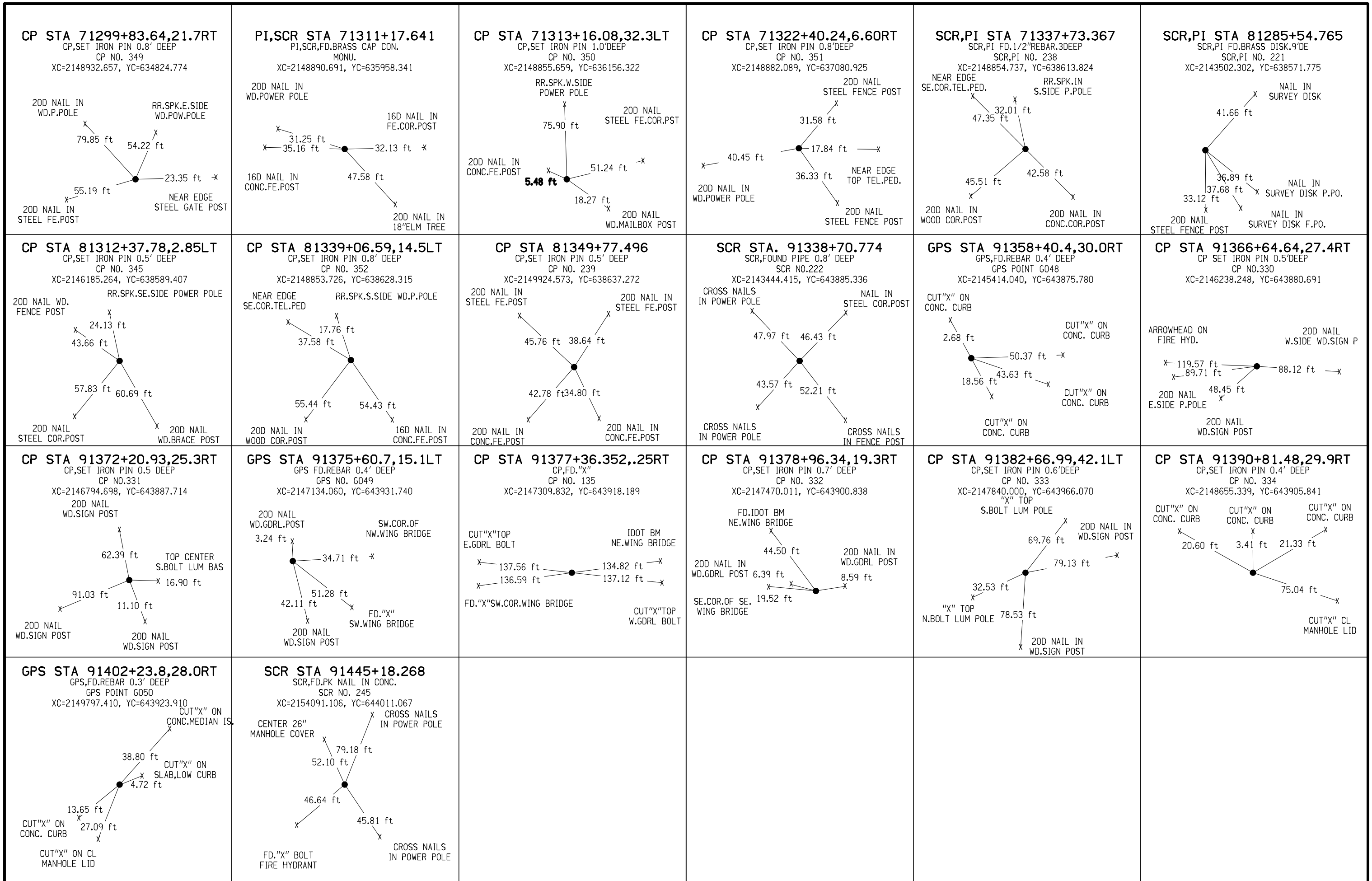
No. 526 ***** FD\IHC-BM-ON-INLET-HDWL
8.0 X 8.0 RCB
= BM 115B ELEV. = 678.77
PCC PAVING PROJECT PLANS
NO. I-IG-80-6(5)245----- 678.693











Survey Information

June/July 2015 Additional Survey Part 2
 Johnson County
 IM-380-6(302)2--13-52
 I-380 & West Forevergreen Road Survey 2 miles north of I-80
 in Coralville, North Liberty and Tiffin SAP 0411.6
 PIN 14-52-380-004

Vertical Datum

This survey is relative to NAVD88 vertical datum.

A level loop originated and terminated on BM 607. BM#607 & BM#608 were held fixed vertically in a bench level loop, that was carried easterly along Forevergreen road to Just East of Ia 965.

BM #607 This survey EL=804.763
 =BM #607 IDOT 2003 Survey EL=804.763

BM #608 This survey EL=804.915
 =BM # 37 1986 AB Plan F-518-4(12)--20-52 (29 Datum) EL=804.85

Horizontal Datum

Control used in Previous IDOT surveys was used on this project.

For additional coordinate information refer to a IDOT Preliminary Survey document called "Johnson80_380rep.doc" to determine the relationships between the survey control and state plane control coordinates on this project.

Alignment Information

Jasper Ave SR "C" found in the original survey, File H0411C1.ali, was renamed to SURJASPER.ali.
 Forevergreen Road SR "F" found in the original survey, File H0411F1.ali, was extended East and renamed to SURFOREVERGREEN.ali, I-380 ML alignment found in the original survey, File H0411ML1.ali, was renamed to SURI380.ali,
 Kansas Ave NE alignment found in the original survey, File 0411G1.ali was renamed to SURKANSAS.ali
 270th Street NE alignment found in the original survey, File0411H1.ali was renamed to SUR270thNE.ali
 SURIa965.ali alignment found in this survey is a new alignment along State Hwy Ia 965.

The SURFOREVERGREEN.ali file found in the 2003 IDOT survey was extended east to the EOP of this survey. The EOP of this survey is just East of Sara Ct.

Alignment Equations:

CP 216 =
 =PI Sta 61257+77.309 Forevergreen Rd IDOT 2003 Survey
 =PI Sta 61257+77.309 Forevergreen Rd This Survey
 =PI Sta 30727+37.559 Jasper Rd IDOT 2003 Survey
 =PI Sta 30727+37.559 Jasper Rd This Survey

CP 128
 =POT 61271+14.248 Forevergreen Rd IDOT 2003 Survey
 =POT 61271+14.248 This Survey
 =POST 11271+12.02 (1.84' South of PI Sta 11271+13.86) I-380 IDOT 2003 Survey
 =POST 11271+12.02 (1.84' South of PI Sta 11271+13.86) This Survey

Ia 965 Alignment Equations:

CP 98
 =POT Sta 94+13.561 This Survey
 =POT Sta 94+13.8 Ia 153 AB Plans

CP 95
 =PI Sta 130+14.200 This Survey
 =PI Sta 130+14.2 Ia 153 AB Plans

CP 104
 =PI Sta 158+85.017 This Survey
 =PI Sta 158+84.6 Ia 153 AB Plans

VERTICAL CONTROL

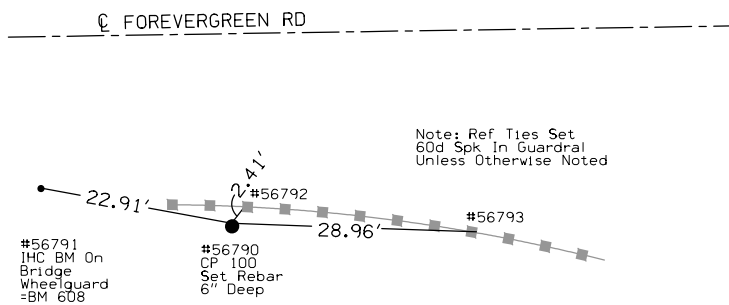
West Forevergreen Rd Stationing BENCHMARKS

				ELEVATION	
No. 636 Sta.	61284+29.098	58.441 Lt. Y:	633364.331 X:2148891.452	Set RR Spk S Side PP-----	782.476
No. 637 Sta.	61290+28.323	27.984 Lt. Y:	633342.785 X:2149490.492	Set RR Spk S Side PP-----	800.033
No. 638 Sta.	61297+79.576	29.256 Lt. Y:	633355.687 X:2150241.635	Set RR Spk S Side PP-----	772.659
No. 639 Sta.	61305+88.321	30.207 Lt. Y:	633369.157 X:2151050.269	Set RR Spk S Side PP-----	779.646
No. 640 Sta.	61313+09.906	31.292 Lt. Y:	633381.843 X:2151771.669	Set RR Spk S Side PP-----	774.634
No. 641 Sta.	61321+73.594	33.198 Lt. Y:	633399.164 X:2152635.185	Fd RR Spk S Side PP-----	750.685
No. 642 Sta.	61326+40.585	33.867 Lt. Y:	633408.168 X:2153102.090	Fd RR Spk S Side PP-----	755.425
No. 643 Sta.	61331+30.345	35.405 Lt. Y:	633418.447 X:2153591.745	Fd RR Spk S Side PP-----	758.866
No. 644 Sta.	61337+12.068	27.789 Rt. Y:	633365.646 X:2154174.503	Cut X SE Bolt F Hyd-----	753.786
No. 645 Sta.	61343+21.111	28.132 Lt. Y:	633423.856 X:2154782.989	Set RR Spk S Side PP-----	774.215
No. 646 Sta.	61348+50.346	29.168 Lt. Y:	633425.788 X:2155312.222	Set RR Spk S Side PP-----	771.919
No. 647 Sta.	61358+51.817	28.203 Lt. Y:	633426.505 X:2156313.693	Set RR Spk S Side PP-----	778.545
No. 648 Sta.	61371+61.573	42.304 Lt. Y:	633441.363 X:2157623.509	Set RR Spk S Side PP-----	782.625
No. 650 Sta.	61365+61.016	28.543 Lt. Y:	633427.791 X:2157022.948	Cut X NW Bolt F Hyd-----	783.793

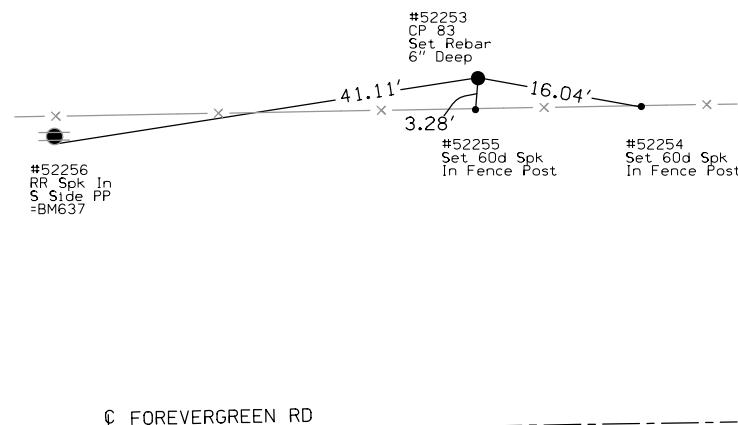
Ia 965 Stationing BENCHMARKS

				ELEVATION	
No. 649 Sta.	140+01.907	1204.768 Rt. Y:	633379.066 X:2158131.345	Set MAG NAIL on 5x4 intake-----	778.695

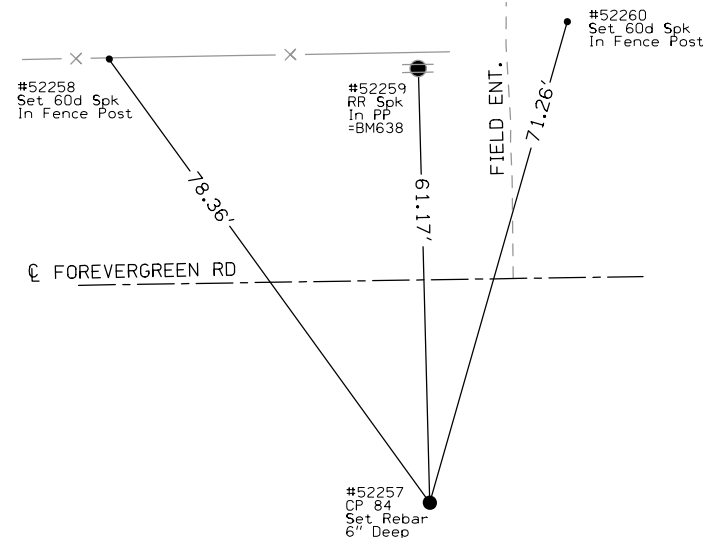
C.P. STA 61272+69.94 RIGHT 19.43
 C.P. 100, Set 5/8" Rebar 6" Deep
 N=633280.186 E=2147732.728



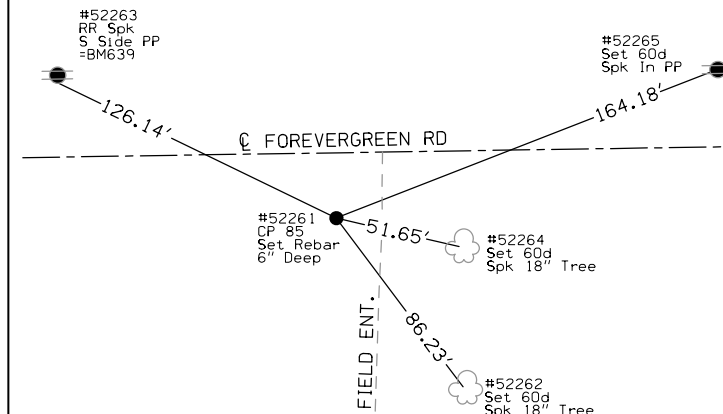
C.P. STA 61290+69.03 LEFT 33.73
 C.P. 83, Set 5/8" Rebar 6" Deep
 N=633349.161 E=2149531.105



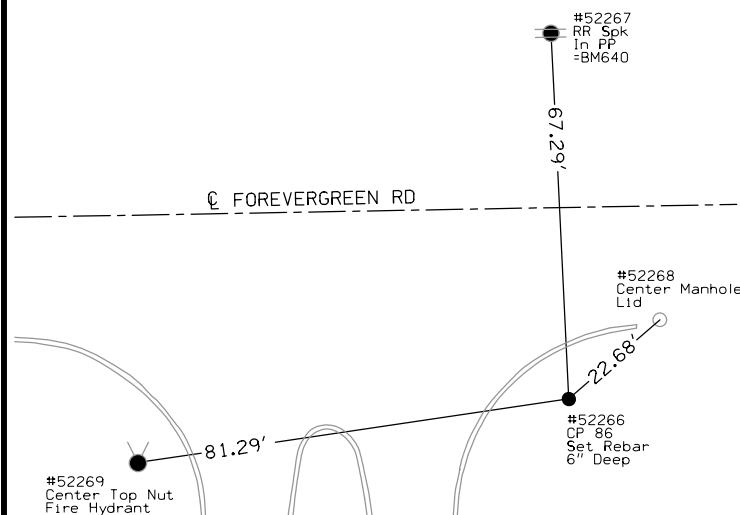
C.P. STA 61297+80.24 RIGHT 31.91
 C.P. 84, Set 5/8" Rebar 6" Deep
 N=633294.538 E=2150243.245



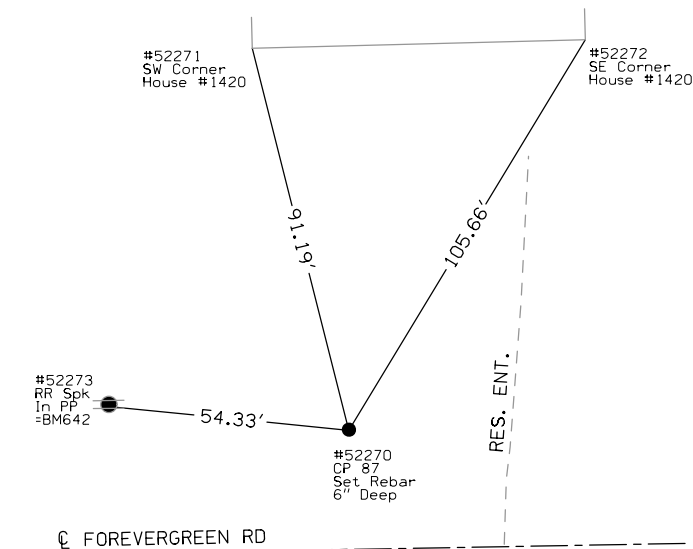
C.P. STA 61307+00.91 RIGHT 26.68
 C.P. 85, Set 5/8" Rebar 6" Deep
 N=633314.025 E=2151163.721



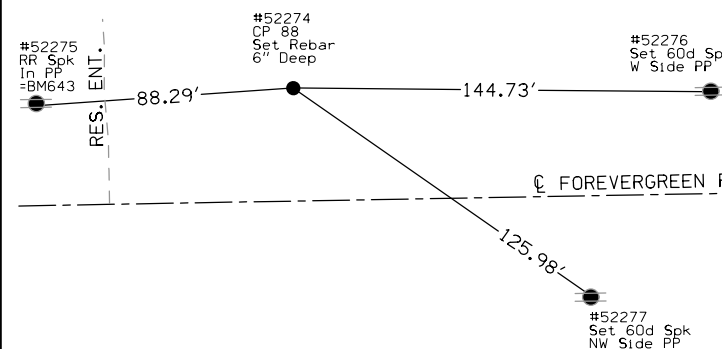
C.P. STA 61313+11.95 RIGHT 35.97
 C.P. 86, Set 5/8" Rebar 6" Deep
 N=633314.628 E=2151774.909



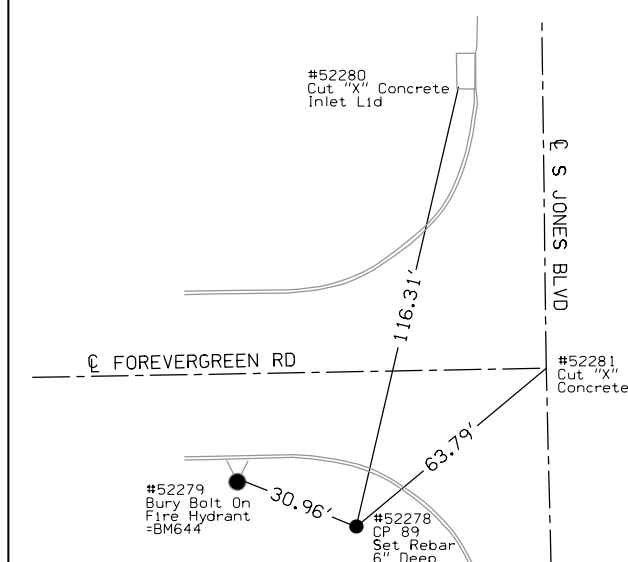
C.P. STA 61326+94.54 LEFT 27.58
 C.P. 87, Set 5/8" Rebar 6" Deep
 N=633402.840 E=2153156.153



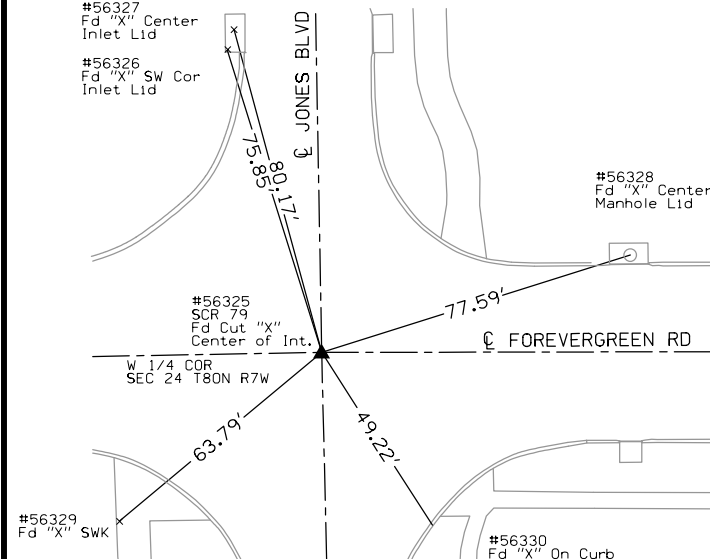
C.P. STA 61332+18.52 LEFT 39.90
 C.P. 88, Set 5/8" Rebar 6" Deep
 N=633424.519 E=2153679.828



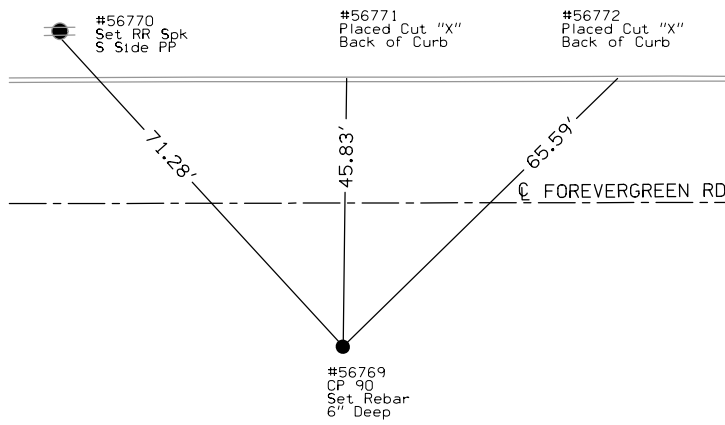
C.P. STA 61337+40.52 RIGHT 39.99
 C.P. 89, Set 5/8" Rebar 6" Deep
 N=633353.952 E=2154203.169



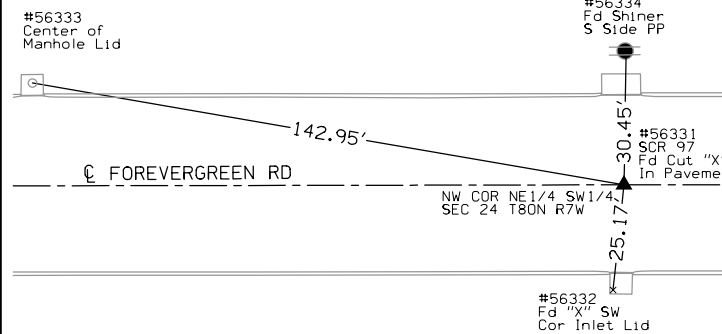
SCR STA 61337+90.21 LEFT 00.00
 SCR 79, Fd Cut "X" In Slab
 N=633394.825 E=2154252.139



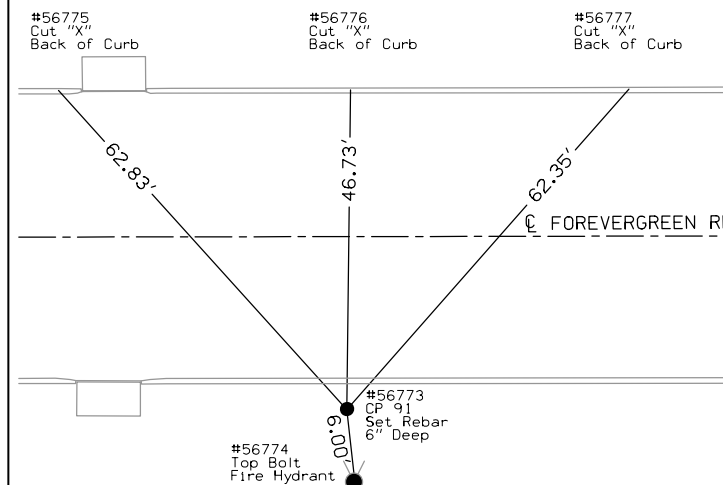
C.P. STA 61343+69.19 RIGHT 24.53
 C.P. 90, Set 5/8" Rebar 6" Deep
 N=633371.278 E=2154831.157

