

**PLYMOUTH CO.**  
**UNKNOWN PAVEMENT - GRADE AND REPLACE**  
**NHSX-075-2(96)--3H-75**

LETTING DATE  
 01/22/20



**Highway Division**

PLANS OF PROPOSED IMPROVEMENT ON THE

PRIMARY ROAD SYSTEM  
**PLYMOUTH COUNTY**  
 PCC PAVEMENT - GRADE AND REPLACE

Woodbury Co. to S of W Grover St in Hinton (SB)

SCALES: As Noted

Refer to the Proposal Form for list of applicable specifications.

Value Engineering Saves. Refer to Article 1105.14 of the Specifications.



REVISIONS

TOTAL

PROJECT IDENTIFICATION NUMBER

16-75-075-020

PROJECT NUMBER

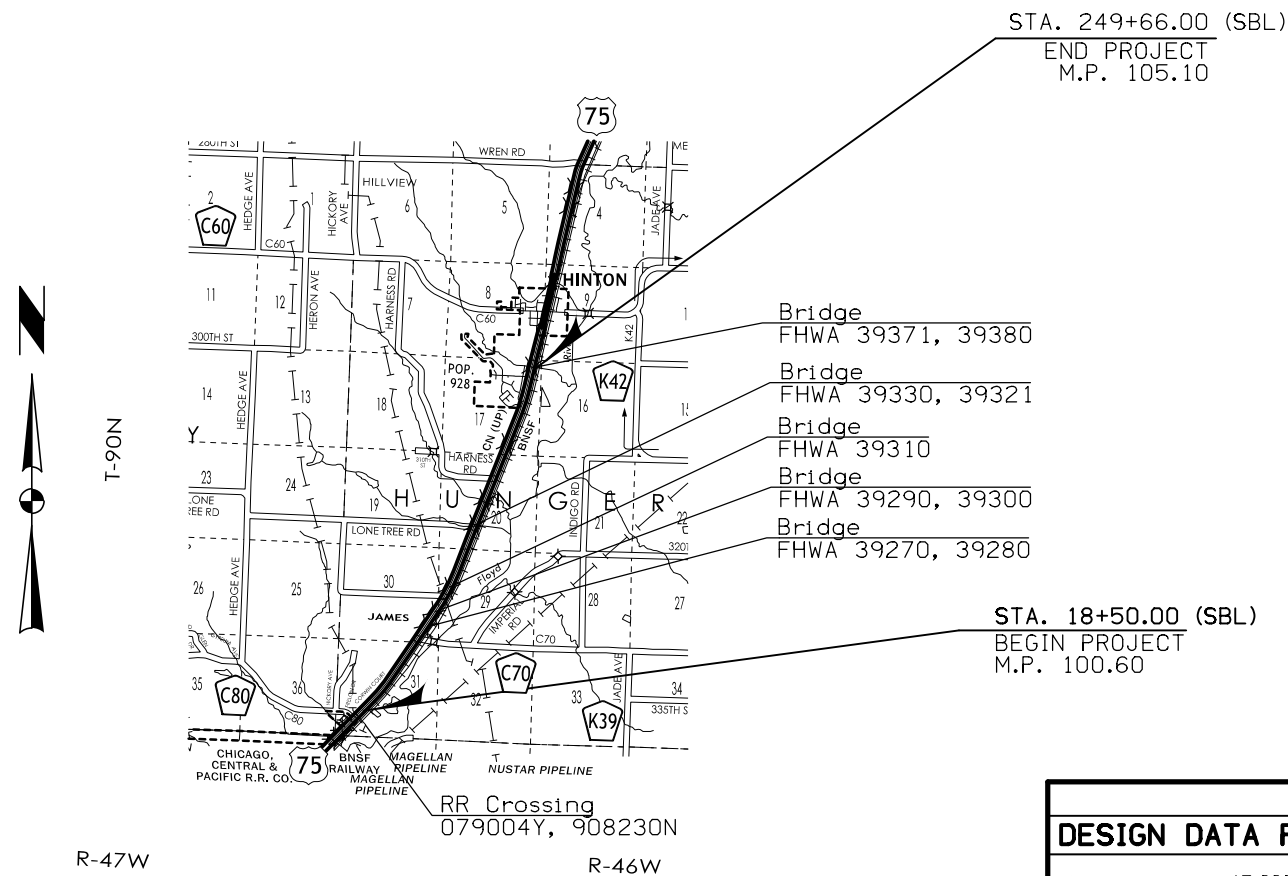
NHSX-075-2(96)--3H-75

R.O.W. PROJECT NUMBER

NHSN-075-2(98)--2R-75

**INDEX OF SHEETS**

No.	DESCRIPTION
<b>A Sheets</b>	<b>Title Sheets</b>
A.1	Title Sheet
<b>B Sheets</b>	<b>Typical Cross Sections and Details</b>
B.1 - 9	Typical Cross Sections and Details
<b>D Sheets</b>	<b>Mainline Plan and Profile Sheets</b>
* D.1	Plan & Profile Legend & Symbol Information Sheet
* D.2	US 75 SB
<b>E Sheets</b>	<b>Side Road Plan and Profile Sheets</b>
* E.1	C-80
* E.2	Plymouth St
* E.3	Main St
* E.4	Fulton St
* E.5	325th St
* E.6	Lone Tree Rd
* E.7	Harness Rd
* E.8	Starview Dr
* E.9	Orbit Dr
* E.10	Titan Rd
<b>G Sheets</b>	<b>Survey Sheets</b>
G.1	Reference Ties and Bench Marks
G.2	Horizontal Control Tab. & Super for all Alignments
<b>V Sheets</b>	<b>Bridge and Culvert Situation Plans</b>
V.1	Bridge and Culvert Situation Plans
<b>X Sheets</b>	<b>Side Road Cross Sections</b>
X.1 - 3	C-80 Cross Sections
X.4 - 5	Plymouth St Cross Sections
X.6 - 7	Main St Cross Sections
X.8 - 9	Fulton St Cross Sections
X.10 - 20	325th St Cross Sections
X.21 - 22	Lone Tree Rd Cross Sections
X.23 - 24	Harnes Rd Cross Sections
X.25 - 26	Starview Dr Cross Sections
X.27	Orbit Dr Cross Sections
X.28 - 30	Tital Rd Cross Sections
	* Color Plan Sheets



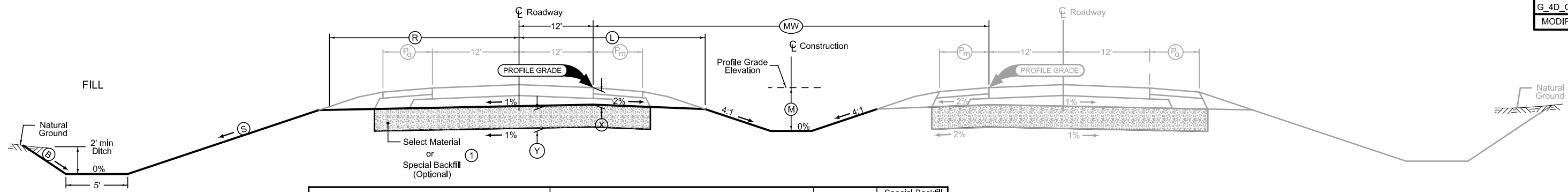
**DESIGN DATA RURAL**

2019 AADT	17,000	V.P.D.
2039 AADT	22,700	V.P.D.
2039 DHV	2,350	V.P.H.
TRUCKS	15	%
Total Design ESALs	--	

**PRELIMINARY PLANS**

Subject to change by final design.

**D5 PLAN - Date: 02-23-2018**

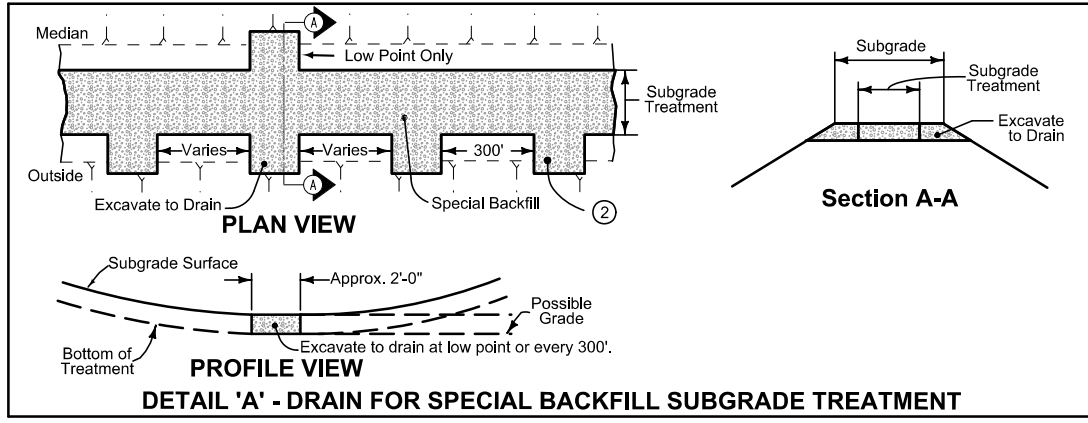


Normal section shown may be modified appropriately in areas of superelevated curves or other locations specifically designated by the Engineer.

See Plan & Profile sheets and cross sections for additional details of ditches and backslopes.

- ① Quantity calculations based on Select Soil.  
Compaction with Moisture Control for Select Soil is required. Maintain moisture content within limits specified in Article 2107.03,H,1 of the Standard Specifications.
- ② Excavate a portion of subgrade as necessary to provide drainage for the treatment. The additional excavation and Special Backfill for outlets is incidental and will not be paid for separately.

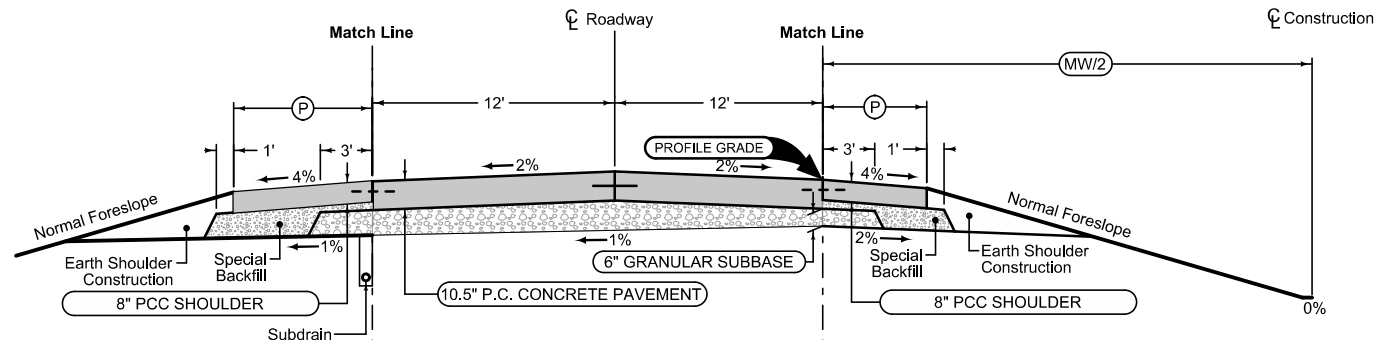
LOCATION		DIMENSIONS							Select Material	Special Backfill (Optional)
ROAD IDENTIFICATION	STATION TO STATION	L Feet	R Feet	X Inches	MW Feet	M Feet	S Feet	B Feet	Y Feet	Y Feet
U.S. 75 (SBL)	18+50.00 - 249+66.18	31.4	25.6	16.5	67-69	Vari.	4:1 / 3:1		2	1



**PCC Paved Shoulder**

PCC Shoulder Jointing:  
 Longitudinal joint: BT-1 or BT-5  
 Transverse joints: C at 20' spacing

Direction of Travel	BEGIN STATION	END STATION	(P) Feet
SB	18+50.00	249+66.18	10



Section shown in the direction of traffic.  
 Mainline Jointing:  
 Transverse joints: CD at 20' spacing  
 Longitudinal joint: L-2

4DP_ MODIFIED			
Direction of Travel	BEGIN STATION	END STATION	(MW) Feet
SB	18+50.00	249+66.18	67-69

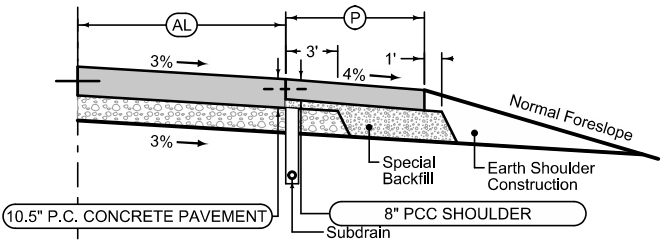
**PCC Paved Shoulder**

PCC Shoulder Jointing:  
 Longitudinal joint: BT-1 or BT-5  
 Transverse joints: C at 20' spacing

Direction of Travel	BEGIN STATION	END STATION	(P) Feet
SB	18+50.00	68+91.94	6
SB	77+00.00	249+66.18	6

**Auxiliary Lane PCC Paved Shoulder**

Auxiliary Lane Jointing:  
 Longitudinal joint: L or KT  
 Transverse joint: Match Mainline  
 PCC Shoulder Jointing:  
 Longitudinal joint: BT-1 or BT-5  
 Transverse joints: C at 20' spacing



4_AuxLane_PCC_				
Direction of Travel	BEGIN STATION	END STATION	(AL) Feet	(P) Feet
SB	68+31.94	68+91.94	0-12	6
SB	68+91.94	75+80.00	12	6
SB	75+80.00	77+00.00	12-0	6

Refer to Tab 100-24 or 100-25 for pavement quantities.  
 Refer to Tab 112-9 for shoulder quantities.

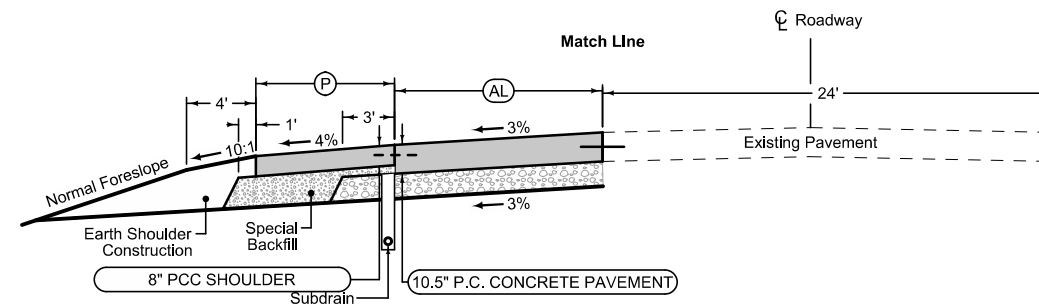
**Auxiliary Lane  
PCC Paved Shoulder**

PCC Shoulder Jointing:  
Longitudinal joint: BT-1 or BT-5  
Transverse joints: C at 20' spacing

**Auxiliary Lane**

Longitudinal joint: L or KT  
Transverse joint: Match Mainline

4_AuxLane_PCC_10-18-16				
Direction of Travel	BEGIN STATION	END STATION	(AL) Feet	(P) Feet
NB	1091+70.00	1092+90.00	0-12.0	6.0
NB	1092+90.00	1095+39.17	12.0	6.0
NB	1095+39.17	1095+99.17	12.0-0	6.0

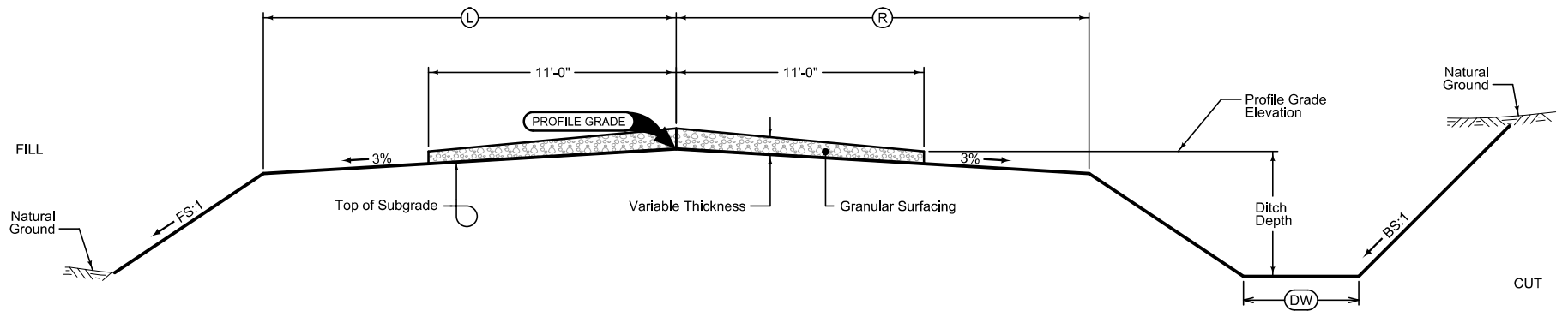


Refer to Tab 100-24 or 100-25 for pavement quantities.  
Refer to Tab 112-9 for shoulder quantities.

**U.S 75 NB**

LOCATION		DIMENSIONS					
ROAD IDENTIFICATION	STATION TO STATION		(L)	(R)	FS	BS	(DW)
			Feet	Feet			Feet
Plymouth St	3064+10.00	3065+17.27	12	12	4 or 6	3	3
Main St	3067+85.00	3069+33.38	12	12	3		
Fulton St	3072+05.00	3073+31.21	13	13	4		

G\_2\_GradeGran  
MODIFIED



**GRADING AND GRANULAR SURFACING**

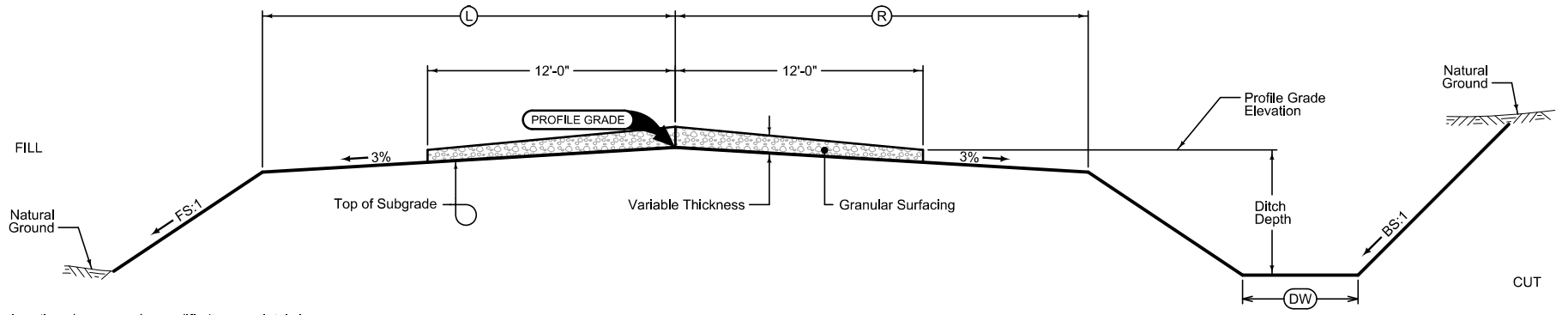
Normal section shown may be modified appropriately in areas of superelevated curves or other locations specifically designated by the Engineer.

See plan & profile sheets and cross sections for additional details of ditches and backslopes.

Place Granular Surfacing as follows:  
Grading design application rate is \_\_\_ tons per mile.  
Paving design application rate is \_\_\_ tons per mile.

LOCATION		DIMENSIONS					
ROAD IDENTIFICATION	STATION TO STATION		(L)	(R)	FS	BS	(DW)
			Feet	Feet			Feet
325th St	3087+50.00	3094+68.00	13	13	3	3	3
Lone Tree Rd	3137+31.00	3138+00.11	15	15	3	U.A.C.	U.A.C.
Harness Rd	3165+00.00	3166+51.84	13	13	6		

G\_2\_GradeGran  
04-17-12



**GRADING AND GRANULAR SURFACING**

Normal section shown may be modified appropriately in areas of superelevated curves or other locations specifically designated by the Engineer.

See plan & profile sheets and cross sections for additional details of ditches and backslopes.

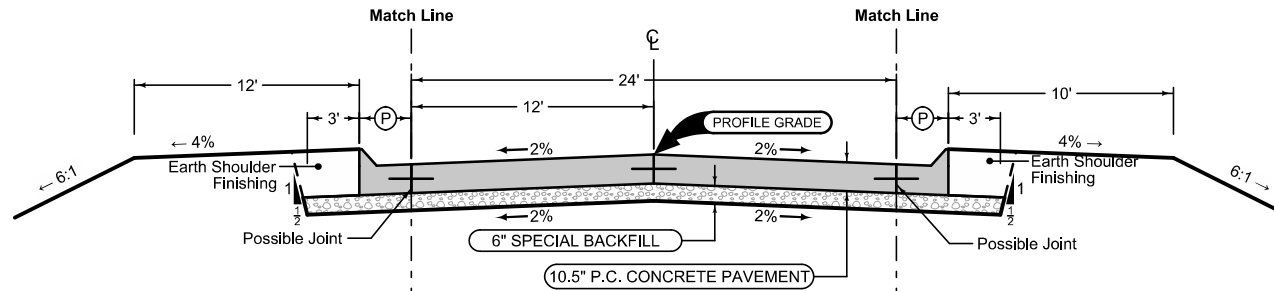
Place Granular Surfacing as follows:  
Grading design application rate is \_\_\_ tons per mile.  
Paving design application rate is \_\_\_ tons per mile.

**Curbed Shoulder**

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

2_Curb_04-19-11			
STATION TO STATION	(P) Feet	Curb Type See PV-102	
3210+35.00	3212+08.00	5.5	6" Standard



Mainline Jointing:  
 Transverse joints: CD at 20' spacing  
 Longitudinal joint: L-2

2P_10-19-10	
STATION TO STATION	
3210+35.00	3212+08.00

**Curbed Shoulder**

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

2_Curb_04-19-11			
STATION TO STATION	(P) Feet	Curb Type See PV-102	
3210+35.00	3212+08.00	5.5	6" Standard

See Tab 100-24 or 100-25 for pavement quantities.  
 See Tab 112-9 for shoulder quantities.

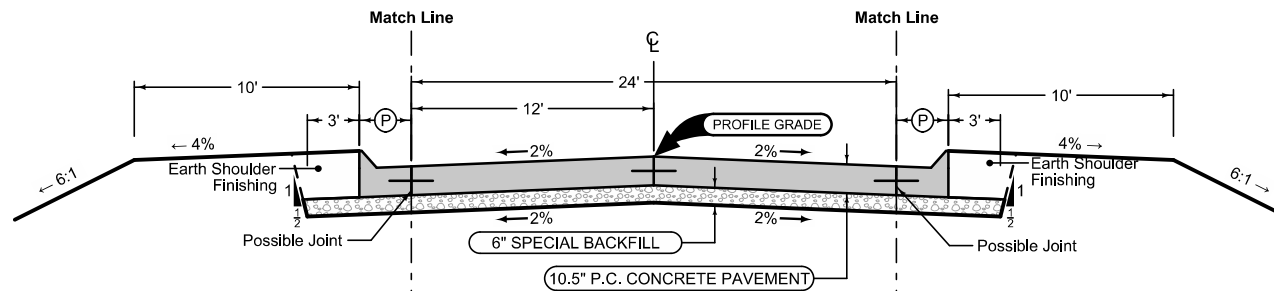
**STARVIEW RD**

**Curbed Shoulder**

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

2_Curb_04-19-11			
STATION TO STATION	(P) Feet	Curb Type See PV-102	
3224+57.00	3225+92.57	3	6" Standard



Mainline Jointing:  
 Transverse joints: CD at 20' spacing  
 Longitudinal joint: L-2

2P_10-19-10	
STATION TO STATION	
3224+57.00	3225+92.57

**Curbed Shoulder**

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

2_Curb_04-19-11			
STATION TO STATION	(P) Feet	Curb Type See PV-102	
3224+57.00	3225+92.57	3	6" Standard

See Tab 100-24 or 100-25 for pavement quantities.  
 See Tab 112-9 for shoulder quantities.

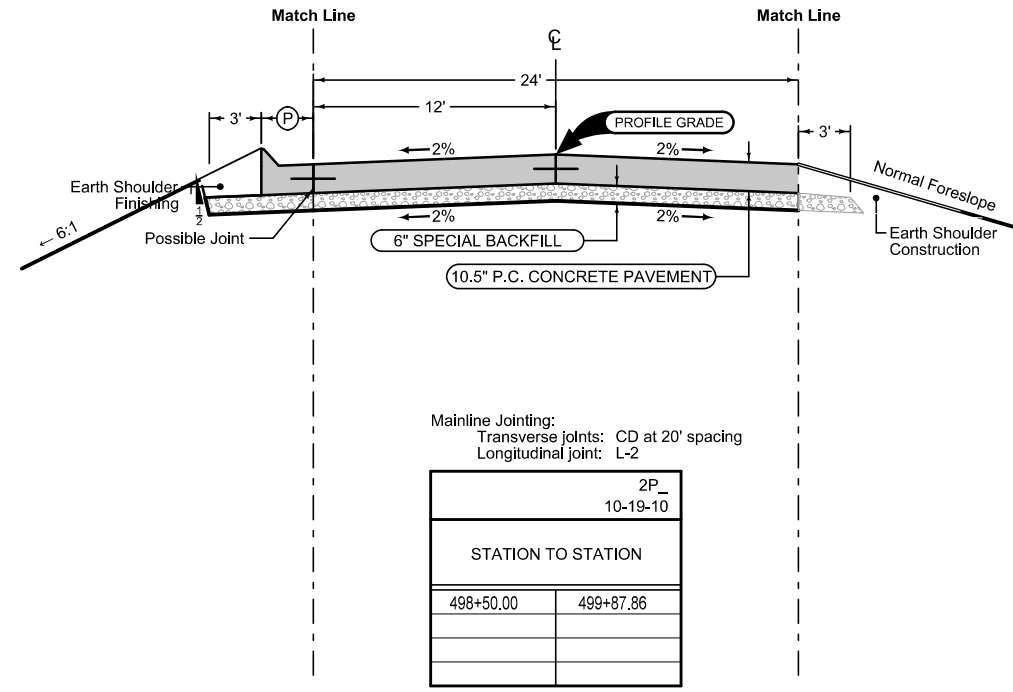
**TITAN RD**

**Curbed Shoulder**

Shoulder Joining:  
 Longitudinal joint not required when distance from back of  
 curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

		2_Curb_ 04-19-11	
STATION TO STATION		(P) Feet	Curb Type See PV-102
498+50.00	499+87.86	2.5	6" Standard



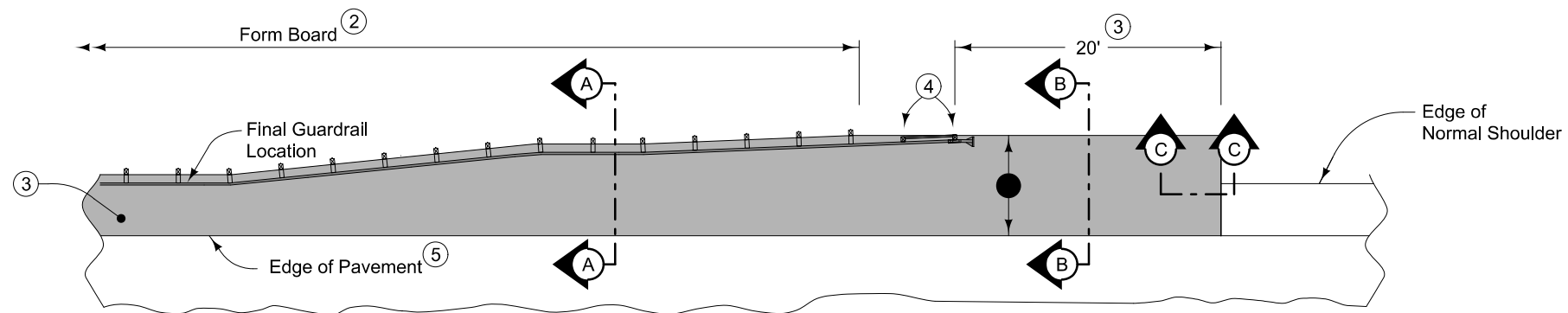
Mainline Joining:  
 Transverse joints: CD at 20' spacing  
 Longitudinal joint: L-2

		2P_ 10-19-10	
STATION TO STATION			
498+50.00	499+87.86		

STATION TO STATION	
498+50.00	499+87.86

See Tab 100-24 or 100-25 for pavement quantities.  
 See Tab 112-9 for shoulder quantities.

**ORBIT DR**



PLAN VIEW

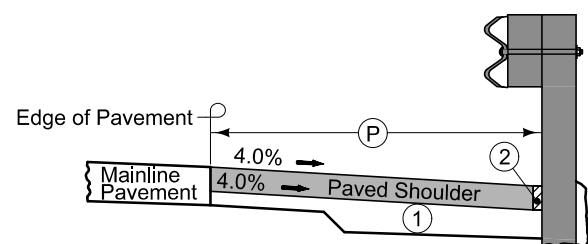
9" HMA Paved Shoulder at guardrail. 8" PCC may be substituted with the following jointing layout:

Match mainline pavement joint spacing. When mainline pavement is 8" or greater in thickness, place additional transverse 'C' joints in shoulder at mid-panel of the mainline pavement. Place longitudinal 'C' joint at P/2 from edge of mainline pavement when P is greater than 10' wide. Terminate longitudinal joint at transverse joint less than 10' in length.

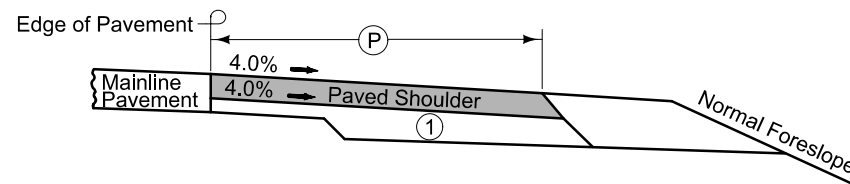
Compaction of HMA is required to face of guardrail post. Hand compaction will be allowed under guardrail. Removal and reinstallation of guardrail will be allowed with no additional payment.

Refer to Tabulation 112-9 for shoulder quantities.

- ① For subgrade treatment, refer to other details in the plan.
- ② PCC option only: When guardrail posts are installed prior to construction of PCC paved shoulder, fasten form board to the face of guardrail posts for the length shown. Refer to note 4 for final 2 posts.
- ③ Continue paved shoulder to existing paved shoulder or 20 feet beyond the center of the first post.
- ④ Shoulder may be notched for final 2 posts or post sleeves may be installed through pavement. Do not drive posts through pavement.
- ⑤ 'KT-1 joint for PCC shoulder.  
'B' joint for HMA shoulder.

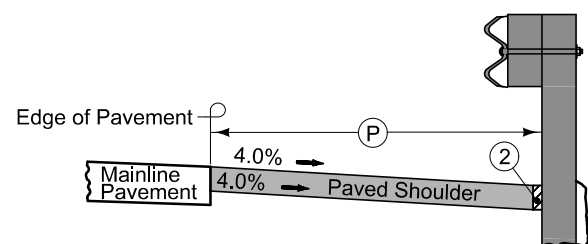


Section A-A

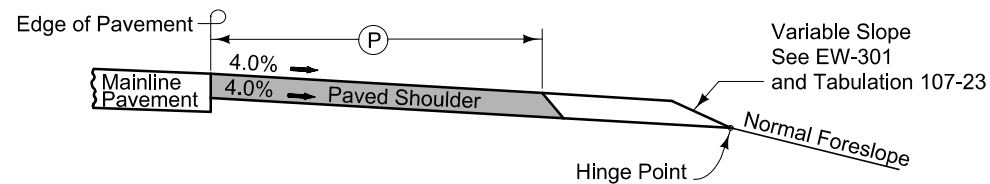


Section B-B

NEW CONSTRUCTION

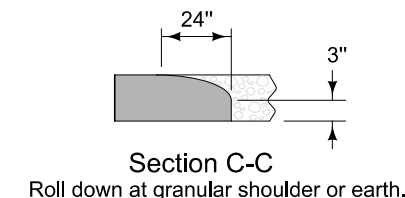


Section A-A



Section B-B

EXISTING SHOULDER



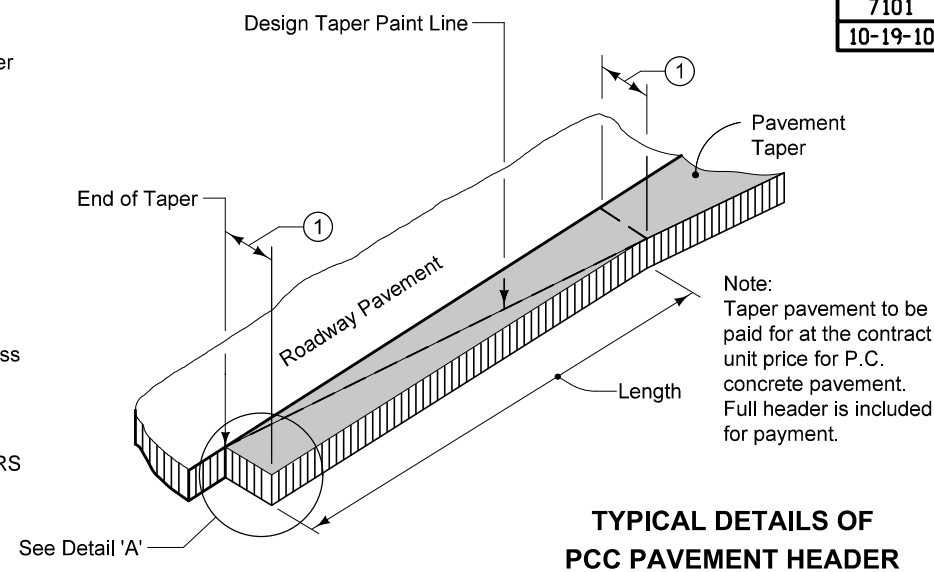
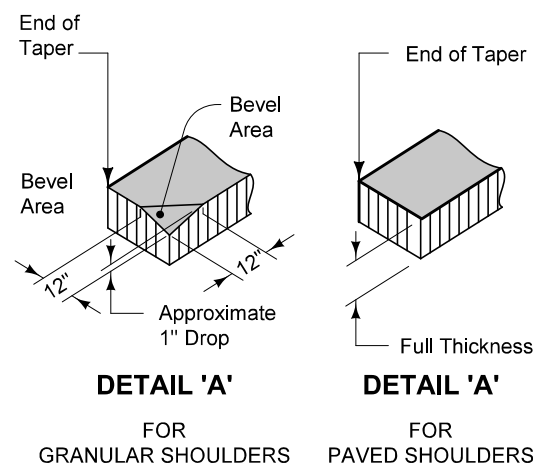
Section C-C

Roll down at granular shoulder or earth.

PAVED SHOULDER AT GUARDRAIL

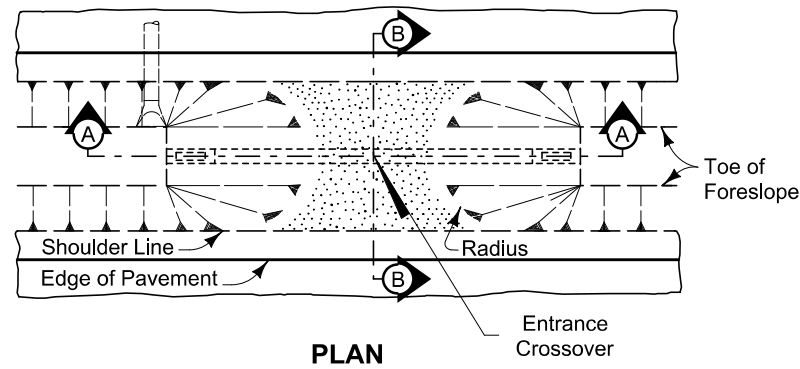


7101  
10-19-10

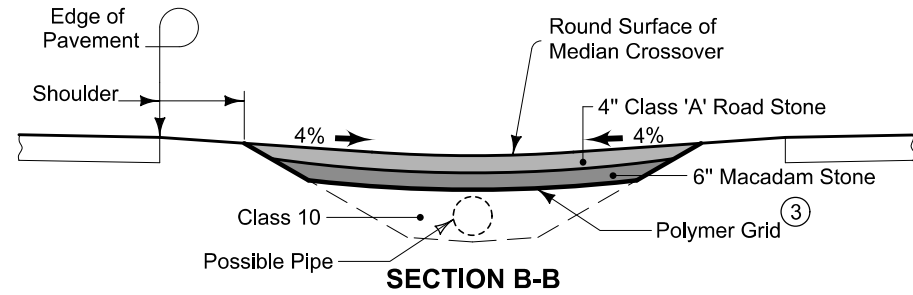
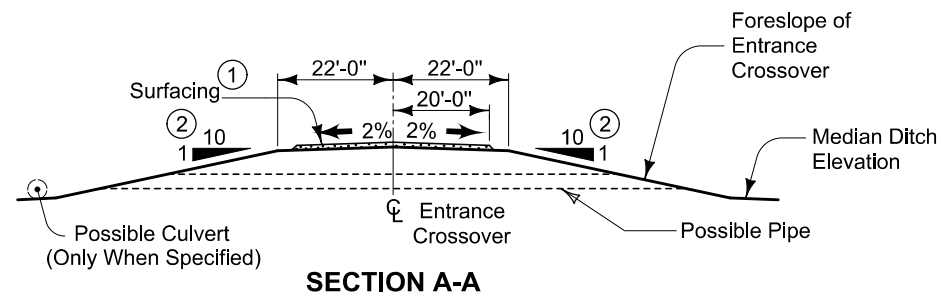


① Normal width is 2'-0". Construct 4'-0" width when butting into 4' wide HMA shoulders (See Typical 7154A).

8101  
MODIFIED



- ① Surfacing quantities based on a 6 inch layer of Macadam Stone base and a 4 inch layer of Class 'A' Road Stone. Apply surfacing as directed by the Engineer.
- ② Construct 8:1 foreslope when drainage pipe is incorporated into the maintenance turnaround.
- ③ Install Polymer Grid between Class 10 and stone material.
- ④ See Standard Road Plan DR-212.



**MEDIAN ENTRANCE CROSSOVER**

Location		Class 'A' Road Stone	Macadam Stone	Polymer Grid	Class 10	Pipe Length	Beveled Pipe & Guard ④	Radius	Remarks
Road Identification	Station	TONS	TONS	SY	CY	LF	EACH	FT	
U.S. 75 (SBL)	30+56.42	x	x	x		-	-	x	
	48+69.79	x	x	x		-	-	x	
	112+46.69	x	x	x		-	-	x	
	125+02.84								
	142+86.83								
	151+90.00								
	156+75.00								
194+48.00									

### SURVEY SYMBOLS

- DIK Centerline of Dike or Dam
- ENU Edge Unpaved Entrance & Parking
- TEV Evergreen Tree
- TDC Tree Deciduous
- PPA Power Pole Co. 1
- SI Sign
- RR Centerline of Railroad Tracks
- RIP Rip-Rap
- RET Retaining Walls
- FCL Chain Link and Security Fence
- RRS Railroad Signal
- SHR Shrub
- FWD Wood Fence
- HDG Hedge Row
- GDG Guard Rail Steel
- BNK Stream Bank
- EW Edge of Water
- IN Storm Sewer Intake
- HT Electrical Highline Tower
- LUM Luminaire
- MH Utility Access (Manhole)
- FHD Fire Hydrants
- FLG Flag Poles
- IN Storm Sewer Intake
- OUT Tile Outlet
- TIL Tile Line
- INB Storm Sewer Beehive Intake
- MH Utility Access (Manhole)
- DU Centerline Draw or Stream (Up)
- D Centerline Draw or Stream (Down)
- ST1B Storm Sewer Co. 1 - Quality B
- TPD Telephone Pedestal
- PPA Power Pole Co. 1
- EB Electrical Box
- UV Underground Utility Vault
- WV Water Valve
- PR Electric Riser Pole
- LUM Luminaire
- GV Gas Valve
- E3 EL3D Electric Line Co. 3 - Quality D
- F02 FO2D Fiber Optic Co. 2 - Quality D
- G-HP GH1D High Pres Gas Co 1 - Quality D
- F03 FO3D Fiber Optic Co. 3 - Quality D
- F0 FO1D Fiber Optic Co. 1 - Quality D
- F04 FO4D Fiber Optic Co. 4 - Quality D
- E1 EL1D Electric Line Co. 1 - Quality D

### UTILITY LEGEND

- San.(B) - City of Hinton- Quality B
- St.S. -
- PPA Midamerican Energy
- F0(B) - Nuetrul Path Comm
- F02(B) - MCI FIBER
- F03(B) - Fiber Comm
- F04(B) - Premier Comm
- F05(B) - Century Link
- F06(B) - Long Lines
- F07(B) - Winstream Communication
- E(B) - City of Hinton
- E2(B) - Midamerican Energy
- E3(B) - Northwest REC
- G2(B) - NUSTAR
- G3HP(B) - Midamerican Energy
- T(B) - Century Link
- T2(B) - Premier (Abandoned)
- TV(B) - Cable One
- W(B) - City of Hinton

### PLAN VIEW COLOR LEGEND OF PLAN AND PROFILE SHEETS

LINEWORK	Design	Color No.	Description
Green	(2)		Existing Topographic Features and Labels
Blue	(1)		Proposed Alignment, Stationing, Tic Marks, and Alignment Annotation
Magenta	(5)		Existing Utilities
SHADING			
SHADING	Design	Color No.	Description
Yellow	(4)		Highlight for Critical Notes or Features
Red	(3)		Delineates Restricted Areas
Lavender	(9)		Temporary Pavement Shading
Gray, Light	(48)		Proposed Pavement Shading
Gray, Med	(80)		Proposed Granular Shading
Gray, Dark	(112)		Proposed Grade and Pave Shading "In conjunction with a paving project"
Brown, Light	(236)		Grading Shading
Tan	(8)		Proposed Sidewalk Shading
Blue, Light	(230)		Proposed Sidewalk Landing Shading
Pink	(11)		Proposed Sidewalk Ramp Shading

### PROFILE VIEW COLOR LEGEND OF PLAN AND PROFILE SHEETS

LINEWORK	Design	Color No.	Description
Green	(2)		Existing Ground Line Profile
Blue	(1)		Proposed Profile and Annotation
Magenta	(5)		Existing Utilities
Blue, Light	(230)		Proposed Ditch Grades, Left
Black	(0)		Proposed Ditch Grades, Median
Rust	(14)		Proposed Ditch Grades, Right

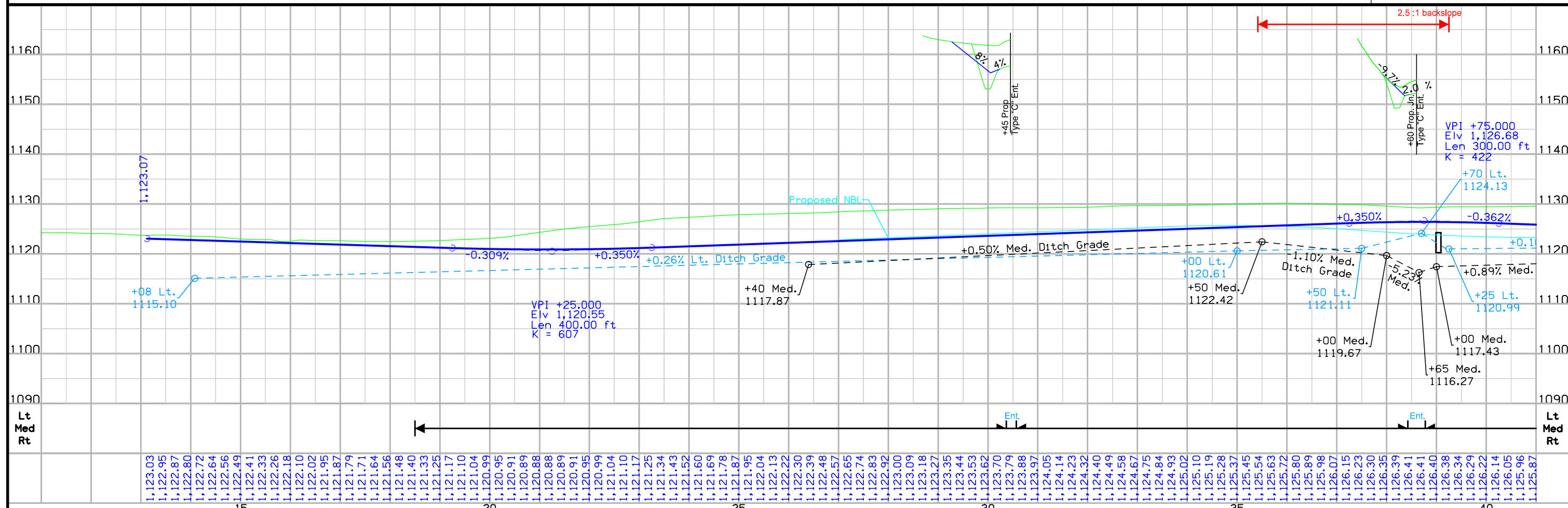
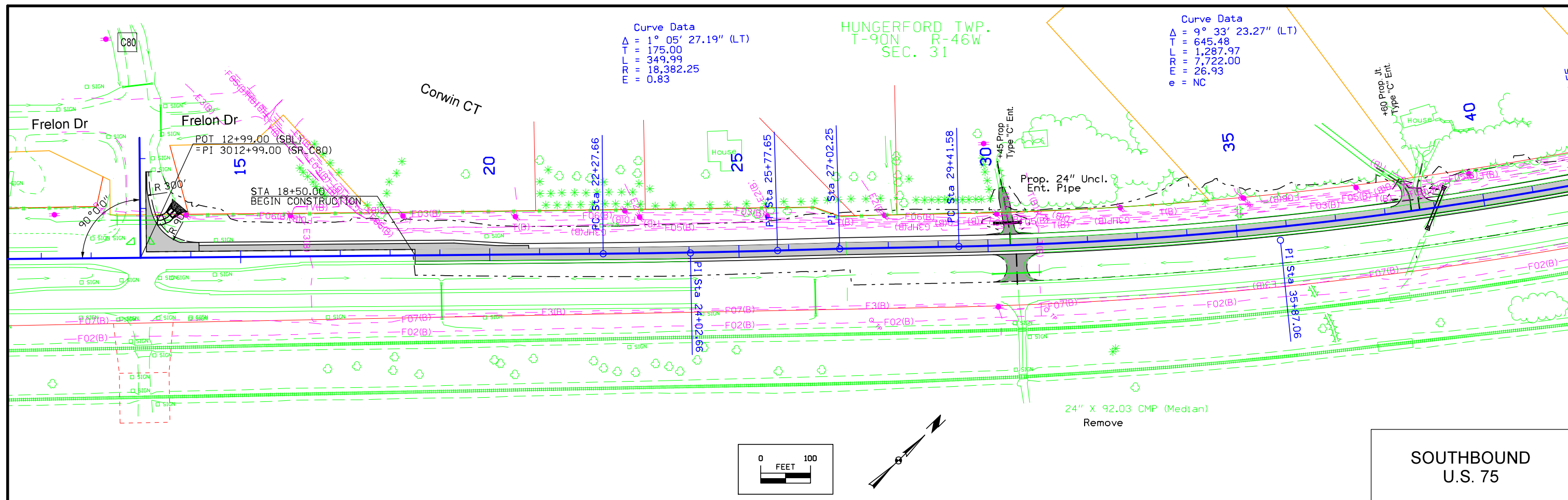
- Reference Point
- Station
- Survey Line
- Section Corner
- Ground Line Intercept
- Saw Cut
- Guardrail
- Trench Drain
- HighTension Cable Guardrail
- Sheet Pile
- Pavement Removal
- Clearing & Grubbing Area

### RIGHT-OF-WAY LEGEND

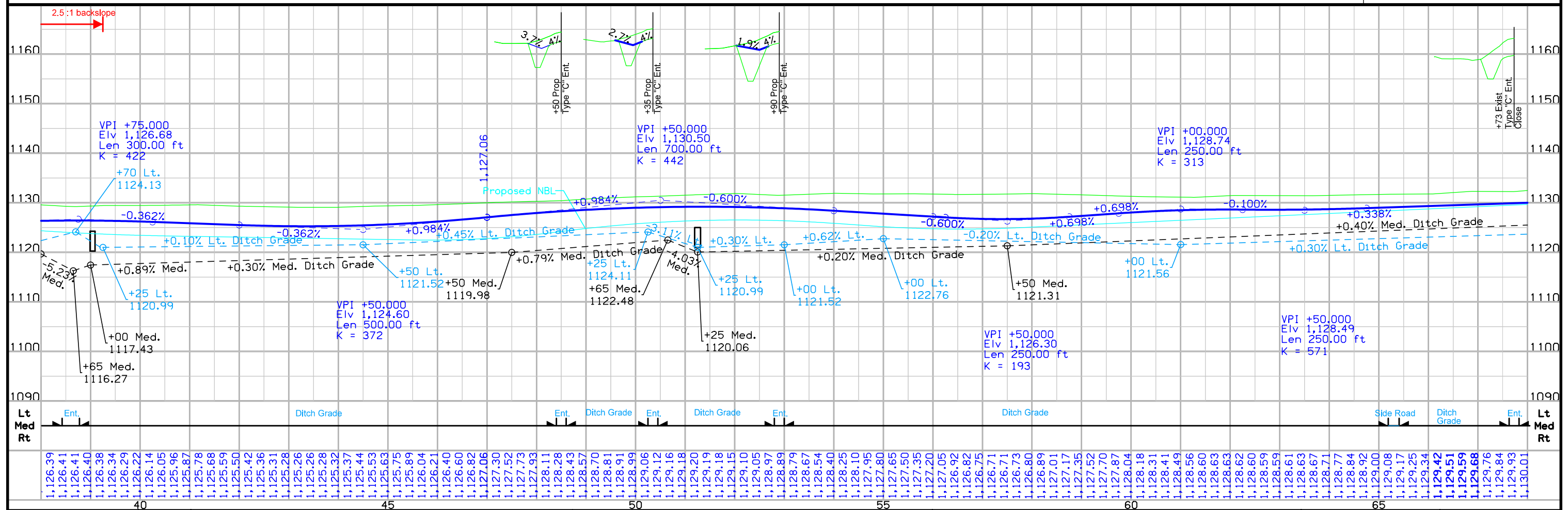
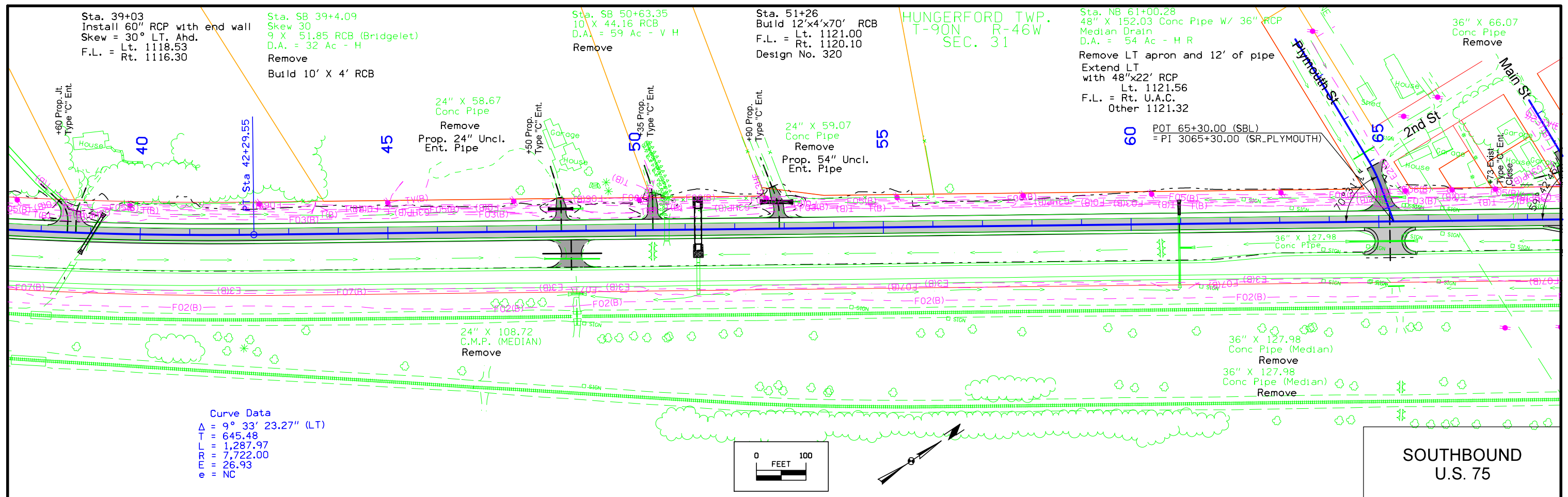
- Proposed Right-of-Way
- Existing Right of Way
- Existing and Proposed Right-of-Way
- Easement and Existing Right-of-Way
- Easement (Temporary)
- Easement
- Access Control
- Property Line

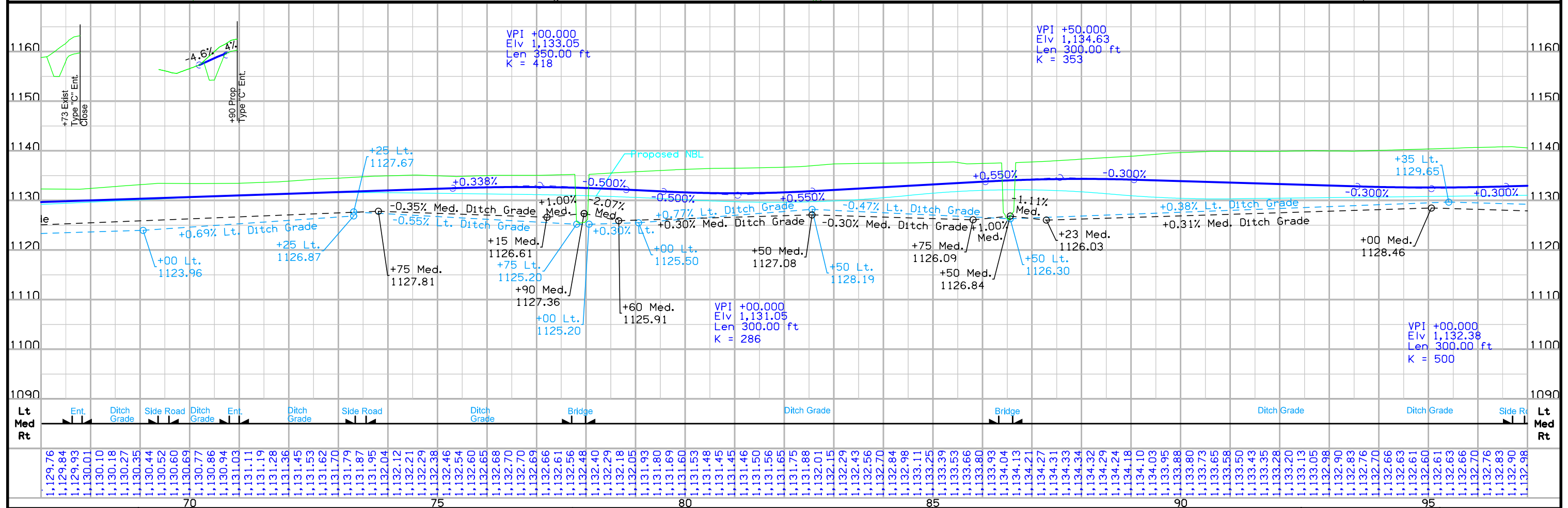
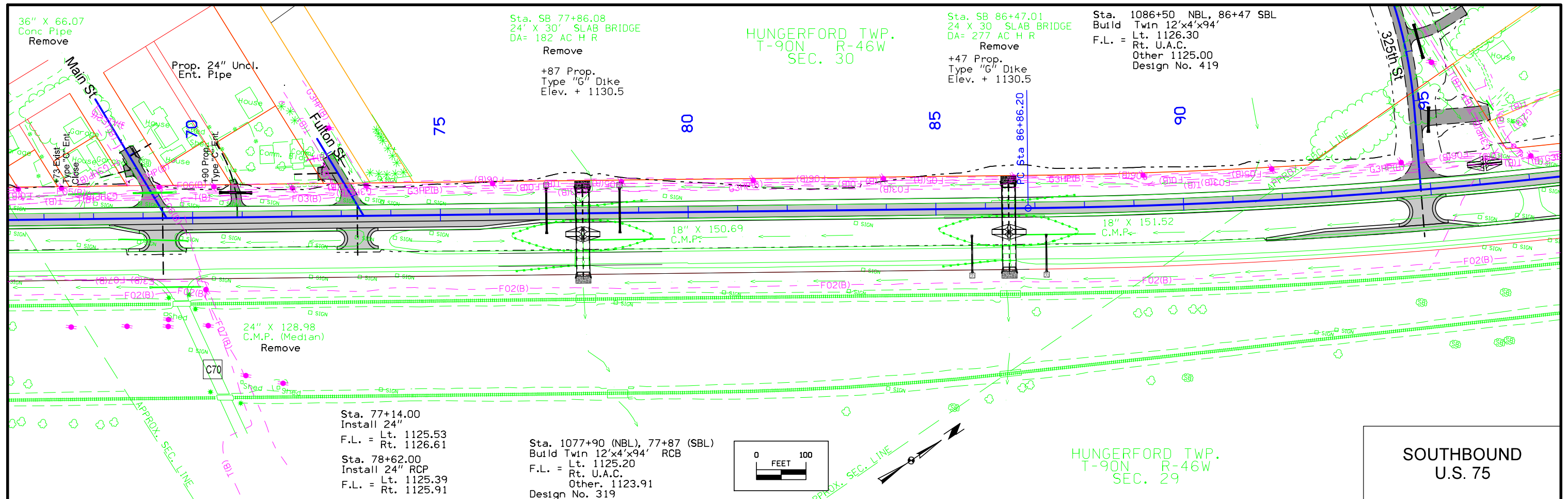
# PLAN AND PROFILE LEGEND AND SYMBOL INFORMATION SHEET

(COVERS SHEET SERIES D, E, F, & K)

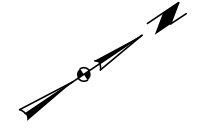
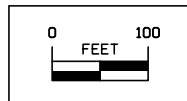
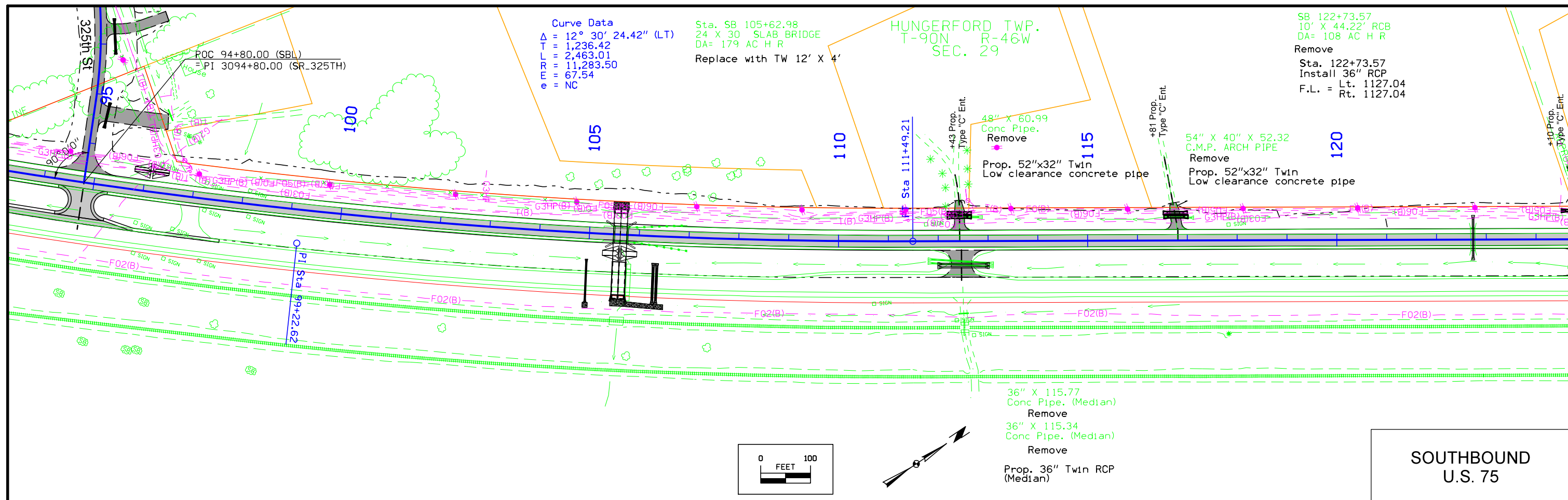


FILE NO.	ENGLISH	DESIGN TEAM	PLYMOUTH COUNTY	PROJECT NUMBER	SHEET NUMBER
		Holst \ Strum \ Janus		NHSX-075-2(96)--3H-75	D.2

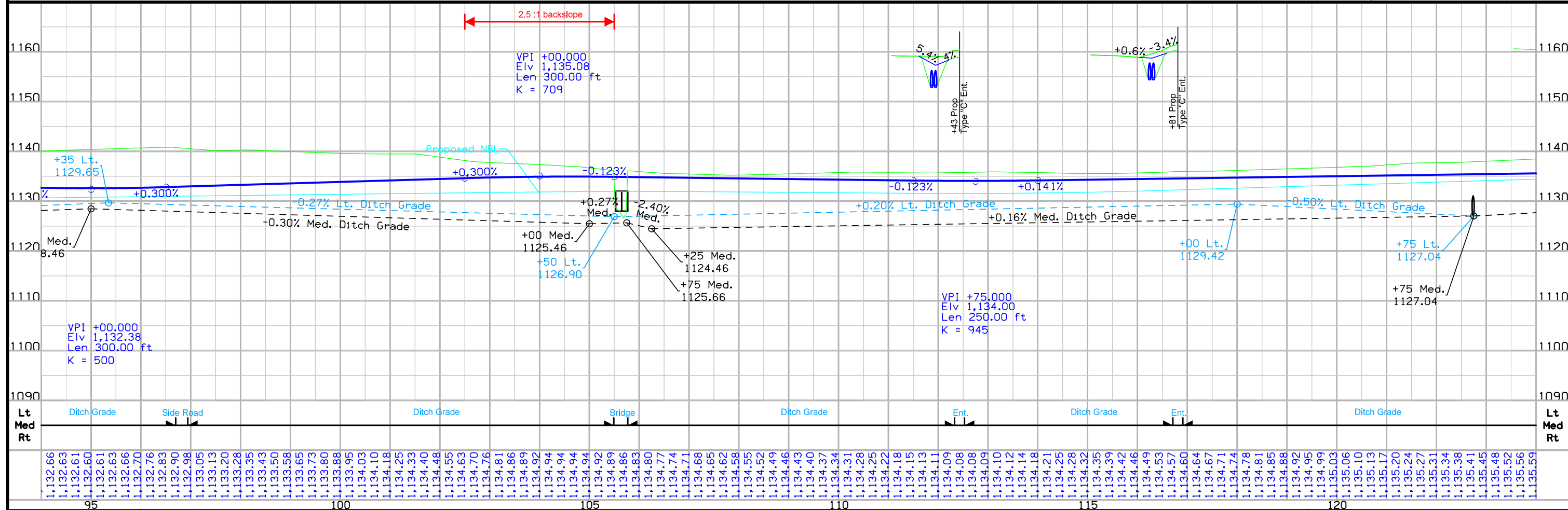




FILE NO.	ENGLISH	DESIGN TEAM	PLYMOUTH COUNTY	PROJECT NUMBER	SHEET NUMBER
		Holst \ Strum \ Janus		NHSX-075-2(96)--3H-75	D.4

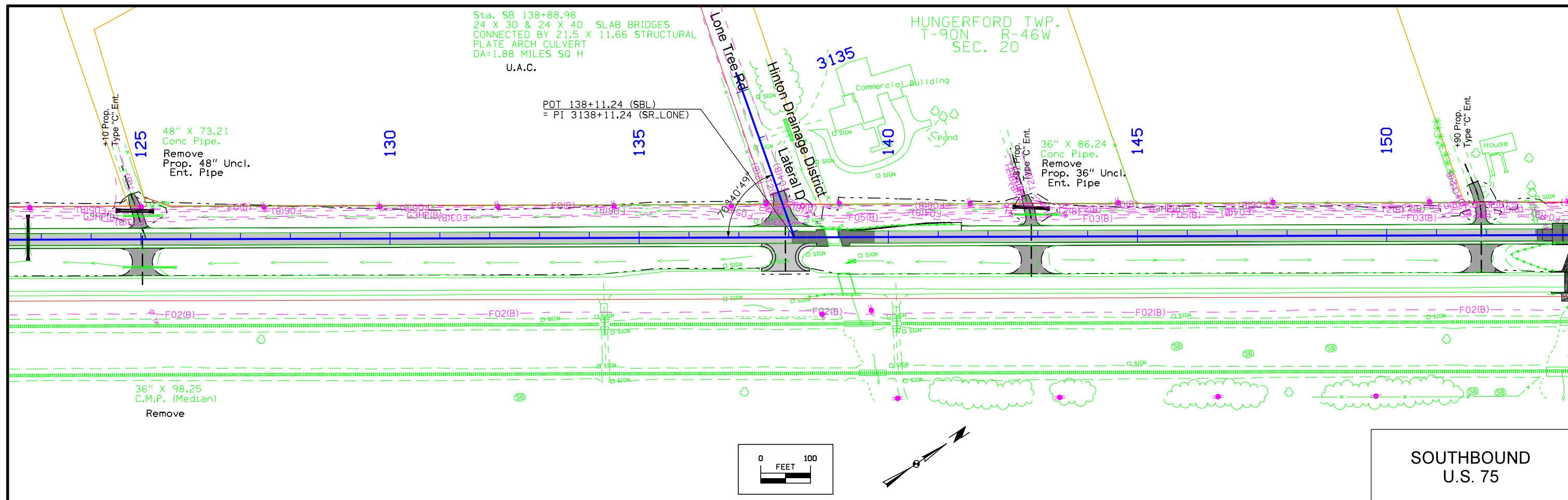


**SOUTHBOUND  
 U.S. 75**

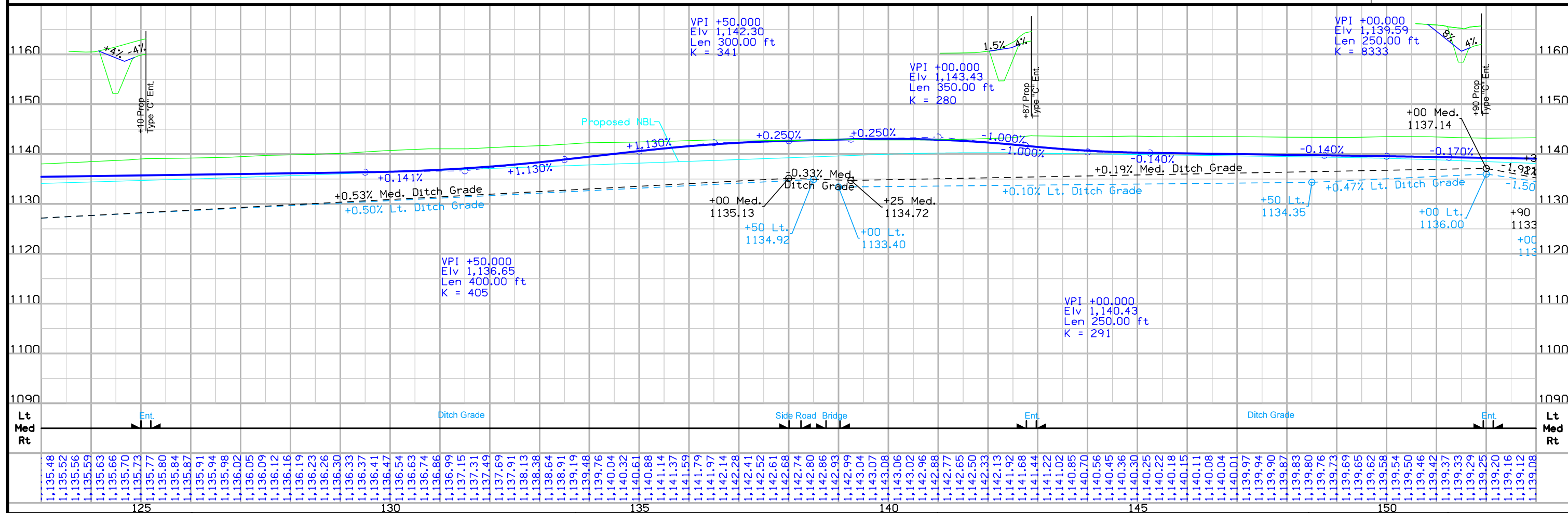


Sta. SB 138+88.98  
 24 X 30 & 24 X 40 SLAB BRIDGES  
 CONNECTED BY 21.5 X 11.66 STRUCTURAL  
 PLATE ARCH CULVERT  
 DA=1.88 MILES SQ H  
 U.A.C.

HUNGERFORD TWP.  
 T-90N R-46W  
 SEC. 20

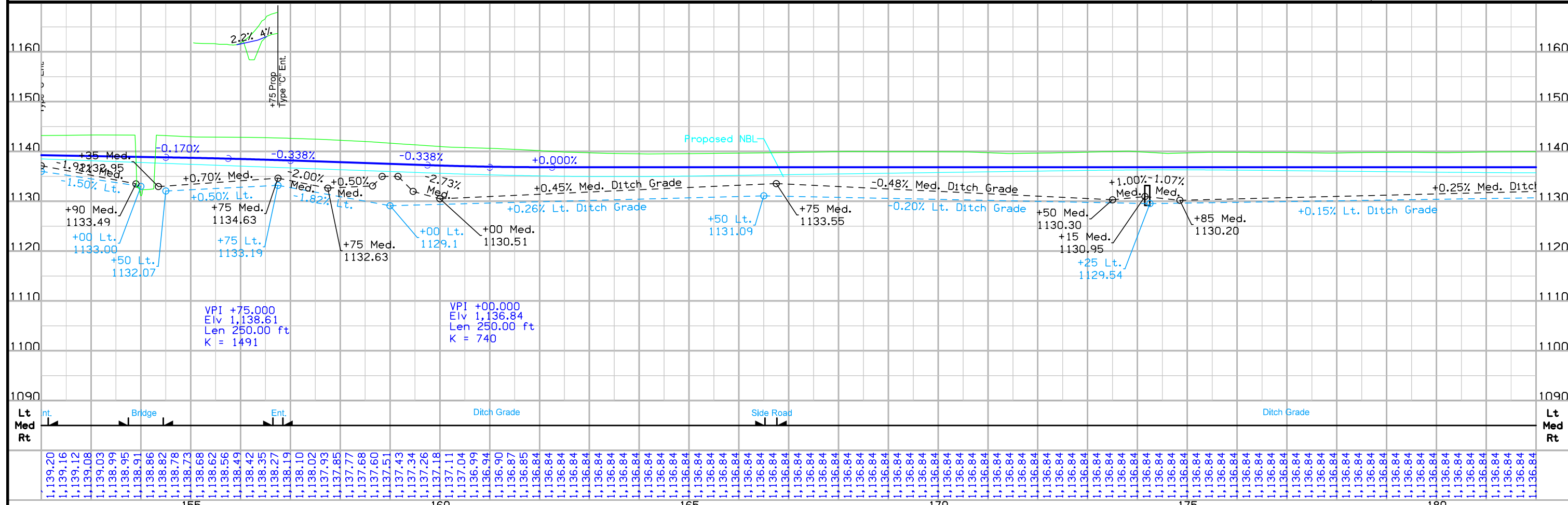
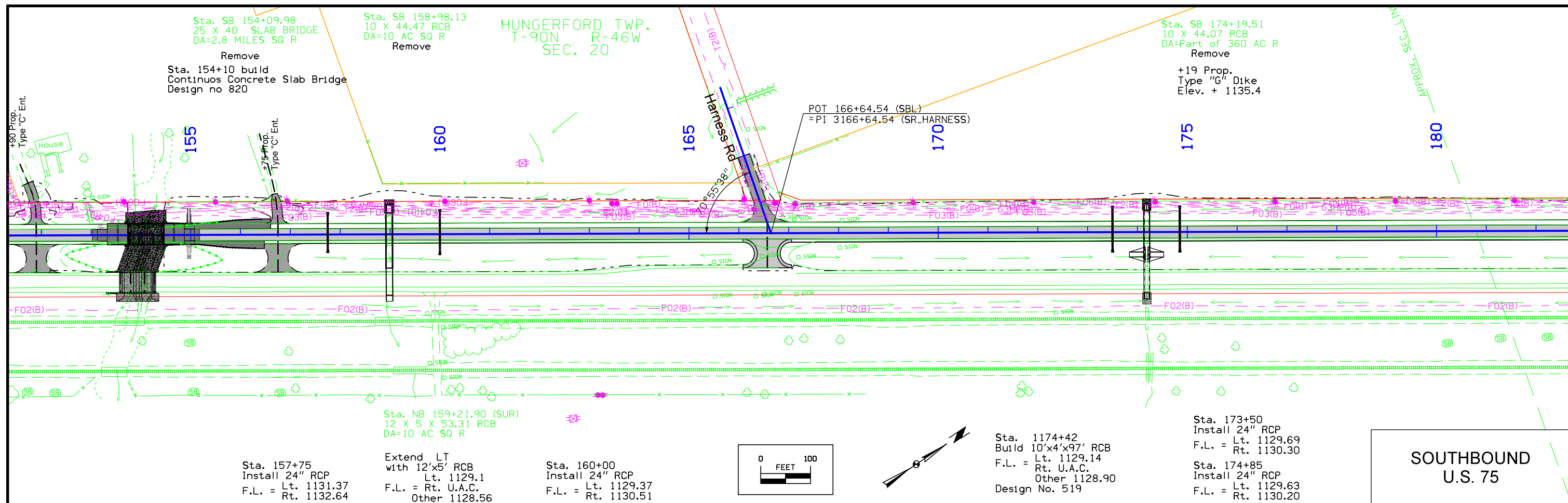


**SOUTHBOUND  
 U.S. 75**



FILE NO.	ENGLISH	DESIGN TEAM	PLYMOUTH COUNTY	PROJECT NUMBER	SHEET NUMBER
		<b>Holst \ Strum \ Janus</b>		<b>NHSX-075-2(96)--3H-75</b>	<b>D.6</b>





FILE NO.	ENGLISH	DESIGN TEAM	<b>Holst \ Strum \ Janus</b>	PLYMOUTH COUNTY	PROJECT NUMBER	<b>NHSX-075-2(96)--3H-75</b>	SHEET NUMBER	<b>D.7</b>
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Sta. SB 188+19.01  
24 X 30 SLAB BRIDGE  
DA=377 AC H R  
Remove  
Sta. 188+20.00  
Build Twin 12'x4'x96 RCB  
F.L. = Lt. 1127.7  
Rt. U.A.C.  
Other 1127.47  
Design No. 1120

Sta. 189+00  
Install 24" RCP  
F.L. = Lt. 1127.89  
Rt. 1130.00

36" X 51.00  
Conc Pipe.  
Remove  
Prop. Twin 48" Uncl.  
Ent. Pipe

HUNGERFORD TWP.  
T-90N R-46W  
SEC. 17

Curve Data  
Δ = 8° 26' 14.41" (LT)  
T = 832.51  
L = 1,662.01  
R = 11,286.28  
e = 30.66  
m = NC

+53 Prop.  
Type "M" Dike  
Elev. + 1132.0

185

+20 Prop.  
Type "G" Dike  
Elev. + 1134.0

Hinton Drainage District  
Lateral C

190

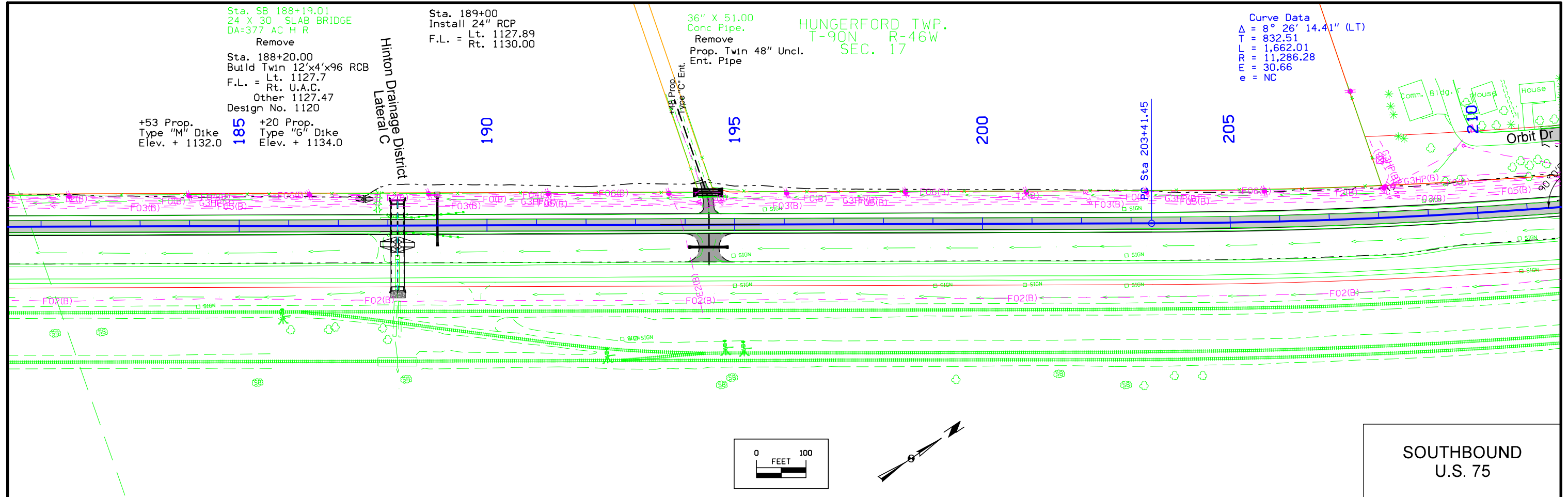
195

200

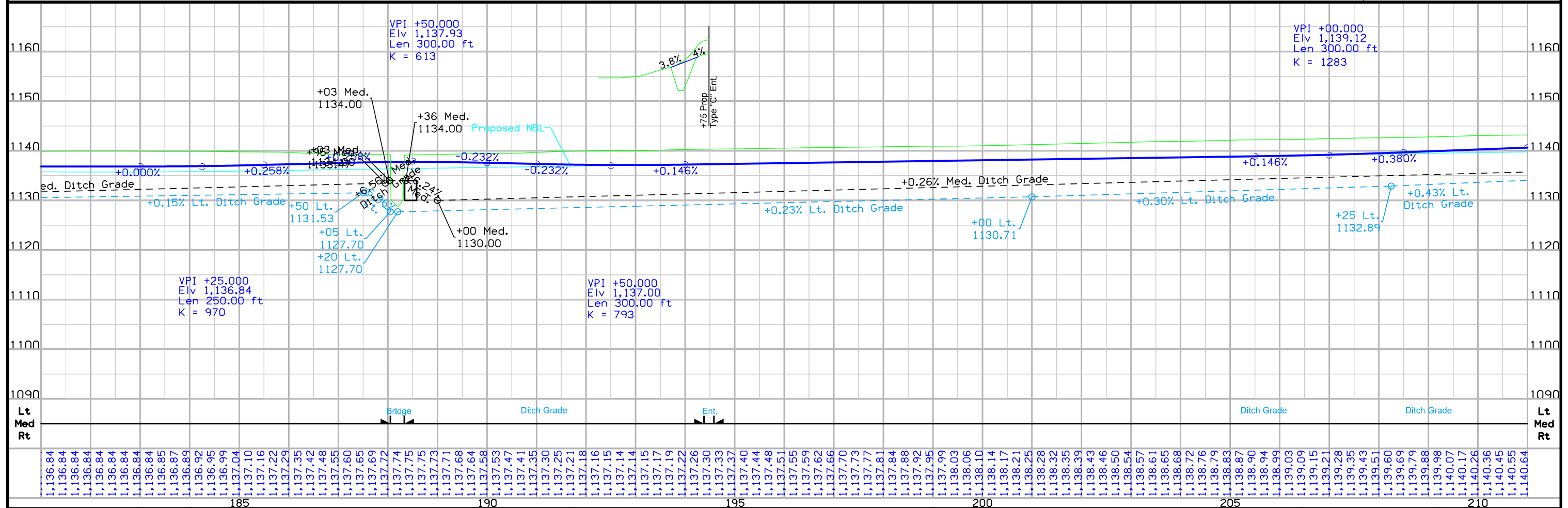
205

210

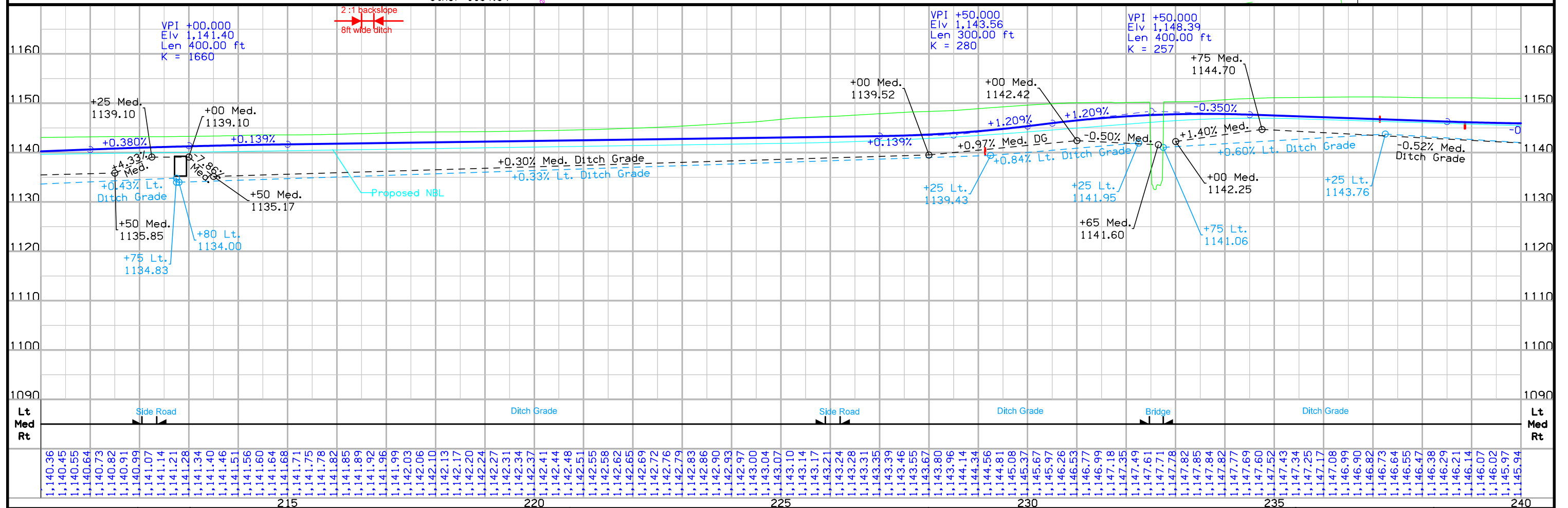
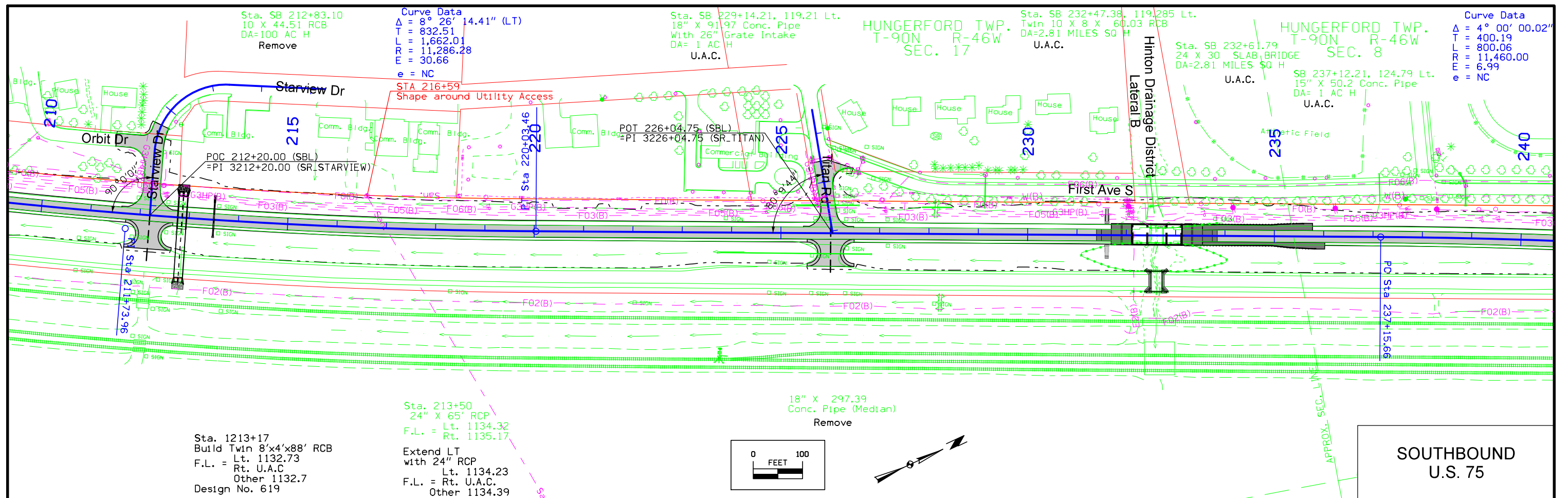
Orbit Dr



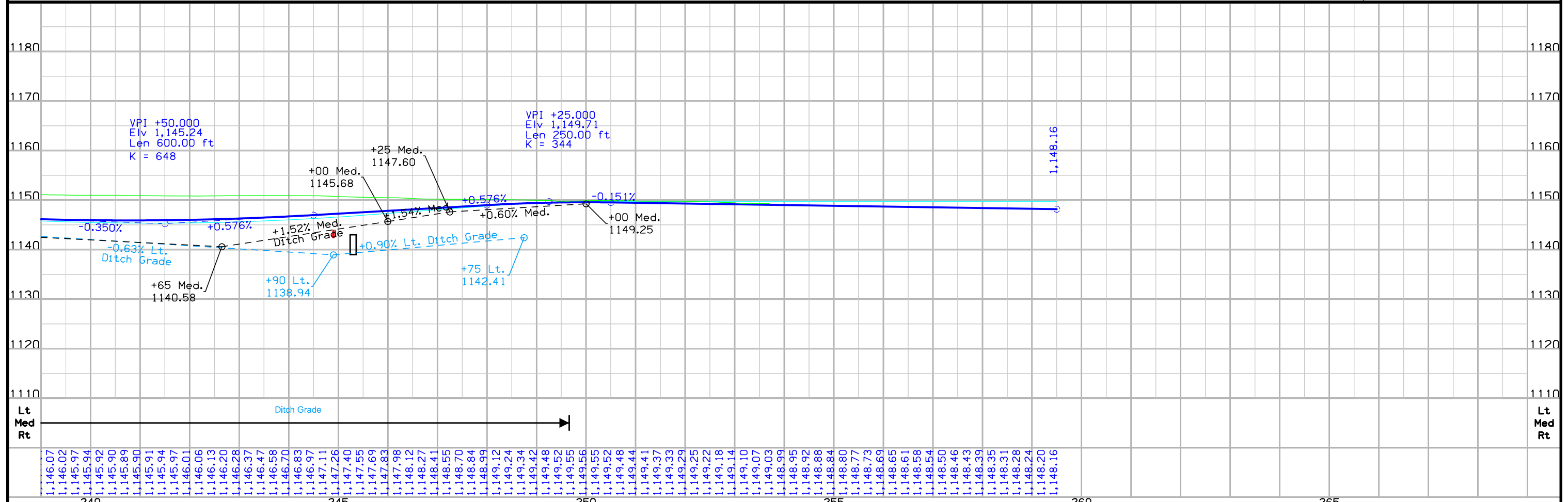
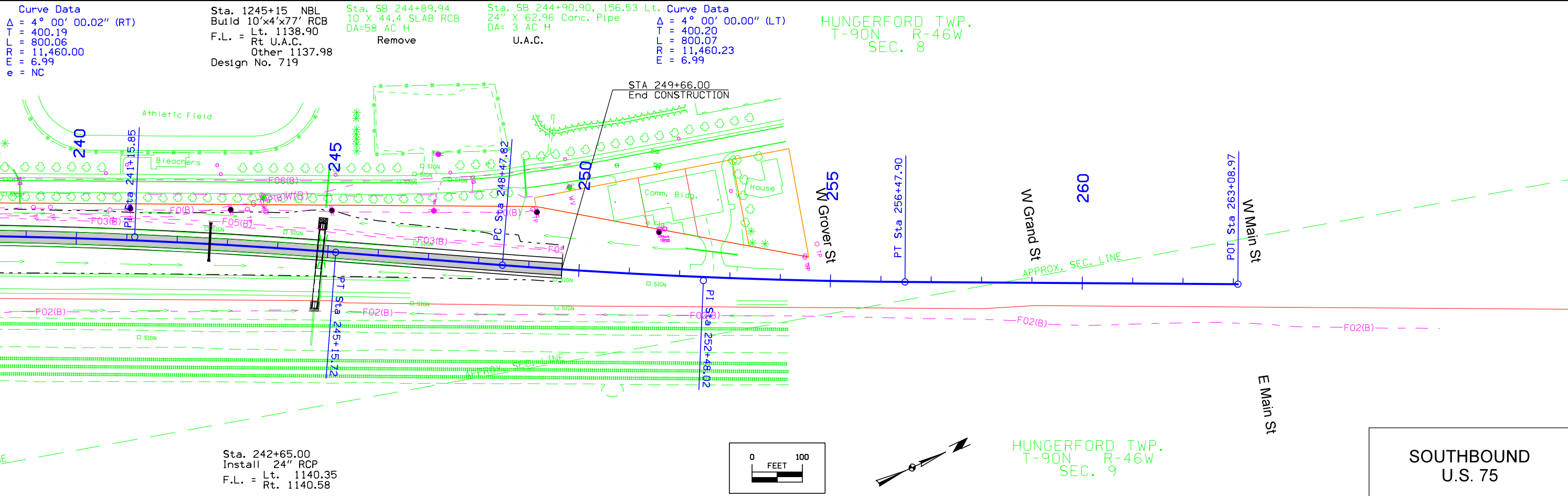
SOUTHBOUND  
U.S. 75



FILE NO.	ENGLISH	DESIGN TEAM	Holst \ Strum \ Janus	PLYMOUTH COUNTY	PROJECT NUMBER	NHSX-075-2(96)--3H-75	SHEET NUMBER	D.8
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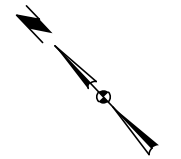


FILE NO.	ENGLISH	DESIGN TEAM	Holst \ Strum \ Janus	PLYMOUTH COUNTY	PROJECT NUMBER	NHSX-075-2(96)--3H-75	SHEET NUMBER	D.9
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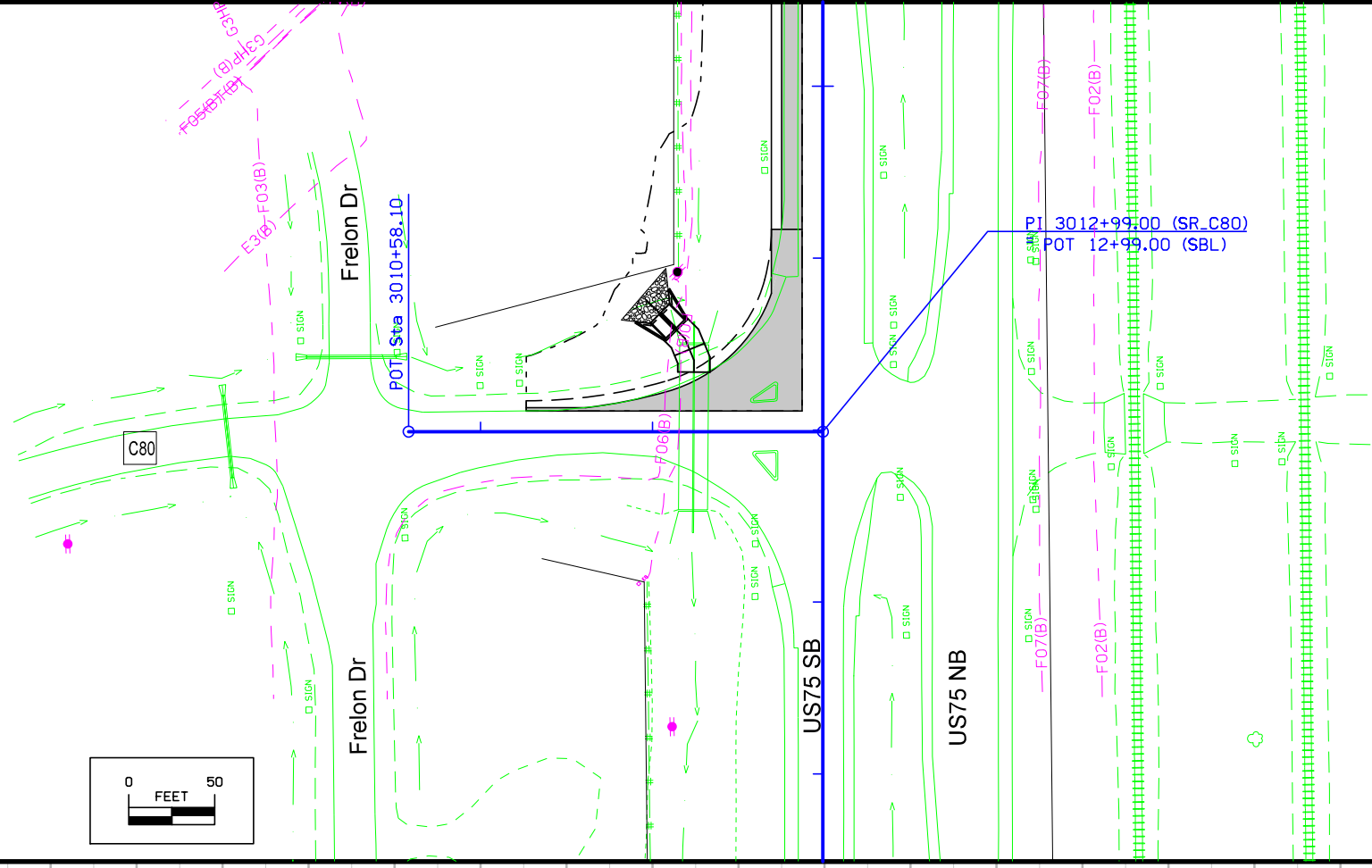
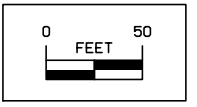


FILE NO.	ENGLISH	DESIGN TEAM	Holst \ Strum \ Janus	PLYMOUTH COUNTY	PROJECT NUMBER	NHSX-075-2(96)--3H-75	SHEET NUMBER	D.10
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HUNGERFORD TWP.  
T-90N R-46W  
SEC. 31



Sta. STA 3012+23.80  
(SB 13+49.56, 74.81 Lt.)  
Twin 8' X 6' X 95.94 RCB  
Remove 16' LT  
Extend LT  
with 8' x 6' Twin RCB  
Lt. 1114.05  
F.L. = Rt. U.A.C.  
Other 1114.05

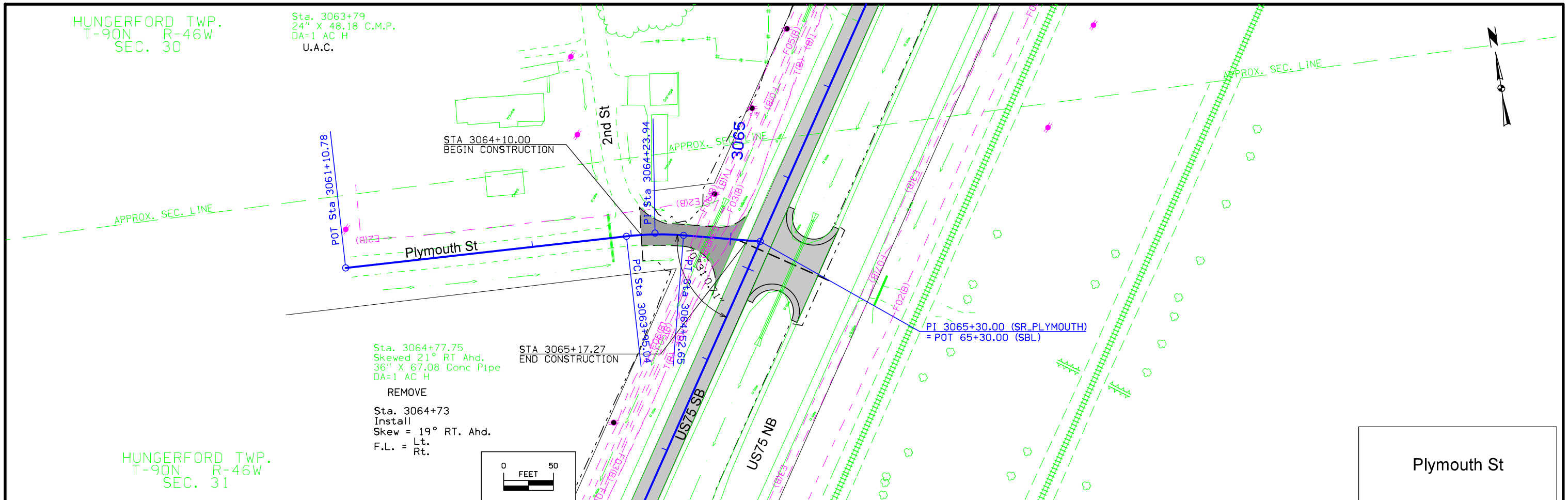


C-80



HUNGERFORD TWP.  
T-90N R-46W  
SEC. 30

Sta. 3063+79  
24" X 48.18 C.M.P.  
DA=1 AC H  
U.A.C.



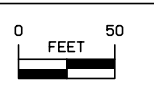
Sta. 3064+77.75  
Skewed 21° RT Ahd.  
36" X 67.08 Conc Pipe  
DA=1 AC H

REMOVE  
Sta. 3064+73  
Install  
Skew = 19° RT. Ahd.  
F.L. = Lt.  
Rt.

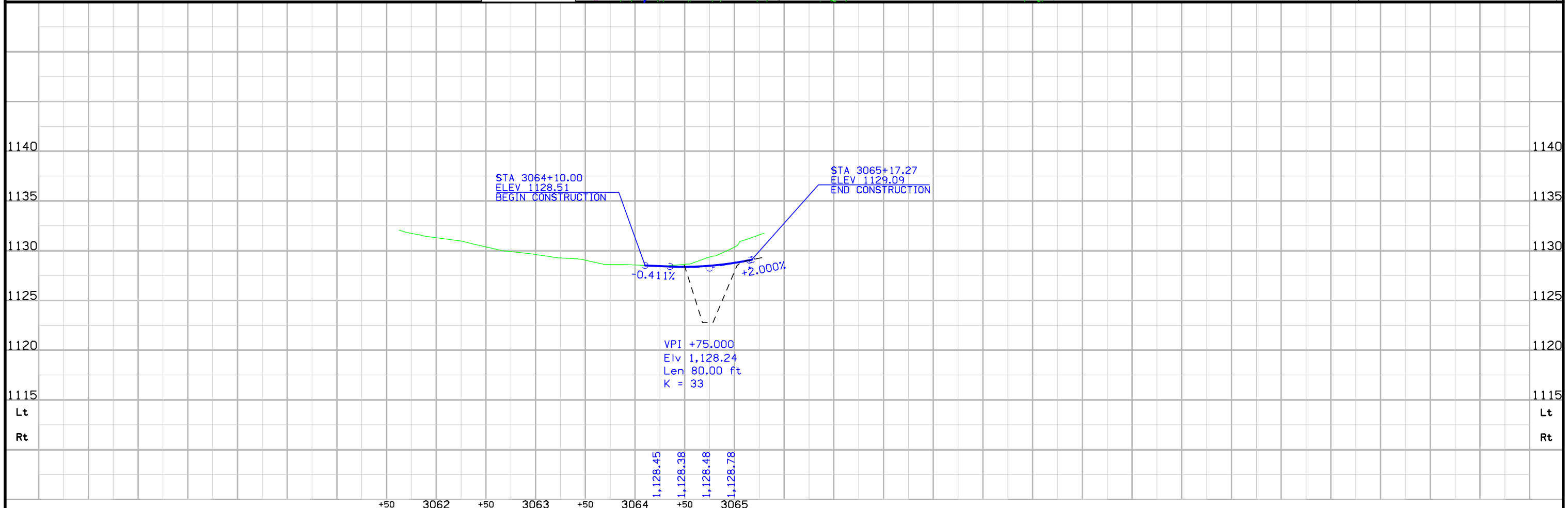
STA 3065+17.27  
END CONSTRUCTION

PI 3065+30.00 (SR\_PLYMOUTH)  
= POT 65+30.00 (SBL)

HUNGERFORD TWP.  
T-90N R-46W  
SEC. 31



Plymouth St

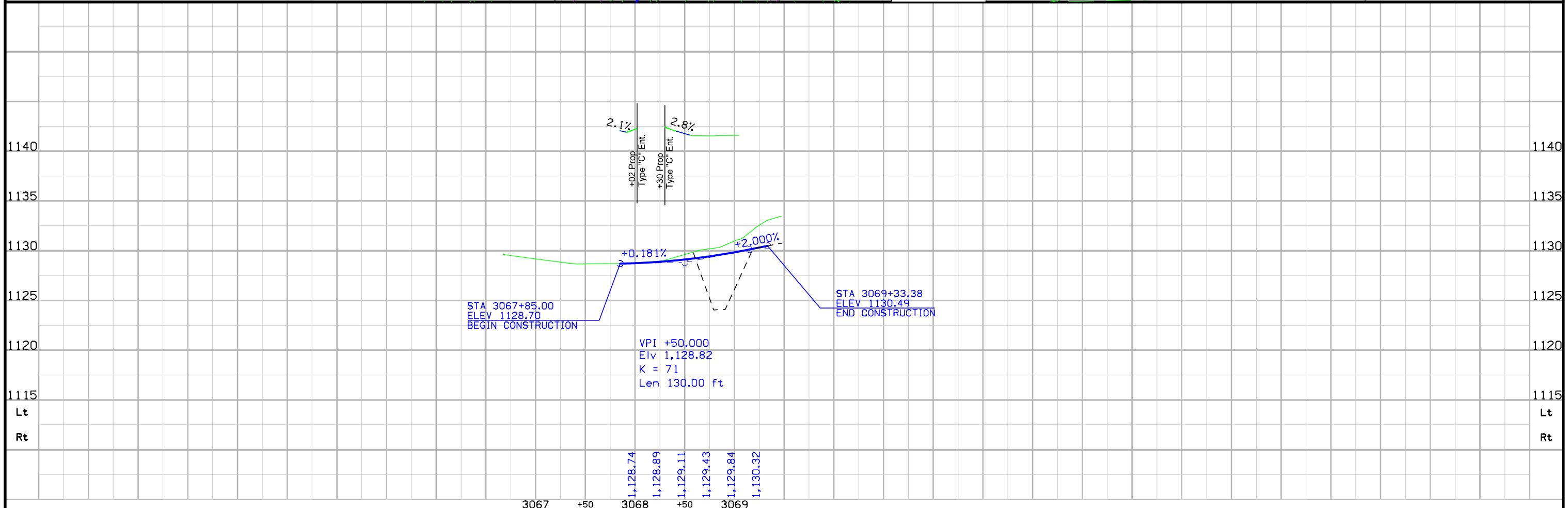
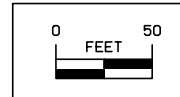
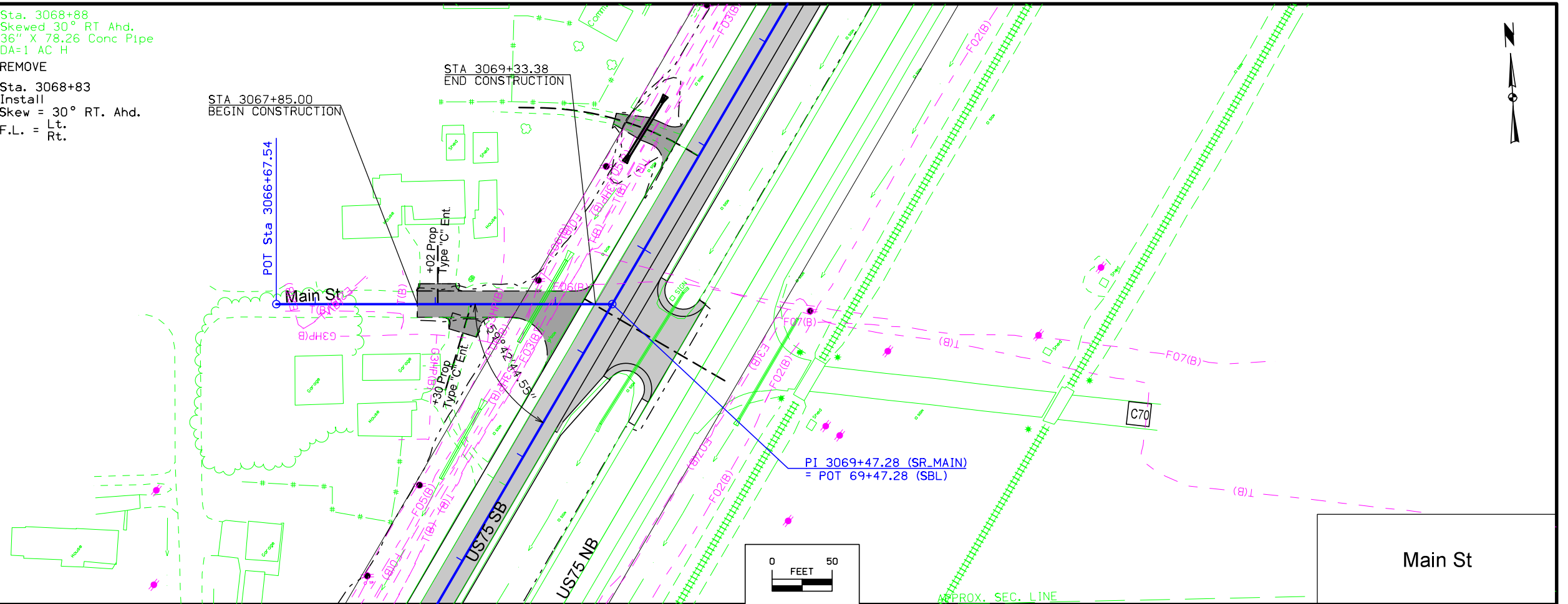


VPI +75.000  
Eiv 1,128.24  
Len 80.00 ft  
K = 33

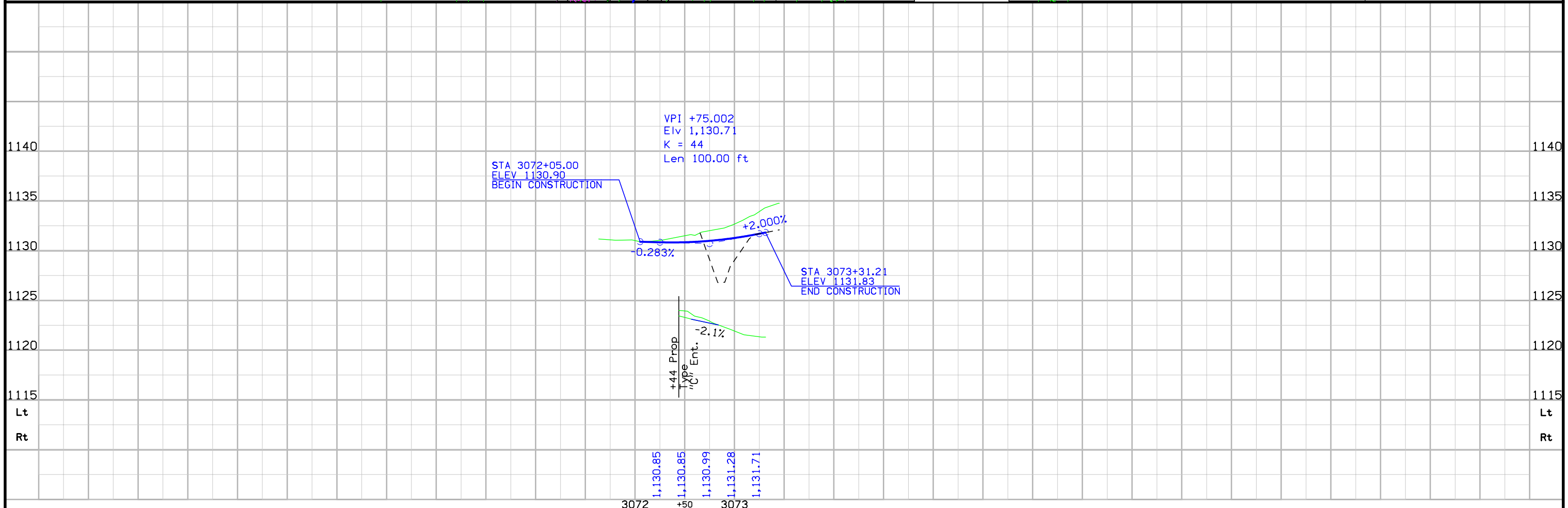
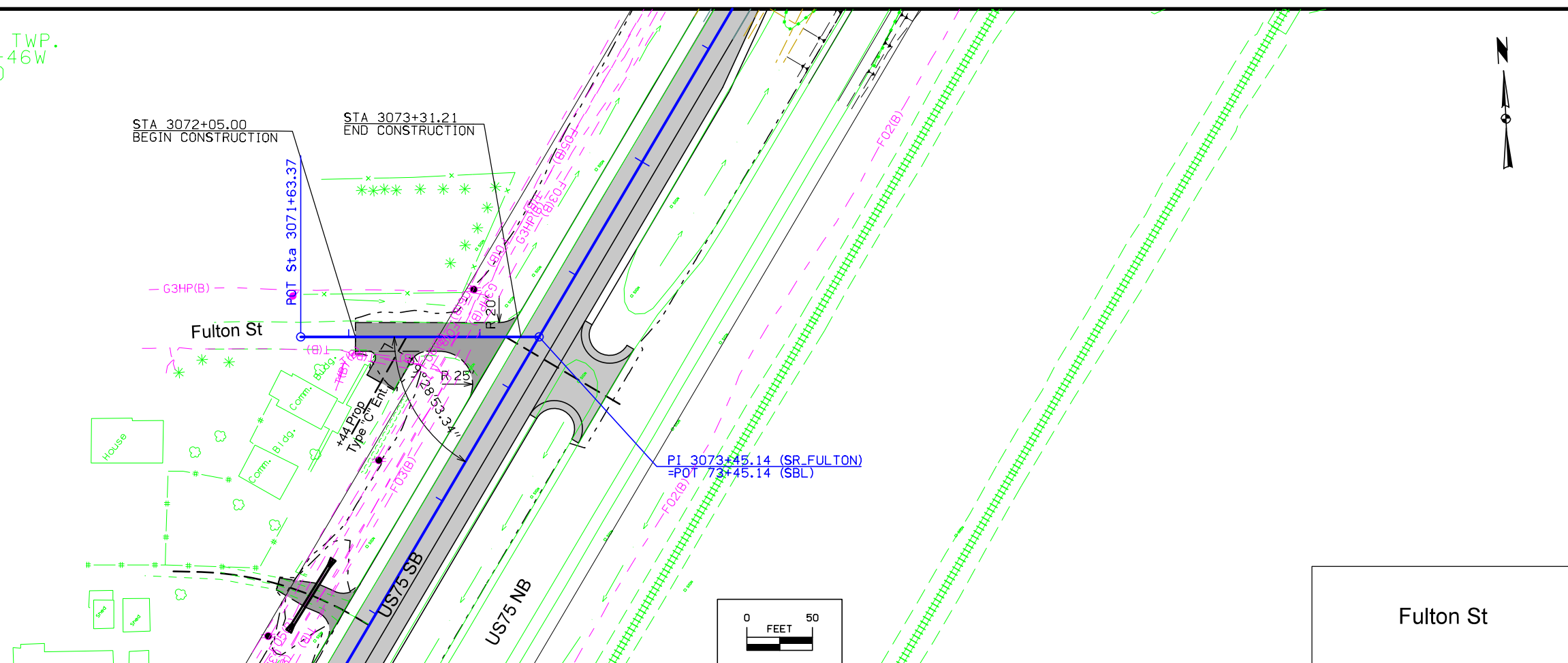
1,128.45  
1,128.38  
1,128.48  
1,128.78

HUNGERFORD TWP.  
T-90N R-46W  
SEC. 30

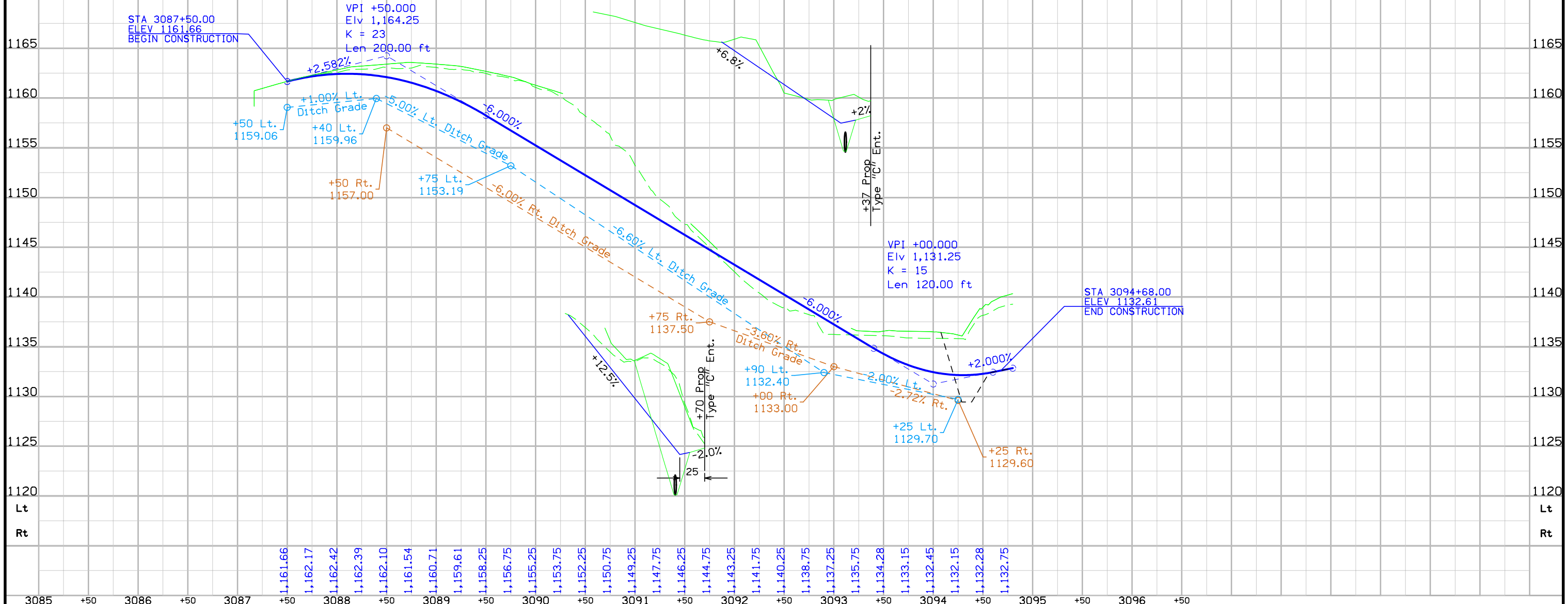
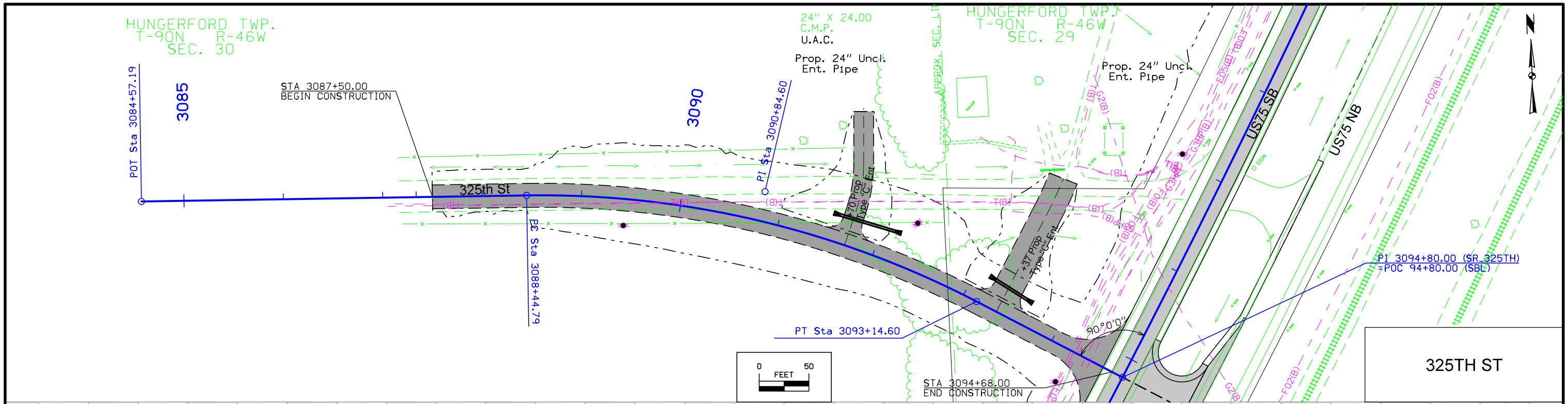
Sta. 3068+88  
Skewed 30° RT Ahd.  
36" X 78.26 Conc Pipe  
DA=1 AC H  
REMOVE  
Sta. 3068+83  
Install  
Skew = 30° RT. Ahd.  
F.L. = Lt.  
Rt.



HUNGERFORD TWP.  
T-90N R-46W  
SEC. 30







FILE NO.	ENGLISH	DESIGN TEAM	PLYMOUTH COUNTY	PROJECT NUMBER	SHEET NUMBER
		Holst \ Strum \ Janus		NHSX-075-2(96)--3H-75	E.5

HUNGERFORD TWP.  
T-90N R-46W  
SEC. 20

96" X 45.2  
STEEL PIPE



POT Sta 3134+61.24

3135

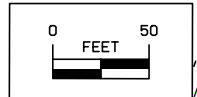
Hinton Drainage District  
Lateral D

Lone Tree Rd

STA 3137+15.00  
BEGIN CONSTRUCTION

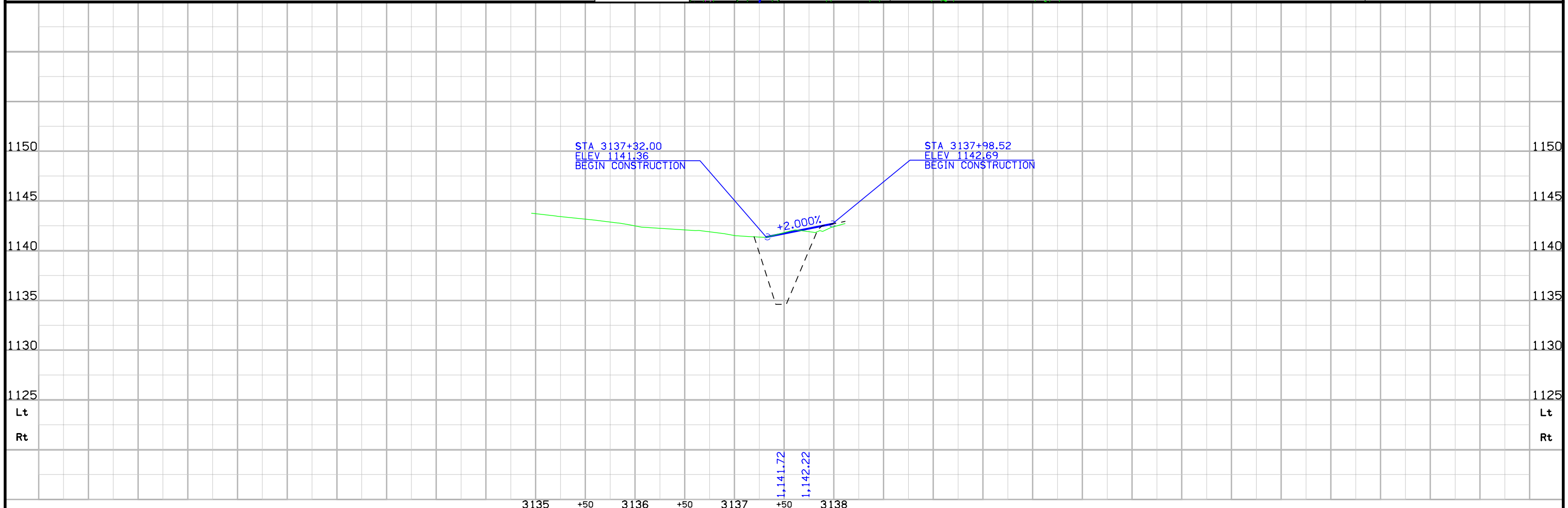
STA 3137+98.52  
BEGIN CONSTRUCTION

PT 3138+11.24 (SR LONE)  
POT 138+11.24 (SBL)

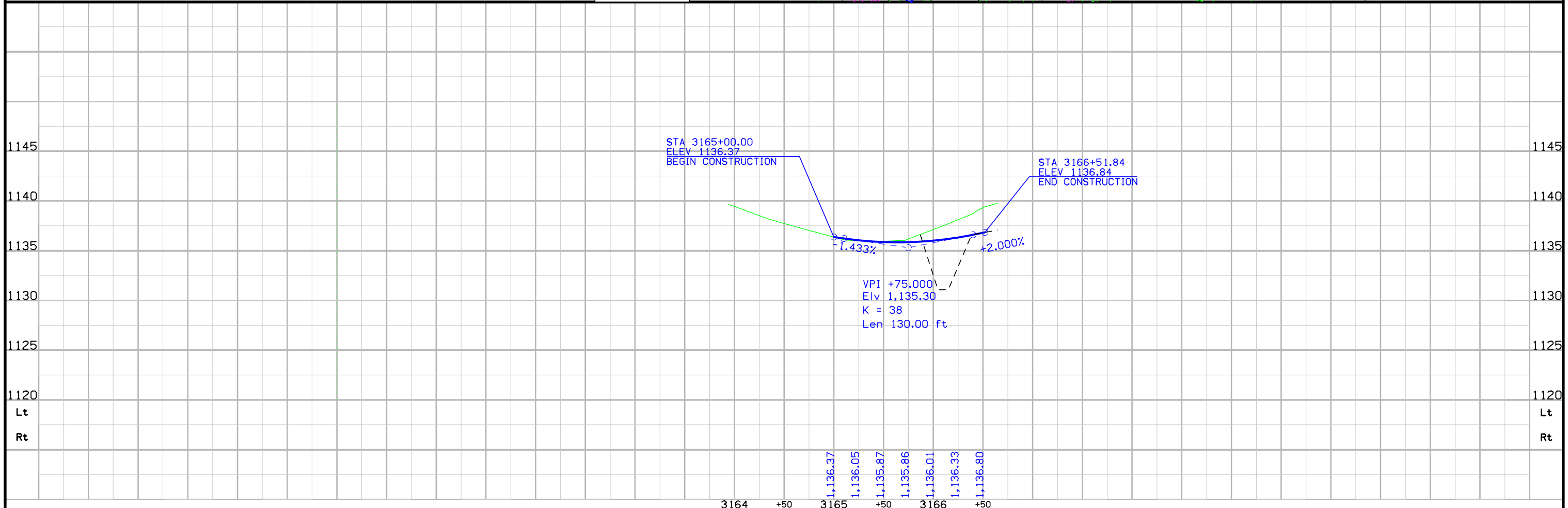
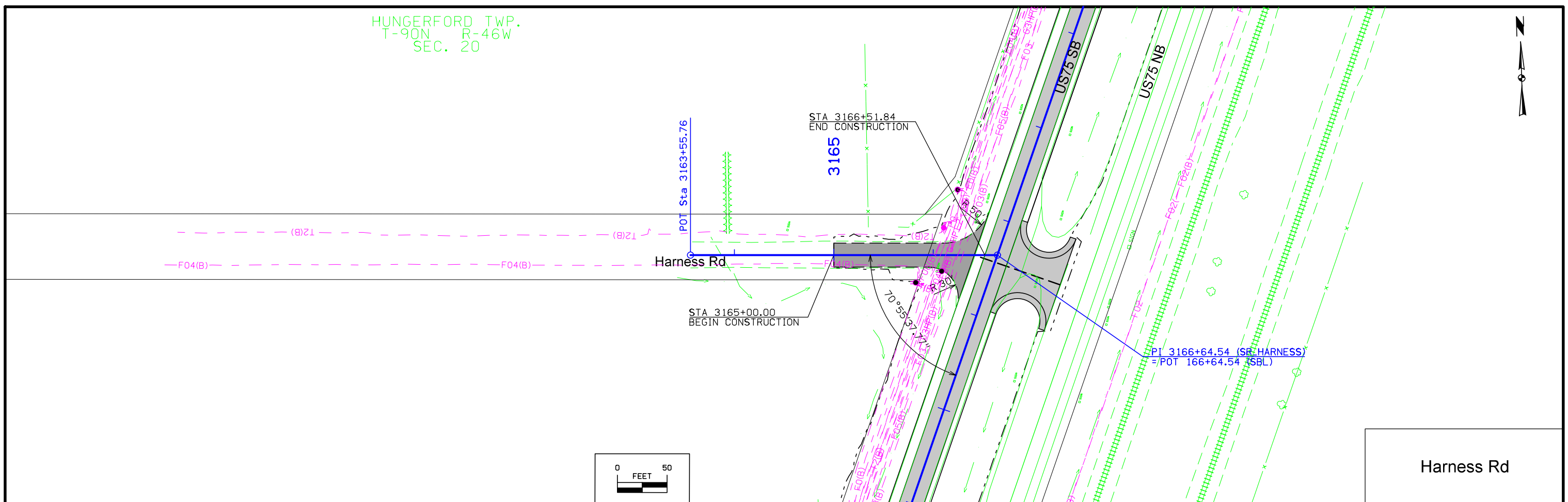


US75 SB  
US75 NB

LONE TREE RD

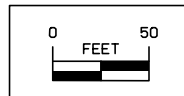
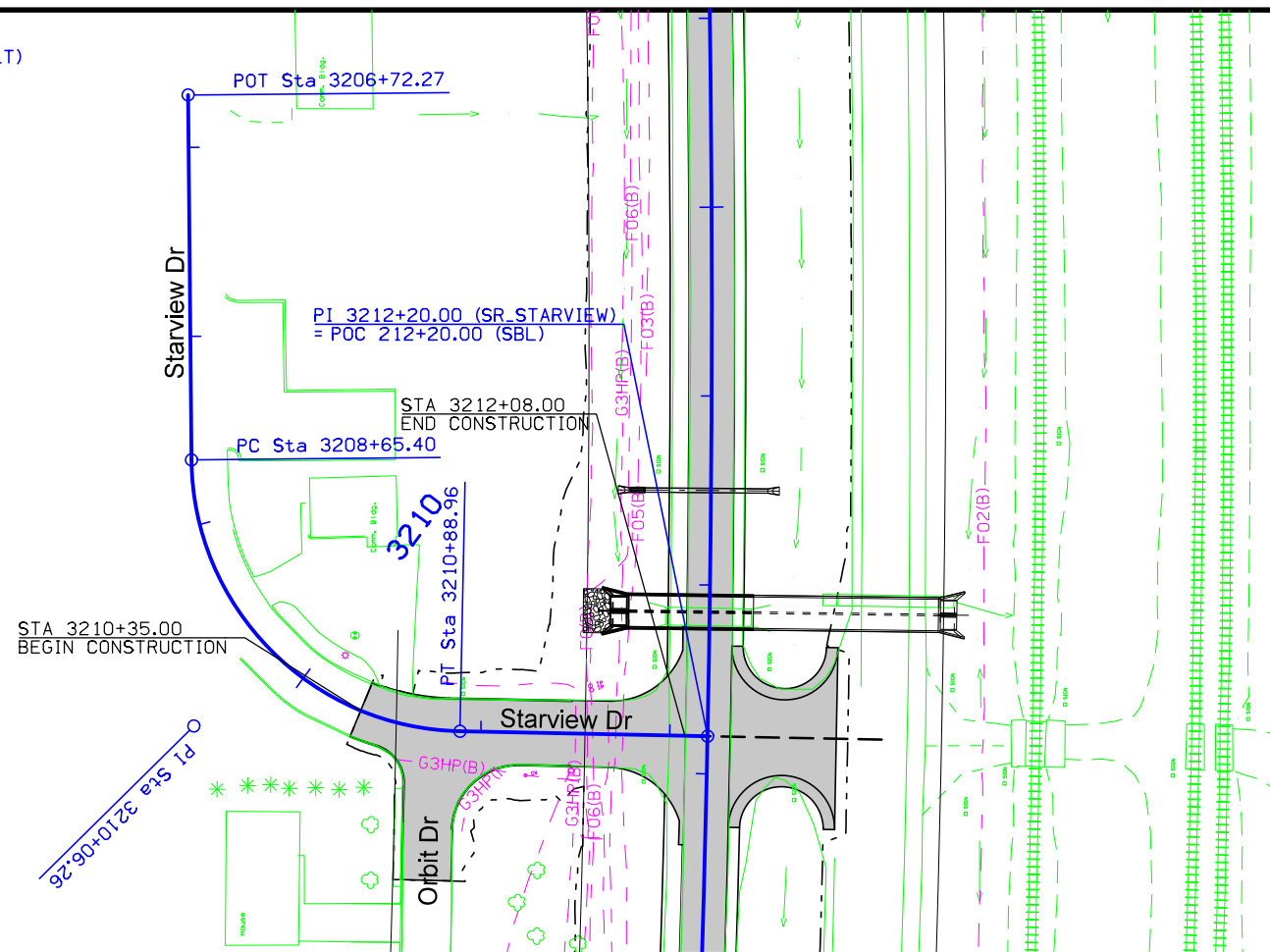


HUNGERFORD TWP.  
T-90N R-46W  
SEC. 20

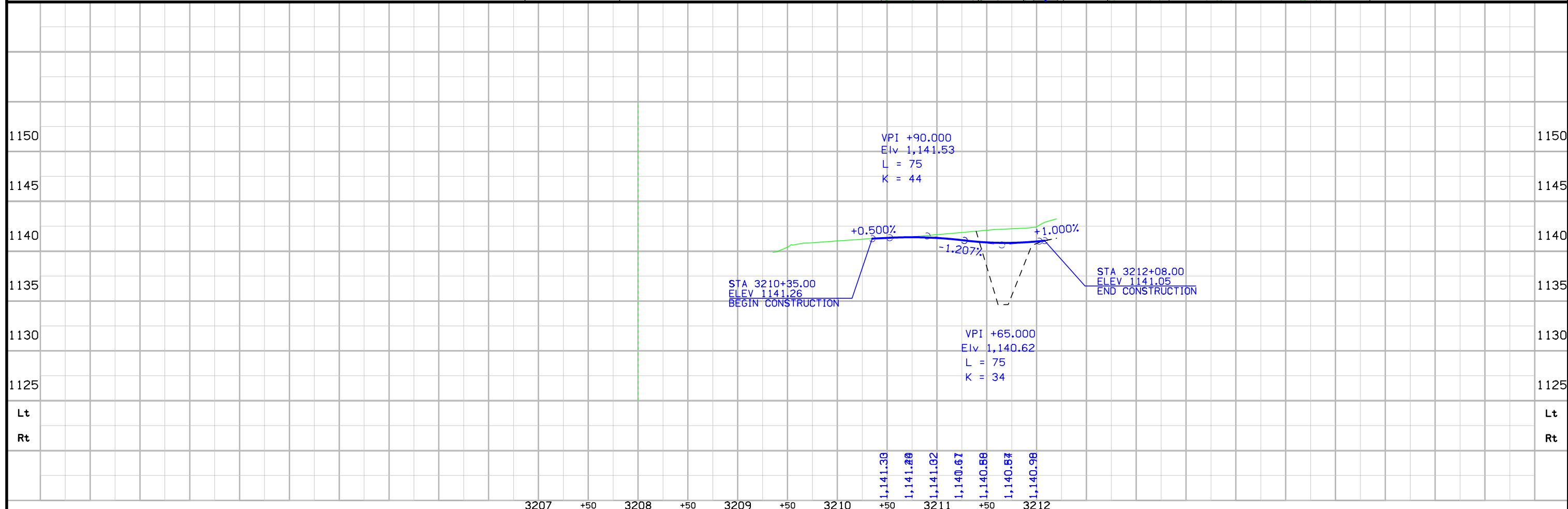


HUNGERFORD TWP.  
T-90N R-46W  
SEC. 17

Curve Data  
 $\Delta = 88^\circ 20' 19.52''$  (LT)  
 T = 140.86  
 L = 223.56  
 R = 145.00  
 E = 57.15

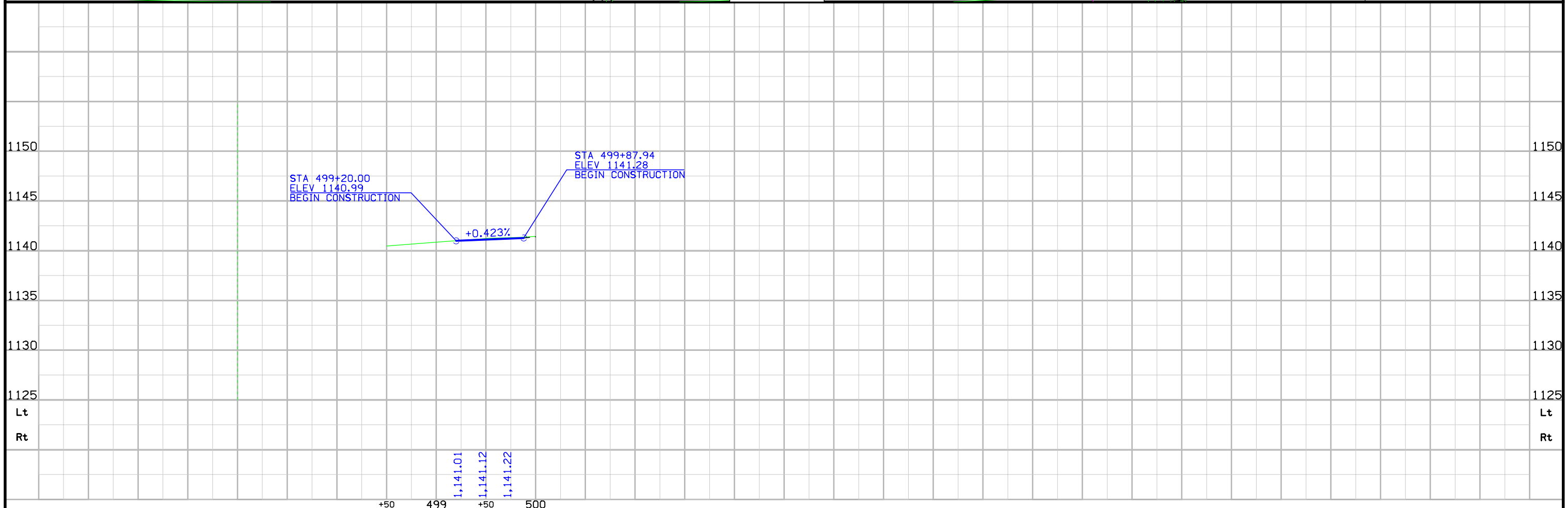
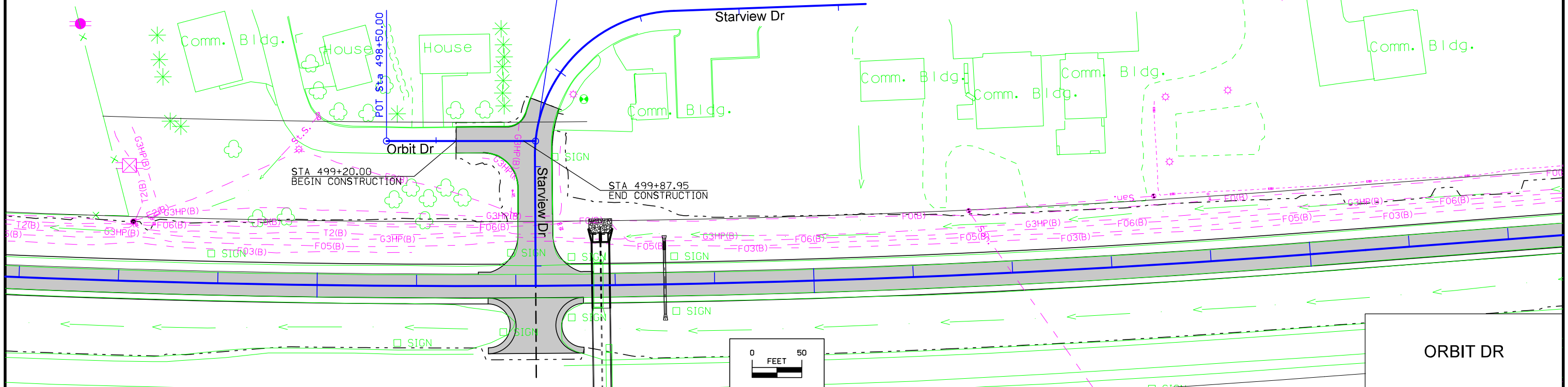


STARVIEW DR



HUNGERFORD TWP.  
T-90N R-46W  
SEC. 17

PI 500+00.00 (SR\_ORBIT)  
= POC 3210+74.23 (SR\_STARVIEW)



HUNGERFORD TWP.  
T-90N R-46W  
SEC. 17

PI 3224+33.91 (SR\_TITAN)  
= POT 600+00.00 (SR\_1STAVE)

STA 3225+92.57  
END CONSTRUCTION

Titan Rd

STA 3224+57.00  
BEGIN CONSTRUCTION

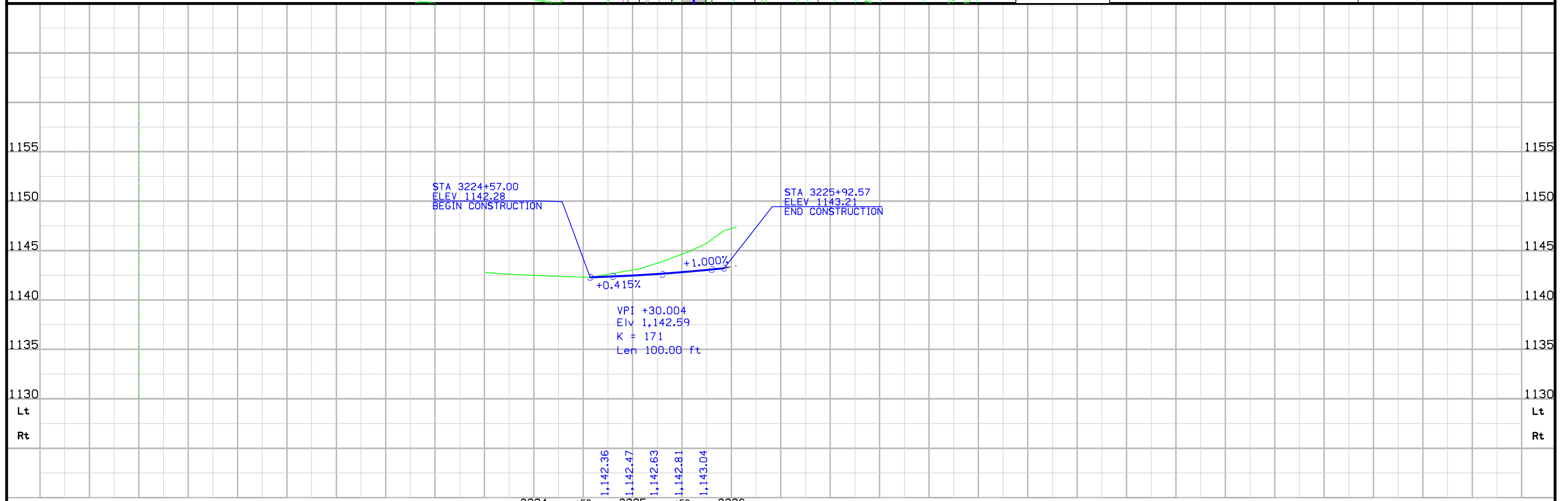
STA 3225+47.59  
18" X 121'  
Conc. Pipe  
REMOVE

Sta. 3225+49  
Install 24" RCP  
Skew = 10° RT. Ahd.  
F.L. = Lt.  
Rt.

PI 3226+04.75 (SR\_TITAN)  
= POT 226+04.75 (SBL)



TITAN RD



## Survey Information

County: Plymouth  
 PIN: 16-75-075-020  
 Project Number: NHSX-075-2(91)--3H-75  
 Location: US 75 North of Sioux City to Grover St  
 In Hinton  
 Type of Work: Preliminary Engineering  
 Project Directory: 7507502016  
 SAP 666.2

### General Information

Measurement units for this survey are US survey feet. This survey is for preliminary engineering of US 75 between Sioux City and Hinton. This project is a combination of field survey and aerial survey. The existing pavement was only field surveyed at the tie in locations at each end of the project. The rest was surveyed using aerial photography.

### Vertical Control

Vertical Control was established on 3 monuments on the project designated as points 13, 67 and 126. These monuments are stable and are expected to hold vertical reasonably well. The vertical datum is NAVD88. Datum was transferred from the Iowa RTN reference station at Le Mars to the projects monuments mentioned above by using concurrent 6 hour static measurements and post processing connecting vectors. Geoid 12 A was used in processing. The Le Mars reference station orthometric height used is 1287.25 US Survey Ft.

This survey observed 4 Plymouth Control Monuments with published NAVD88 heights to validate the reference station height used in computation of project heights. The County control has a stated vertical accuracy of + or  $\frac{1}{2}$  0.10 ft. The survey heights determined at the two county monuments validate the NAVD 88 height used at the Le Mars reference station and the positions of the project control monuments within acceptable tolerance.

County Control mark designated 067 has a published height. Of 1189.29  
 Survey height. = 1189.45.

County Control mark designated 126 has a published height. Of 1218.54  
 Survey height. = 1218.55

County Control mark designated 057 has a published height. Of 1199.53  
 Survey height. = 1199.612

County Control mark designated 125 has a published height. Of 1326.99  
 Survey height. = 1327.167

### Horizontal Control

Horizontal Control was established on 3 monuments on the project designated as points 13, 67 and 126. These monuments are stable and are expected to hold vertical reasonably well. The horizontal datum is NAD83(2011) (EPOCH 2010.00). Datum was transferred from the Iowa RTN reference station at LeMars to the projects monuments mentioned above by using concurrent 6 hour static measurements and post processing connecting vectors. Iowa Regional Coordinate System Zone 4 is used. The Zone 4 coordinates used at the Le Mars reference station are:  
 N= 8699224.17, E= 14146951.66. See [http://www.iowadot.gov/rtn/pdfs/IaRCS/IaRCS\\_04\\_SiouxCity\\_IowaFalls.pdf](http://www.iowadot.gov/rtn/pdfs/IaRCS/IaRCS_04_SiouxCity_IowaFalls.pdf) for information regarding Iowa Regional Coordinate System Zone 4.

Additional static observations of county monuments and feno monuments extended control south from Merrill to Sioux City.

### Survey Alignment Information

The horizontal alignment for this survey is prepared by the District 3 ROW office.

## PROJECT CONTROL (BENCHMARKS)

Point	North	East	Elevation	Station	Offset	Feature	Description
1	8612460.571	14094986.545	1118.369	Off Chain	Off Chain	CP	Iowa DOT Feno Monument 6" deep- in east right of way US 75 between highway and train tracks-40 feet east of mile marker 100
2	8619288.380	14101557.731	1126.053	74+07.52	747.945	CP	Iowa DOT Feno Monument 6" deep- in northwest berm of Floyd River Bridge 900 feet east of US 75 along Plymouth County road C 70 -100 feet north of northwest corner of bridge
3	8625665.153	14103836.665	1137.387	140+66.67	44.408	CP	Iowa DOT Feno Monument 6" deep- located 18 feet southeast of station post 140- 45 feet east of centerline northbound US 75- 20 feet north of north edge of field entrance to east.
125	8637478.516	14104870.080	1327.167	258+94.32	-2527.599	CP	

ALIGNMENT COORDINATES

101-16  
10-20-09

Table with columns: Name, Location, Point on Tangent (Station, Y (Northing), X (Easting)), Begin Spiral (Station, Y (Northing), X (Easting)), Begin Curve (Station, Y (Northing), X (Easting)), Simple Curve PI or Master PI of SCS (Station, Y (Northing), X (Easting)), End Curve (Station, Y (Northing), X (Easting)), End Spiral (Station, Y (Northing), X (Easting)).

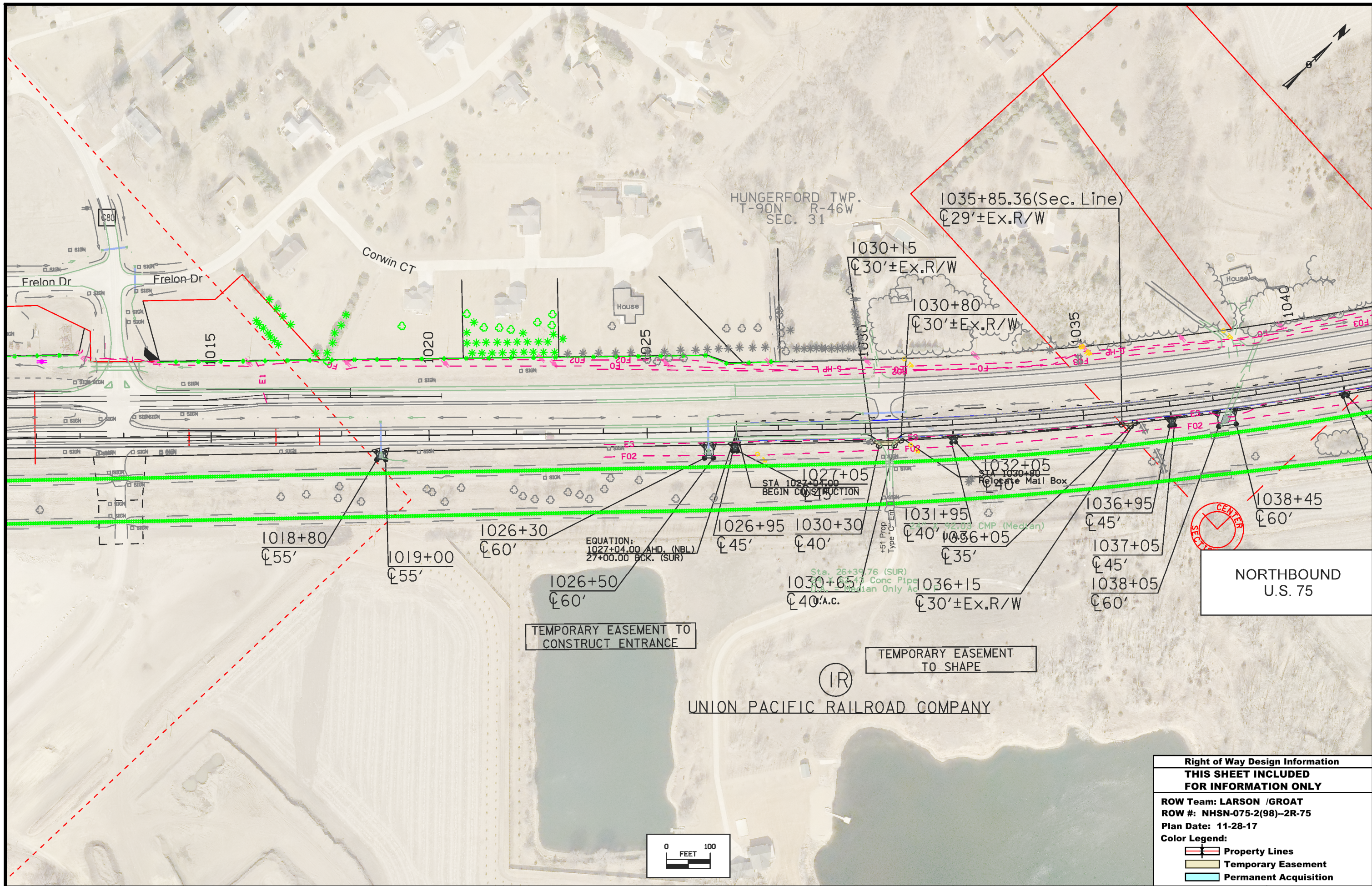


**SPIRAL OR CIRCULAR CURVE DATA**

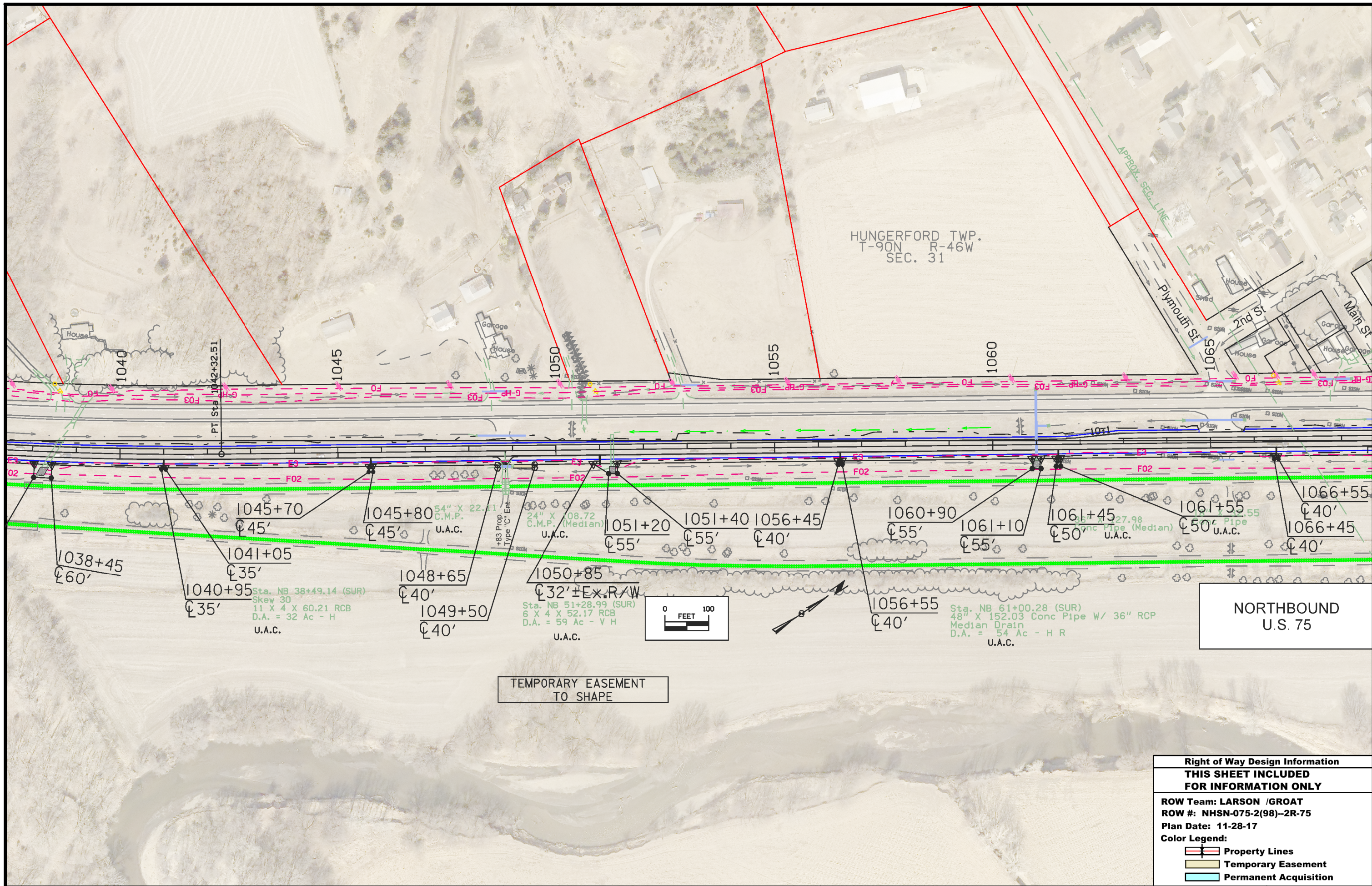
**101-17  
04-19-11**

Name	Location	△ <sub>sCs</sub>	Horizontal Alignment Data													Remarks	
			Spiral Data							Curve Data							
			θs	Ls	Ts	Es	Xc	Yc	L.T.	S.T.	Δ <sub>c</sub>	T	L	R	E		
24+21.73	NBL												0° 36' 11.50" LT	99.99'	199.98'	18,995.86'	0.26'
35+71.5	NBL												9° 33' 23.27" LT	653.09'	1,303.15'	7,813.04'	27.25'
99+21.72	NBL												12° 30' 03.40" LT	1,158.52'	2,307.84'	10,577.57'	63.25'
211+80.24	NBL												8° 25' 51.84" LT	831.87'	1,660.74'	11,286.01'	30.62'
239+90.07	NBL												1° 00' 41.25" RT	529.61'	1,059.20'	60,000.00'	2.34'
250+84	NBL												1° 00' 41.49" LT	150.07'	300.13'	17,000.00'	0.66'
SBL23+79.78	SBL												1° 05' 27.19" LT	175.00'	349.99'	18,382.25'	0.83'
SBL35+63.9	SBL												9° 33' 23.27" LT	645.48'	1,287.97'	7,722.00'	26.93'
SBL99+22.6	SBL												12° 30' 24.42" LT	1,236.42'	2,463.01'	11,283.50'	67.54'
SBL212+03.4	SBL												8° 26' 14.41" LT	832.51'	1,662.01'	11,286.28'	30.66'
SBL241+56	SBL												4° 00' 00.02" RT	400.19'	800.06'	11,460.00'	6.99'
SBL252+85.7	SBL												4° 00' 00.00" LT	400.20'	800.07'	11,460.23'	6.99'
31000	Plymouth St												11° 00' 10.68" RT	28.89'	57.61'	300.00'	1.39'
33014	325th St												28° 20' 06.99" RT	239.82'	469.82'	950.00'	29.80'
36000	Starview Rd												88° 20' 19.52" LT	140.86'	223.56'	145.00'	57.15'

NO ACCESS RIGHTS ARE TO BE ACQUIRED ON THIS PROJECT.



<b>Right of Way Design Information</b>	
<b>THIS SHEET INCLUDED FOR INFORMATION ONLY</b>	
ROW Team: LARSON /GROAT	
ROW #: NHSN-075-2(98)-2R-75	
Plan Date: 11-28-17	
Color Legend:	
	Property Lines
	Temporary Easement
	Permanent Acquisition

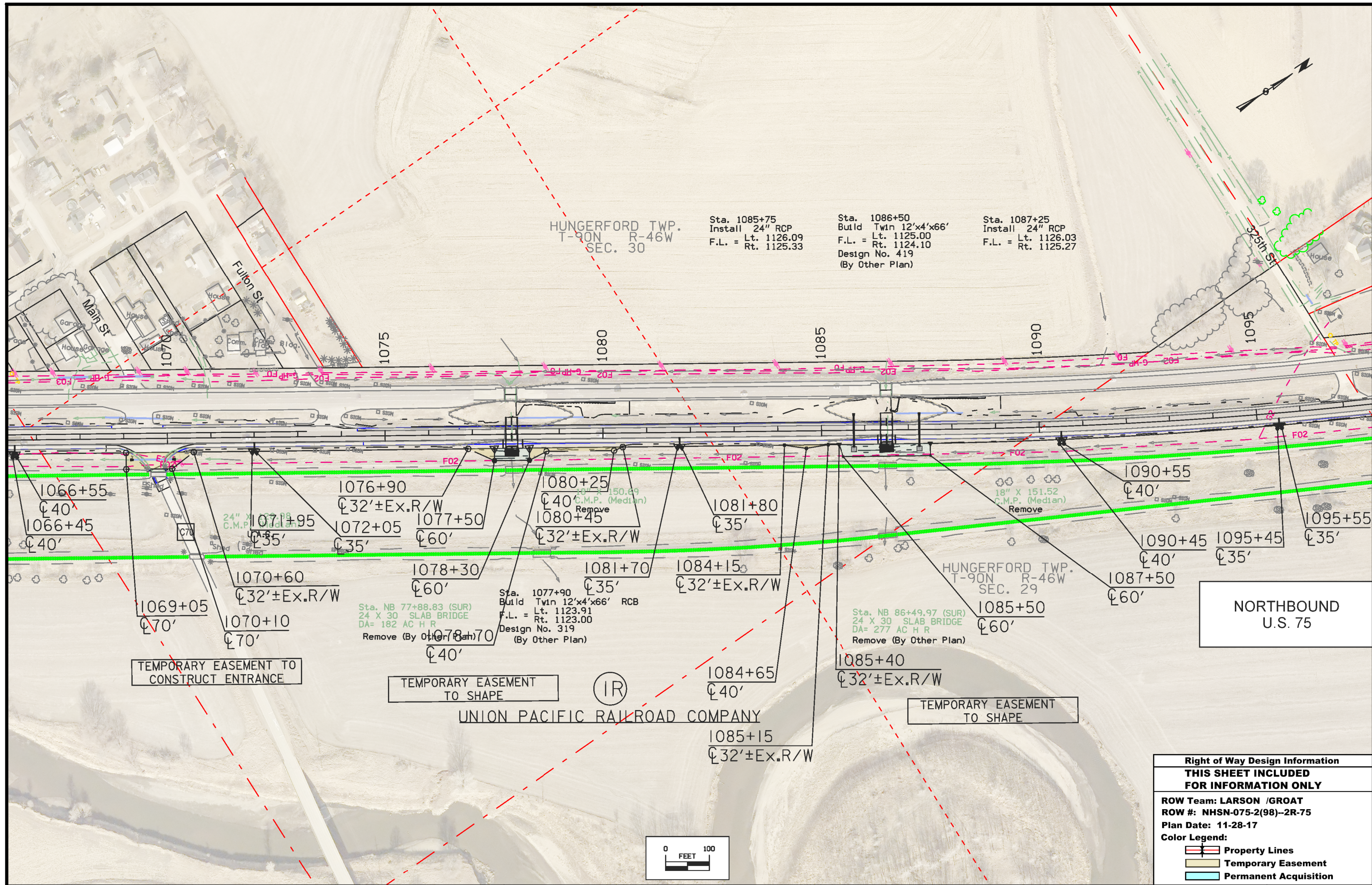


HUNGERFORD TWP.  
T-90N R-46W  
SEC. 31

**NORTHBOUND  
U.S. 75**

**TEMPORARY EASEMENT  
TO SHAPE**

<b>Right of Way Design Information</b>	
<b>THIS SHEET INCLUDED FOR INFORMATION ONLY</b>	
ROW Team: LARSON /GROAT	
ROW #: NHSN-075-2(98)-2R-75	
Plan Date: 11-28-17	
Color Legend:	
	Property Lines
	Temporary Easement
	Permanent Acquisition



HUNGERFORD TWP.  
T-90N R-46W  
SEC. 30

Sta. 1085+75  
Install 24" RCP  
F.L. = Lt. 1126.09  
Rt. 1125.33

Sta. 1086+50  
Build Twin 12'x4'x66'  
F.L. = Lt. 1125.00  
Rt. 1124.10  
Design No. 419  
(By Other Plan)

Sta. 1087+25  
Install 24" RCP  
F.L. = Lt. 1126.03  
Rt. 1125.27

Sta. NB 77+88.83 (SUR)  
24 X 30 SLAB BRIDGE  
DA= 182 AC H R  
Remove (By Other Plan)

Sta. 1077+90  
Build Twin 12'x4'x66' RCB  
F.L. = Lt. 1123.91  
Rt. 1123.00  
Design No. 319  
(By Other Plan)

Sta. NB 86+49.97 (SUR)  
24 X 30 SLAB BRIDGE  
DA= 277 AC H R  
Remove (By Other Plan)

HUNGERFORD TWP.  
T-90N R-46W  
SEC. 29

NORTHBOUND  
U.S. 75

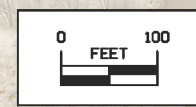
TEMPORARY EASEMENT TO  
CONSTRUCT ENTRANCE

TEMPORARY EASEMENT  
TO SHAPE

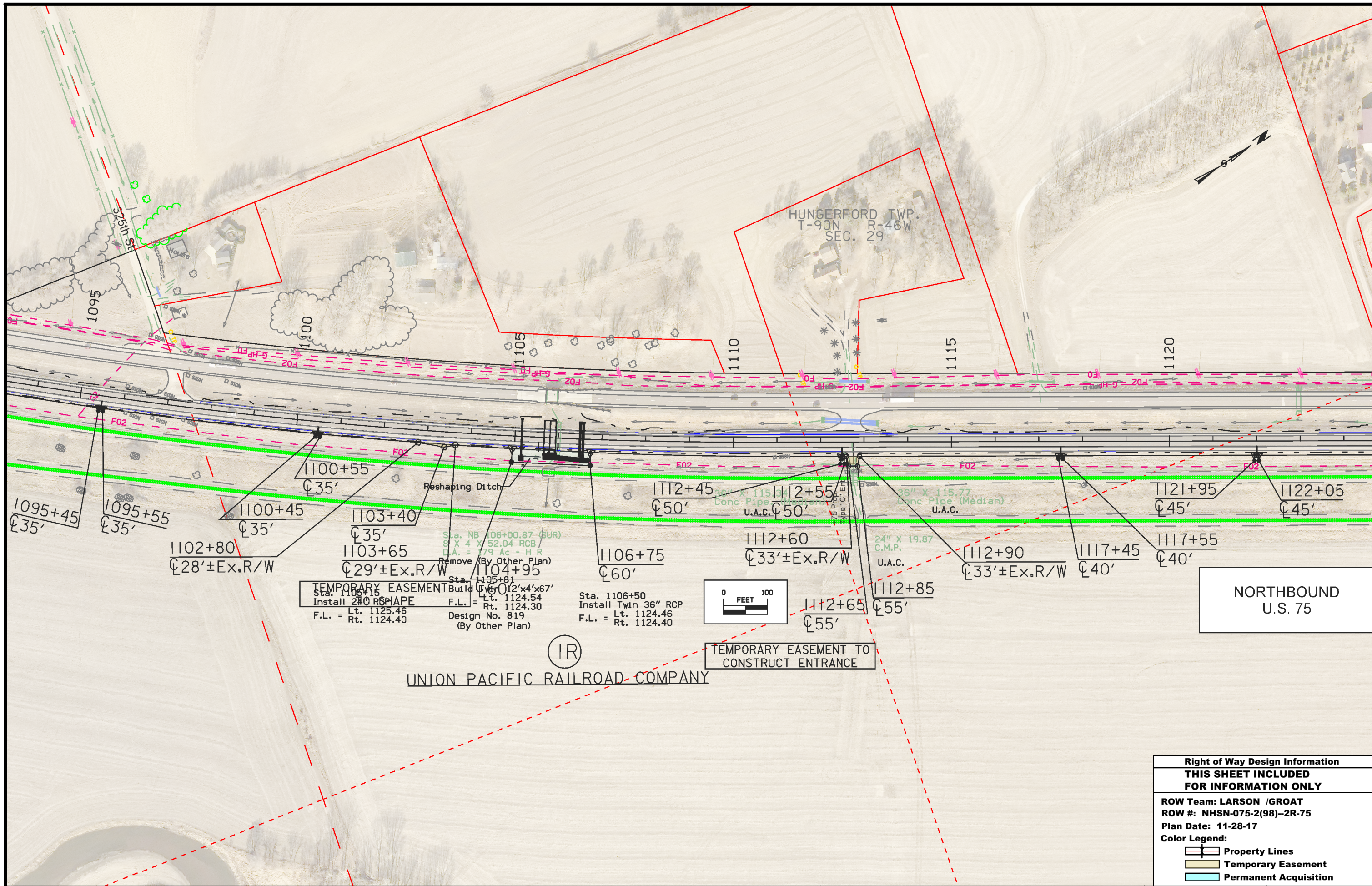


UNION PACIFIC RAILROAD COMPANY

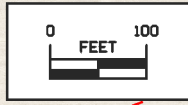
TEMPORARY EASEMENT  
TO SHAPE



<b>Right of Way Design Information</b>	
<b>THIS SHEET INCLUDED FOR INFORMATION ONLY</b>	
ROW Team: LARSON /GROAT	
ROW #: NHSN-075-2(98)-2R-75	
Plan Date: 11-28-17	
Color Legend:	
	Property Lines
	Temporary Easement
	Permanent Acquisition



HUNGERFORD TWP.  
T-90N R-46W  
SEC. 29



TEMPORARY EASEMENT  
Install 24\"/>

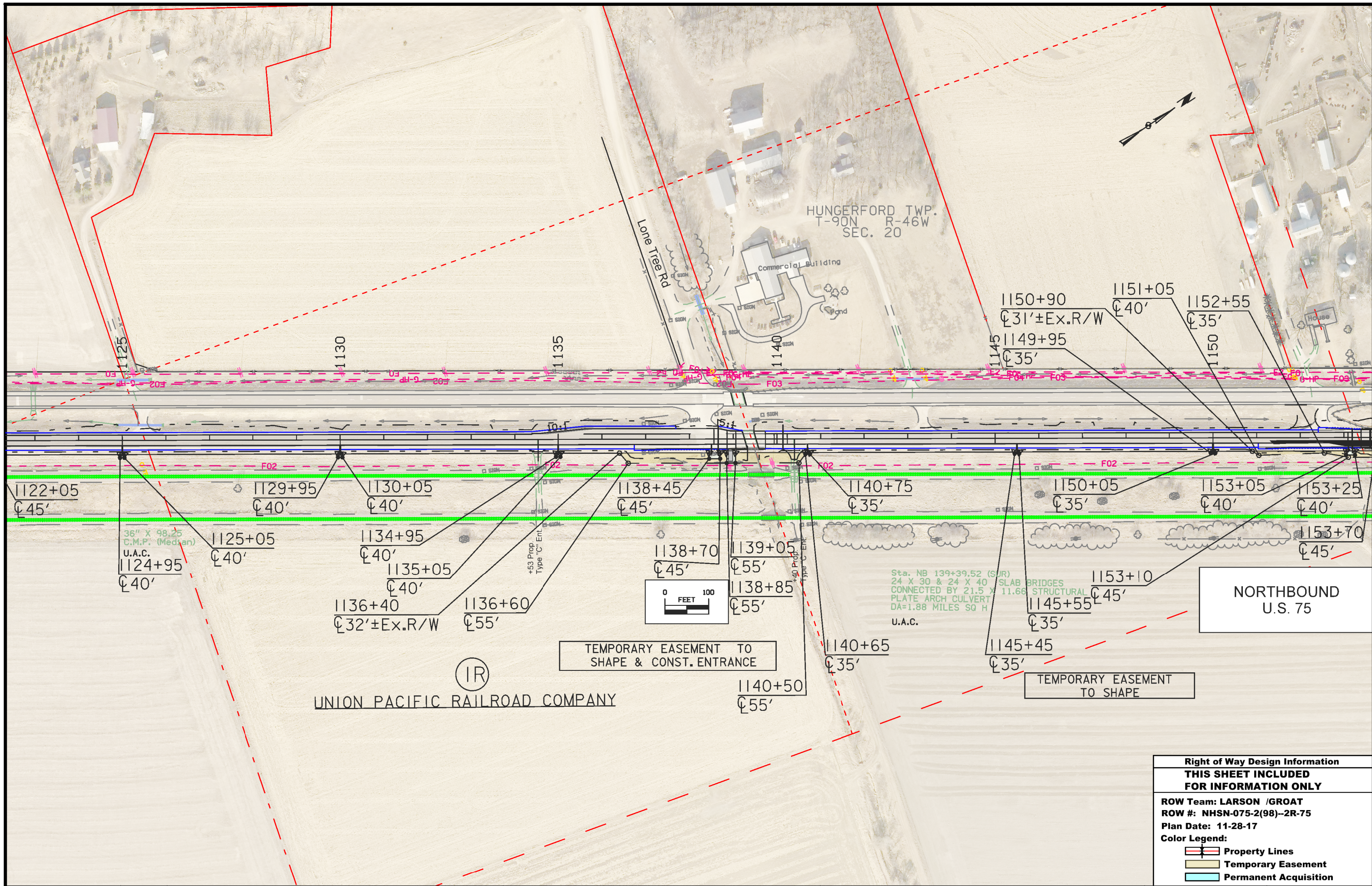
Sta. 1106+50  
Install Twin 36\"/>

TEMPORARY EASEMENT TO  
CONSTRUCT ENTRANCE

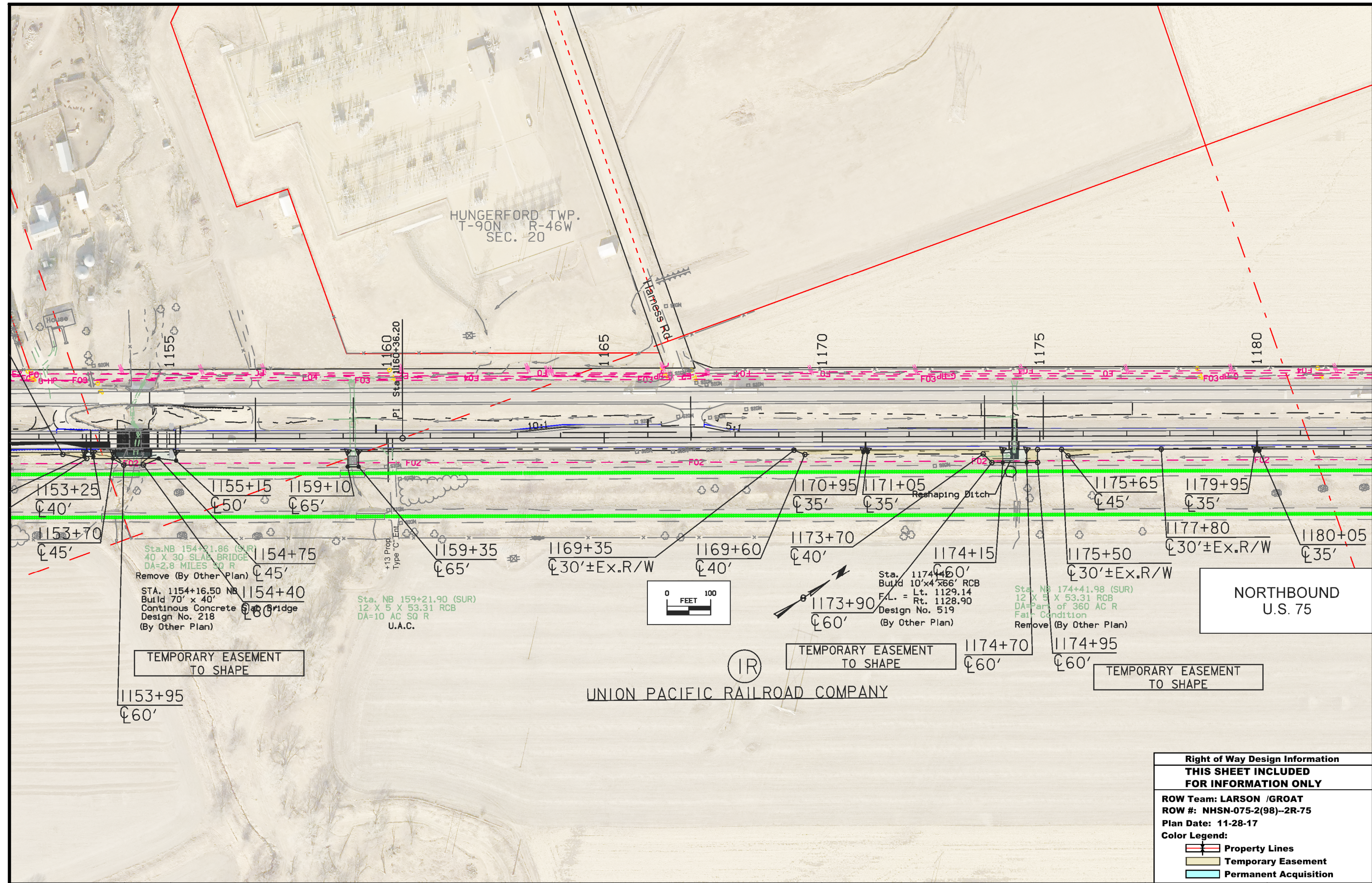
(IR)  
UNION PACIFIC RAILROAD COMPANY

NORTHBOUND  
U.S. 75

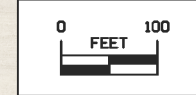
<b>Right of Way Design Information</b>	
<b>THIS SHEET INCLUDED FOR INFORMATION ONLY</b>	
ROW Team: LARSON /GROAT	
ROW #: NHSN-075-2(98)-2R-75	
Plan Date: 11-28-17	
Color Legend:	
	Property Lines
	Temporary Easement
	Permanent Acquisition



<b>Right of Way Design Information</b>	
<b>THIS SHEET INCLUDED FOR INFORMATION ONLY</b>	
ROW Team: LARSON /GROAT	
ROW #: NHSN-075-2(98)-2R-75	
Plan Date: 11-28-17	
Color Legend:	
	Property Lines
	Temporary Easement
	Permanent Acquisition



HUNGERFORD TWP.  
T-90N R-46W  
SEC. 20



UNION PACIFIC RAILROAD COMPANY

NORTHBOUND  
U.S. 75

Sta. NB 154+21.86 (SUR)  
40 X 30 SLAB BRIDGE  
DA=2.8 MILES SQ R  
Remove (By Other Plan)  
STA. 1154+16.50 NB  
Build 70' x 40'  
Continuous Concrete Slab Bridge  
Design No. 218  
(By Other Plan)

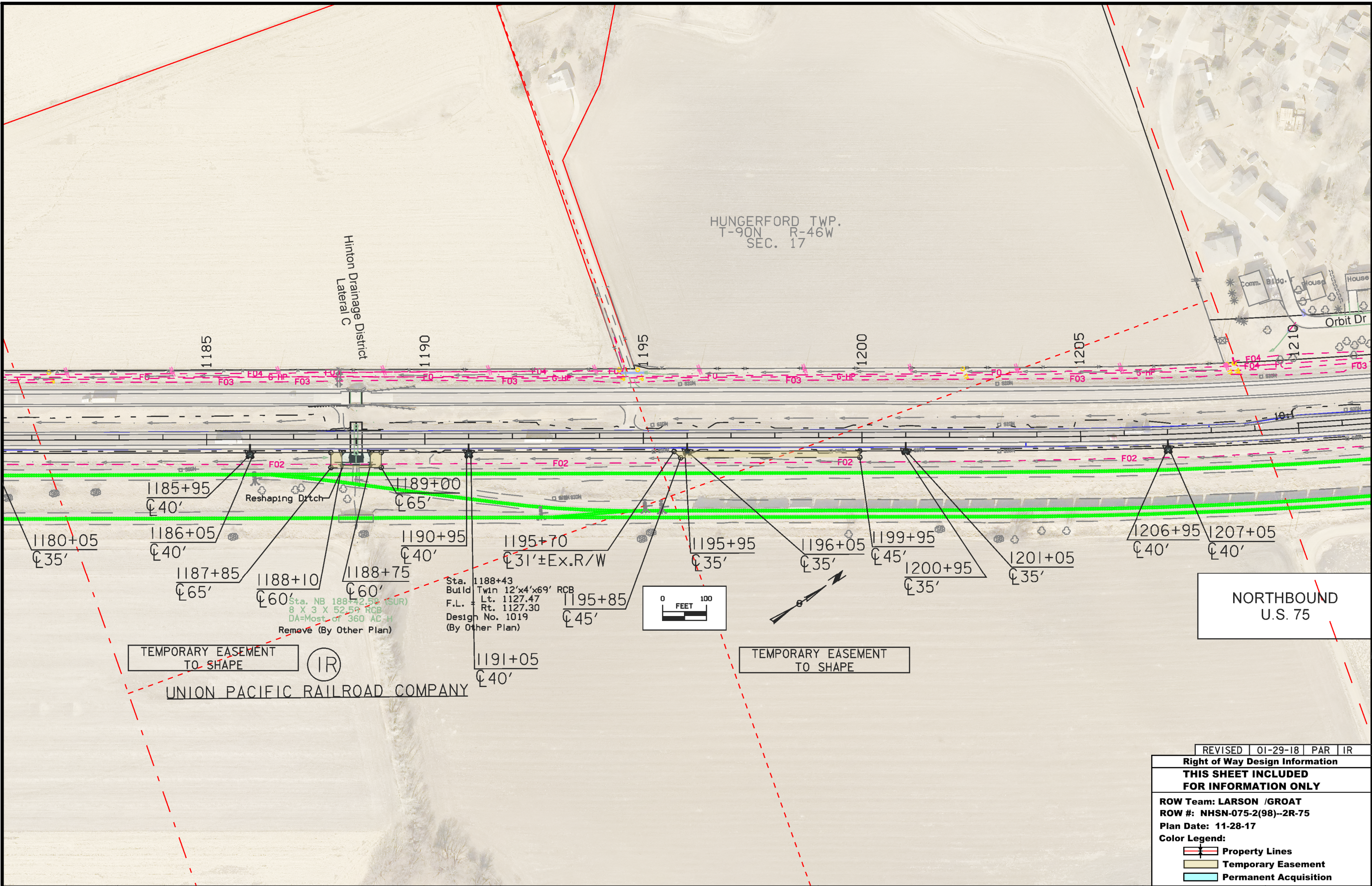
Sta. NB 159+21.90 (SUR)  
12 X 5 X 53.31 RCB  
DA=10 AC SQ R  
U.A.C.