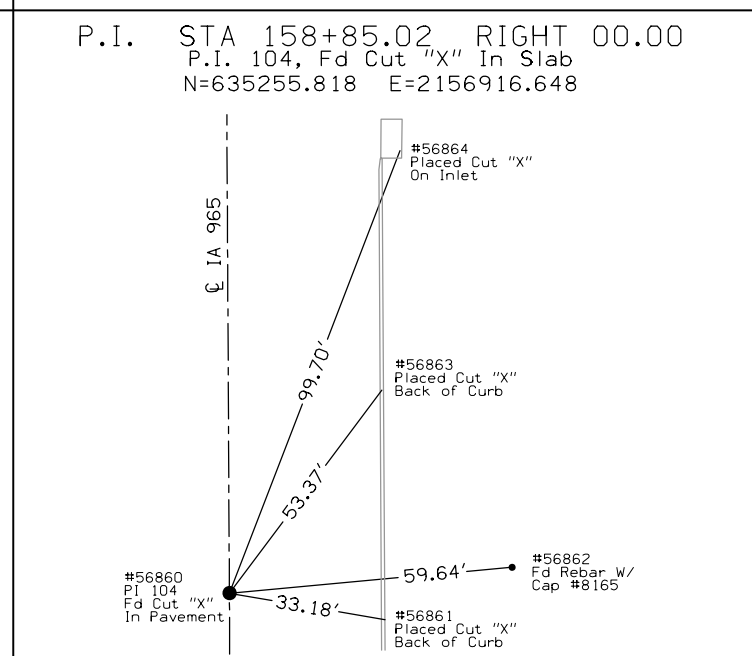
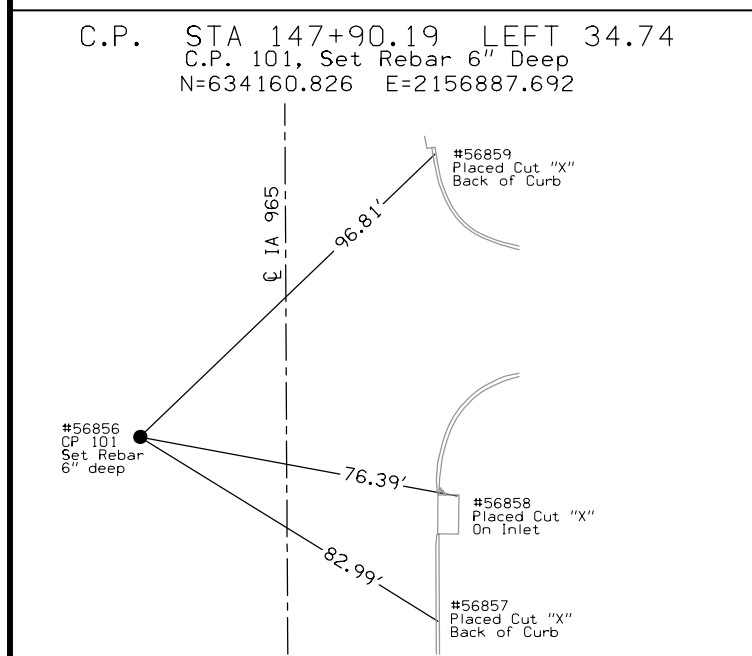
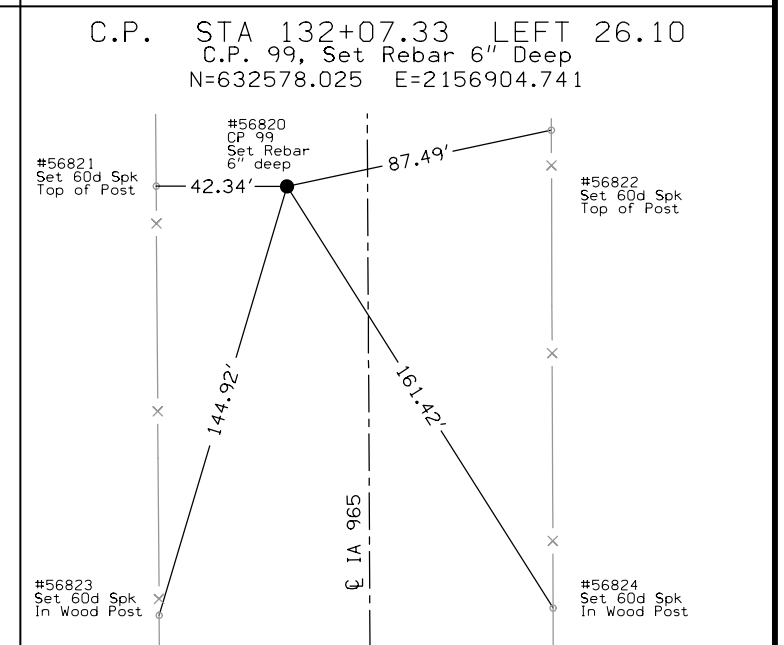
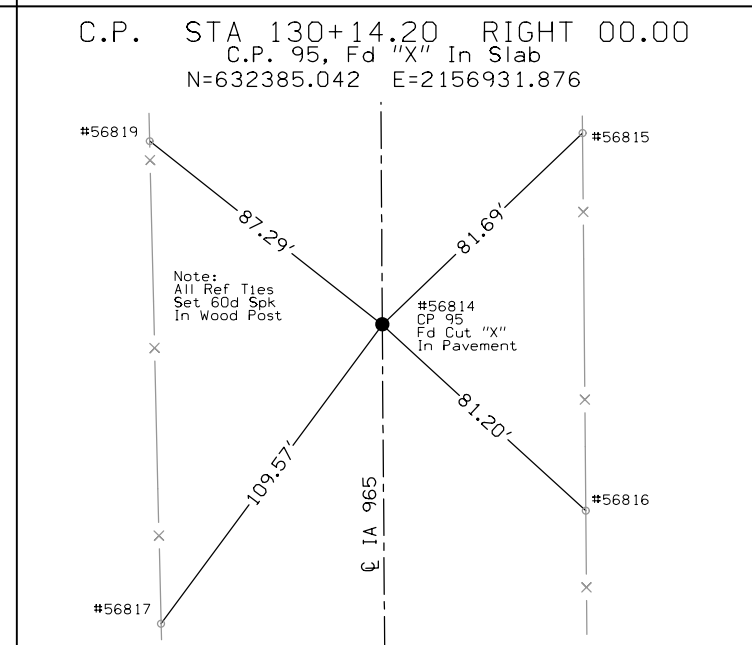
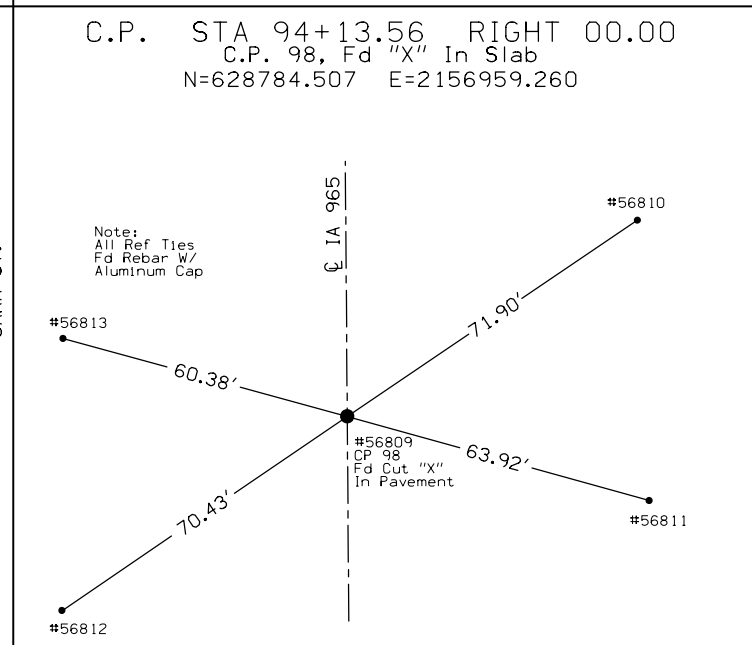
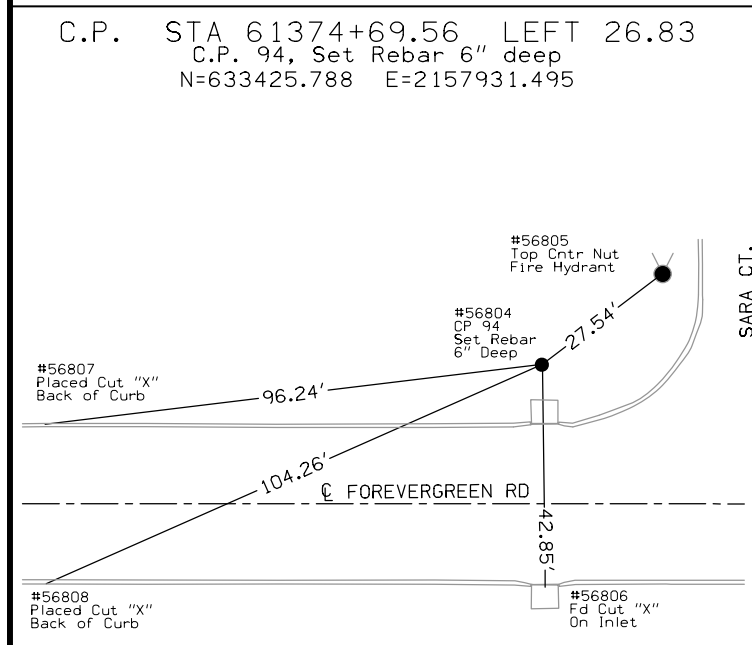
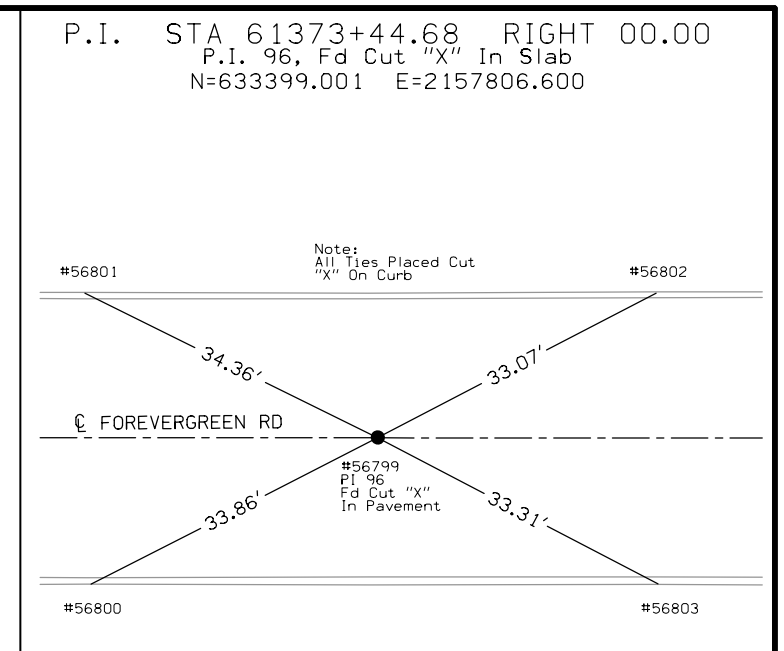
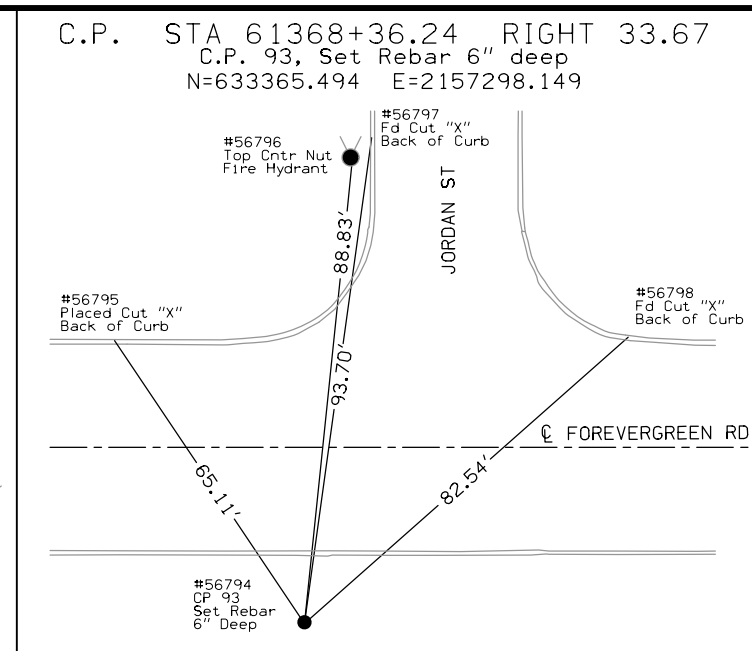
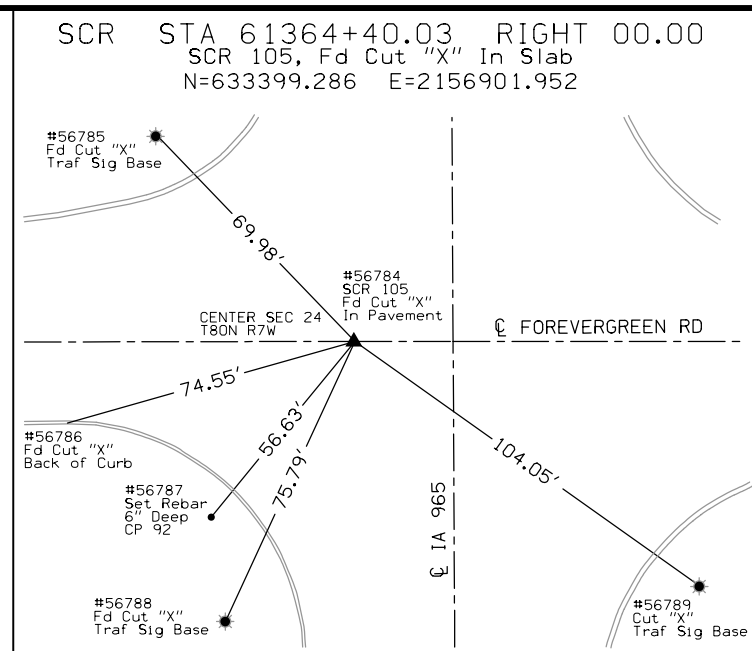
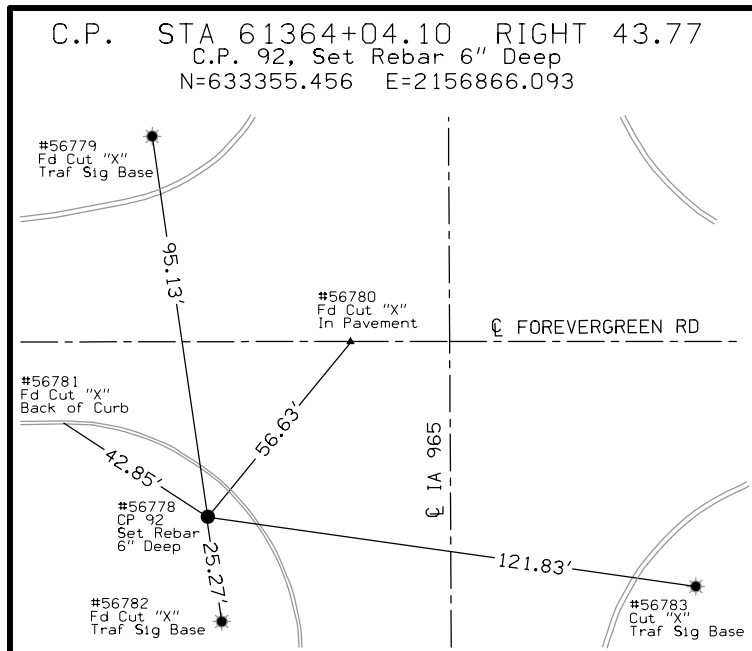


SCR STA 61351+15.23 RIGHT 00.00
 SCR 97, Fd Cut "X" In Slab
 N=633397.069 E=2155577.150



C.P. STA 61353+65.98 RIGHT 25.29
 C.P. 91, Set Rebar 6" Deep
 N=633372.203 E=2155827.944





Johnson	ROW: IMN-380-6(344)2--0E-52					PIN	16-52-380-010										
	W Forevergreen Rd in North Liberty from E of Kansas Ave to S Jones Blvd																
		STATE		COUNTY		CITY		BORROW									
PARCEL NO.	OWNER NAME	FEE	EASE	FEE	EASE	FEE	EASE	EXCESS	FEE	T.E.	MITIGATION	OTHER	HOUSE	BUILDING(S)	A/C ONLY	TOTAL ACQ.	
12	Bartel Irvin Ruba - Fee											.05 AC					
13	Thomas L. Dornbush - Fee											.05 AC					
14	Ellis Rodney Shultz - Fee											.05 AC					
15	Matthew W. Moeller - Fee Sylwia Szemplinska - Fee						0.09 AC					.12 AC					
16	Paul Allen Lang - Fee											.05 AC					
17	Brian Patrick Wayson - Fee											.05 AC					
18	Arthur C. Chipman - Fee						0.02 AC					.13 AC					
19	Julia Oxley - Fee											.06 AC					
21	Donna J. Young - Fee											.04 AC					
22	Roberta Renee Berdo - Fee											.04 AC					
23	Keystone North Liberty, LC - Fee						0 AC										
24	Hodge Construction Company - Fee											0 AC					
12 Parcels	"TOTALS	0 AC	0 AC	0 AC	0 AC	0 AC	0.11 AC	0 AC	0 AC	0 AC	0 AC						
		0 SF	0 SF	0 SF	0 SF	0 SF	0 SF	0 SF	0 SF	0 SF	0 SF						

PERMANENT EASEMENT FOR UTILITY PURPOSES
313+29 LT. TO 329+11 LT.

BARTEL IRVIN RUBA

THOMAS L. &
CAROL A. DORNBUSH

ELLIS RODNEY SHULTZ

MATTHEW W. MOELLER &
SYLWIA SZEMPLINSKA

PAUL ALLEN LANG

BRIAN PATRICK WAYSON
SHEILA MARGARET WAYSON

ARTHUR COLUCCI

12

13

14

15

16

17

TEMPORARY EASEMENT TO
SHAPE & CONSTRUCT ENTRANCE

TEMPORARY EASEMENT TO
SHAPE & CONSTRUCT ENTRANCE

TEMPORARY EASEMENT TO
SHAPE & CONSTRUCT ENTRANCE

TEMPORARY EASEMENT TO
SHAPE & CONSTRUCT ENTRANCE

TEMPORARY EASEMENT TO
SHAPE & CONSTRUCT ENTRANCE

TEMPORARY EASEMENT TO
SHAPE & CONSTRUCT ENTRANCE

TE
SHAPE

313+29±P
±65'
313+29±P
±60'

314+25
±76'
313+85
±76'
313+85
±60'

314+25
±60'

315+70
±76'
315+35
±76'
315+35
±60'

315+70
±60'

316+91
±67'
316+75
±67'
316+75
±60'

316+04±P
±65'
316+04±P
±60'

317+42±P
±65'
317+42±P
±60'

319+40
±50'±EX.R/W
318+55
±60'

319+05
±115'
318+90
±125'

320+20
±50'±EX.R/W
319+75
±100'

320+78±P
±90'
320+78±P
±65'

321+10
±80'
321+40
±76'
321+75
±76'

322+16±P
±65'
322+16±P
±62'

323+16
±75'
322+88
±76'
322+87
±61'

323+54±P
±65'
323+54±P
±61'

324+80±
±65'
324+80±
±62'

Forevergreen Road

Forevergreen Road

CITY OF NORTH
LIBERTY

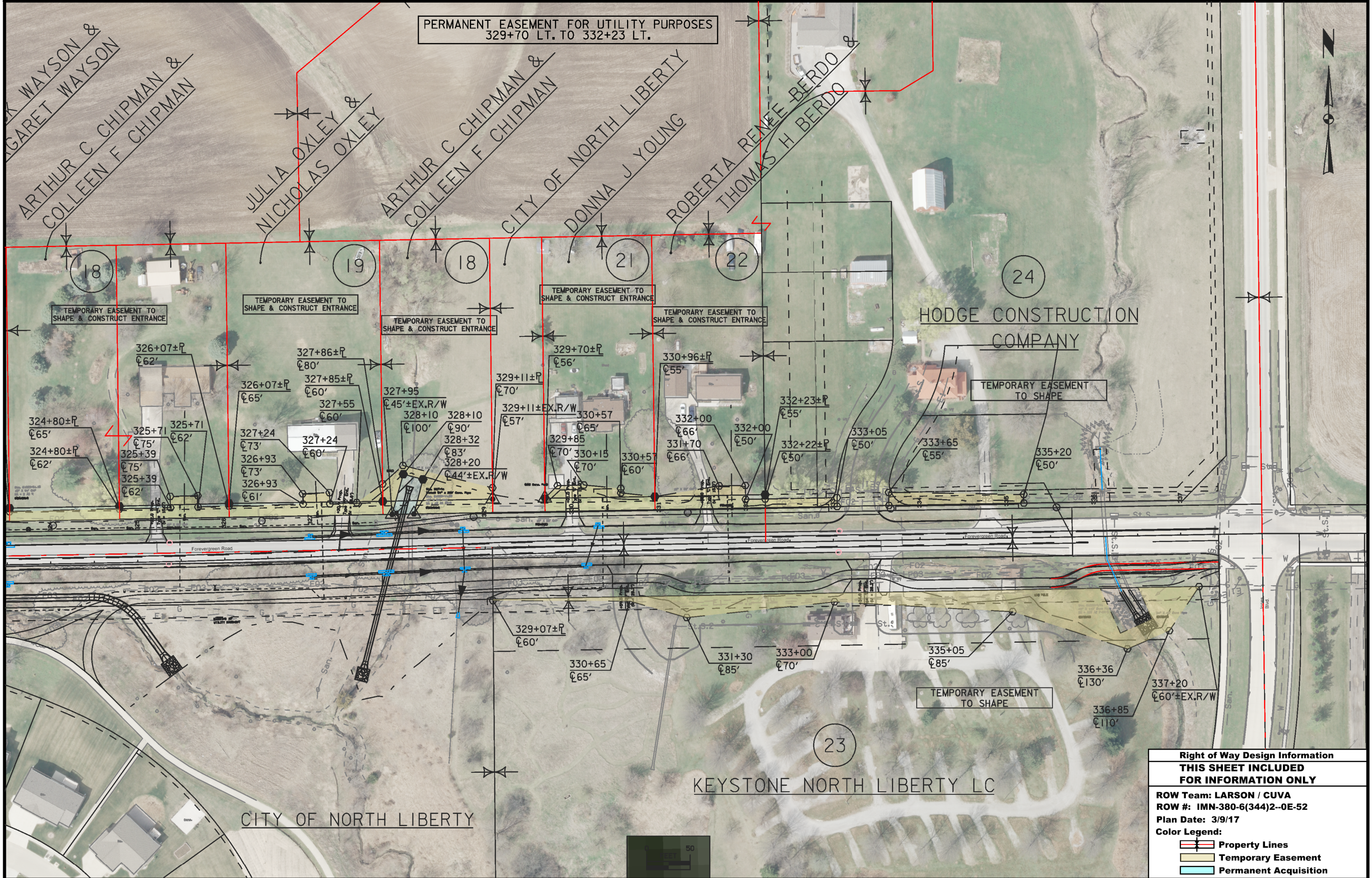
Right of Way Design Information
THIS SHEET INCLUDED
FOR INFORMATION ONLY

ROW Team: LARSON / CUVA
ROW #: IMN-380-6(344)2--0E-52
Plan Date: 3/9/17

Color Legend:
Property Lines
Temporary Easement
Permanent Acquisition



PERMANENT EASEMENT FOR UTILITY PURPOSES
329+70 LT. TO 332+23 LT.



Right of Way Design Information	
THIS SHEET INCLUDED FOR INFORMATION ONLY	
ROW Team: LARSON / CUVA	
ROW #: IMN-380-6(344)2--0E-52	
Plan Date: 3/9/17	
Color Legend:	
	Property Lines
	Temporary Easement
	Permanent Acquisition





Forevergreen Road



ROAD CLOSED TO THRU TRAFFIC

R11-4
60" x 30"



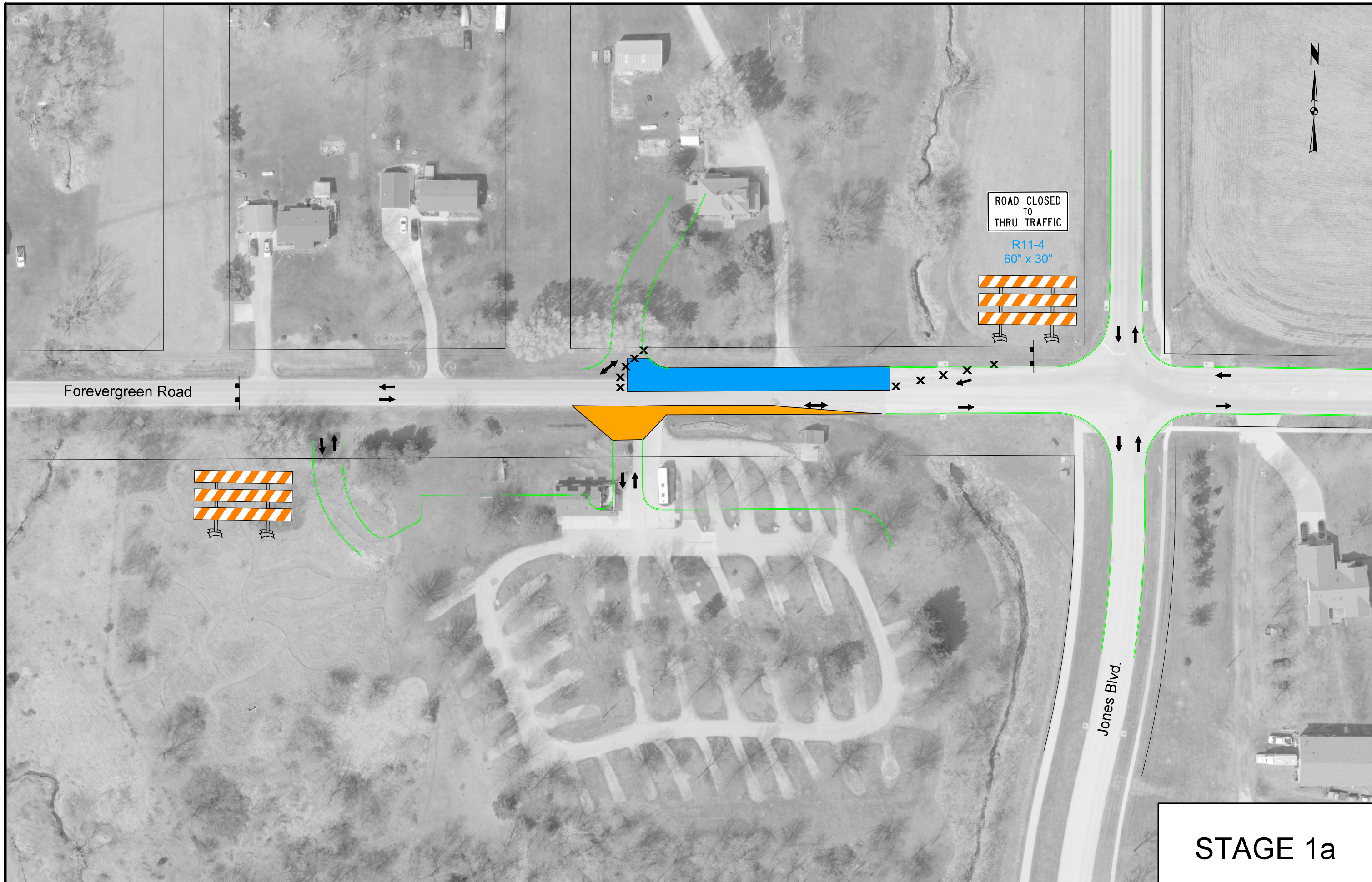
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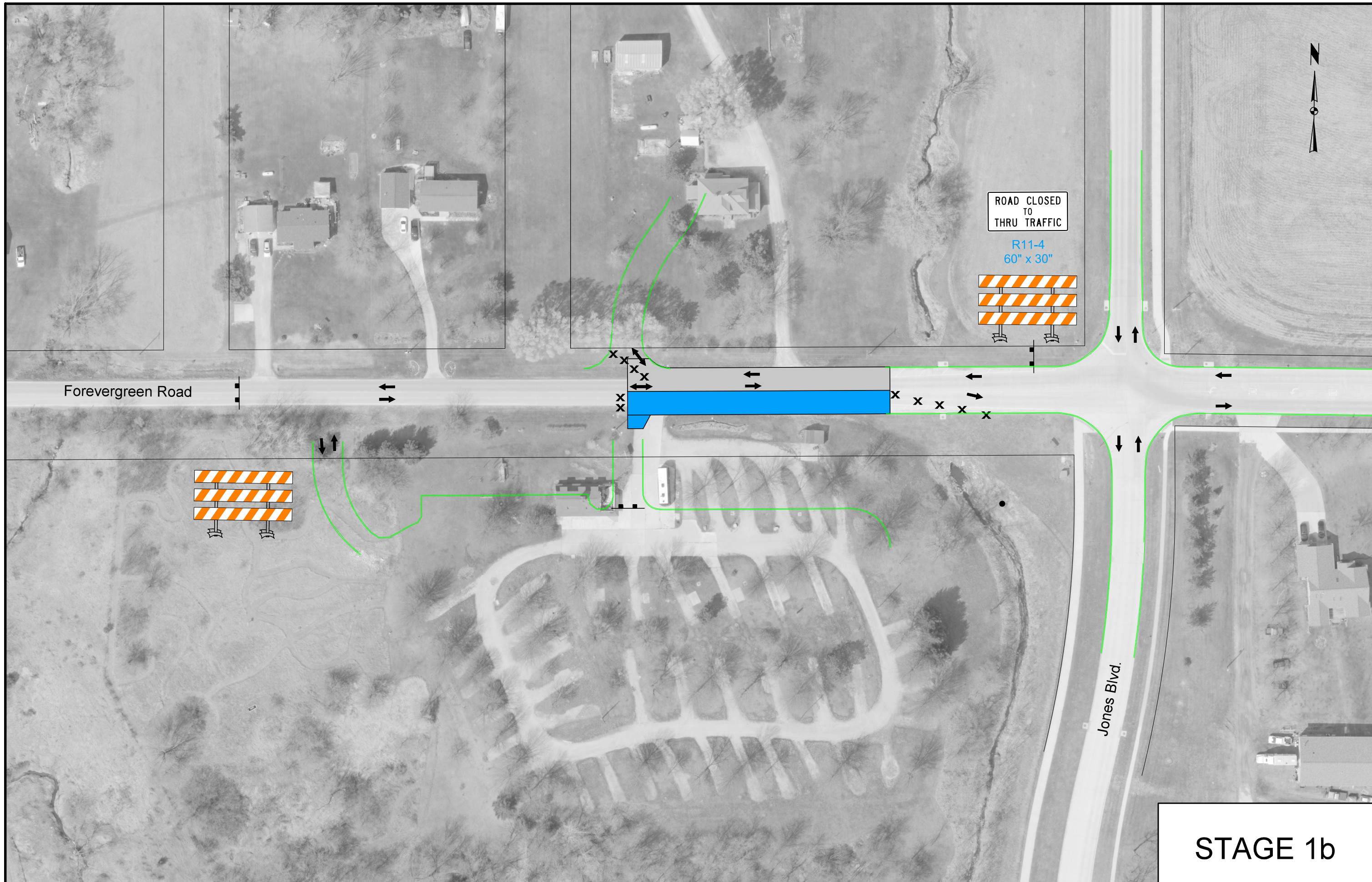