Sta. 1174+42  
Build 10'x4'x66' RCB  
F.L. = Lt. 1129.14  
= Rt. 1128.90  
Design No. 519  
(By Other Plan)

Sta. NB 174+41.98 (SUR)  
12 X 5 X 53.31 RCB  
DA=Part of 360 AC R  
Fair Condition  
Remove (By Other Plan)

Right of Way Design Information	
<b>THIS SHEET INCLUDED FOR INFORMATION ONLY</b>	
ROW Team: LARSON /GROAT	
ROW #: NHSN-075-2(98)-2R-75	
Plan Date: 11-28-17	
Color Legend:	
	Property Lines
	Temporary Easement
	Permanent Acquisition





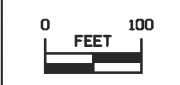
HUNGERFORD TWP.  
T-90N R-46W  
SEC. 17

Hinton Drainage District  
Lateral C

NORTHBOUND  
U.S. 75

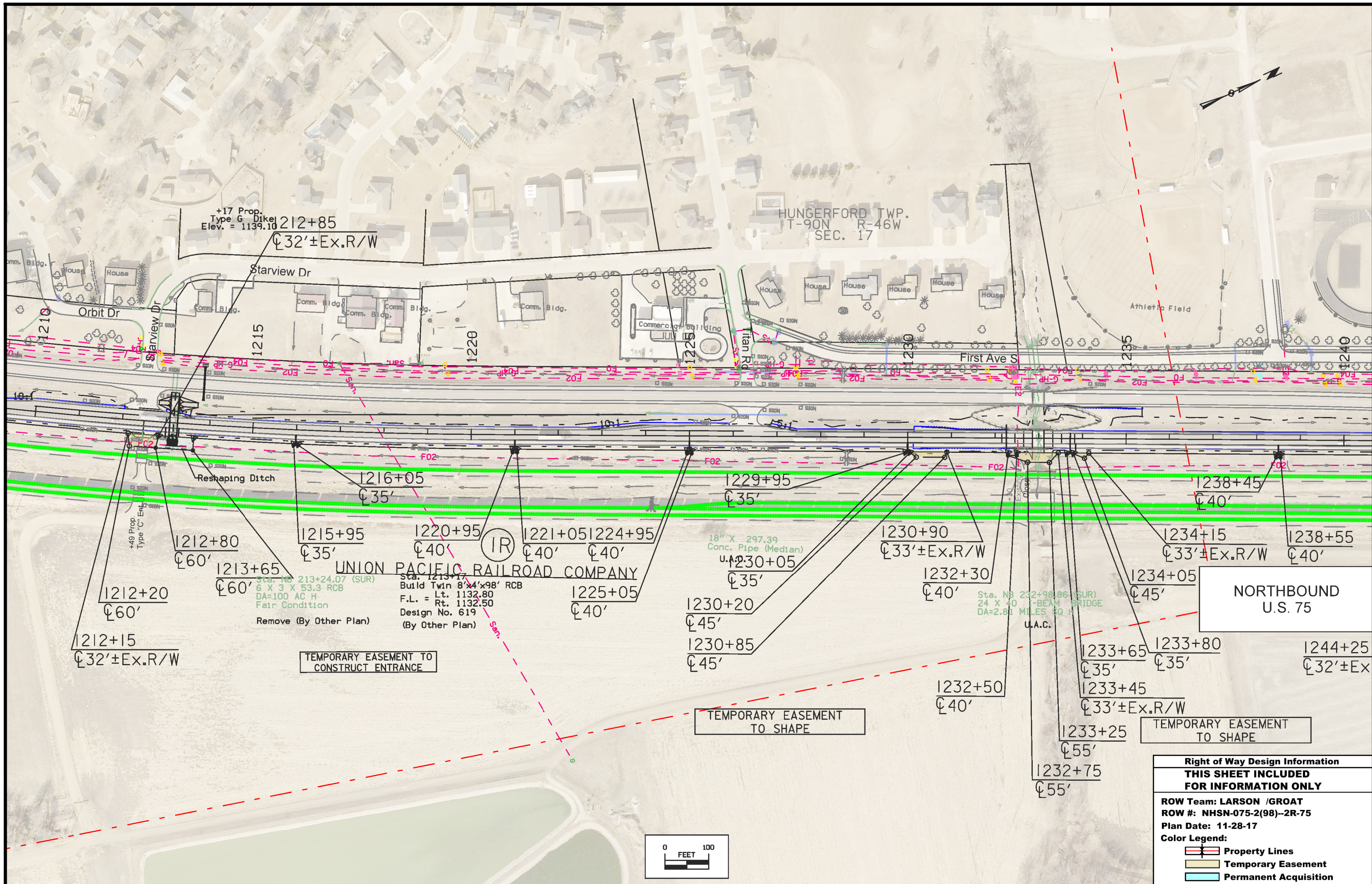
TEMPORARY EASEMENT  
TO SHAPE (IR)  
UNION PACIFIC RAILROAD COMPANY

Sta. 1188+43  
Build Twin 12'x4'x69' RCB  
F.L. Lt. 1127.47  
Rt. 1127.30  
Design No. 1019  
(By Other Plan)

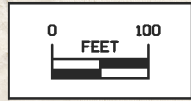


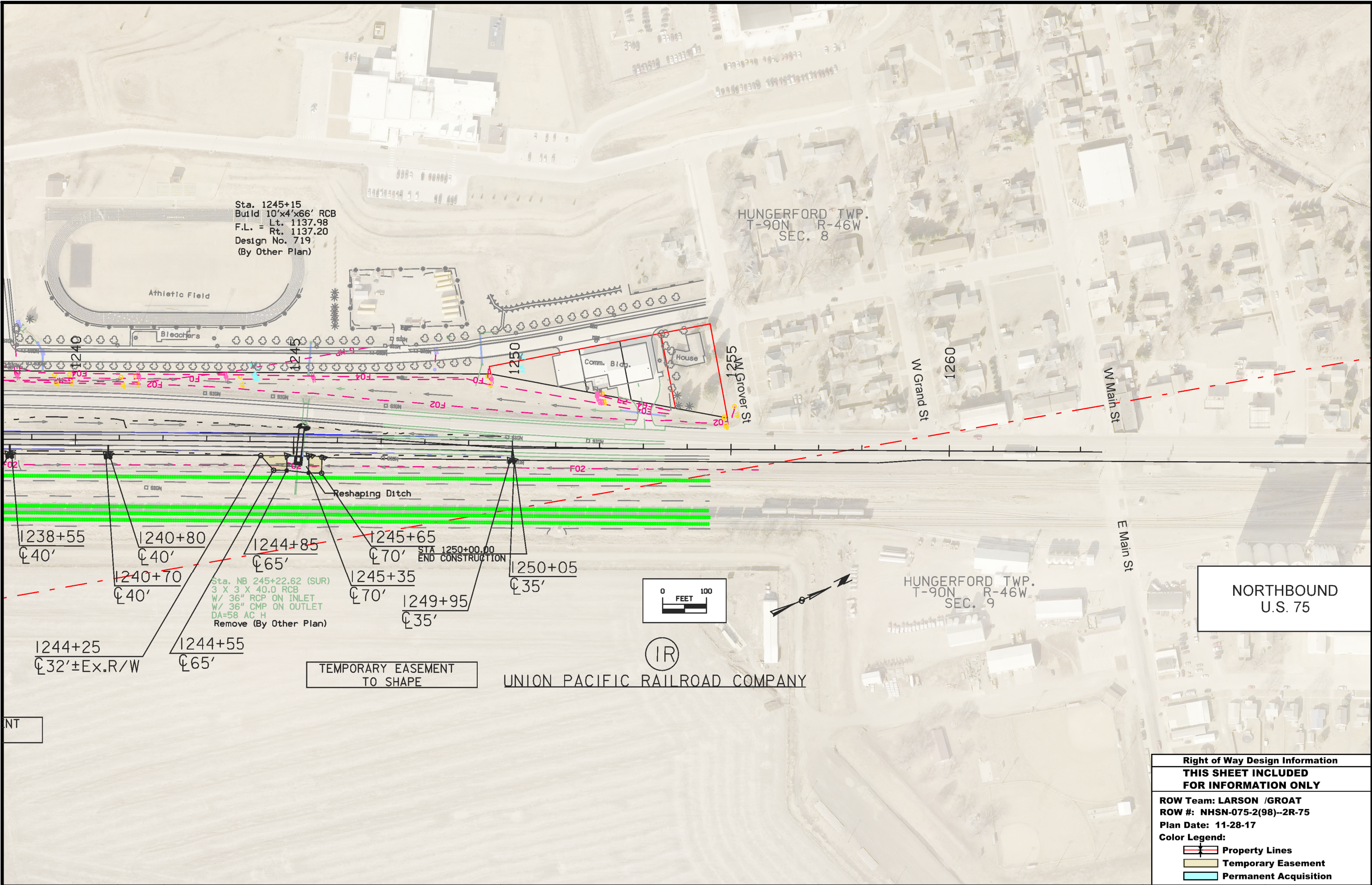
TEMPORARY EASEMENT  
TO SHAPE

REVISED	01-29-18	PAR	IR
<b>Right of Way Design Information</b>			
<b>THIS SHEET INCLUDED FOR INFORMATION ONLY</b>			
ROW Team: LARSON /GROAT			
ROW #: NHSN-075-2(98)--2R-75			
Plan Date: 11-28-17			
Color Legend:			
	Property Lines		
	Temporary Easement		
	Permanent Acquisition		



<b>Right of Way Design Information</b>
<b>THIS SHEET INCLUDED FOR INFORMATION ONLY</b>
ROW Team: LARSON /GROAT
ROW #: NHSN-075-2(98)-2R-75
Plan Date: 11-28-17
<b>Color Legend:</b>
Property Lines
Temporary Easement
Permanent Acquisition





Sta. 1245+15  
 Build 10'x4'x66' RCB  
 F.L. = Lt. 1137.98  
 F.L. = Rt. 1137.20  
 Design No. 719  
 (By Other Plan)

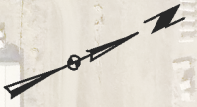
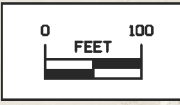
HUNGERFORD TWP.  
 T-90N R-46W  
 SEC. 8

HUNGERFORD TWP.  
 T-90N R-46W  
 SEC. 9

NORTHBOUND  
 U.S. 75

TEMPORARY EASEMENT  
 TO SHAPE

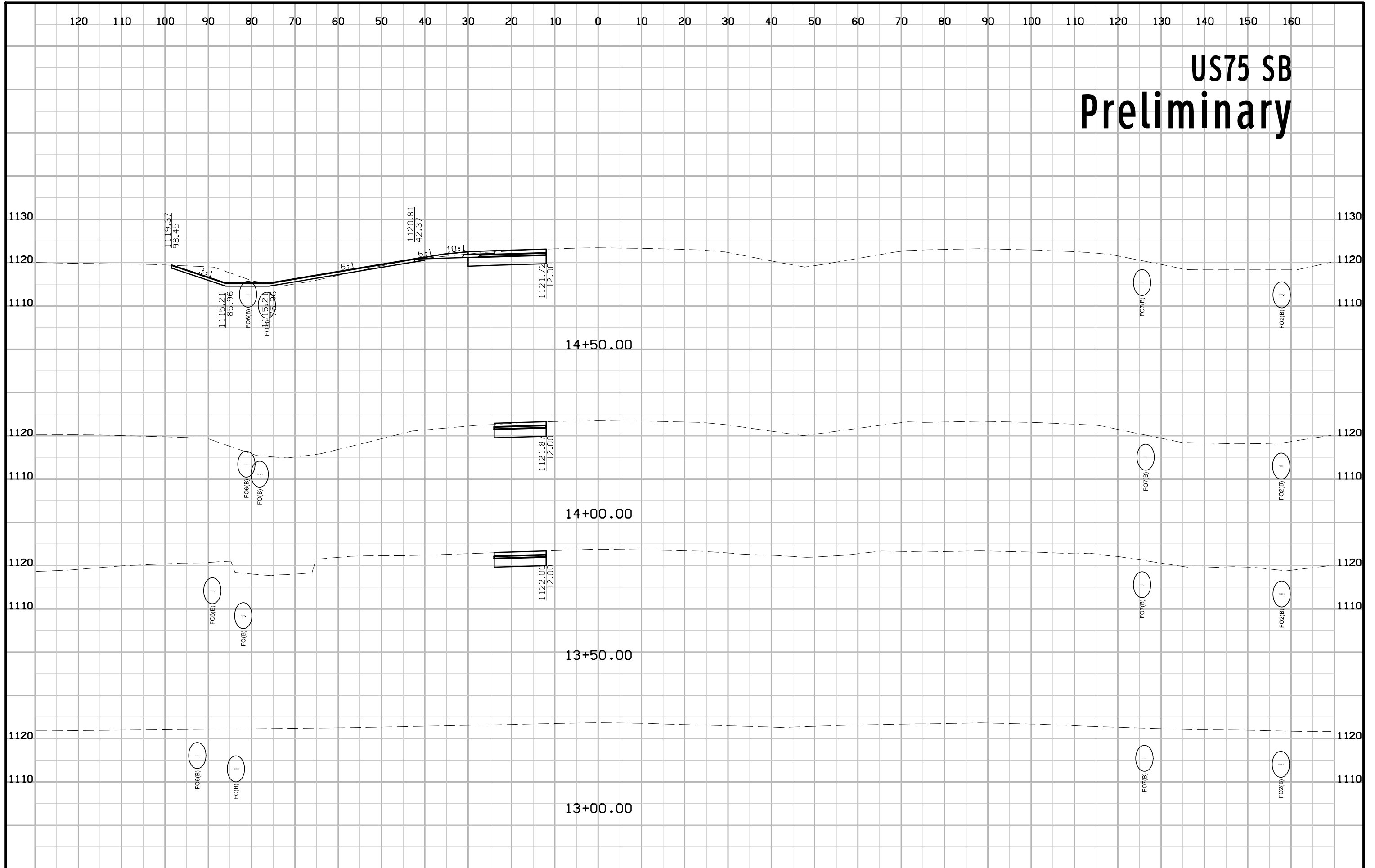
UNION PACIFIC RAILROAD COMPANY



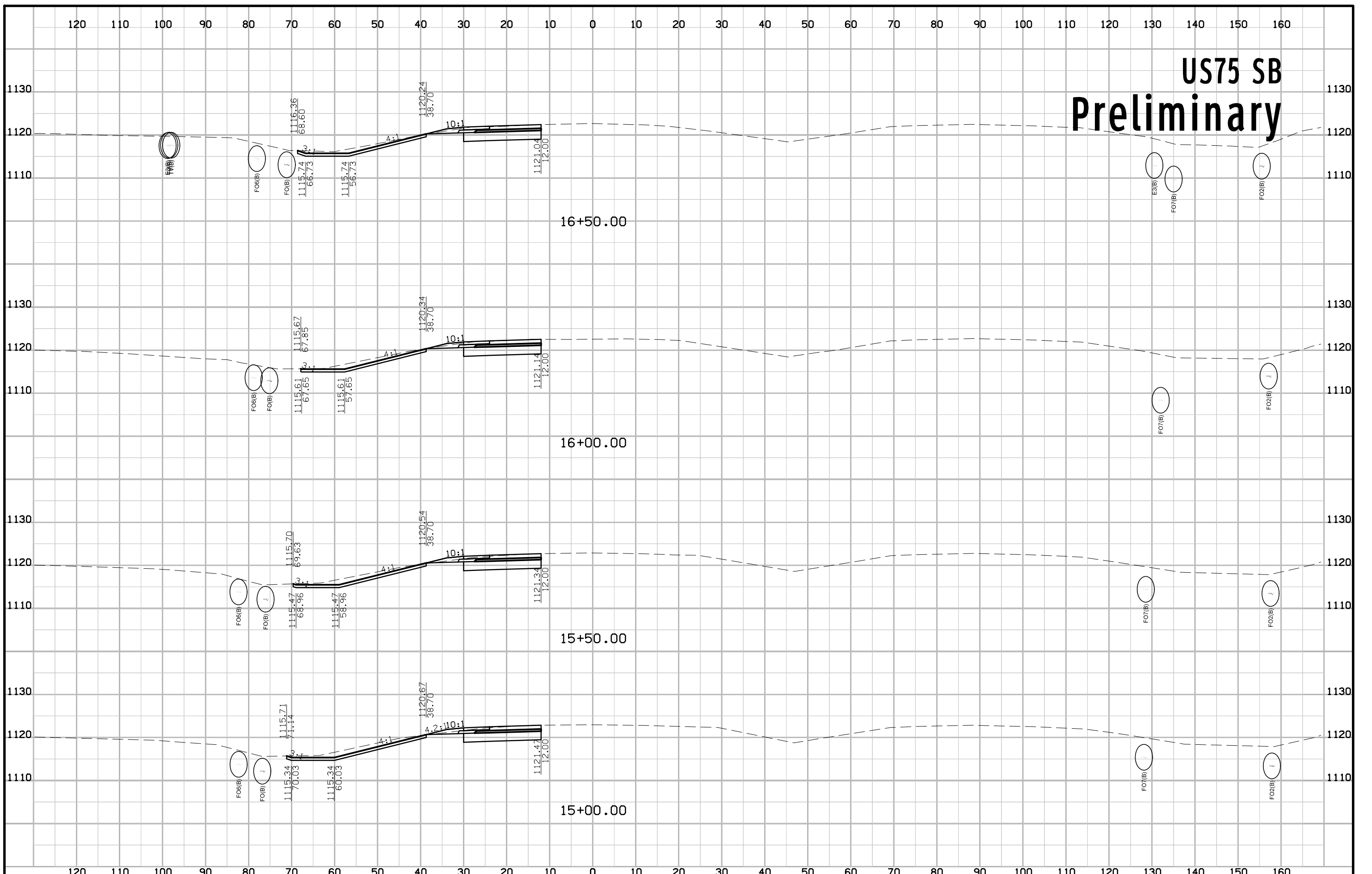
**Right of Way Design Information**  
**THIS SHEET INCLUDED**  
**FOR INFORMATION ONLY**

ROW Team: LARSON /GROAT  
 ROW #: NHSN-075-2(98)-2R-75  
 Plan Date: 11-28-17  
 Color Legend:  
 - Property Lines  
 - Temporary Easement  
 - Permanent Acquisition

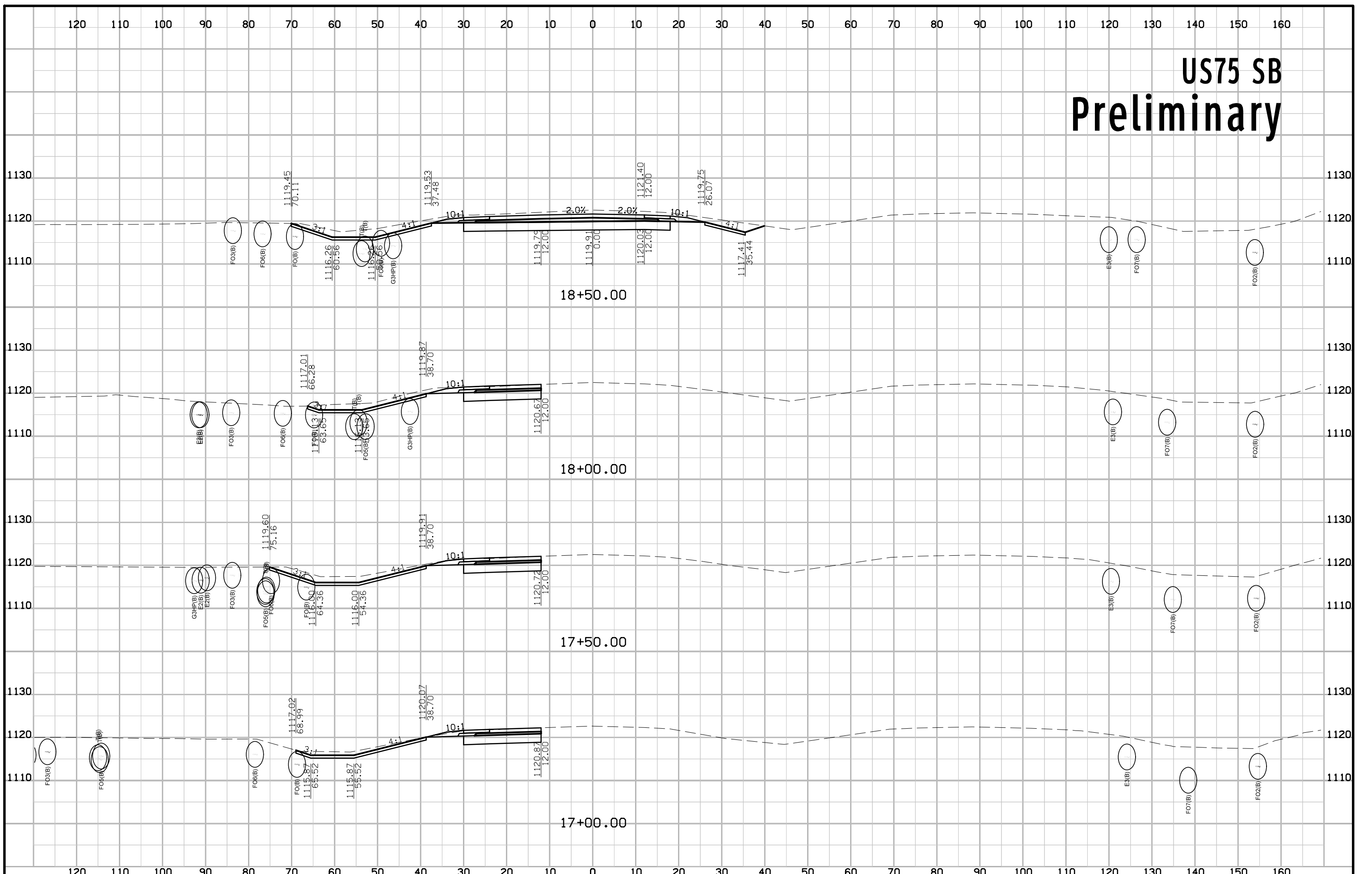
# US75 SB Preliminary



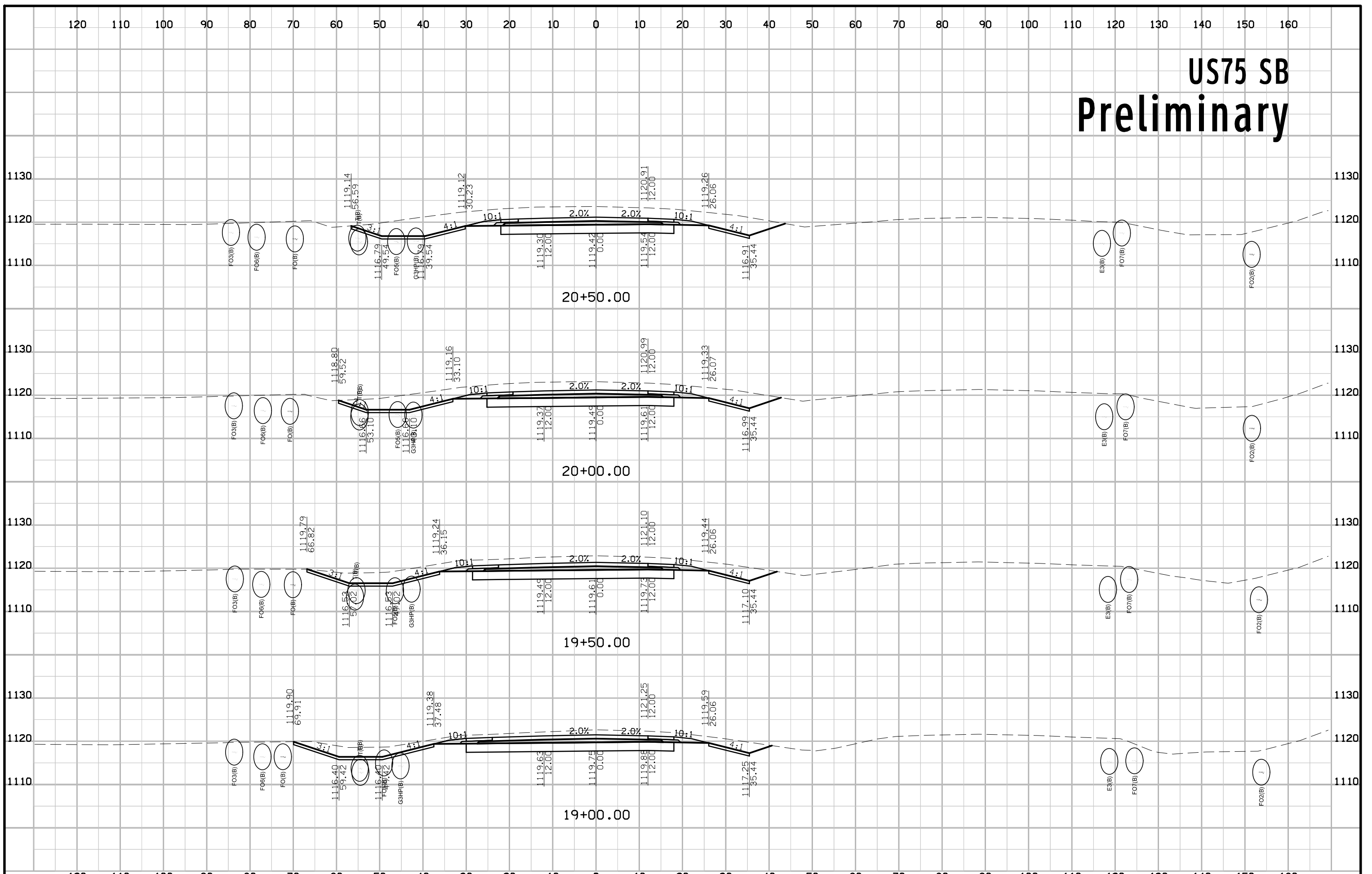
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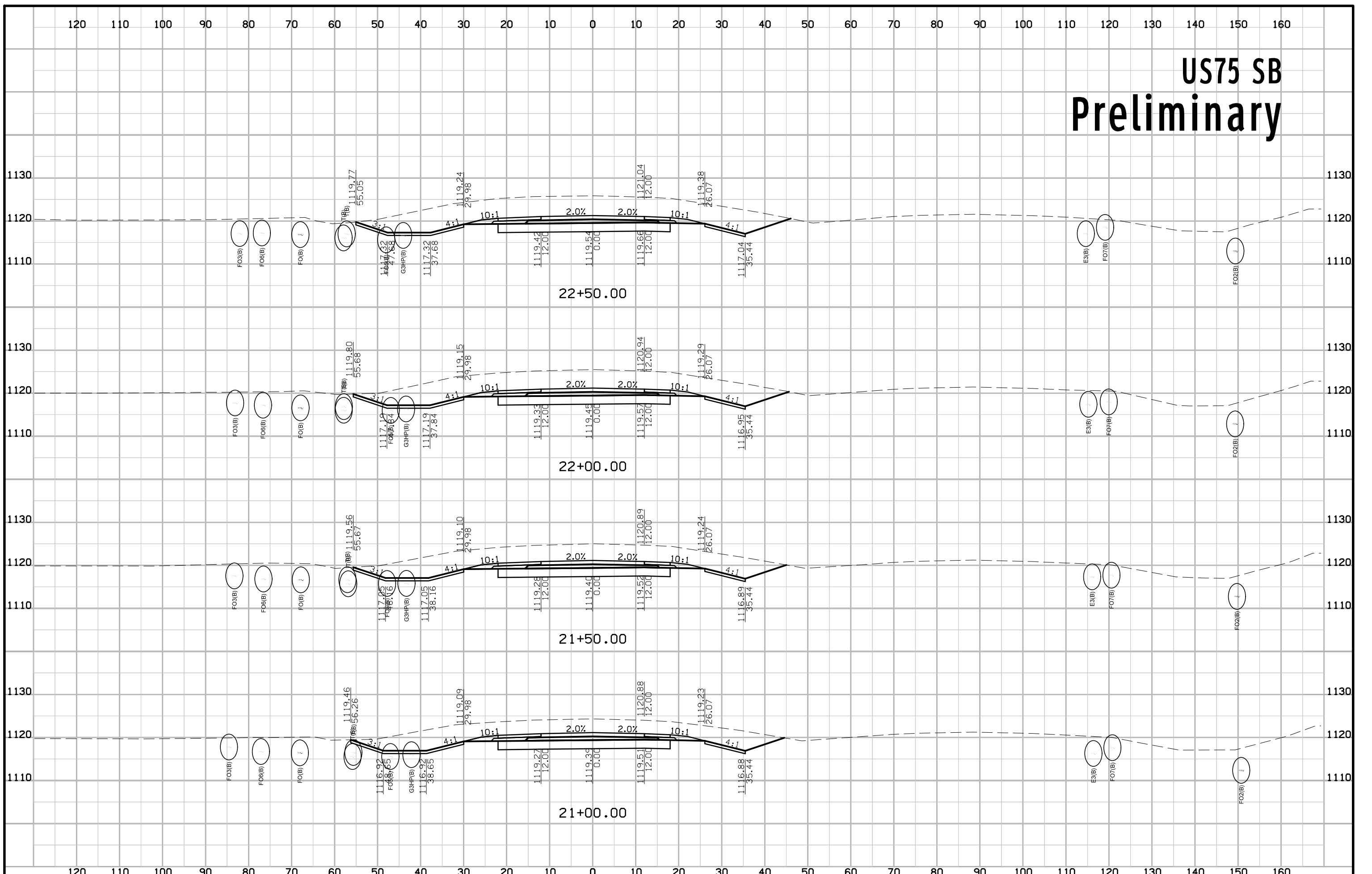
# US75 SB Preliminary



# US75 SB Preliminary

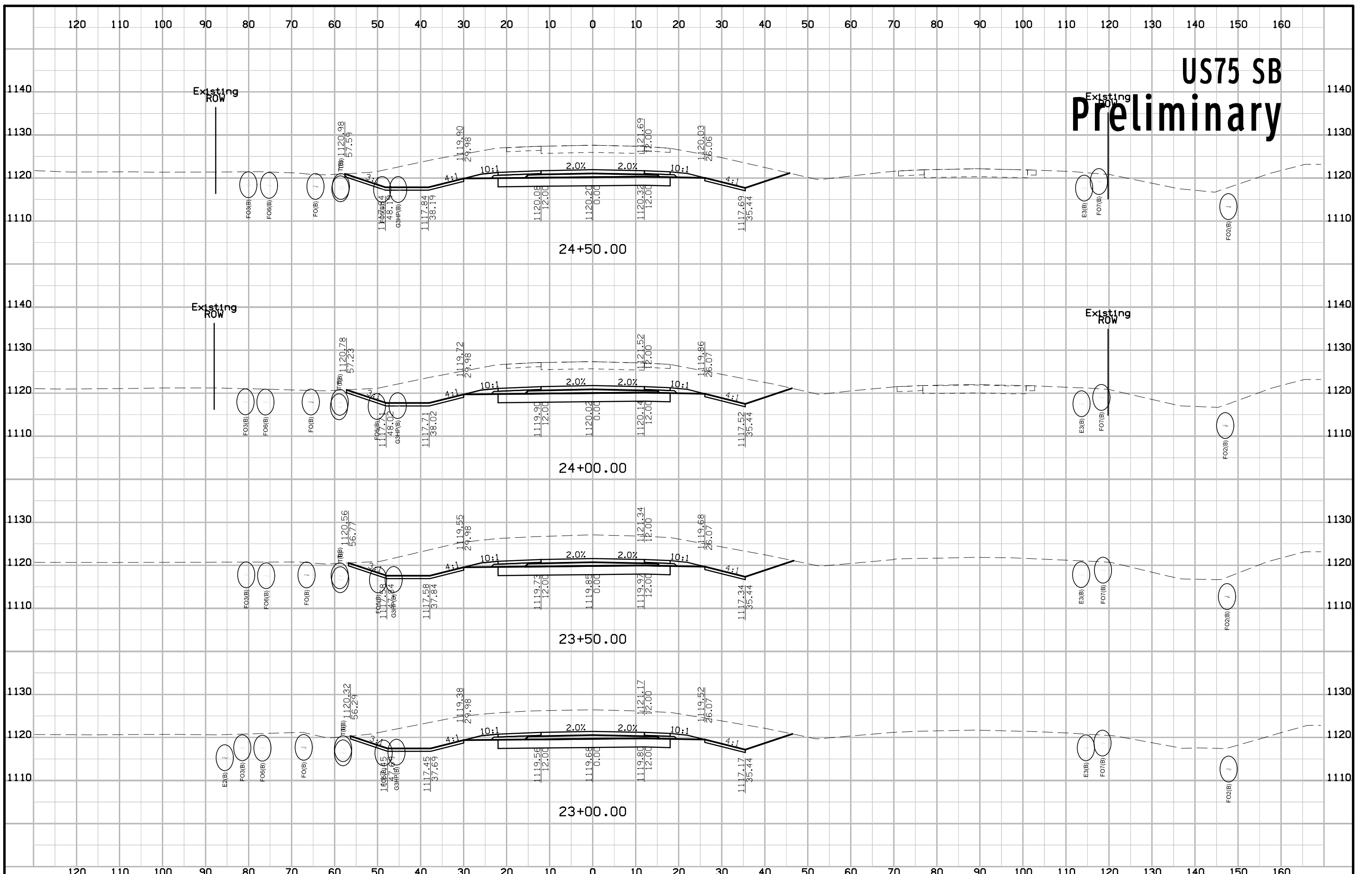


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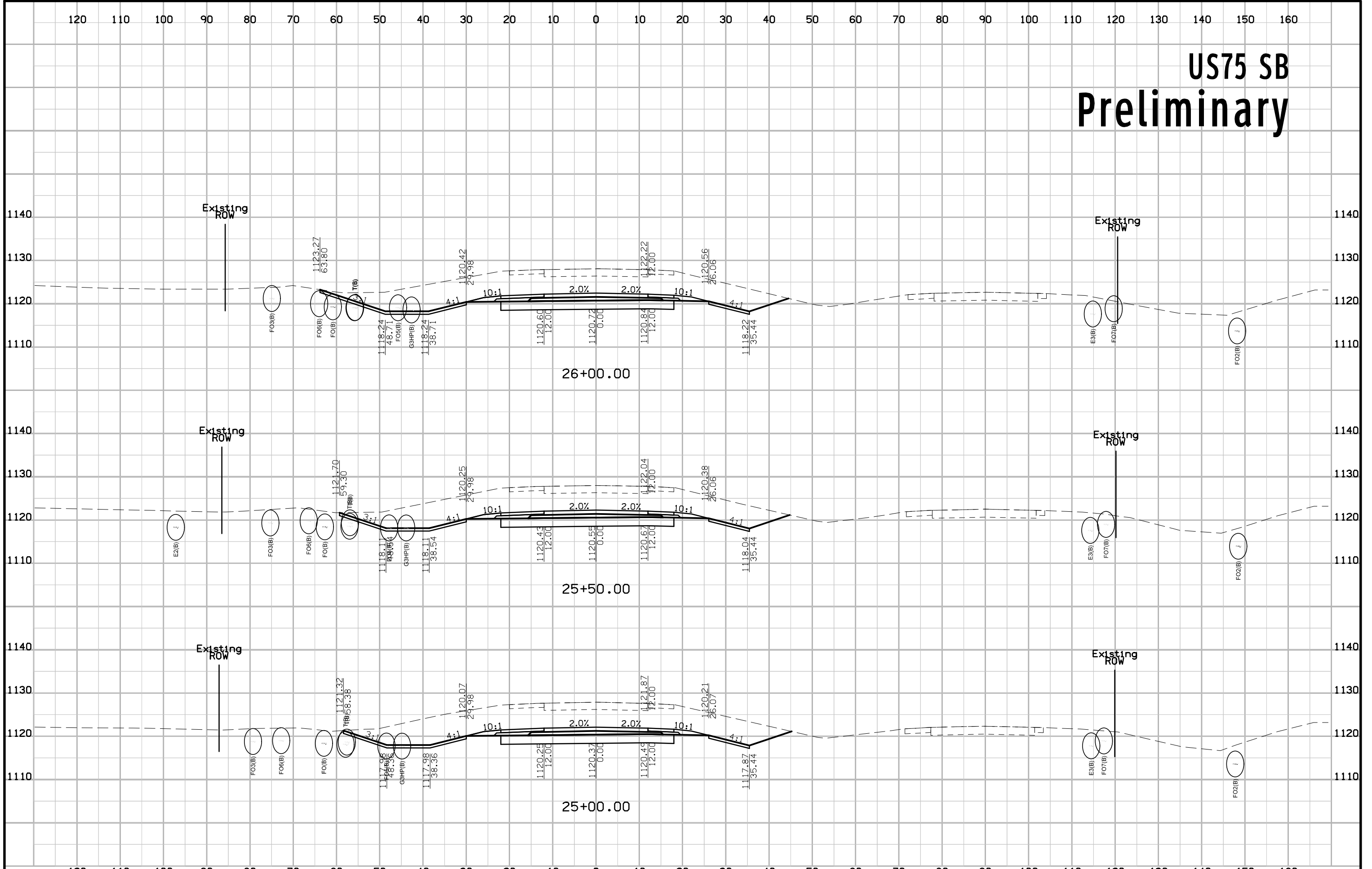




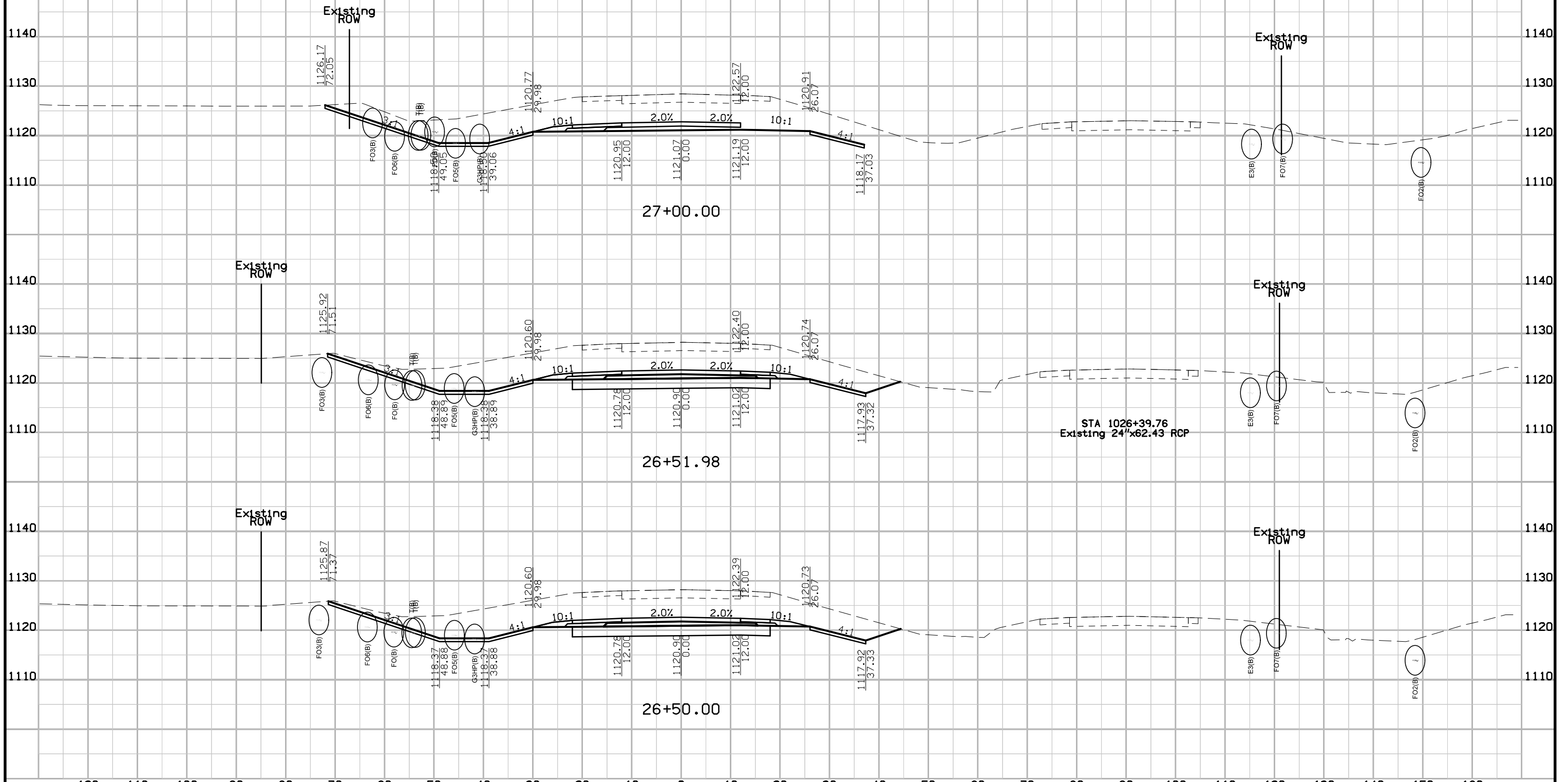
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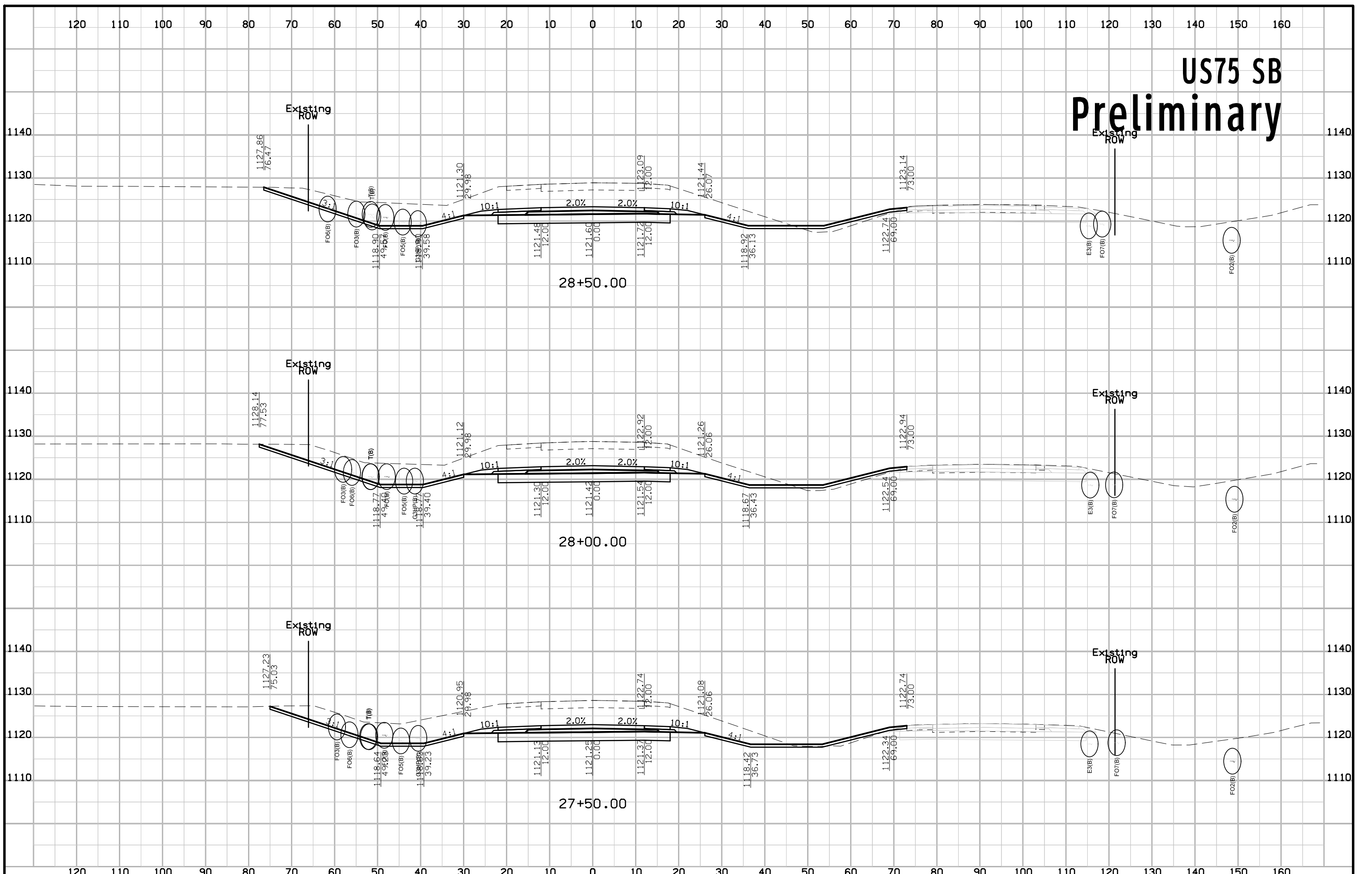
# US75 SB Preliminary



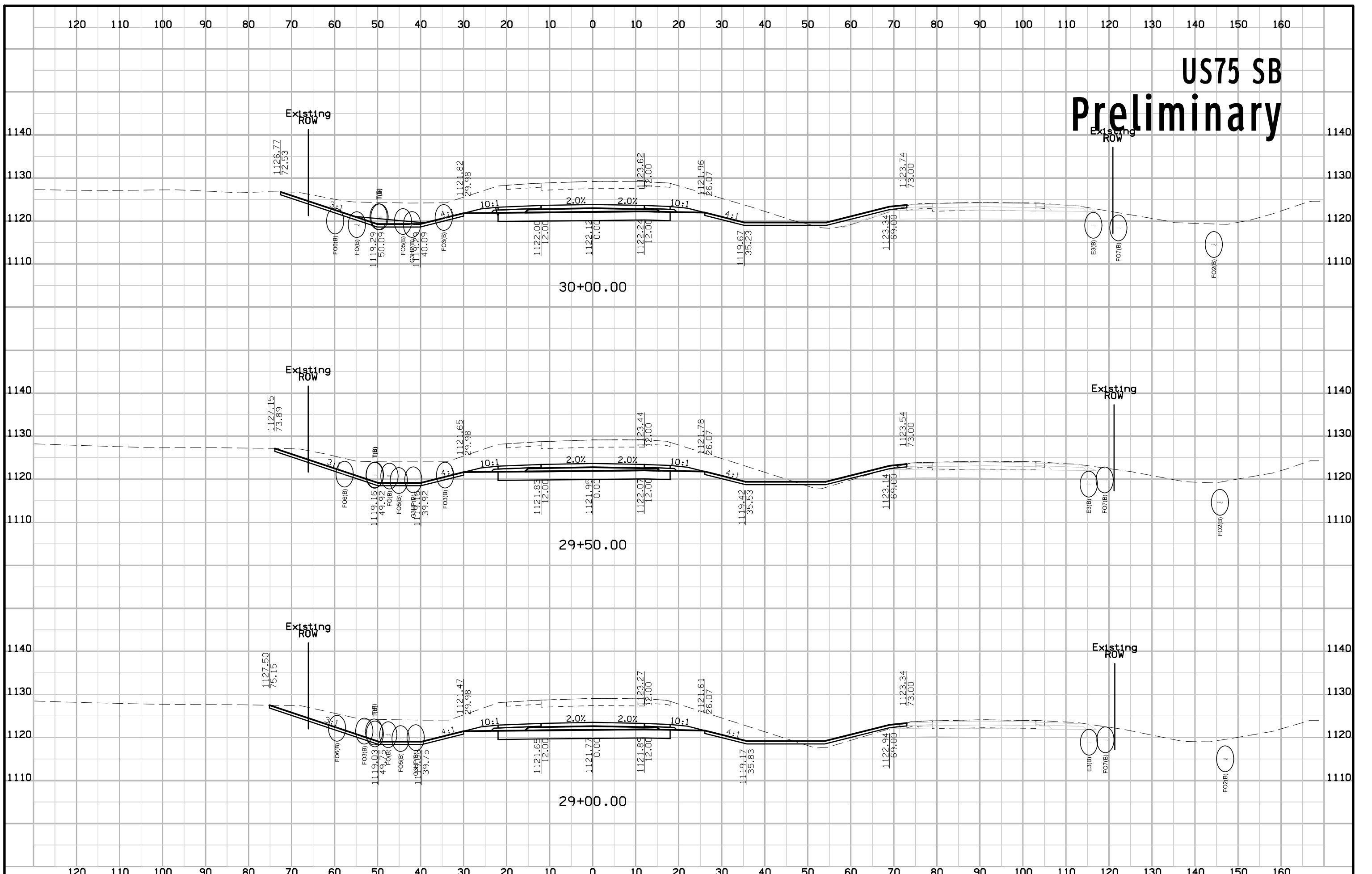
# US75 SB Preliminary



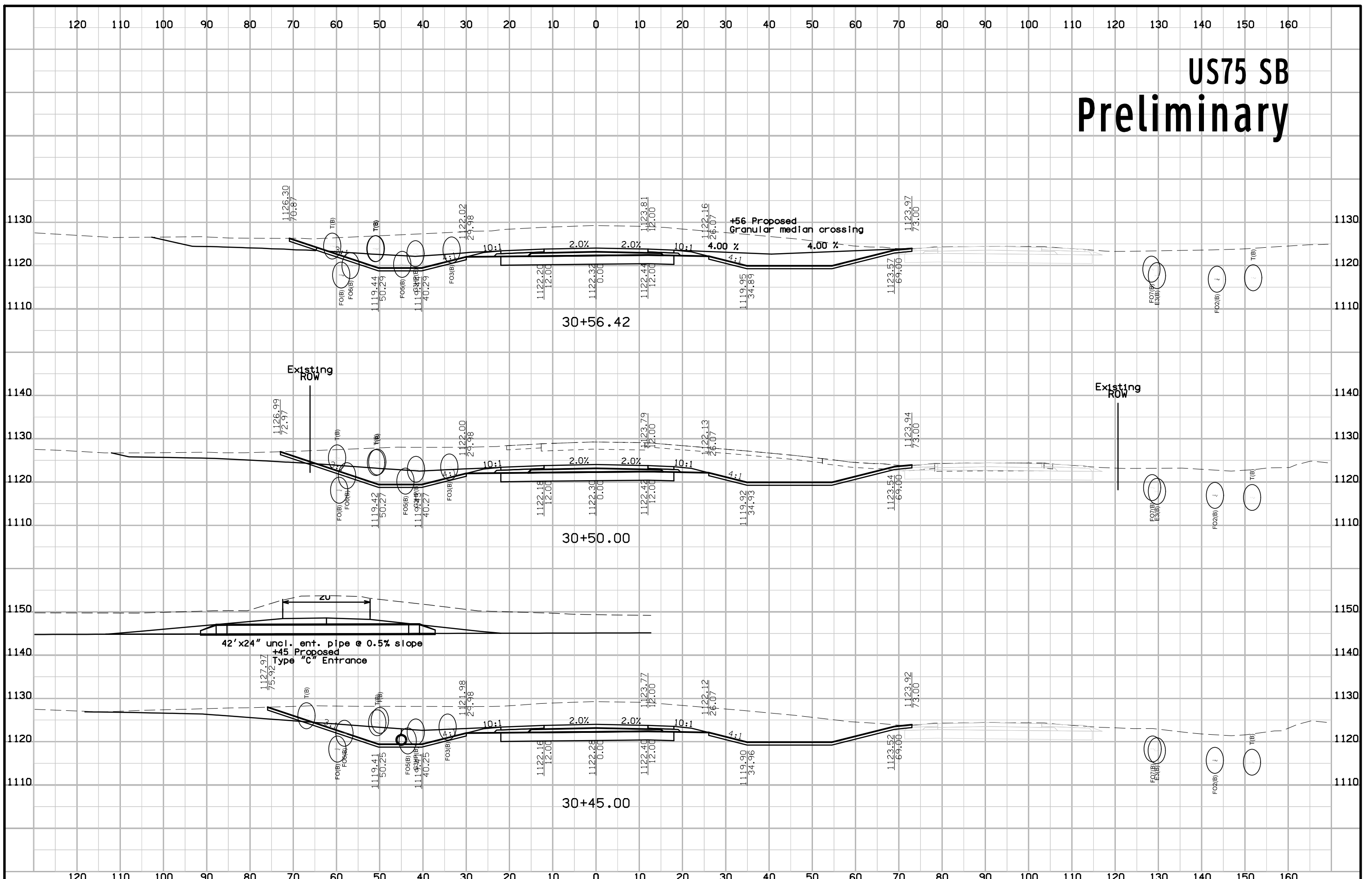
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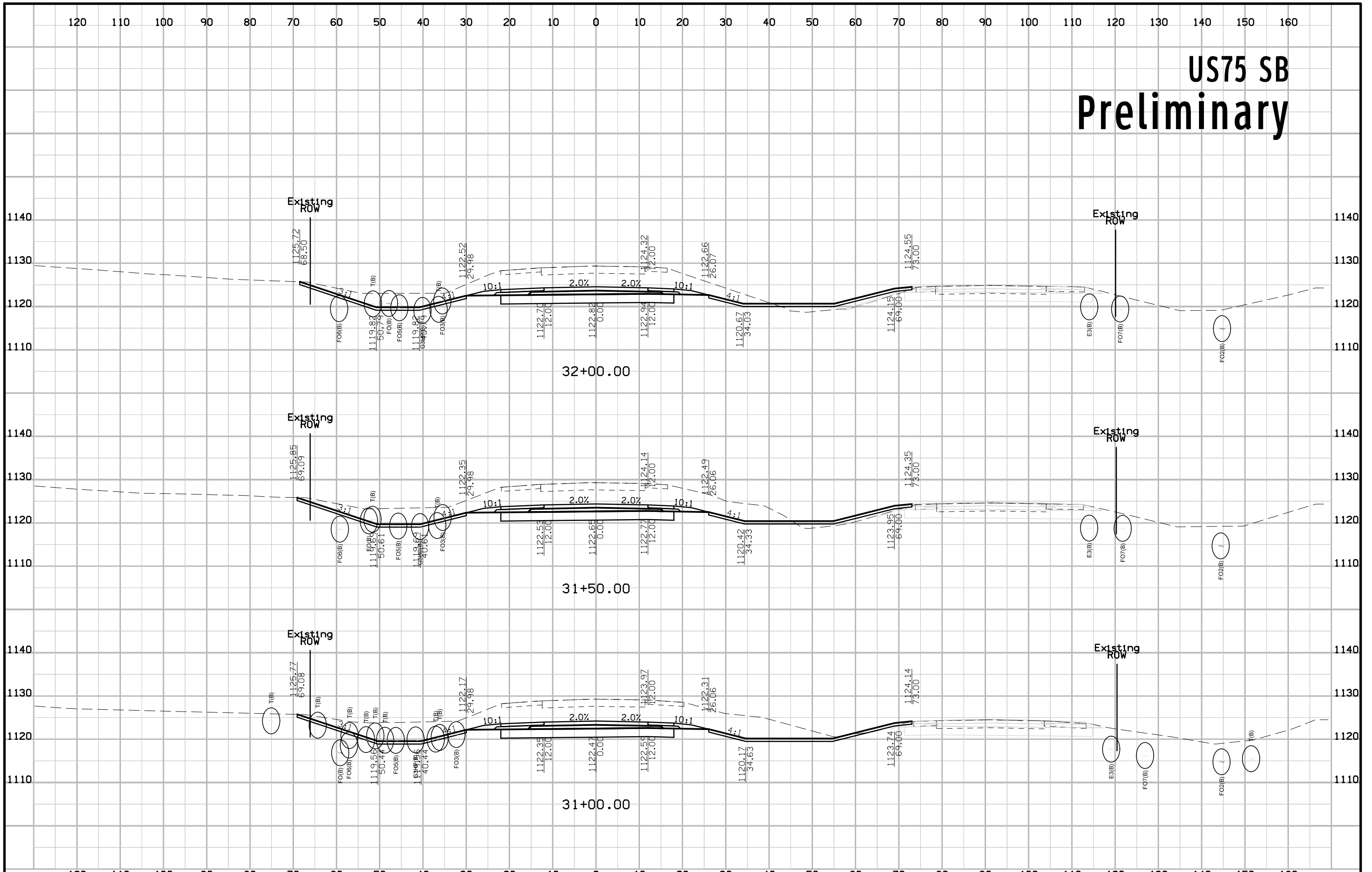
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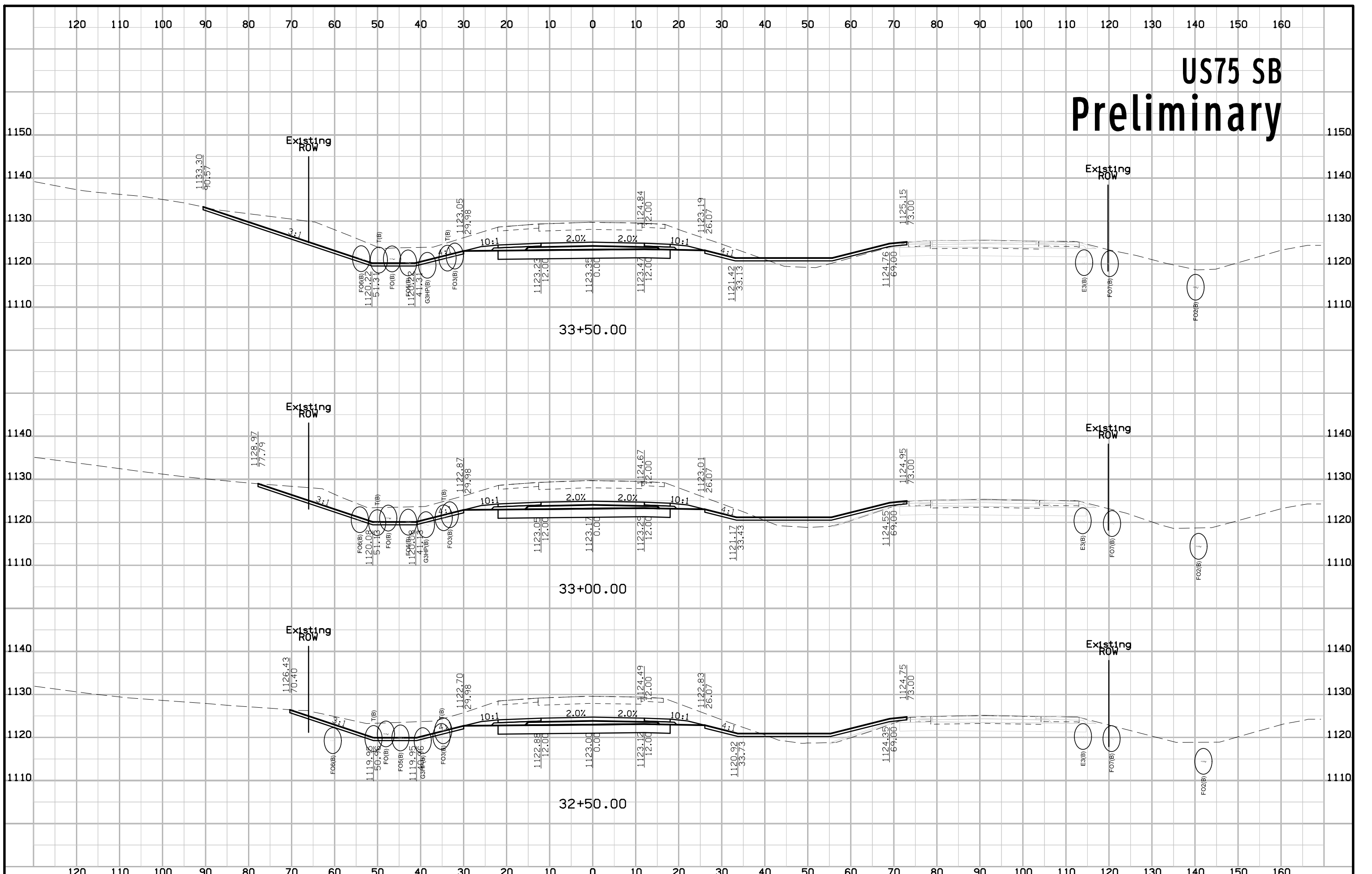
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# US75 SB Preliminary

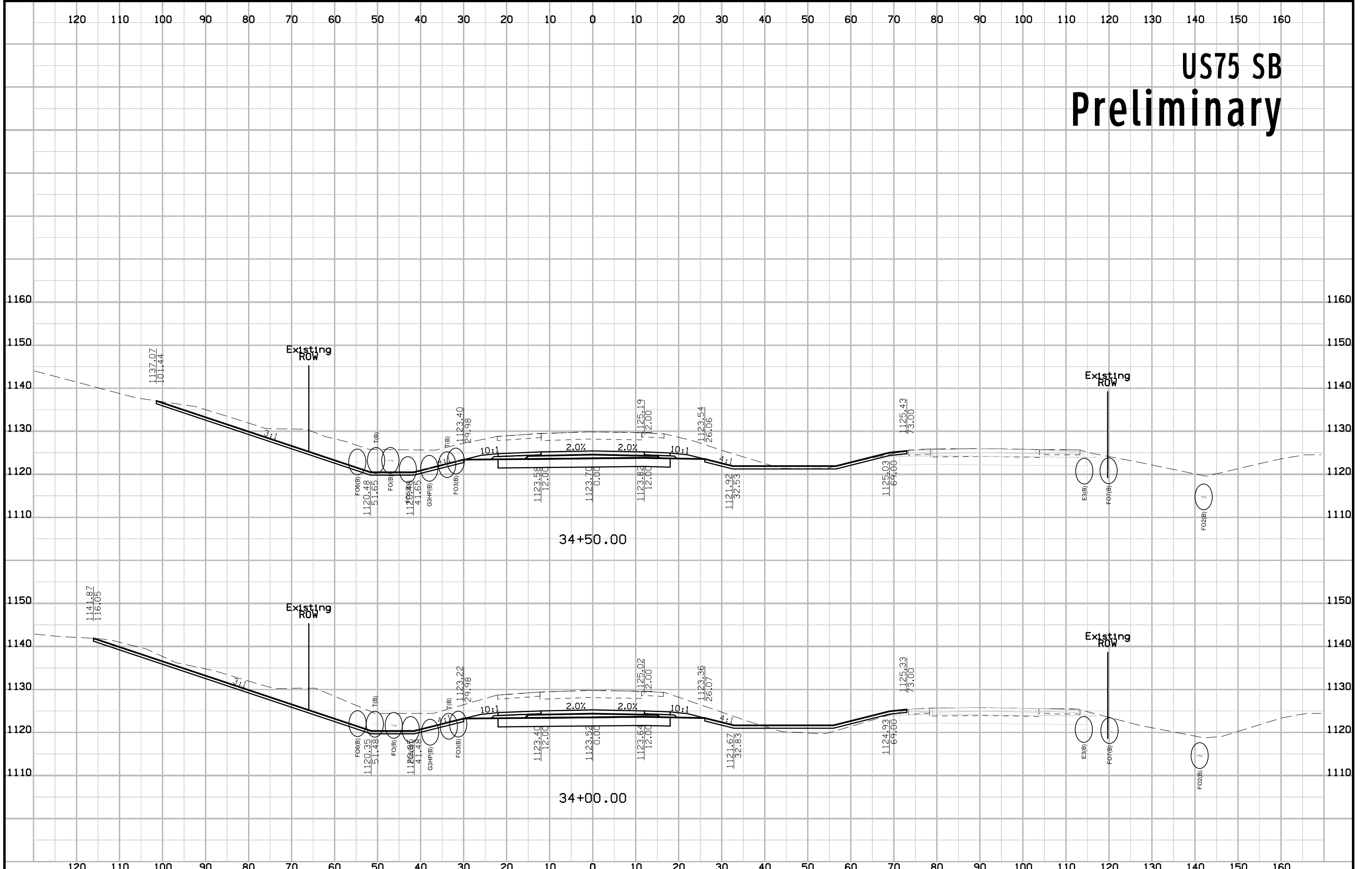


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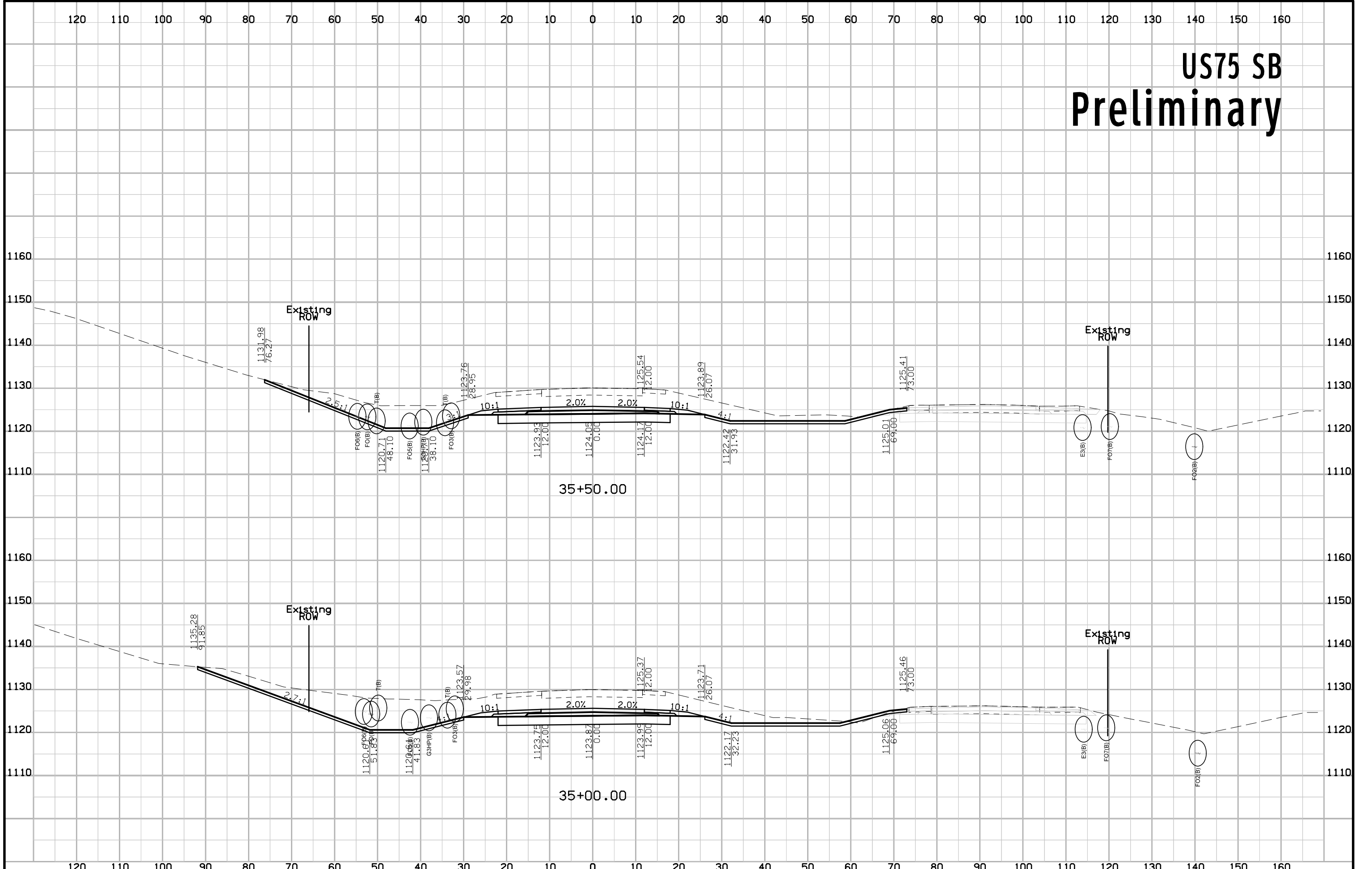




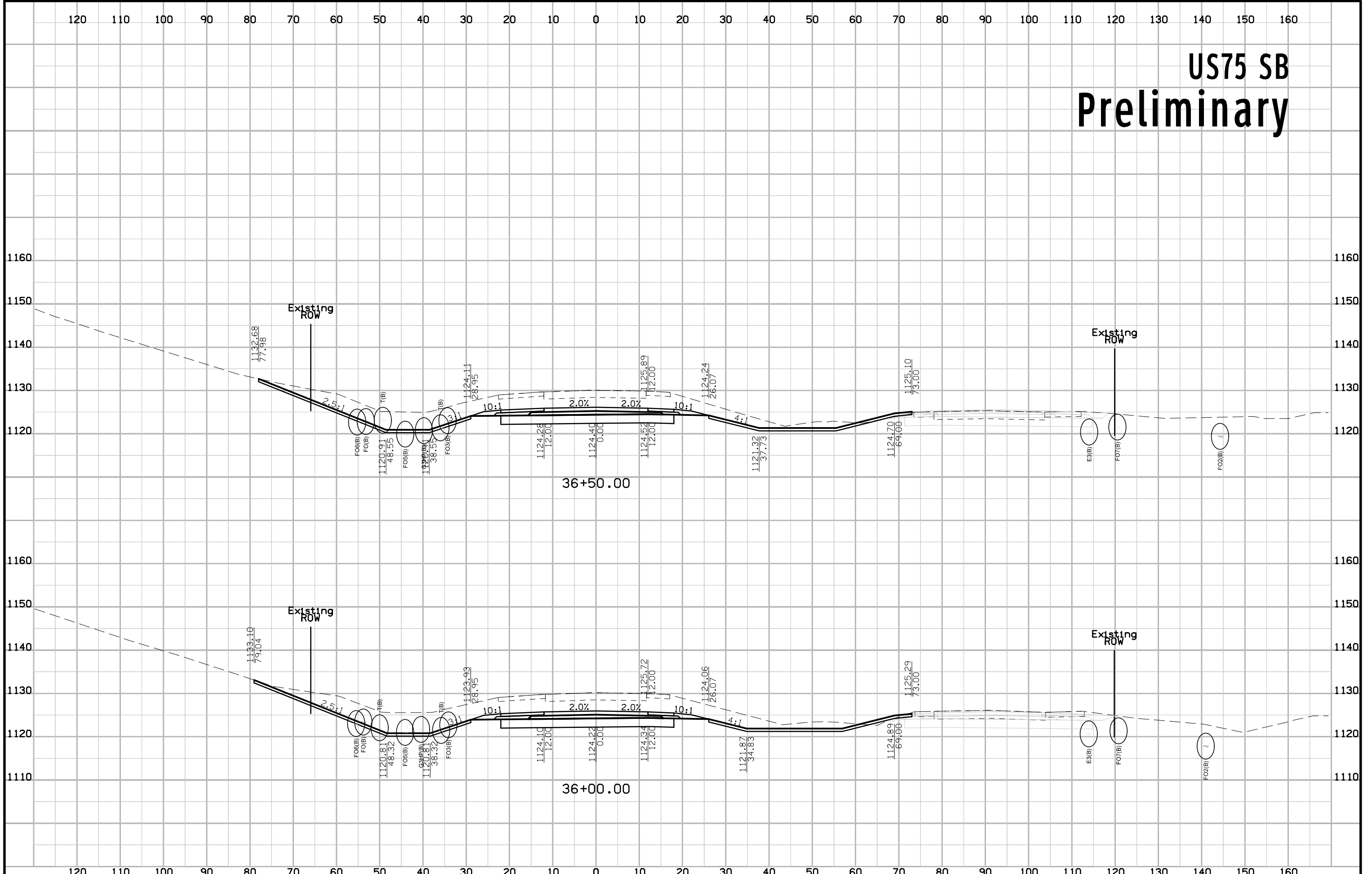
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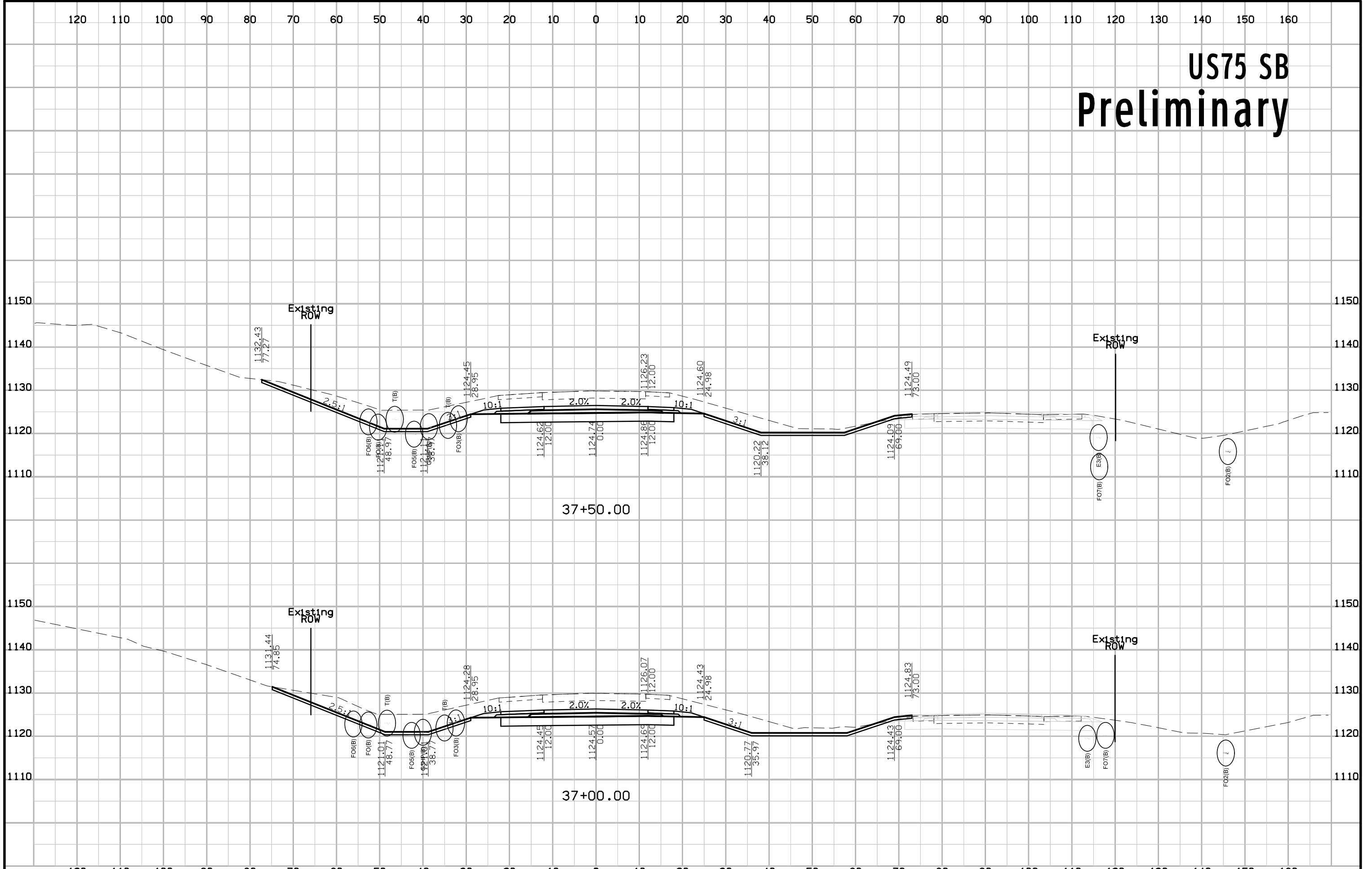
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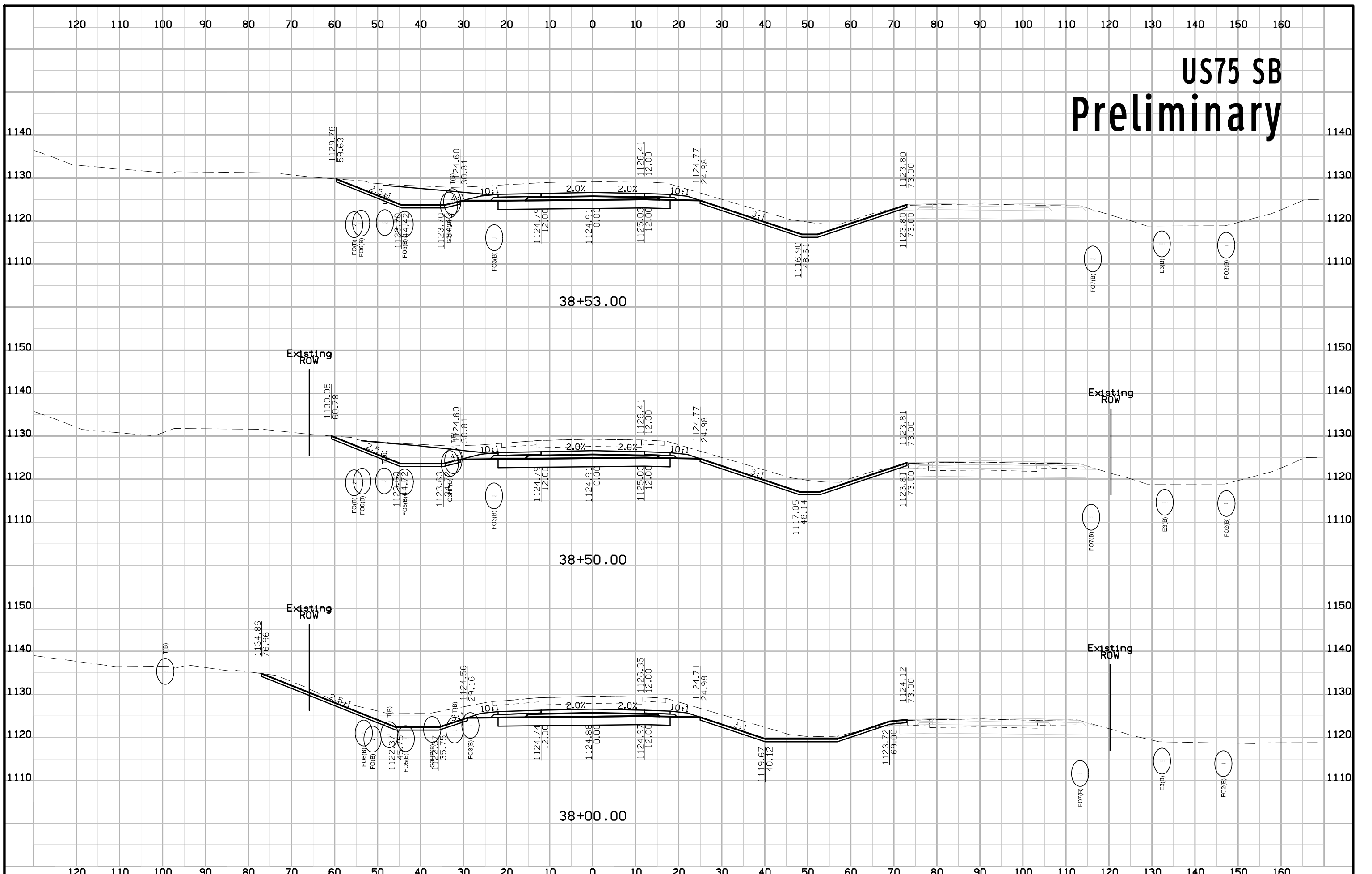
# US75 SB Preliminary



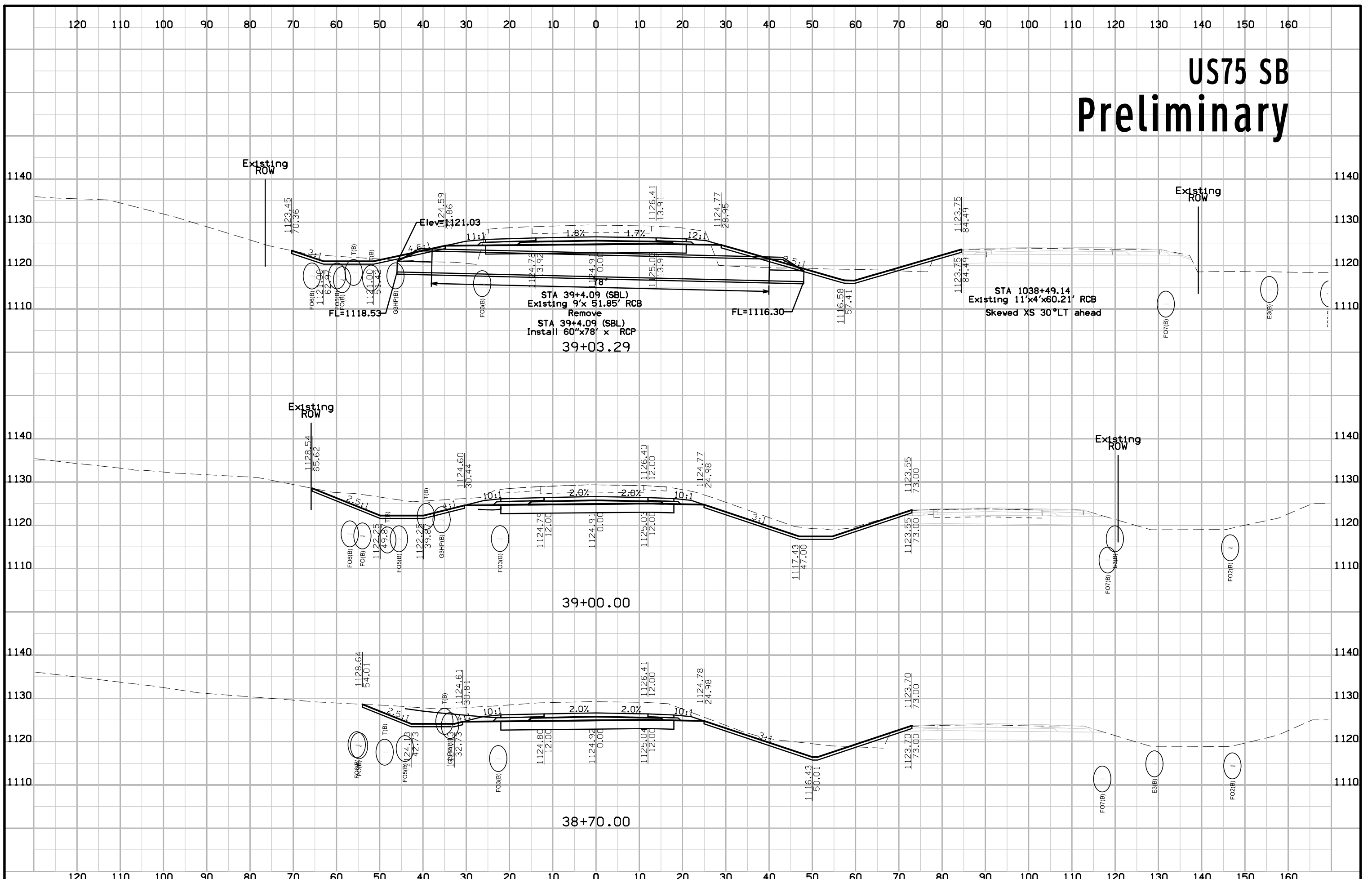
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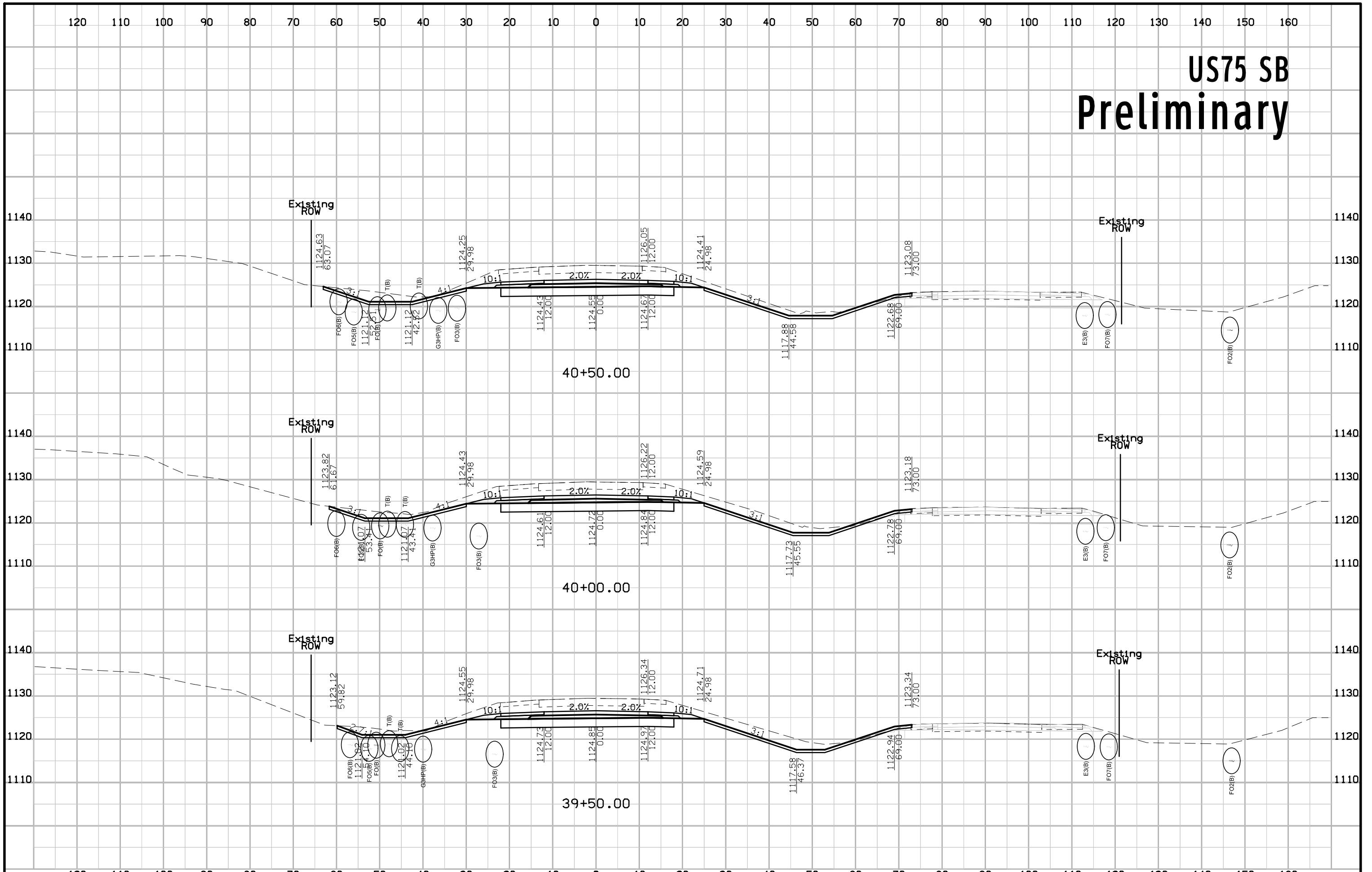
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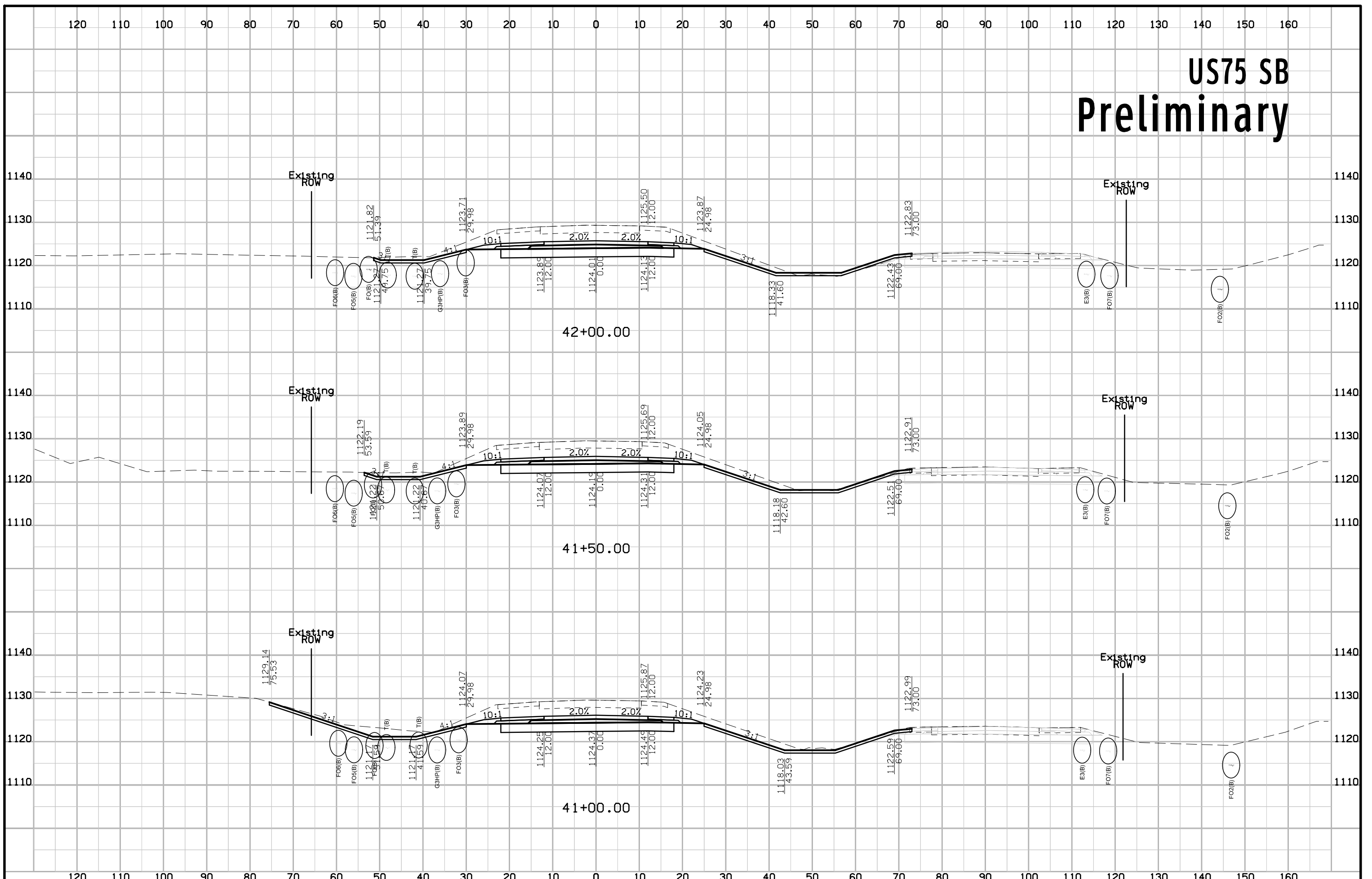
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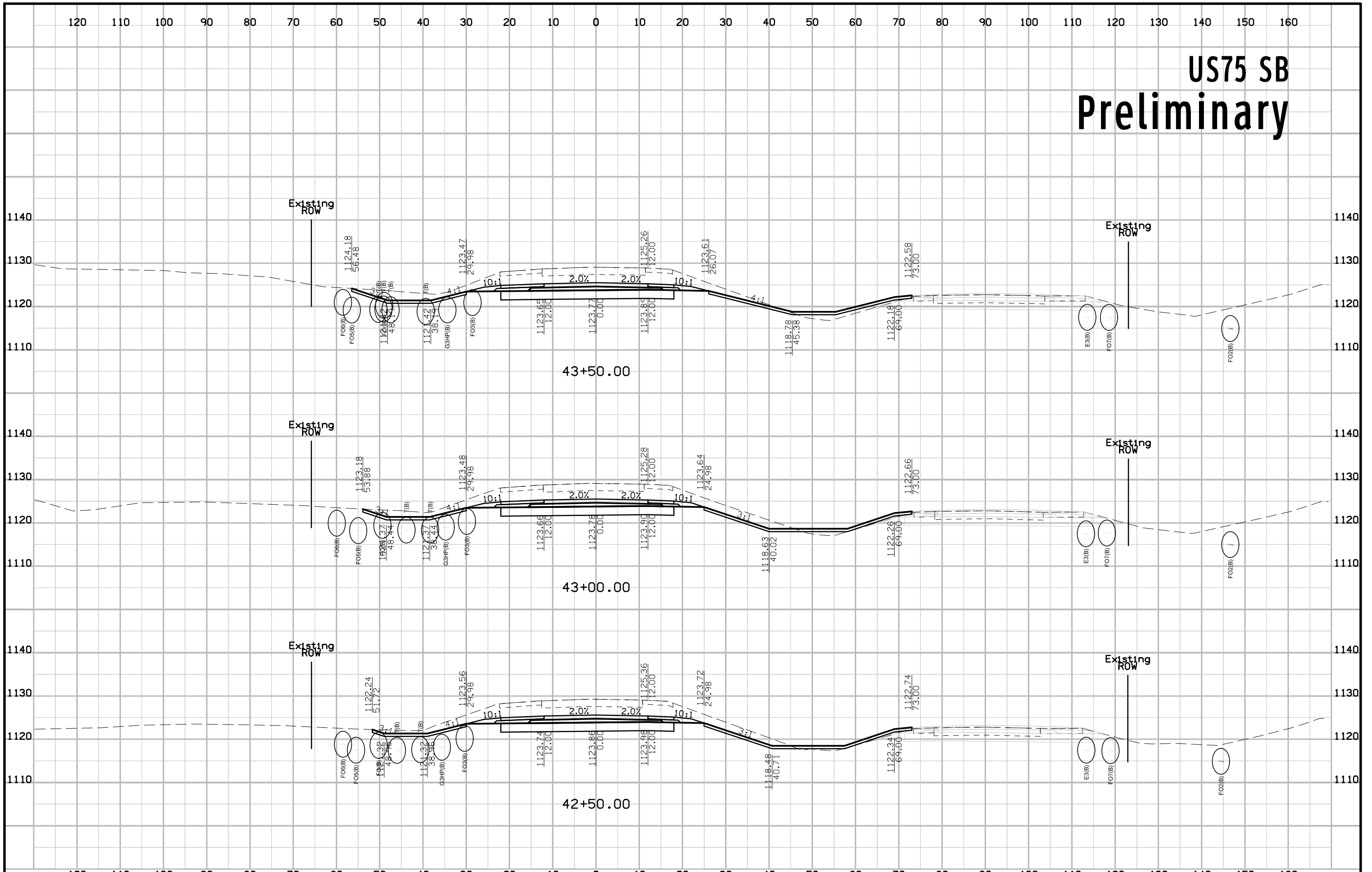


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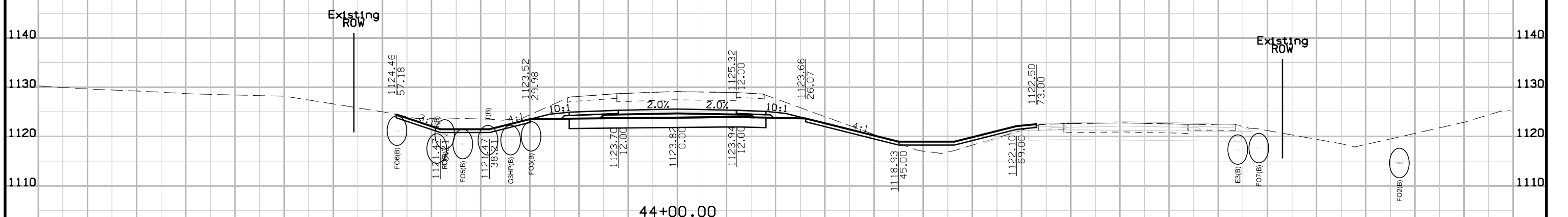
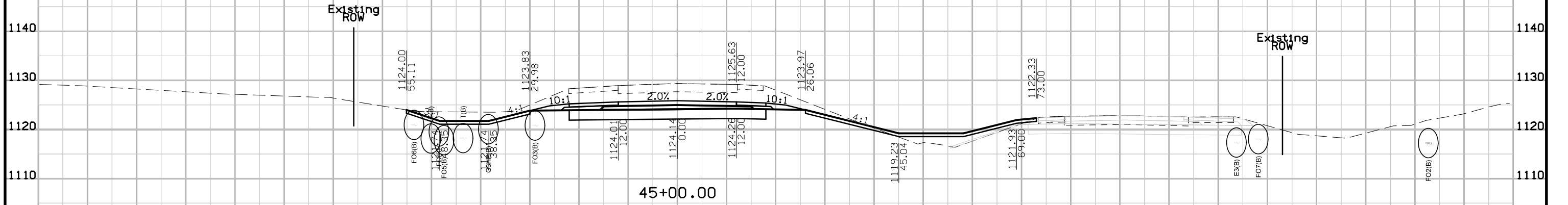




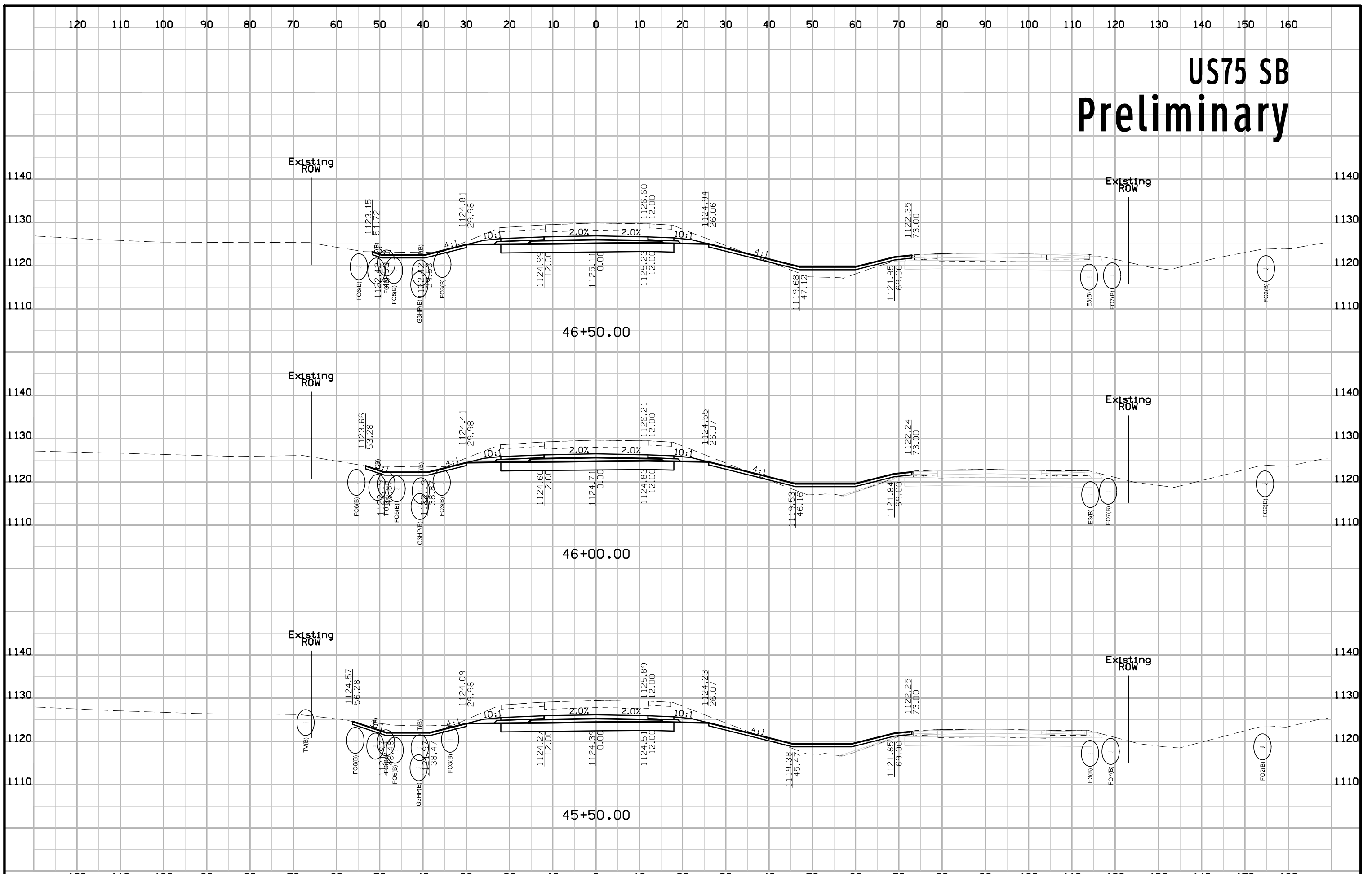
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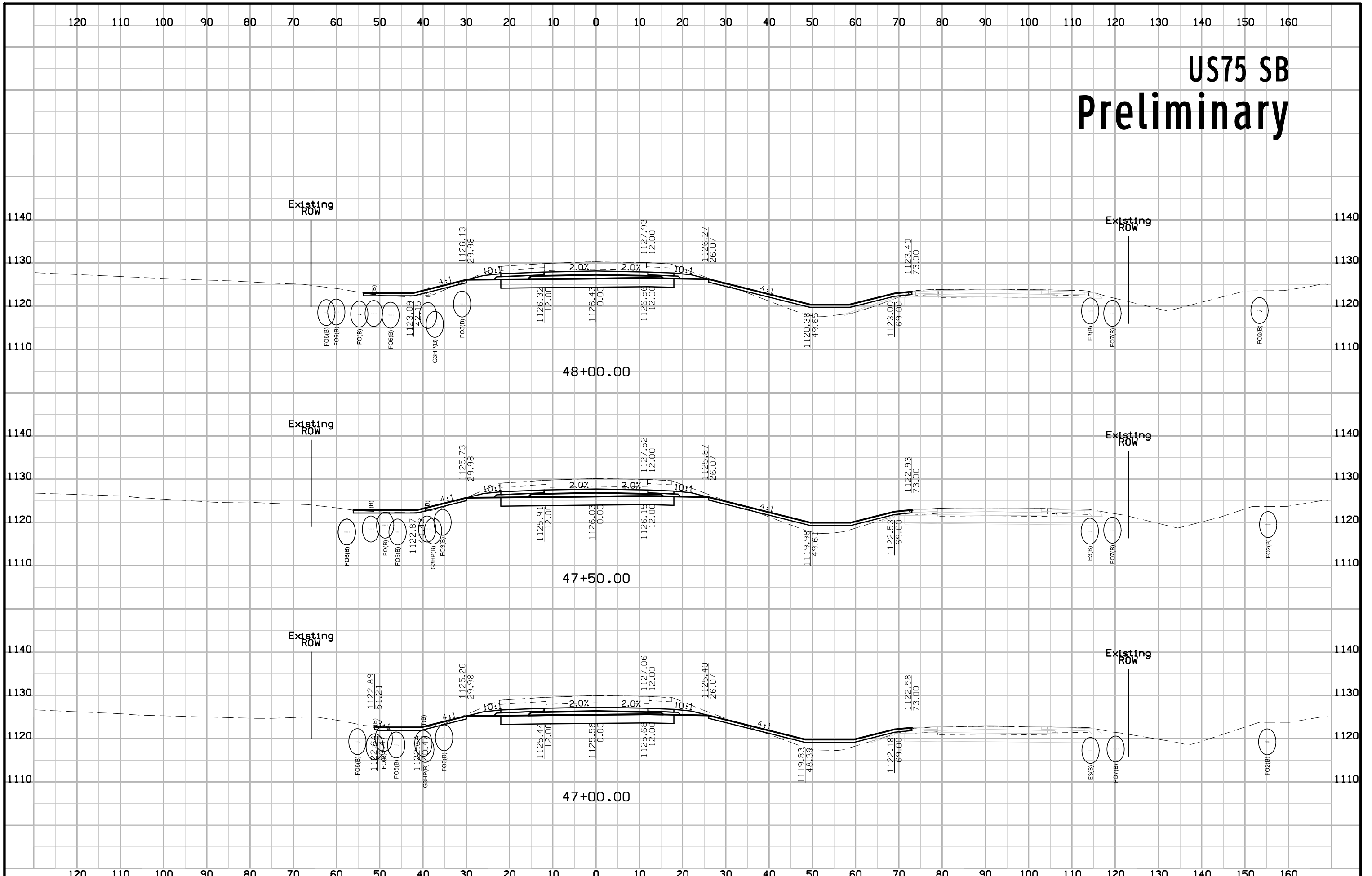
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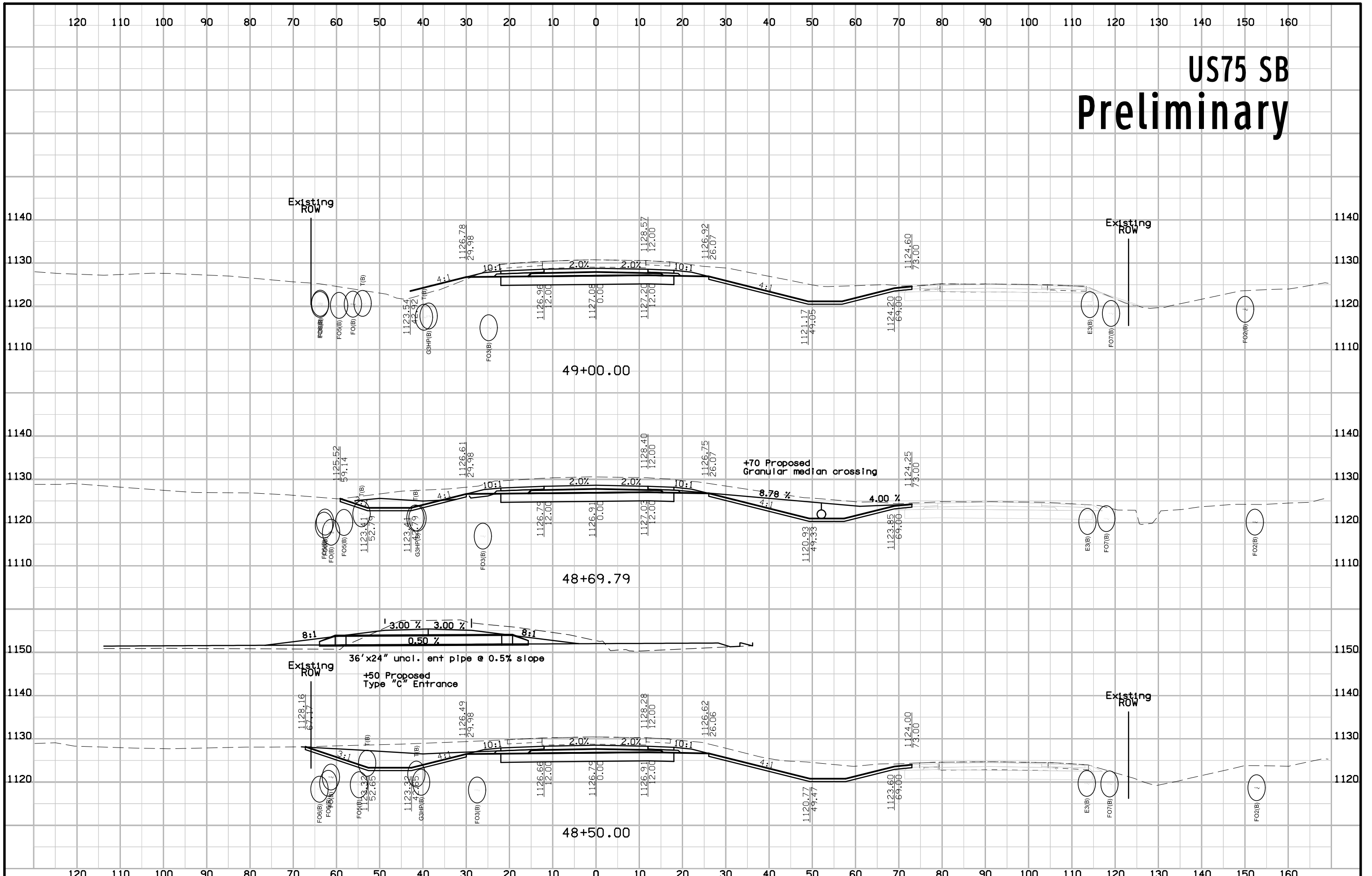
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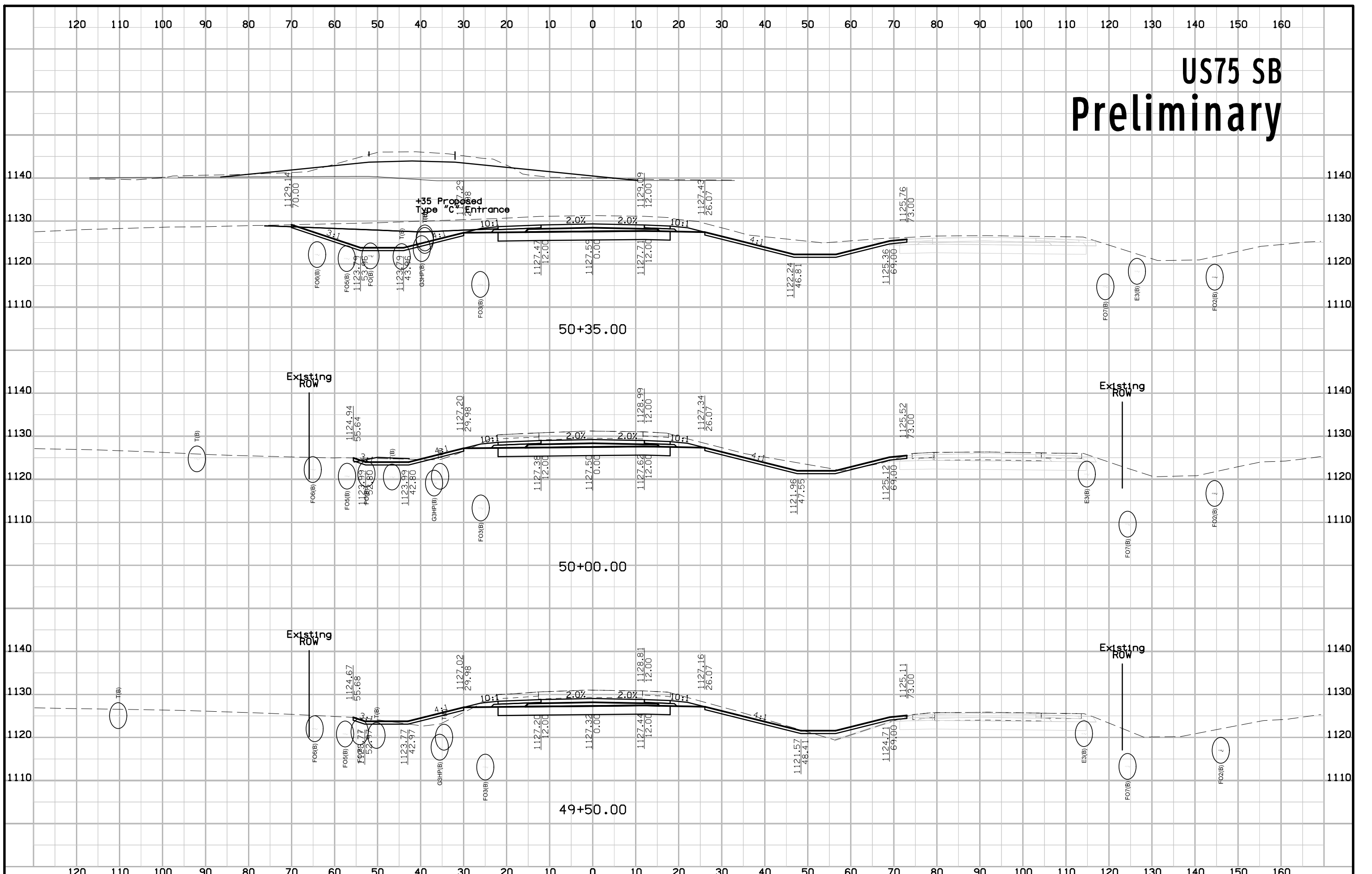
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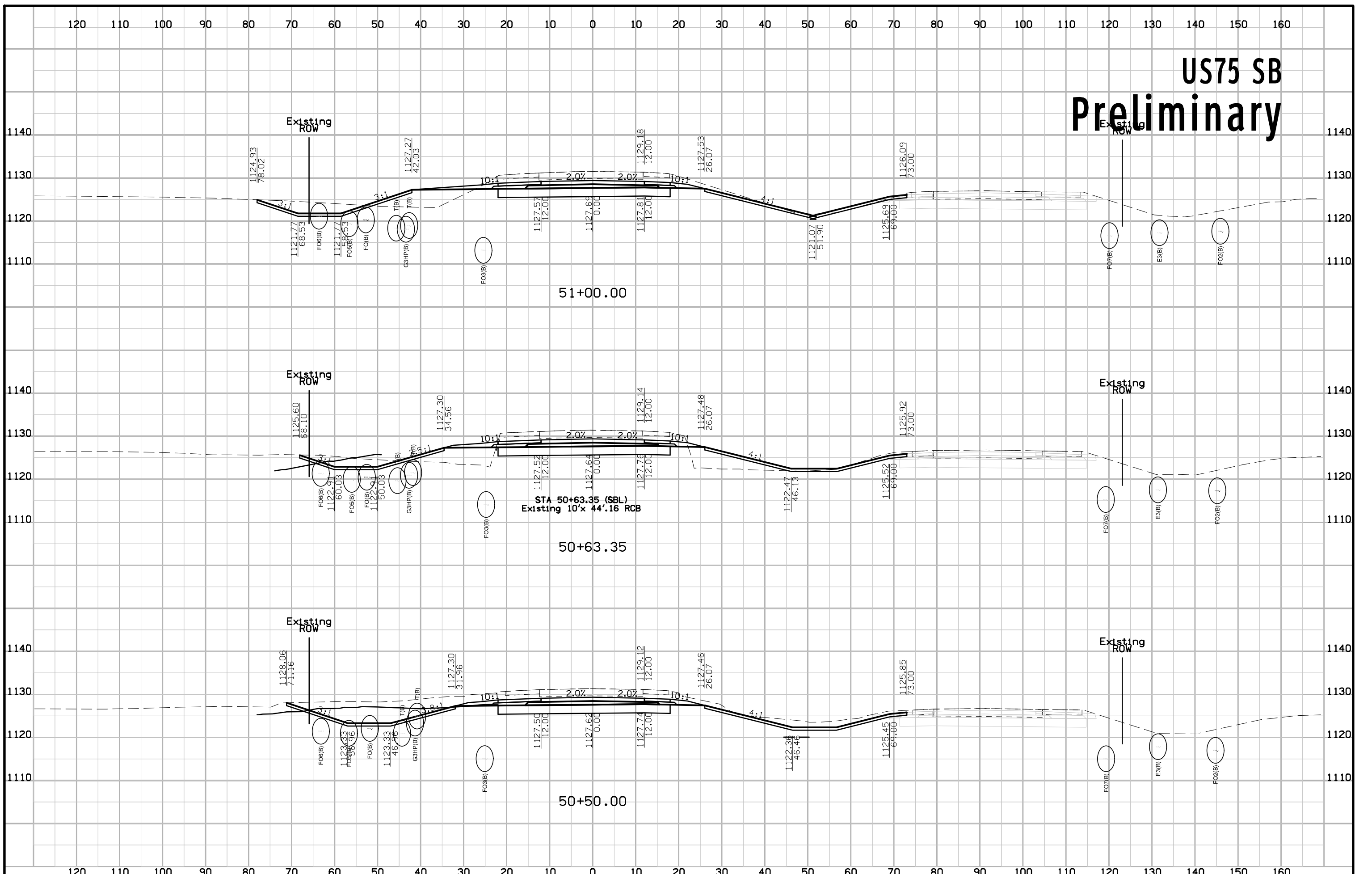
# US75 SB Preliminary



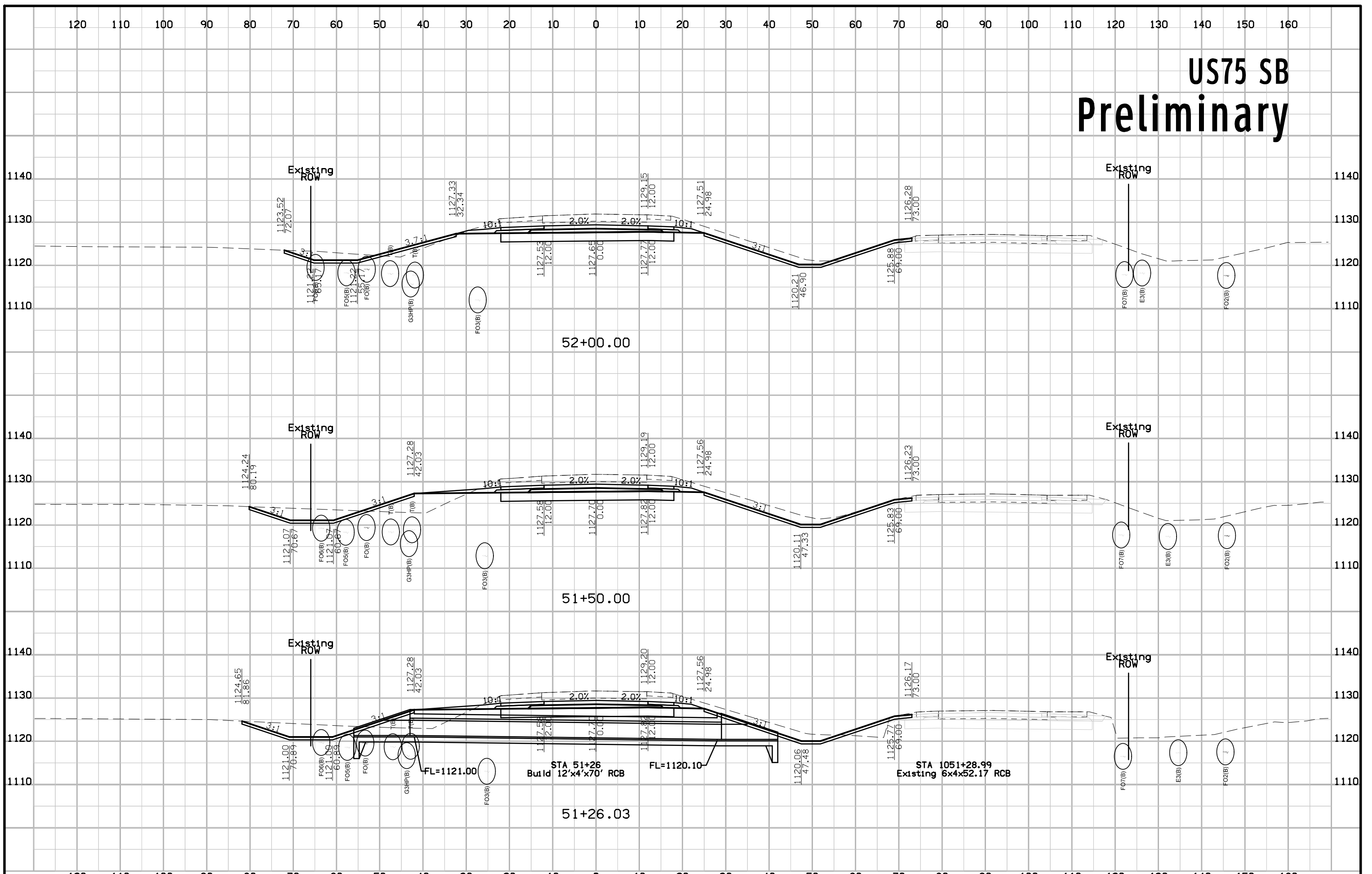
# US75 SB Preliminary



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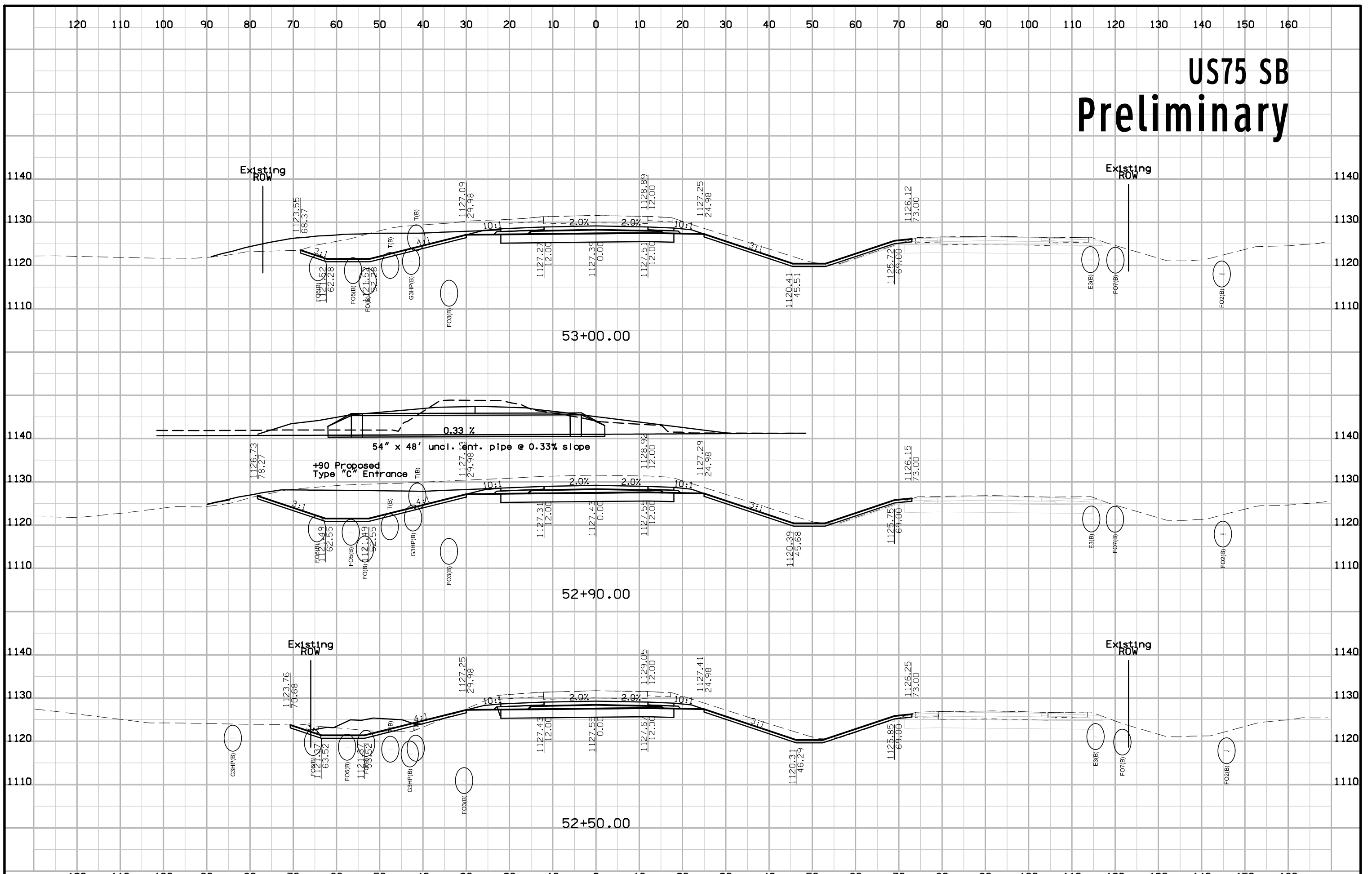


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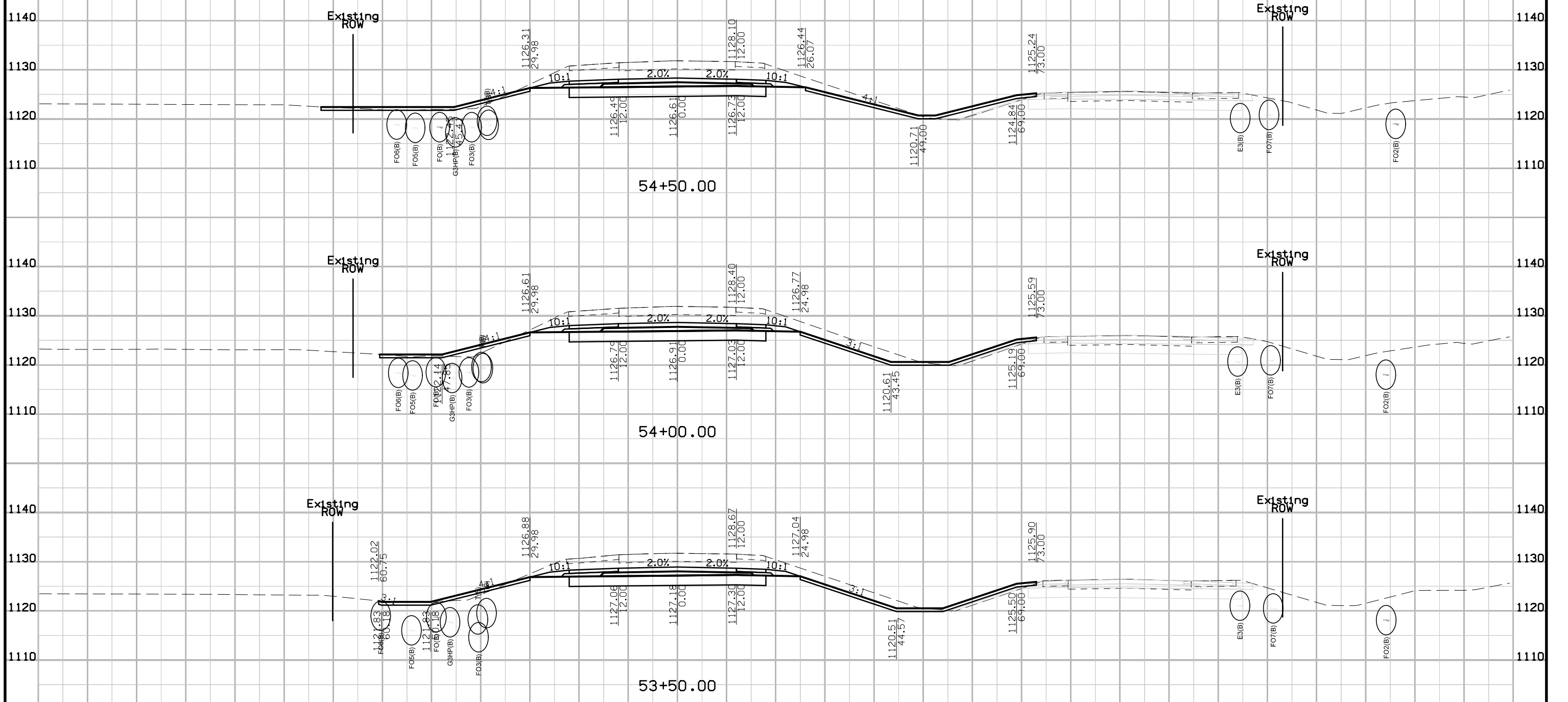




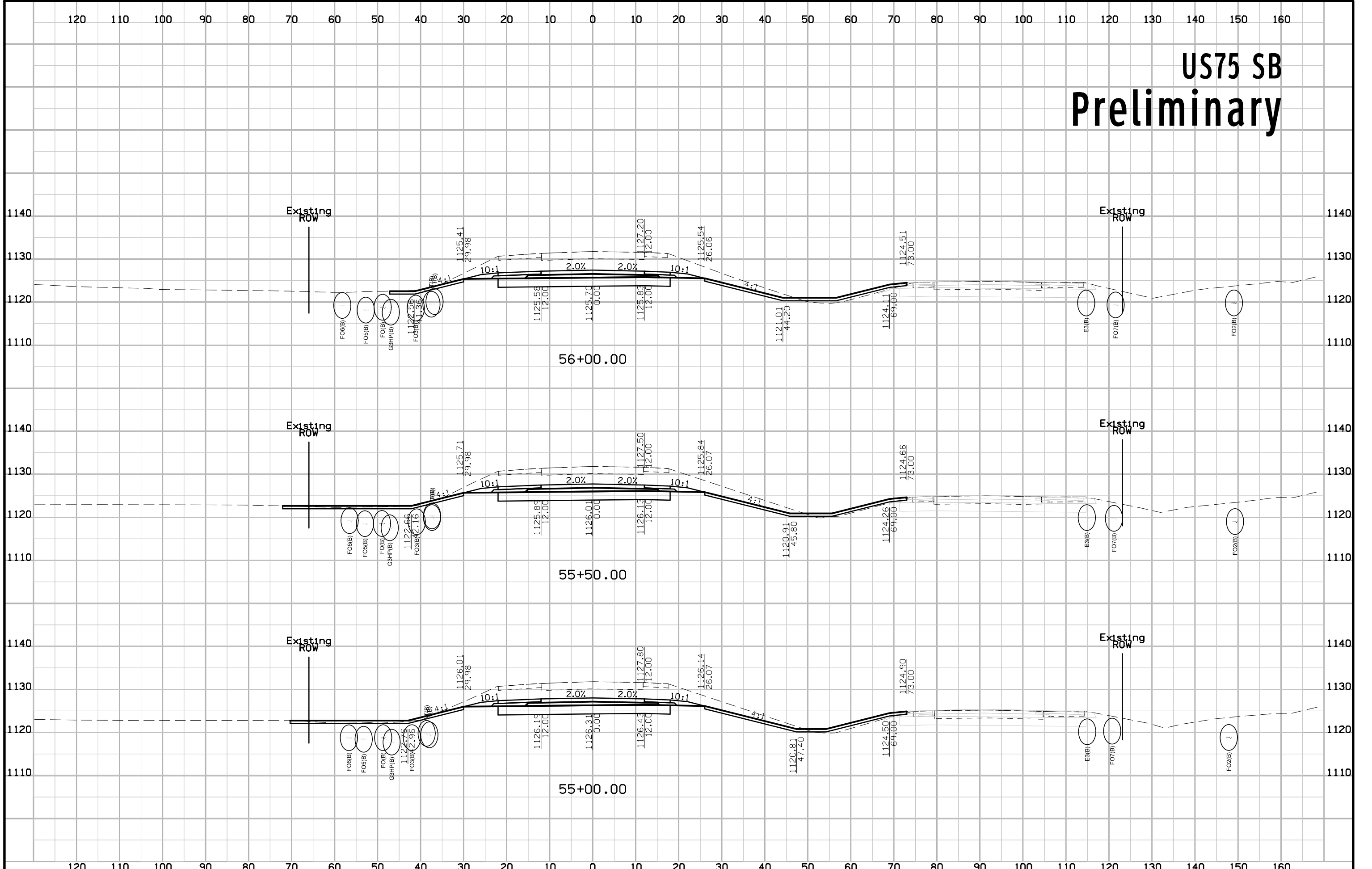
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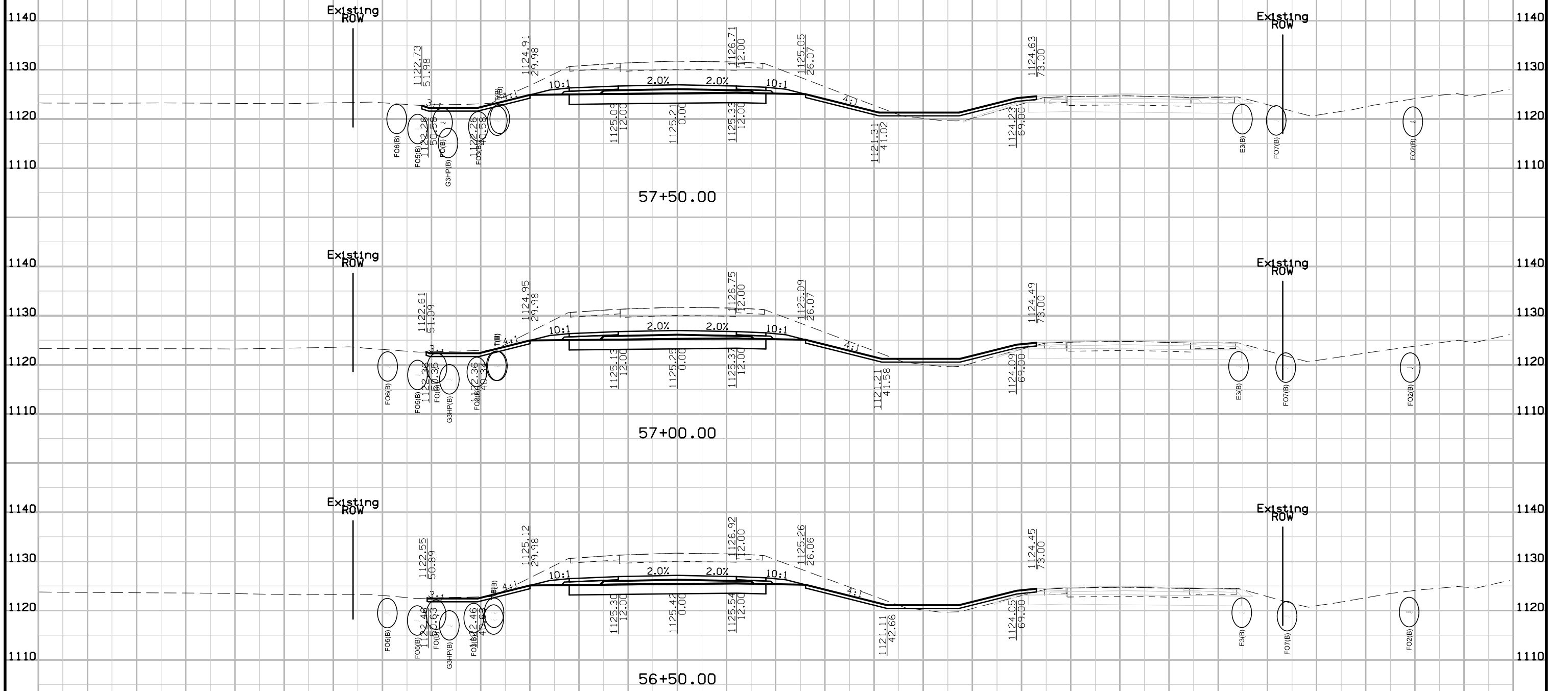
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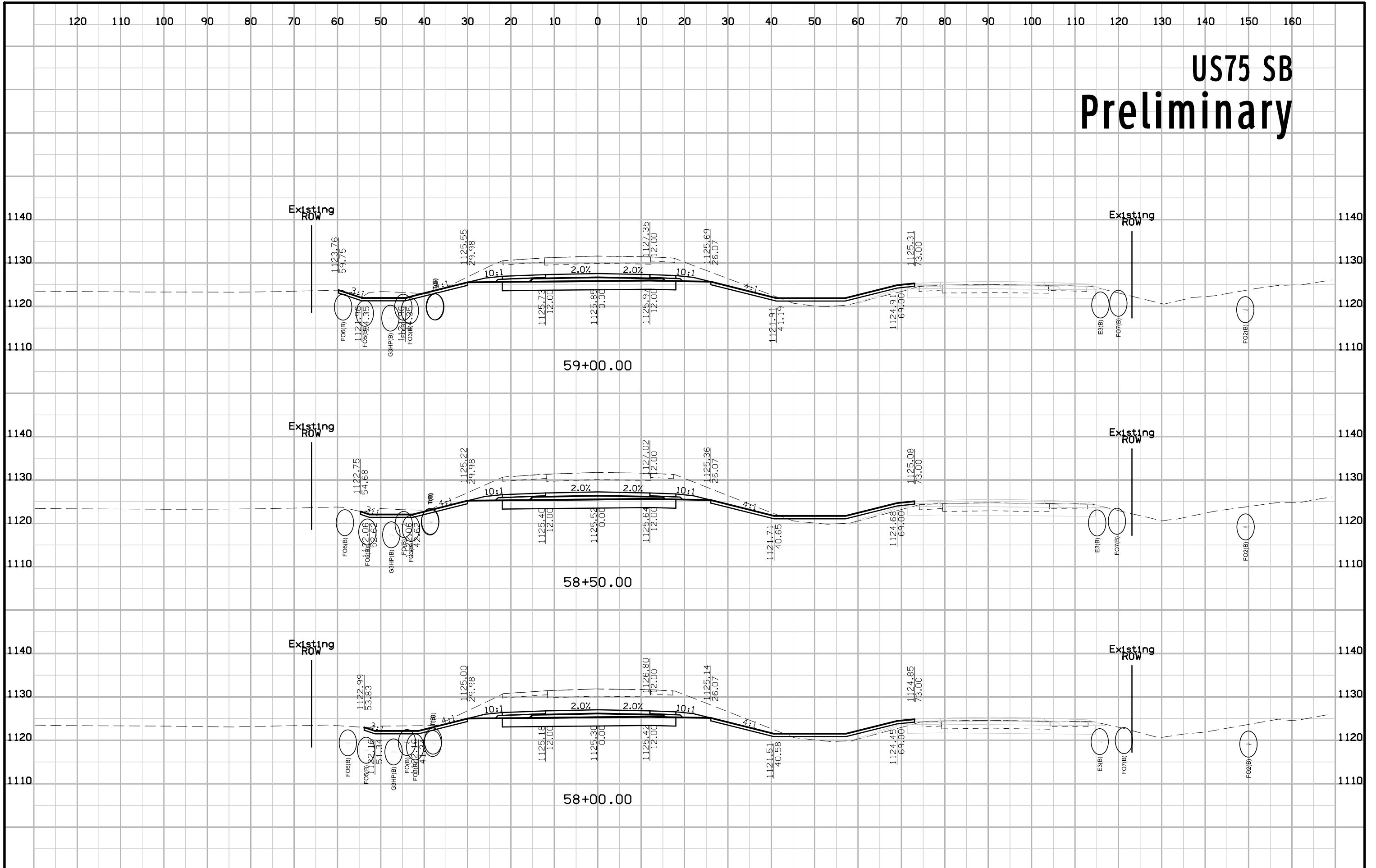
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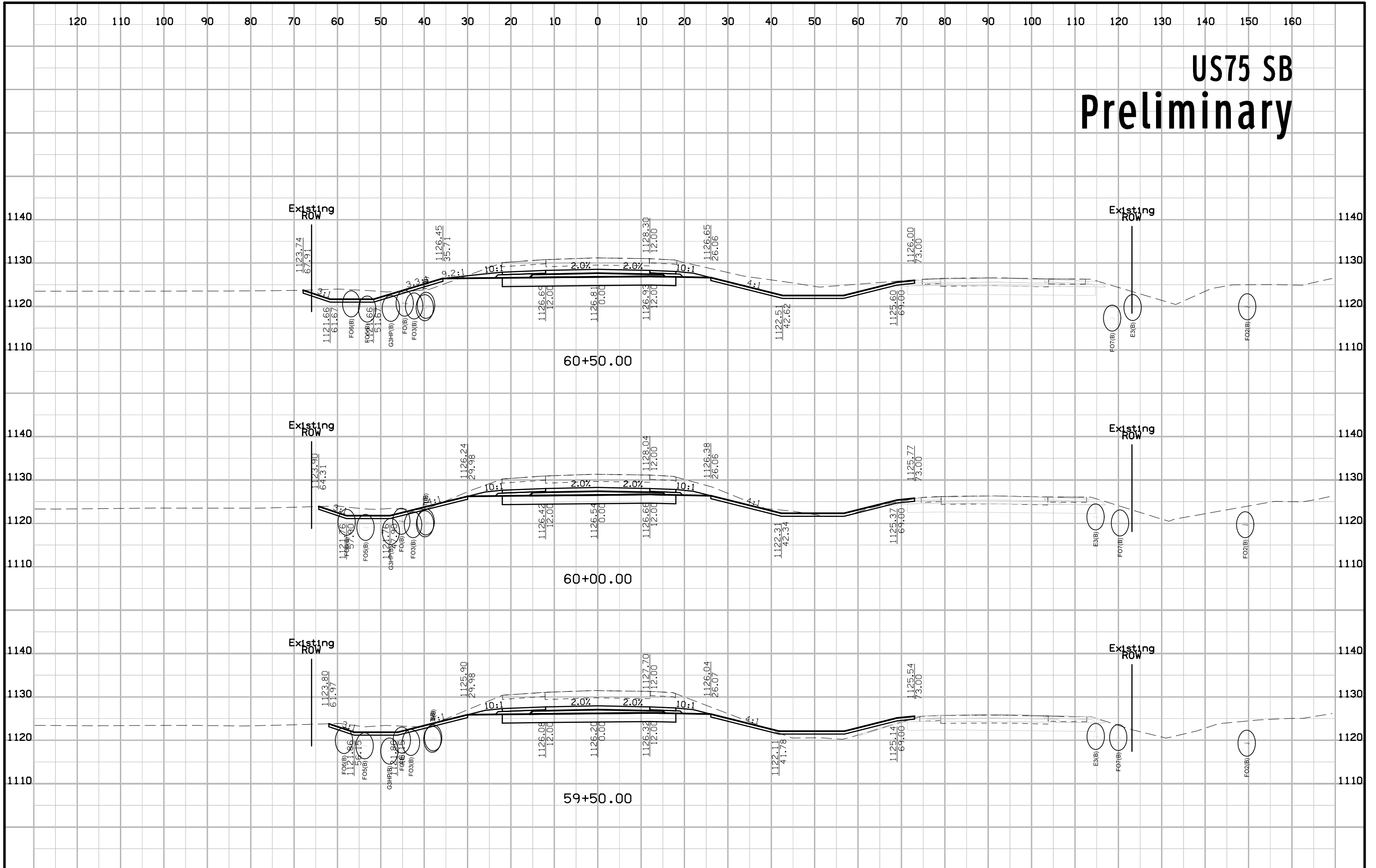
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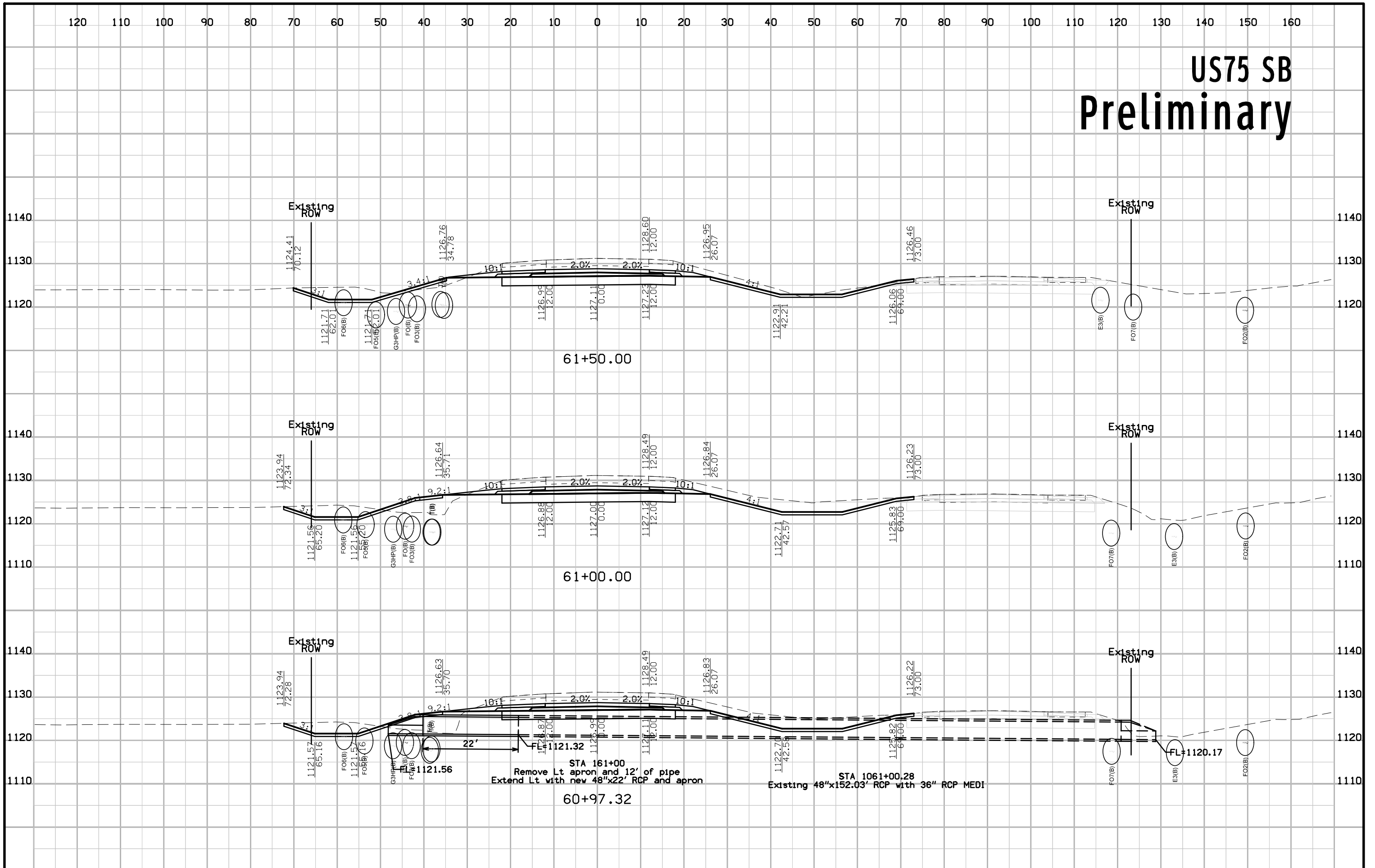
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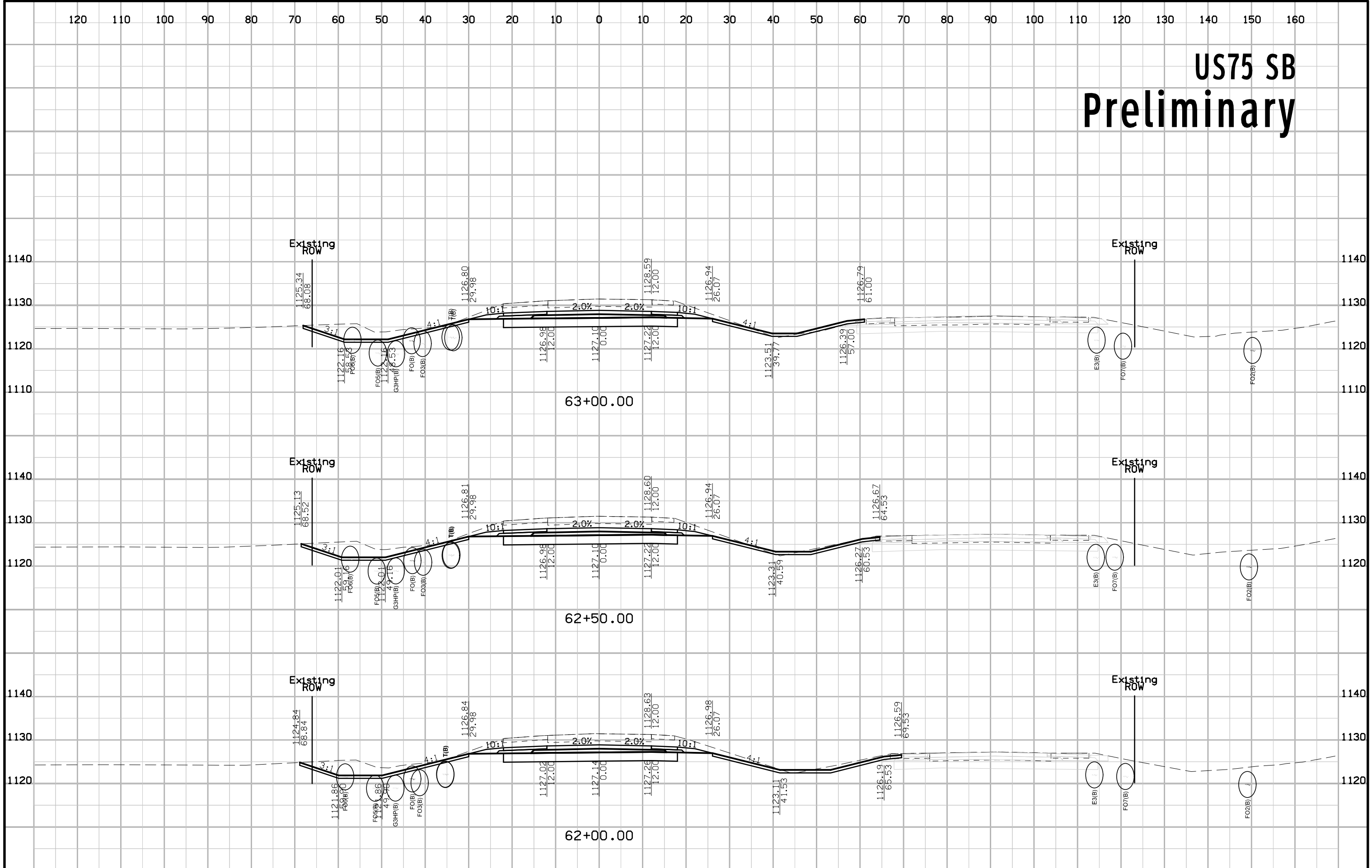
# US75 SB Preliminary



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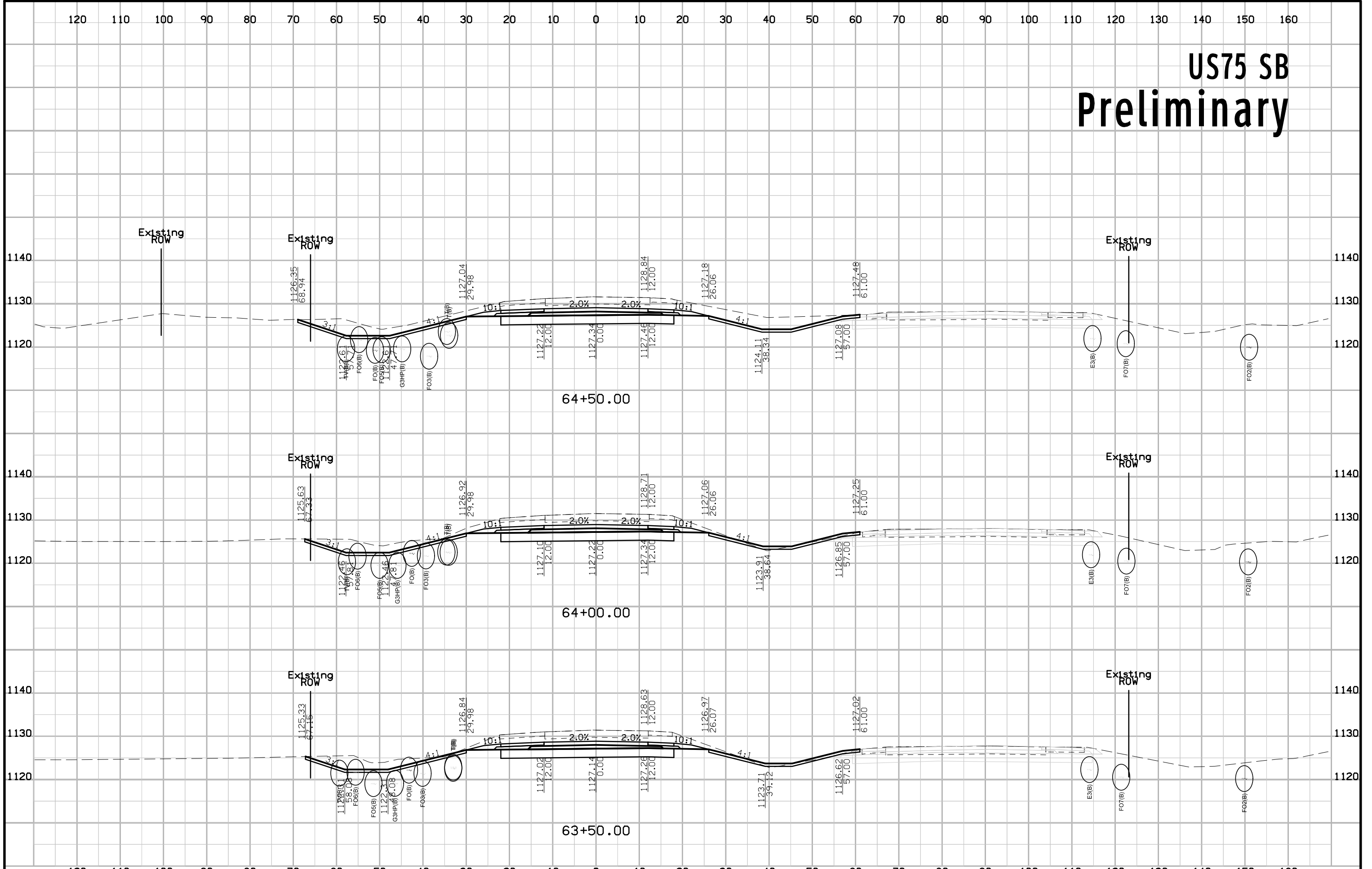


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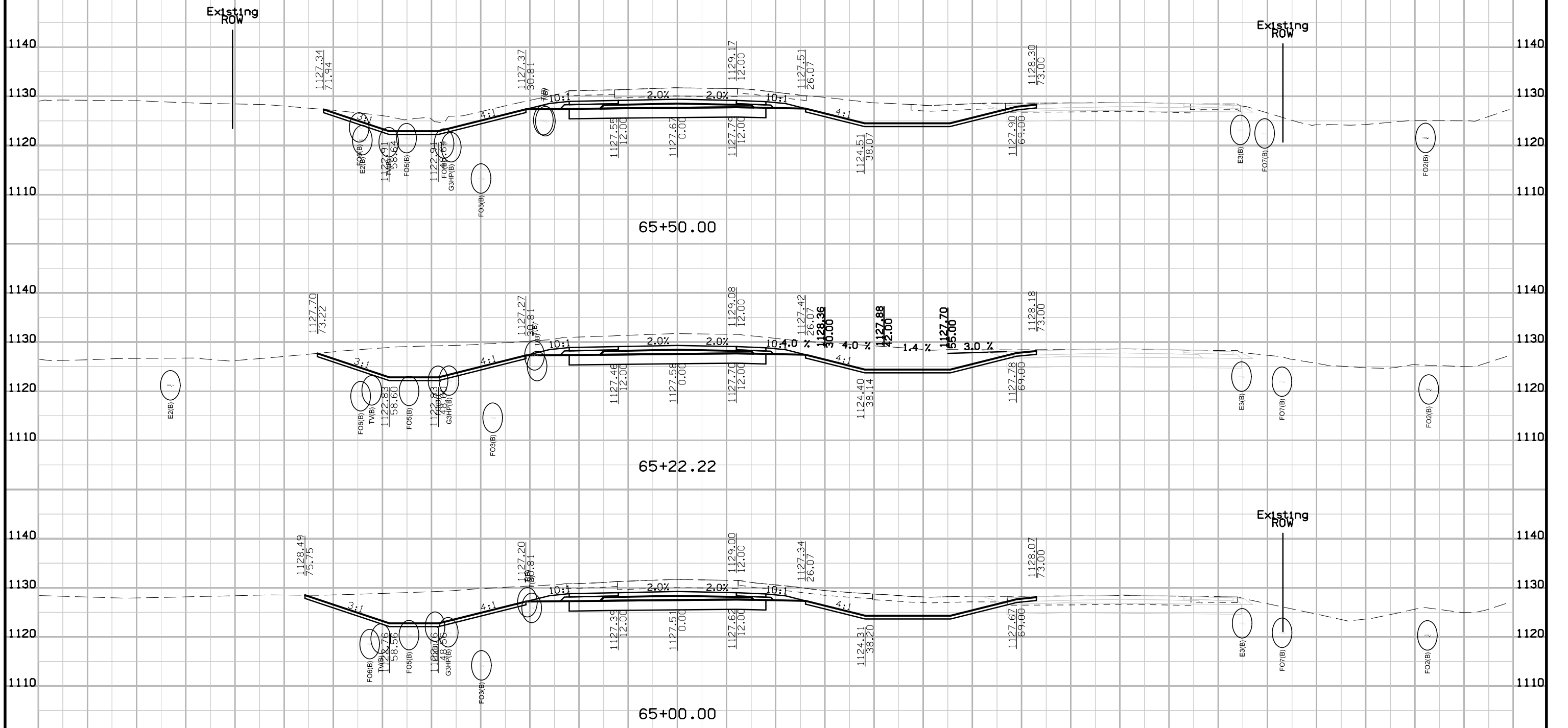