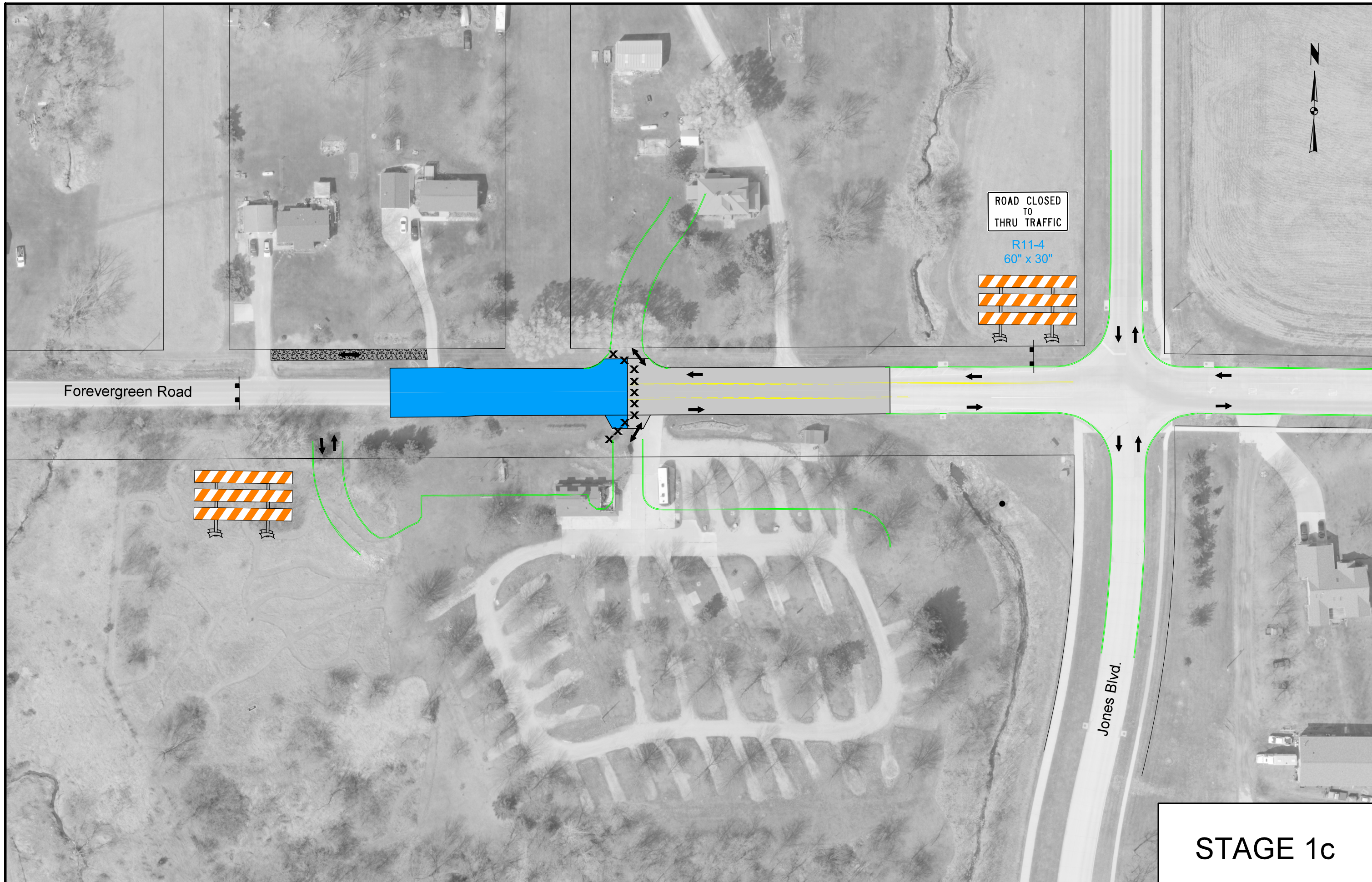
STAGE 1



STAGE 1



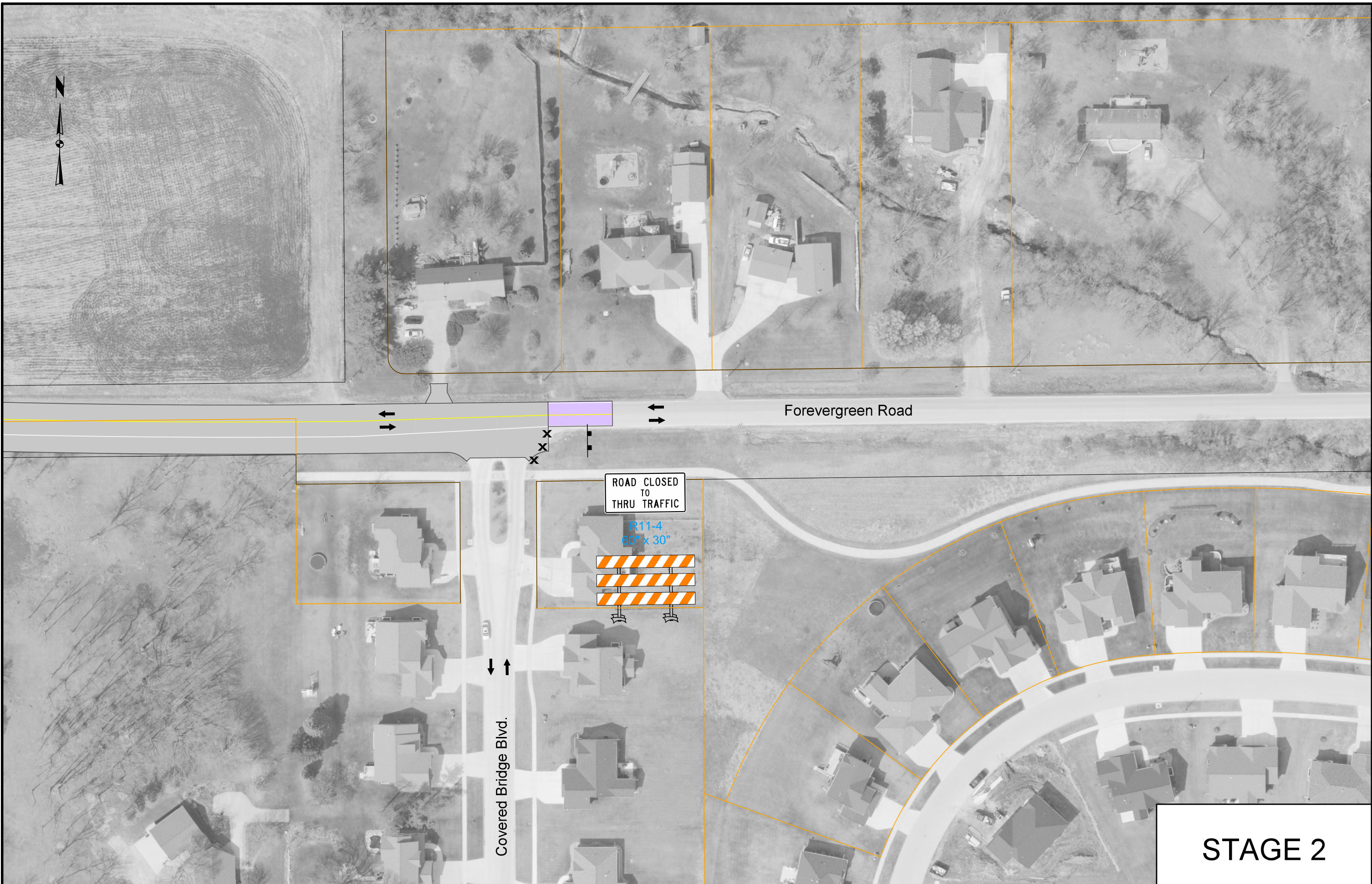




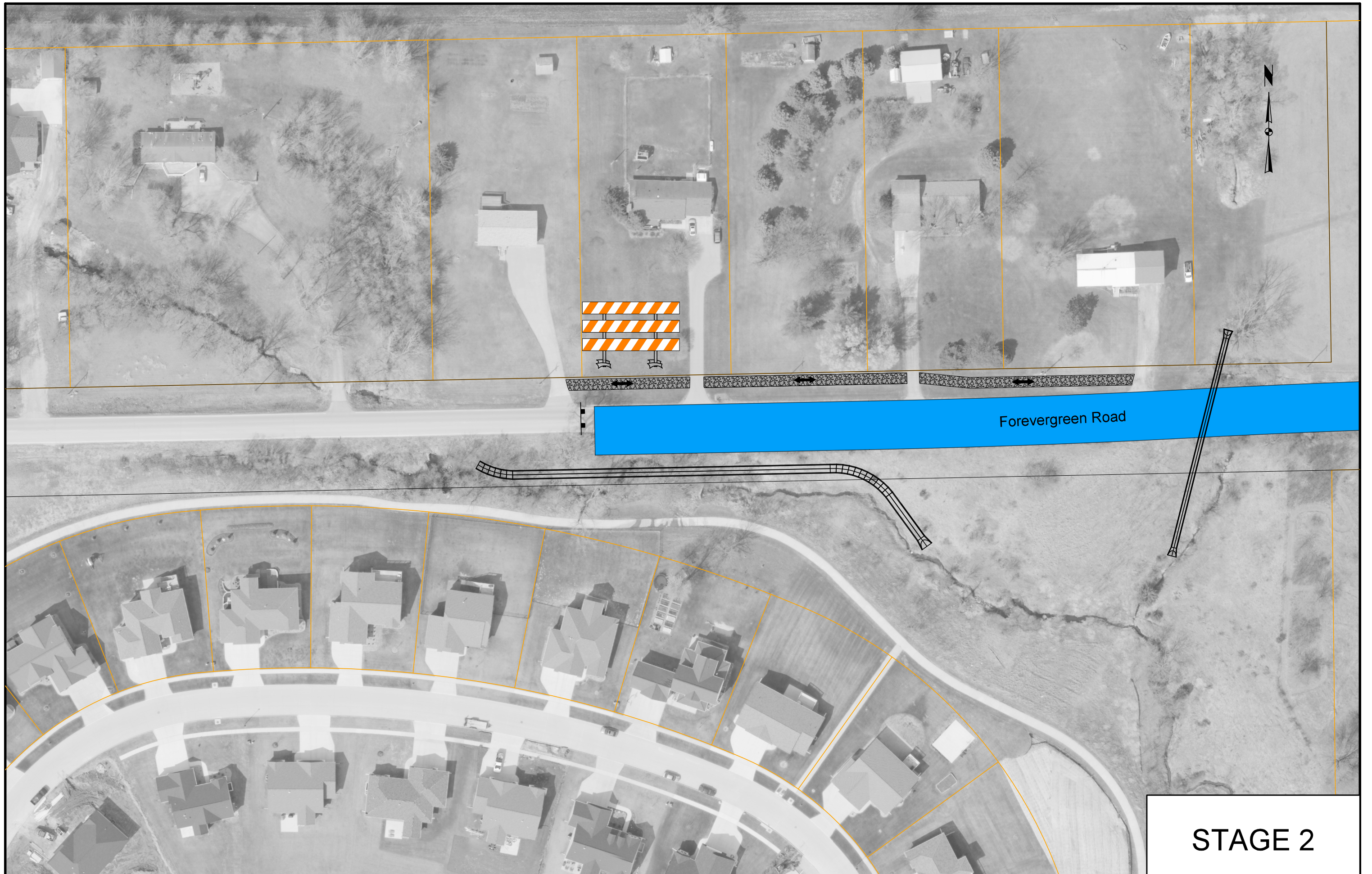
Forevergreen Road

Jones Blvd.

STAGE 1c

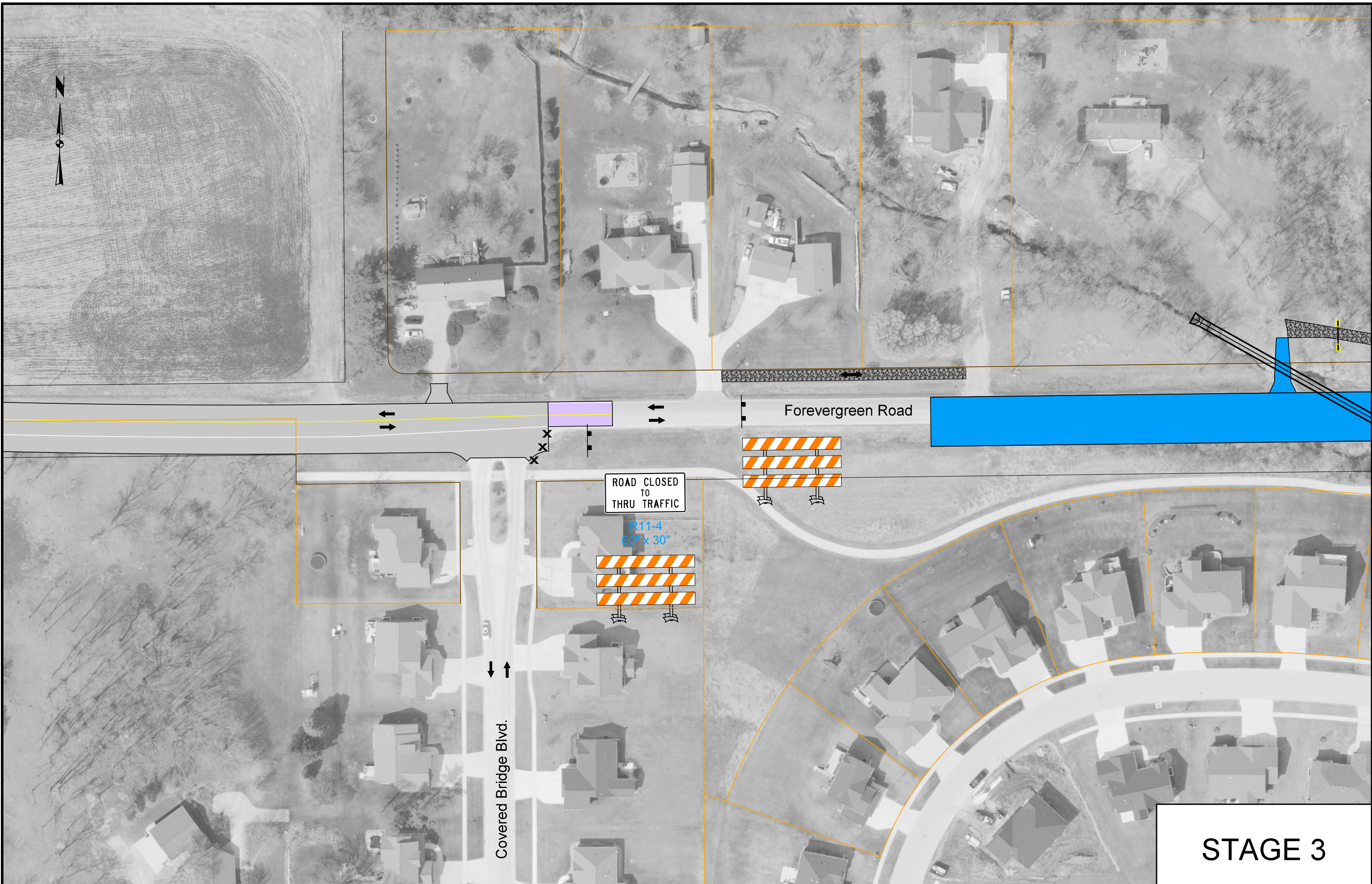


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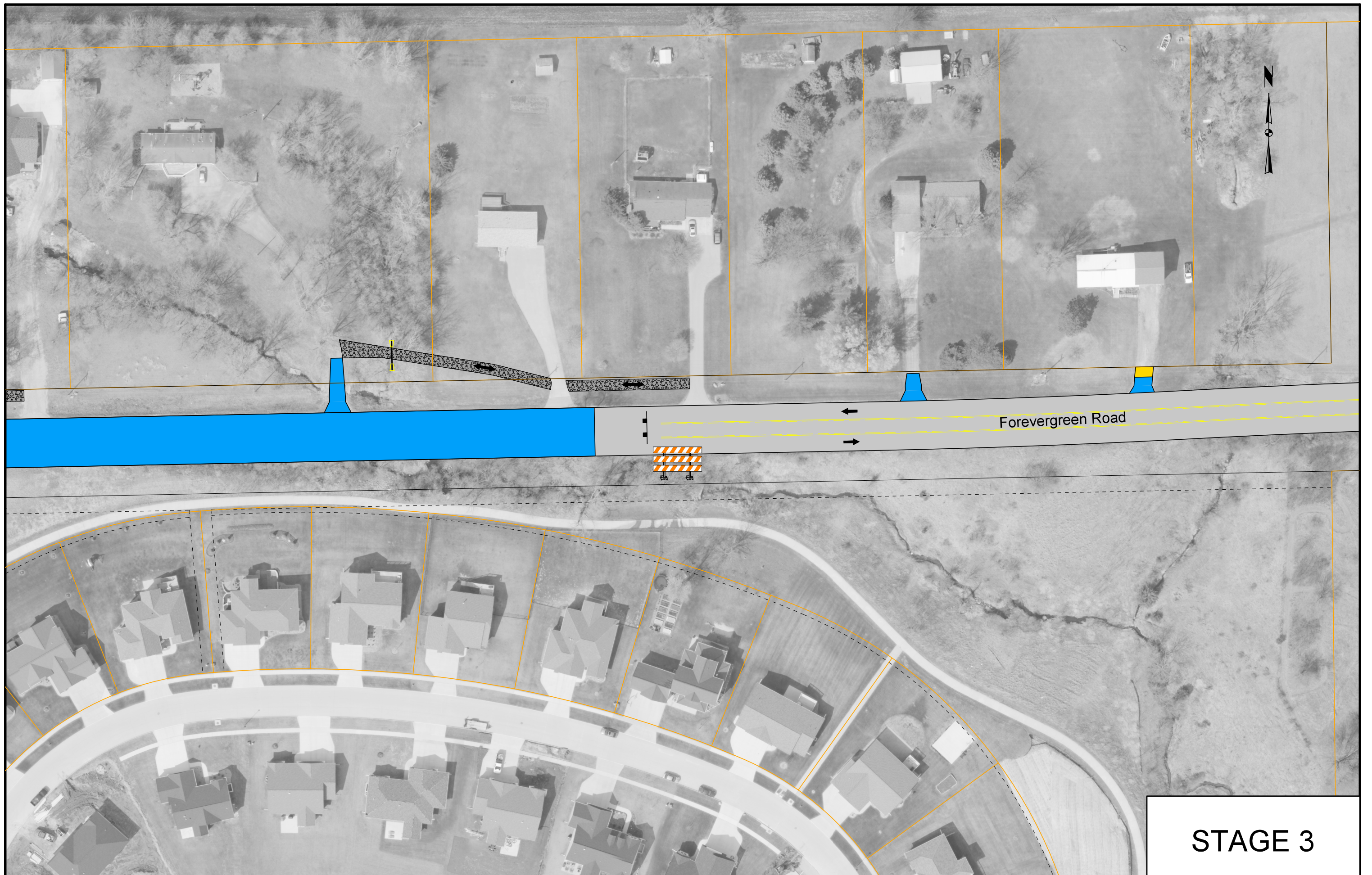


STAGE 2





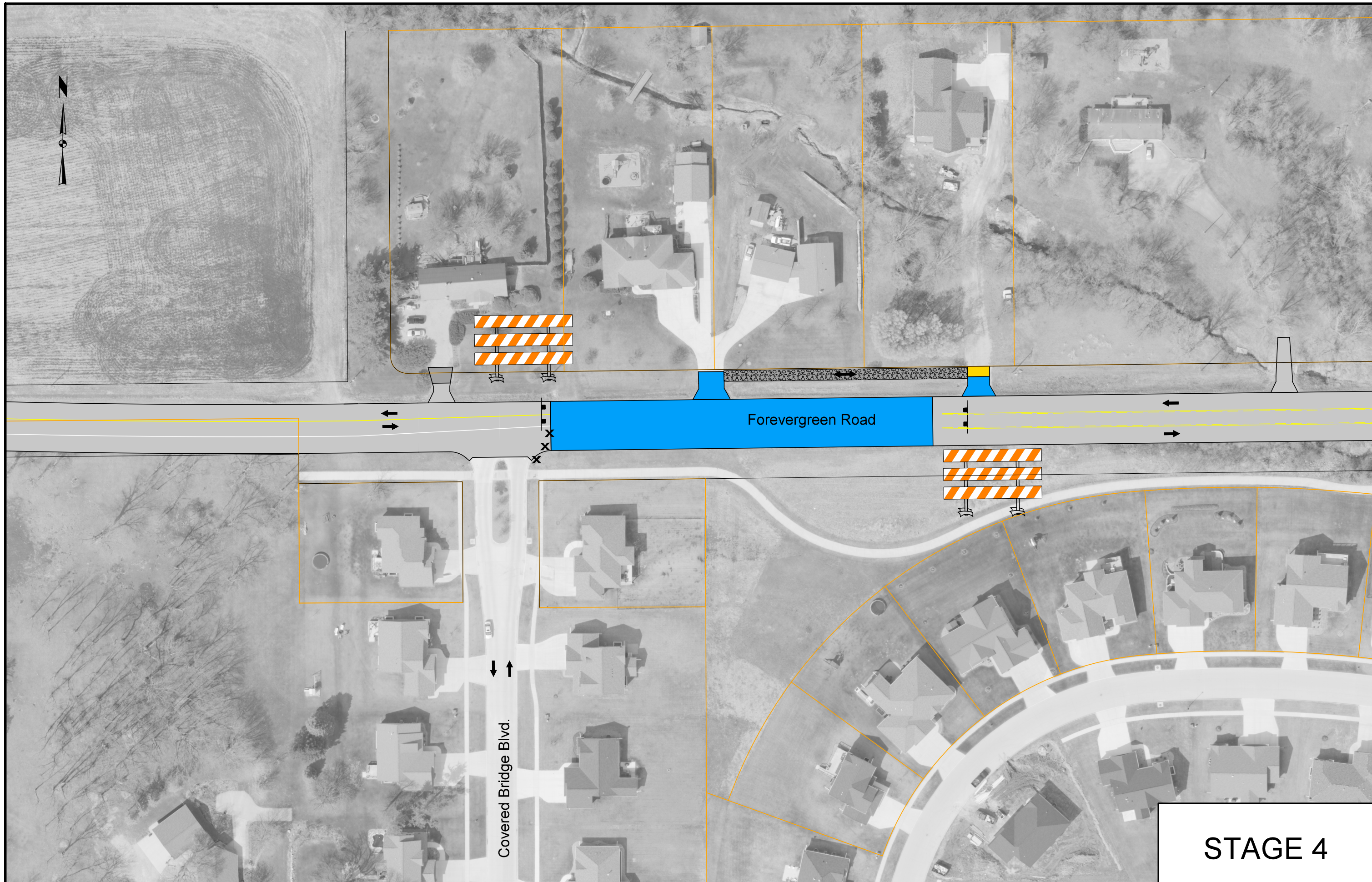
STAGE 3



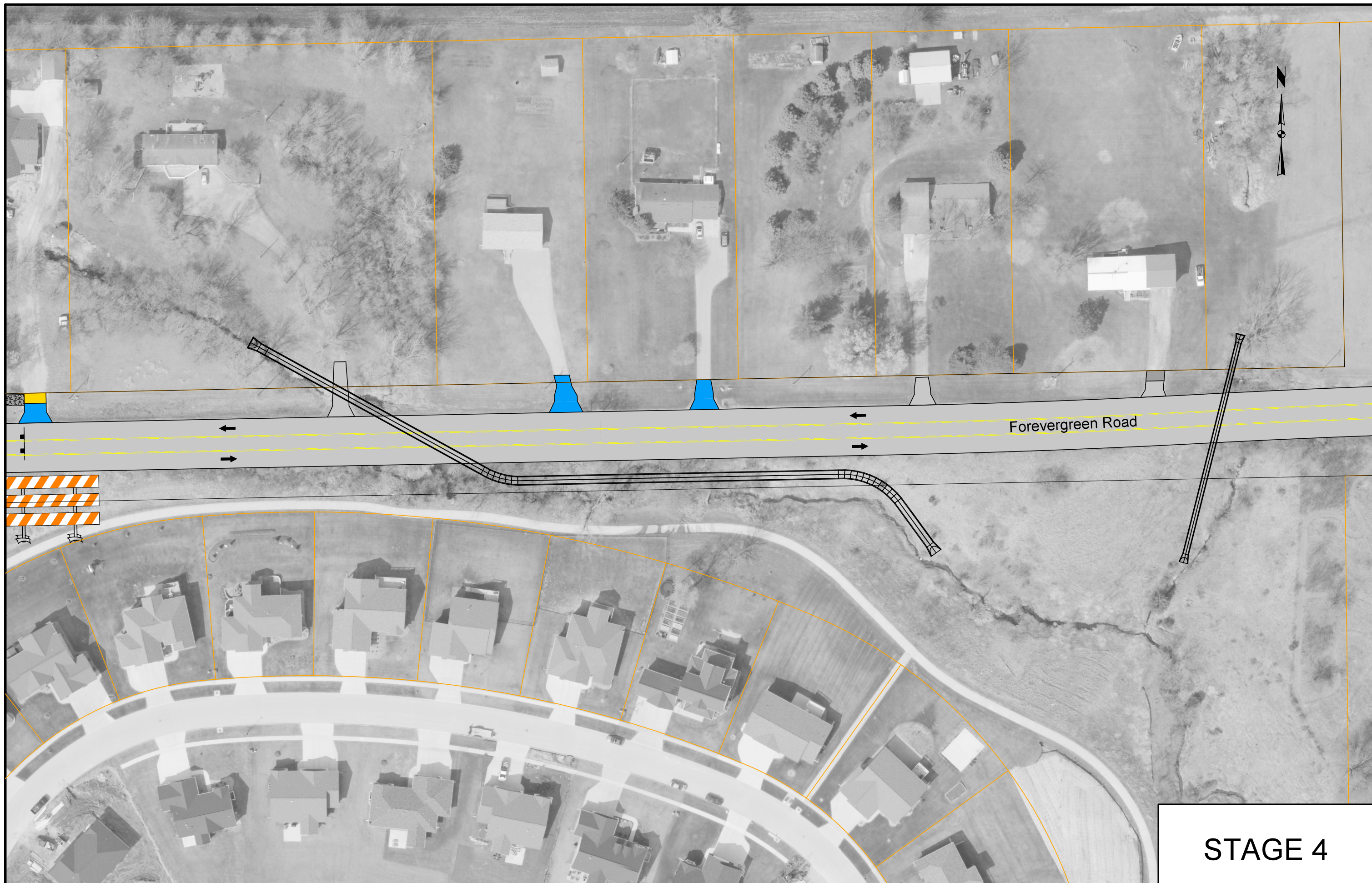
STAGE 3



STAGE 3



STAGE 4

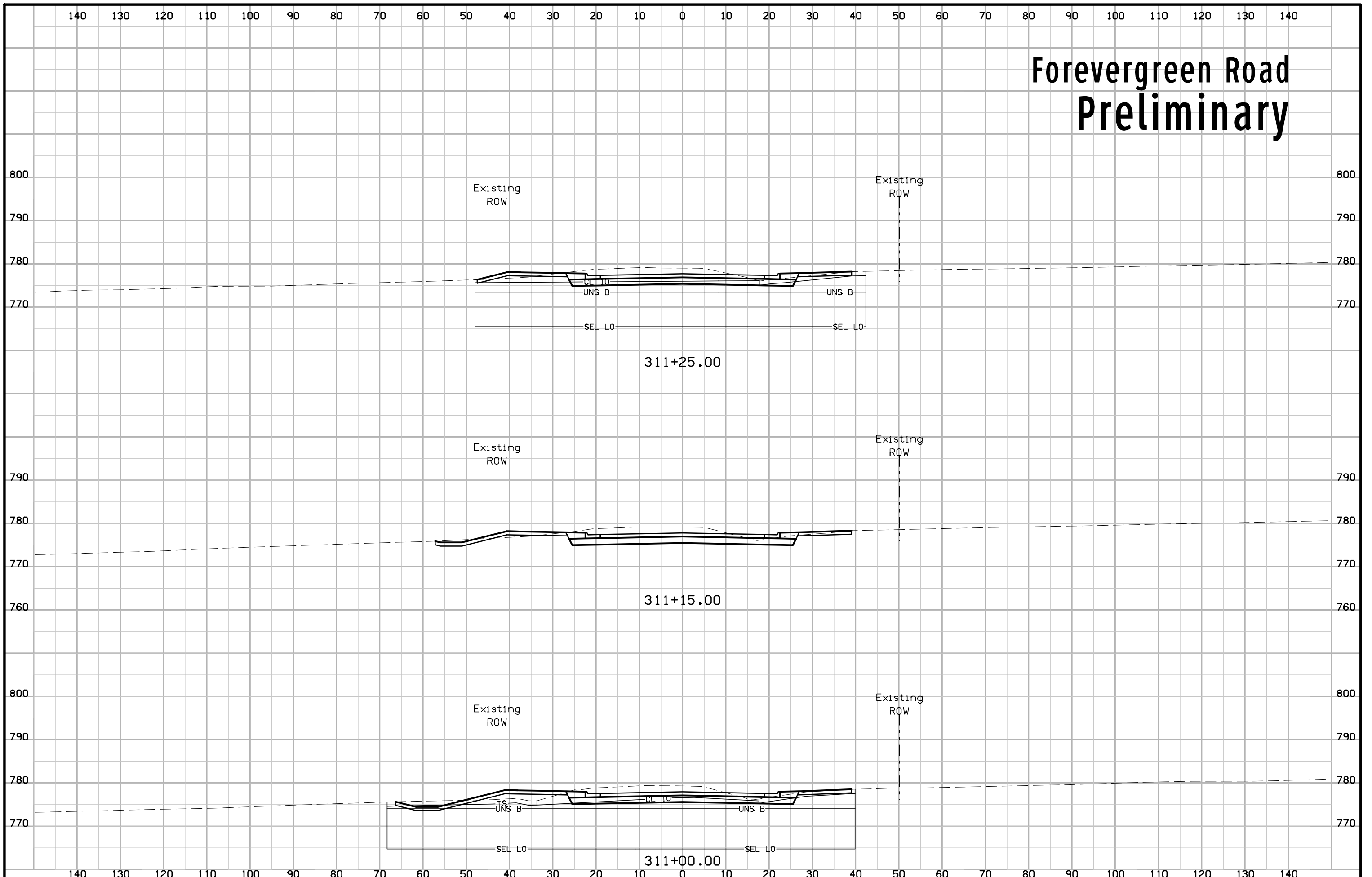


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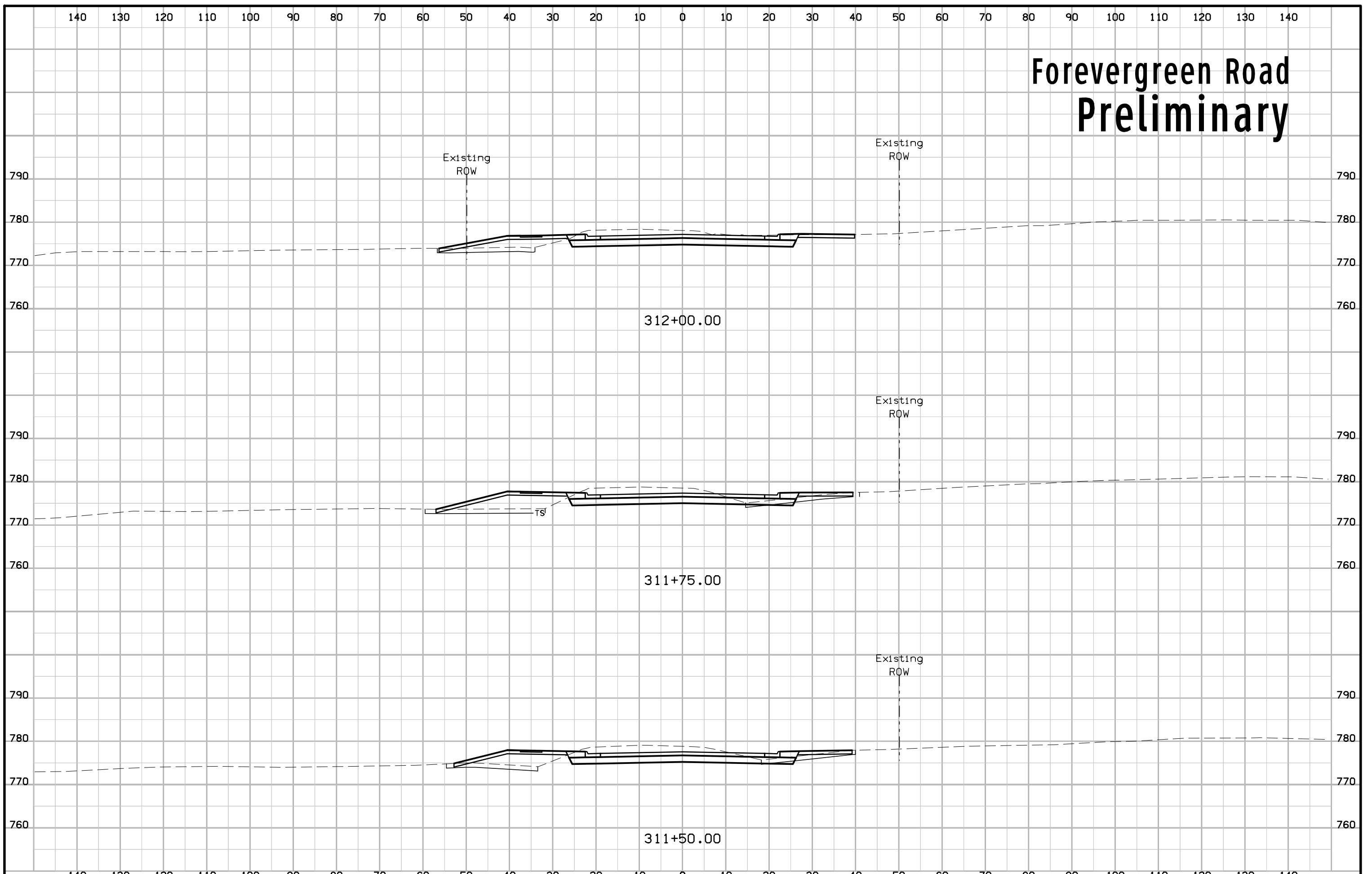