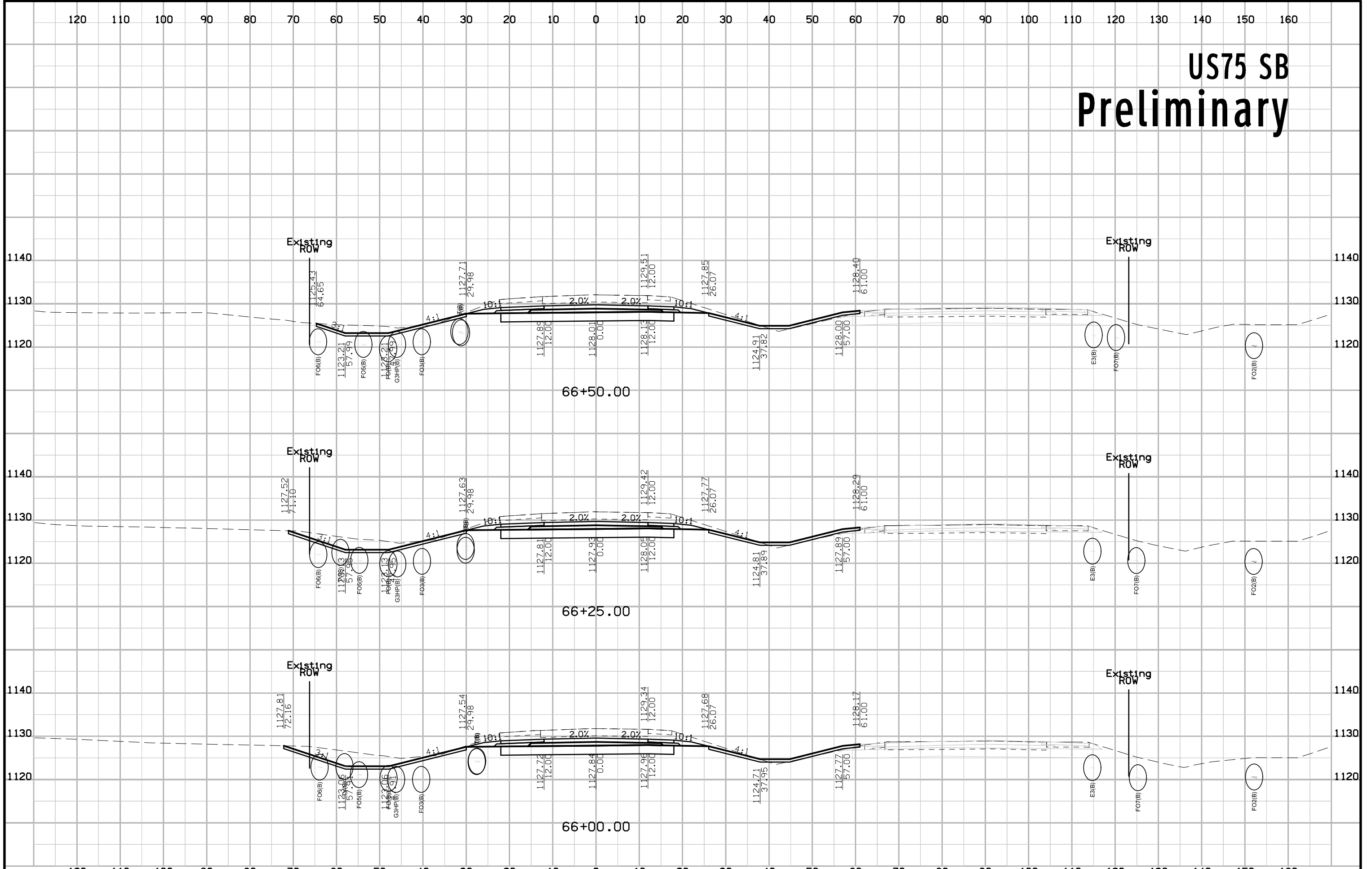
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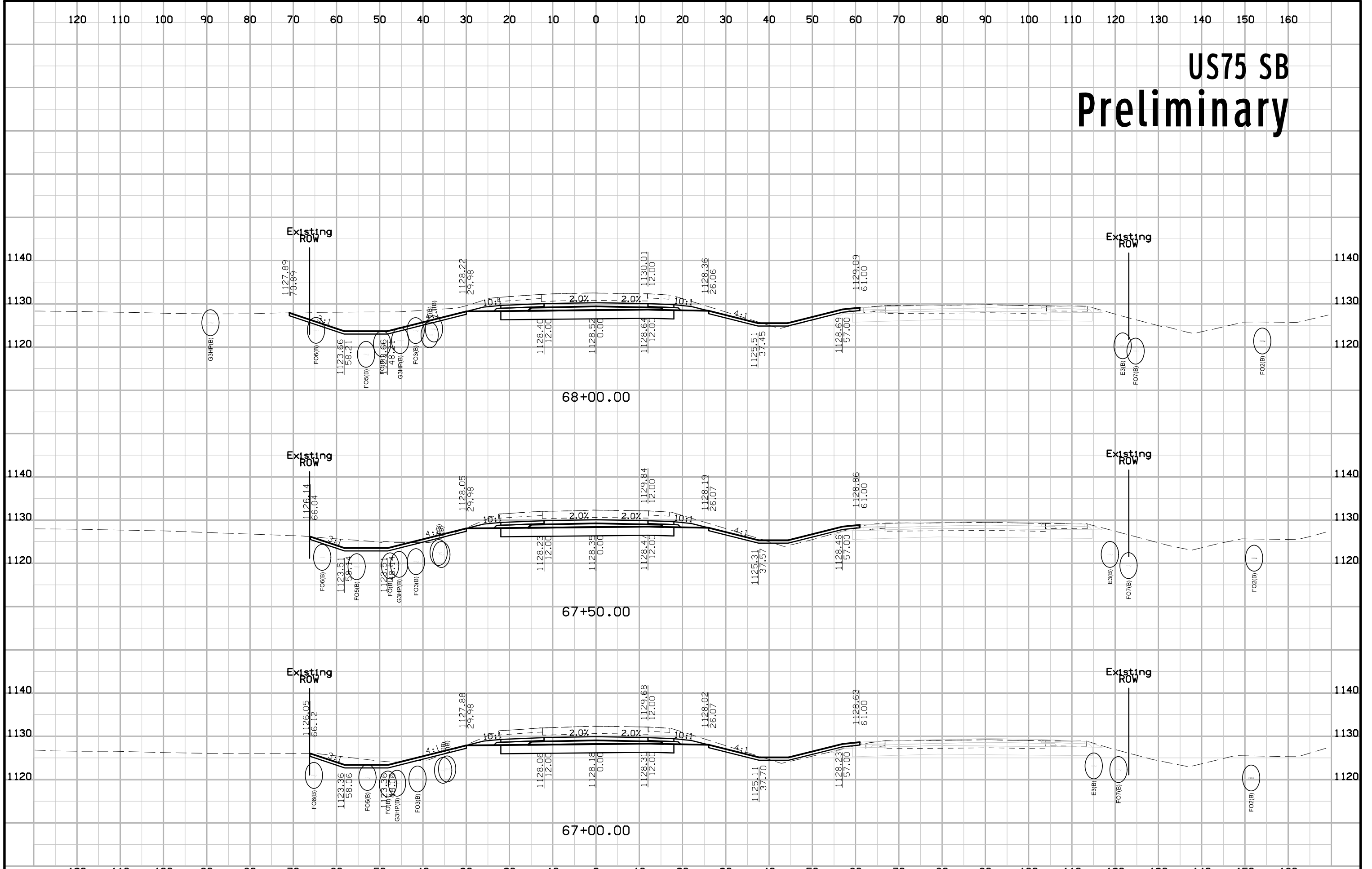
# US75 SB Preliminary



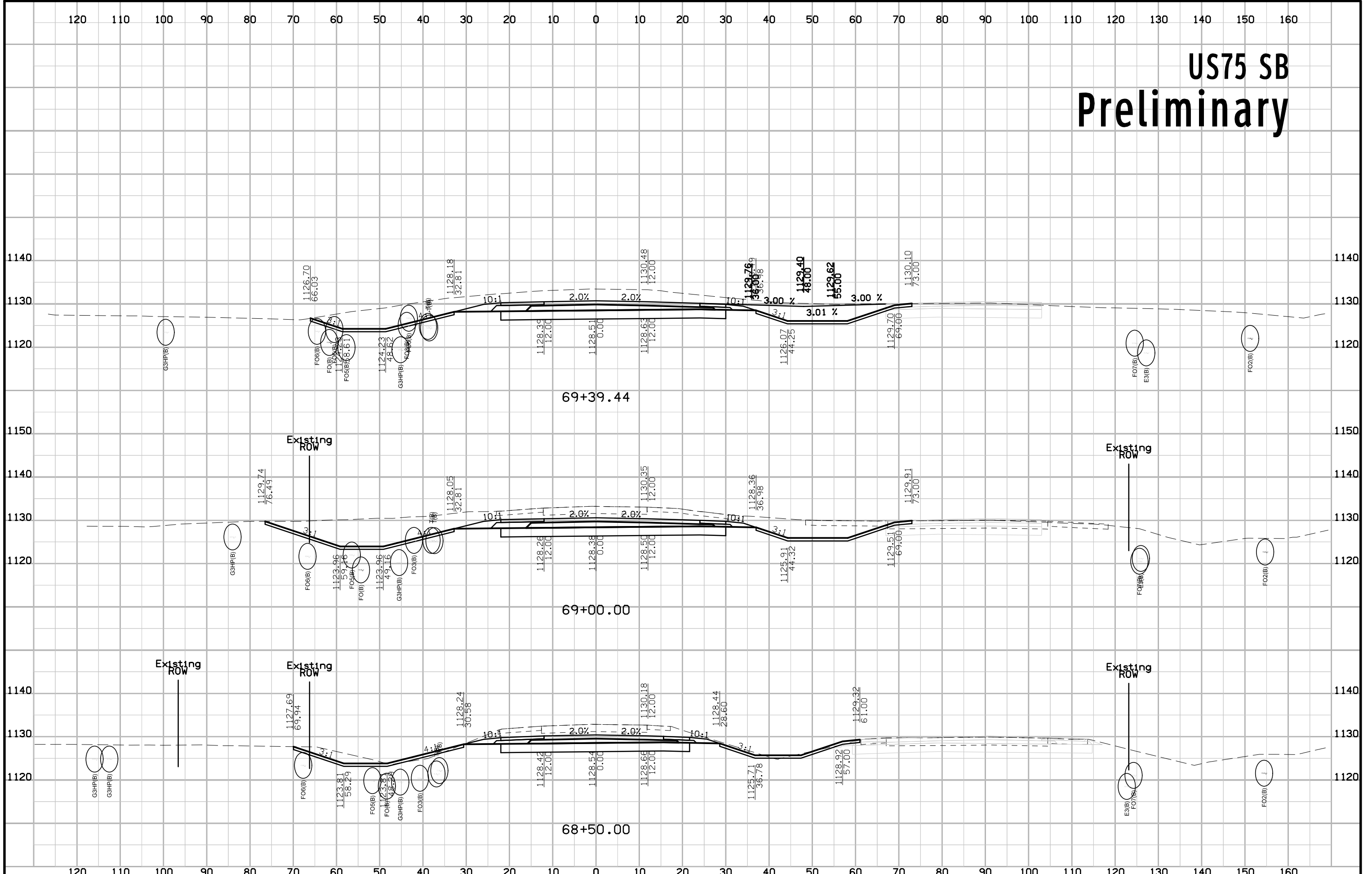
# US75 SB Preliminary



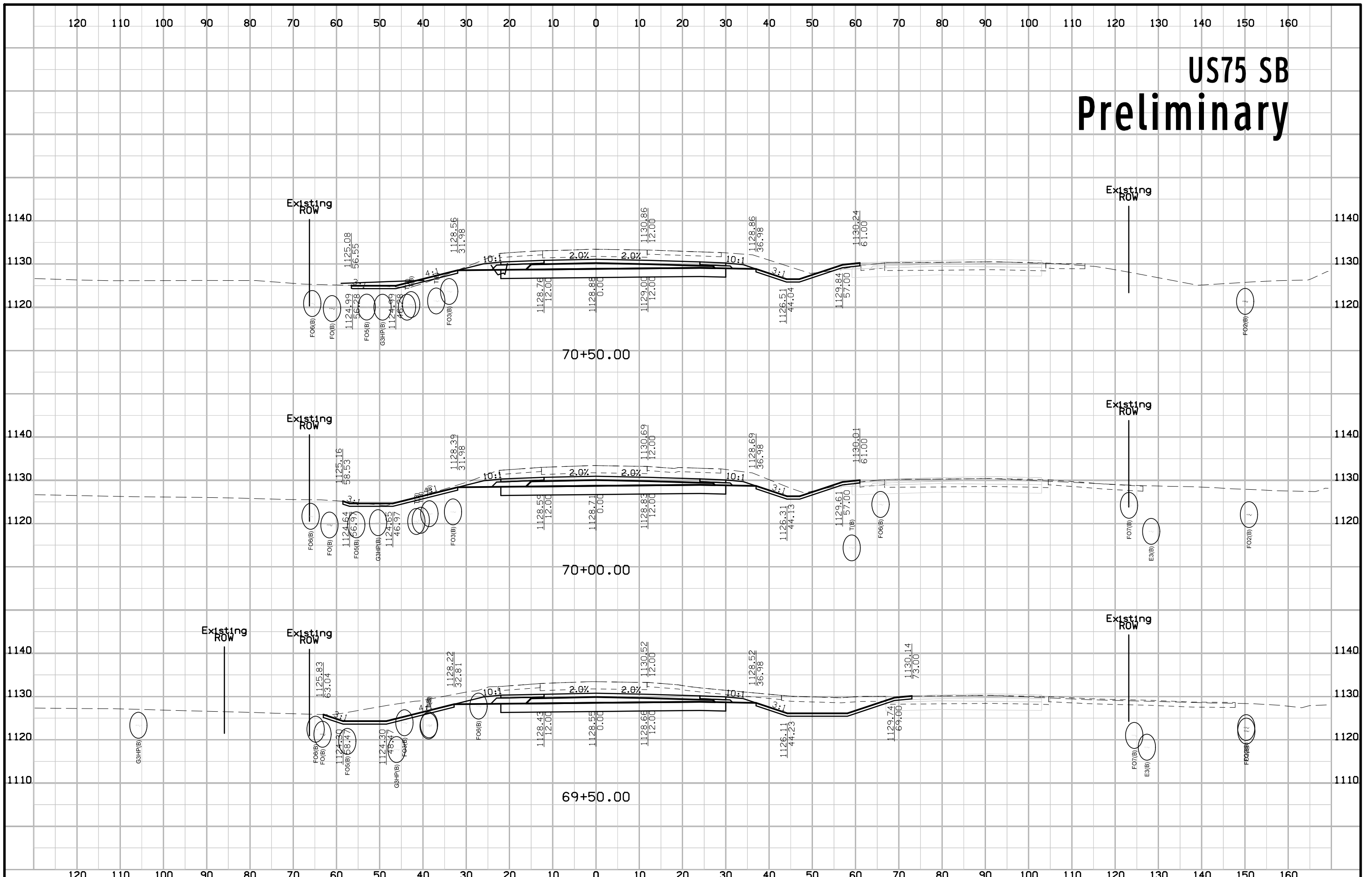
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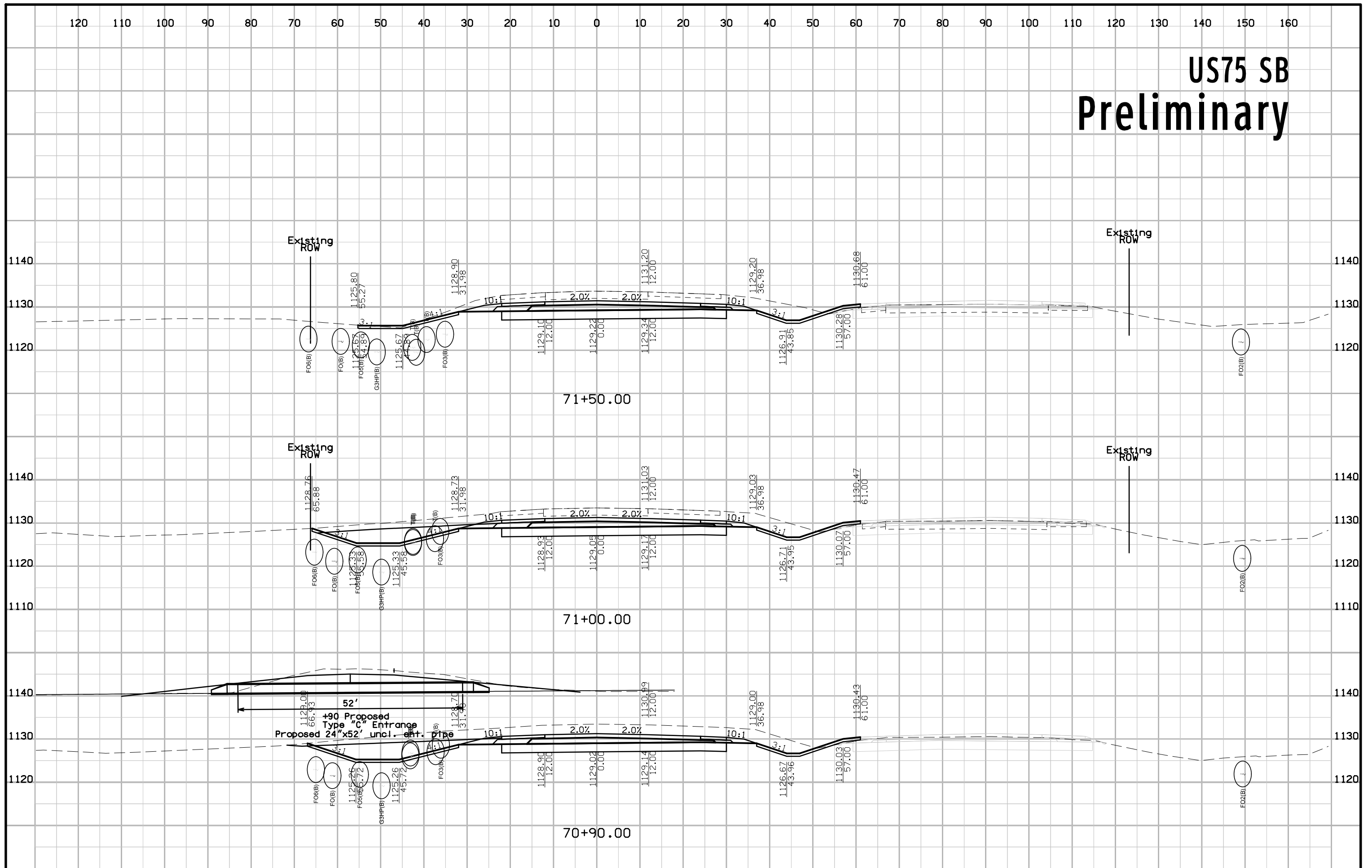
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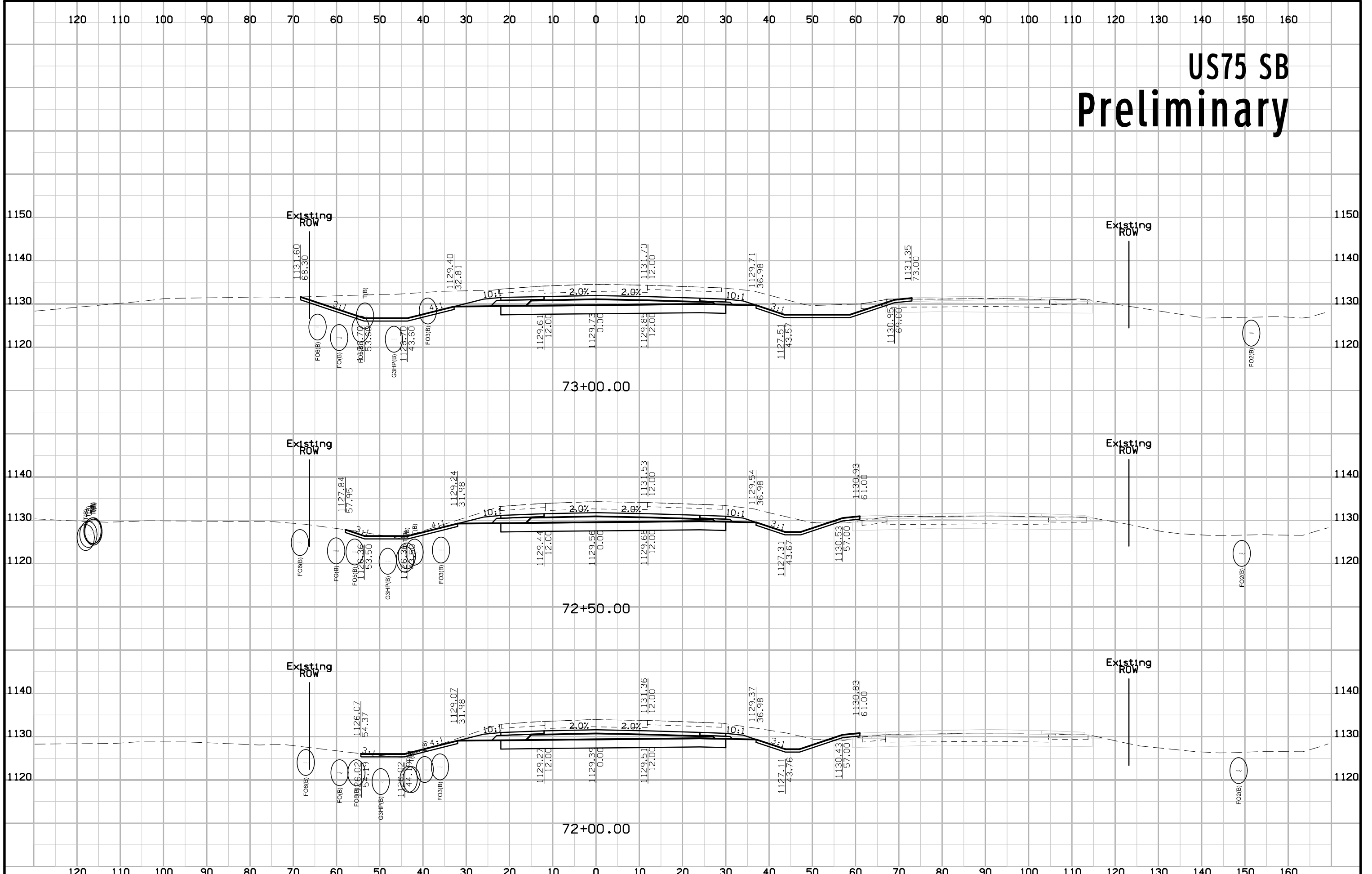
# US75 SB Preliminary



# US75 SB Preliminary

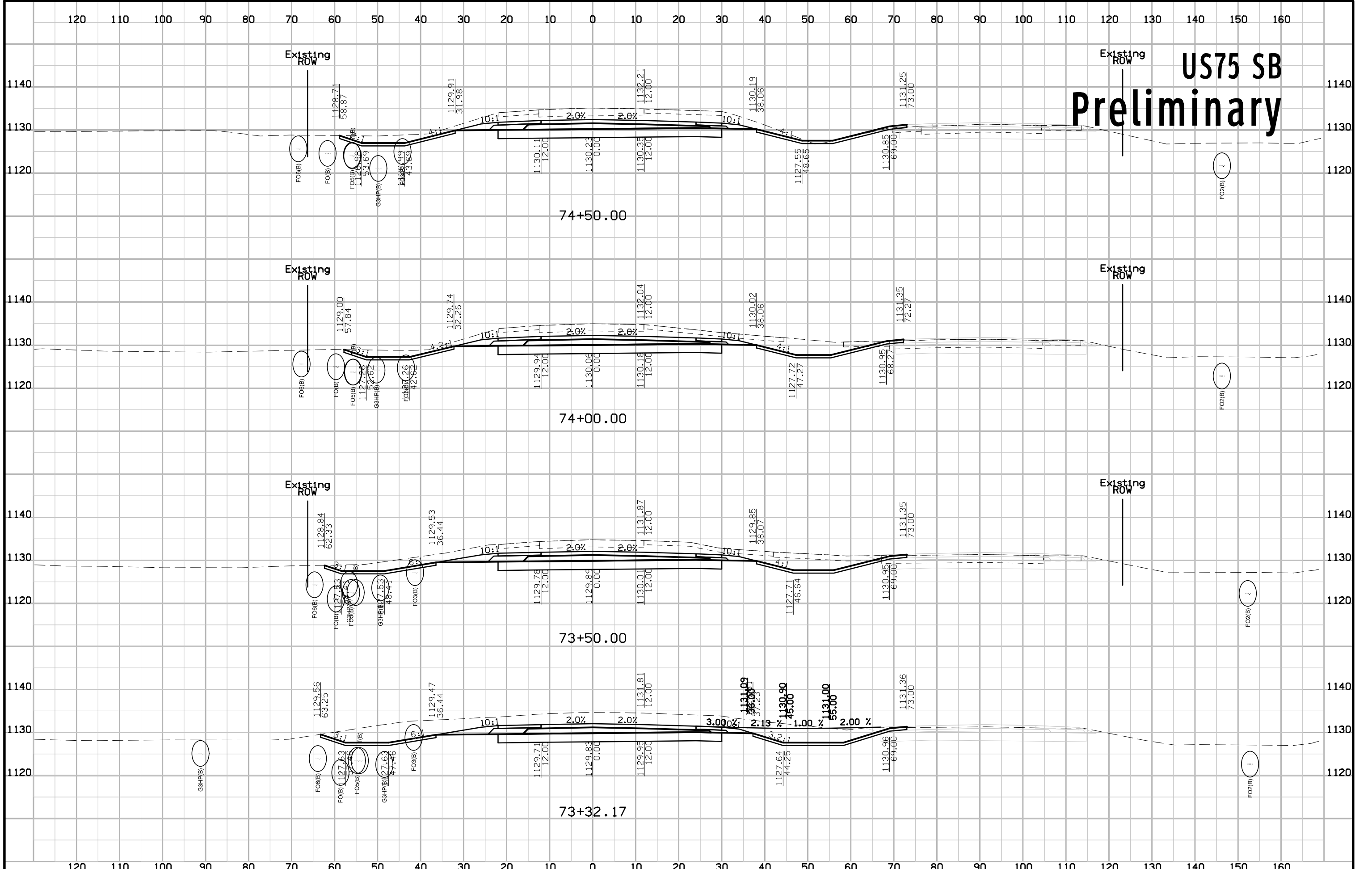


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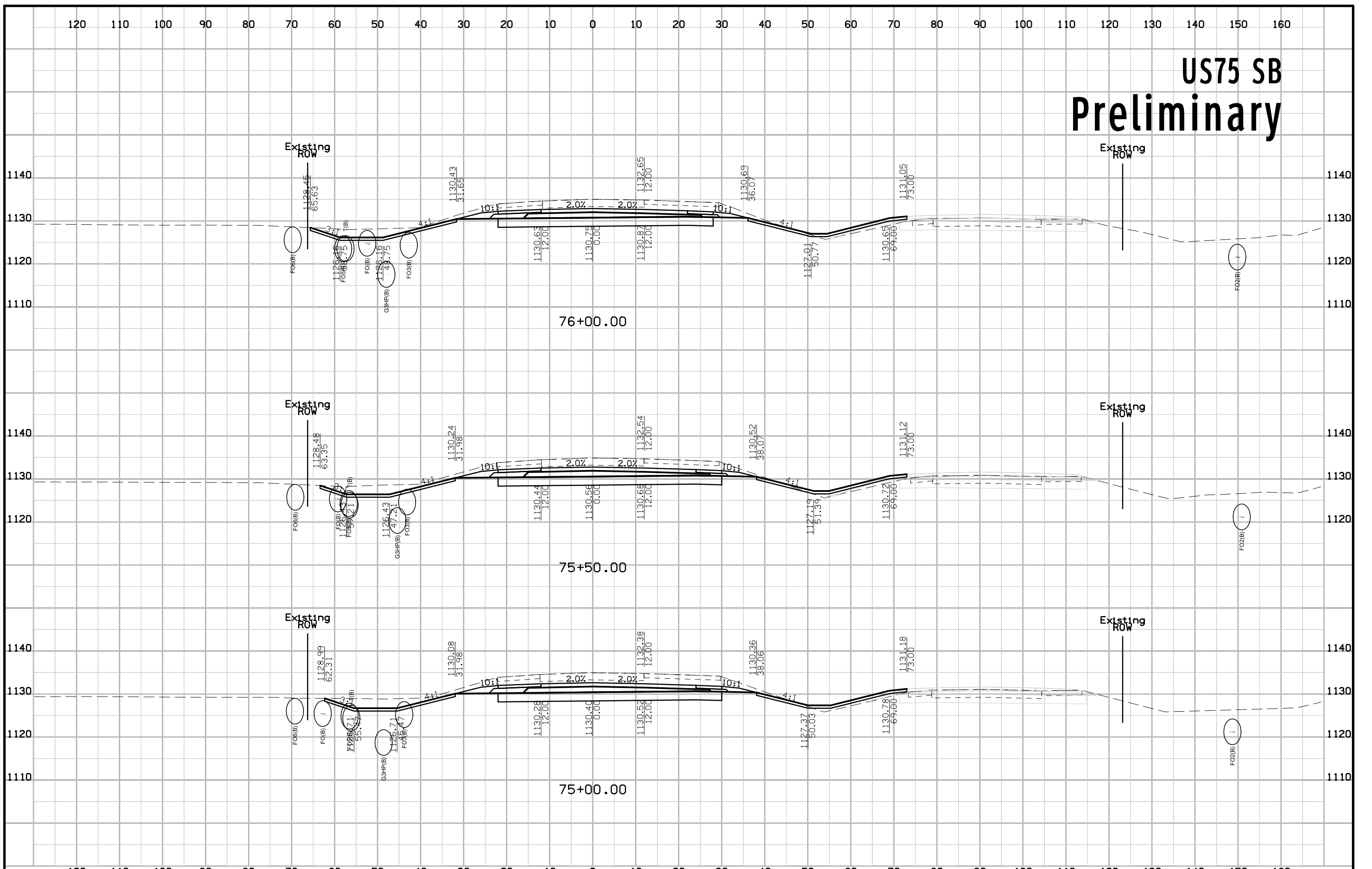




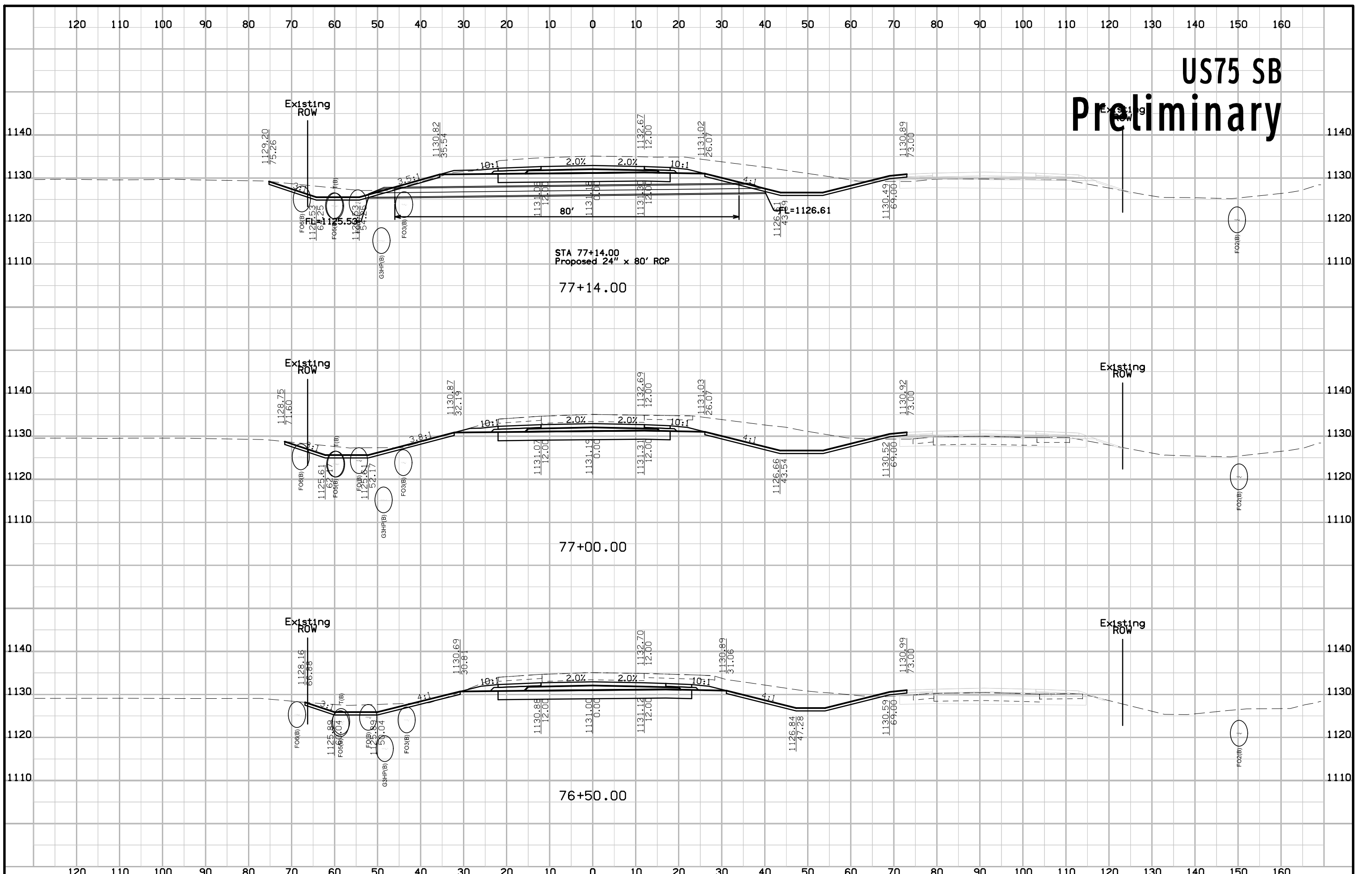
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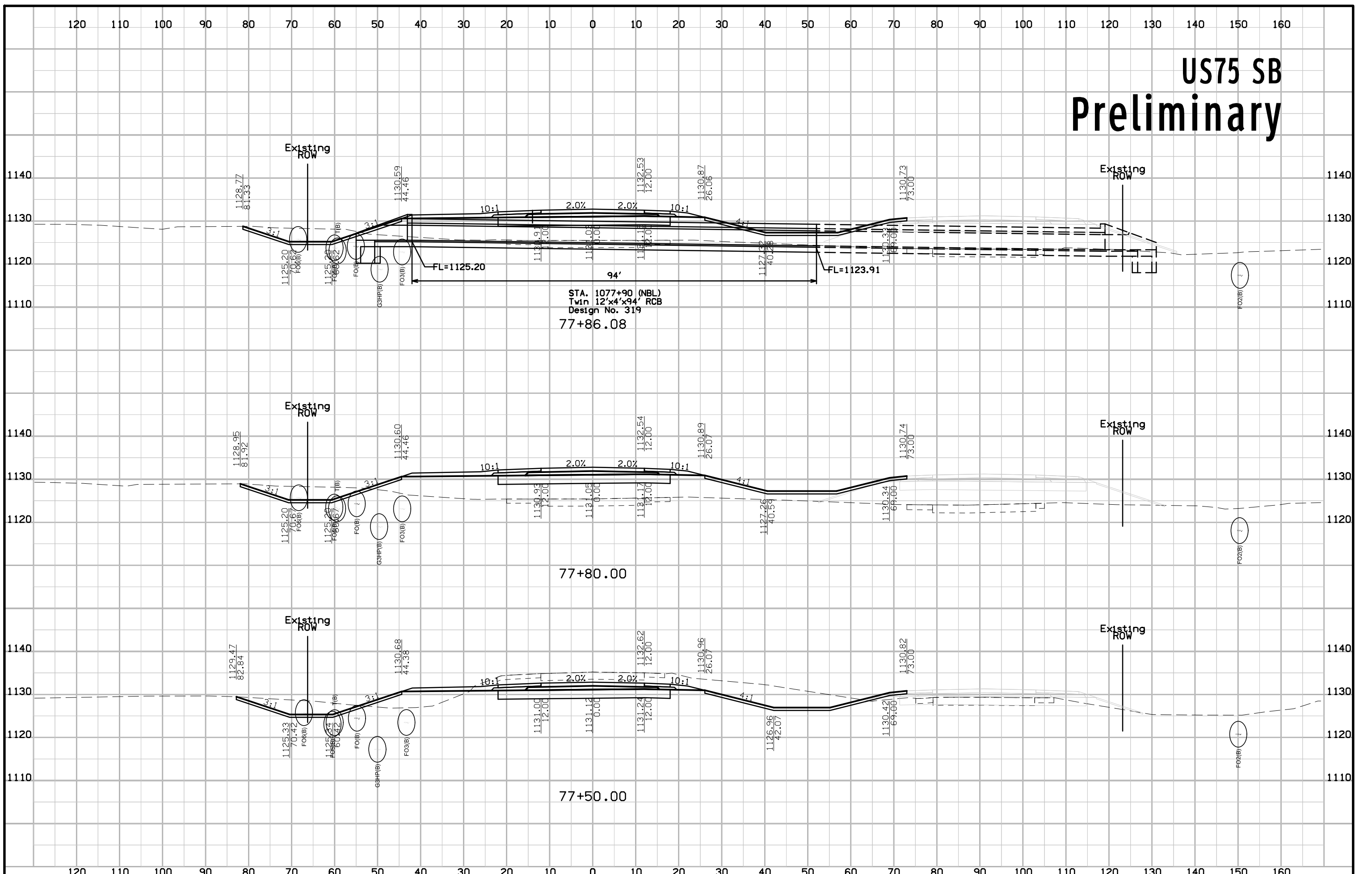
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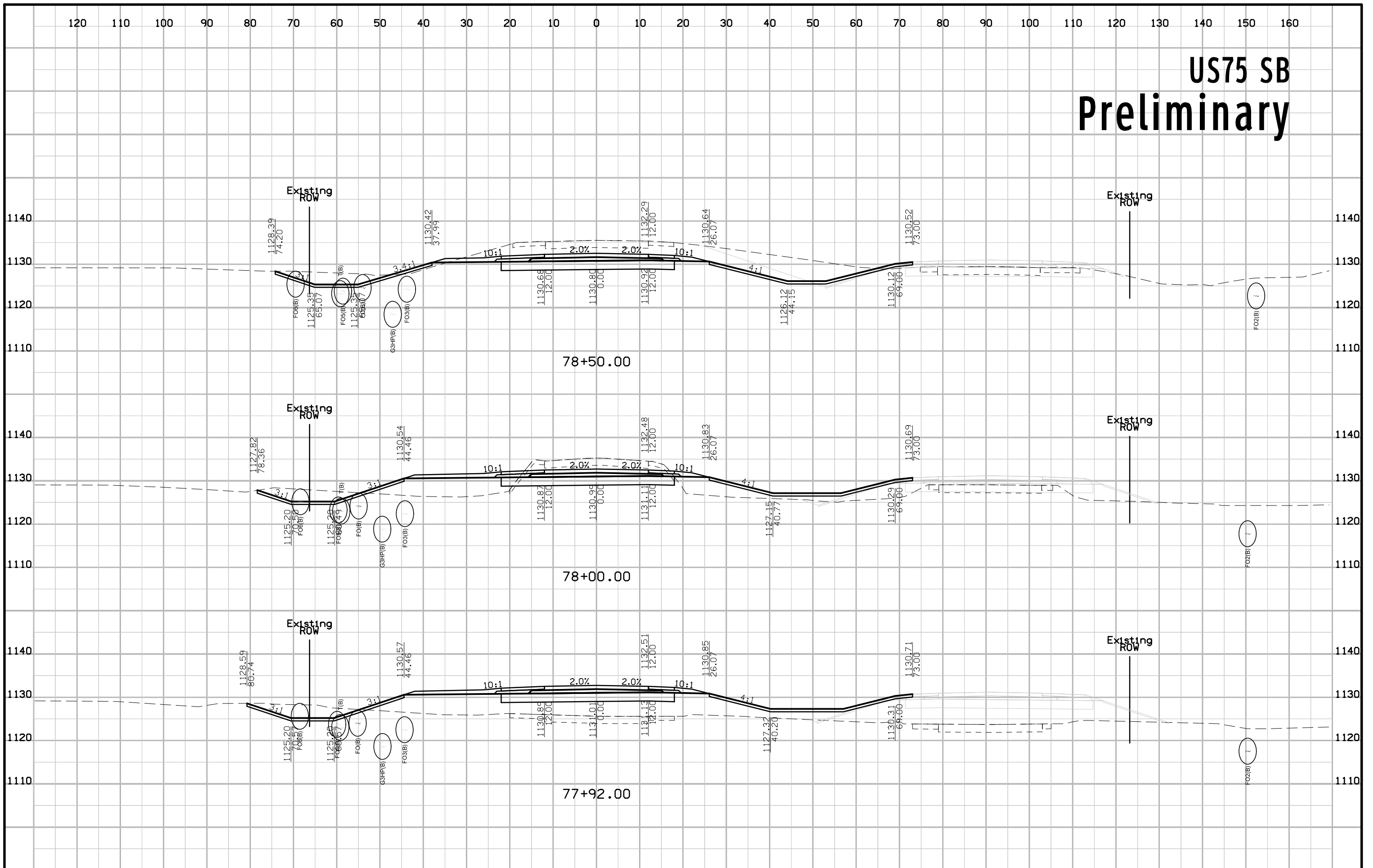
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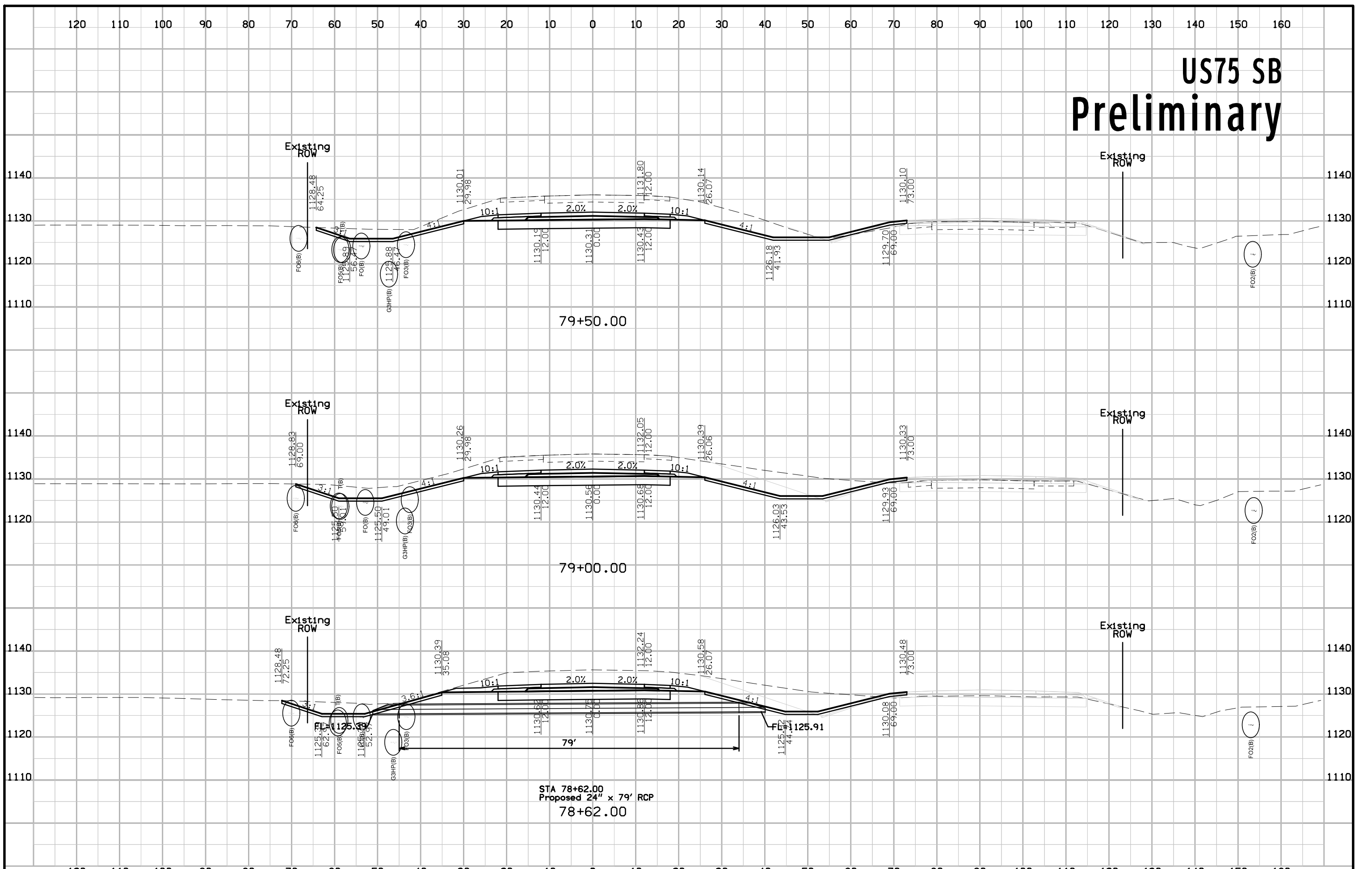
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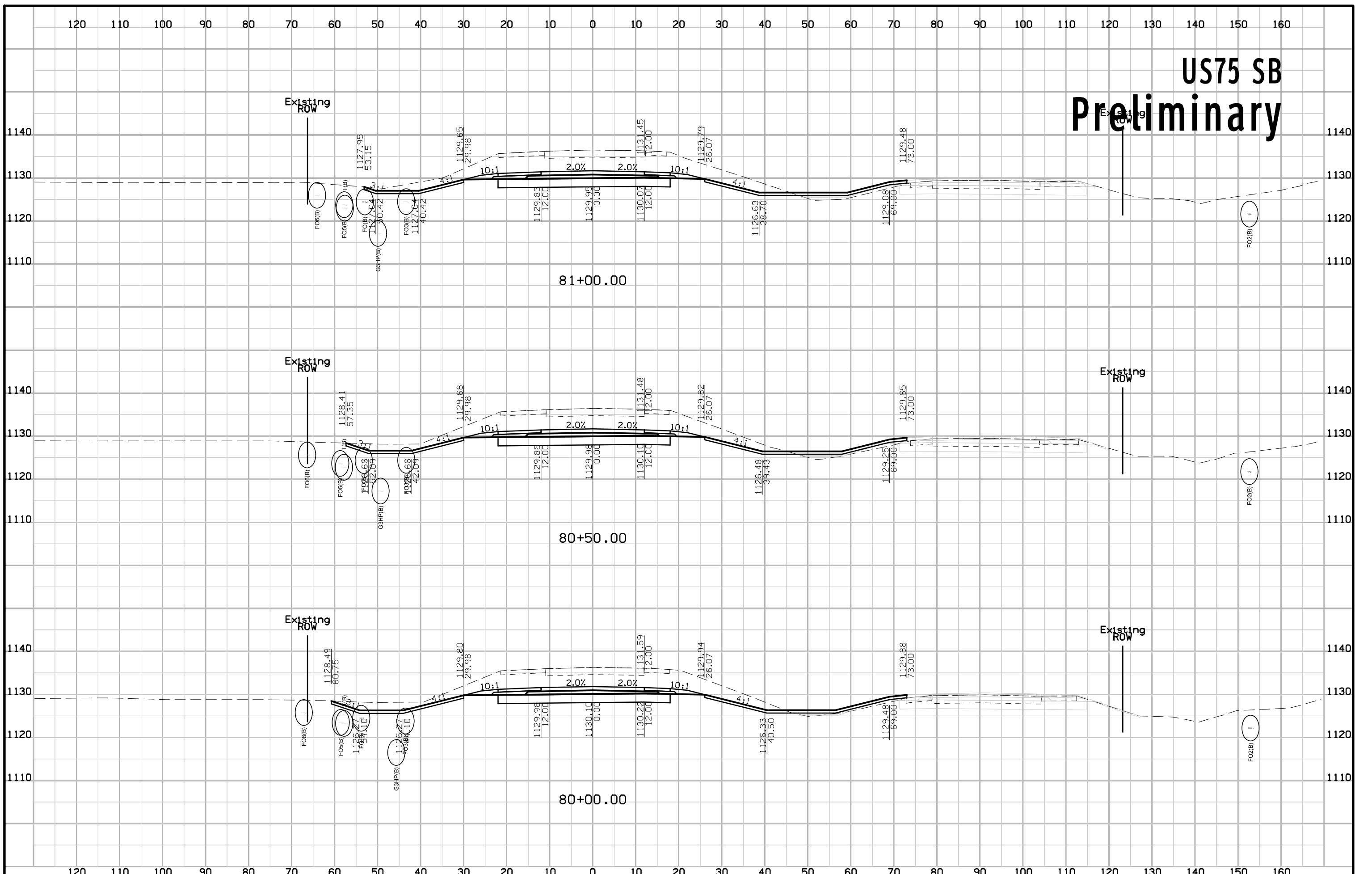
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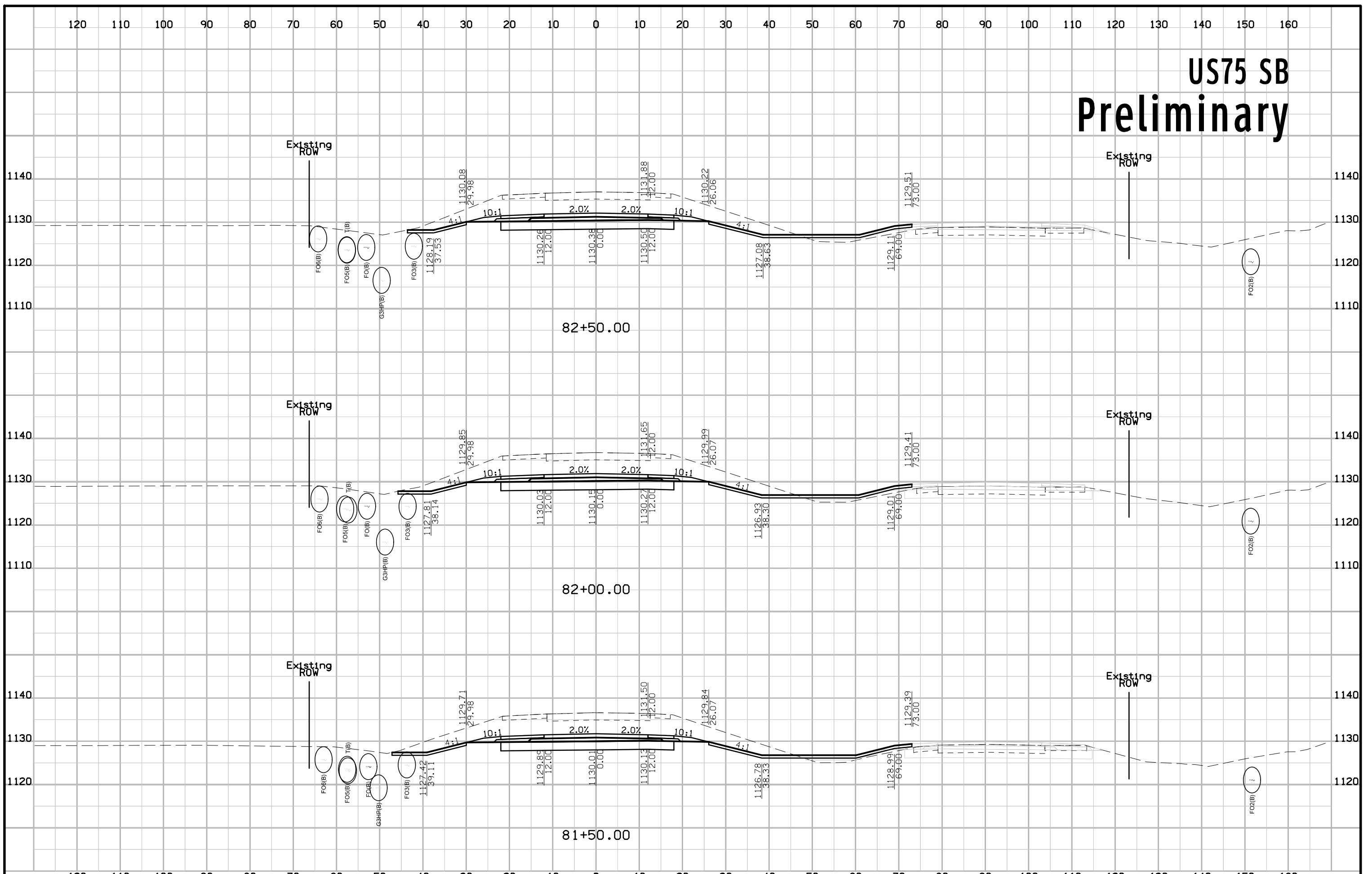
# US75 SB Preliminary



# US75 SB Preliminary

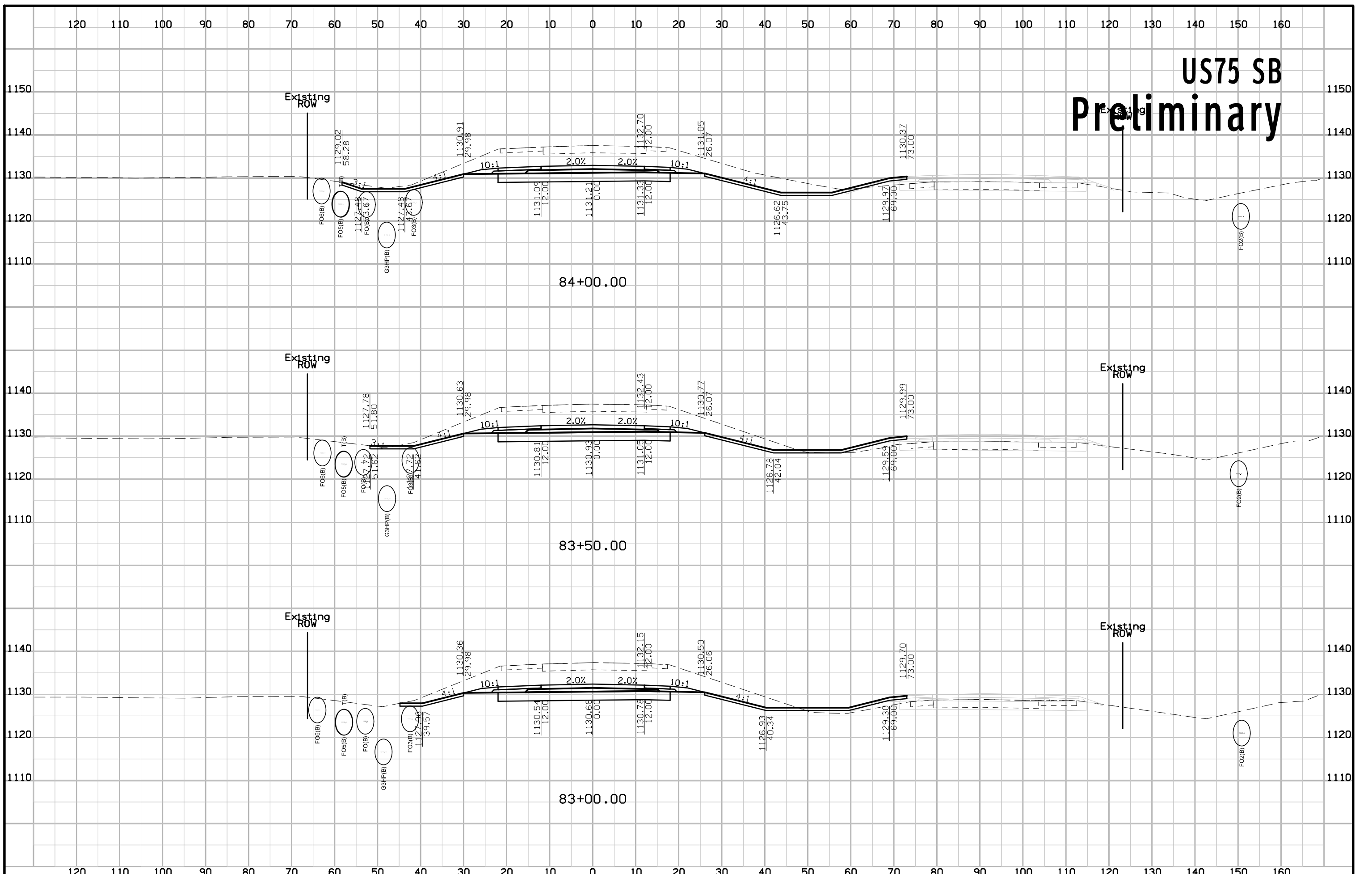


# US75 SB Preliminary

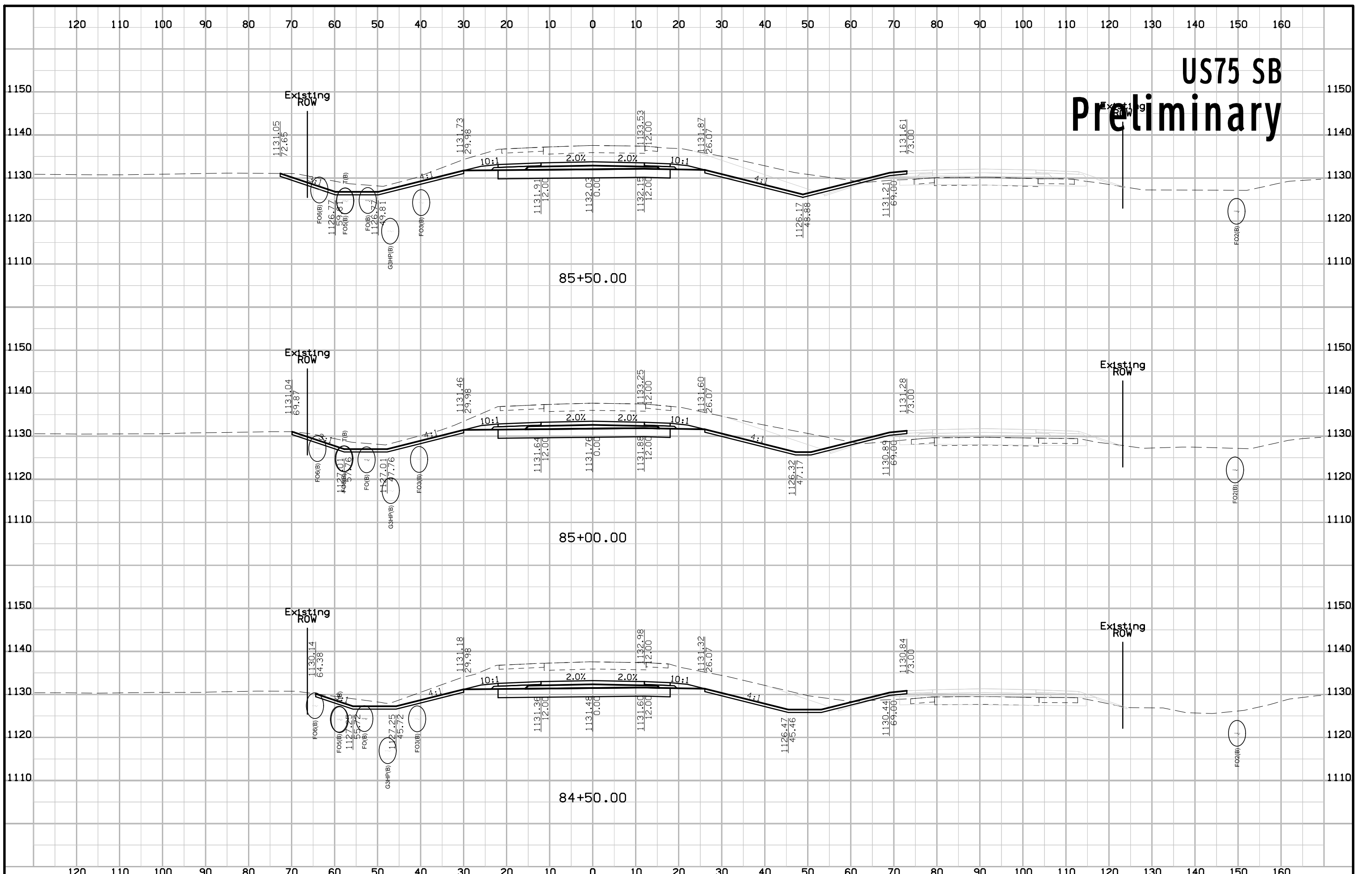




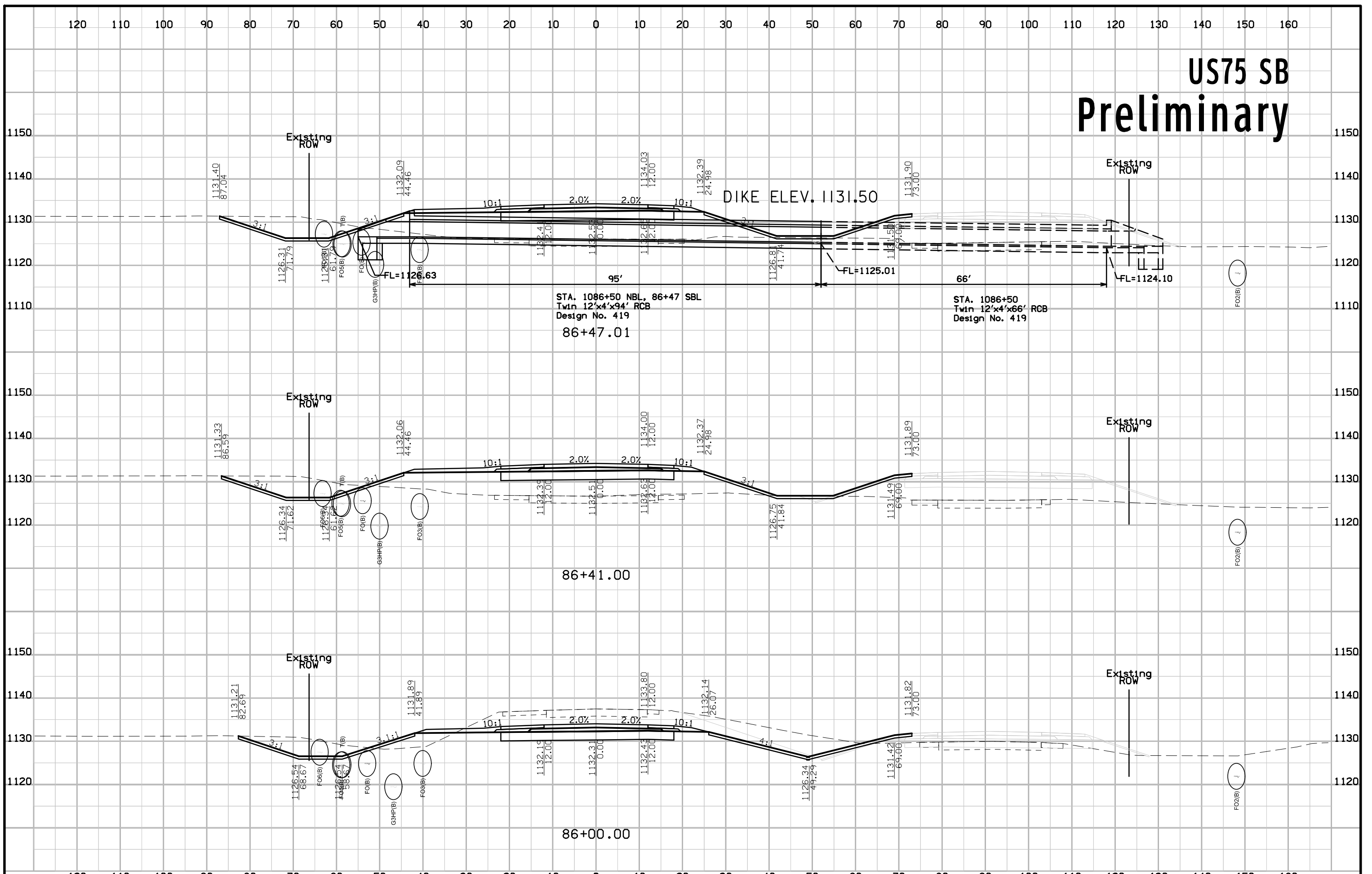
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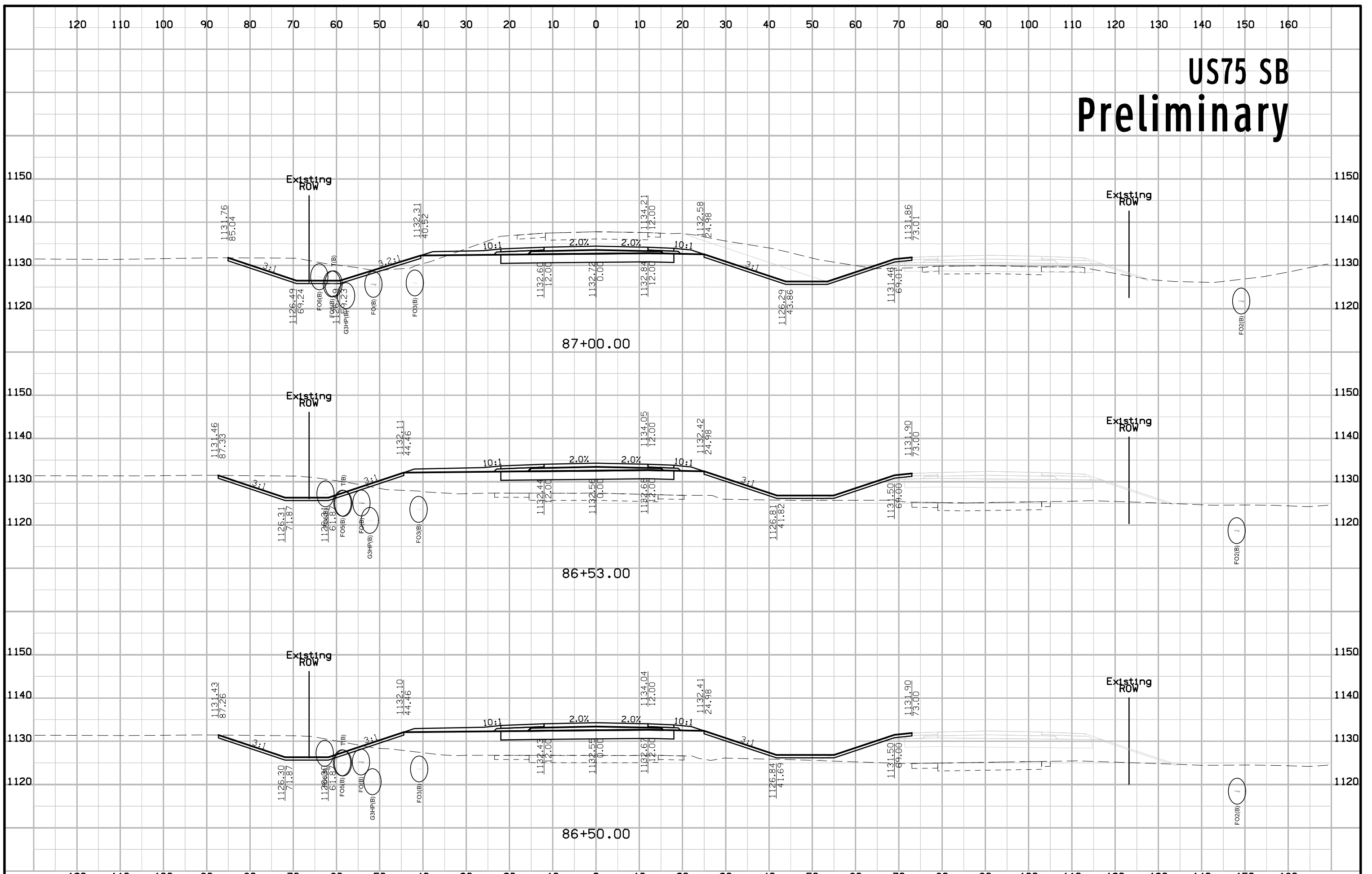
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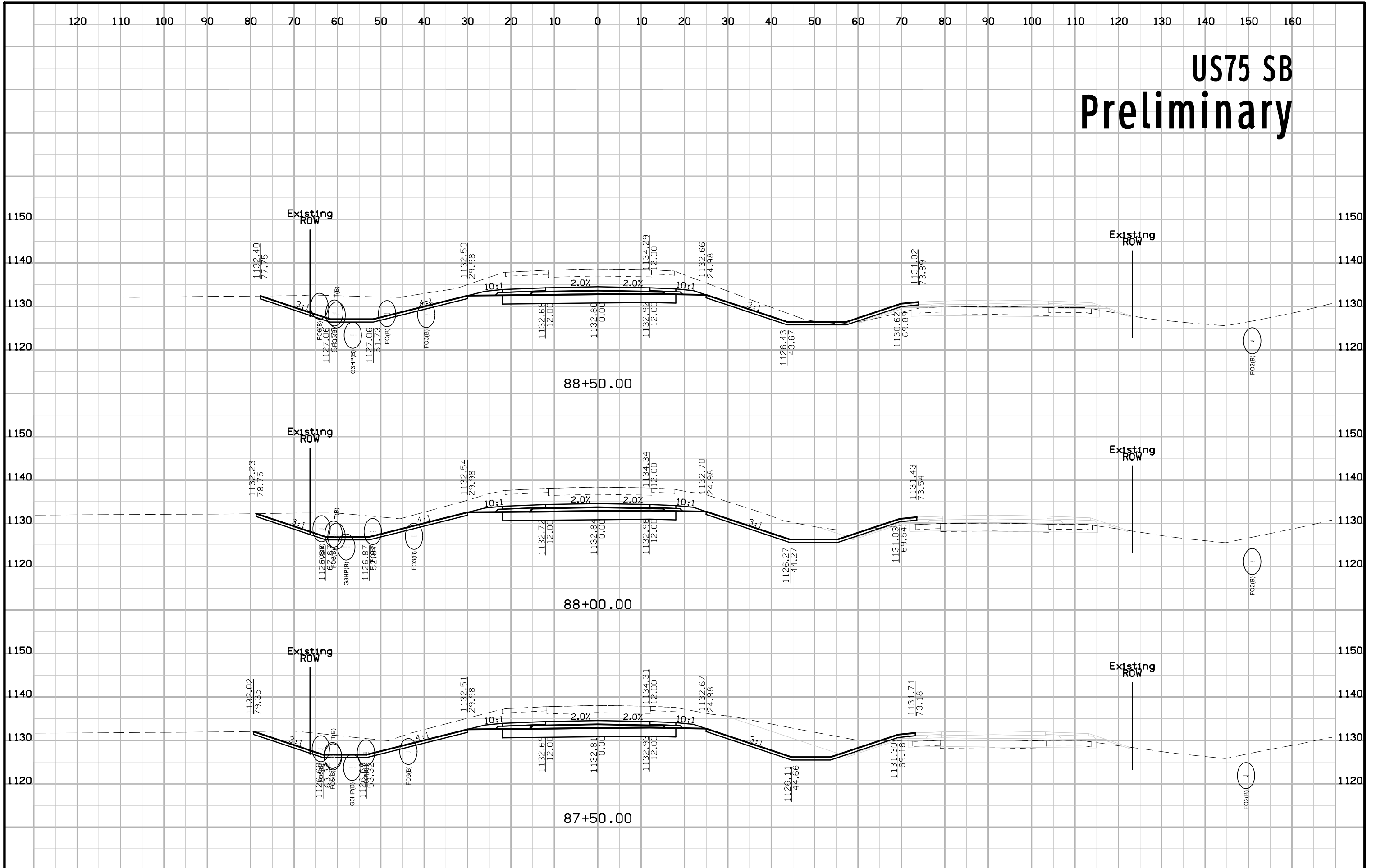
# US75 SB Preliminary



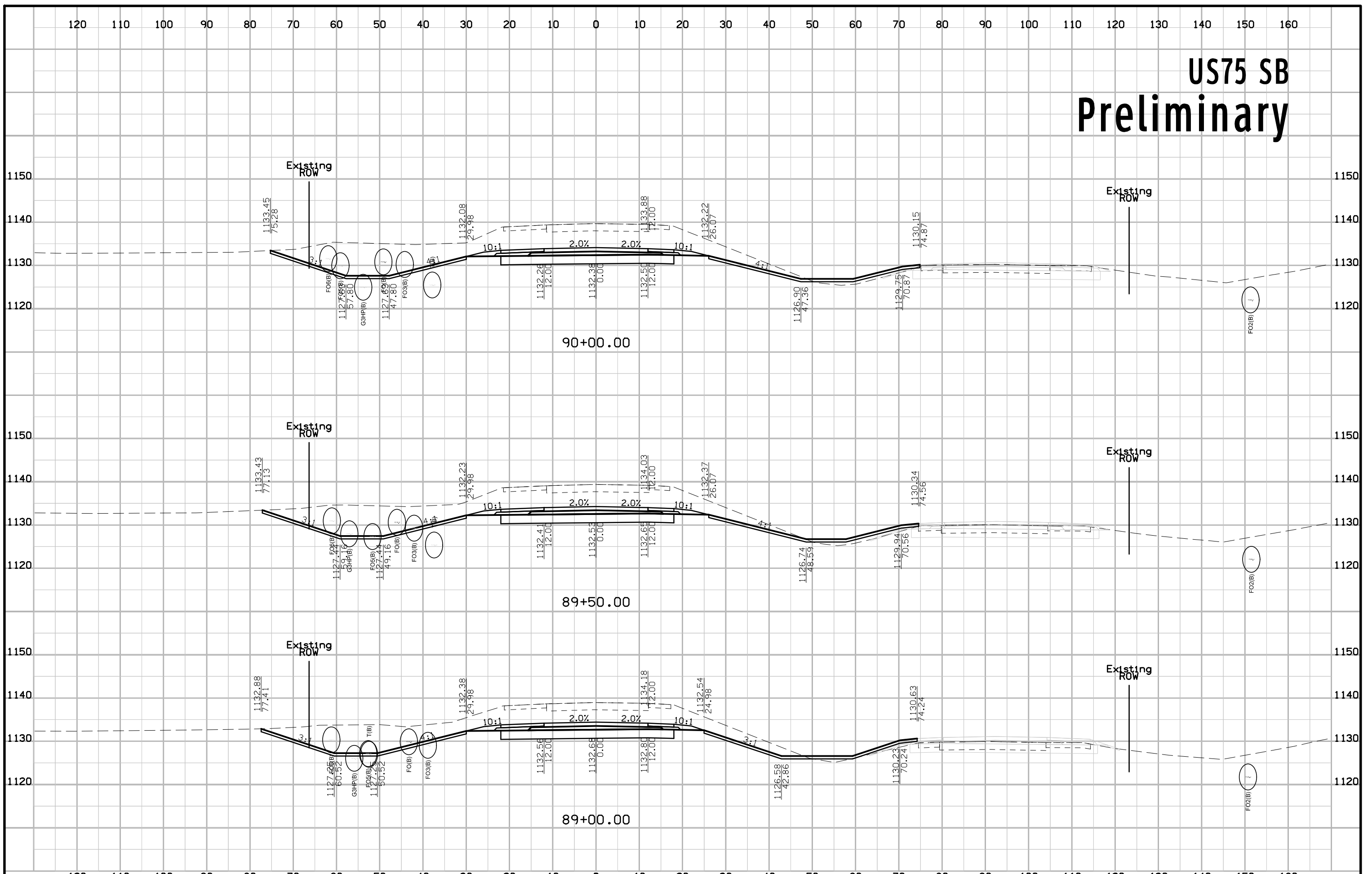
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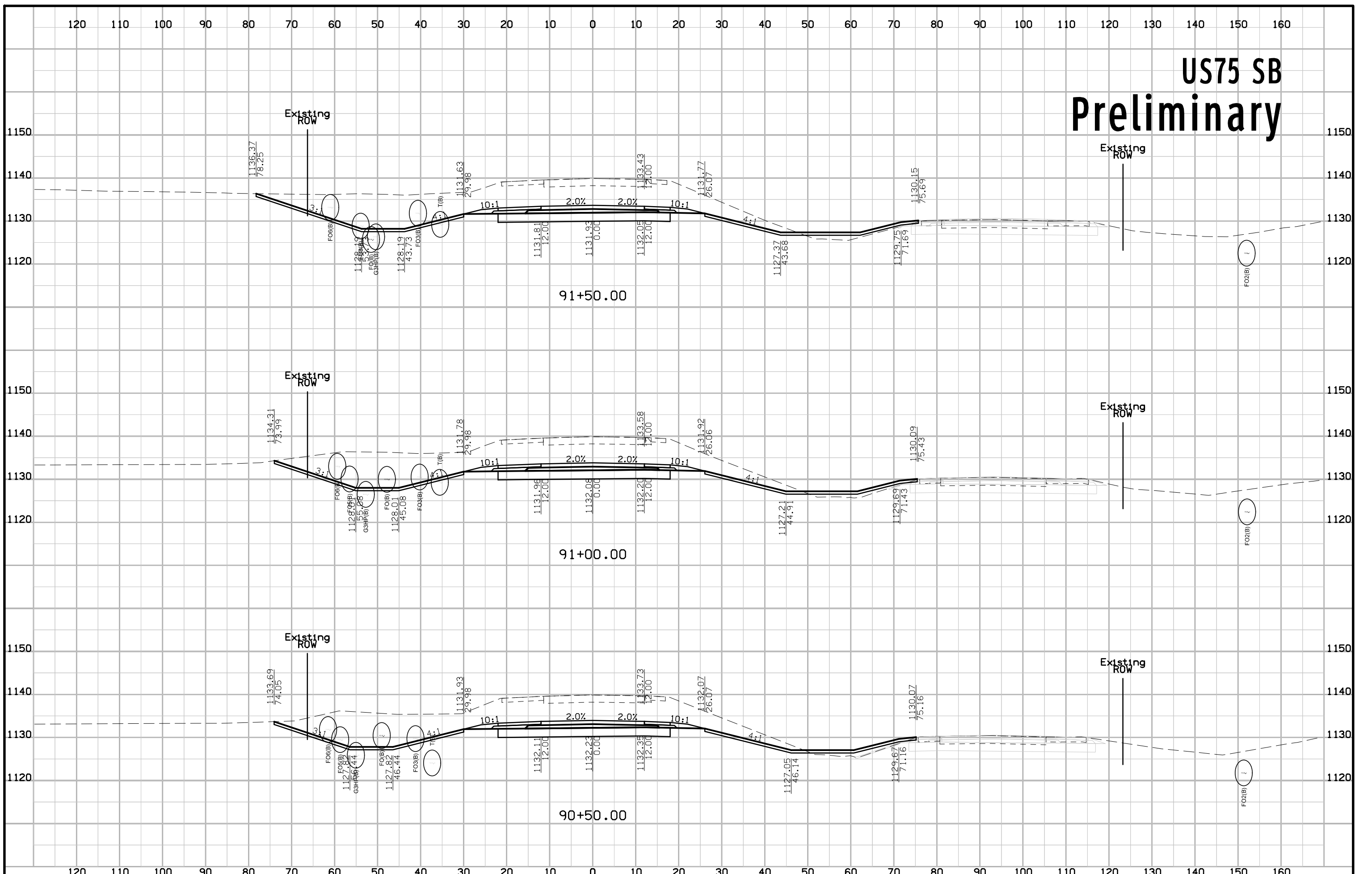
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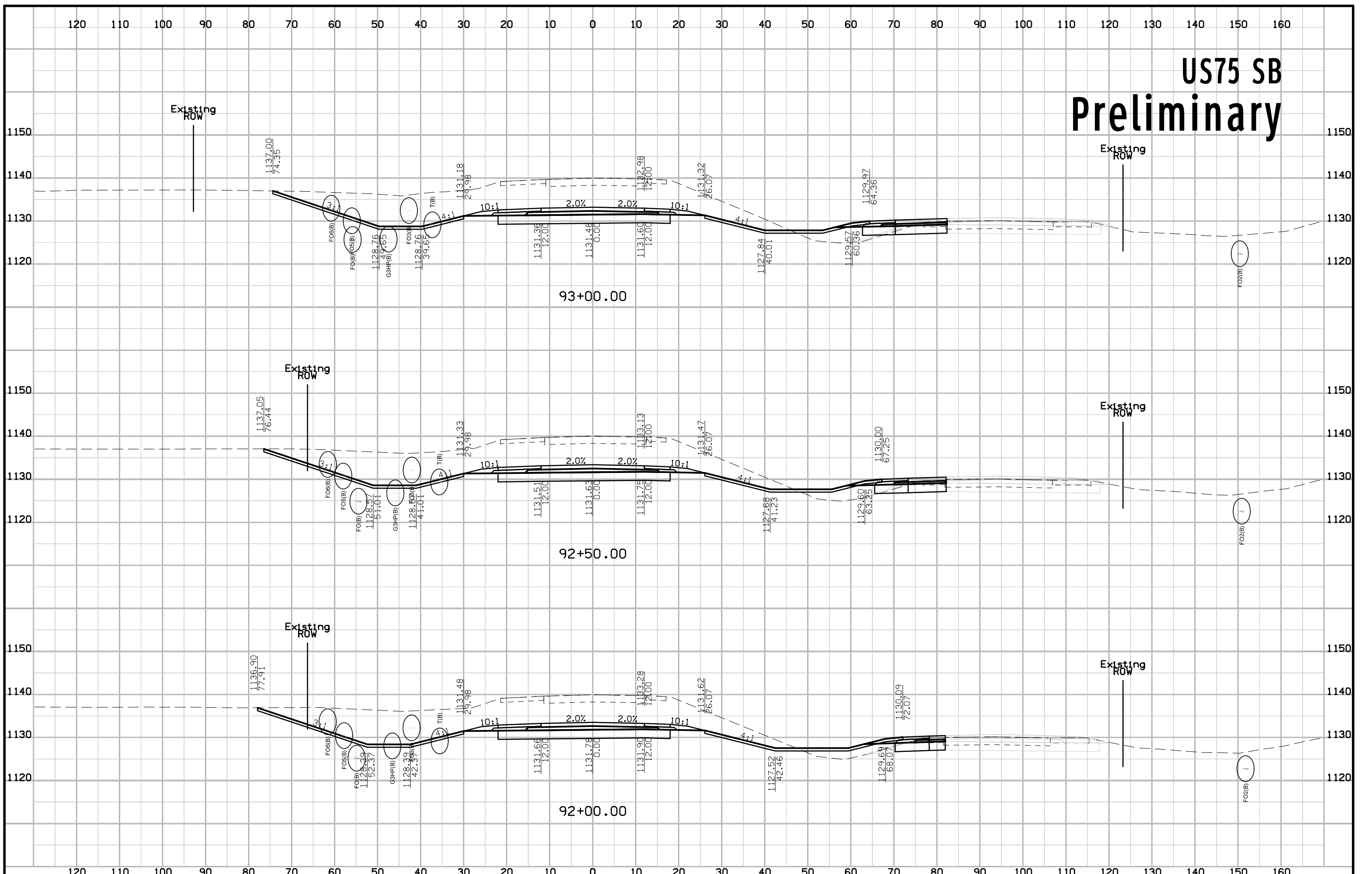
# US75 SB Preliminary