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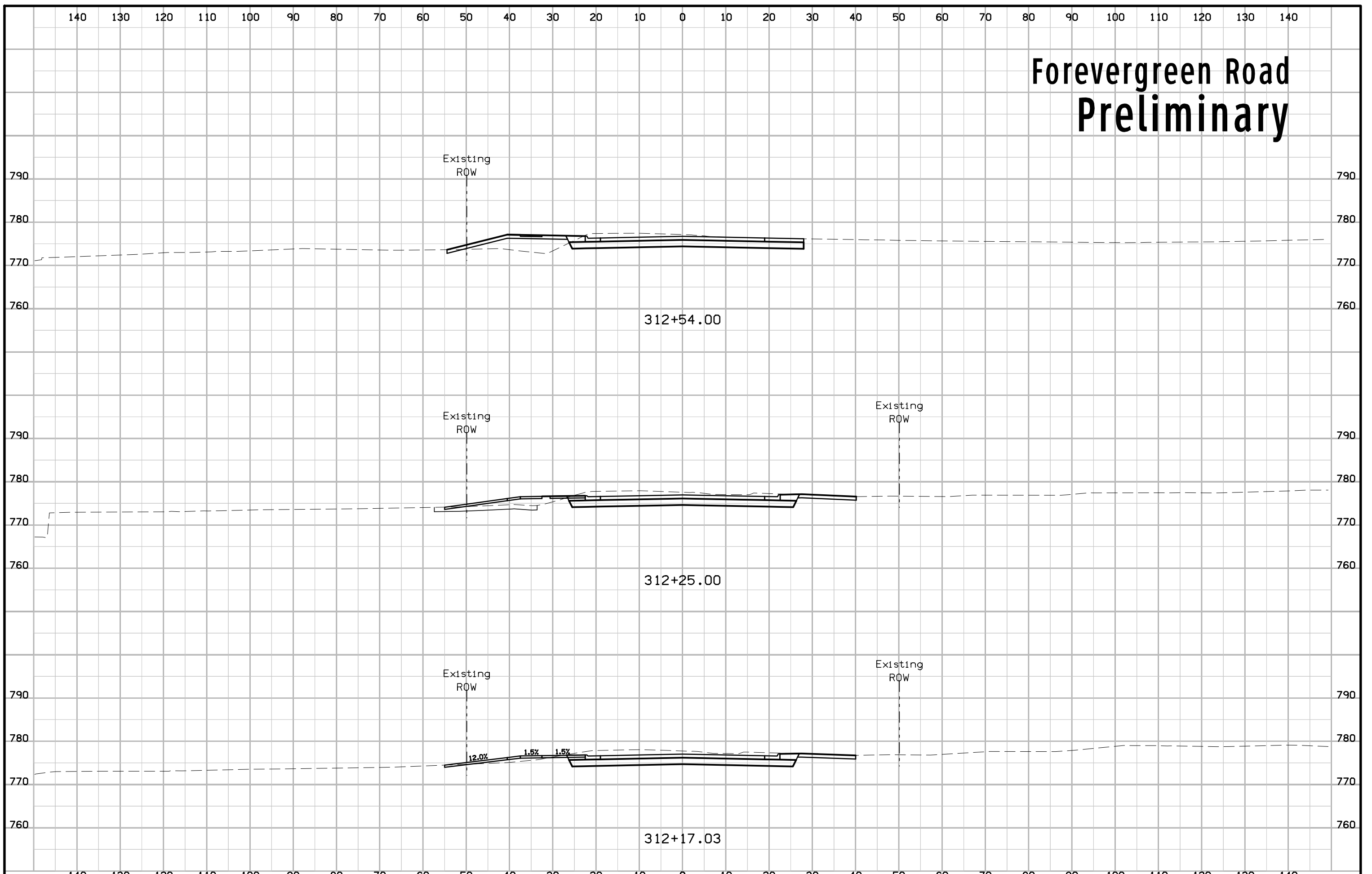
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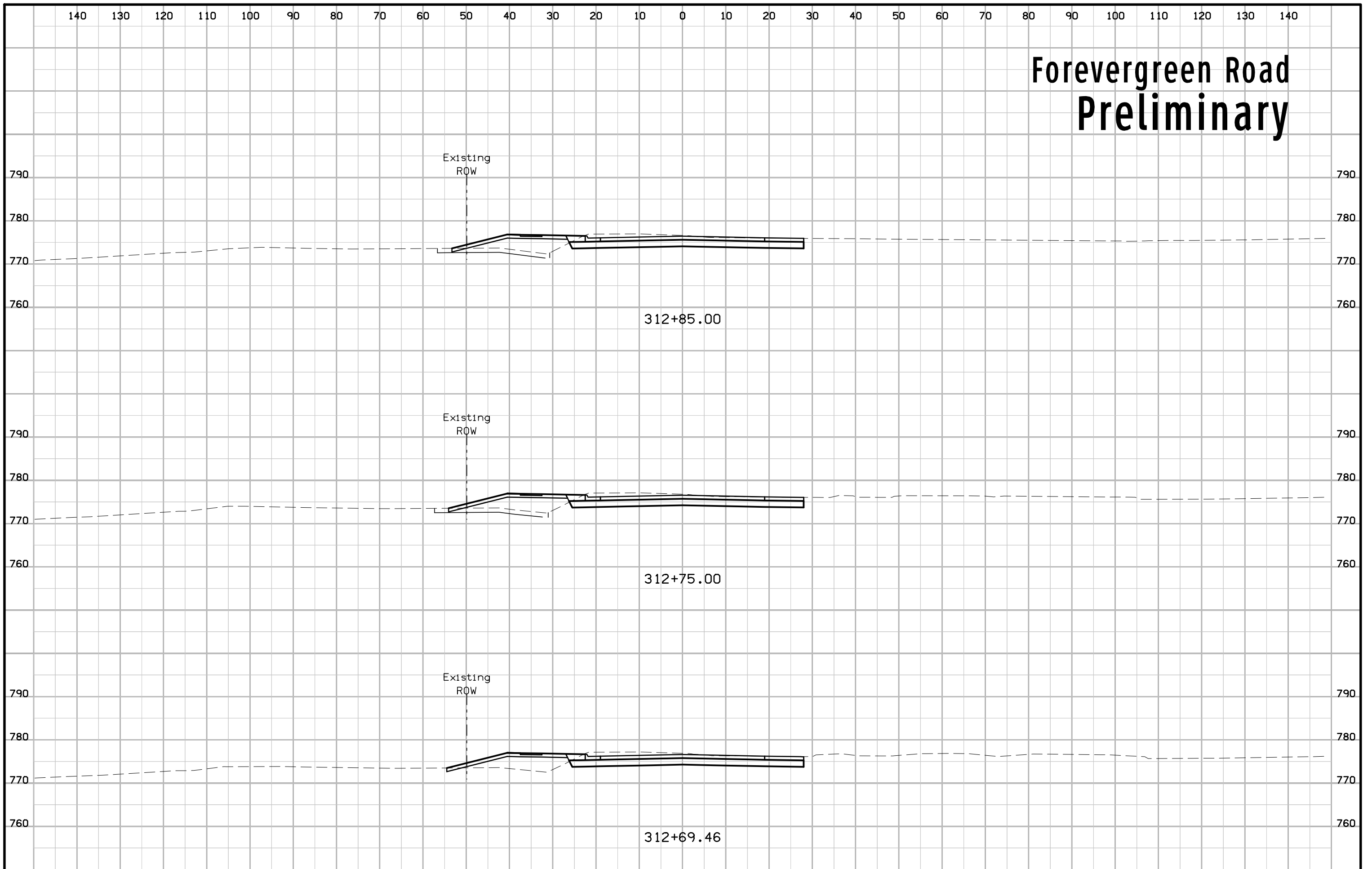
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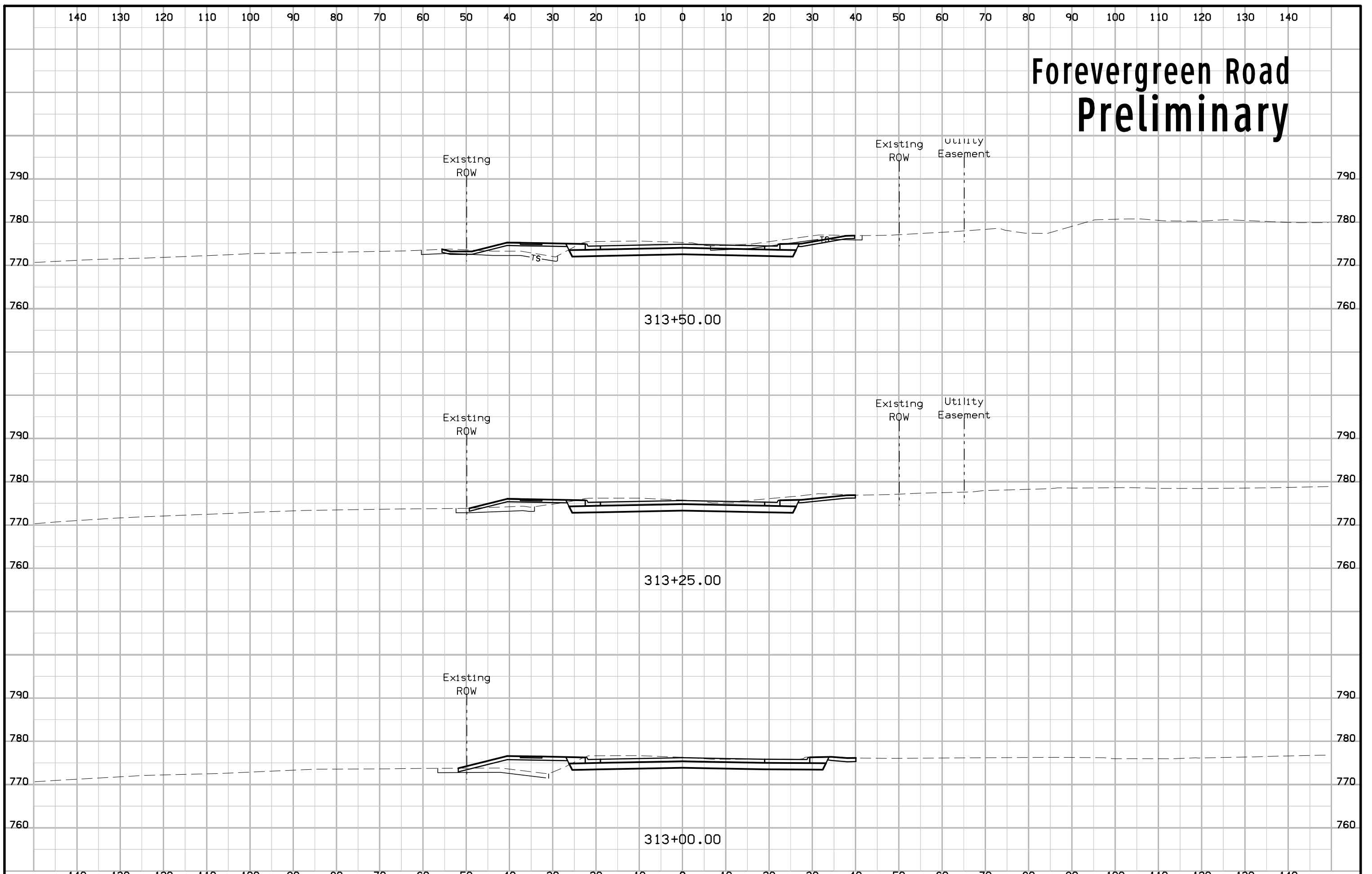
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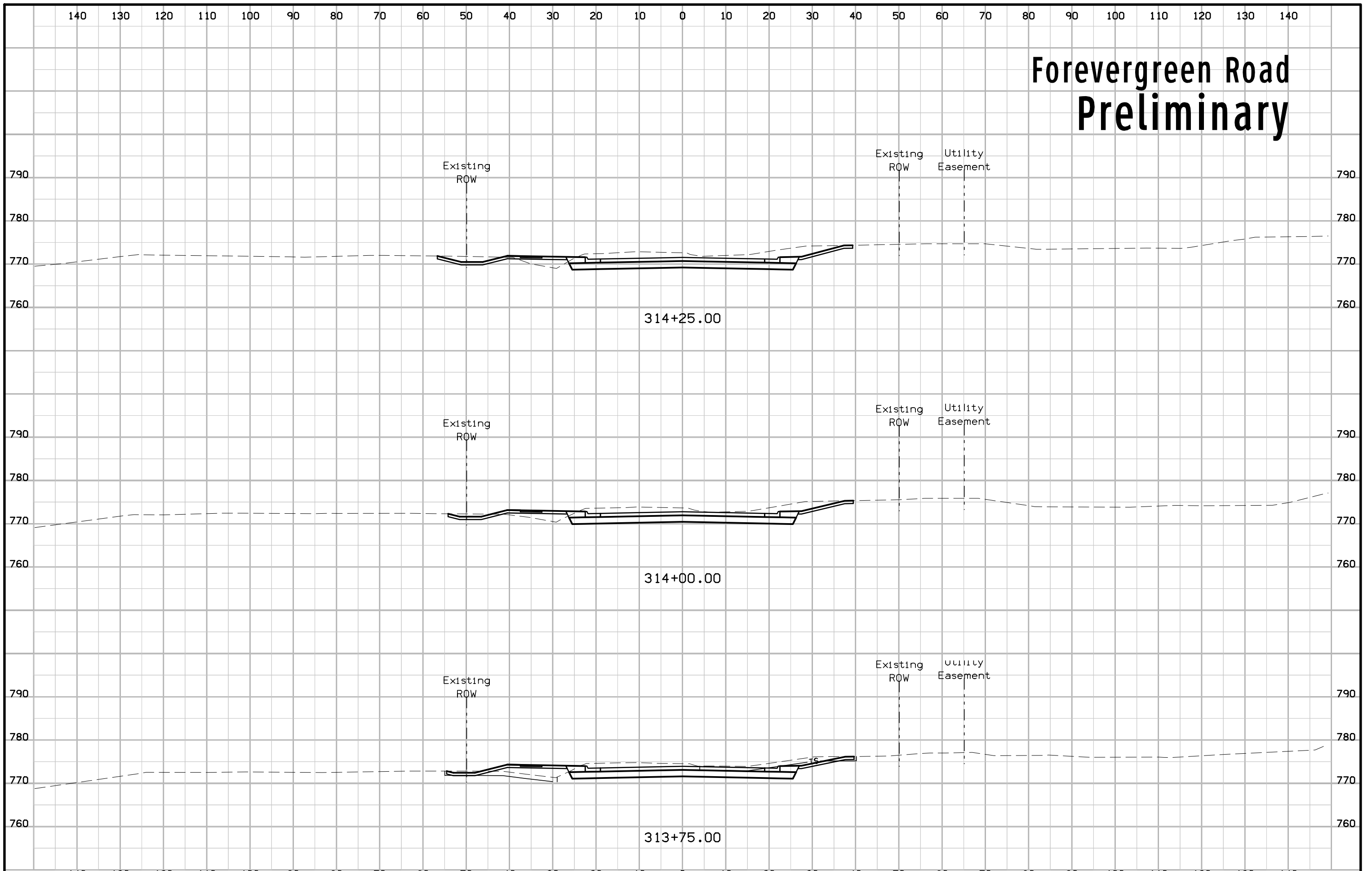
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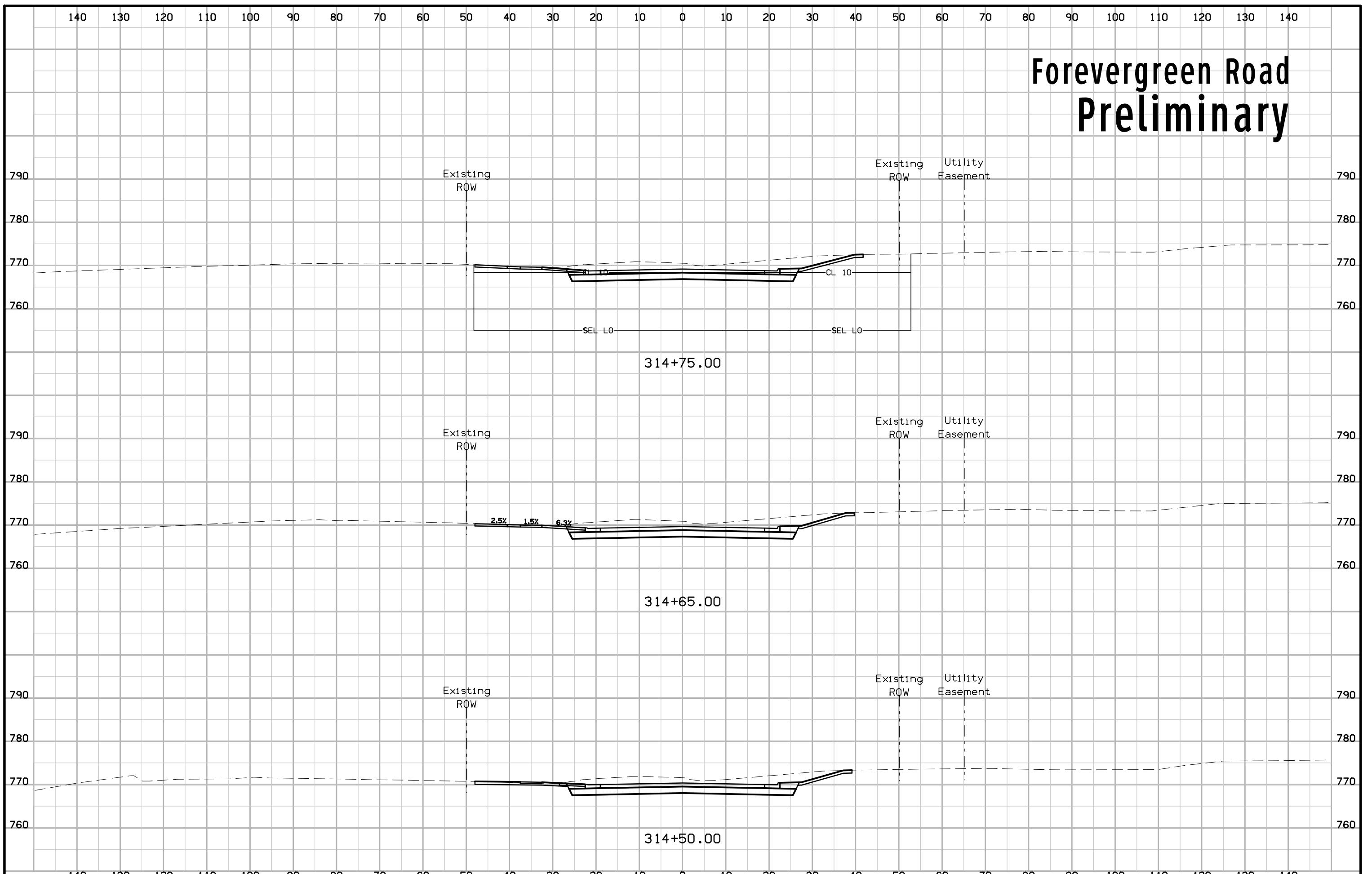
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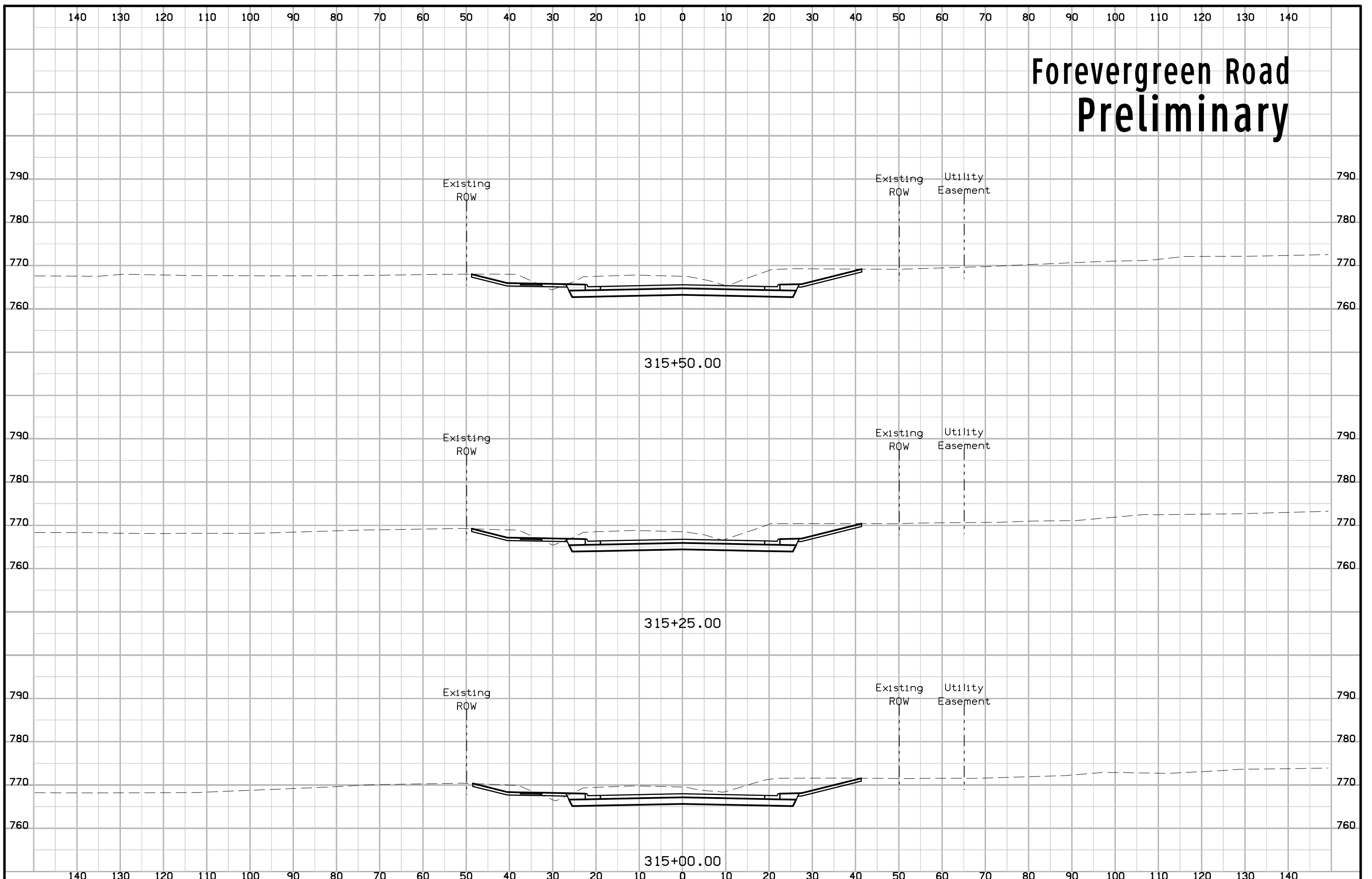
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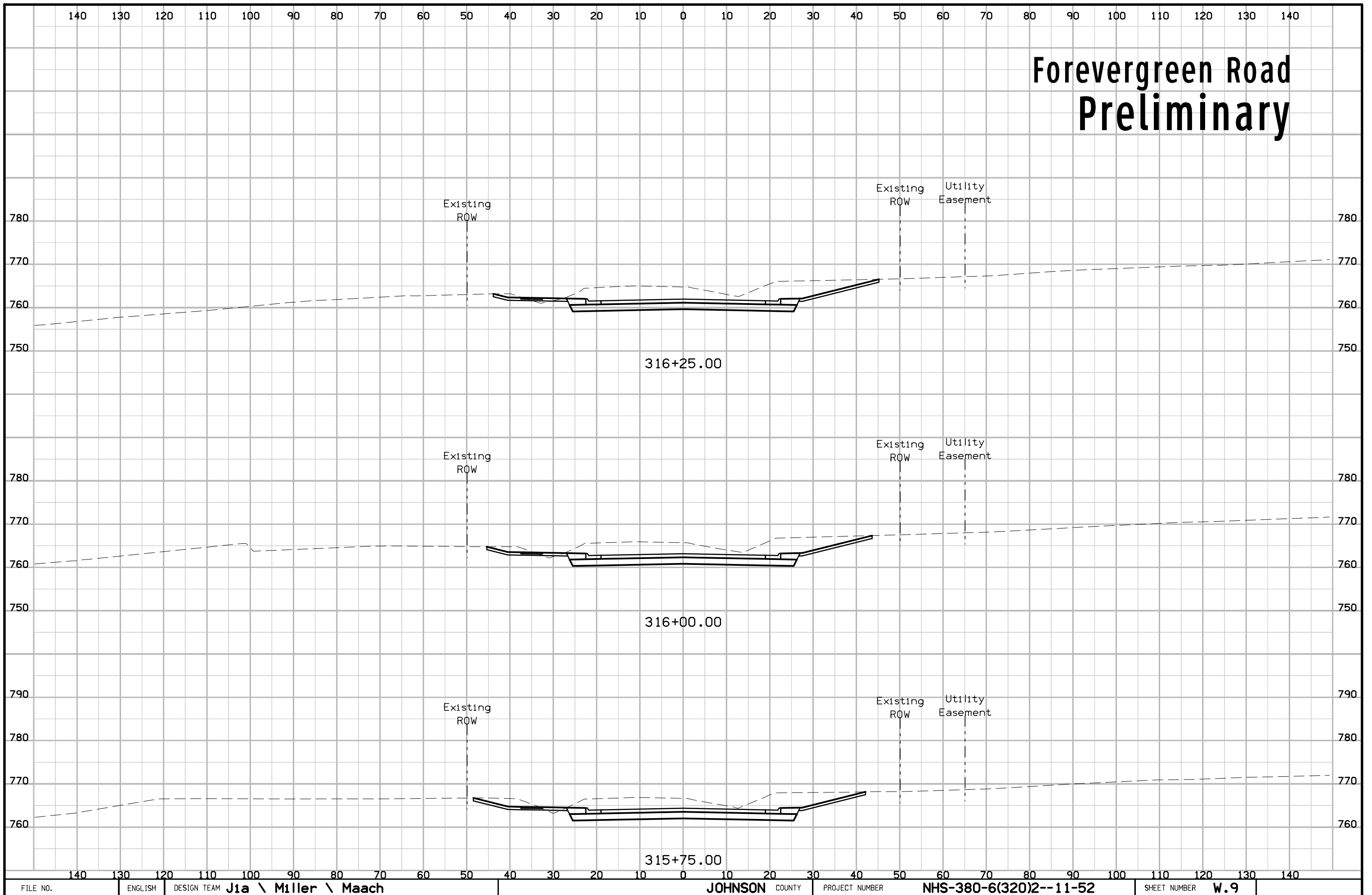
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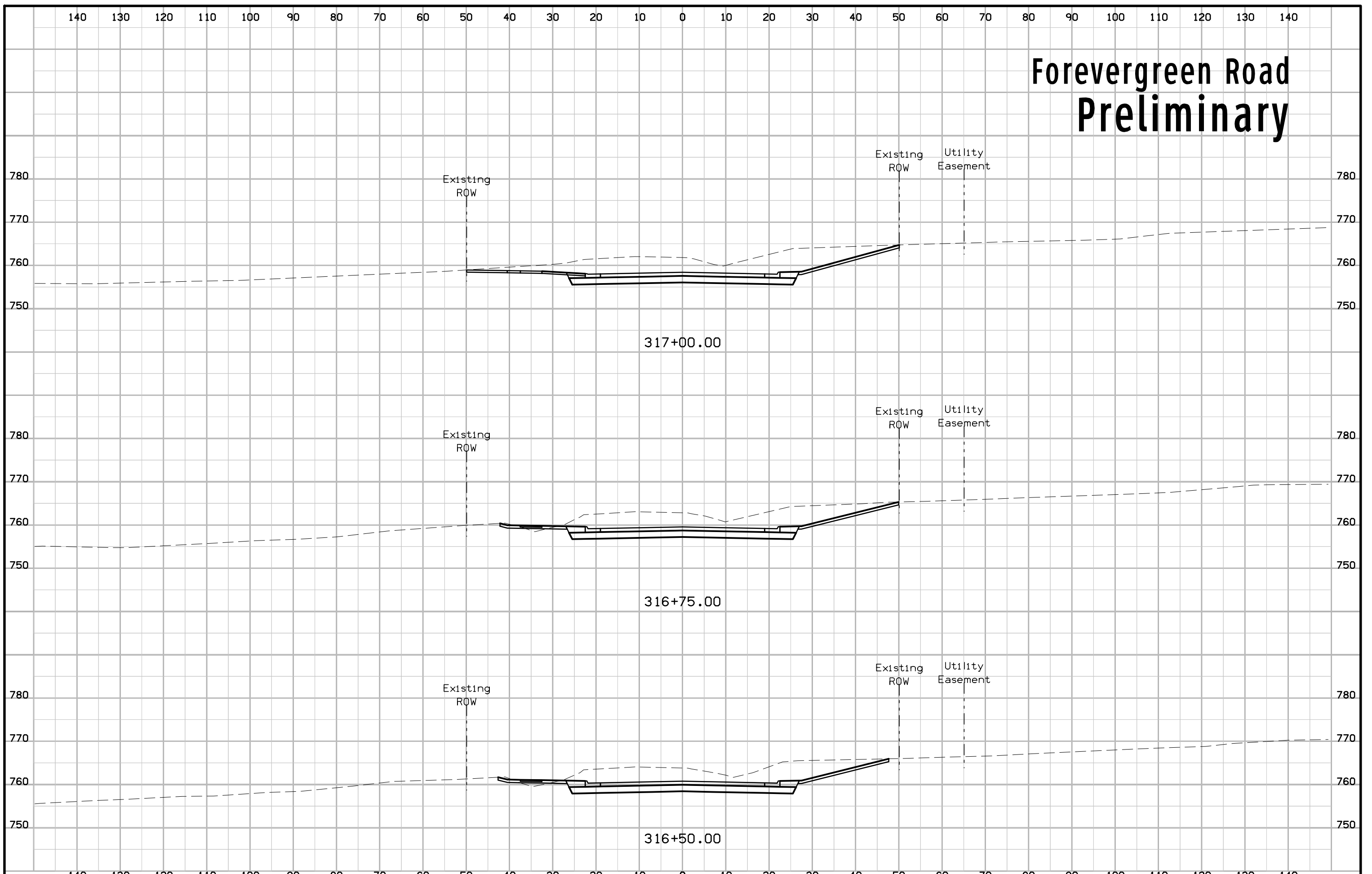