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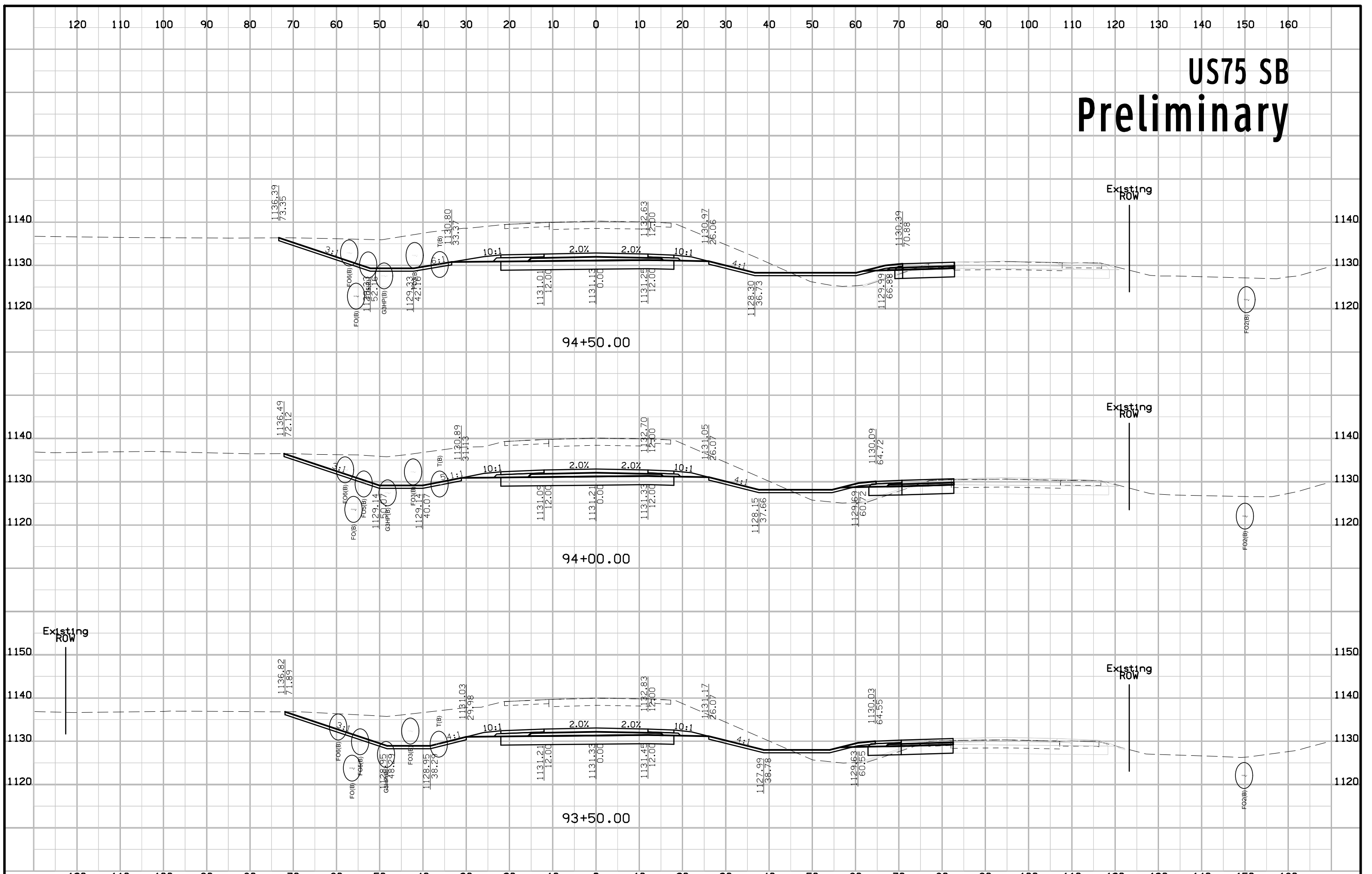


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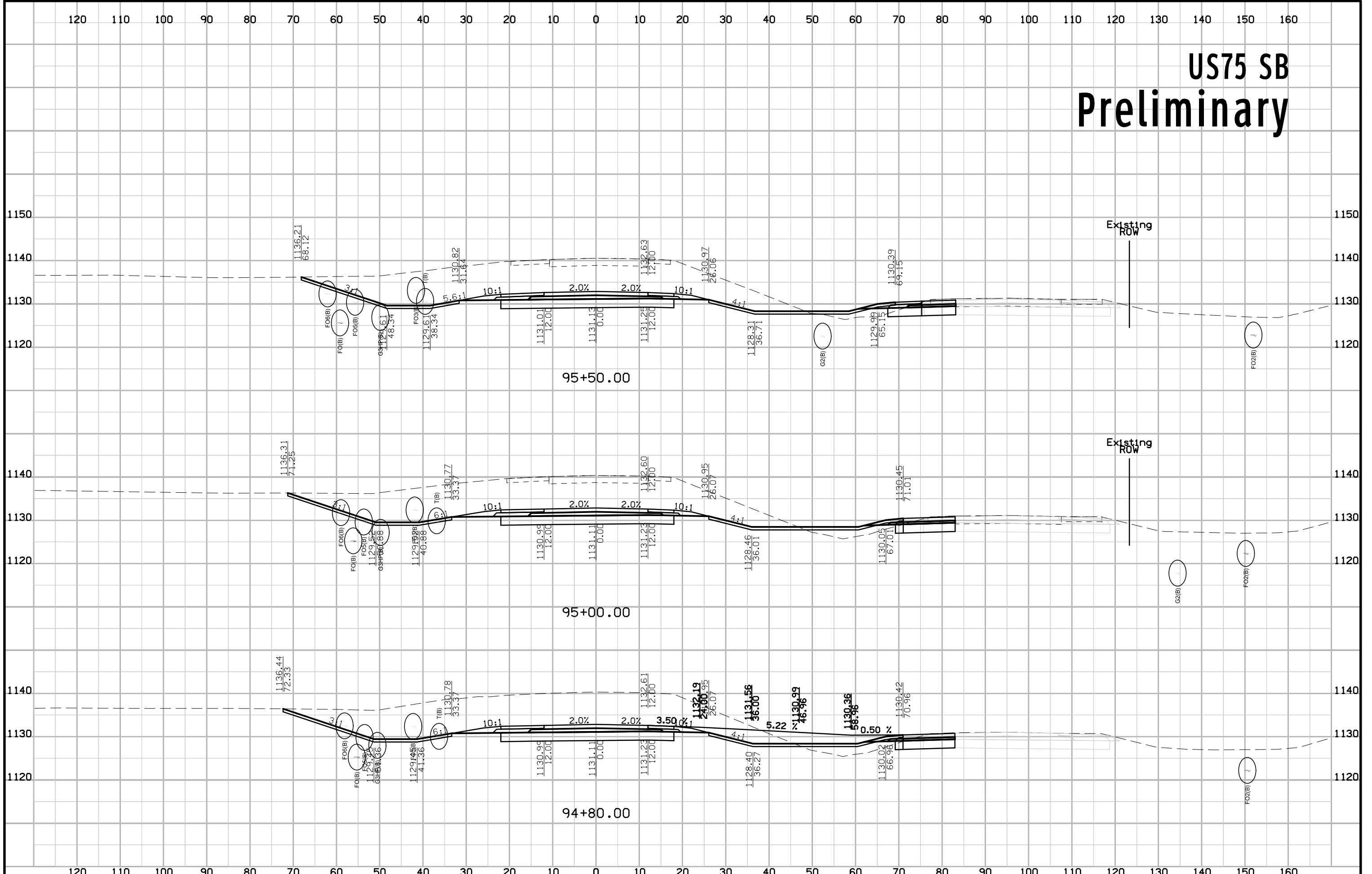




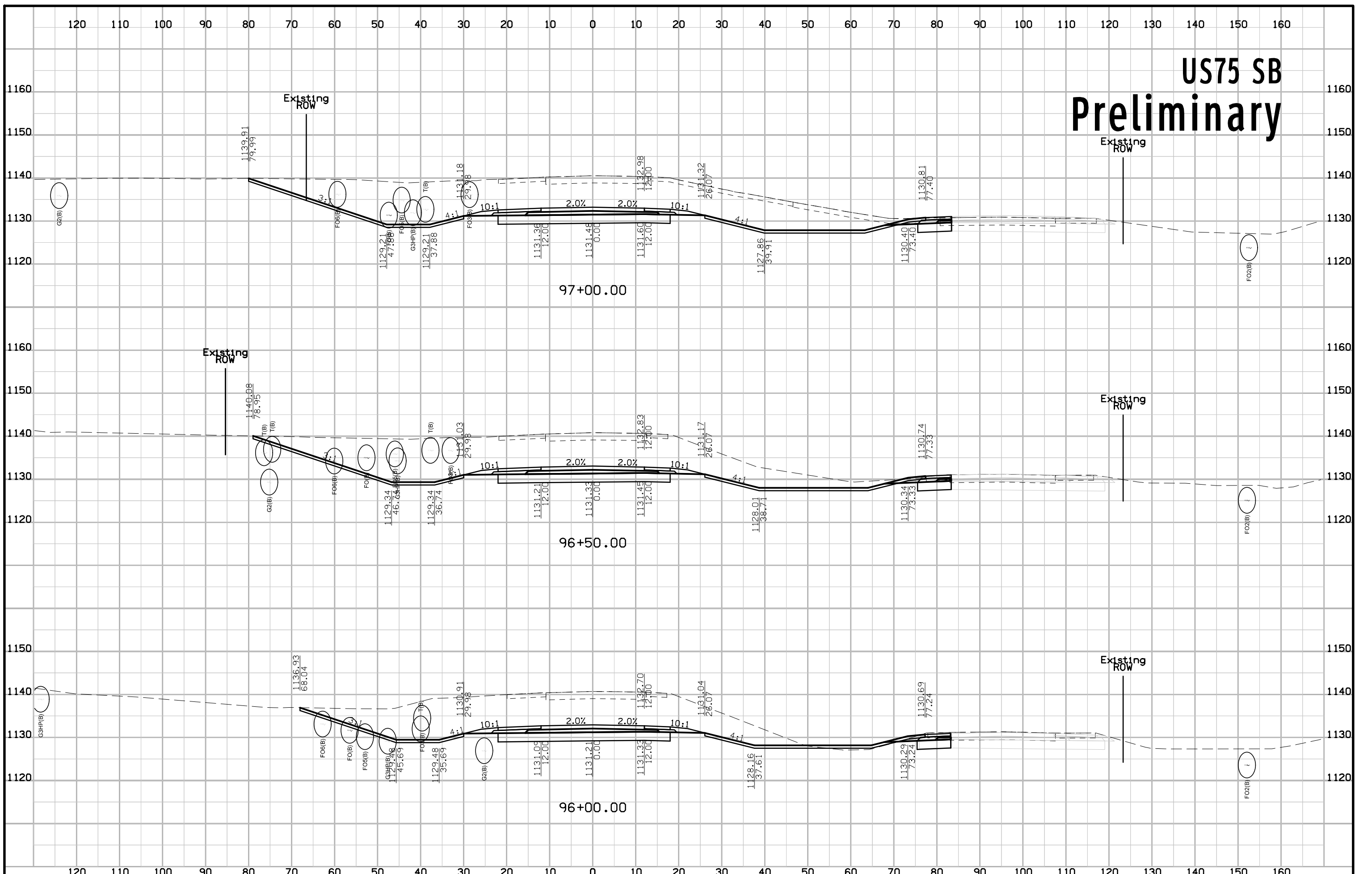
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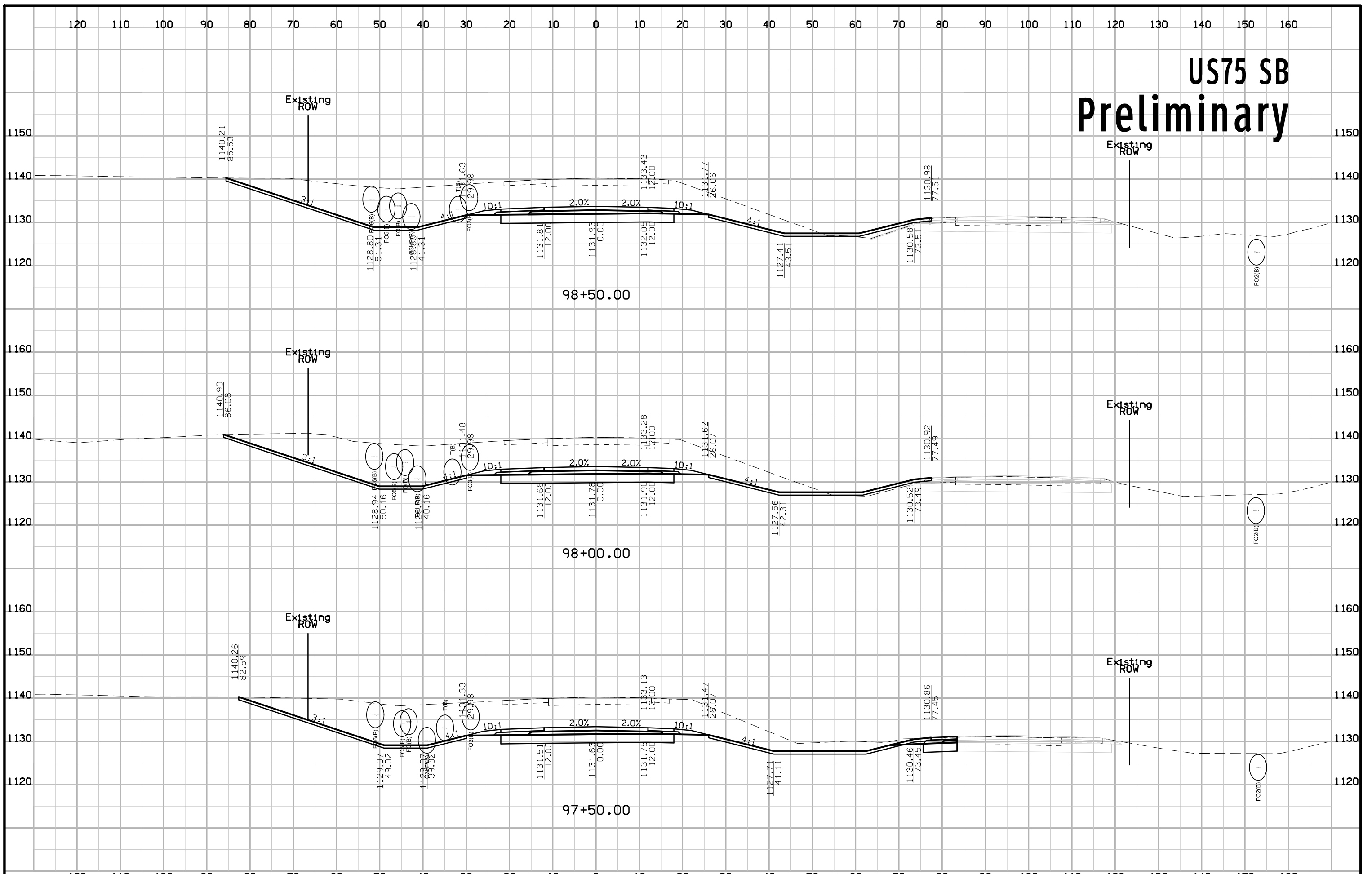
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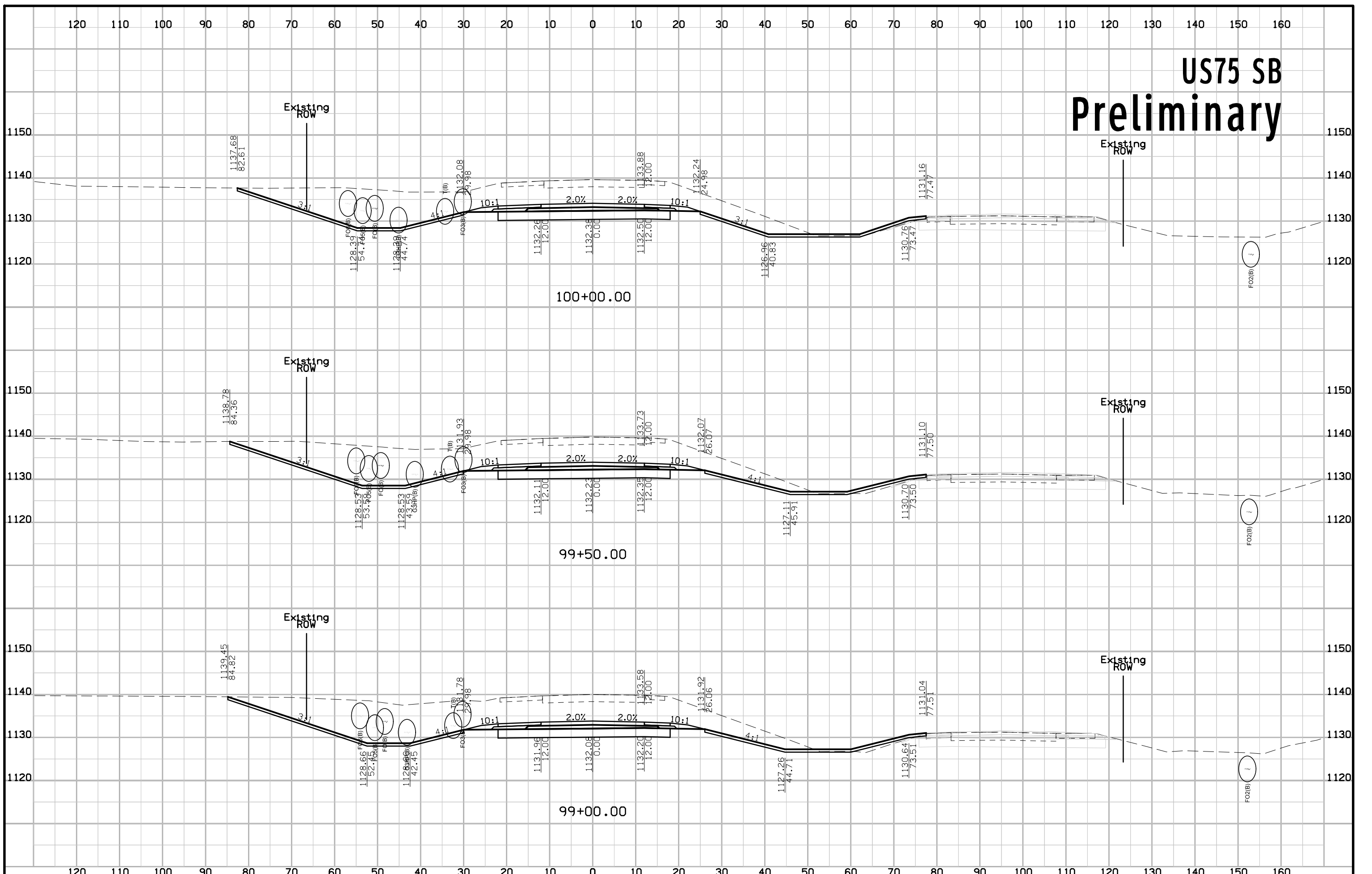
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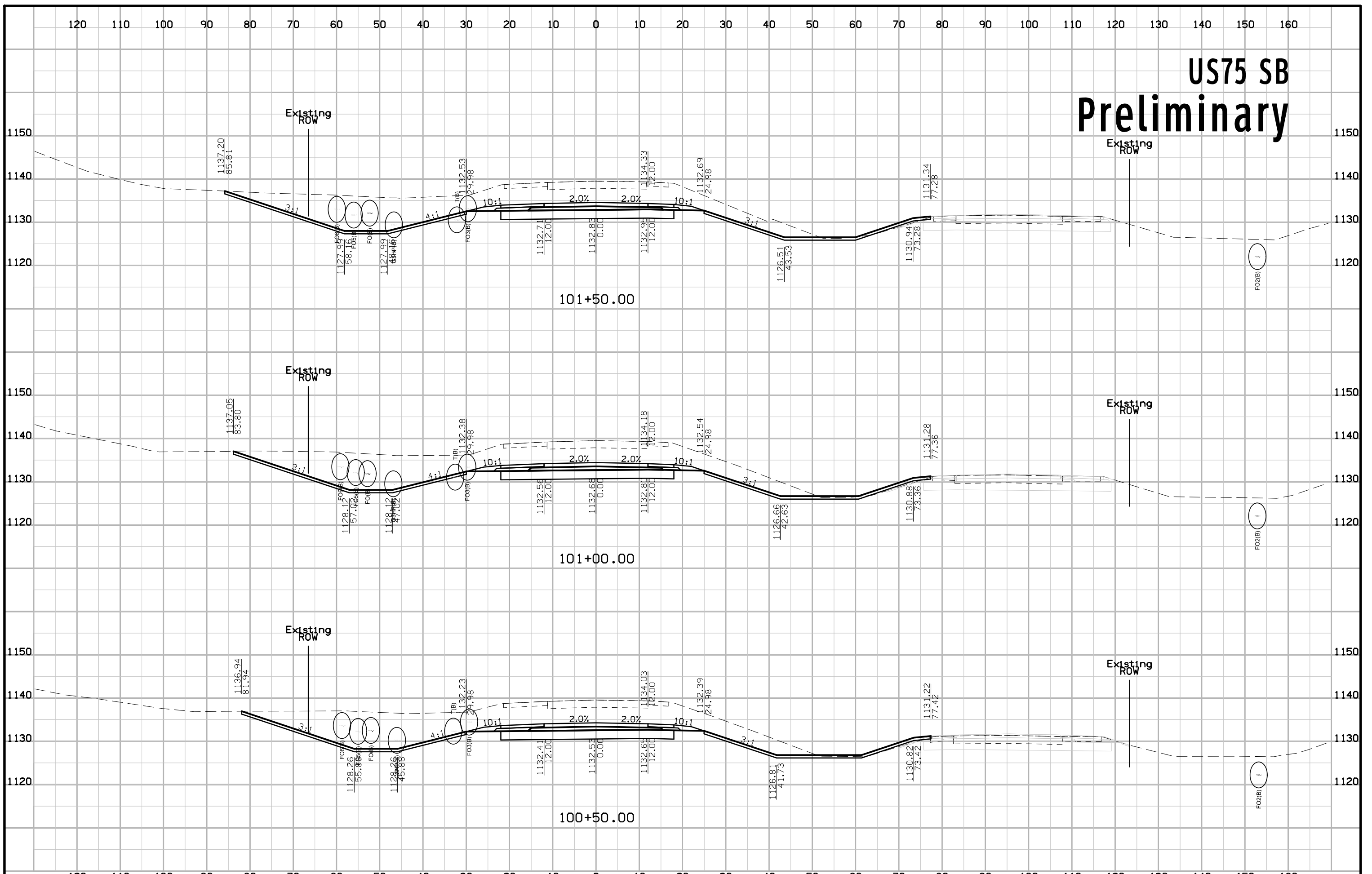
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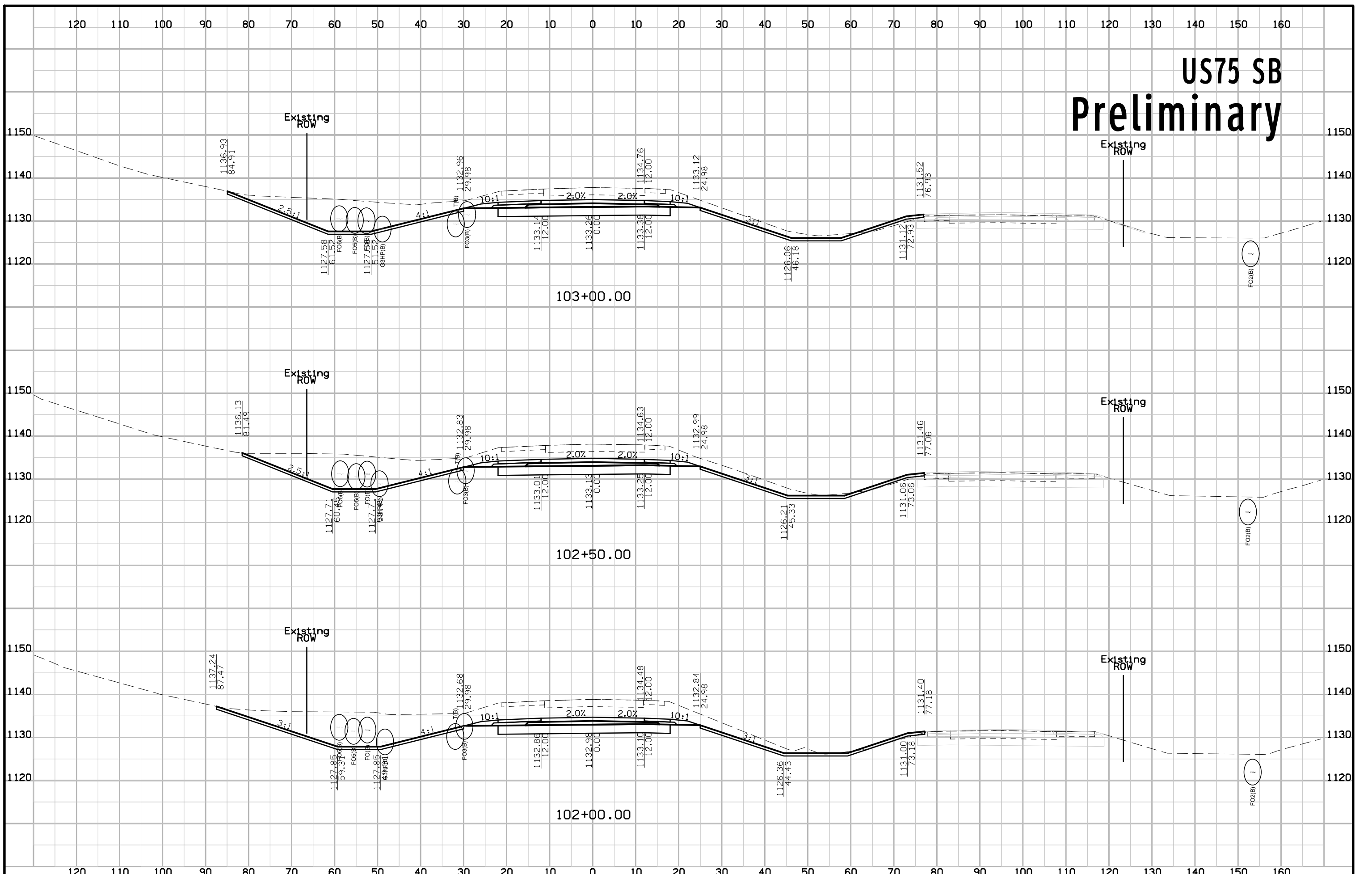
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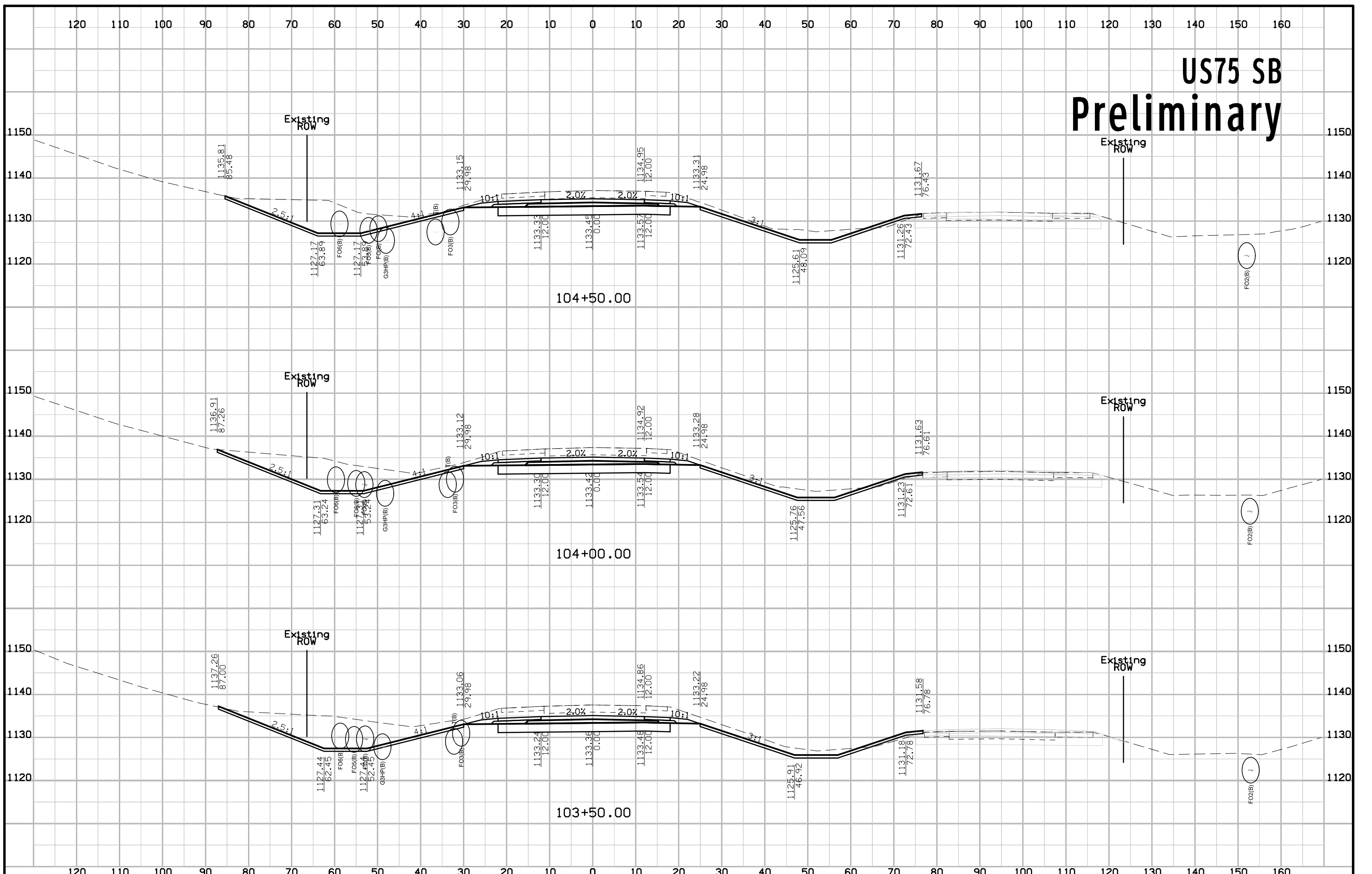
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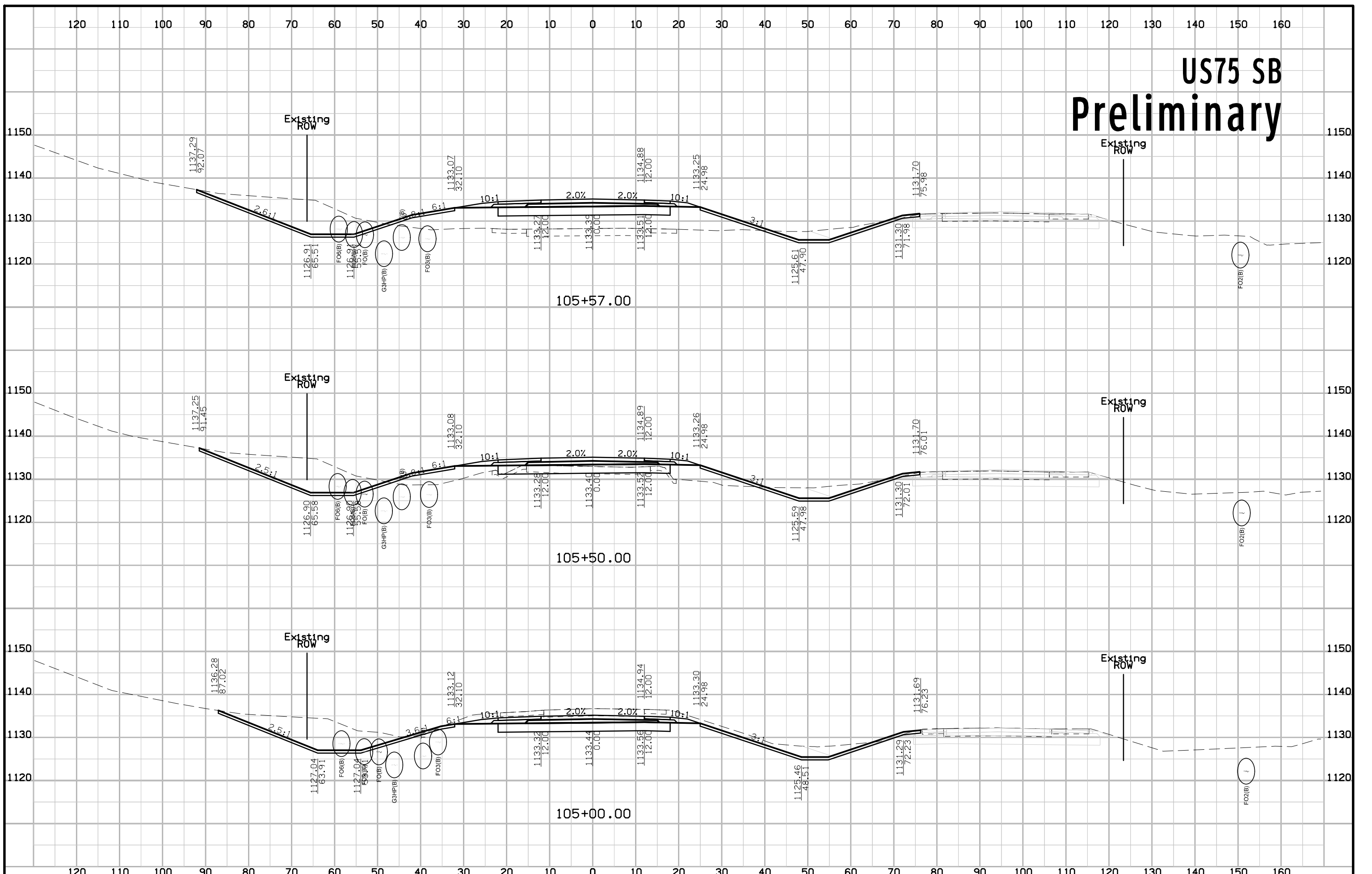


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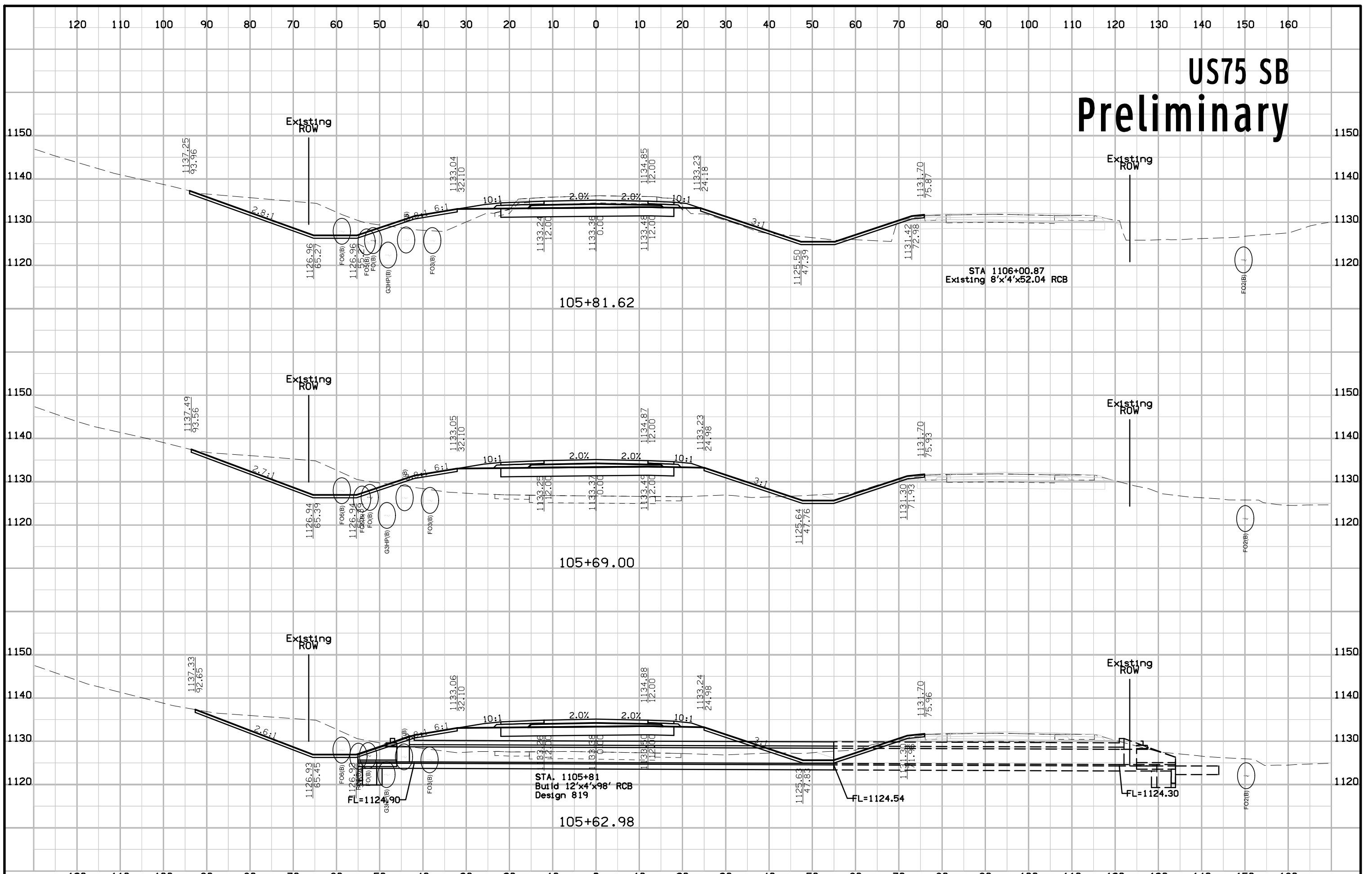




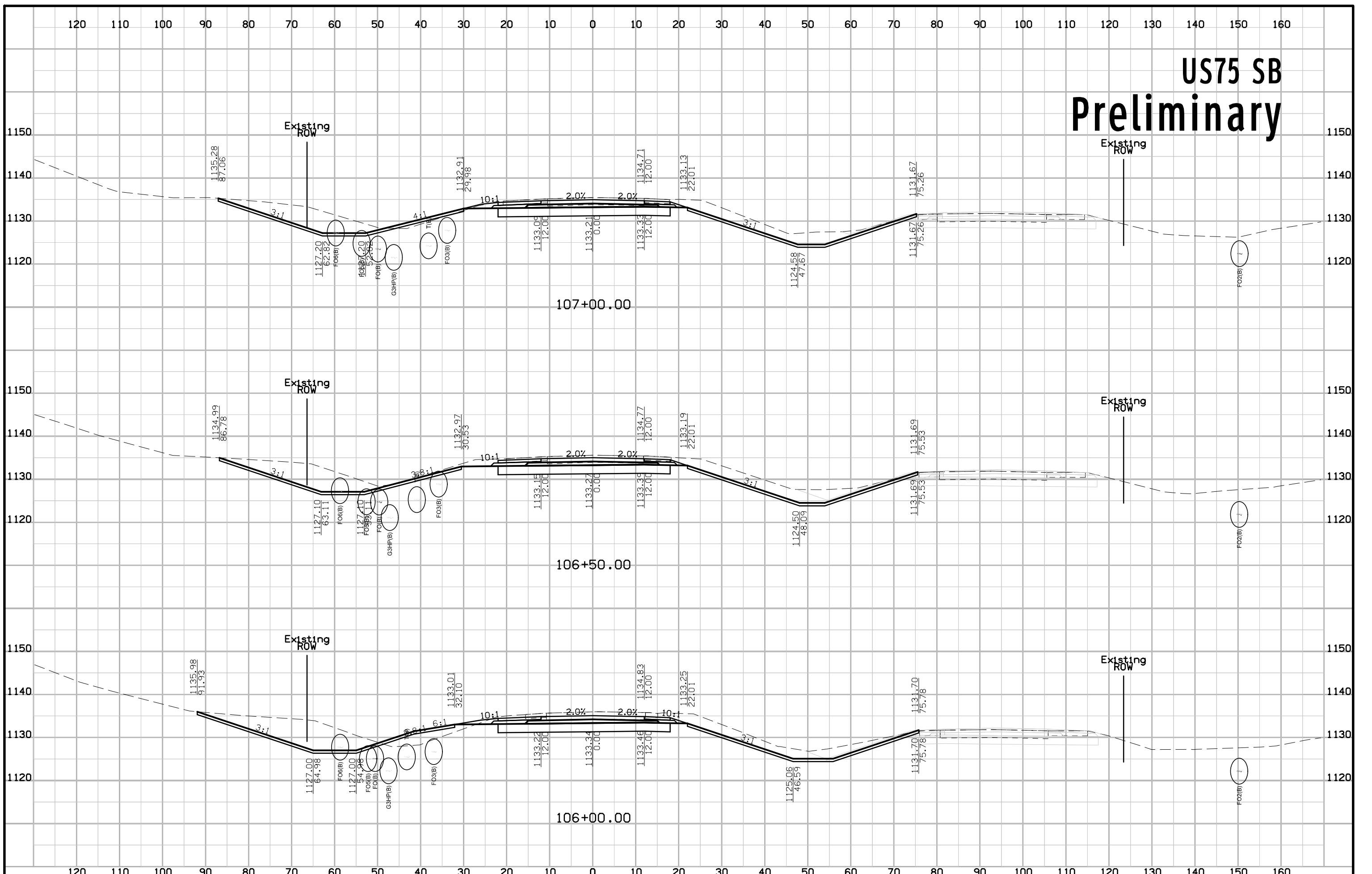
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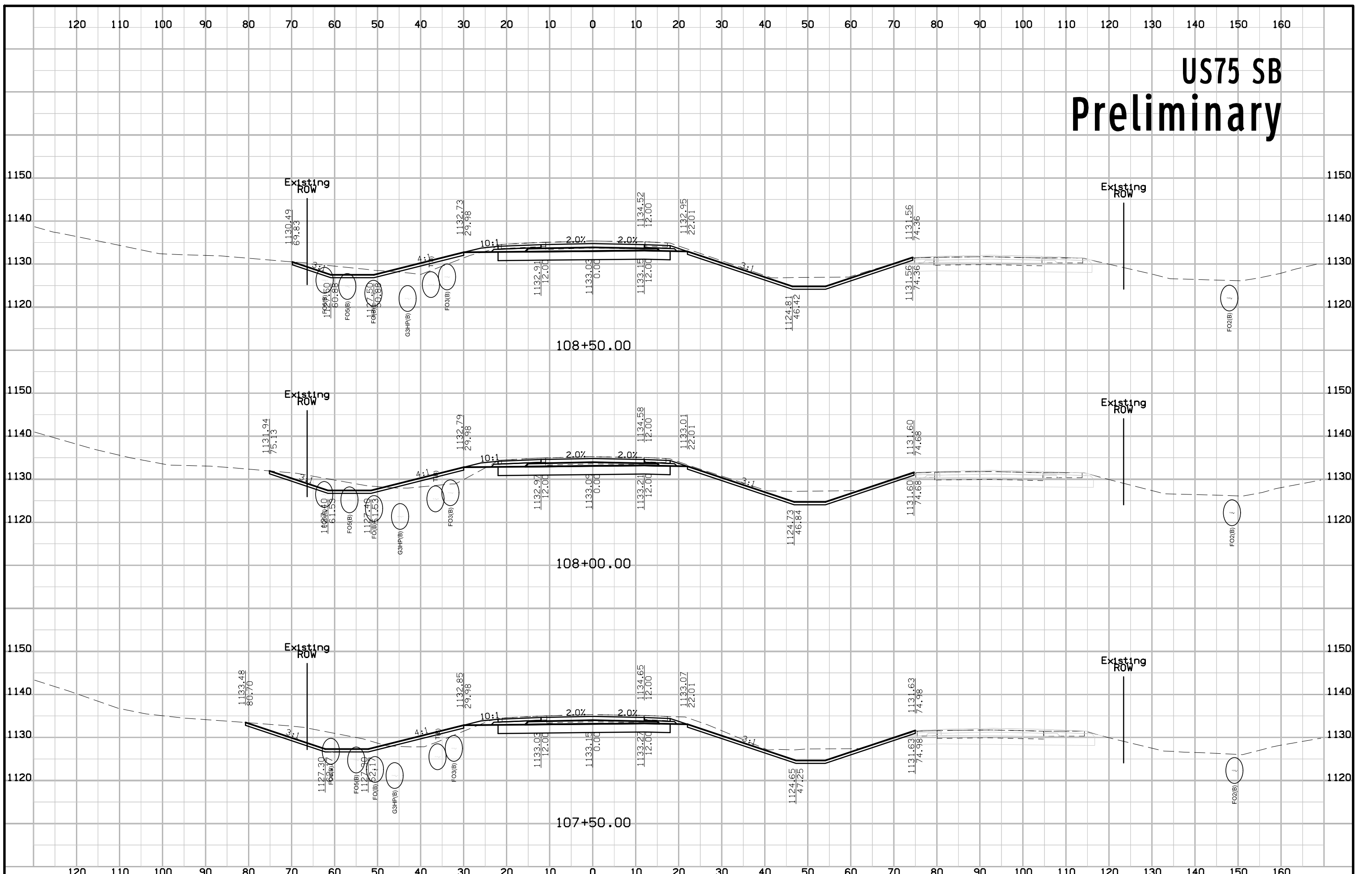
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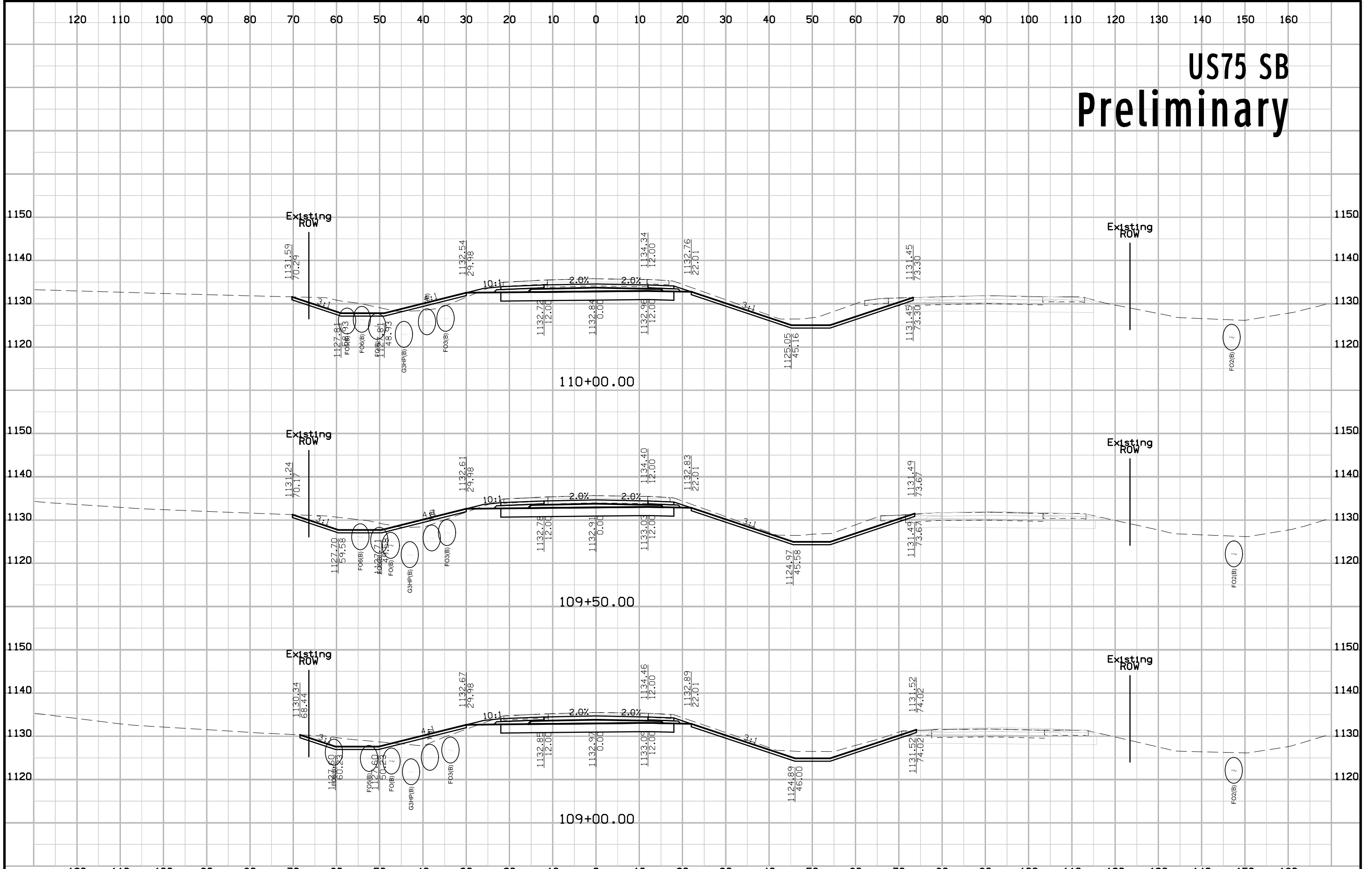
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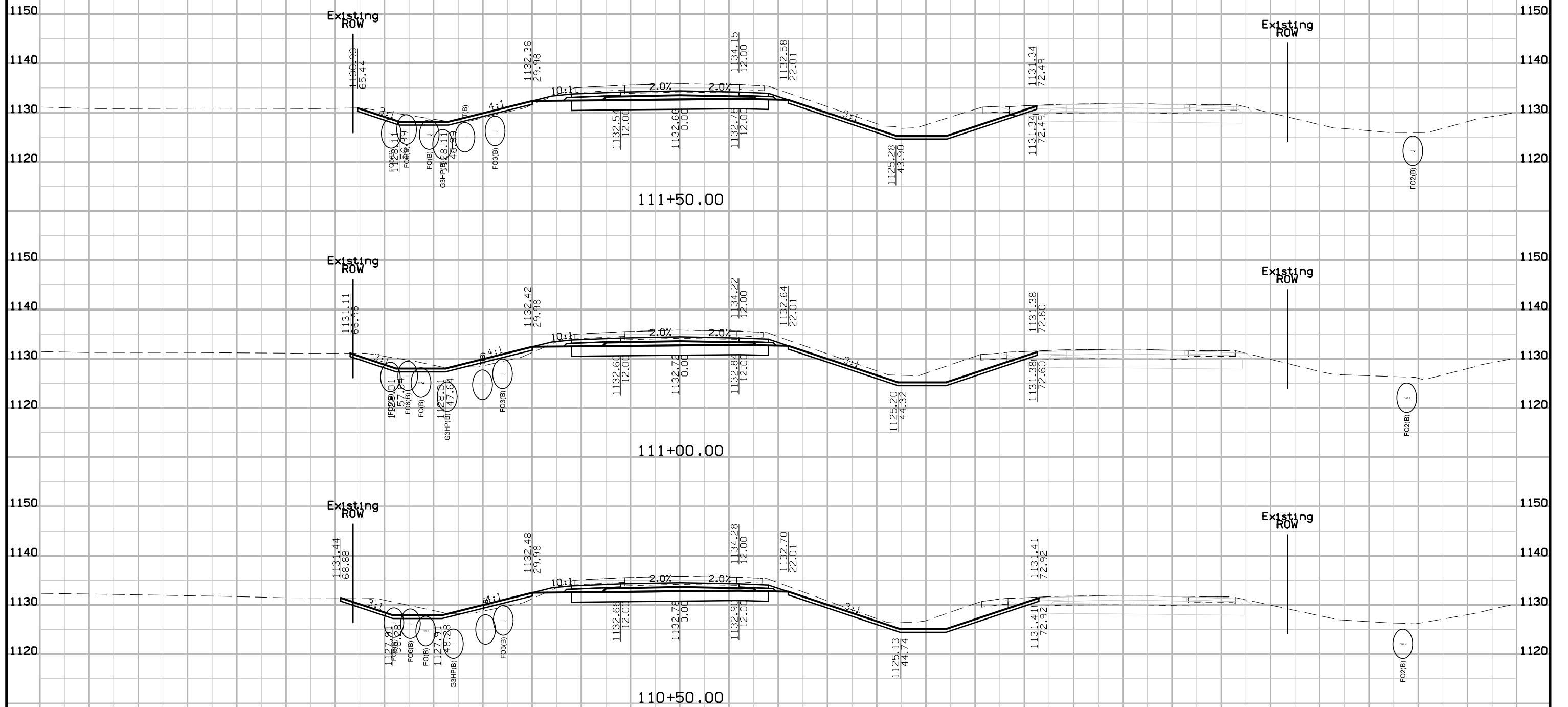
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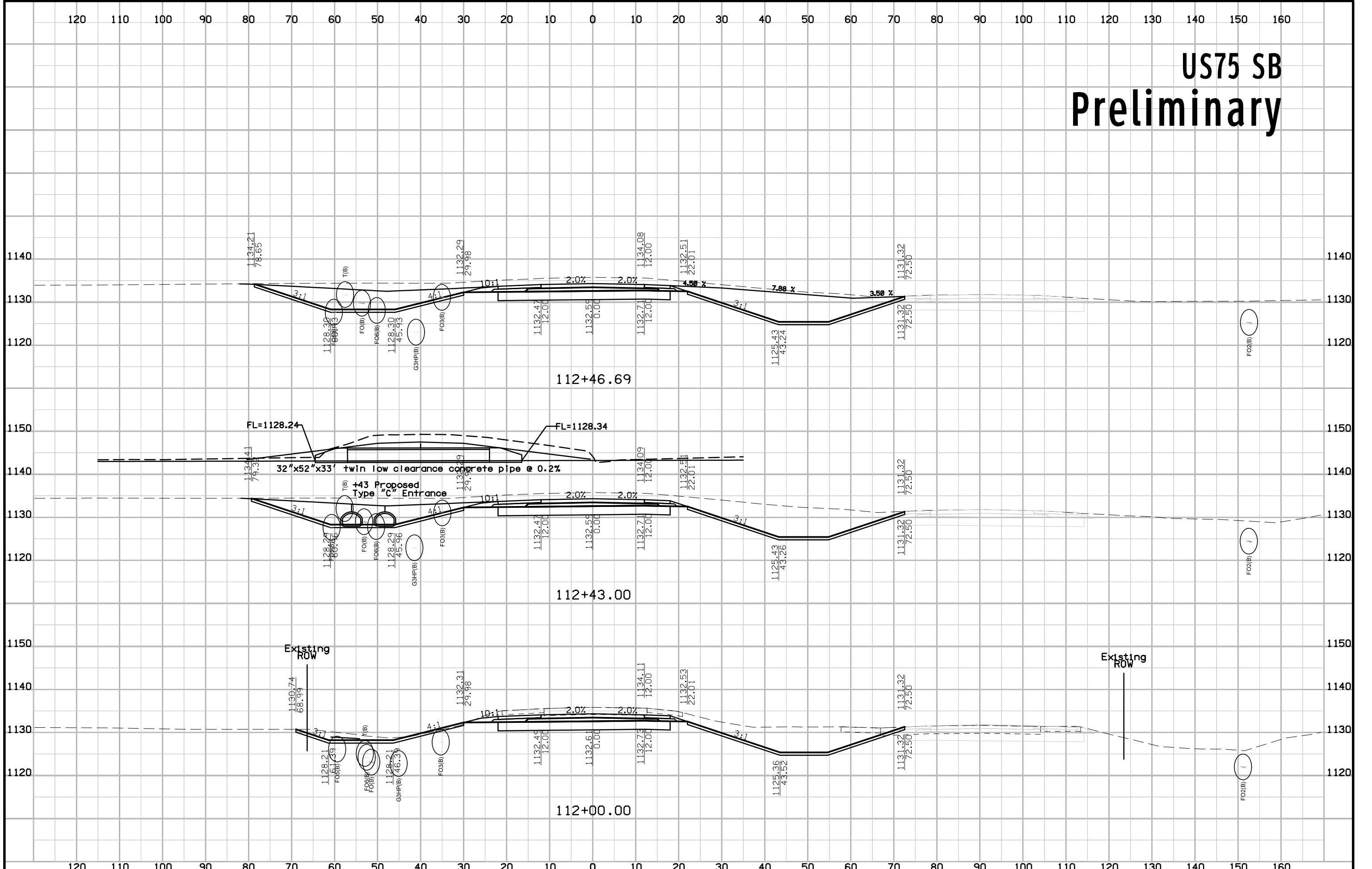
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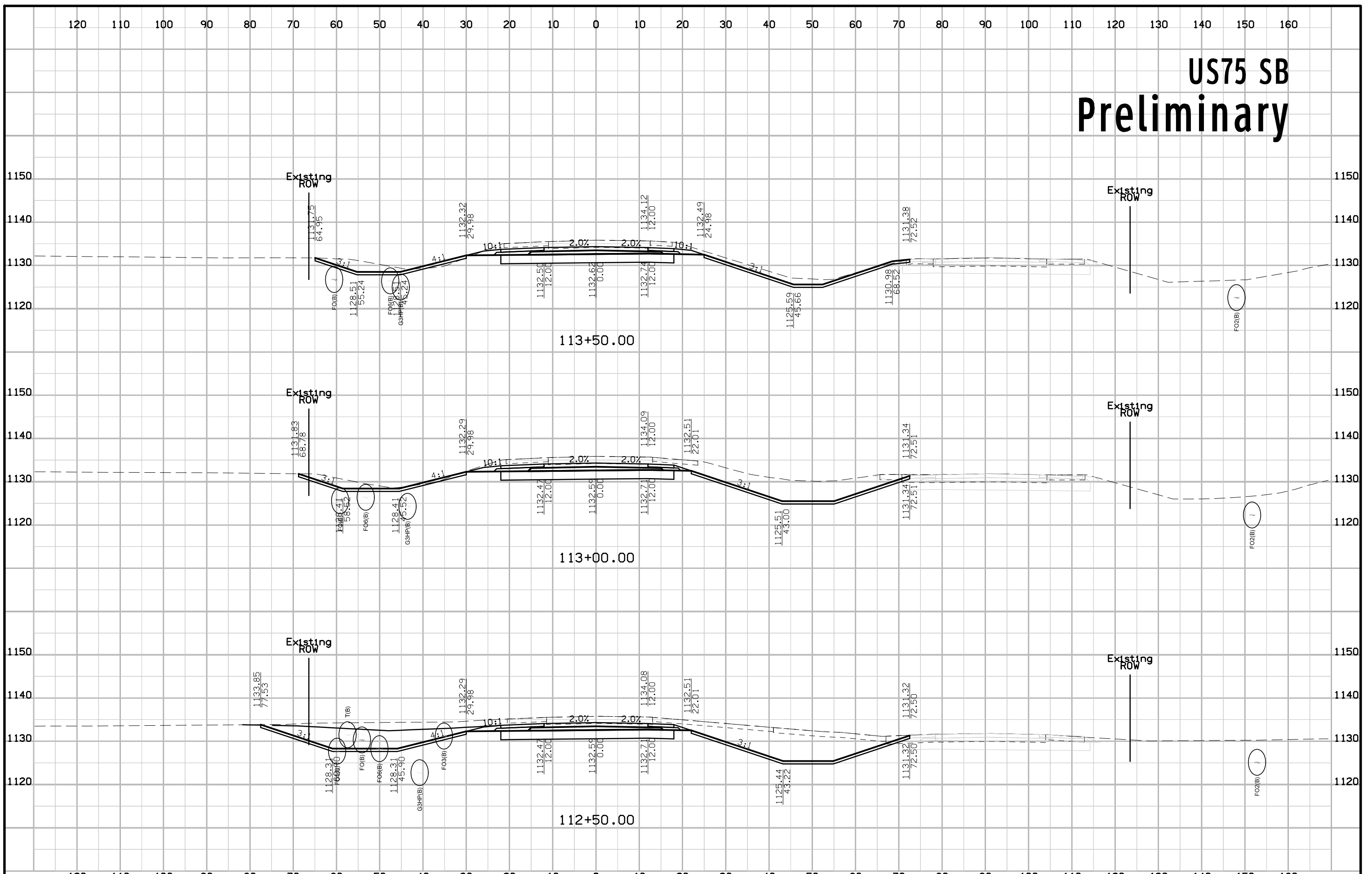
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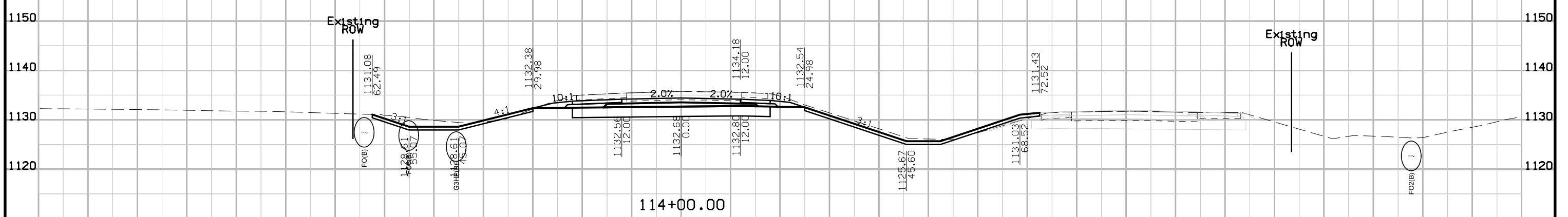
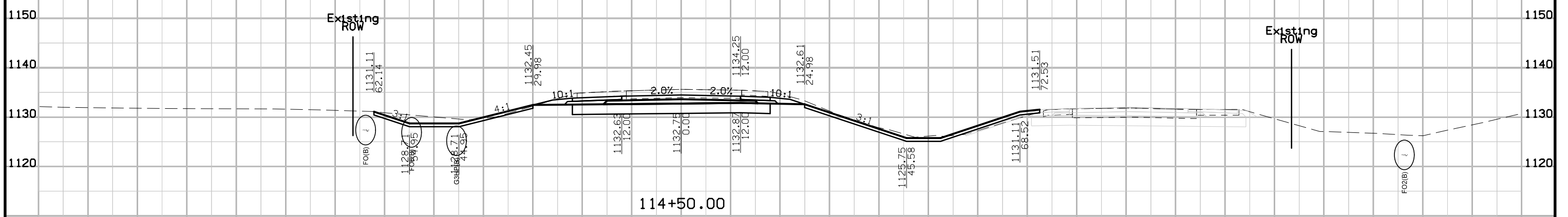
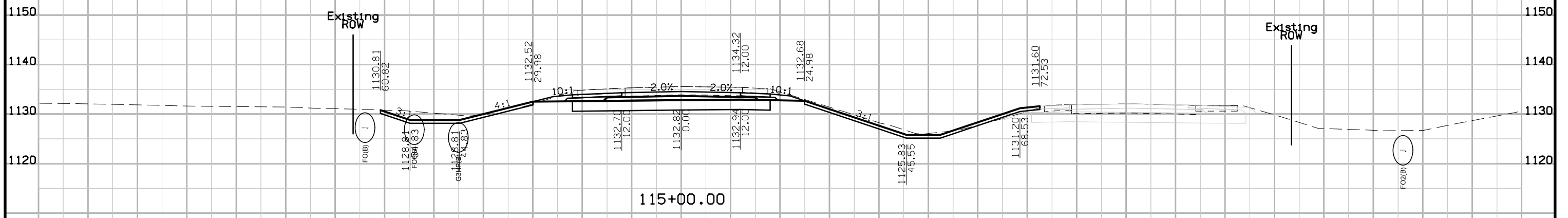


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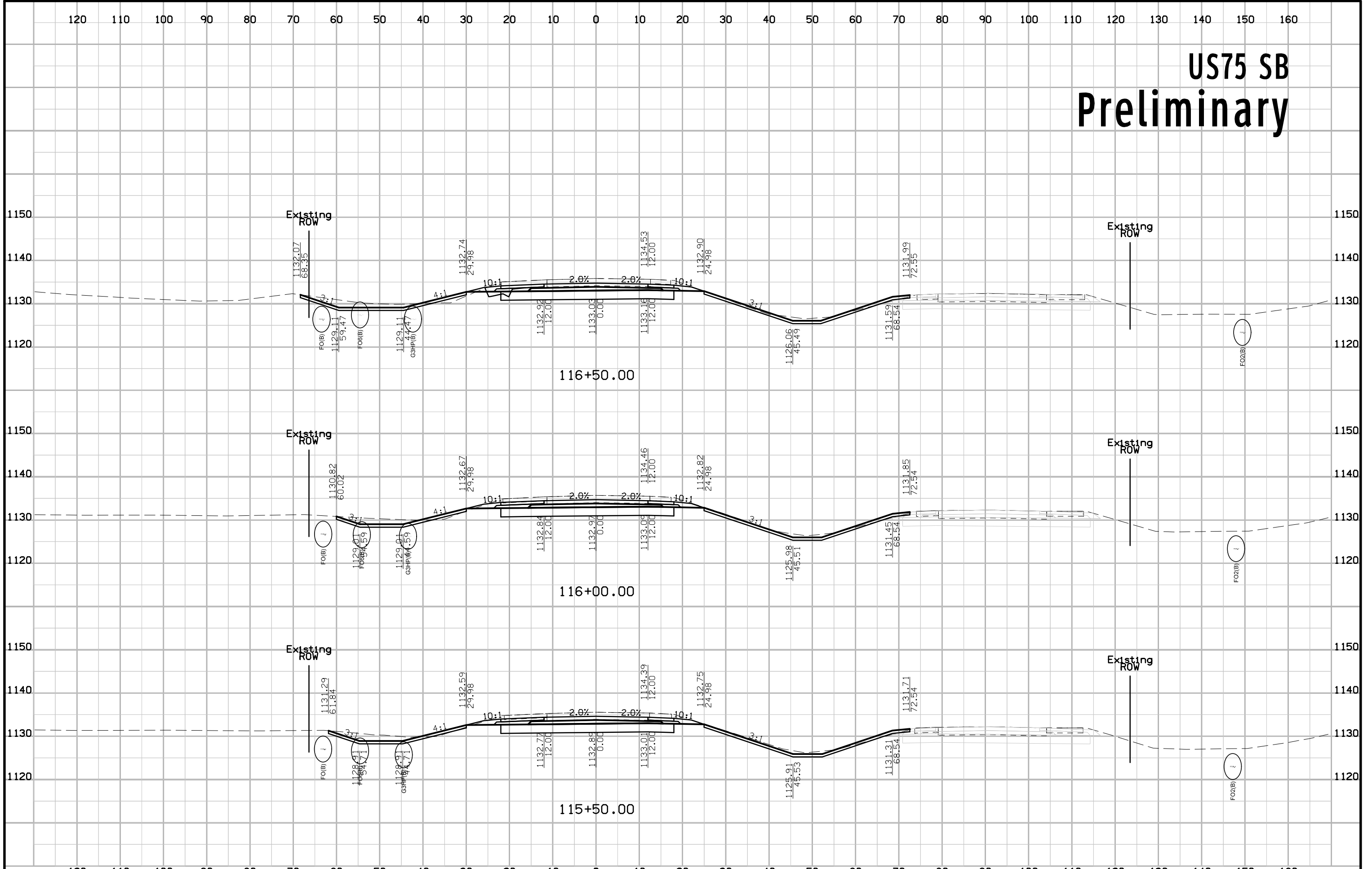




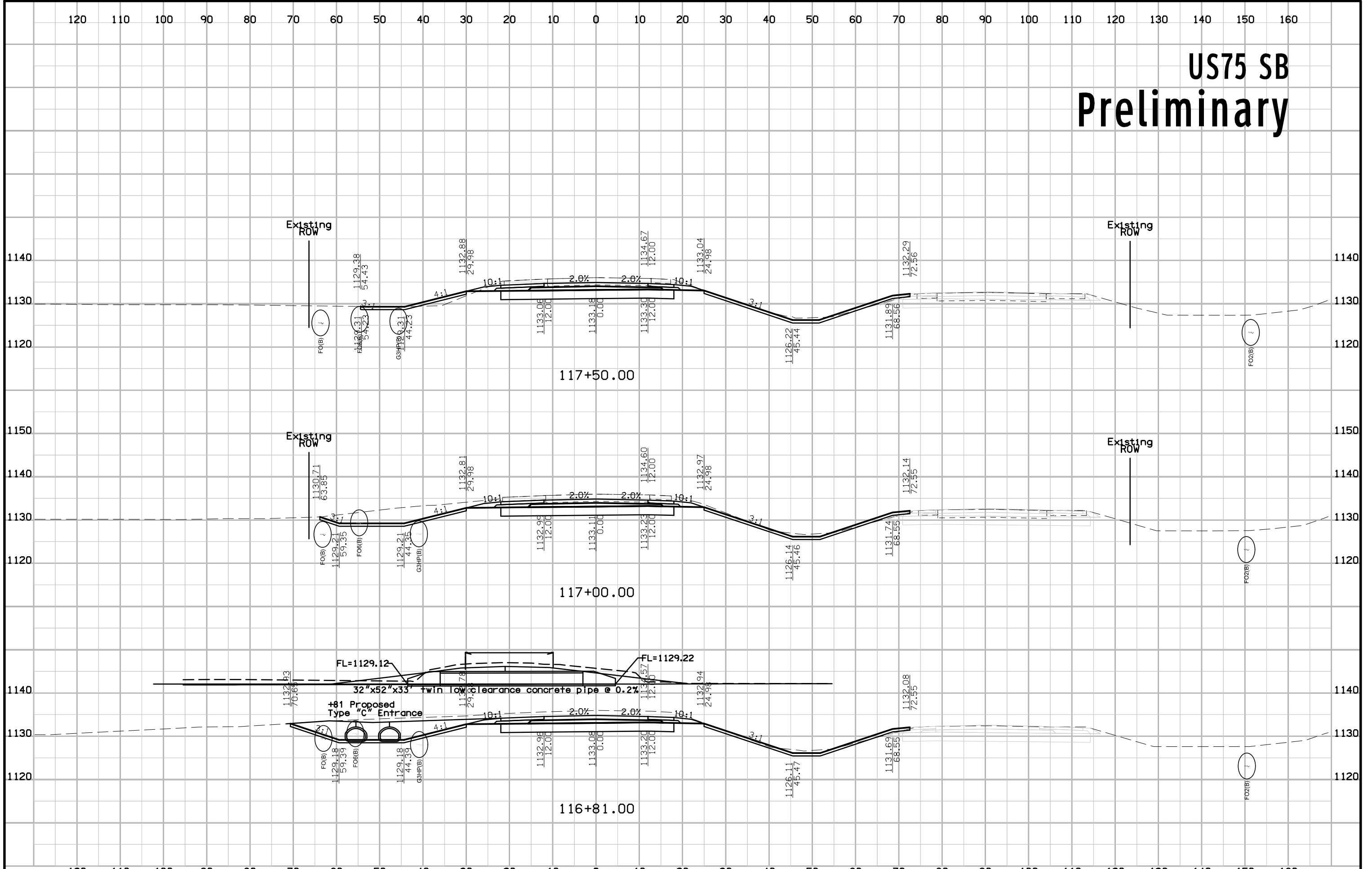
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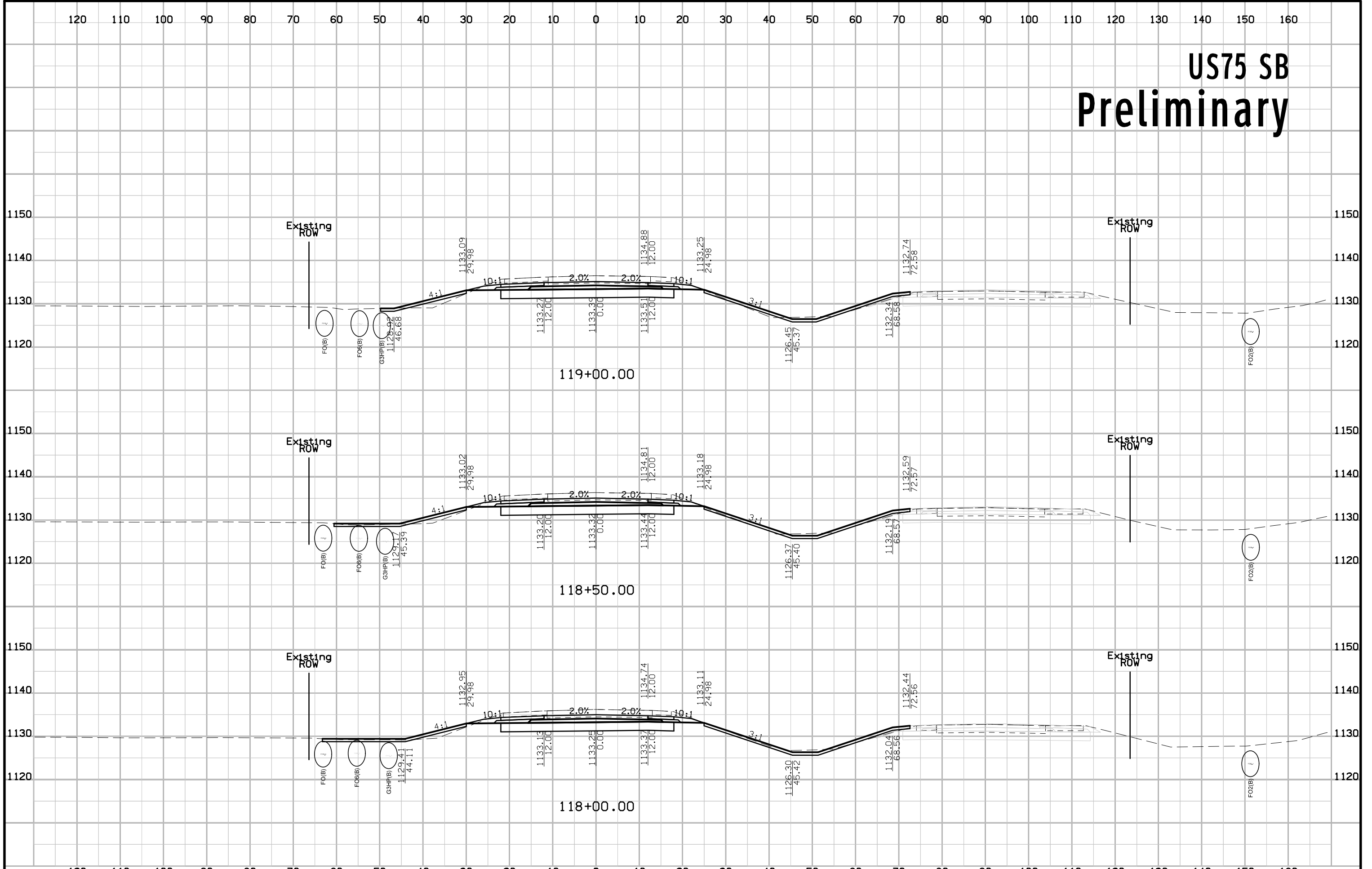
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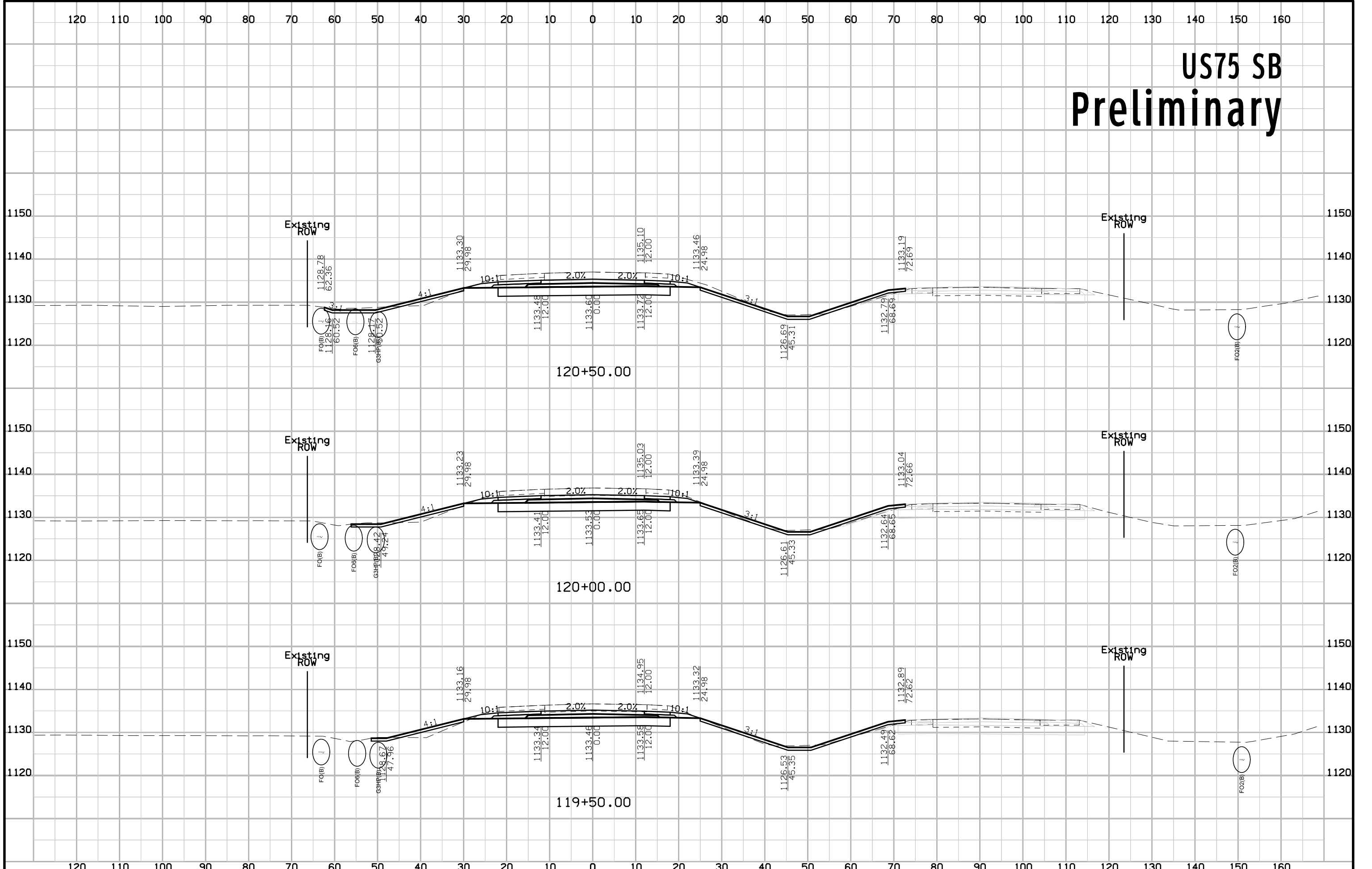
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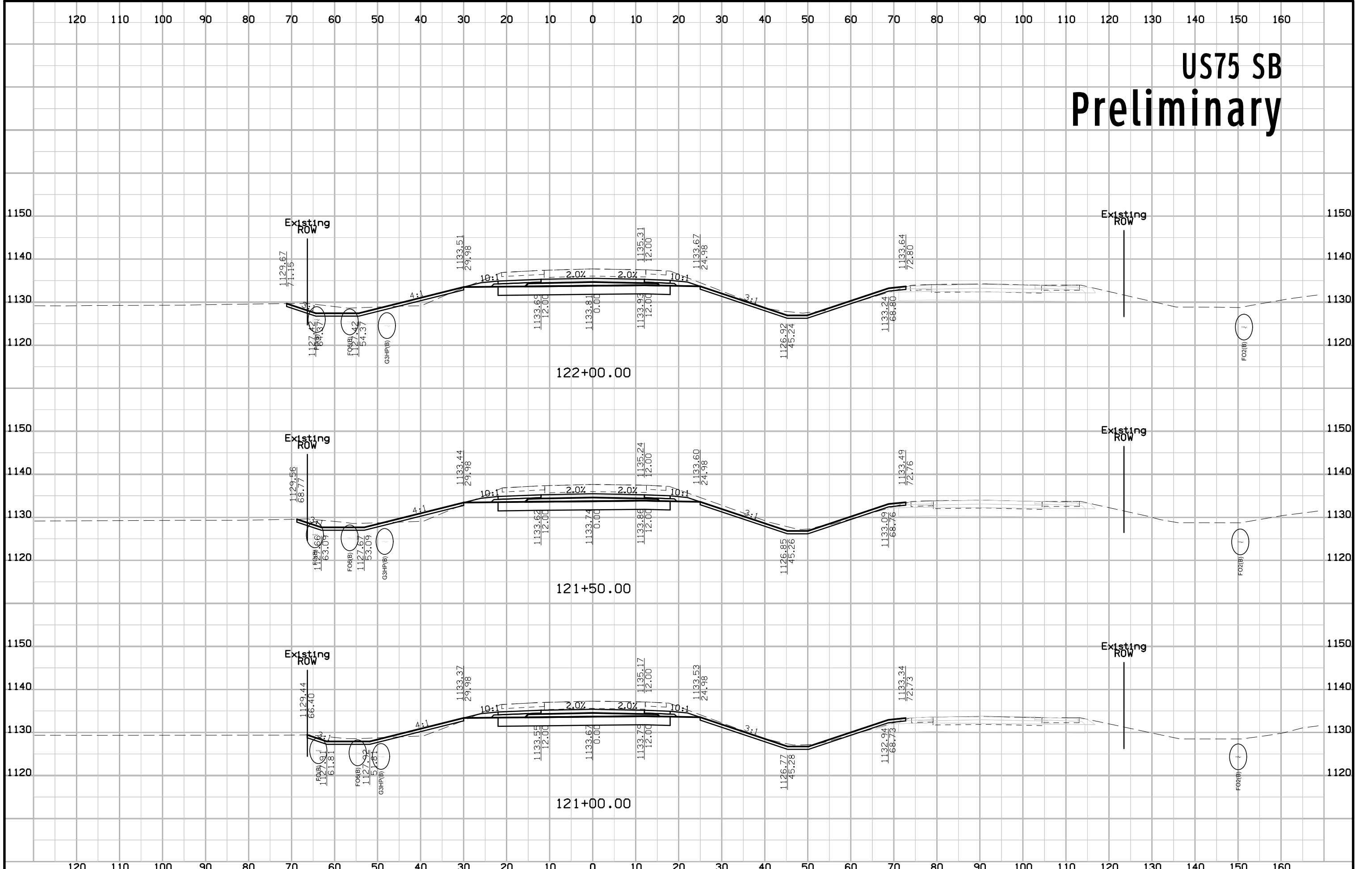
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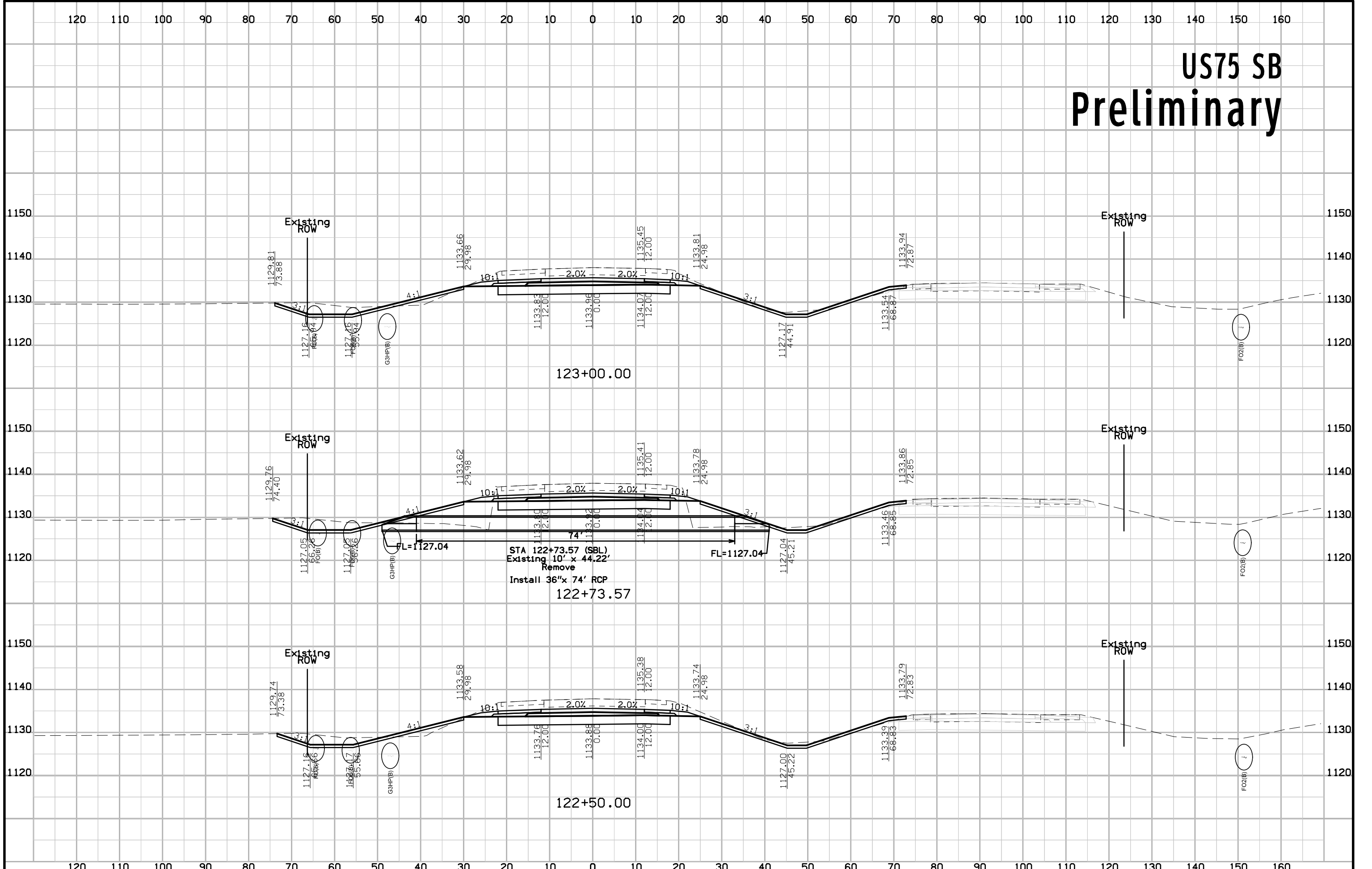
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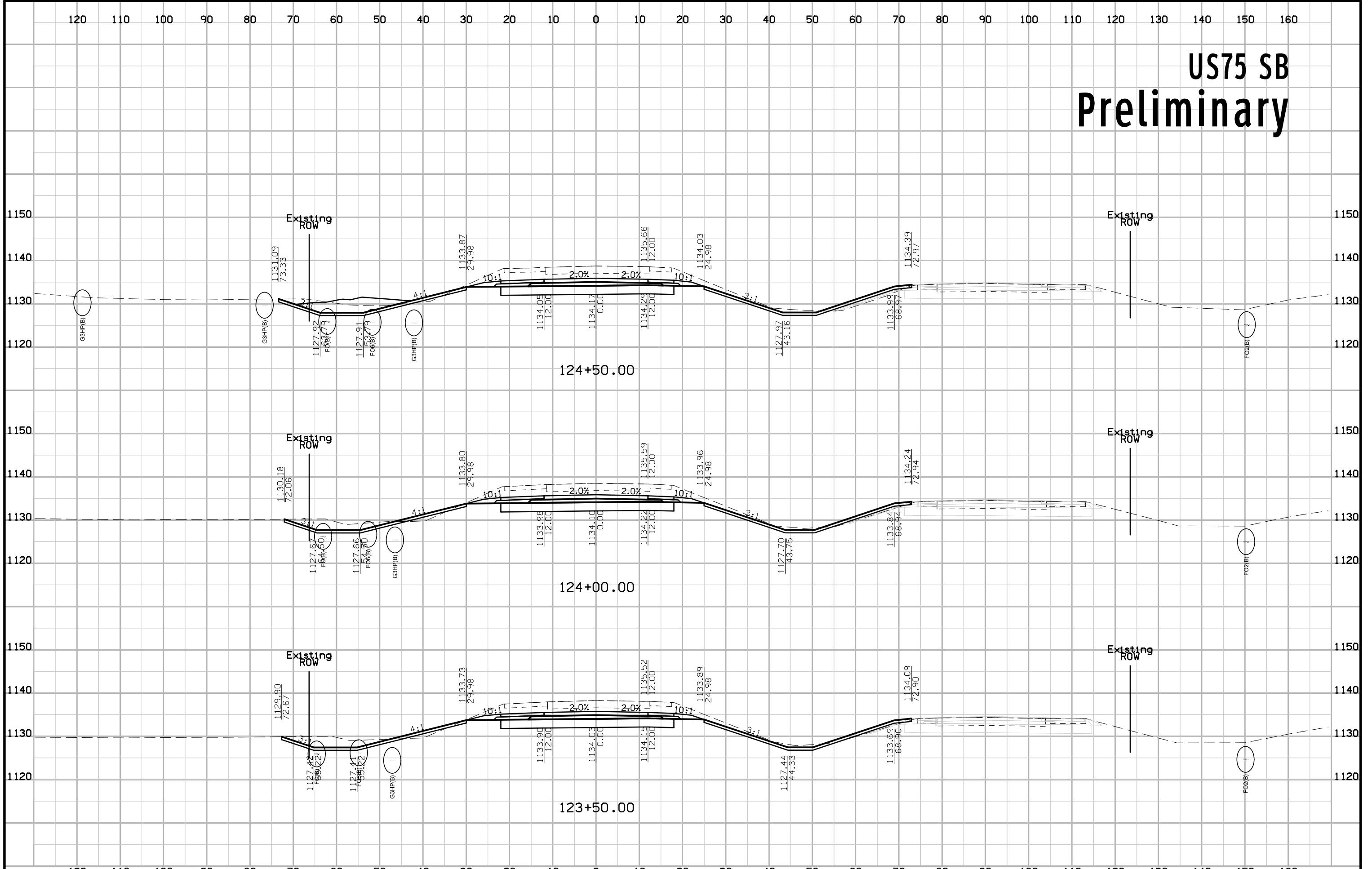
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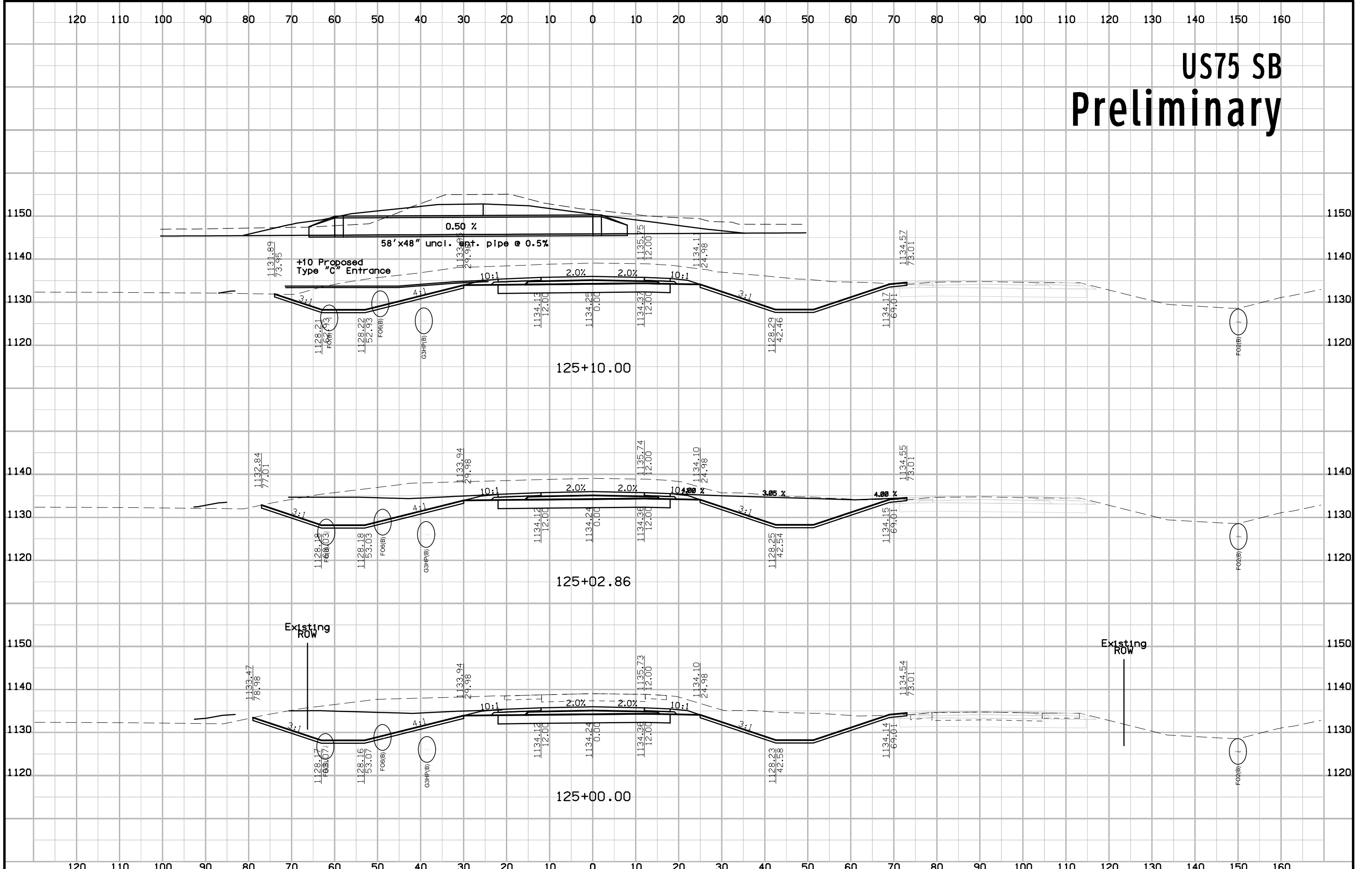


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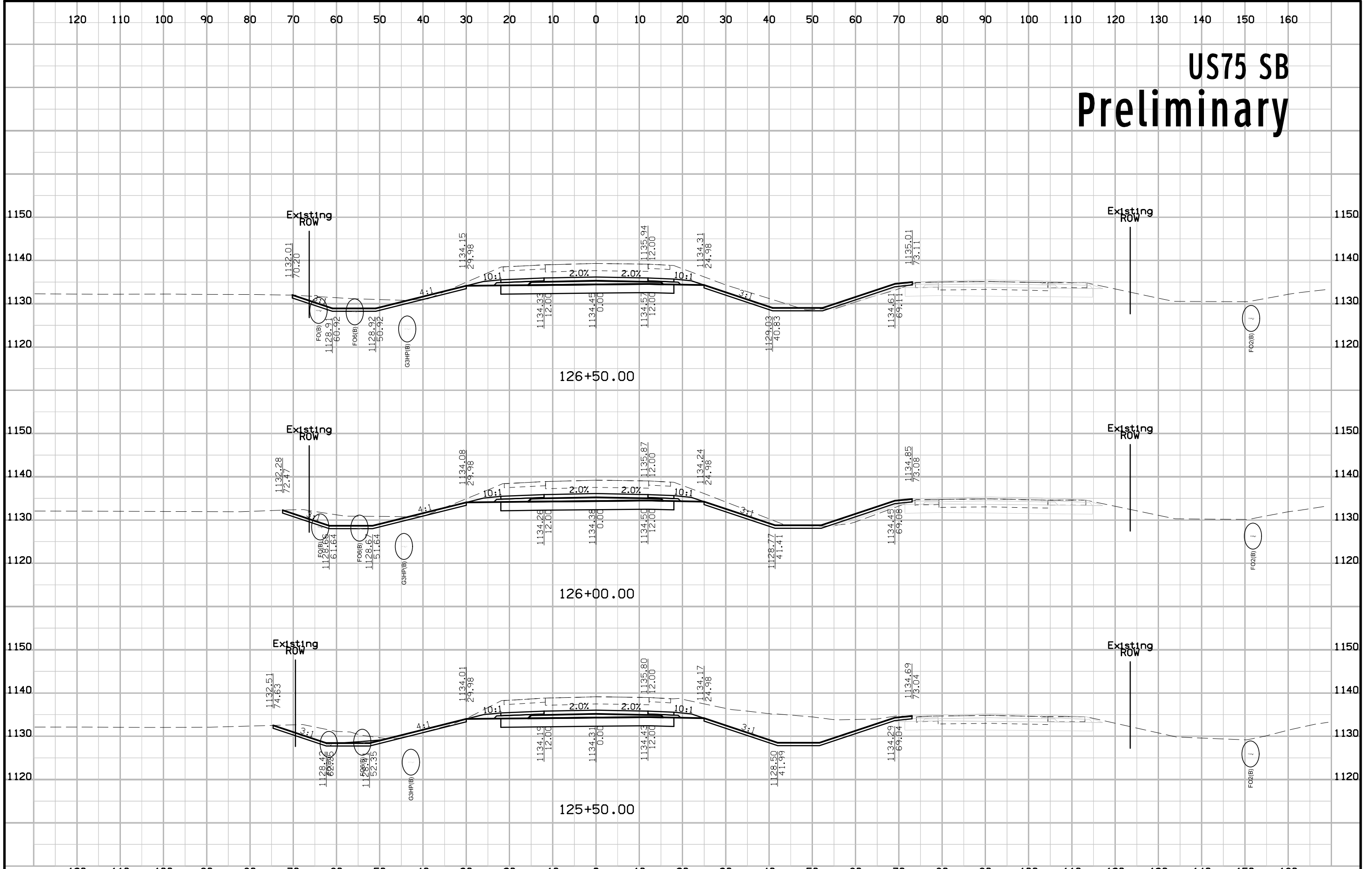




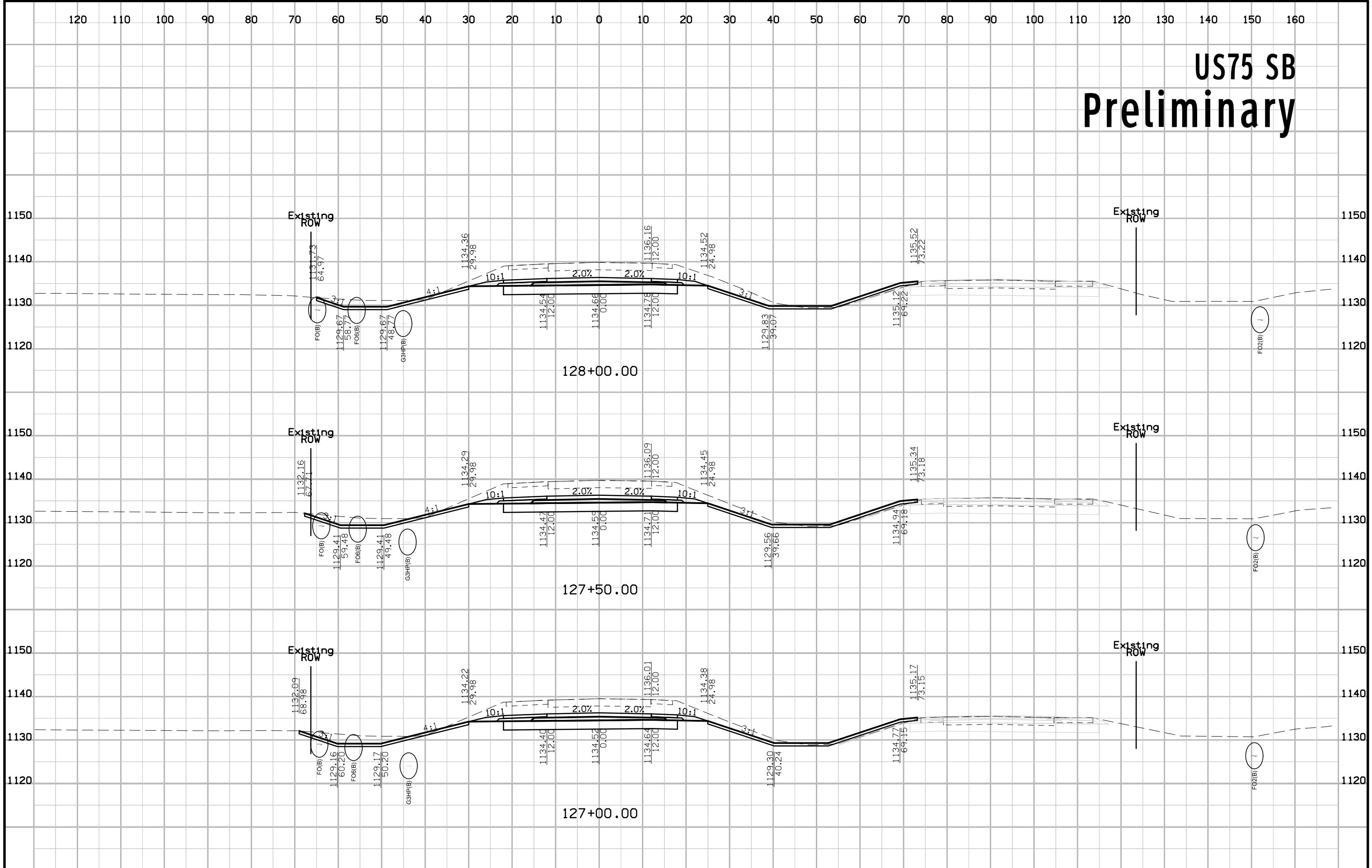
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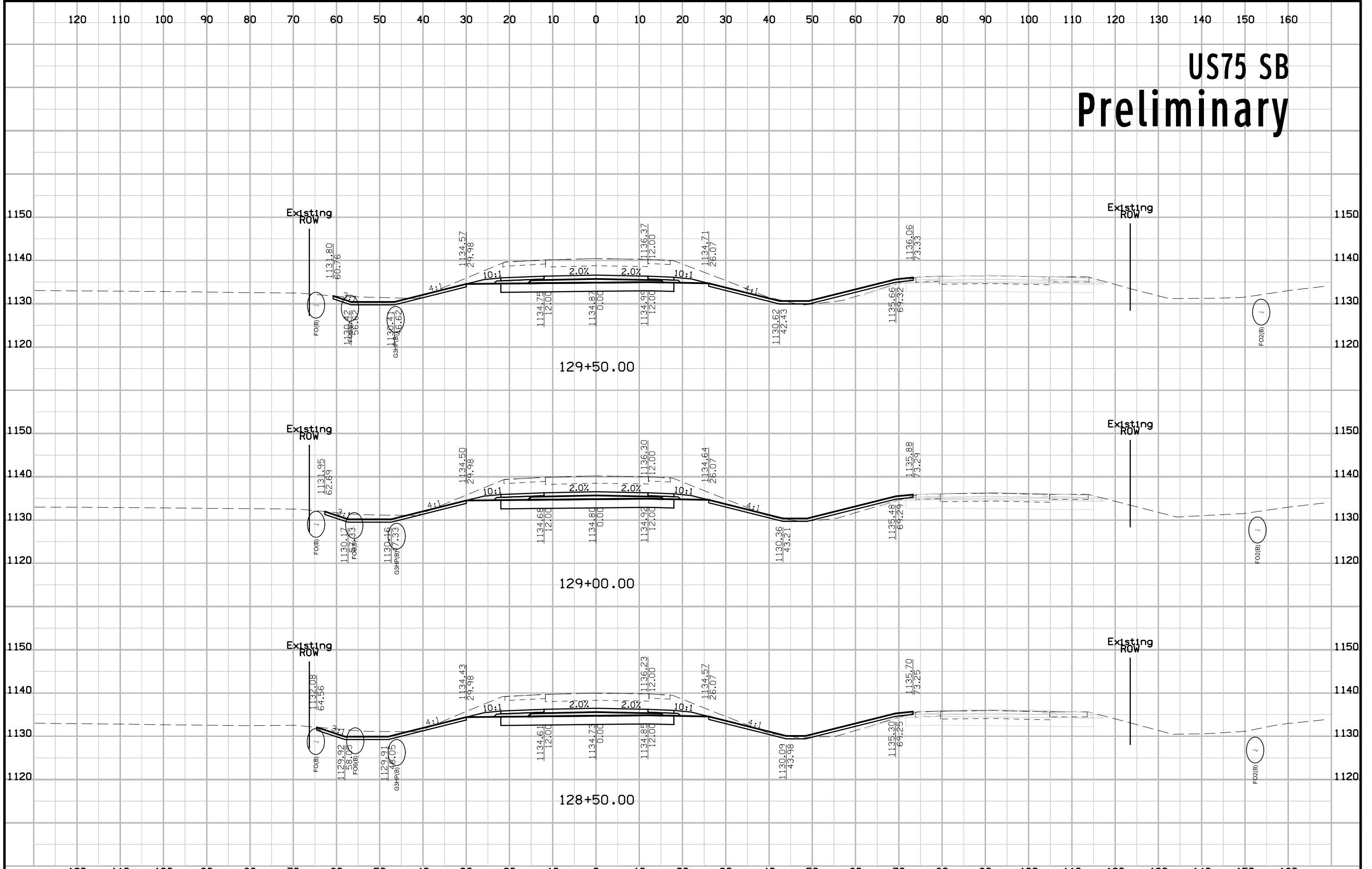
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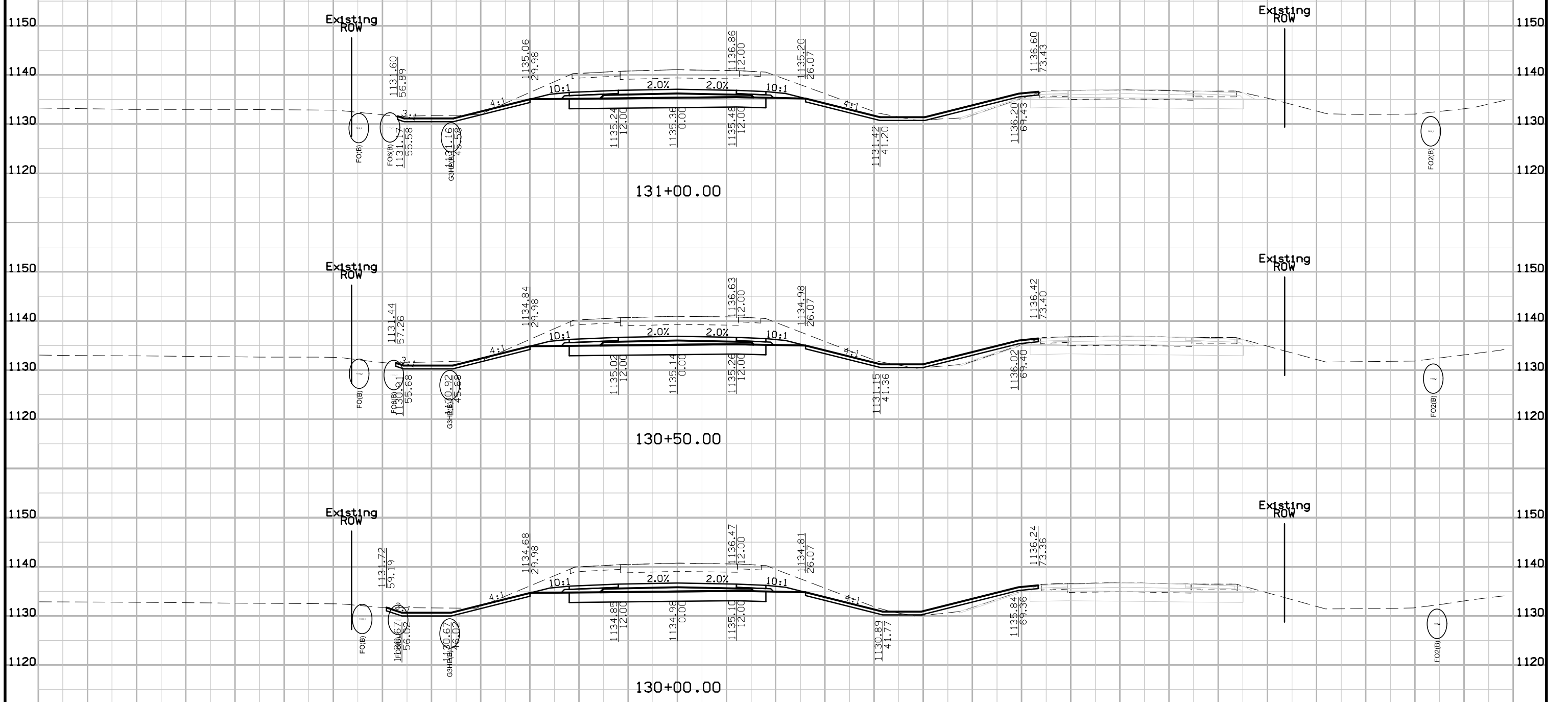
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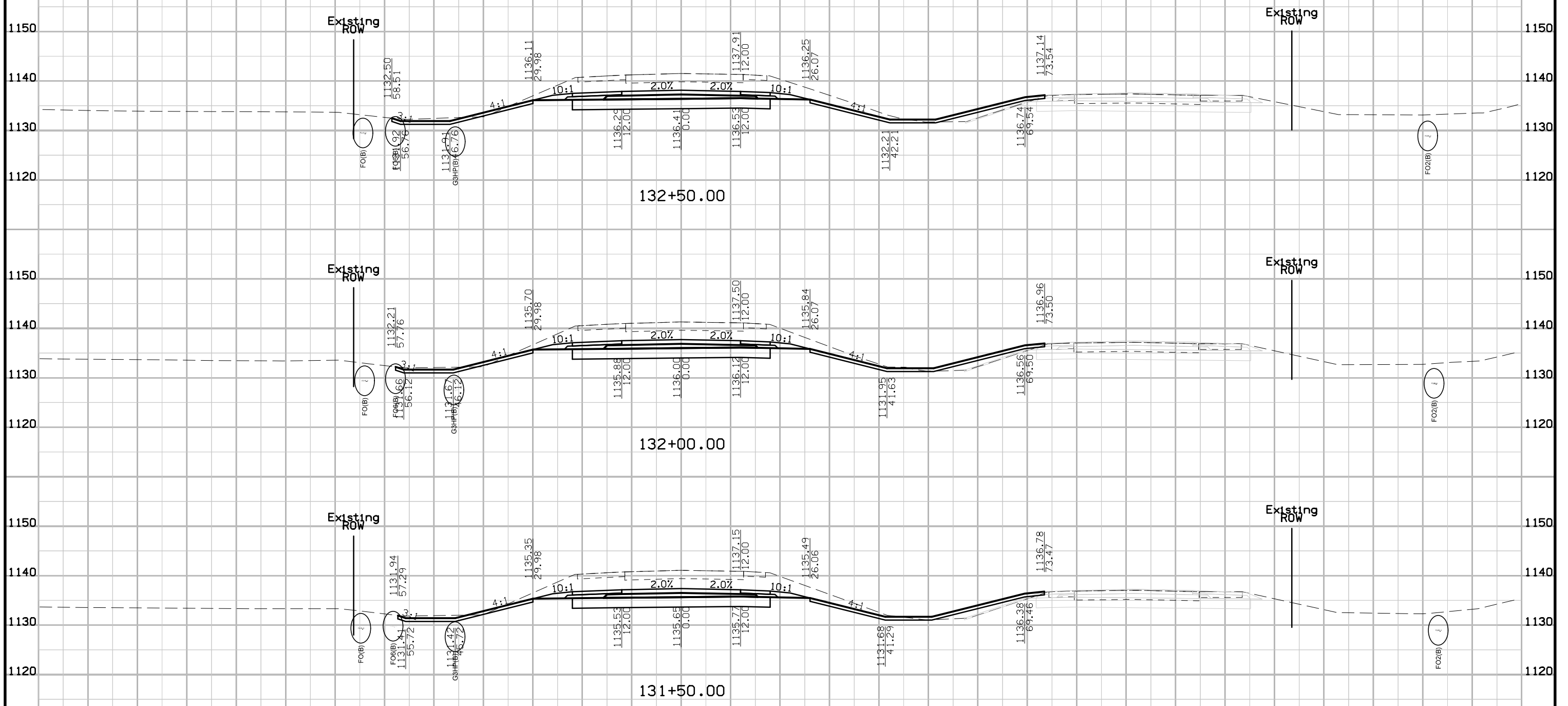
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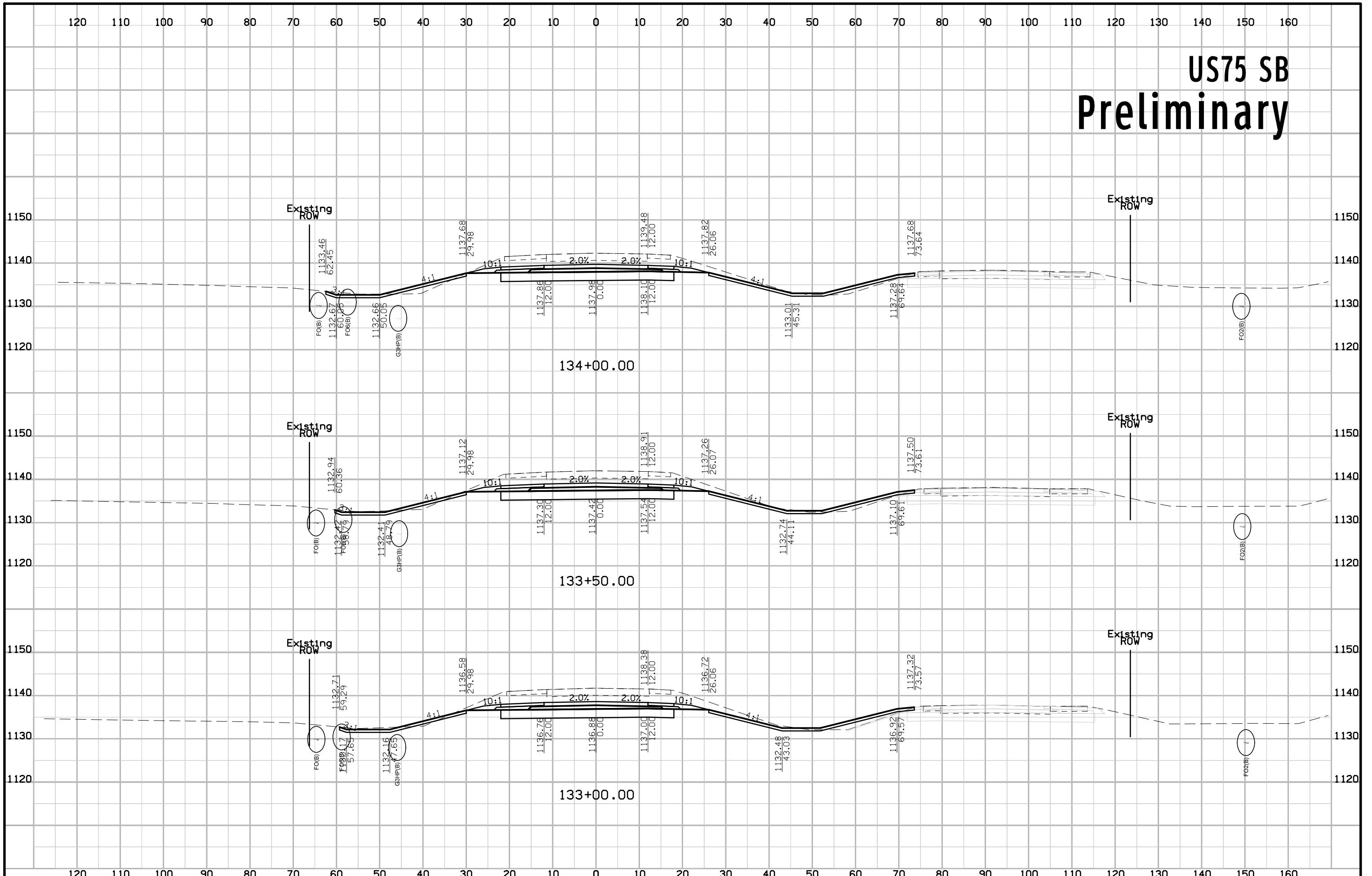
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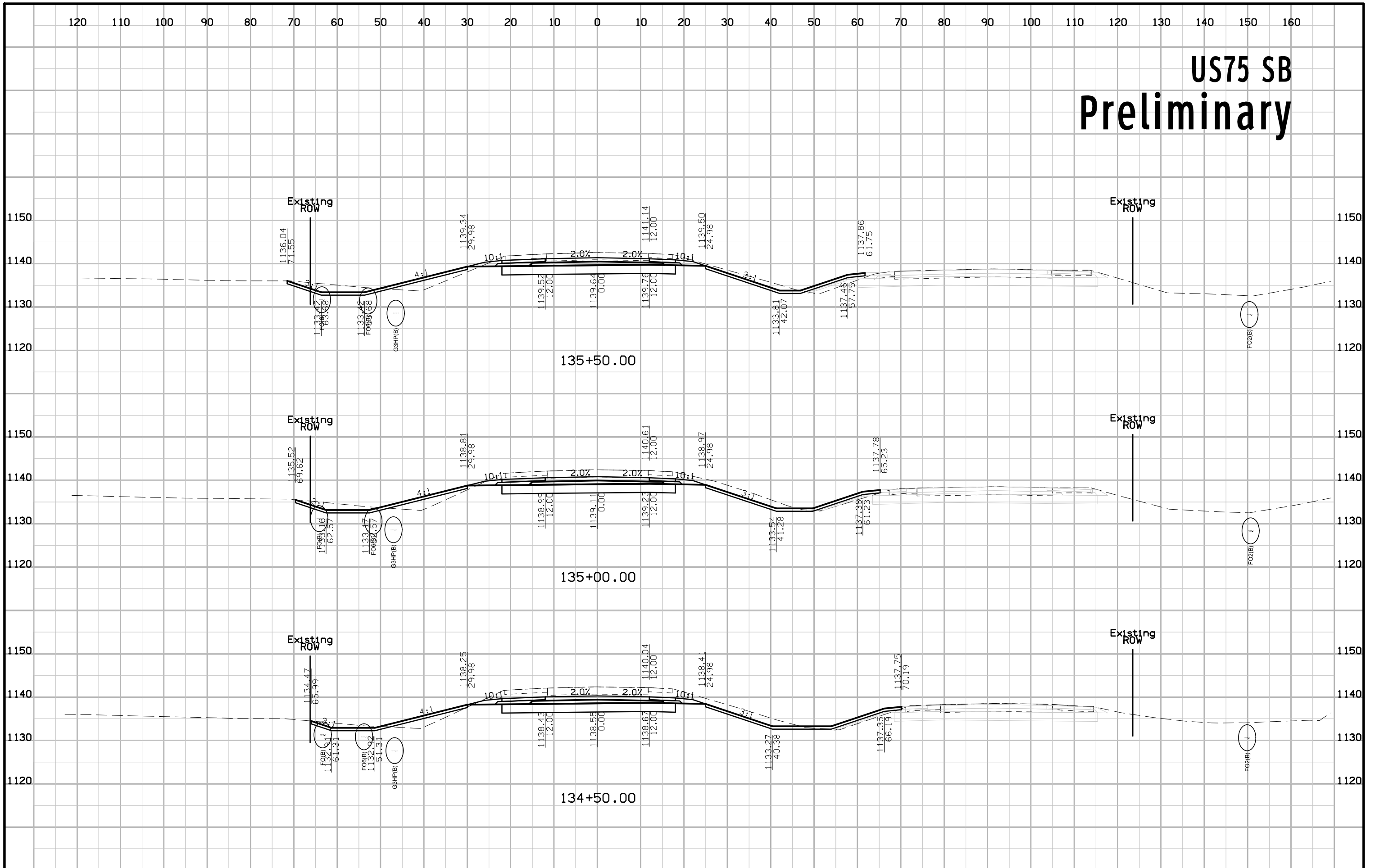
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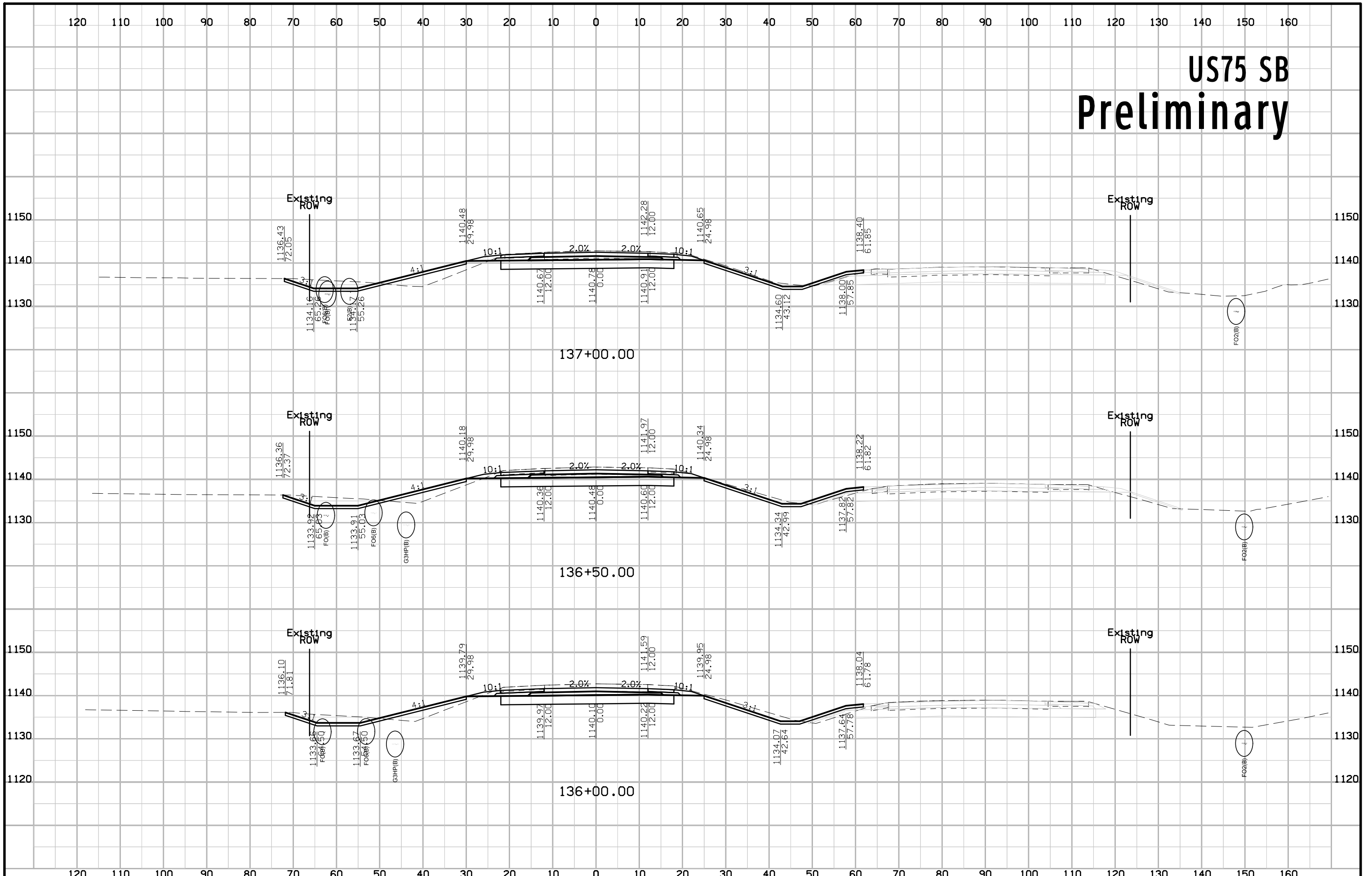


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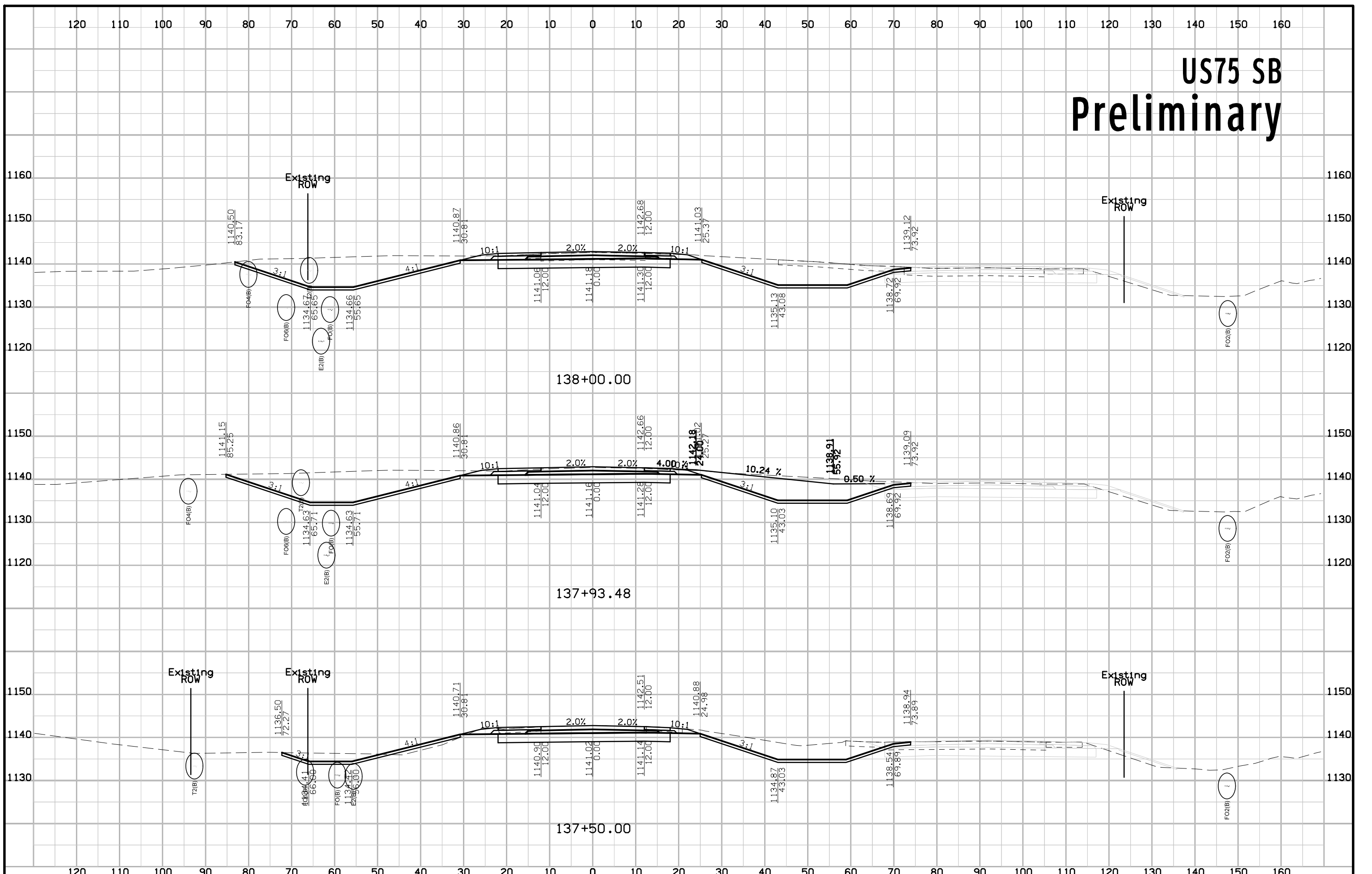




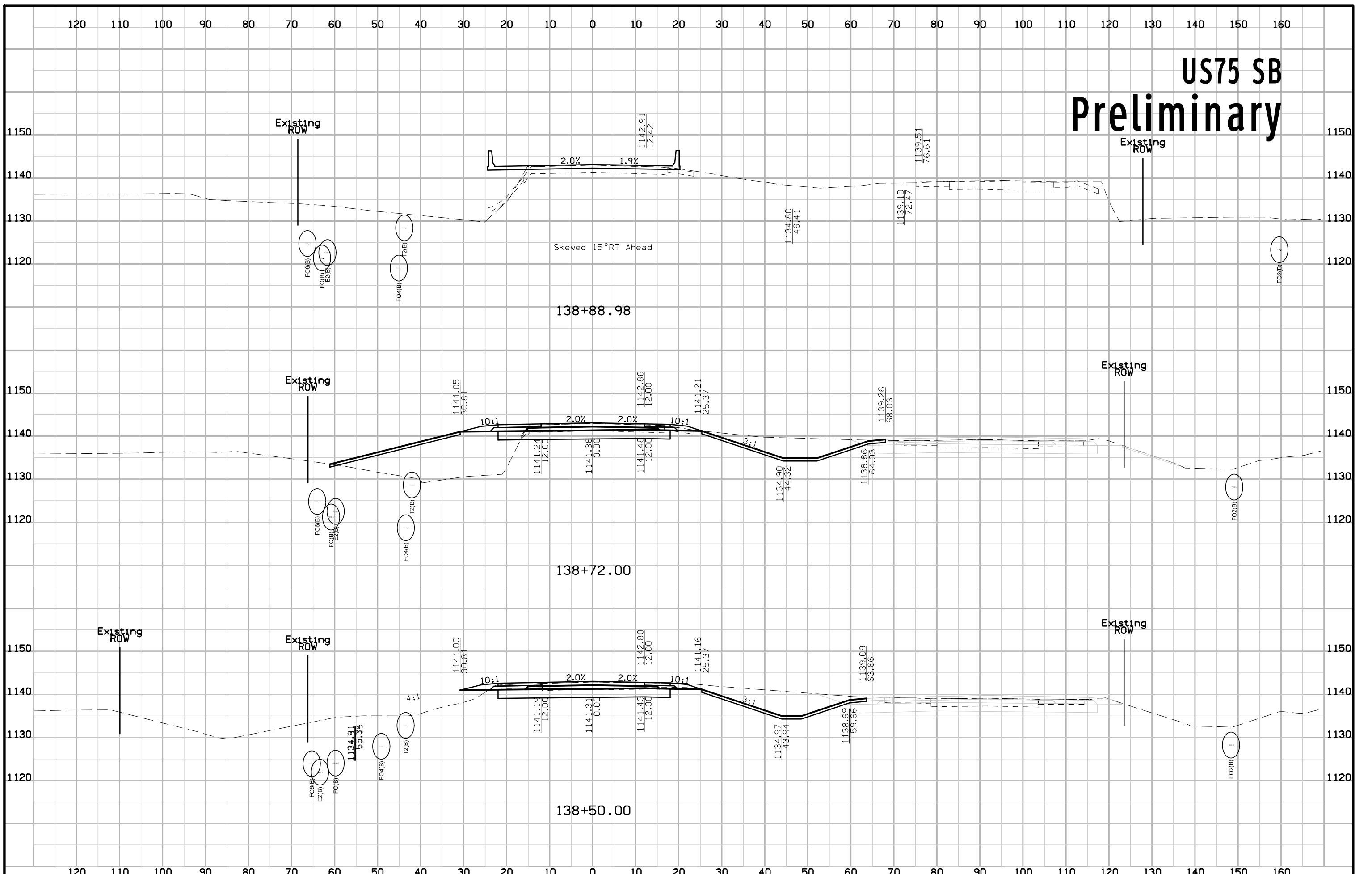
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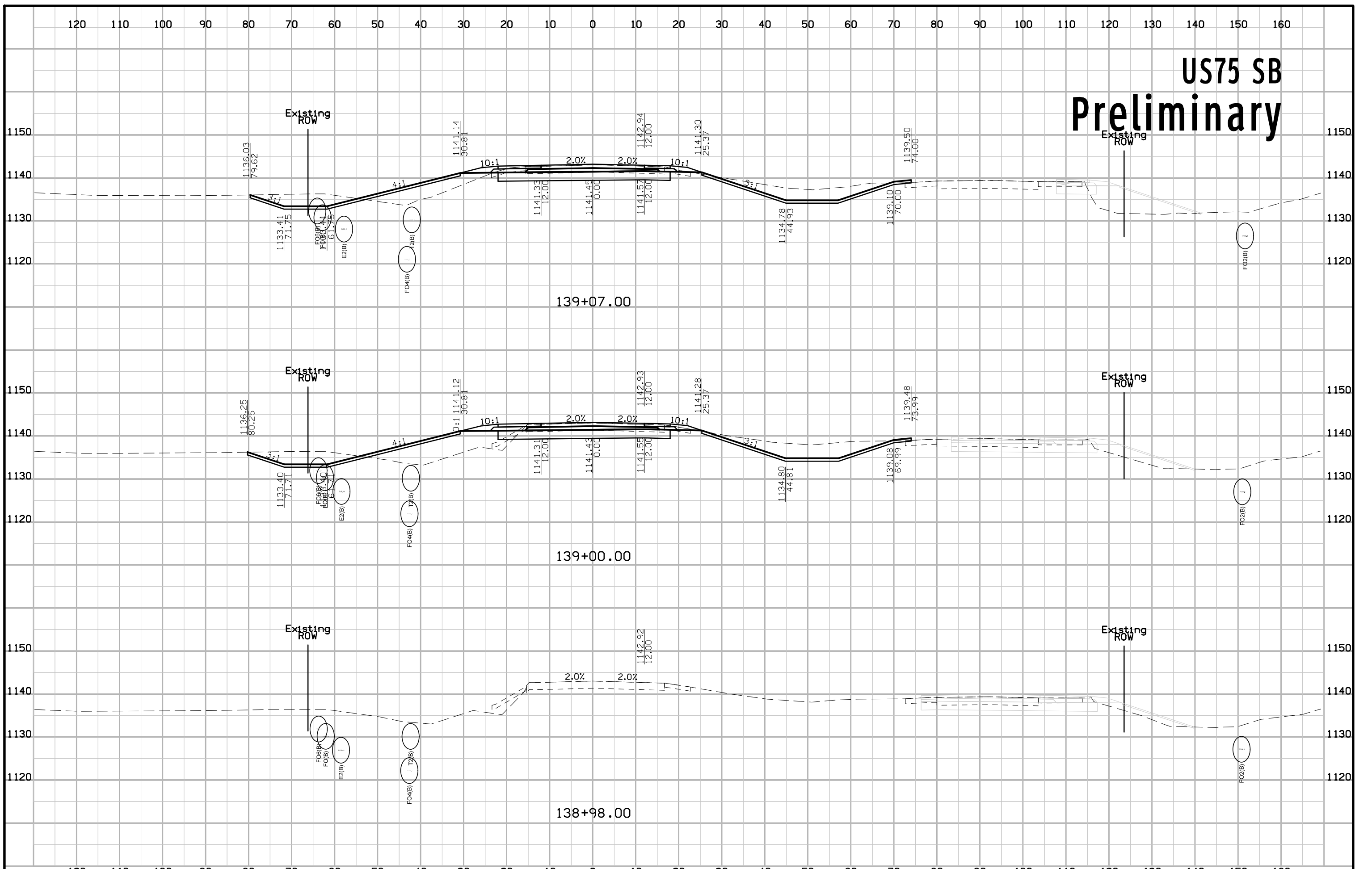
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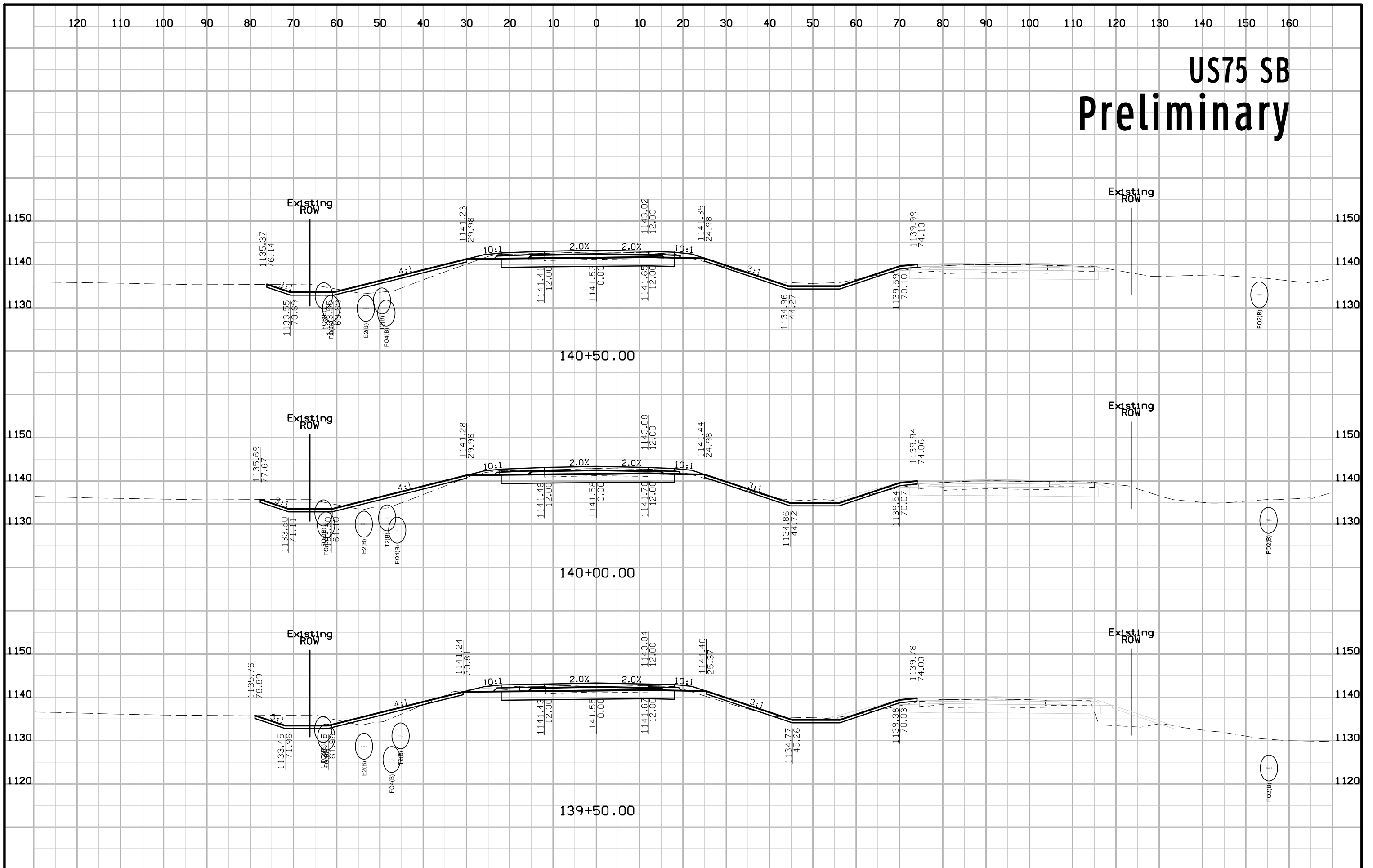
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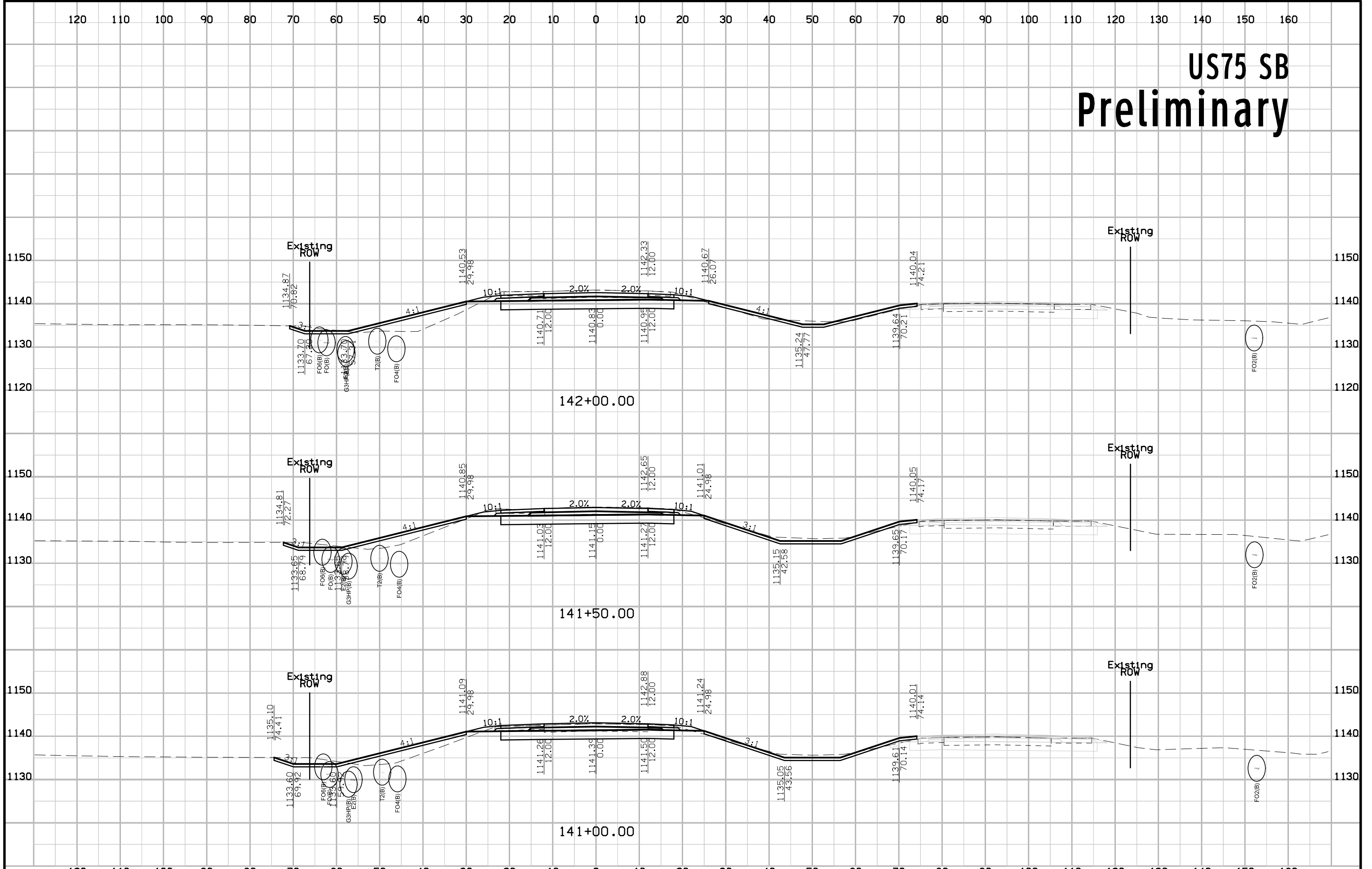
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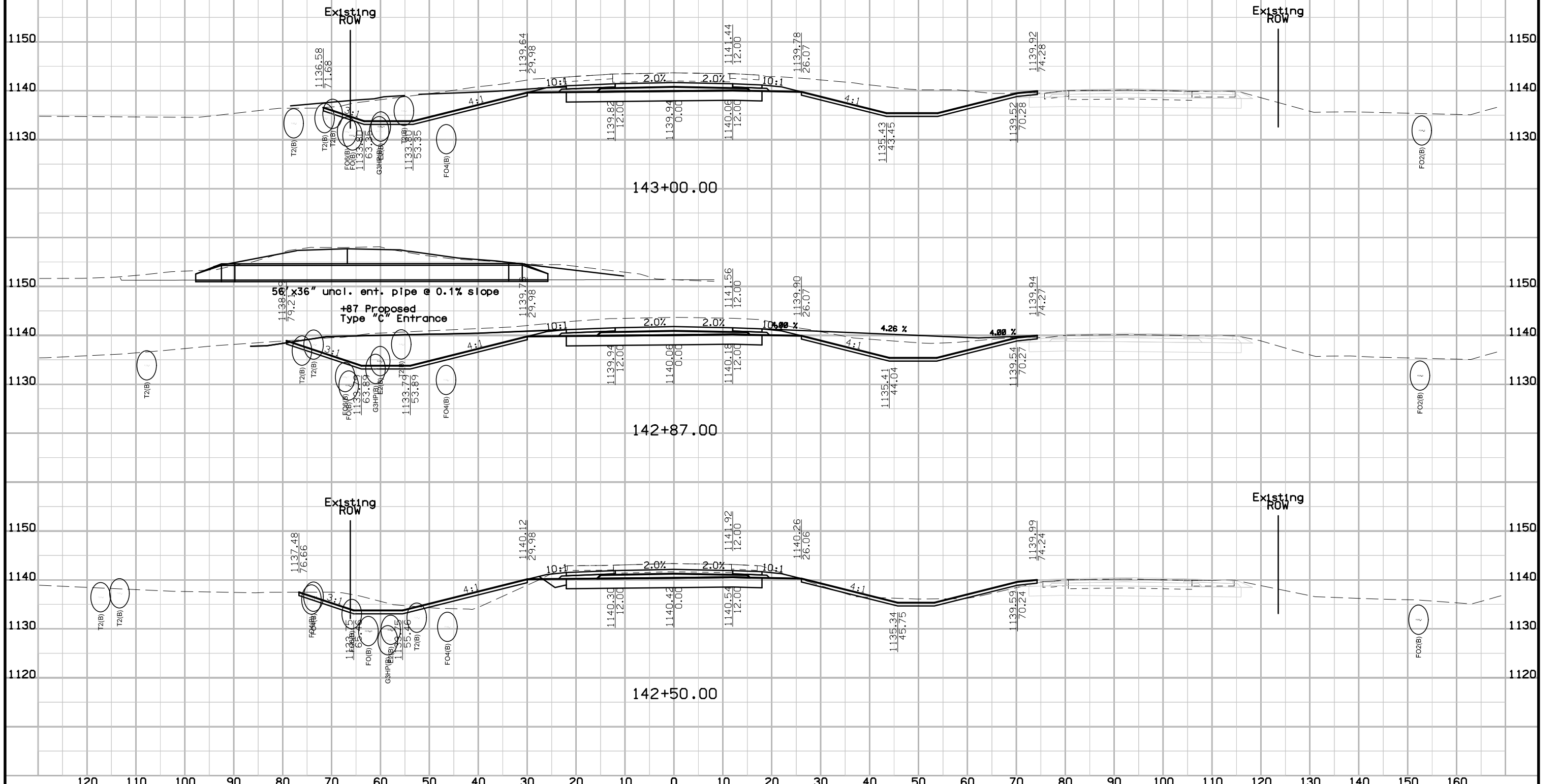
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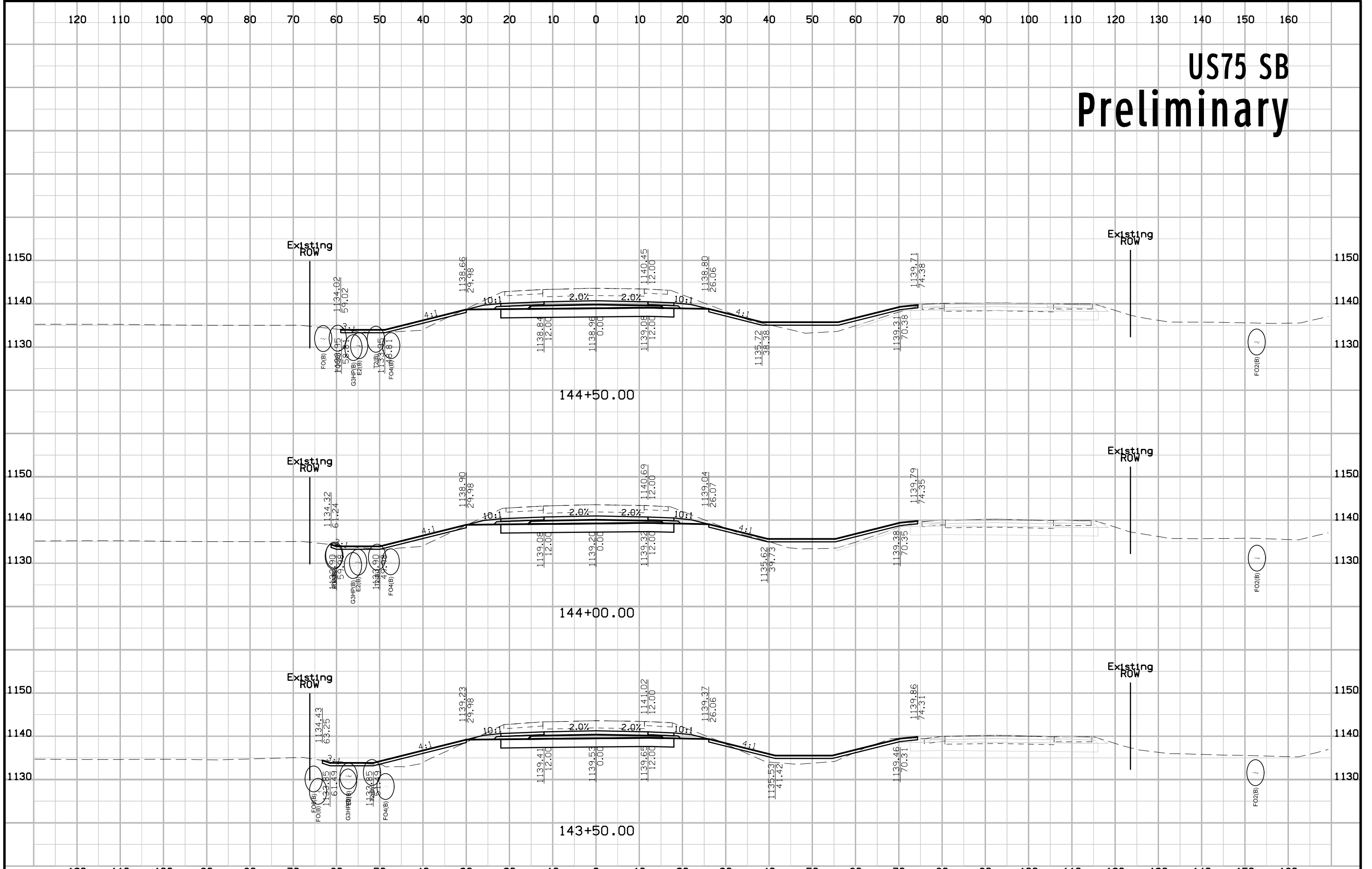
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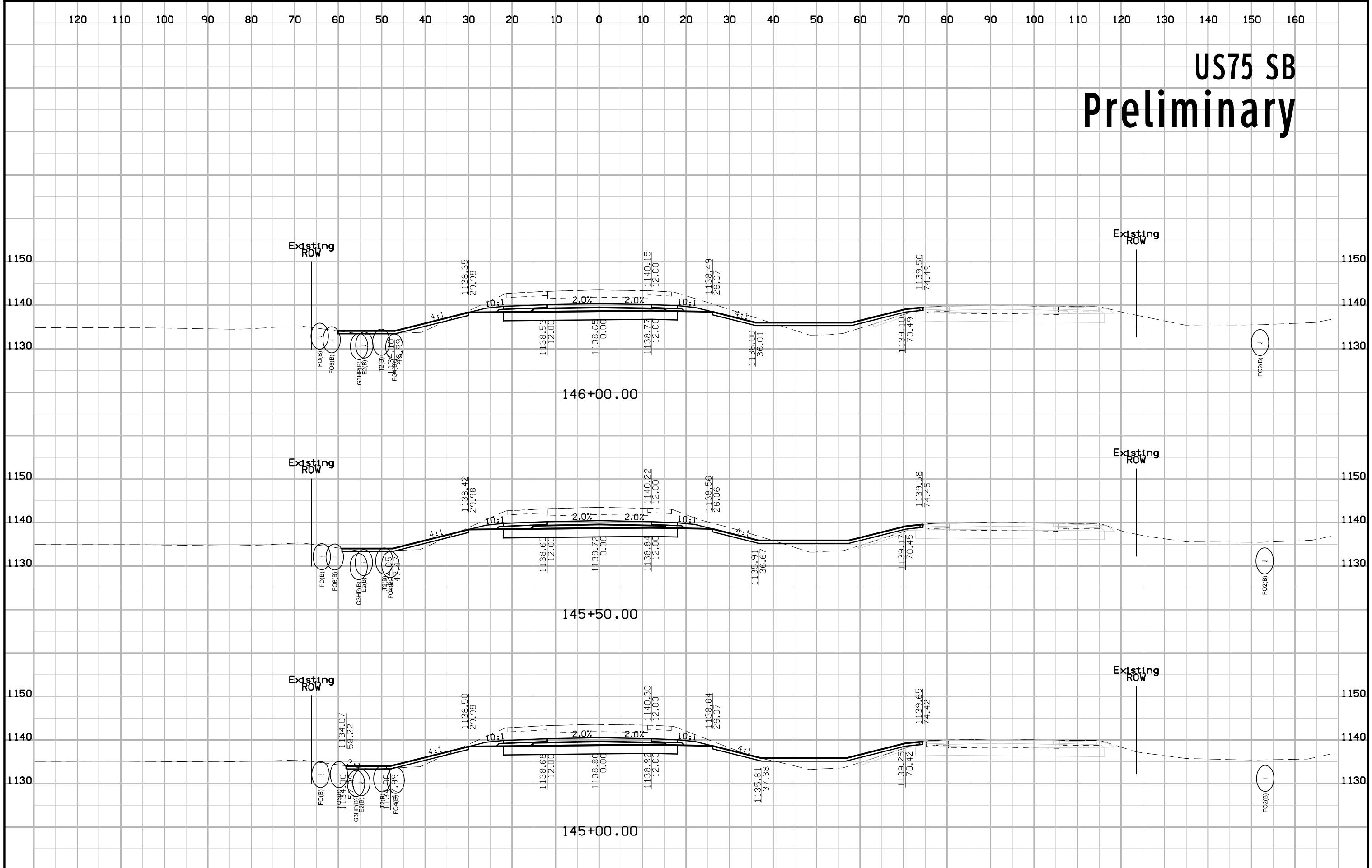


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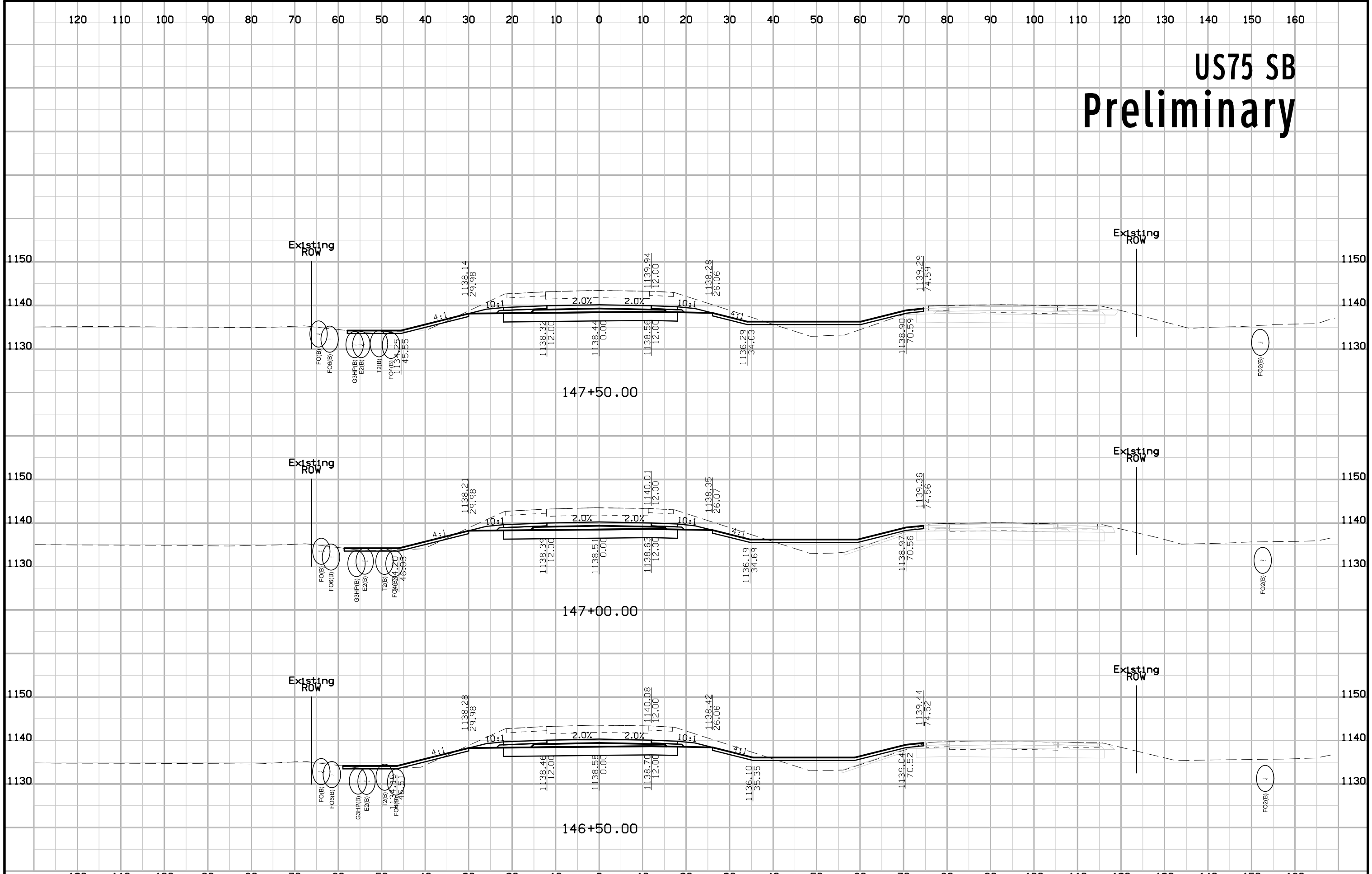