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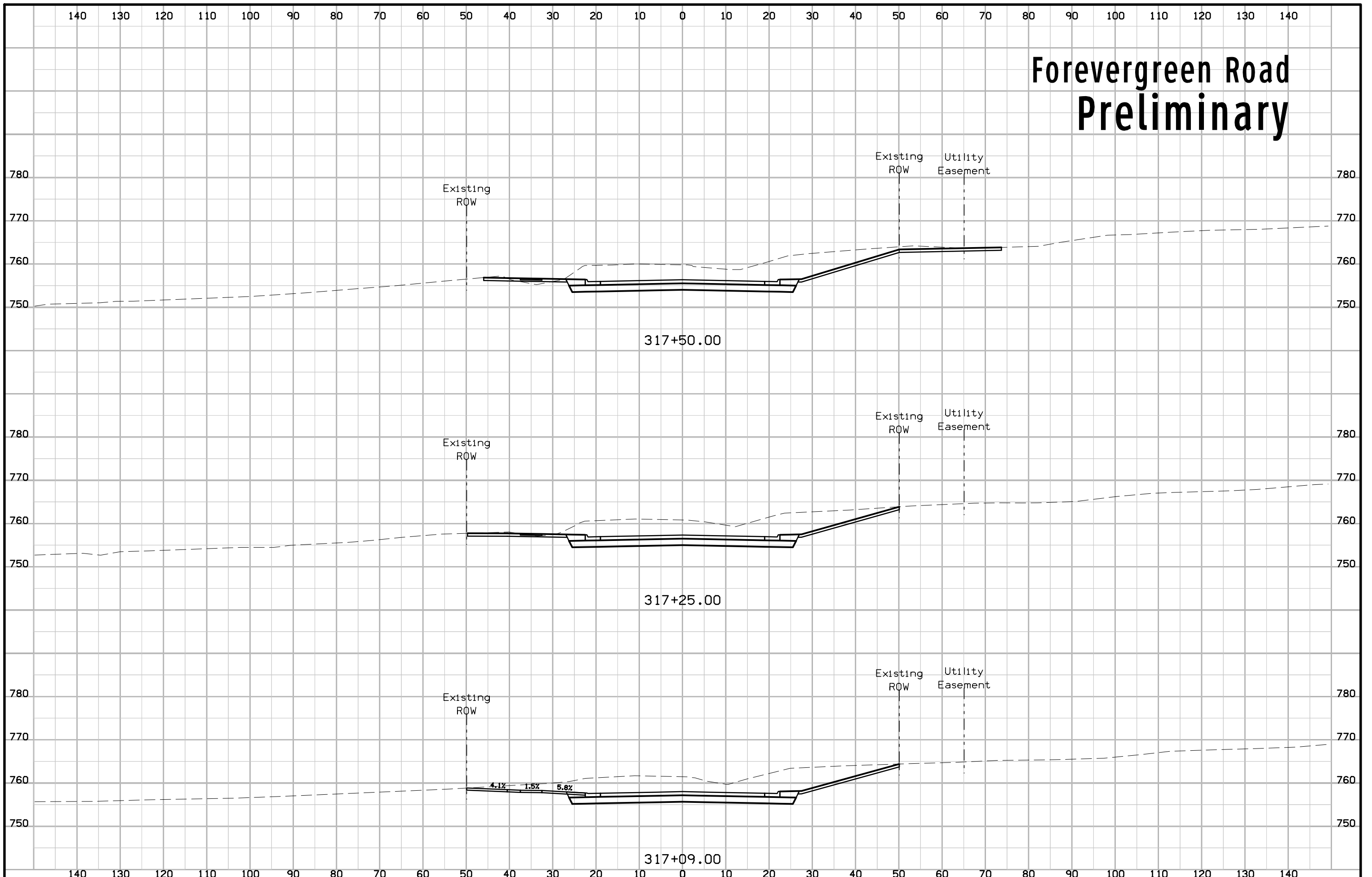
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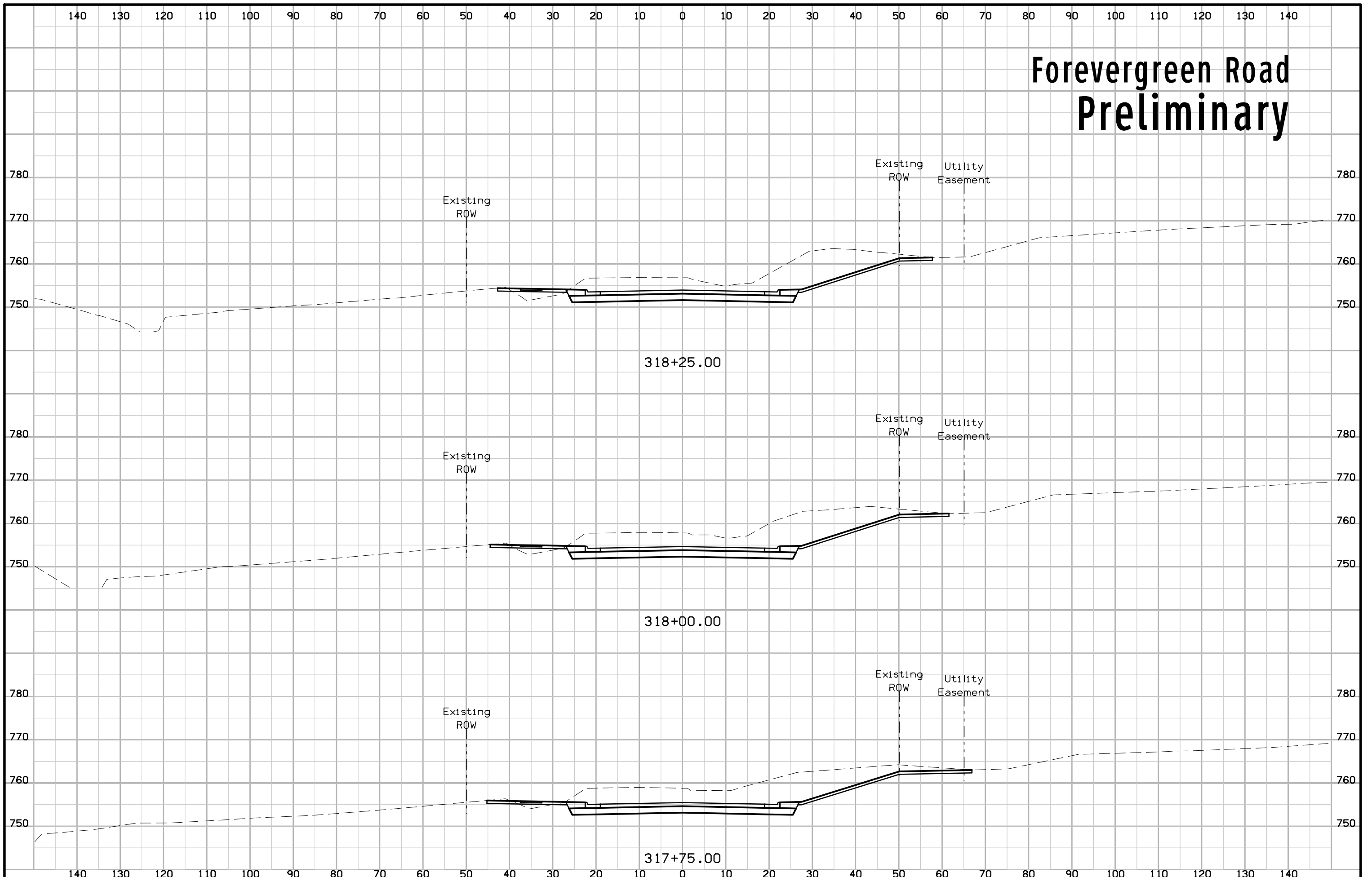
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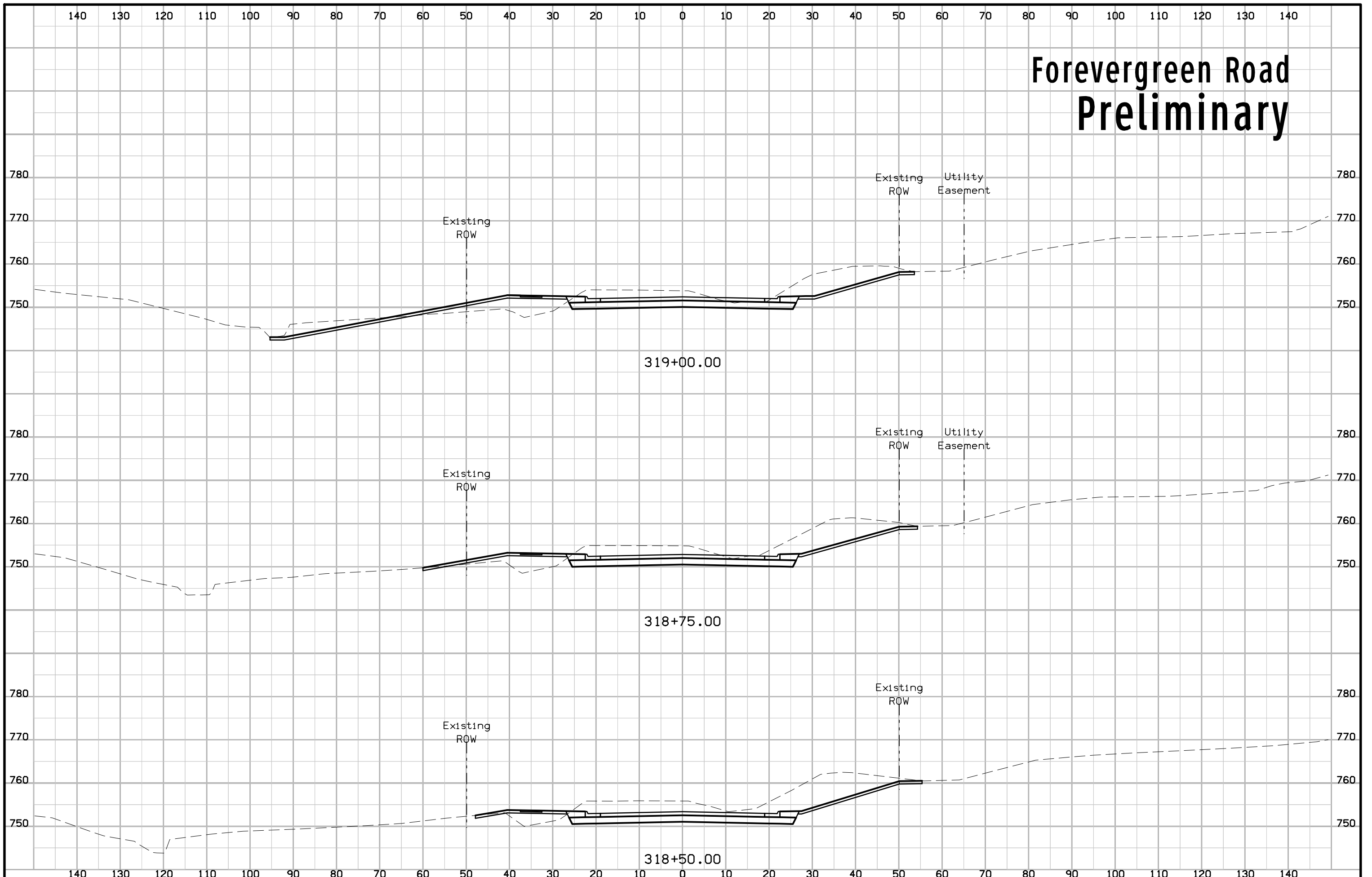
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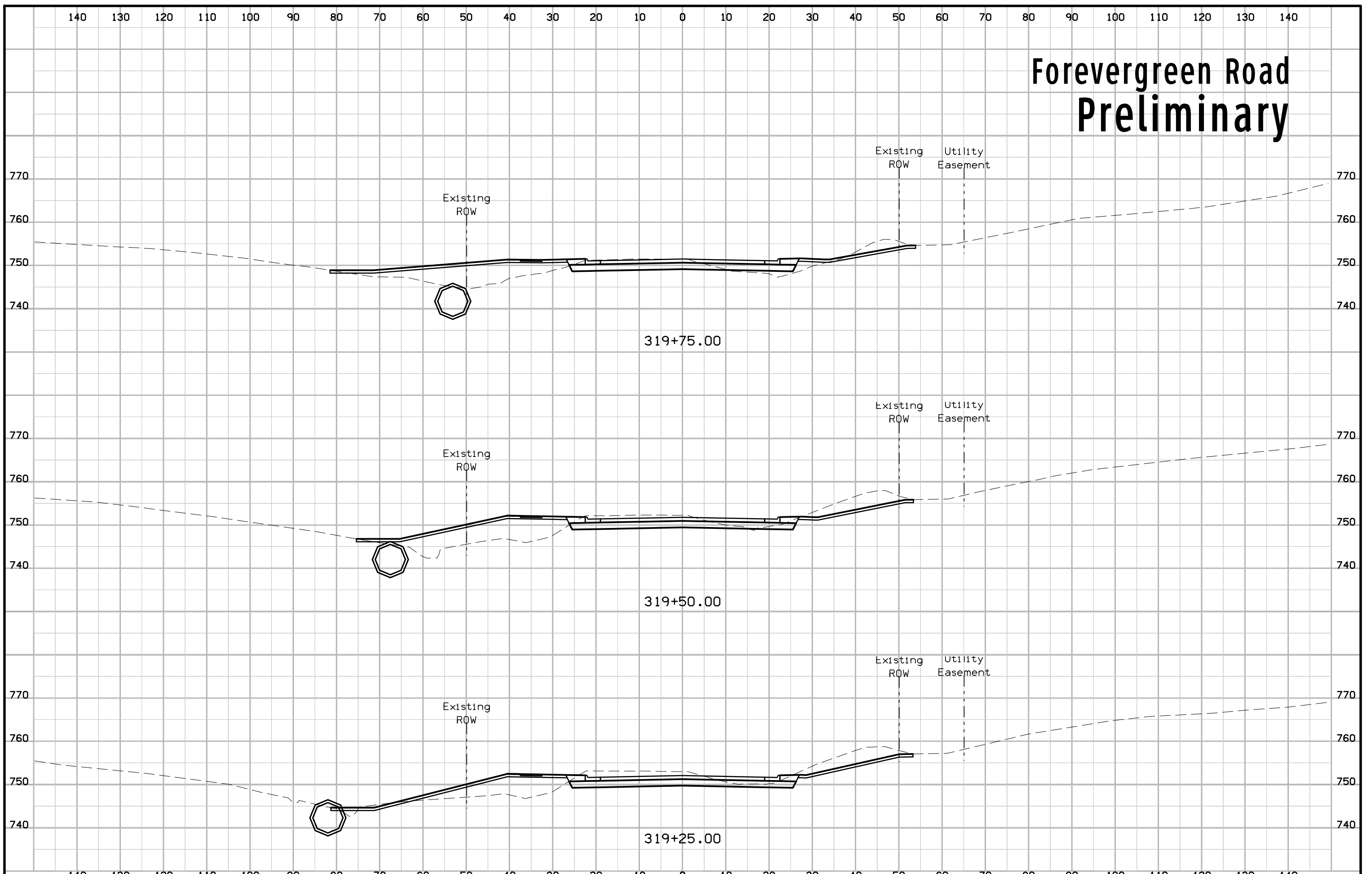
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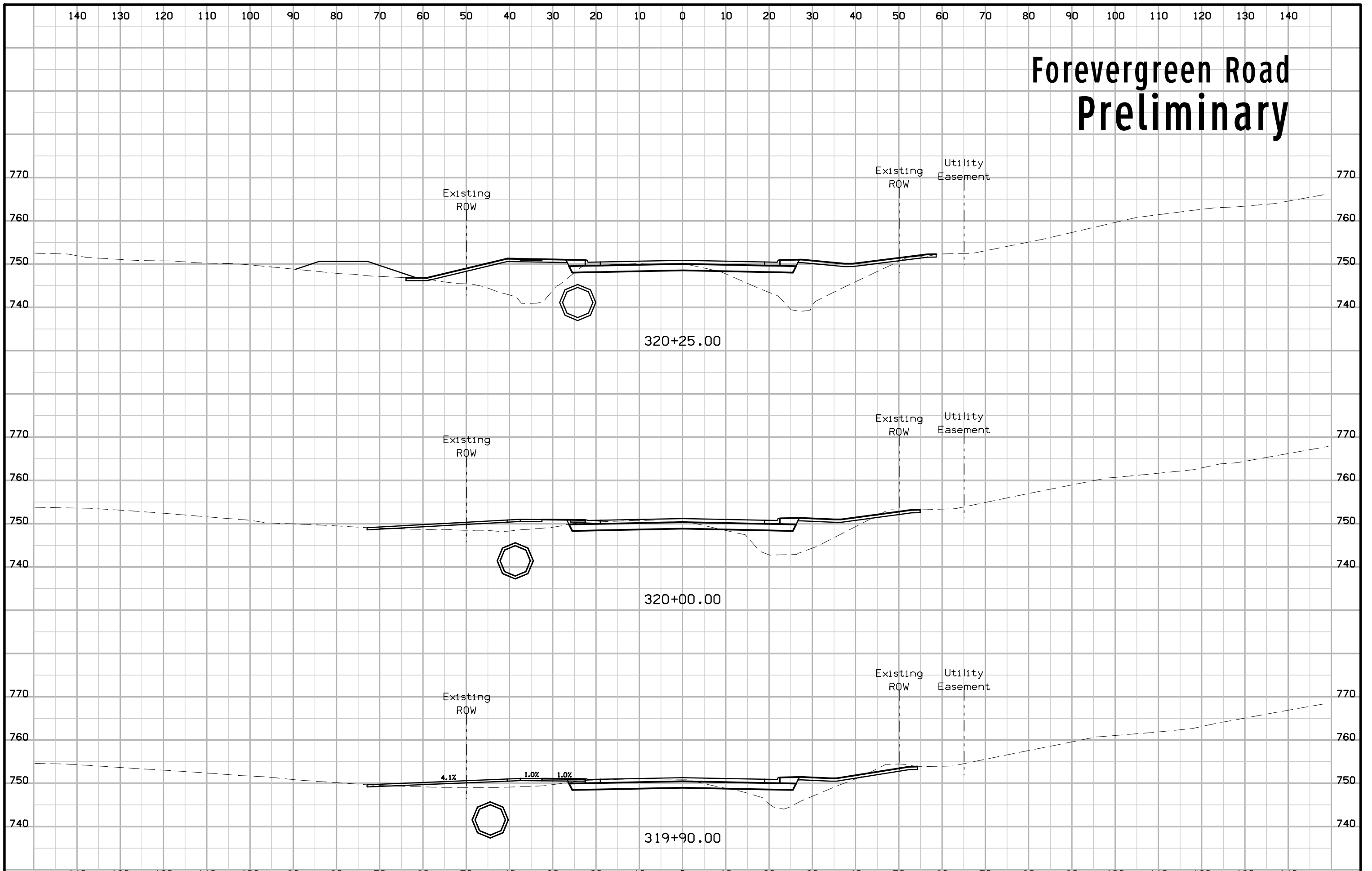
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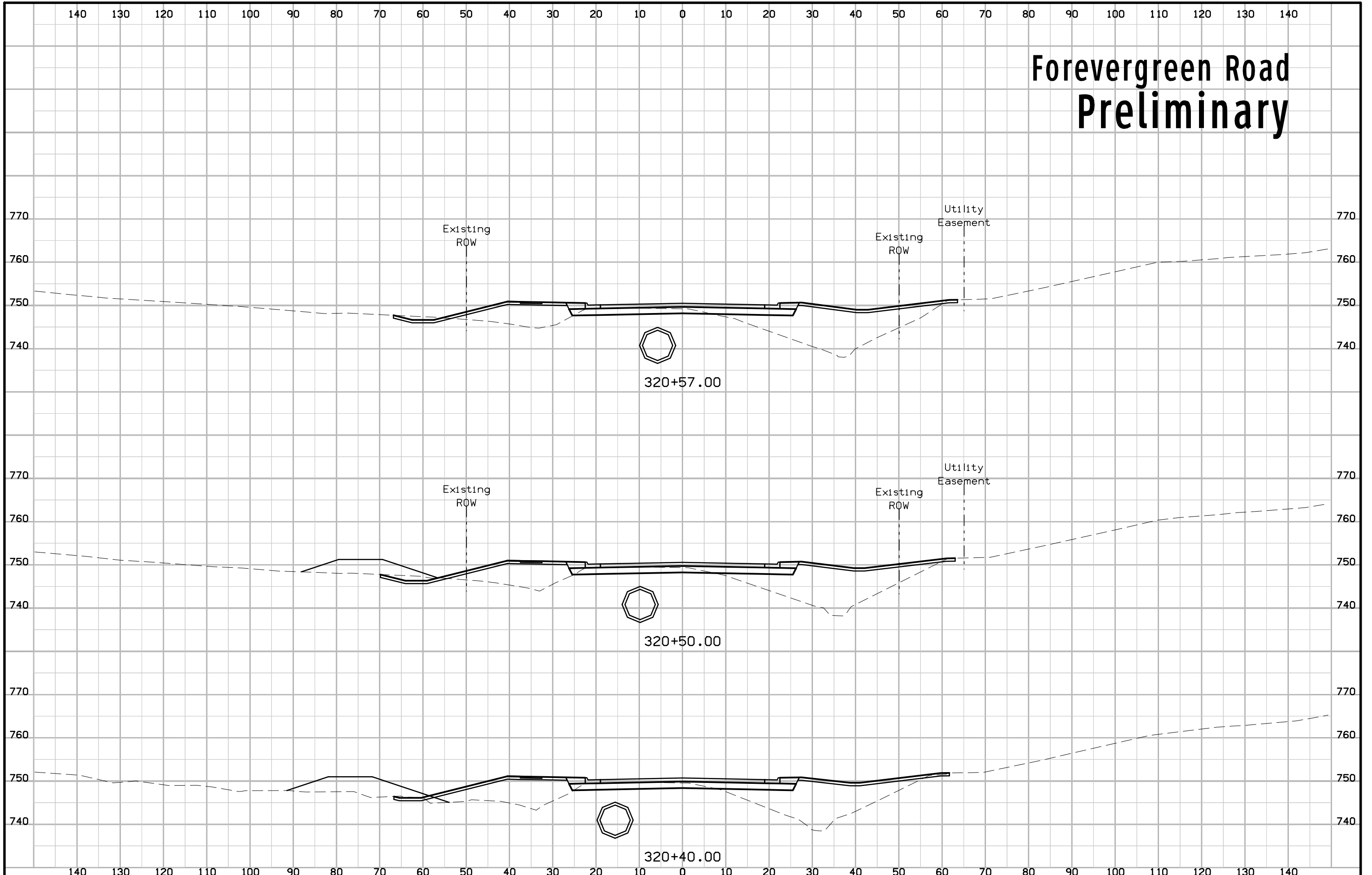
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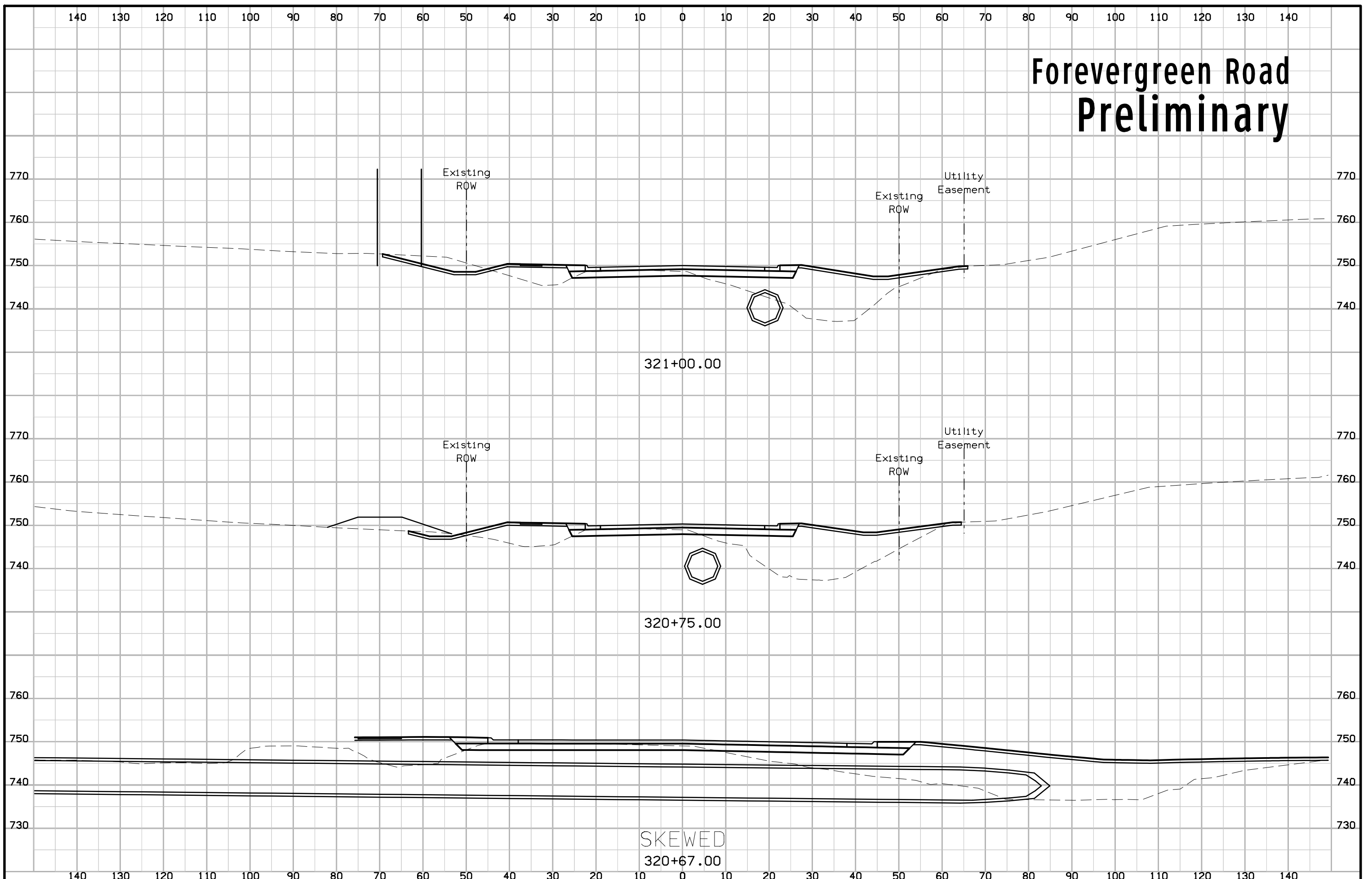
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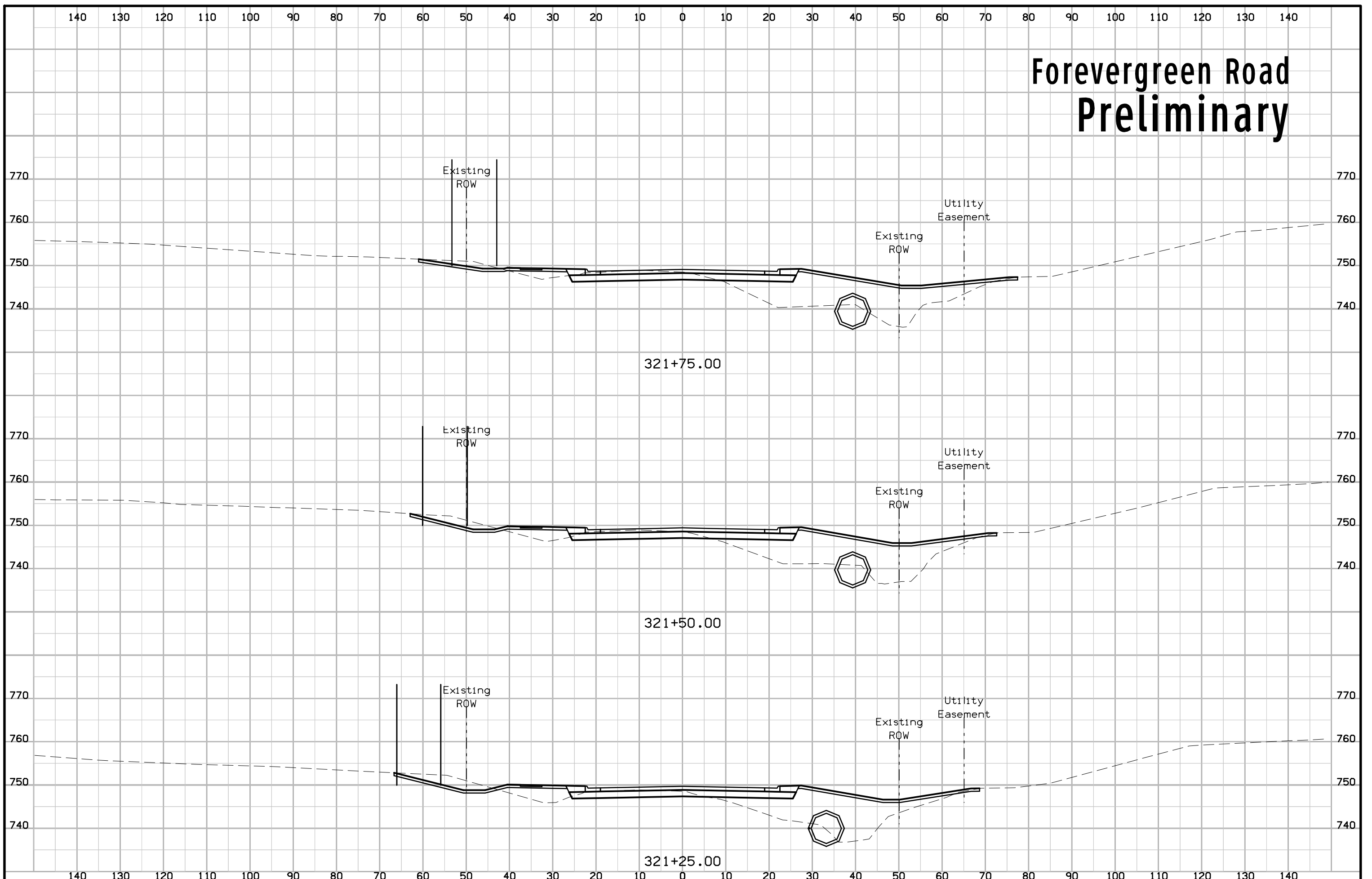
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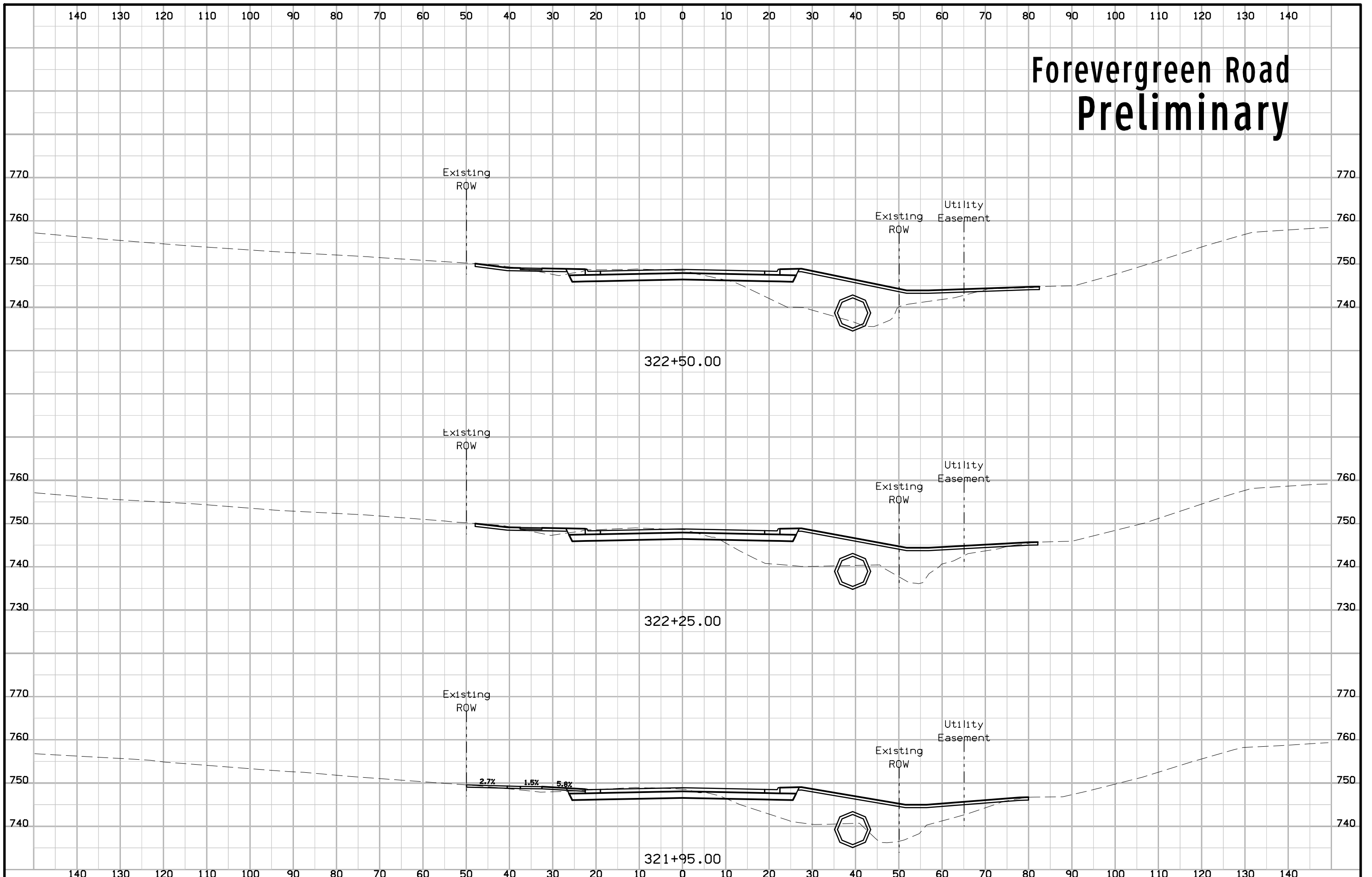
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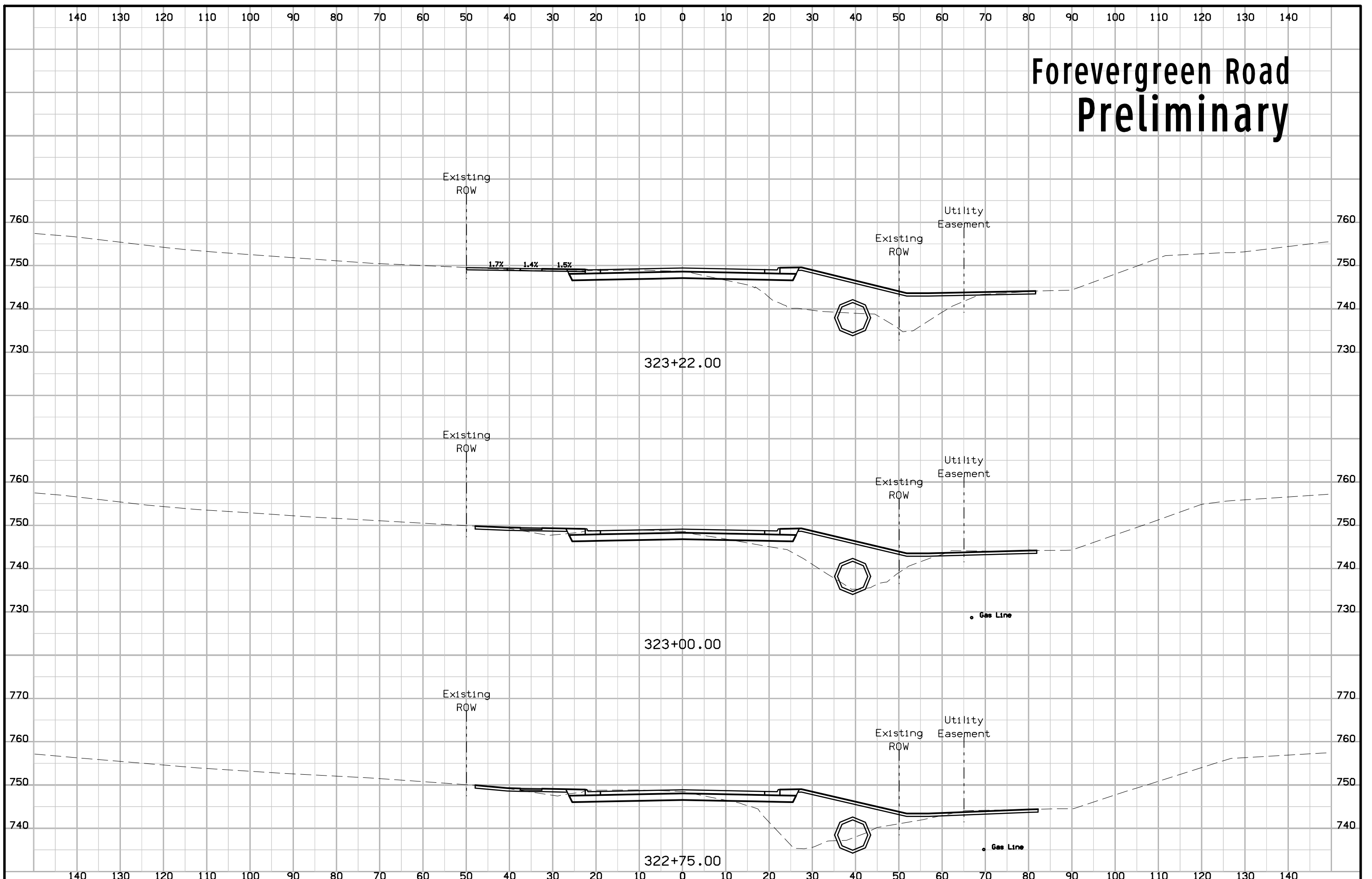
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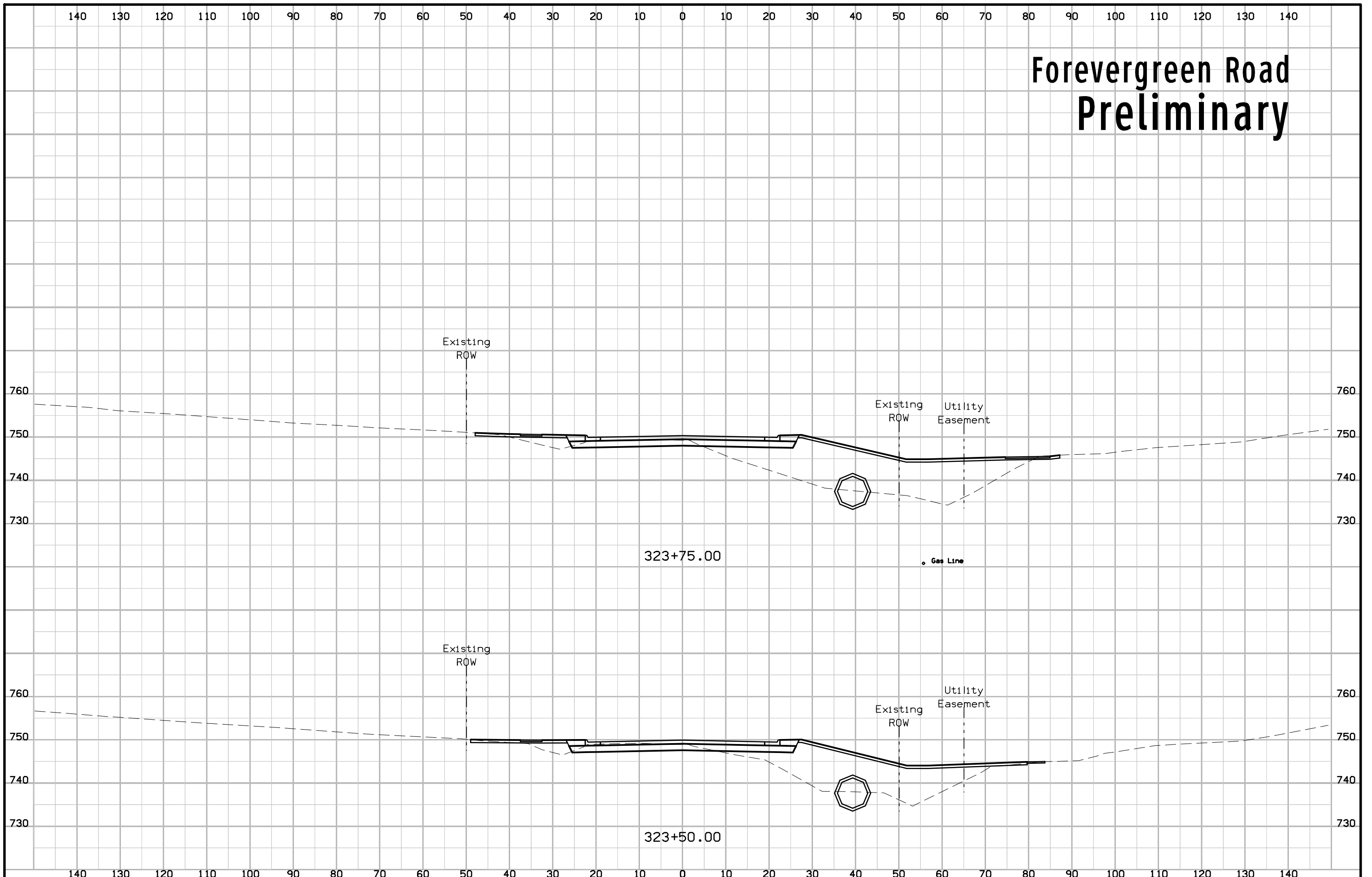
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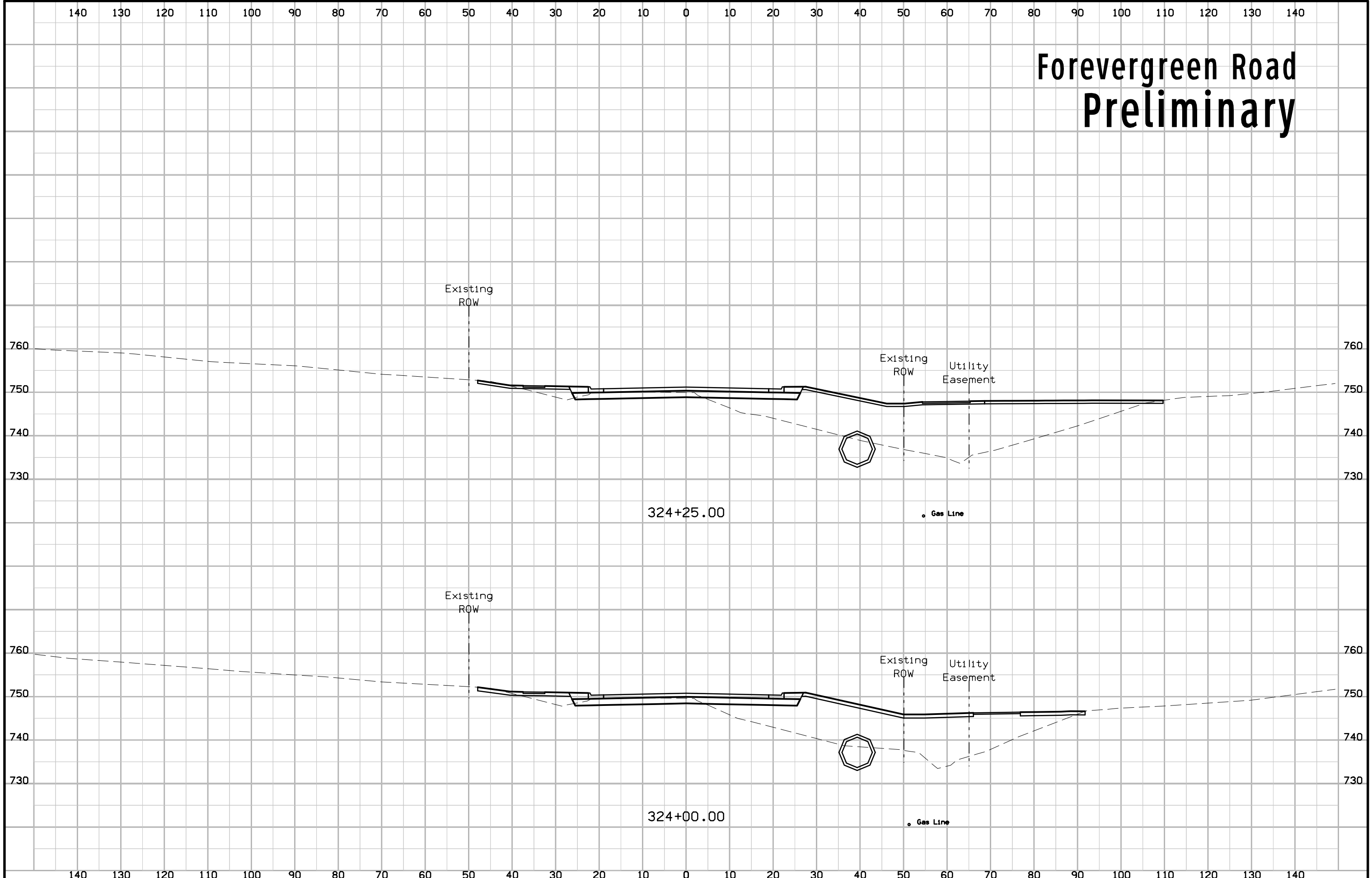
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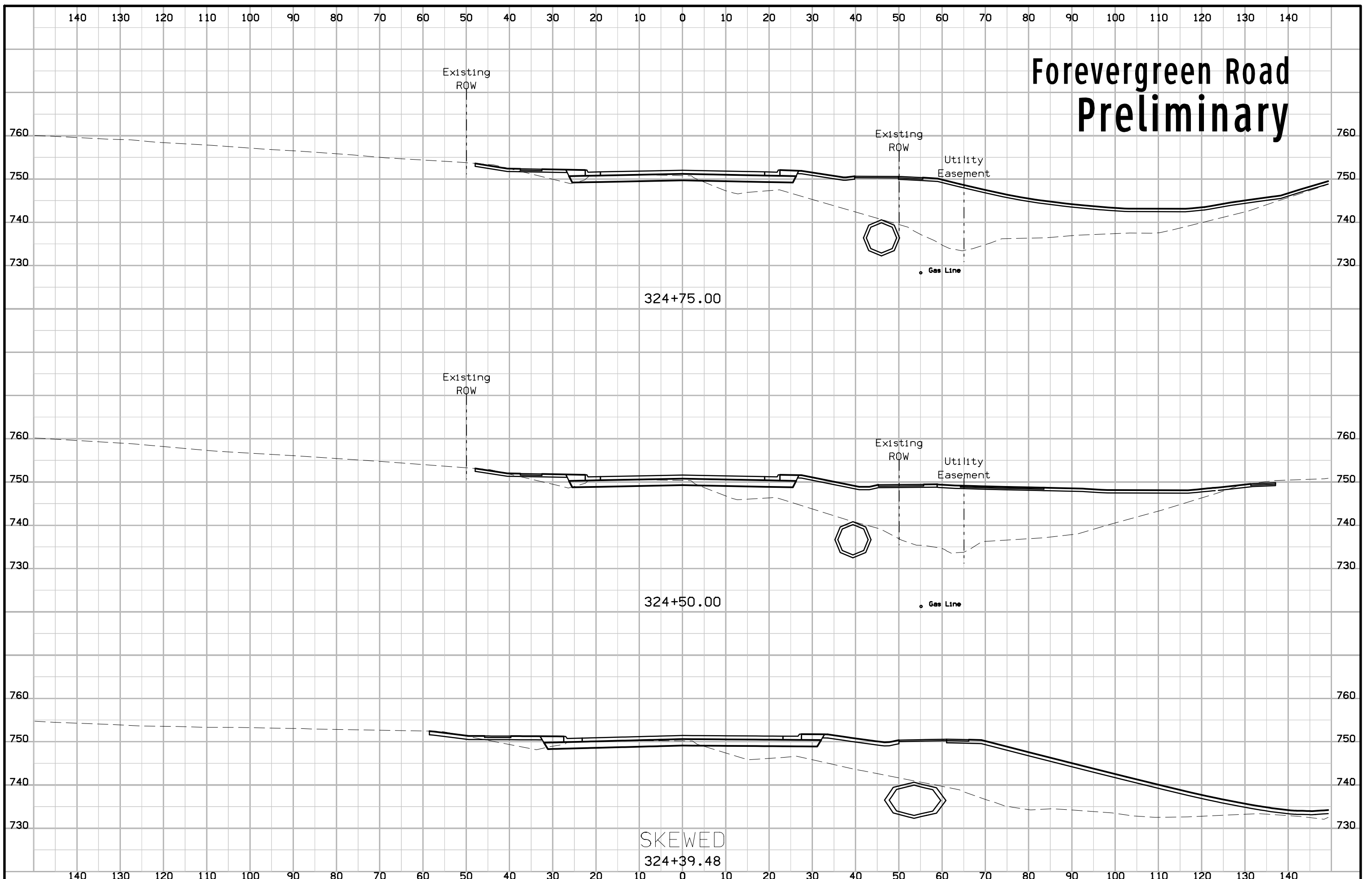
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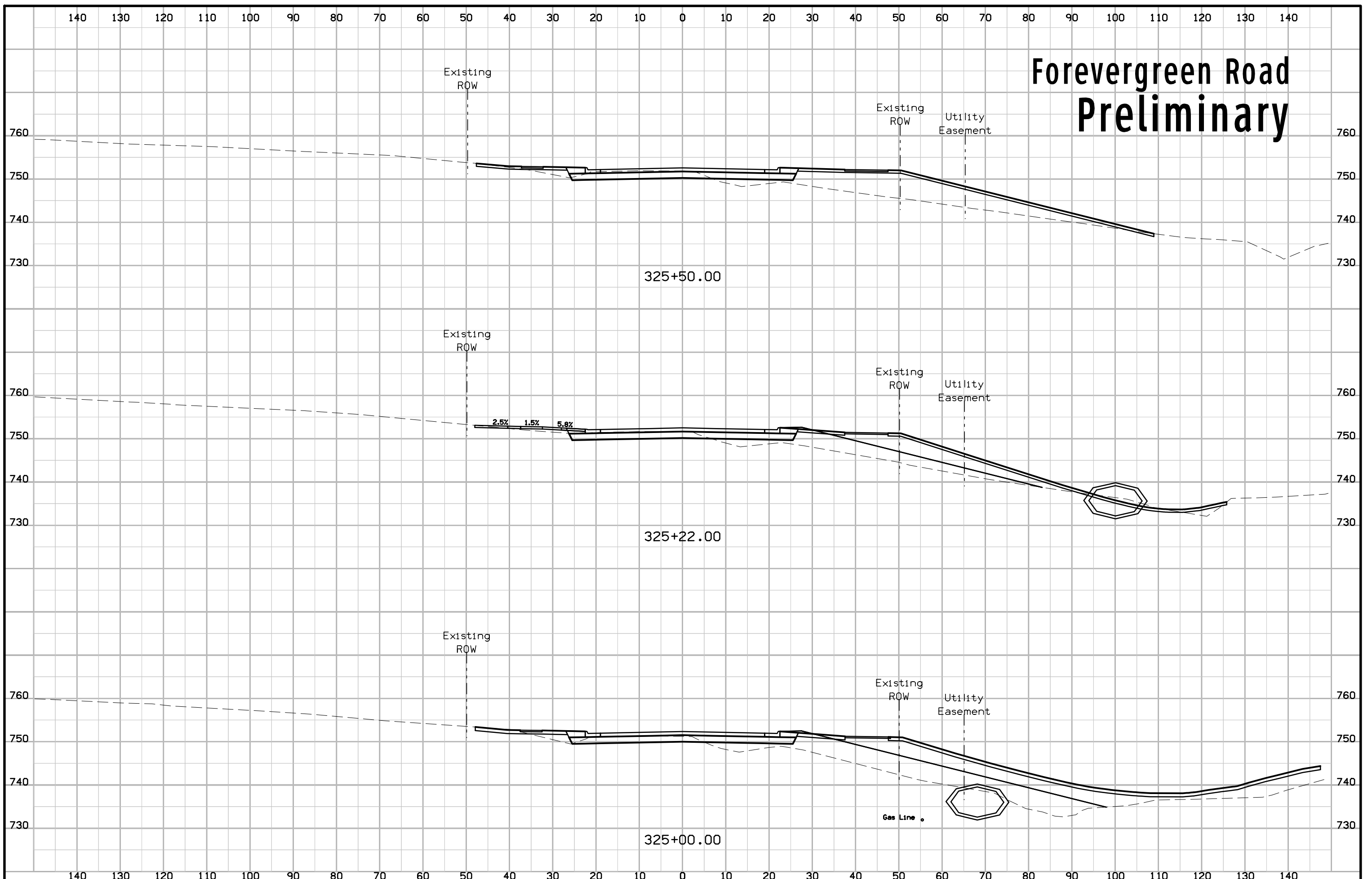
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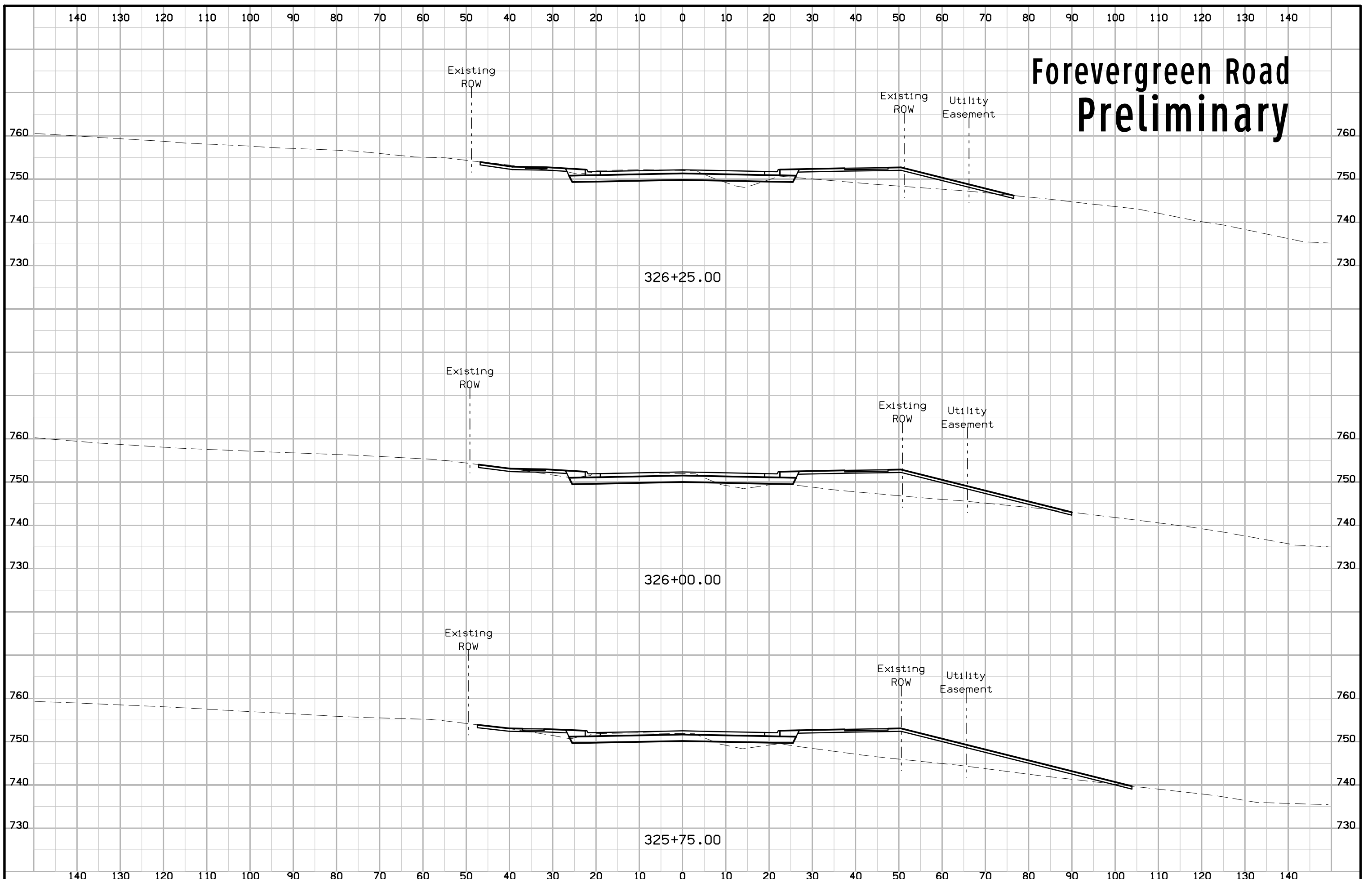
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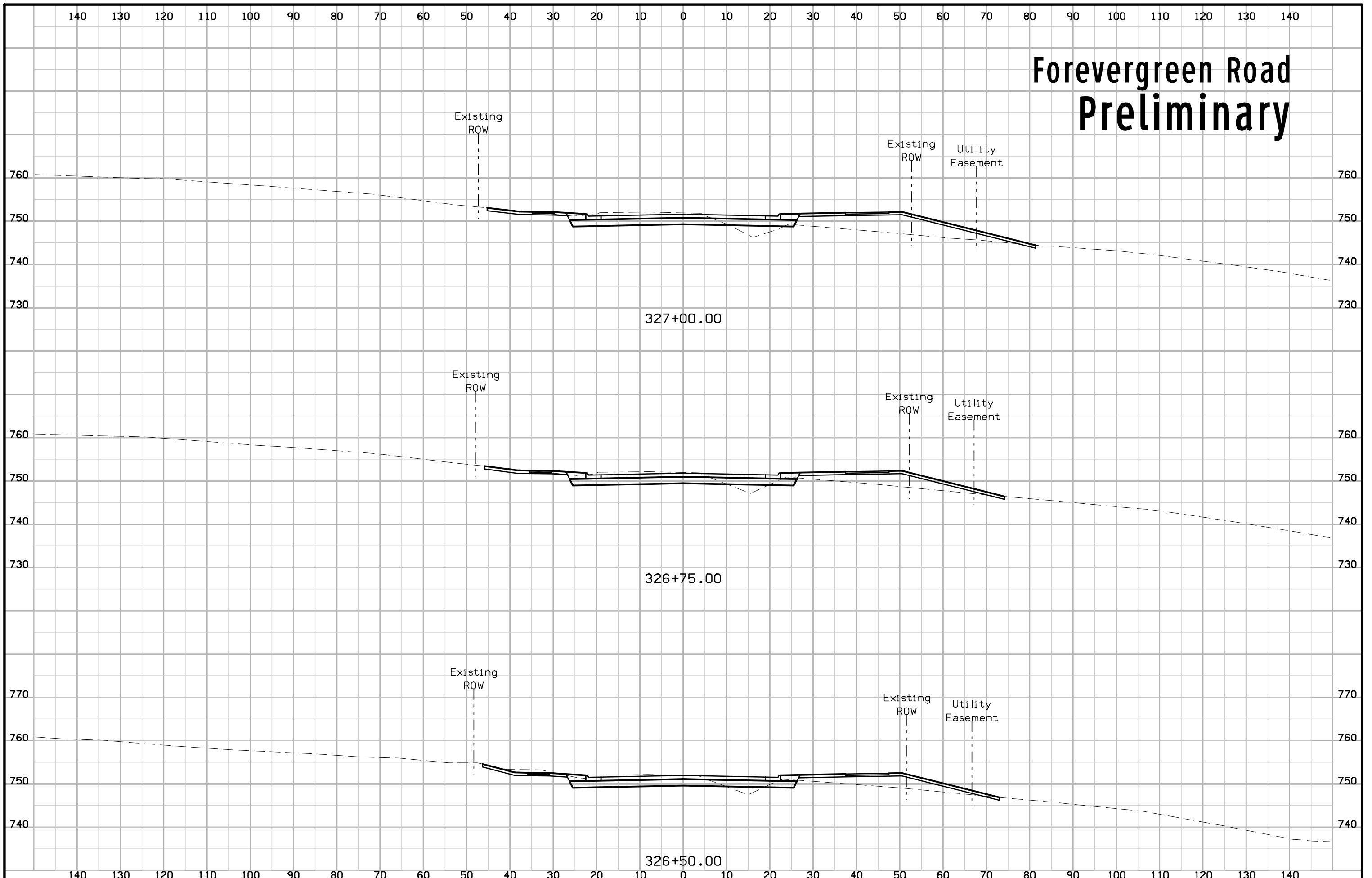
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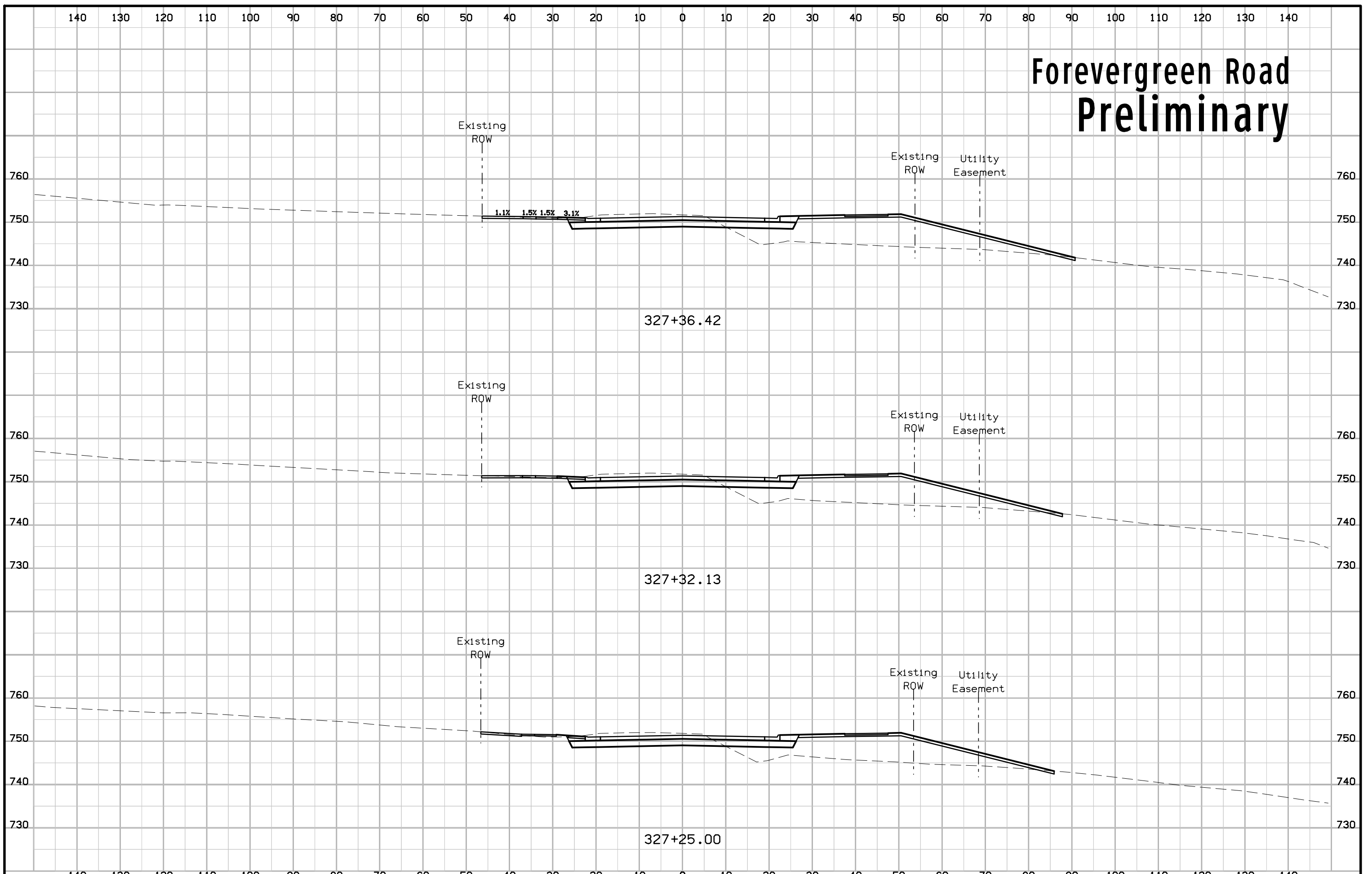
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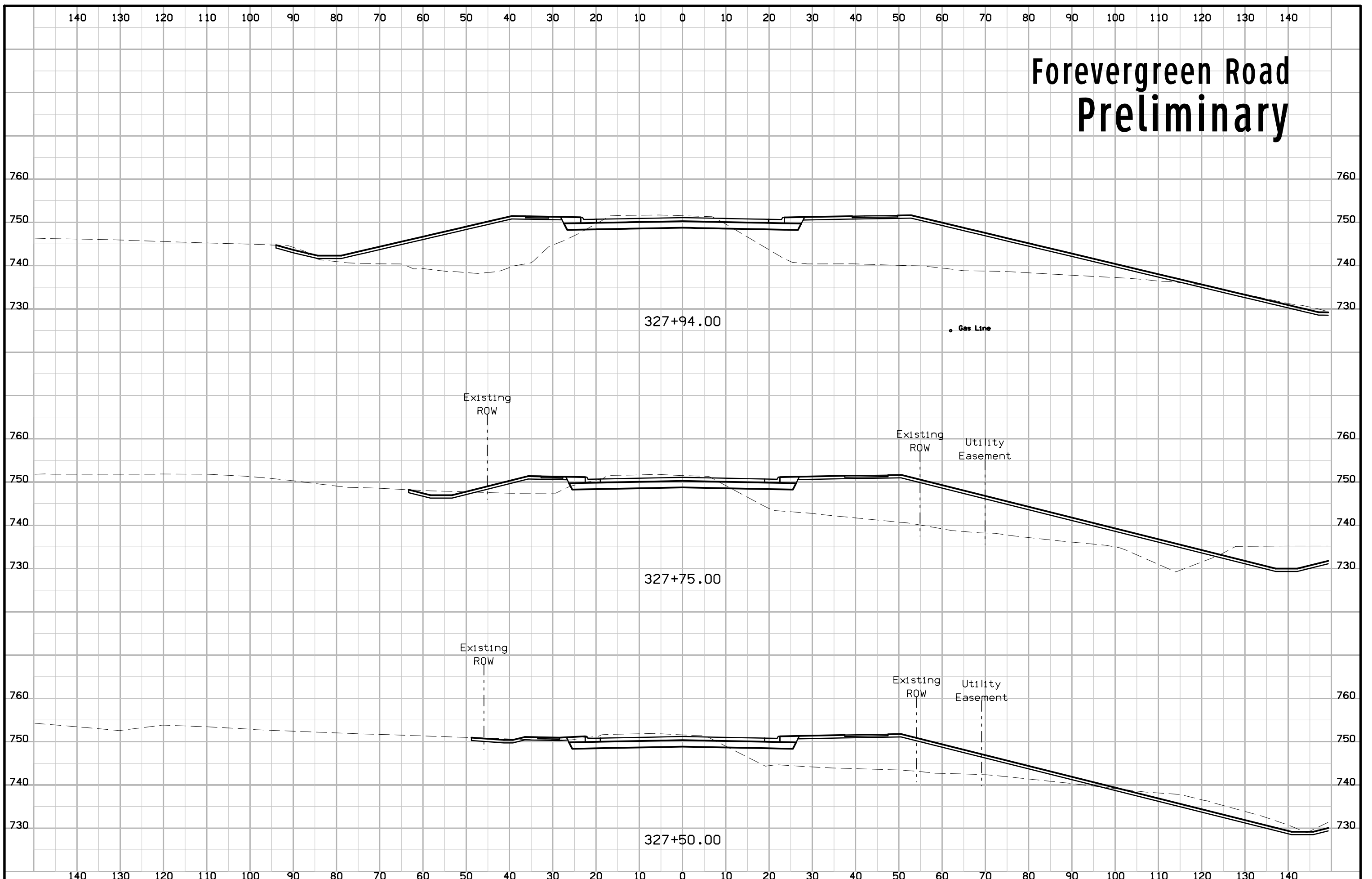
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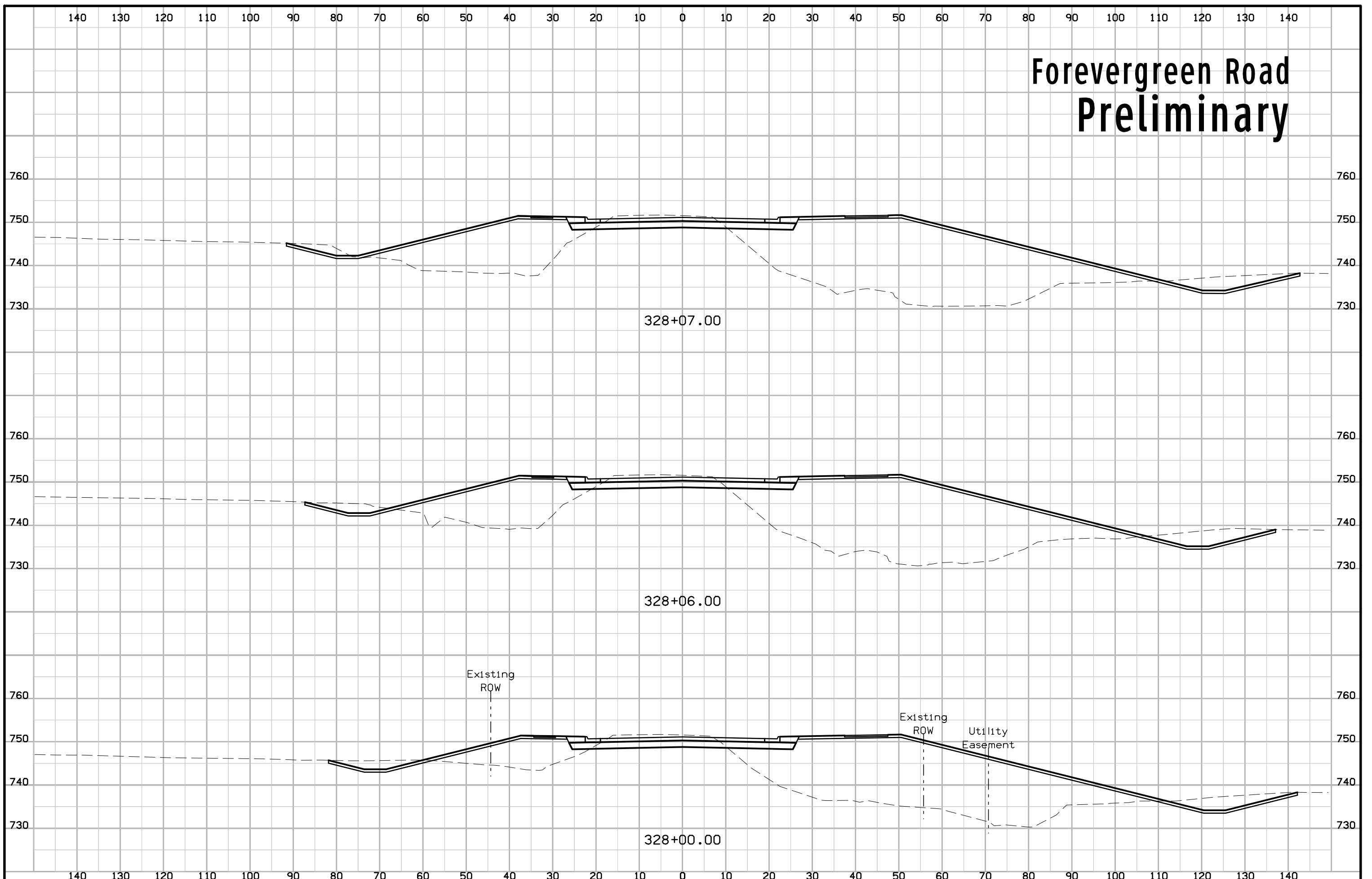
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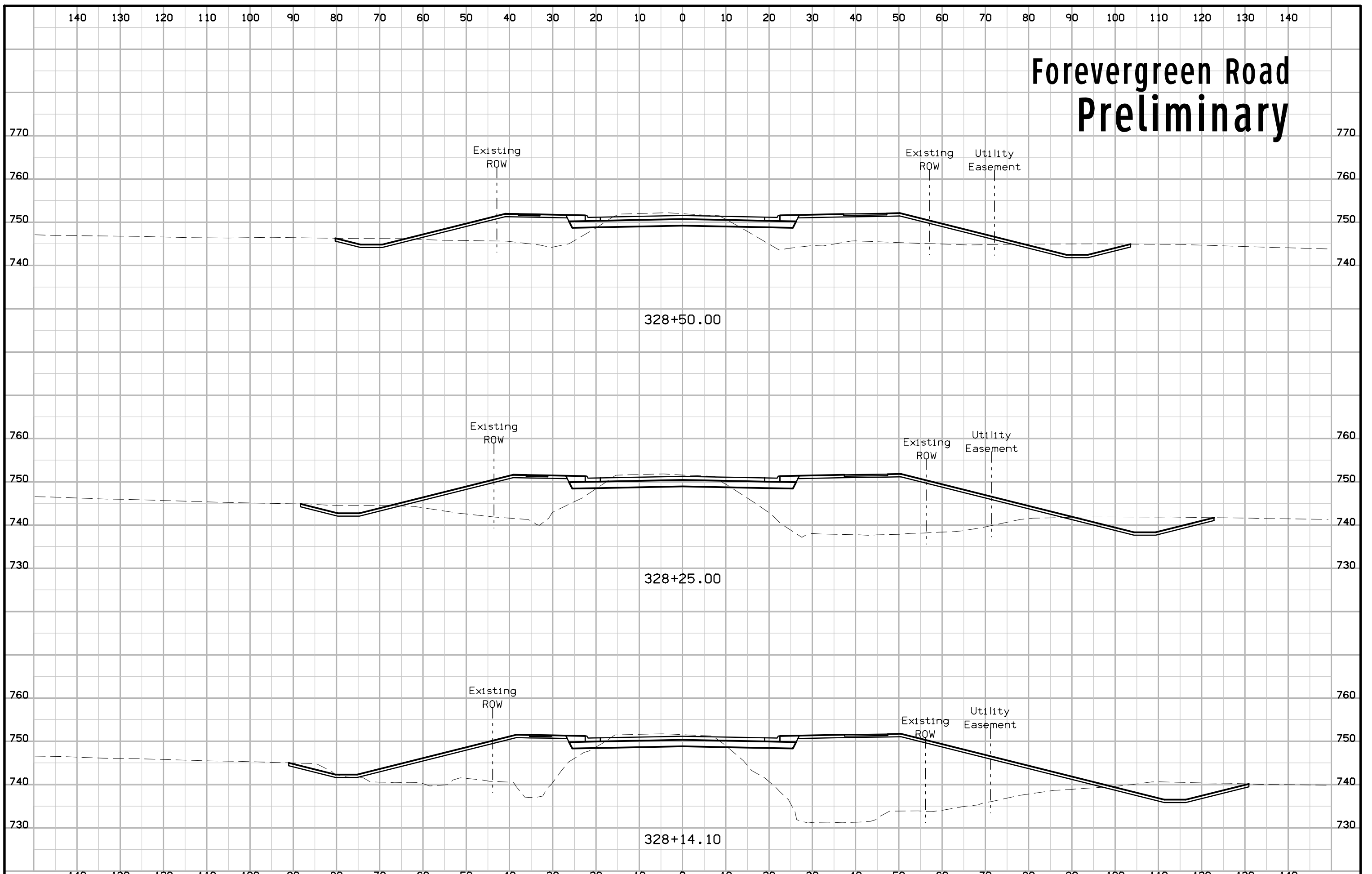
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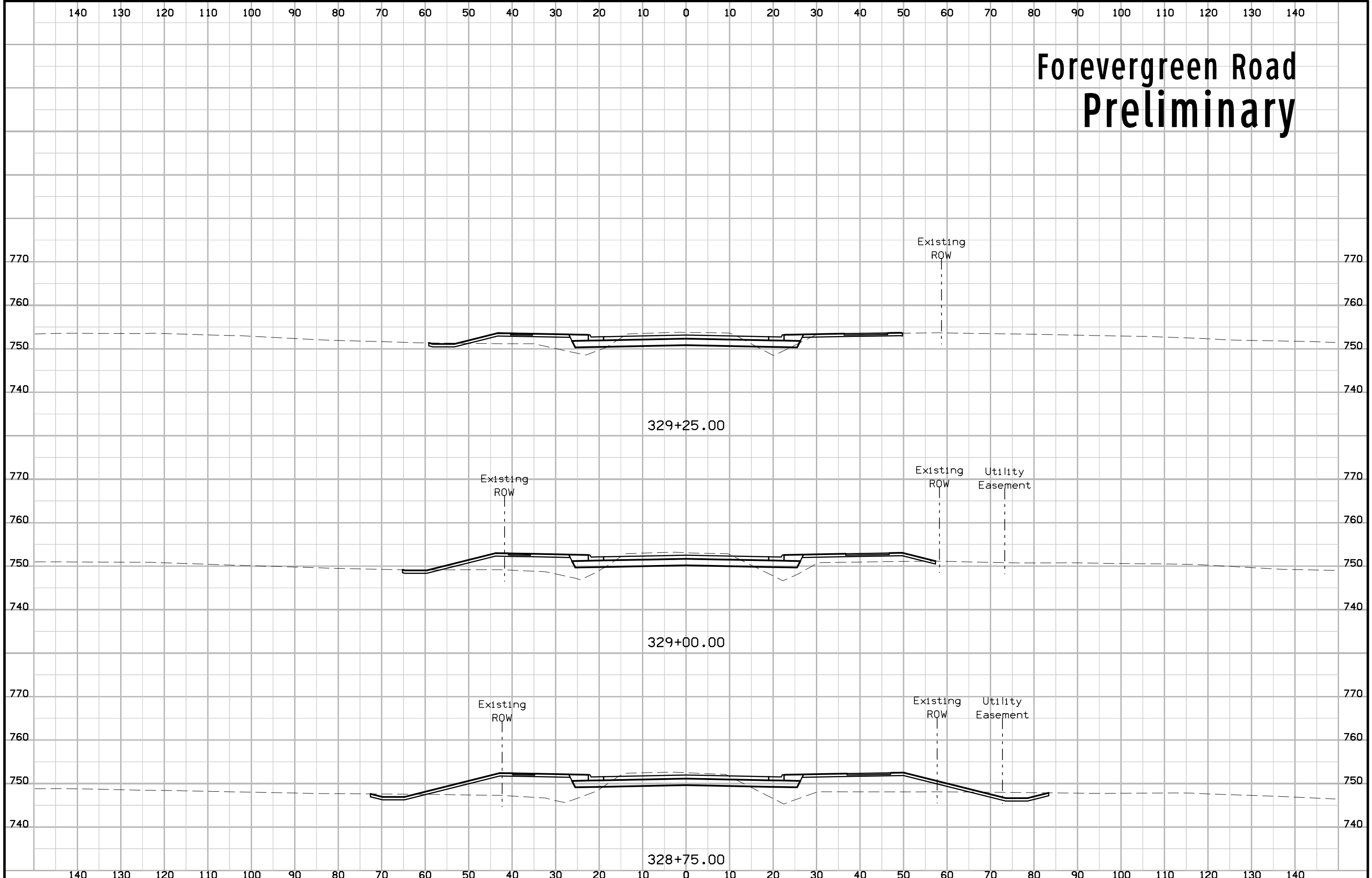
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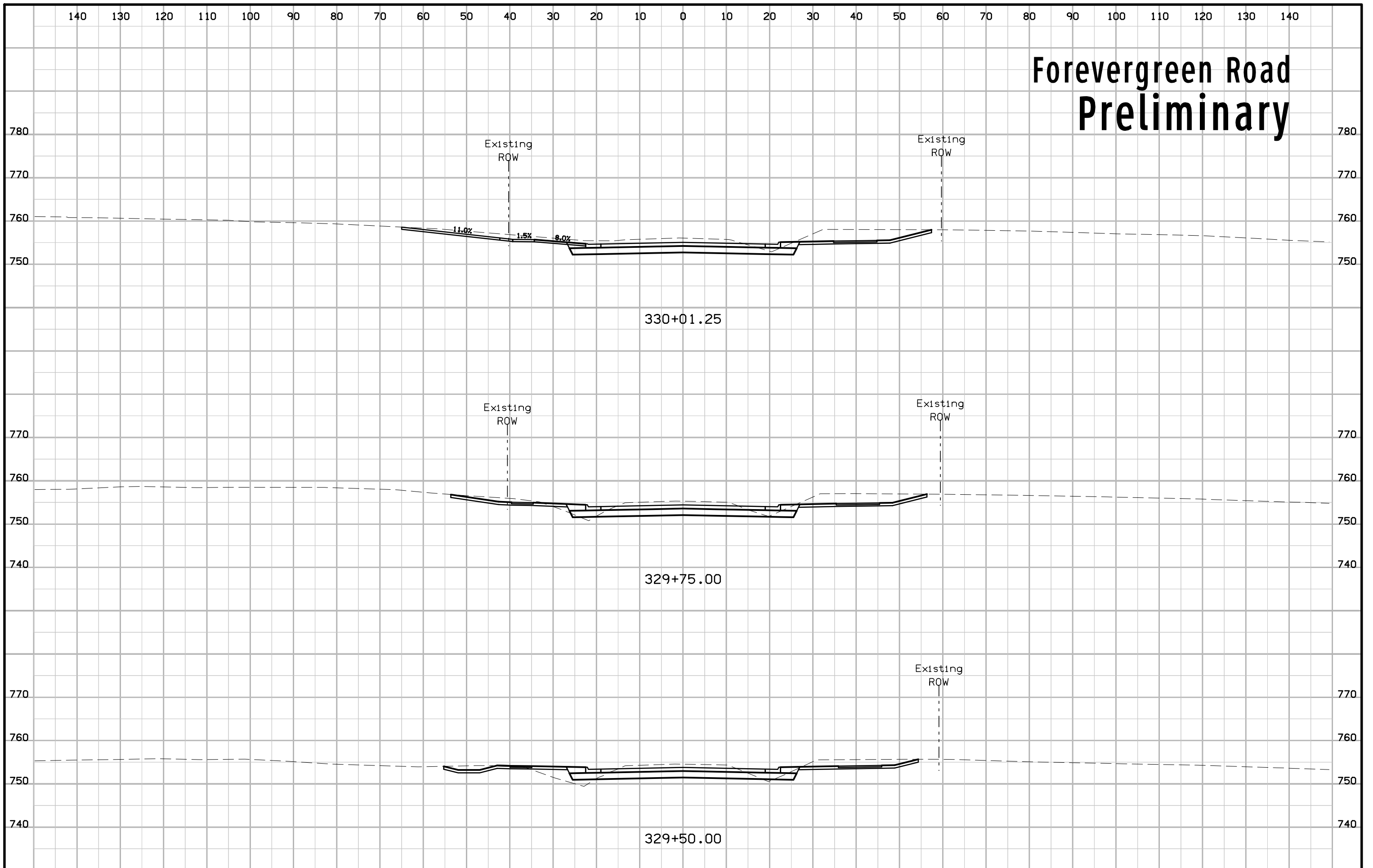
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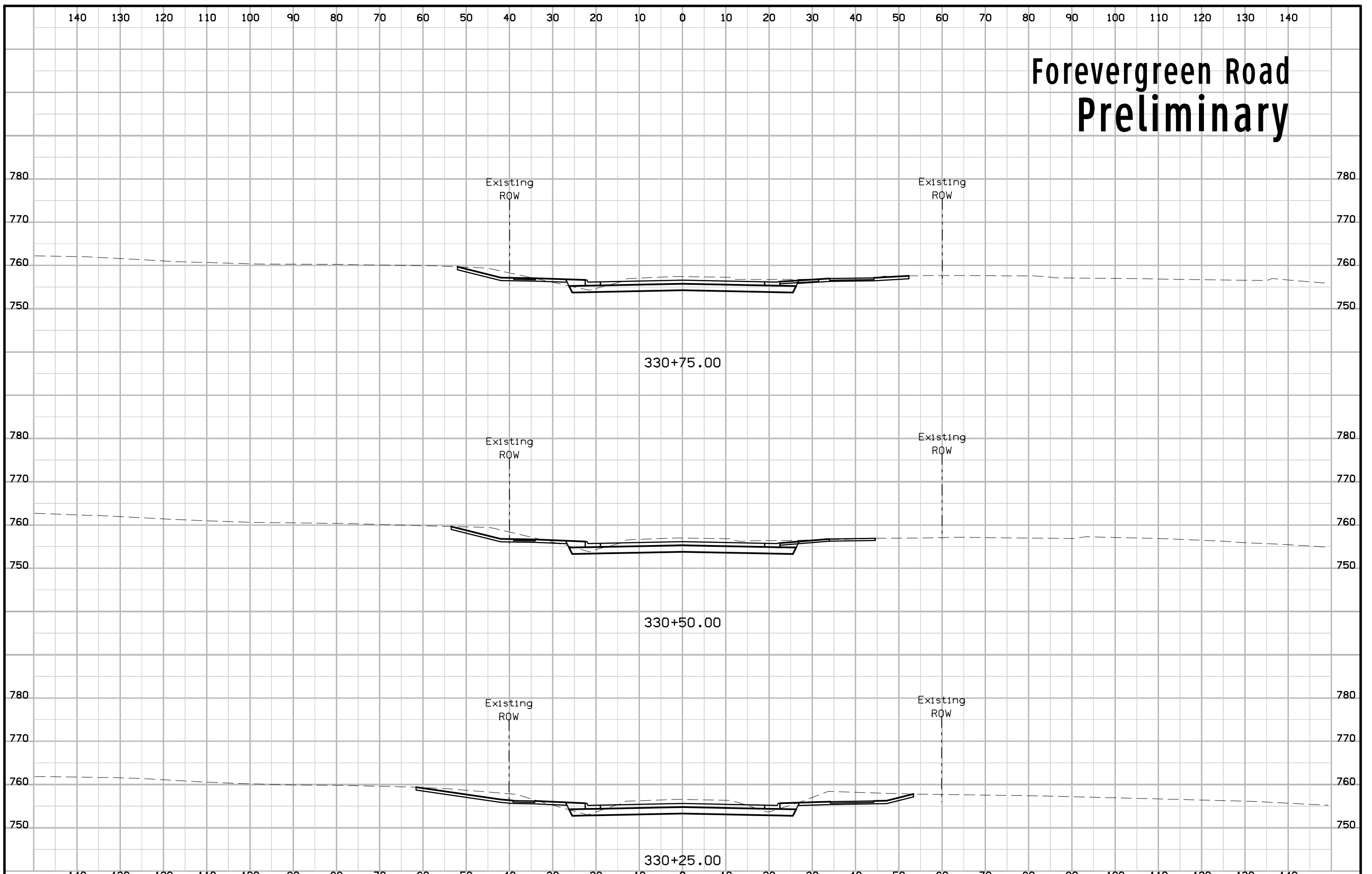
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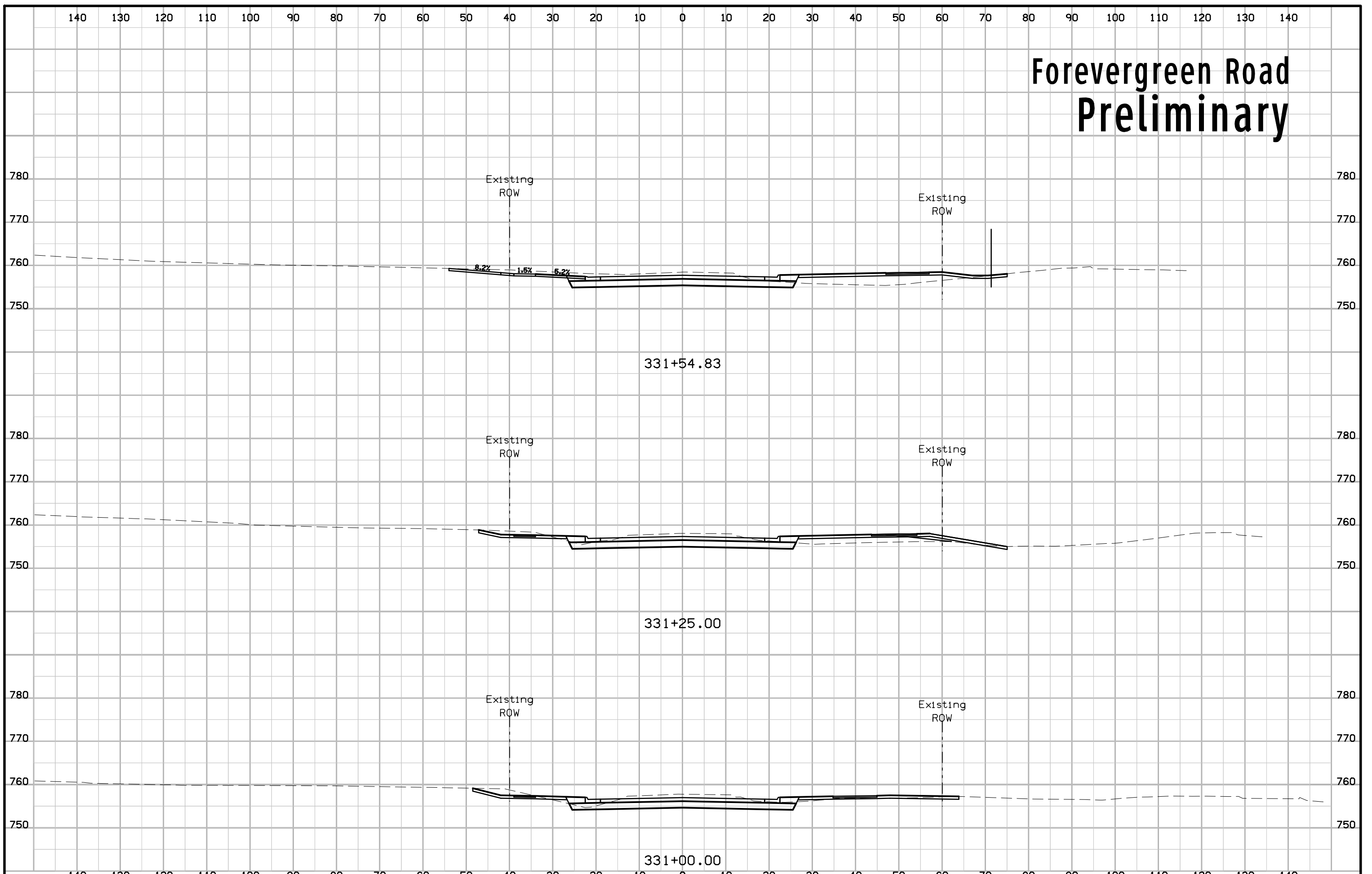
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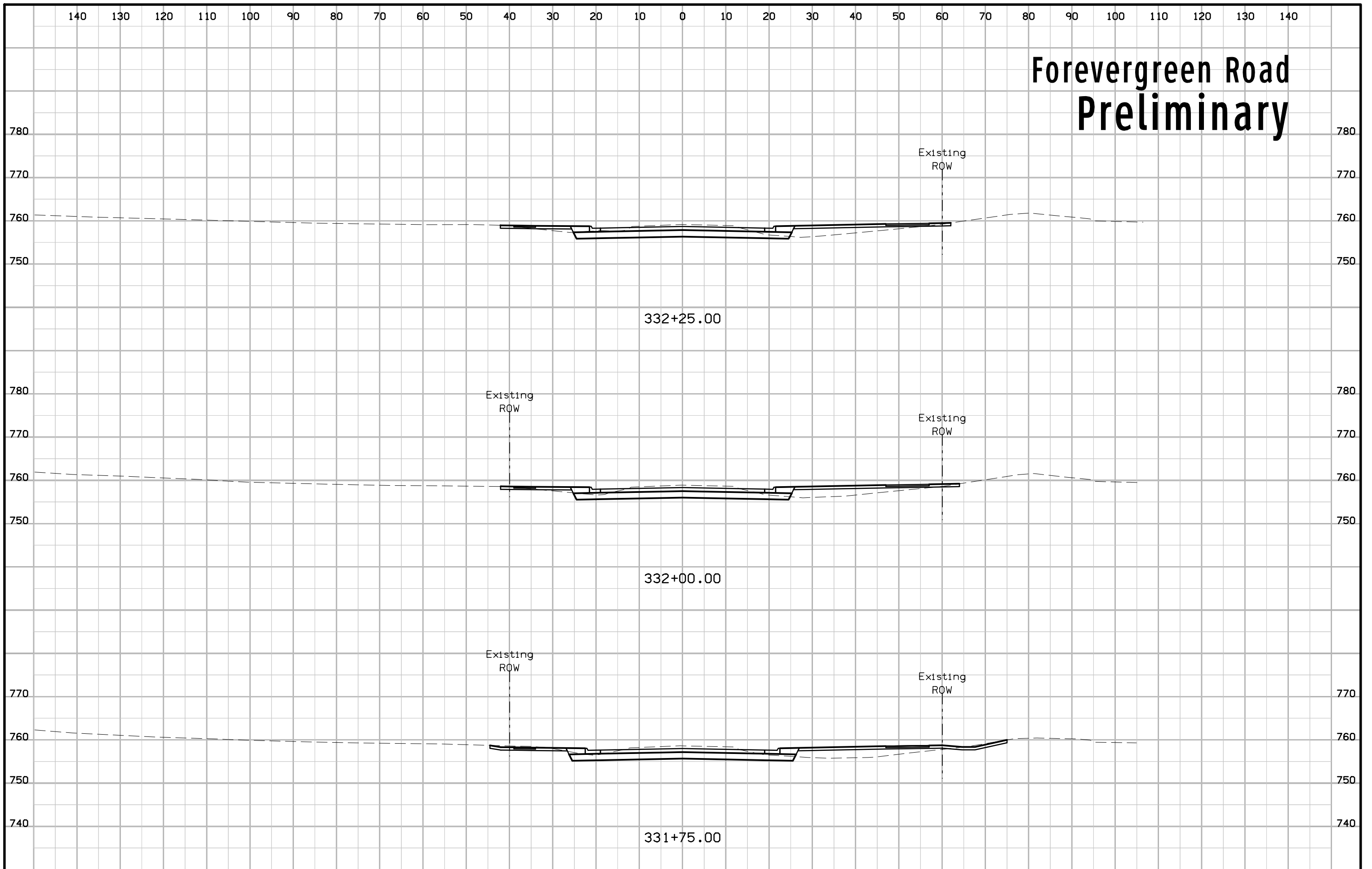
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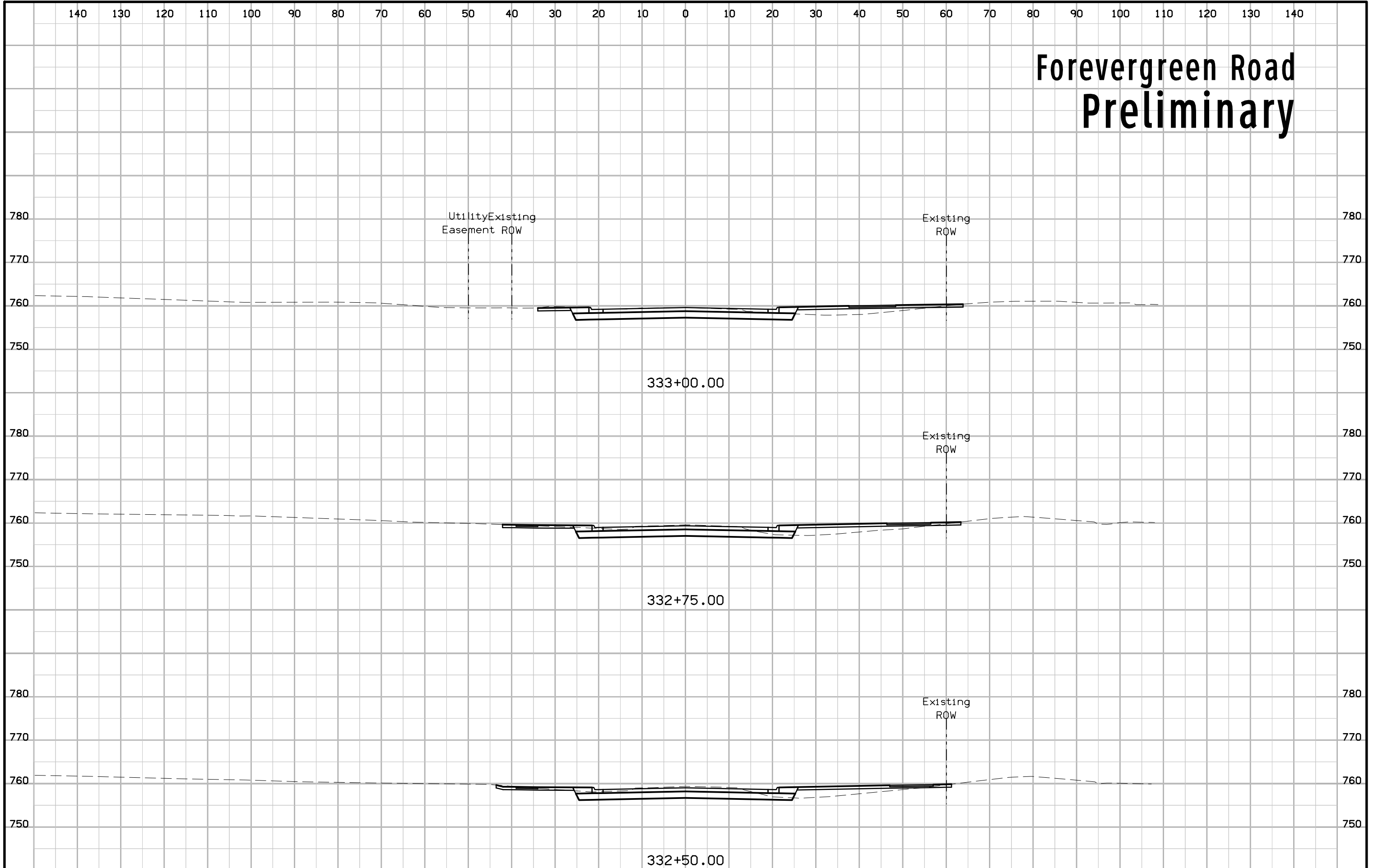
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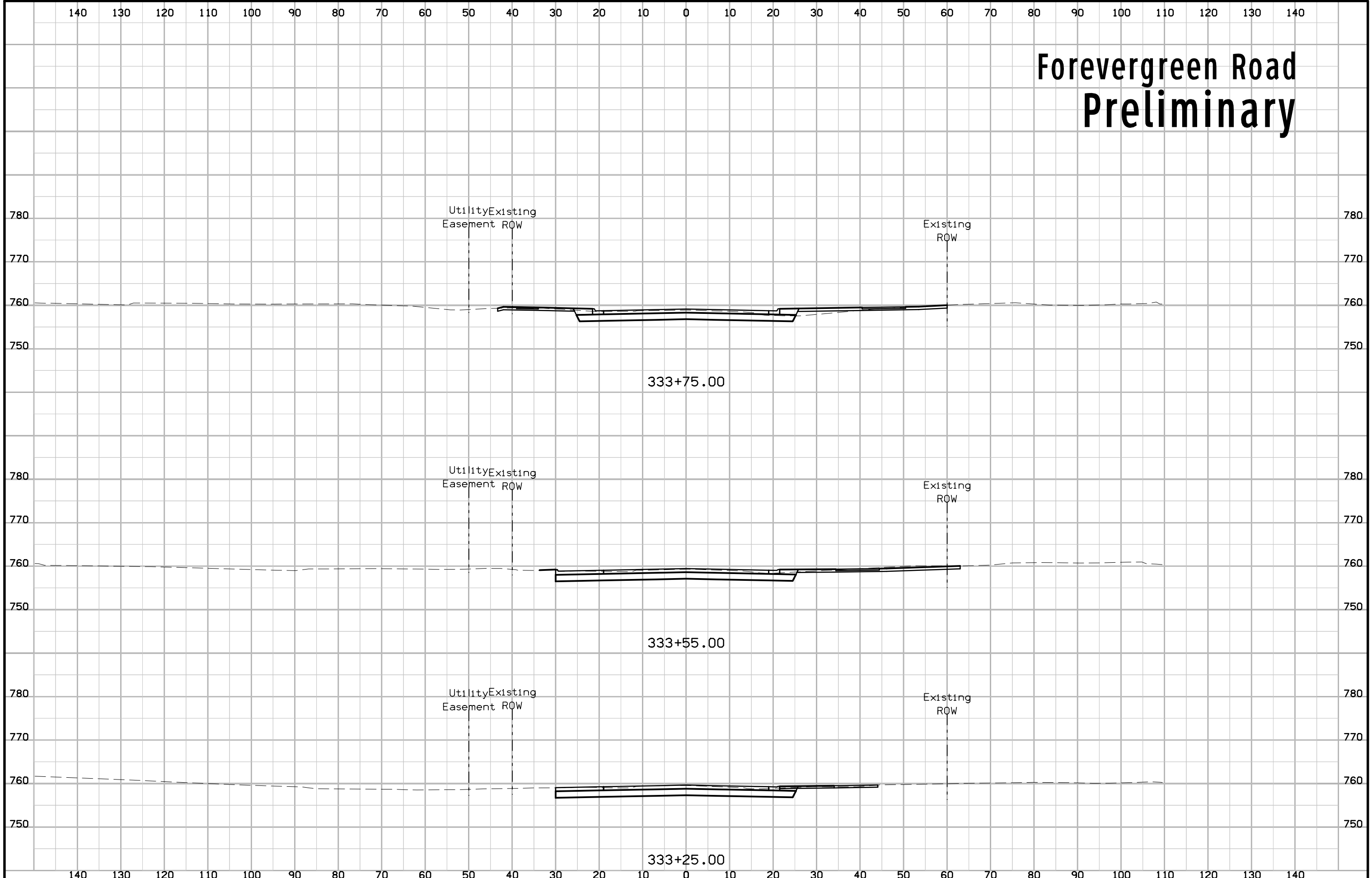
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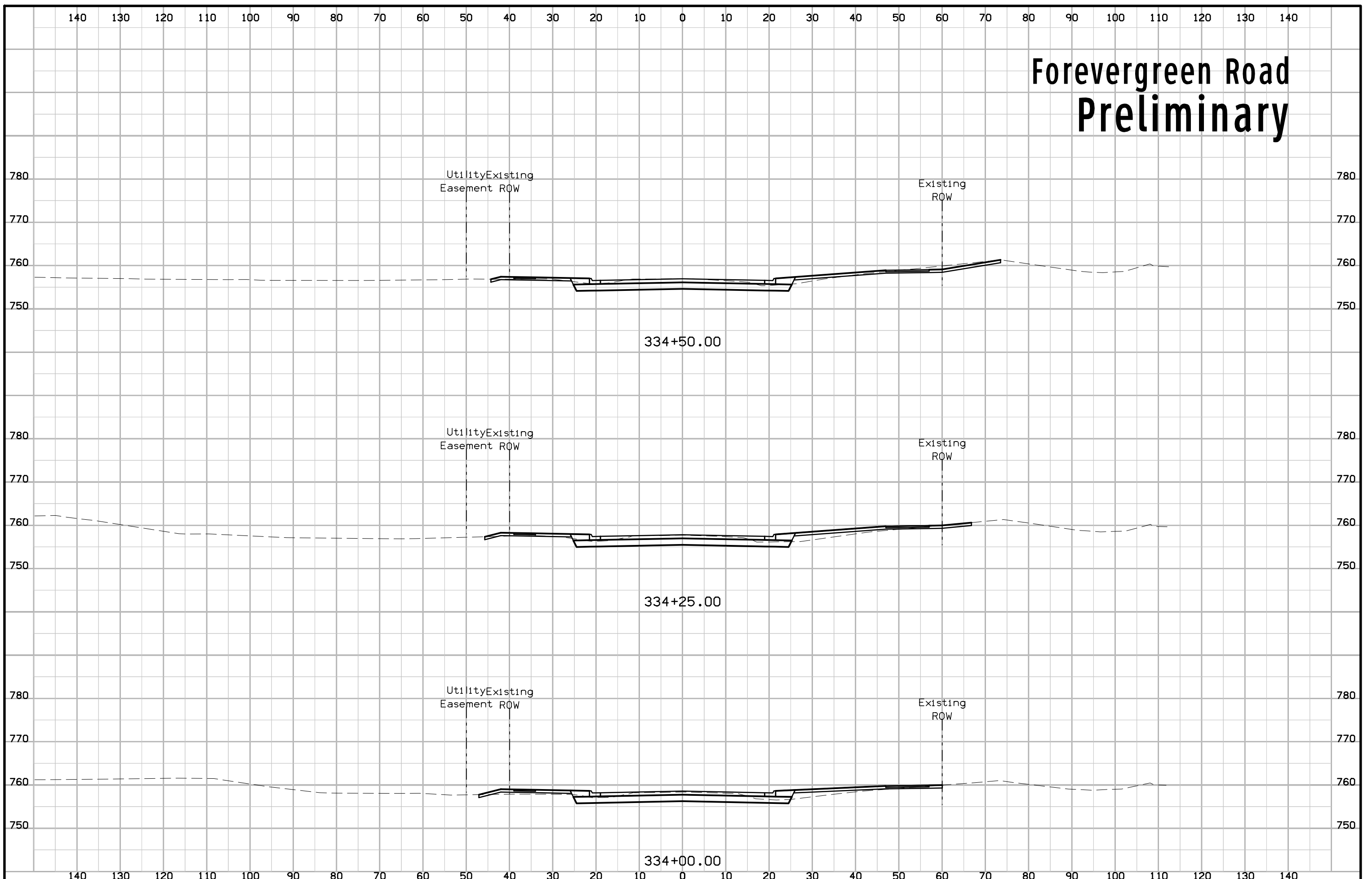
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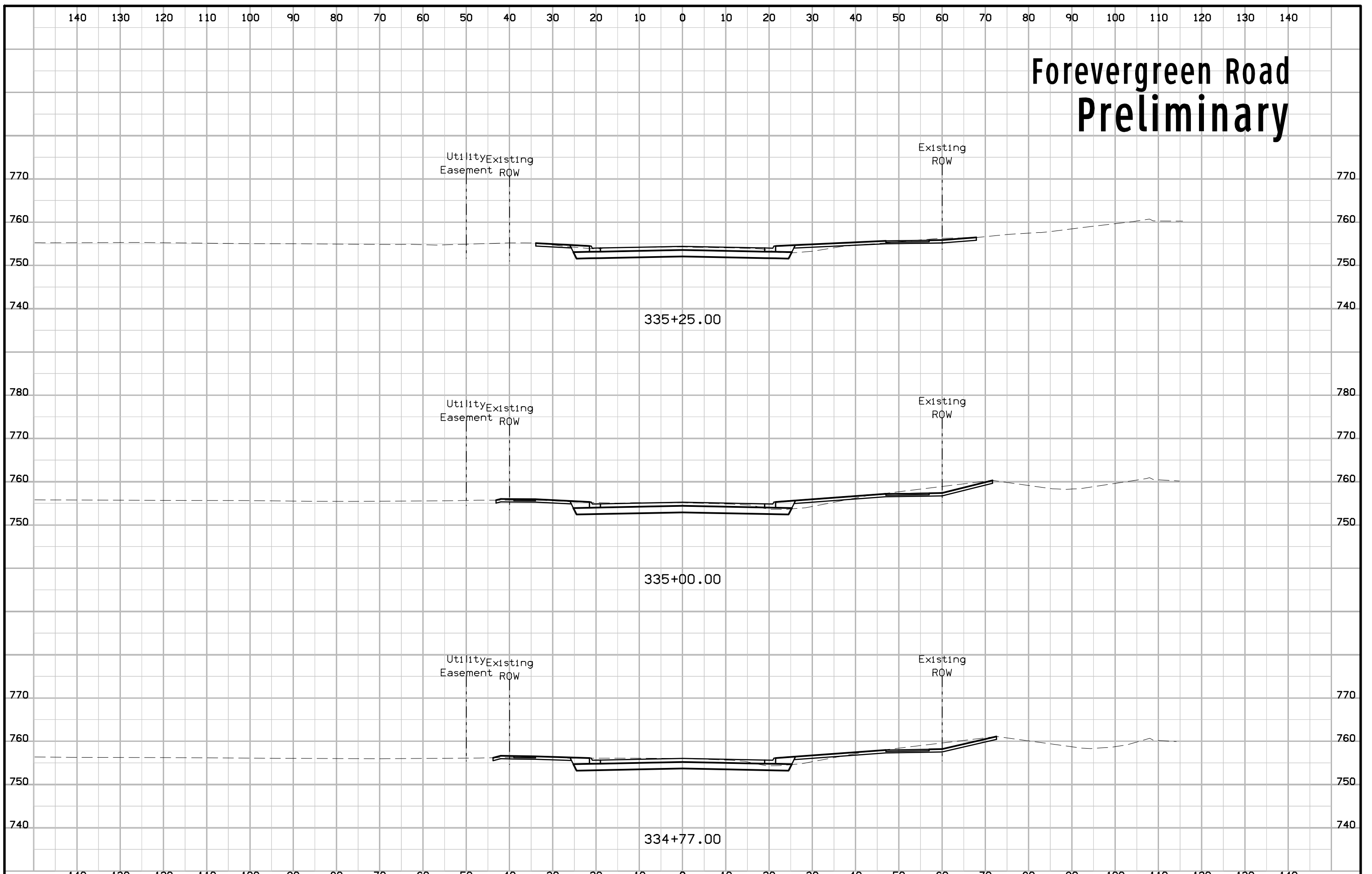
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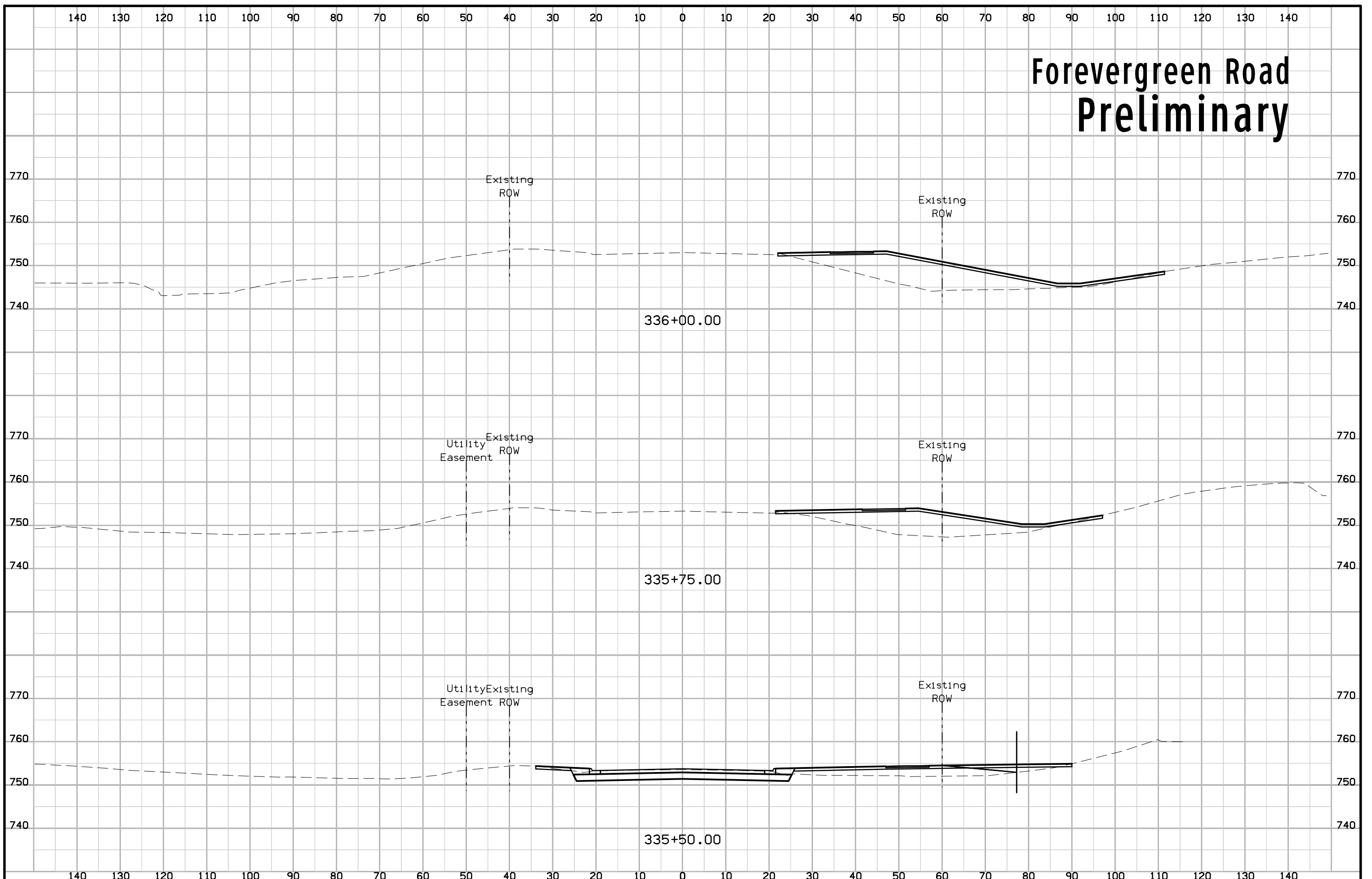
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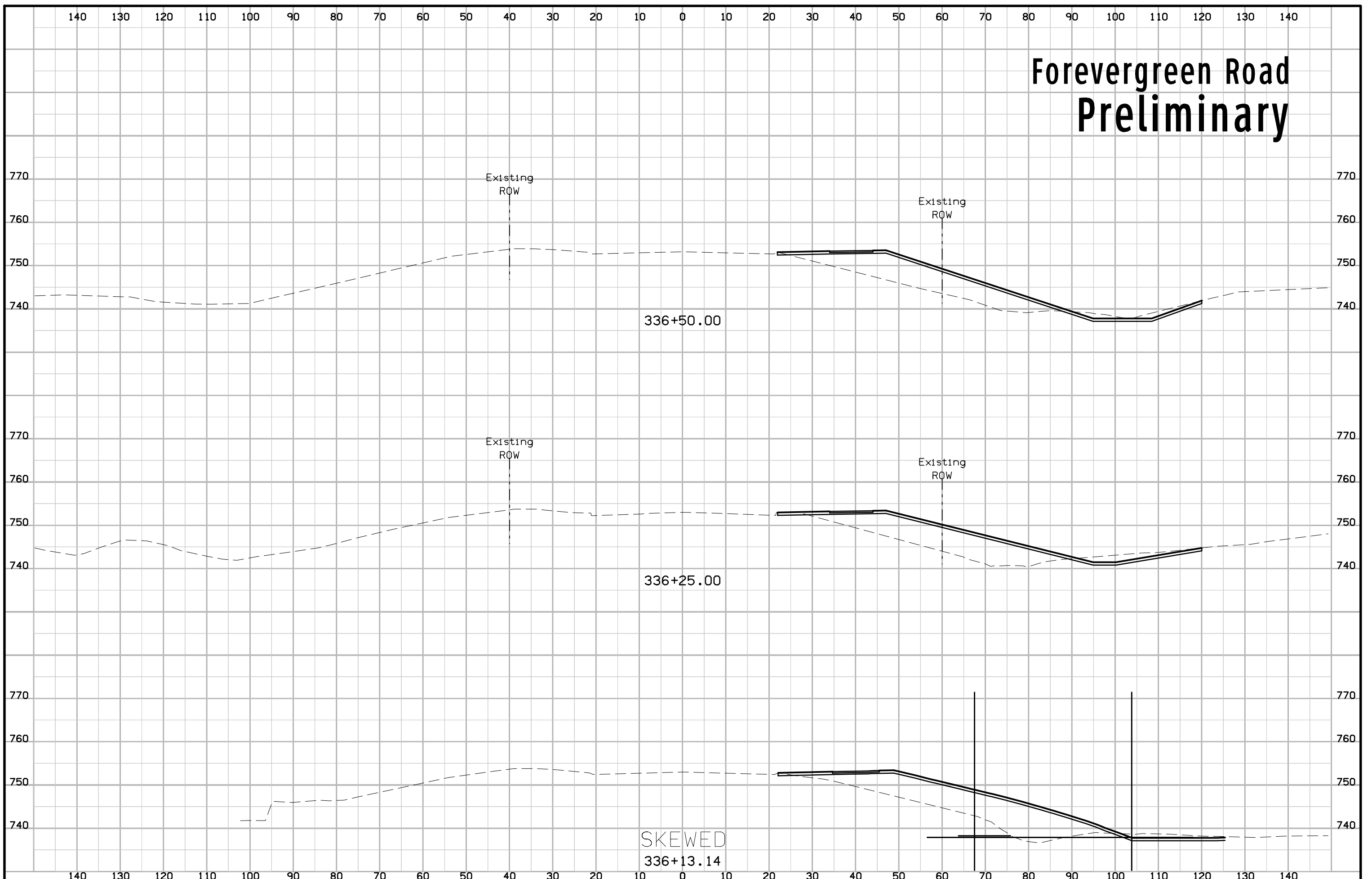
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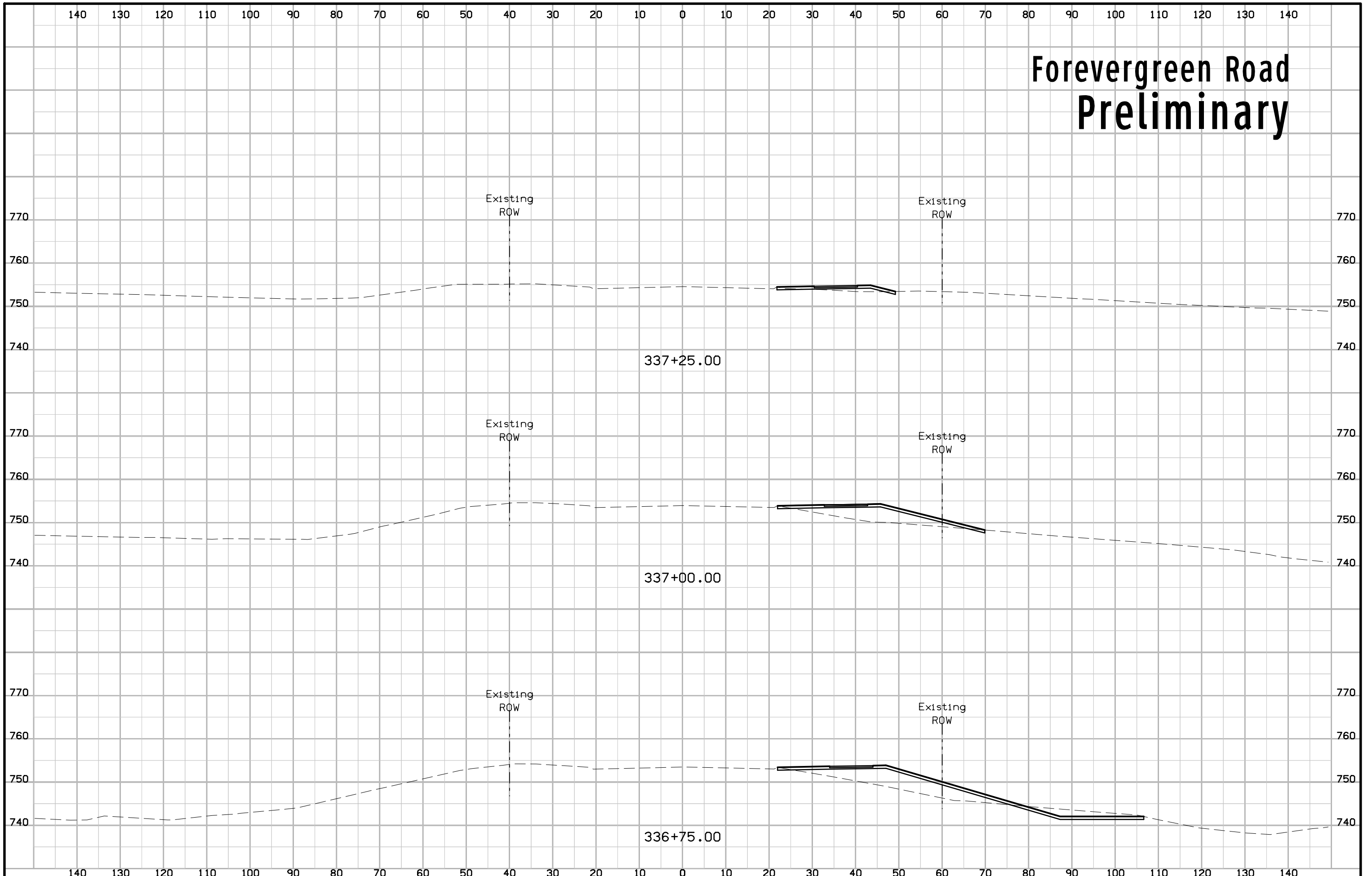
Forevergreen Road Preliminary



Forevergreen Road Preliminary



Forevergreen Road Preliminary



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Forevergreen Road Preliminary