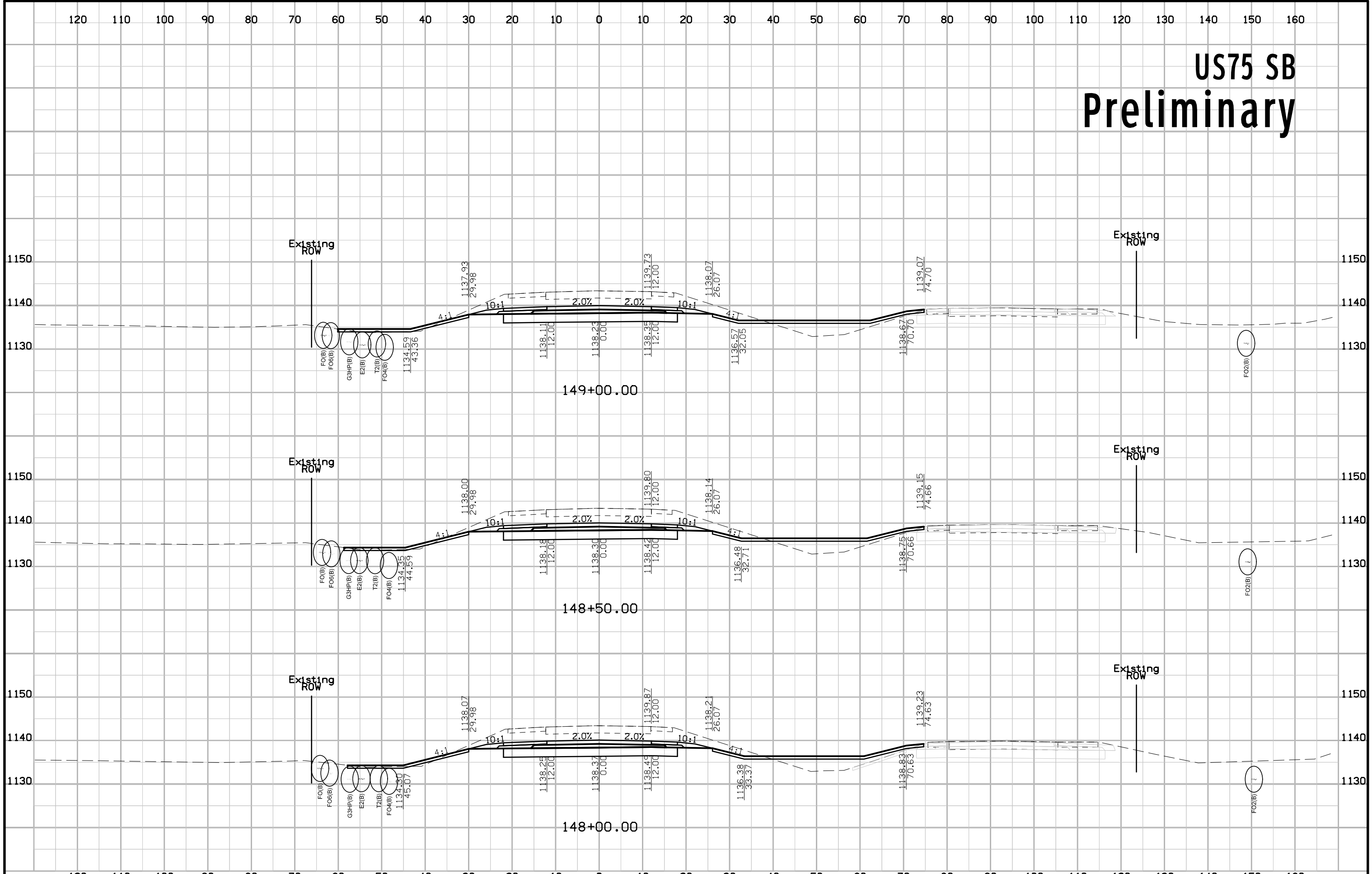
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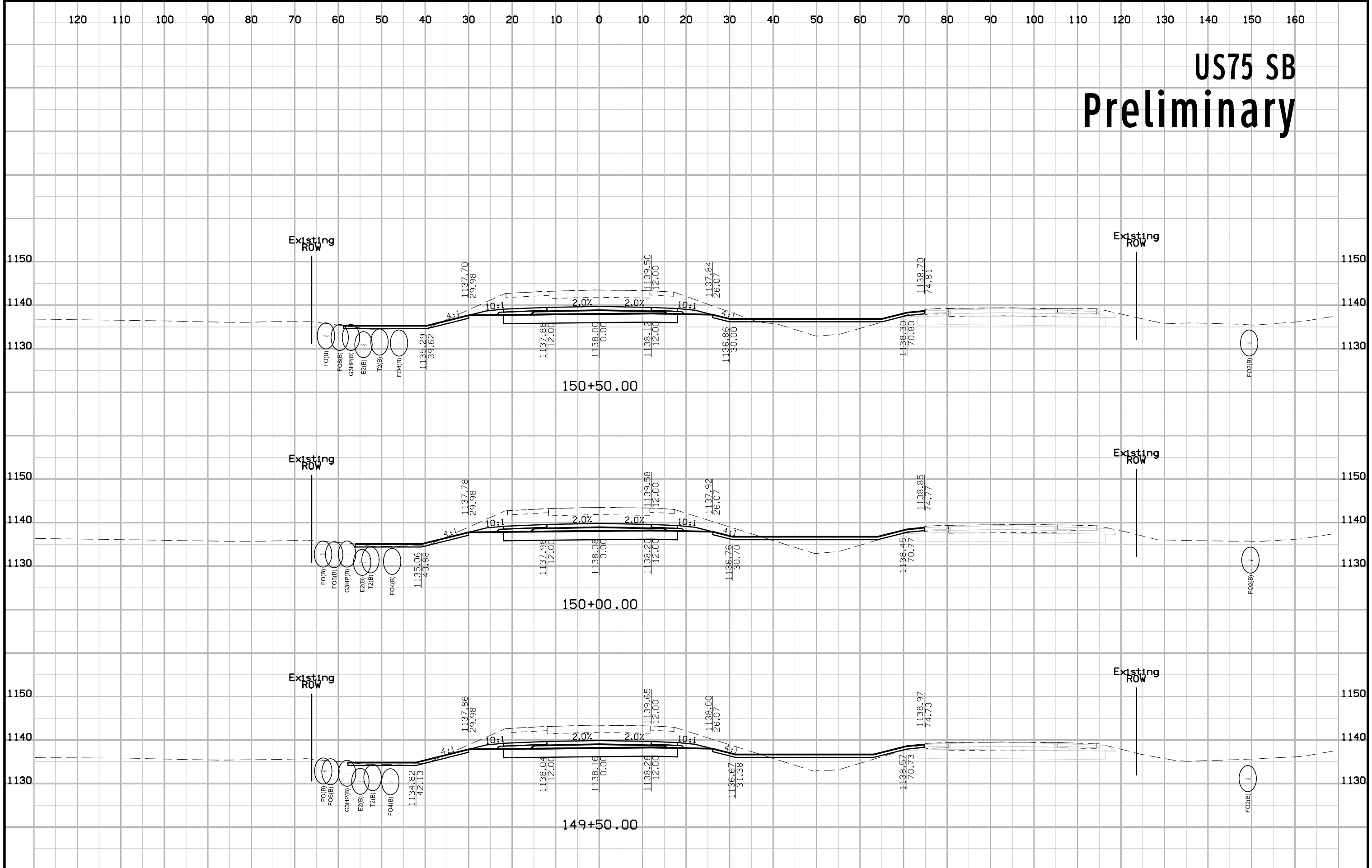
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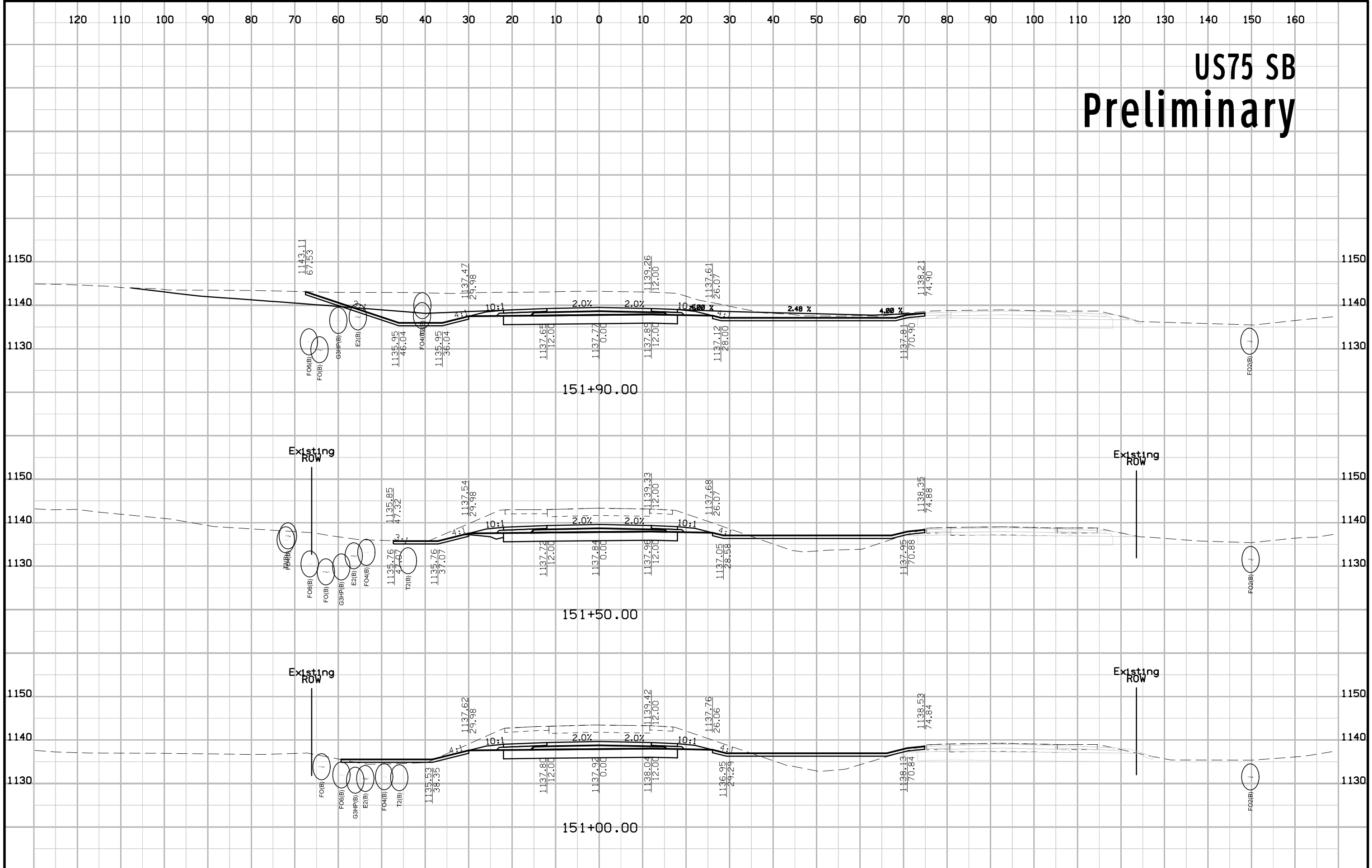
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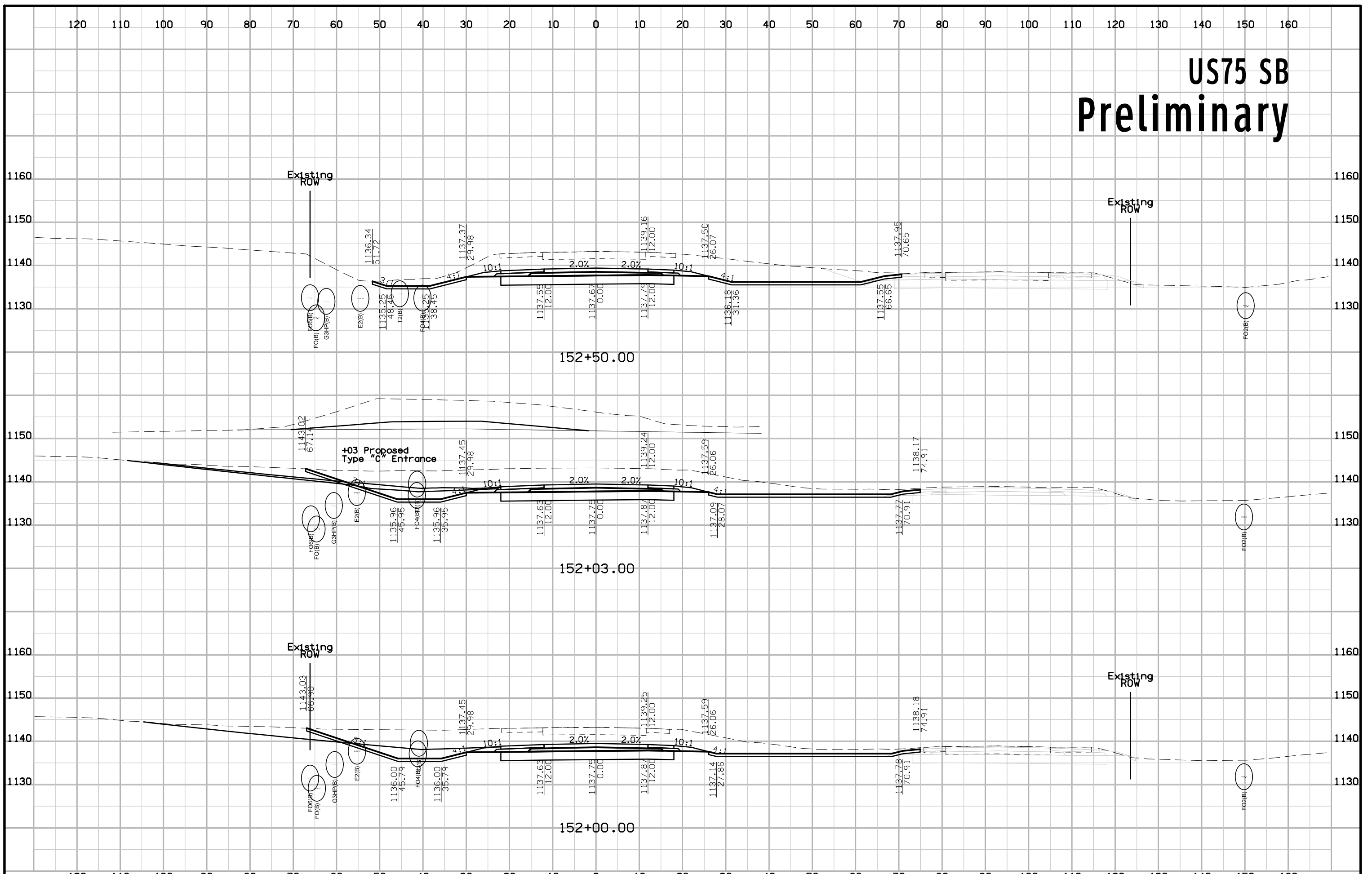
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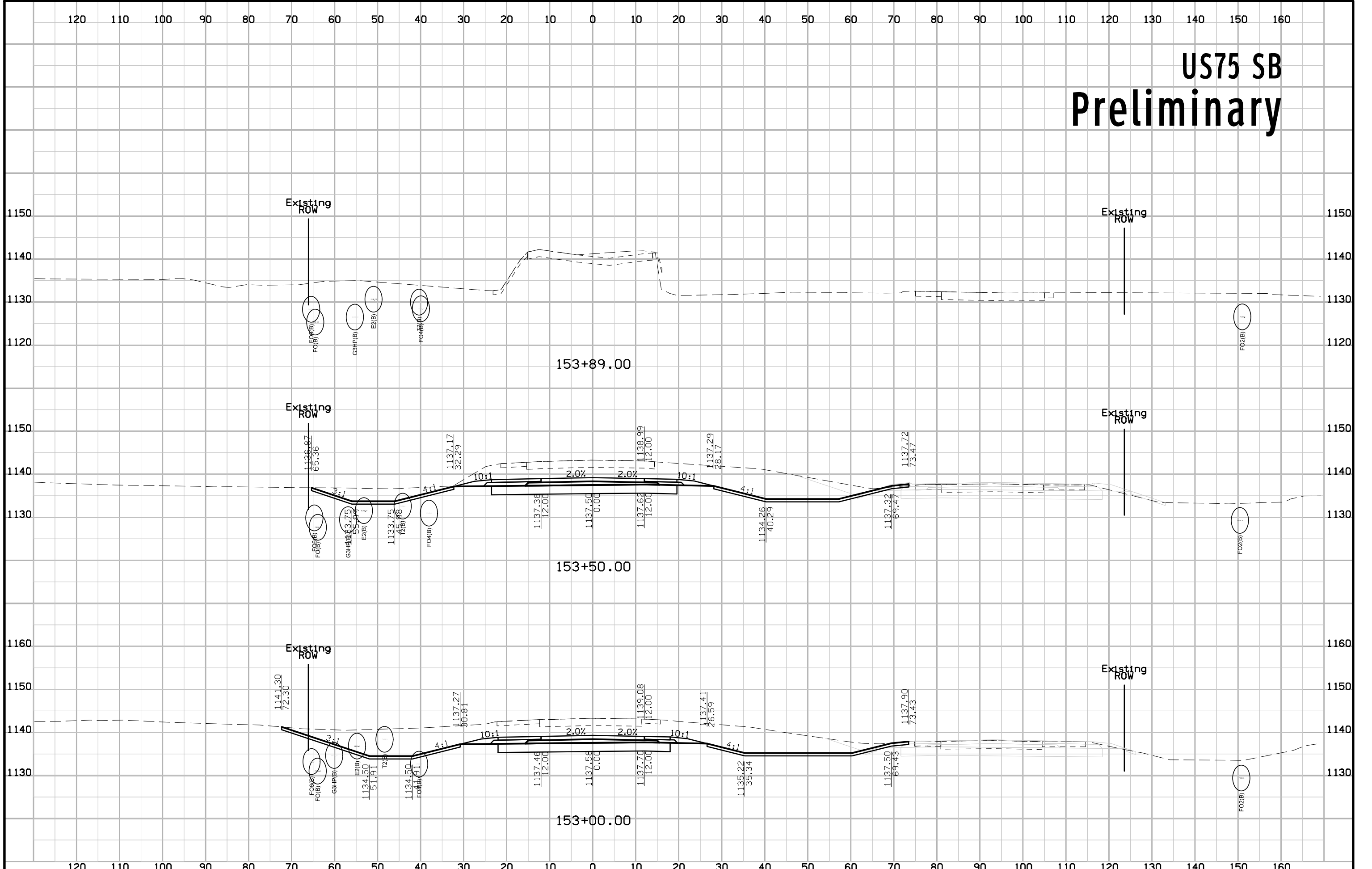
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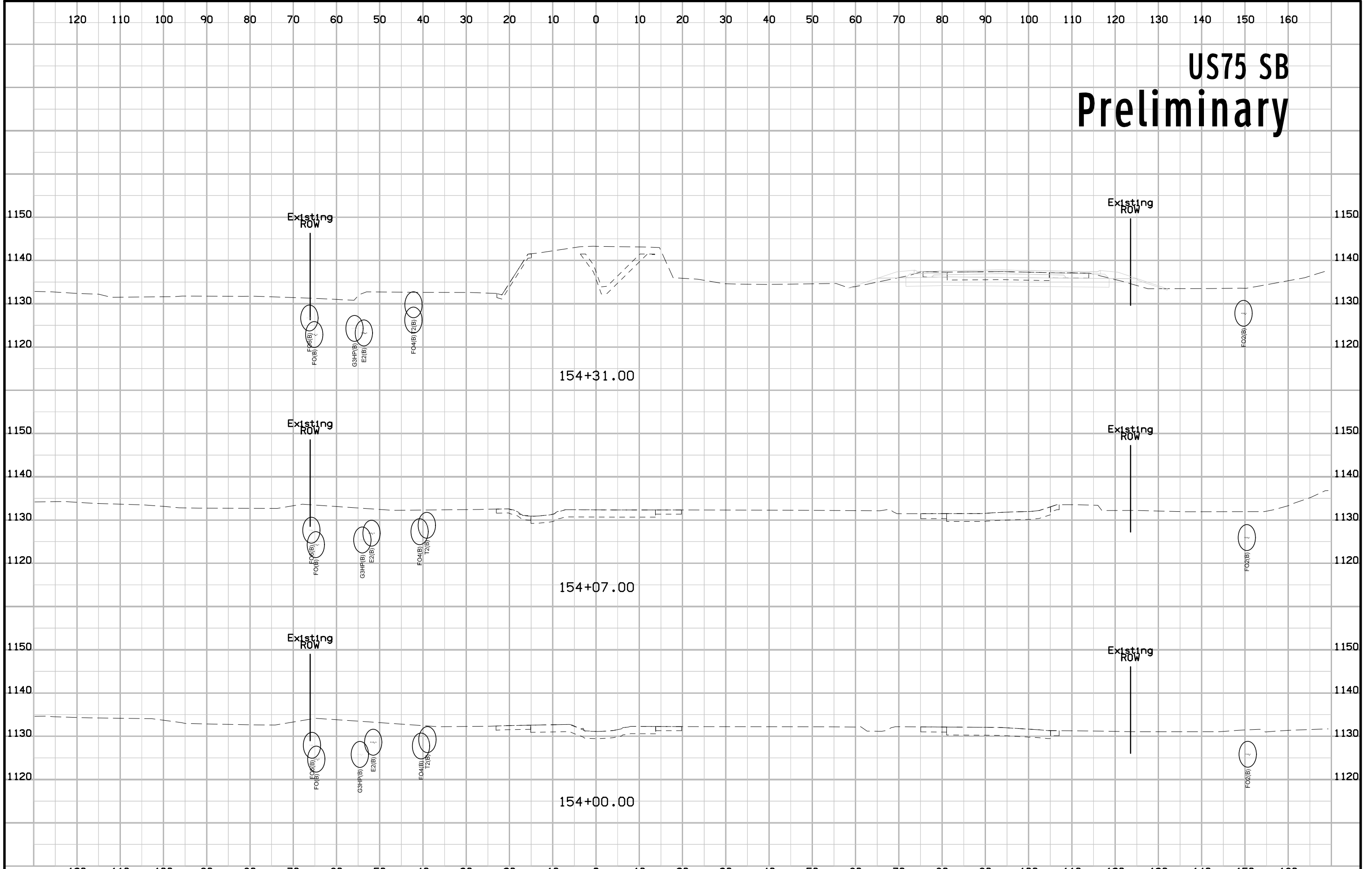
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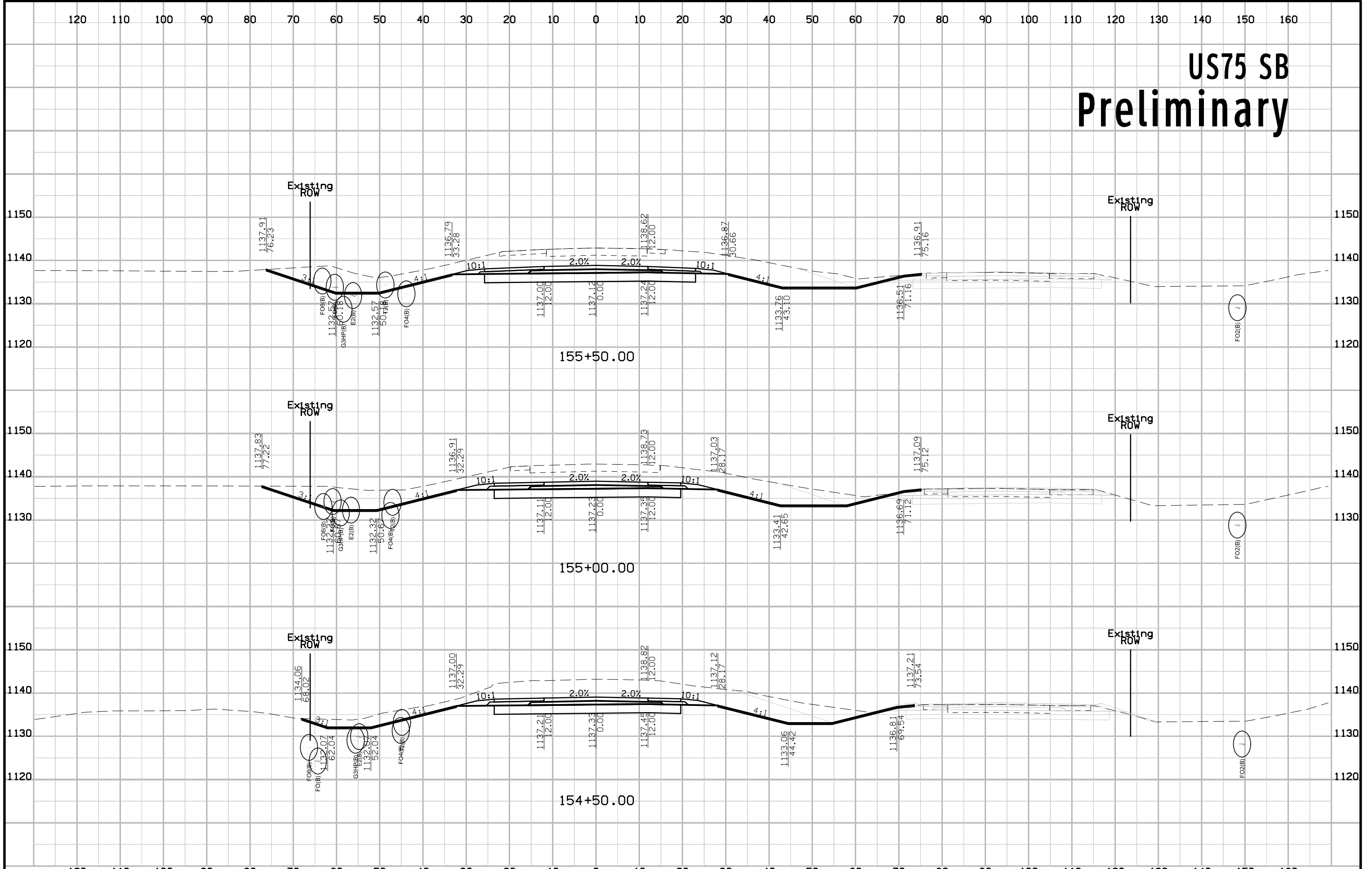


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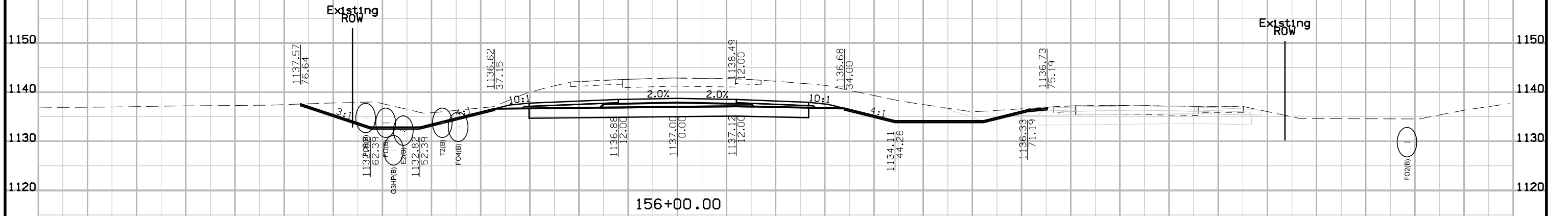
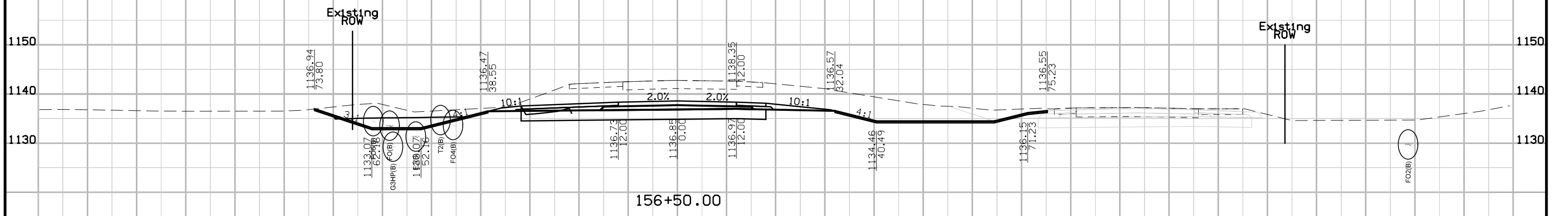
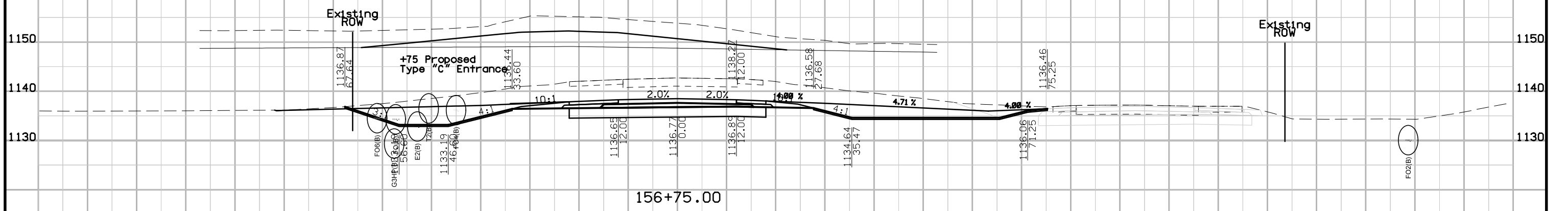




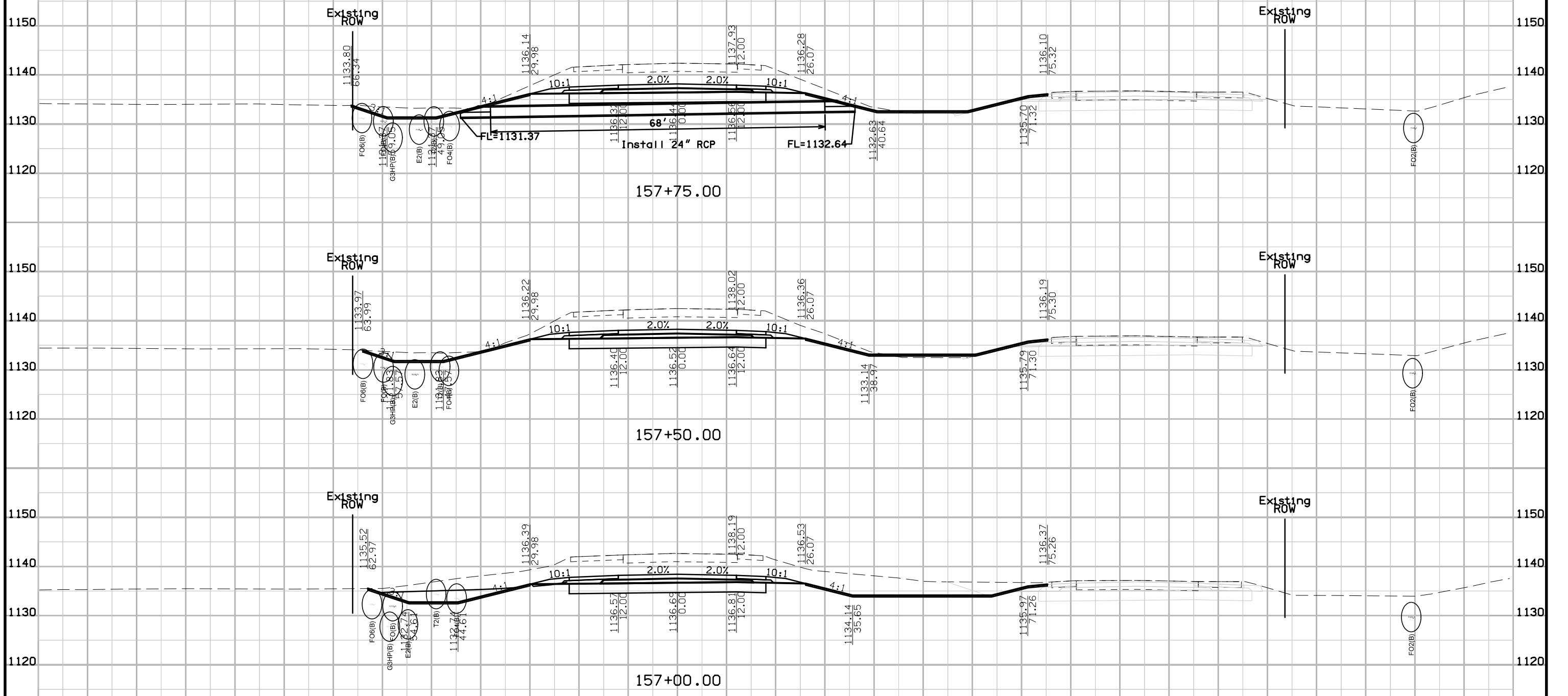
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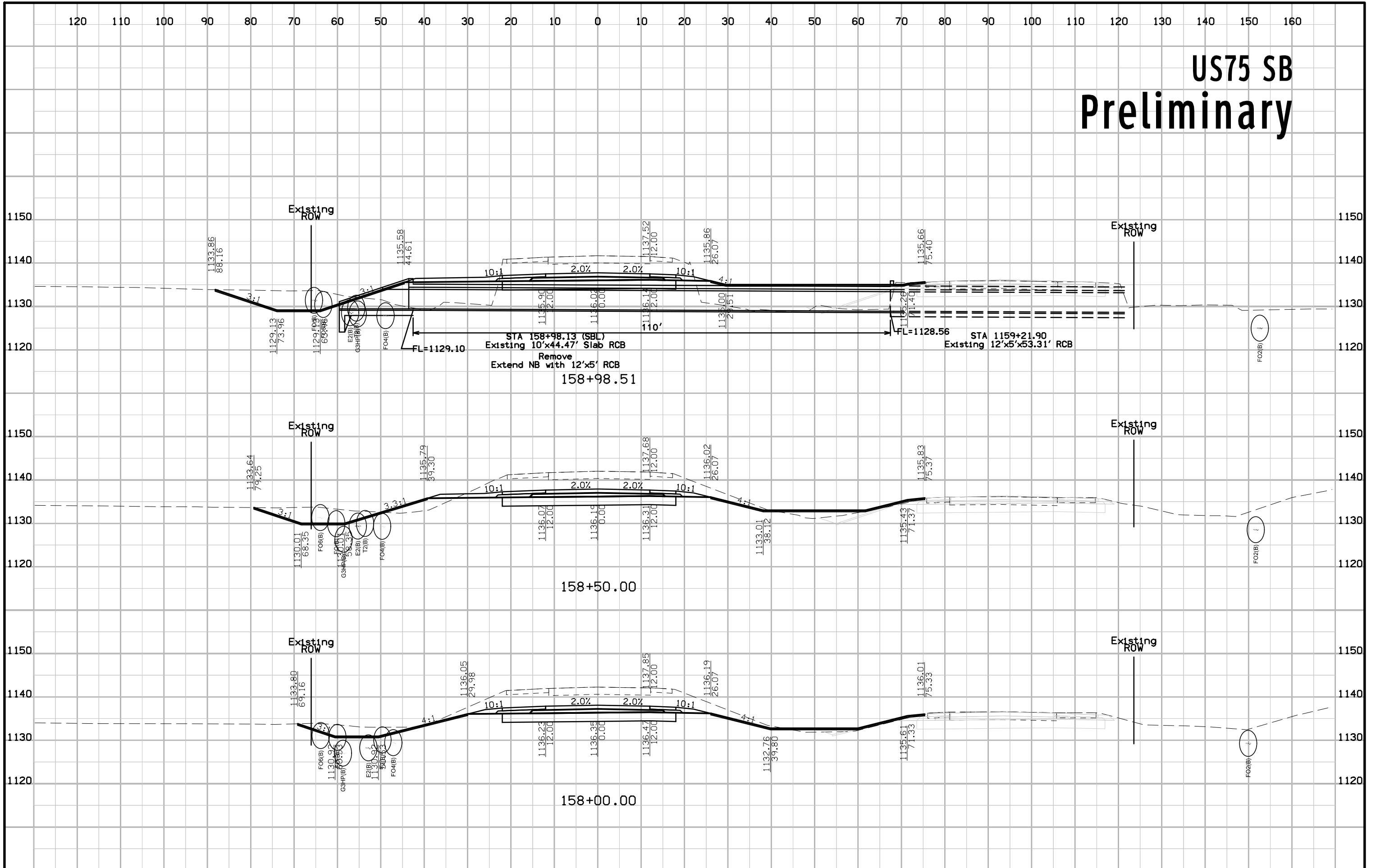
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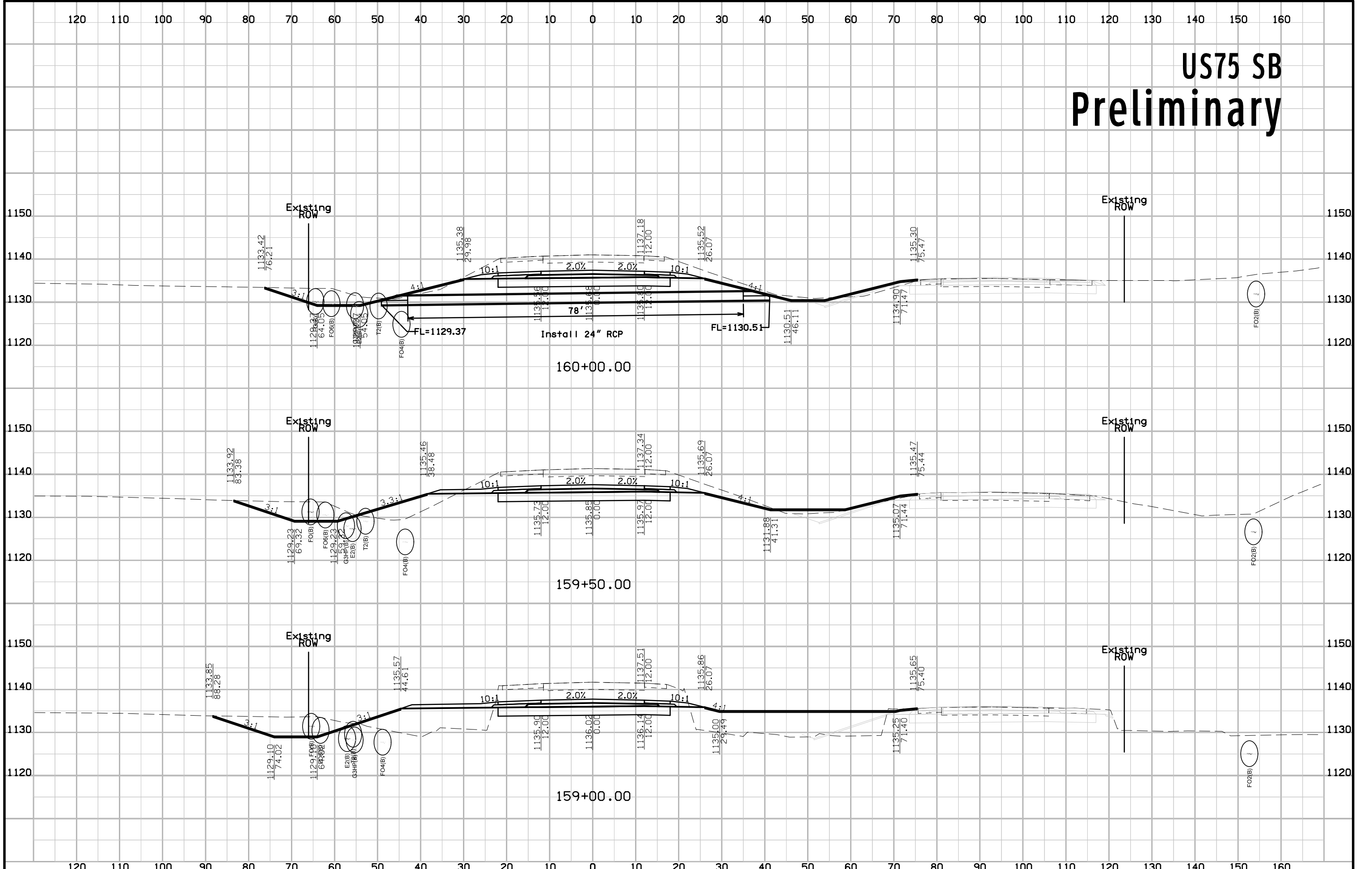
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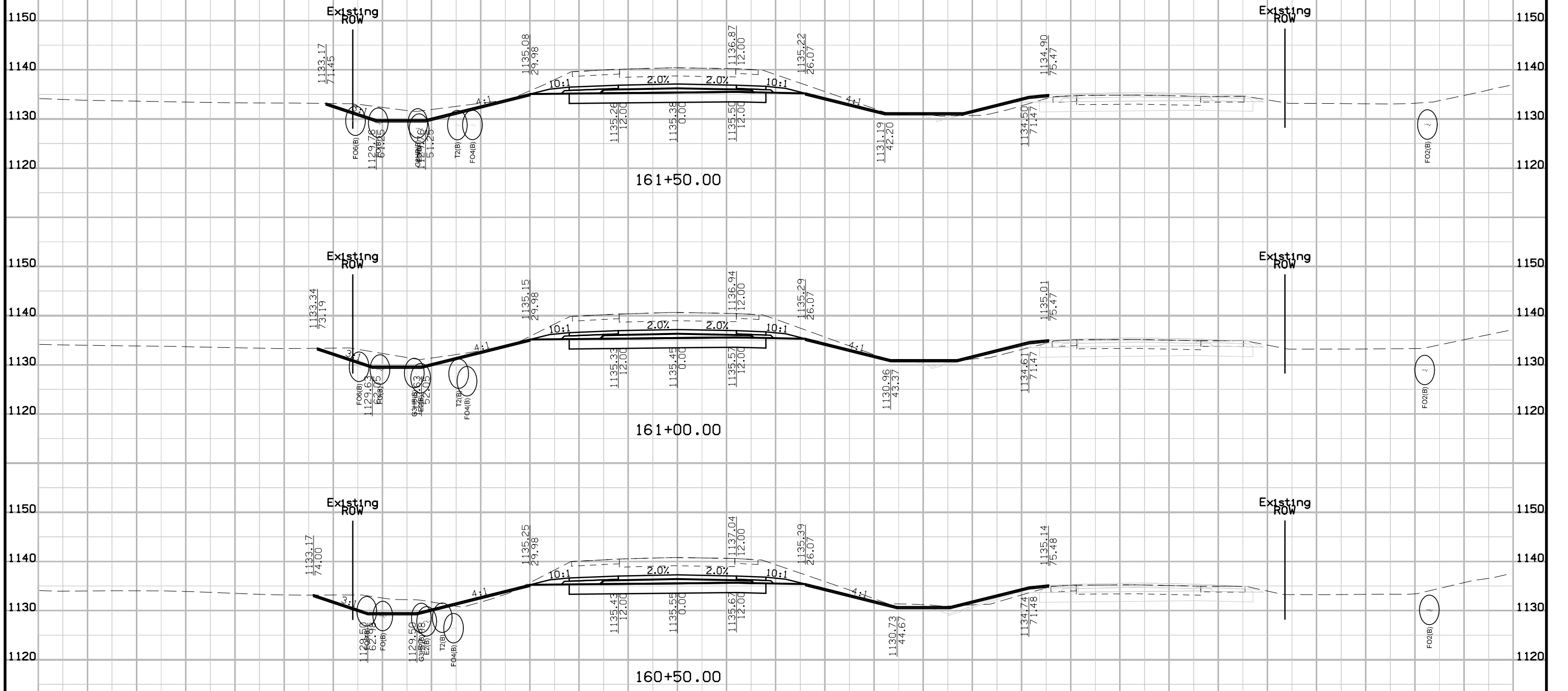
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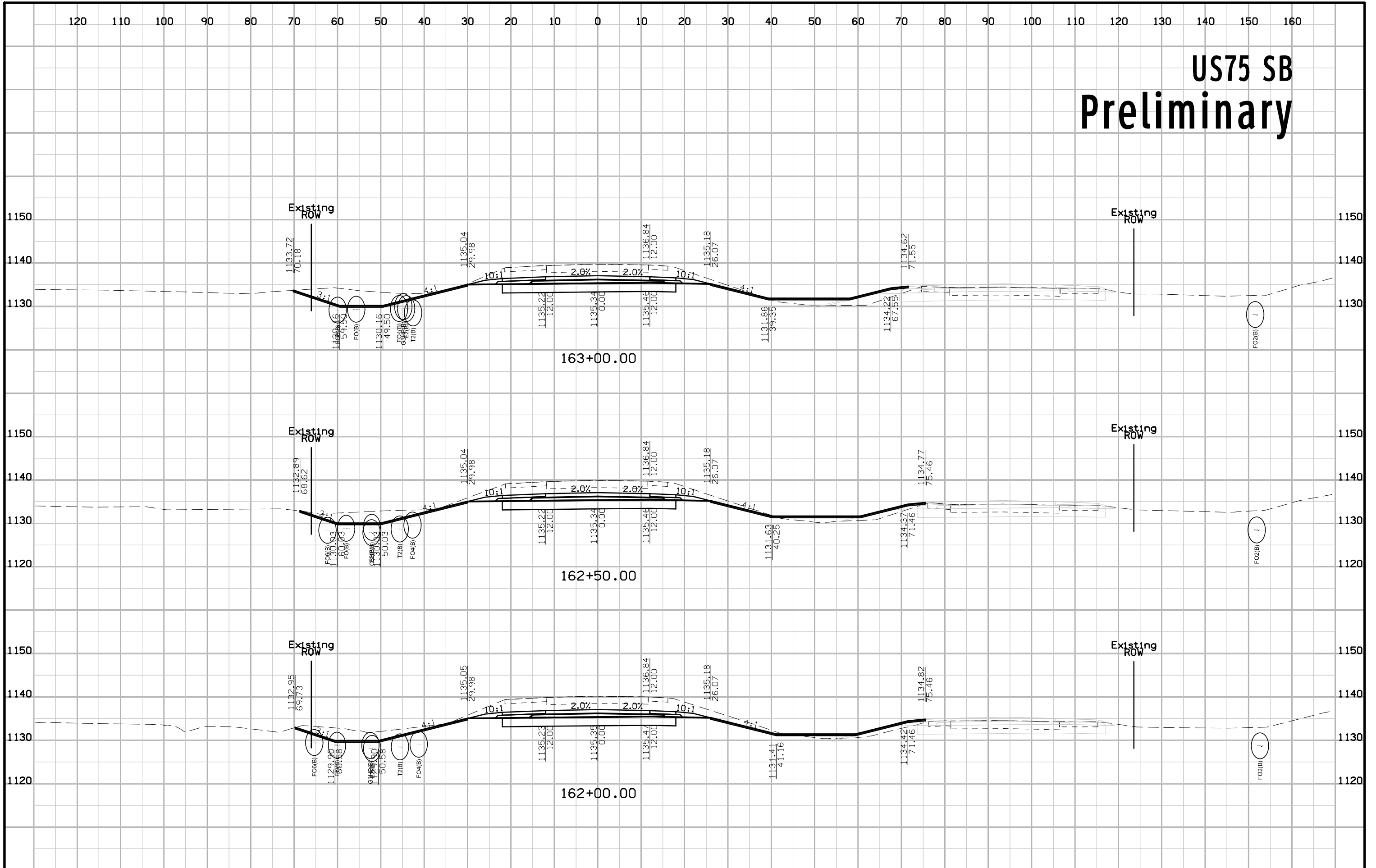
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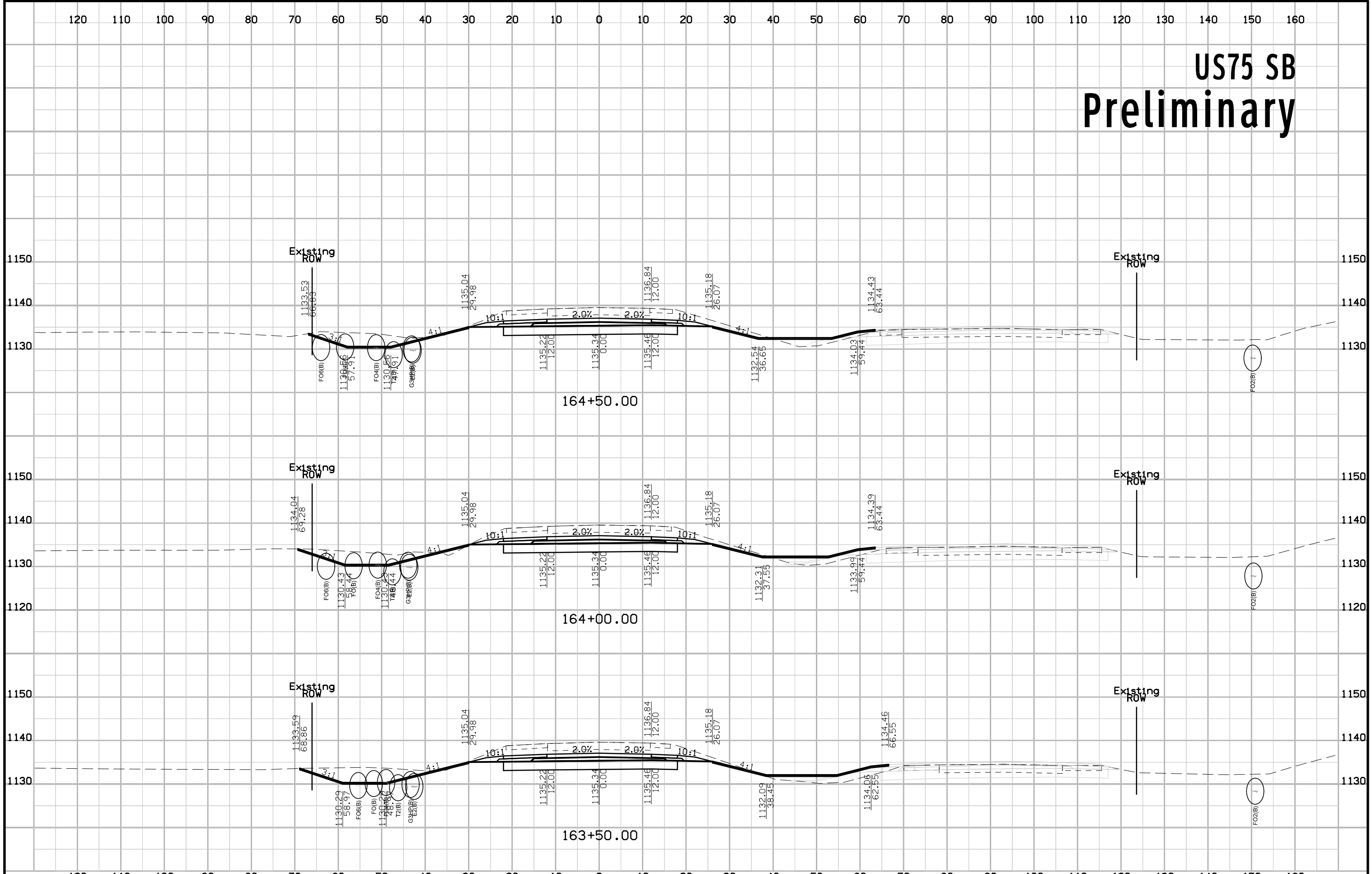
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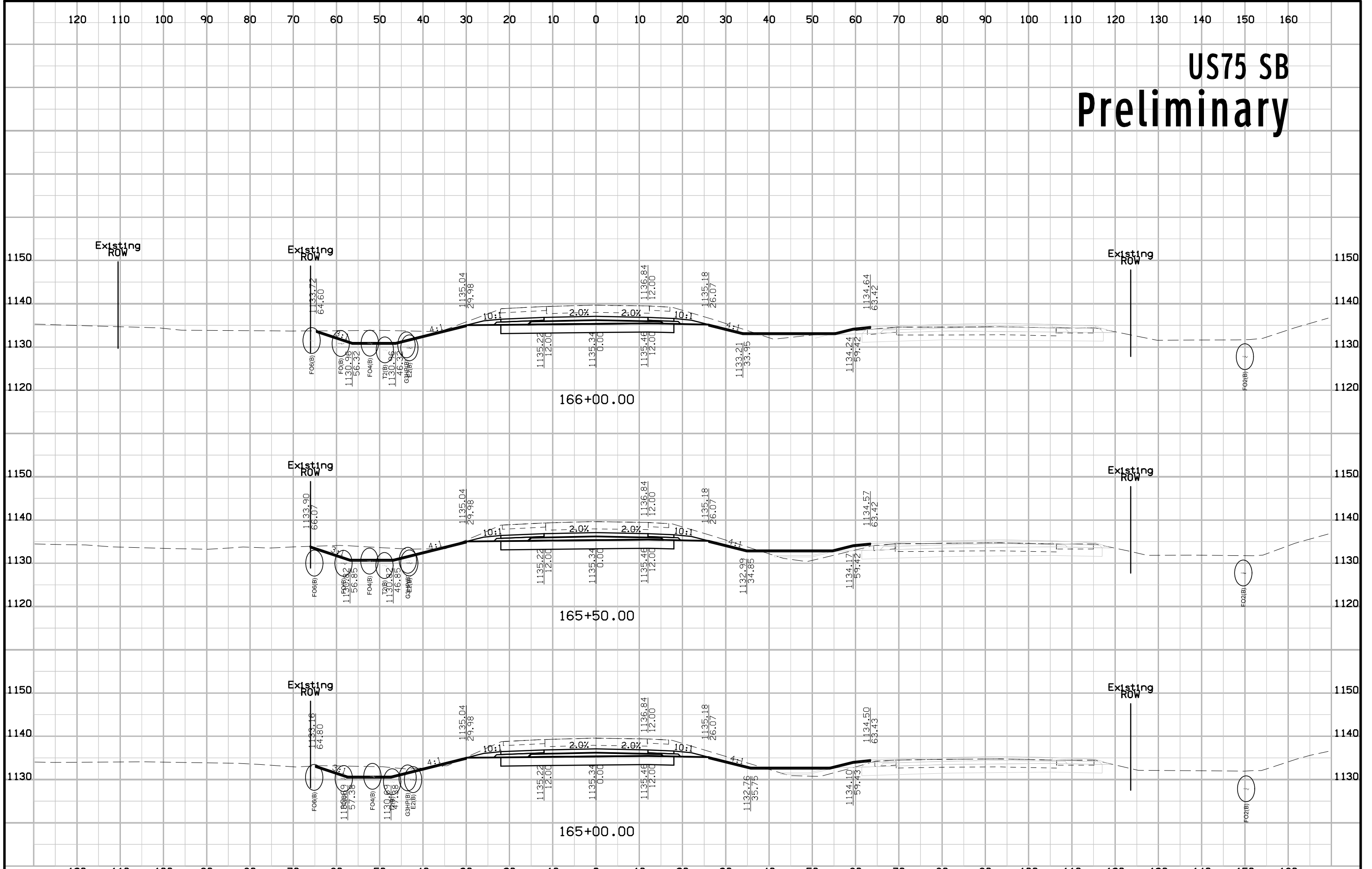


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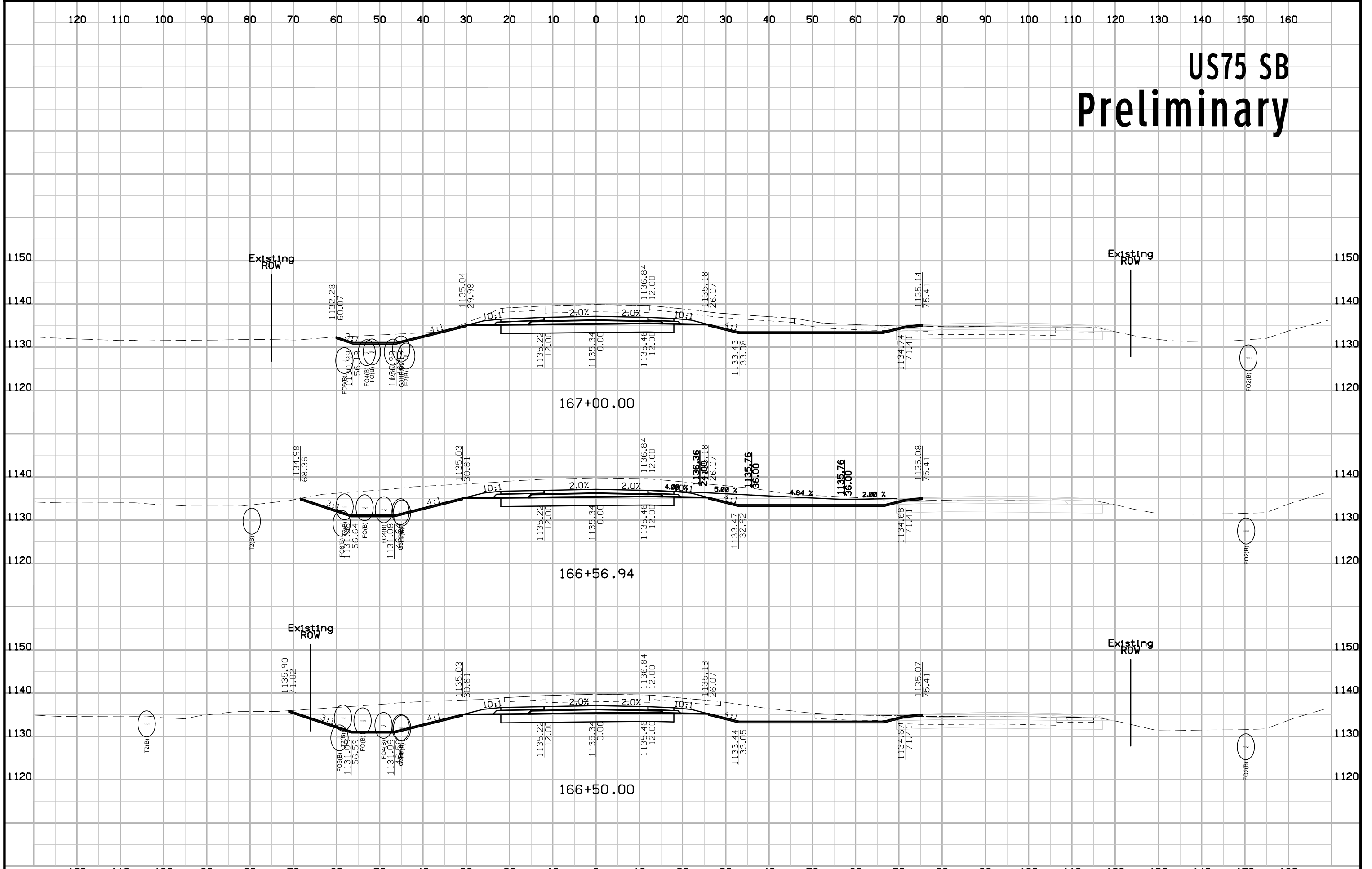




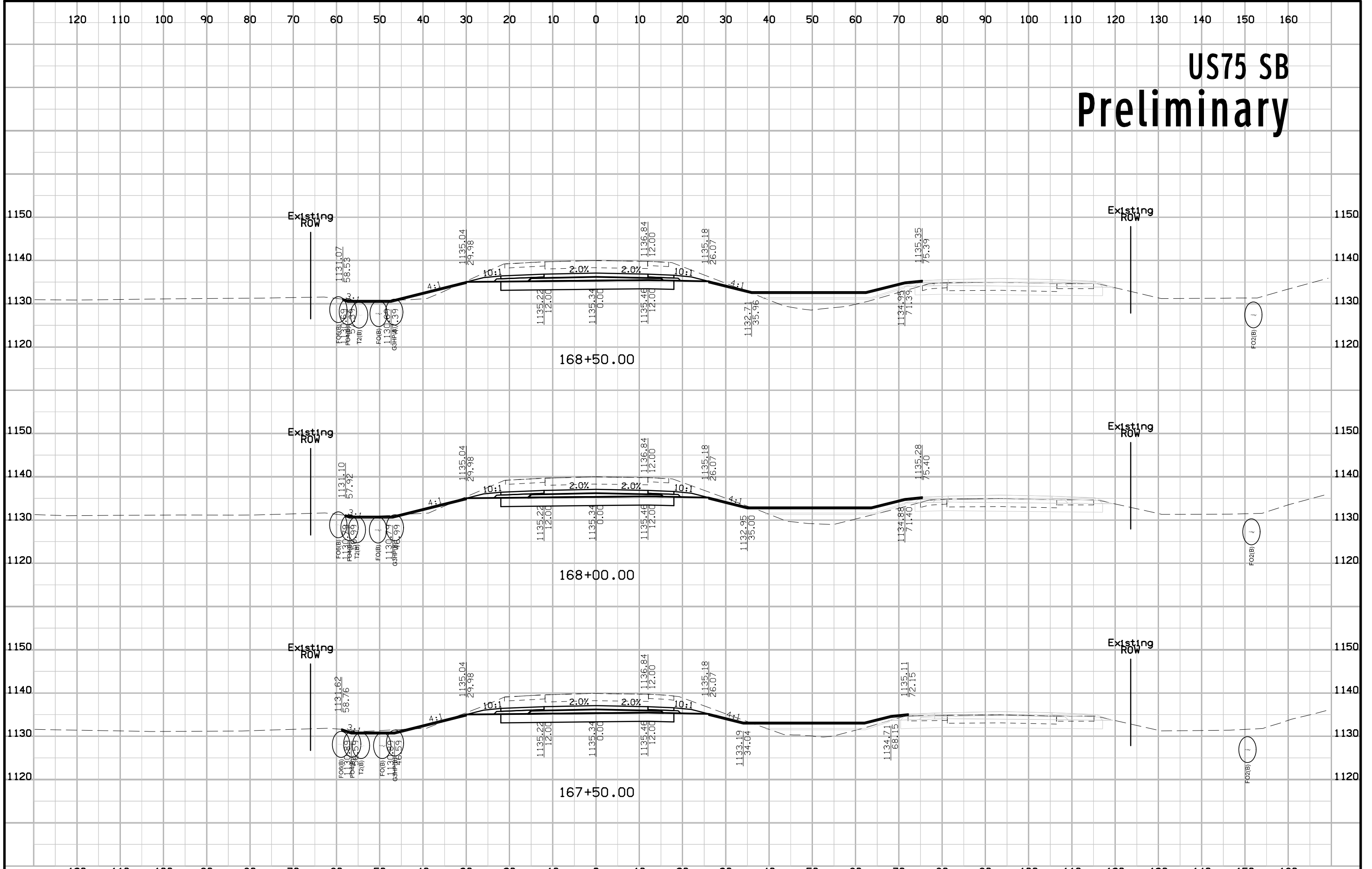
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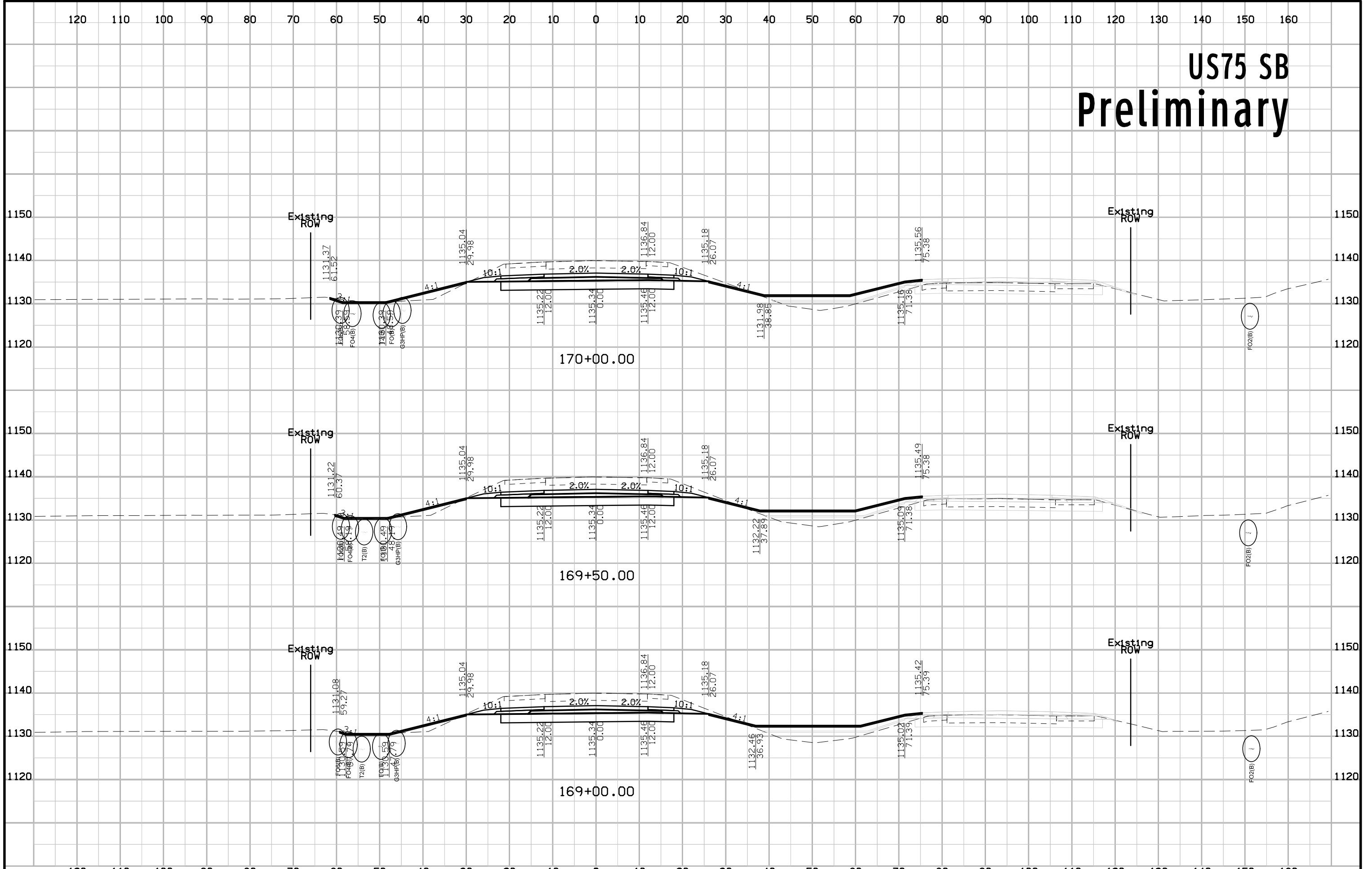
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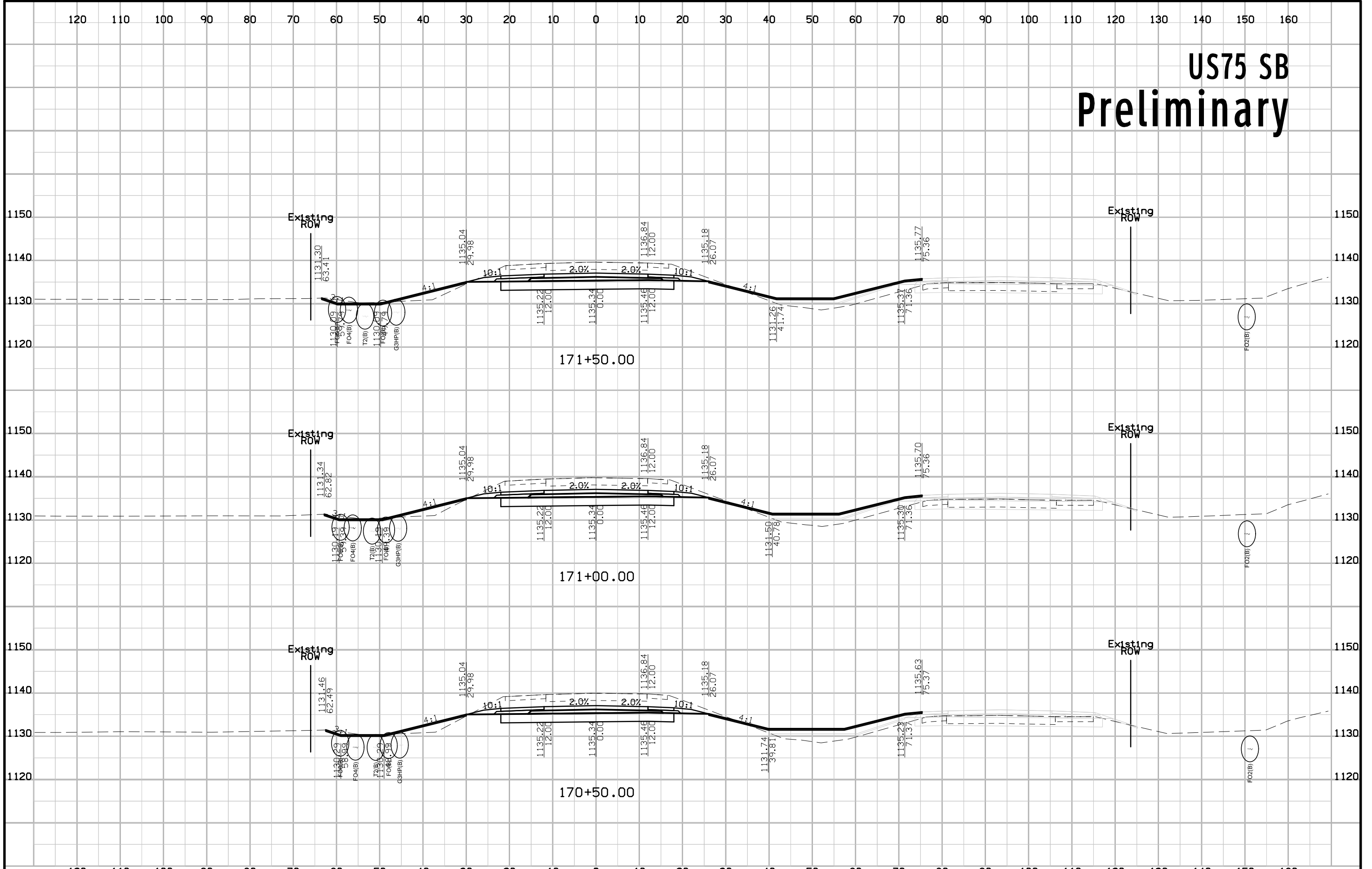
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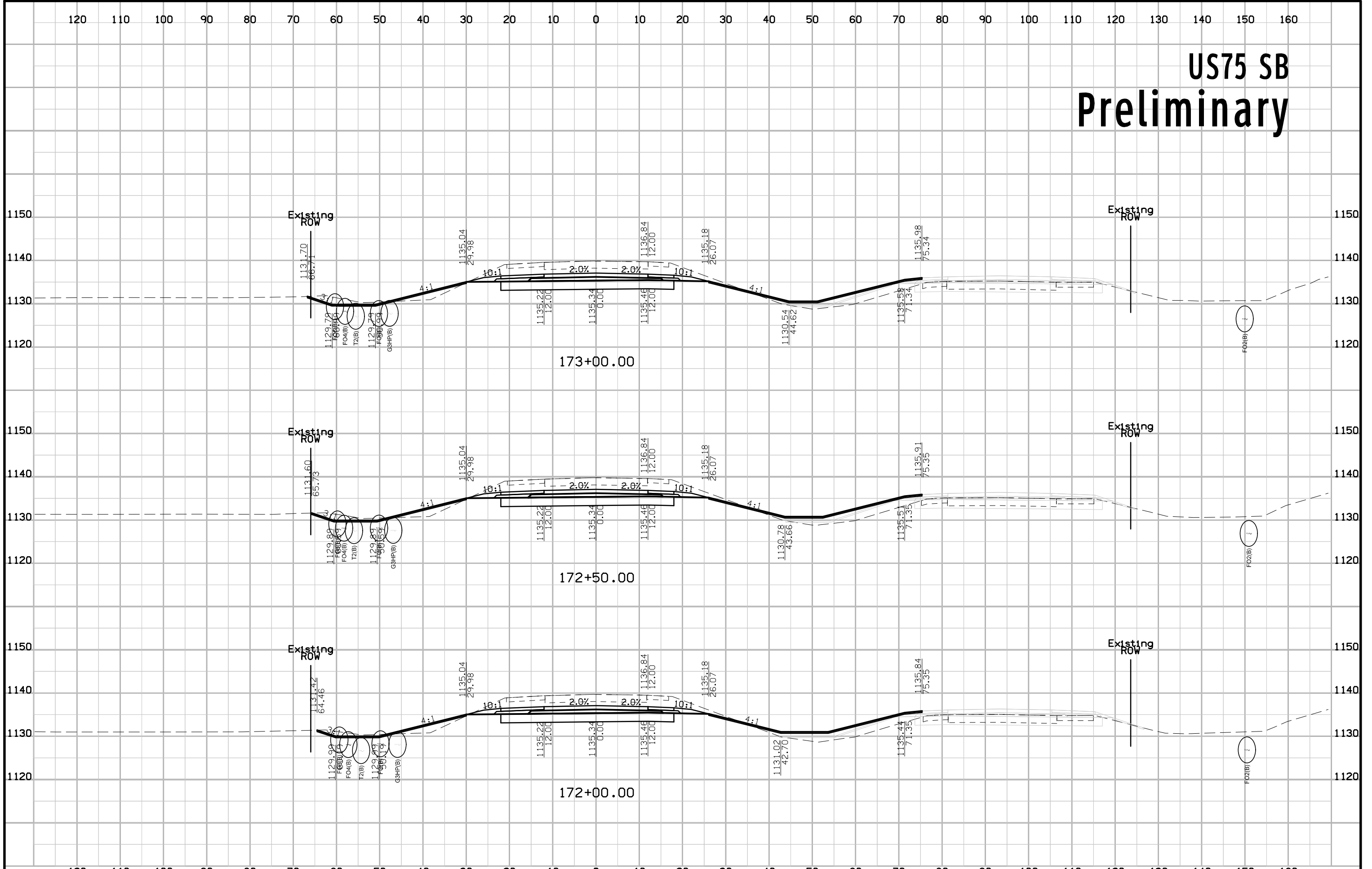
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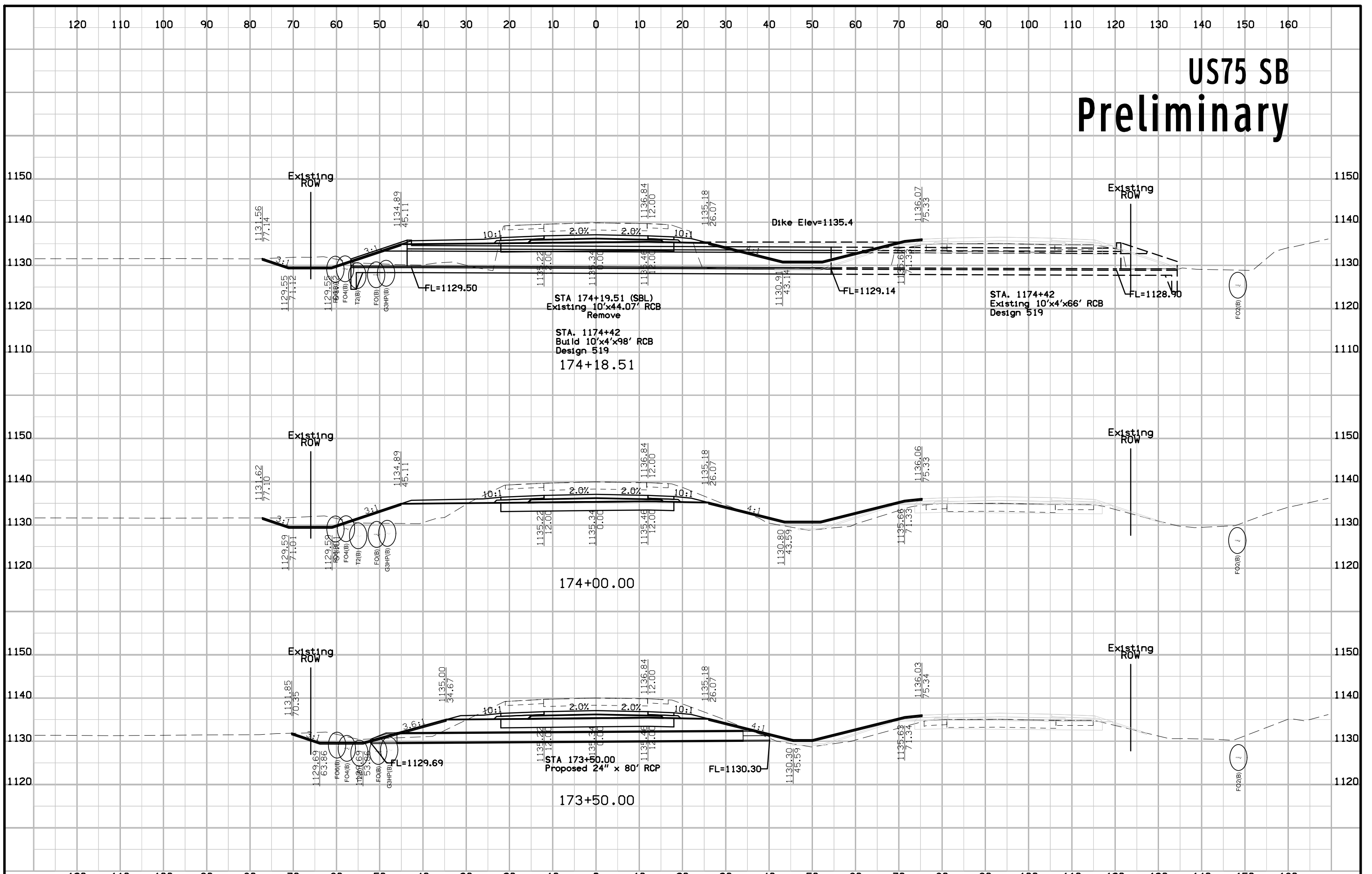
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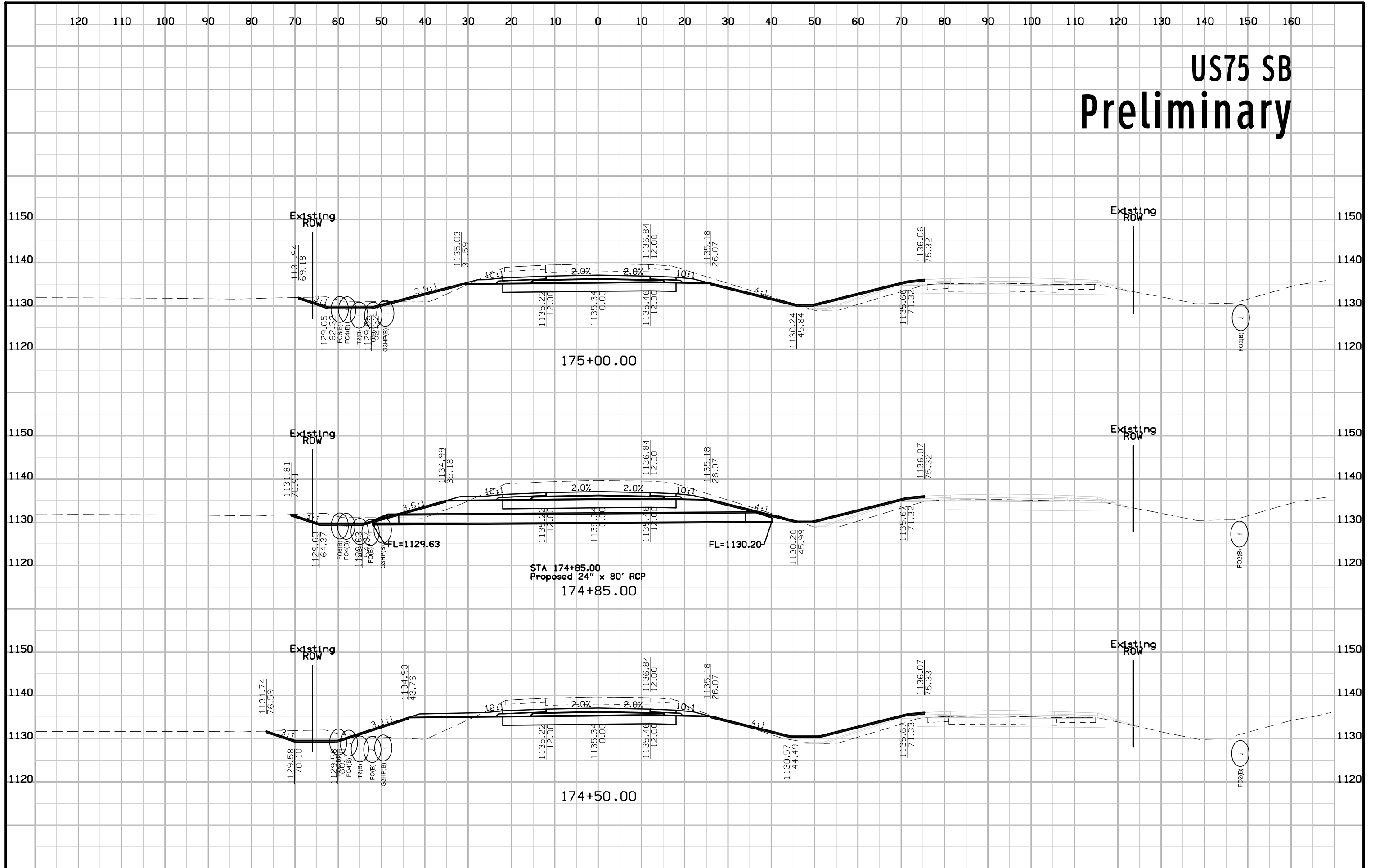
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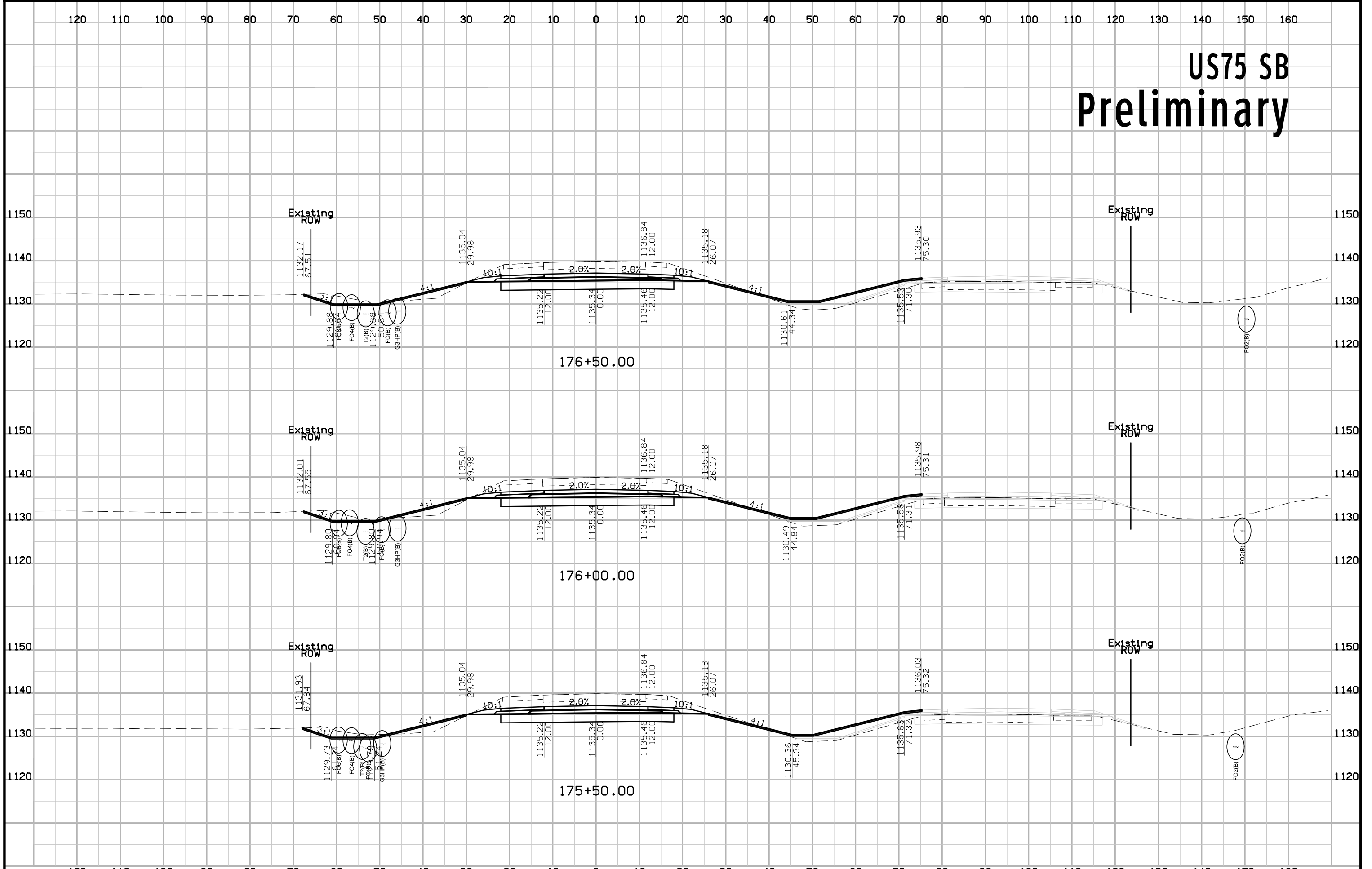


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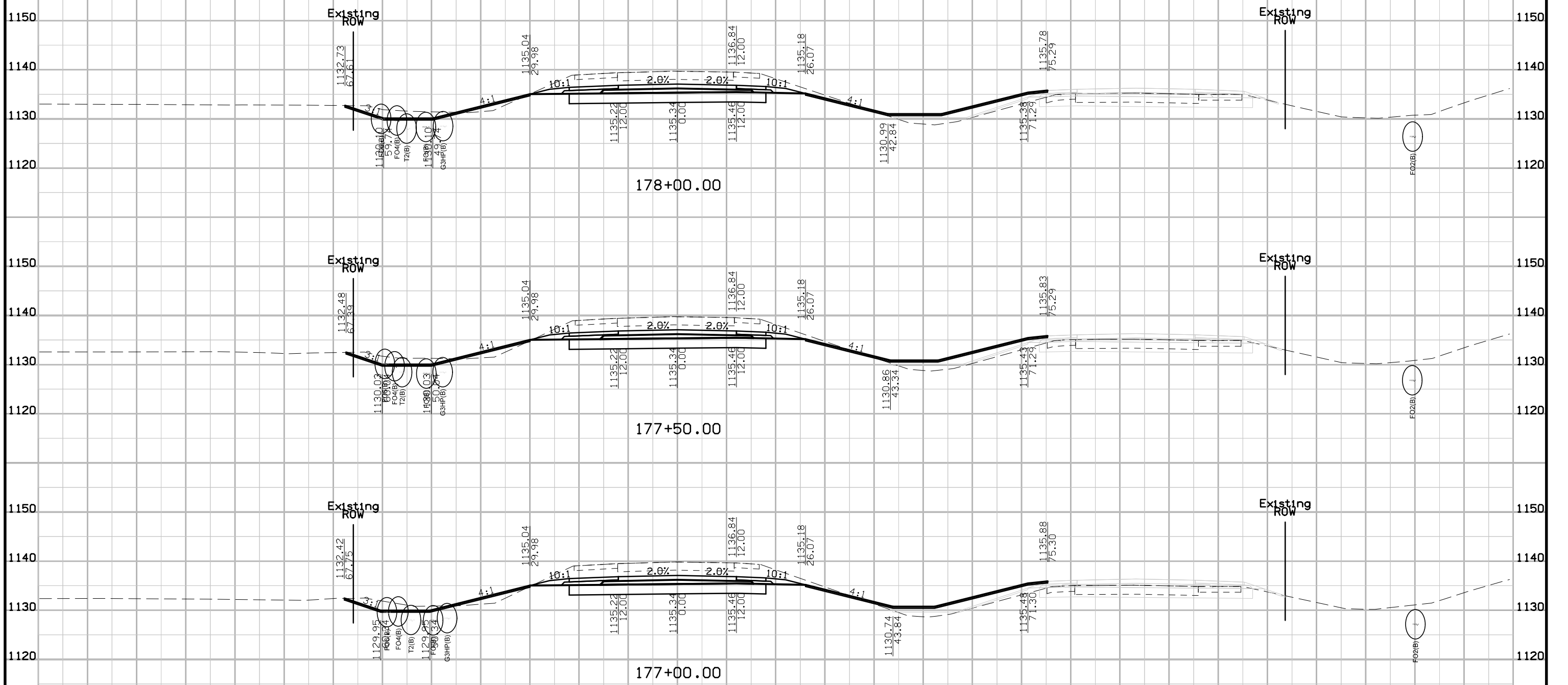




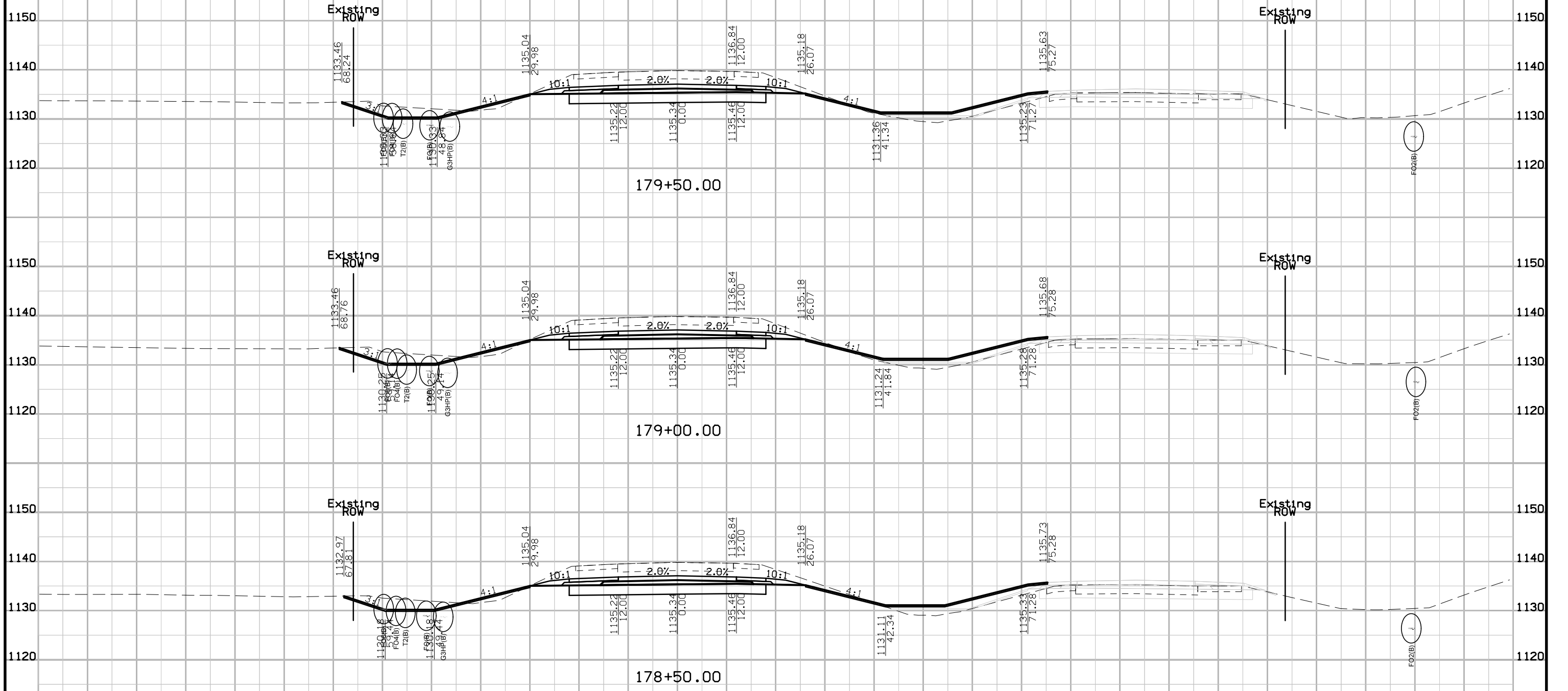
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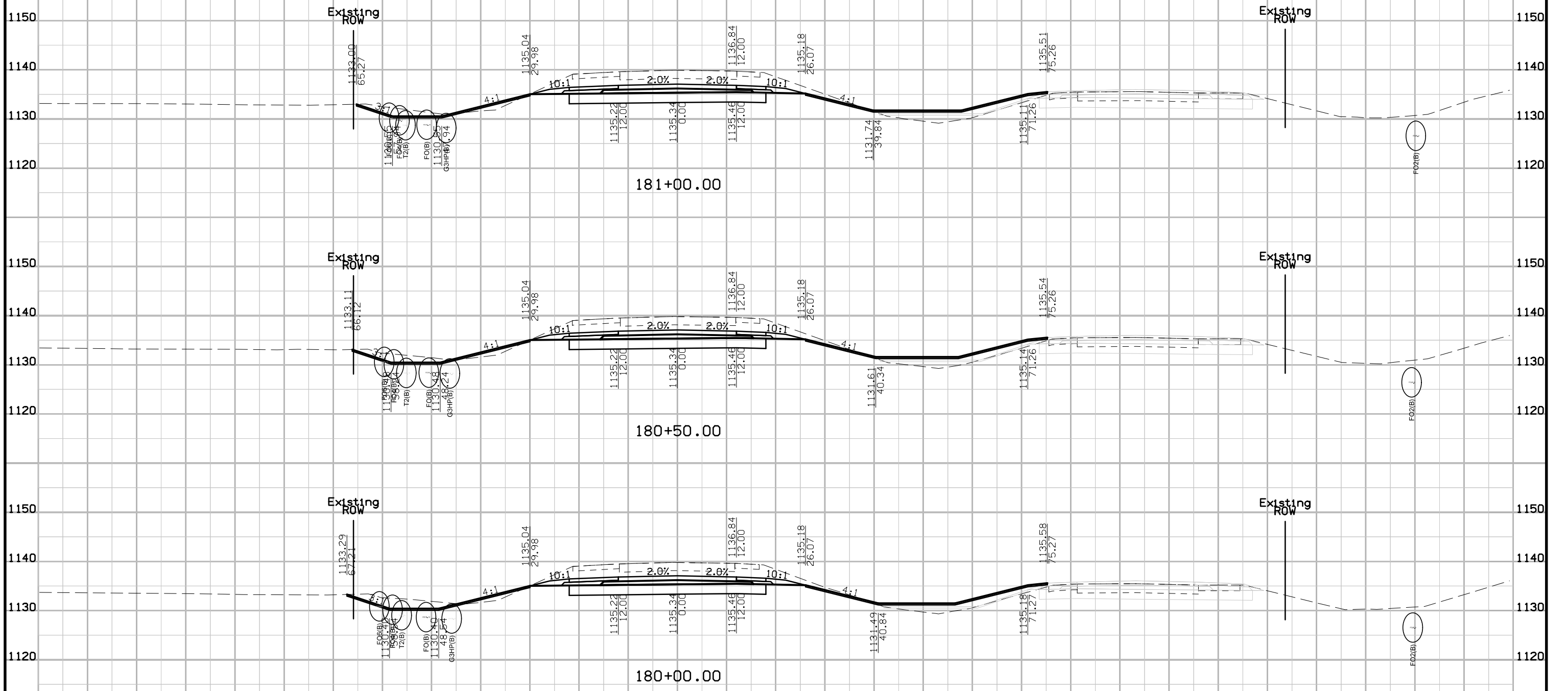
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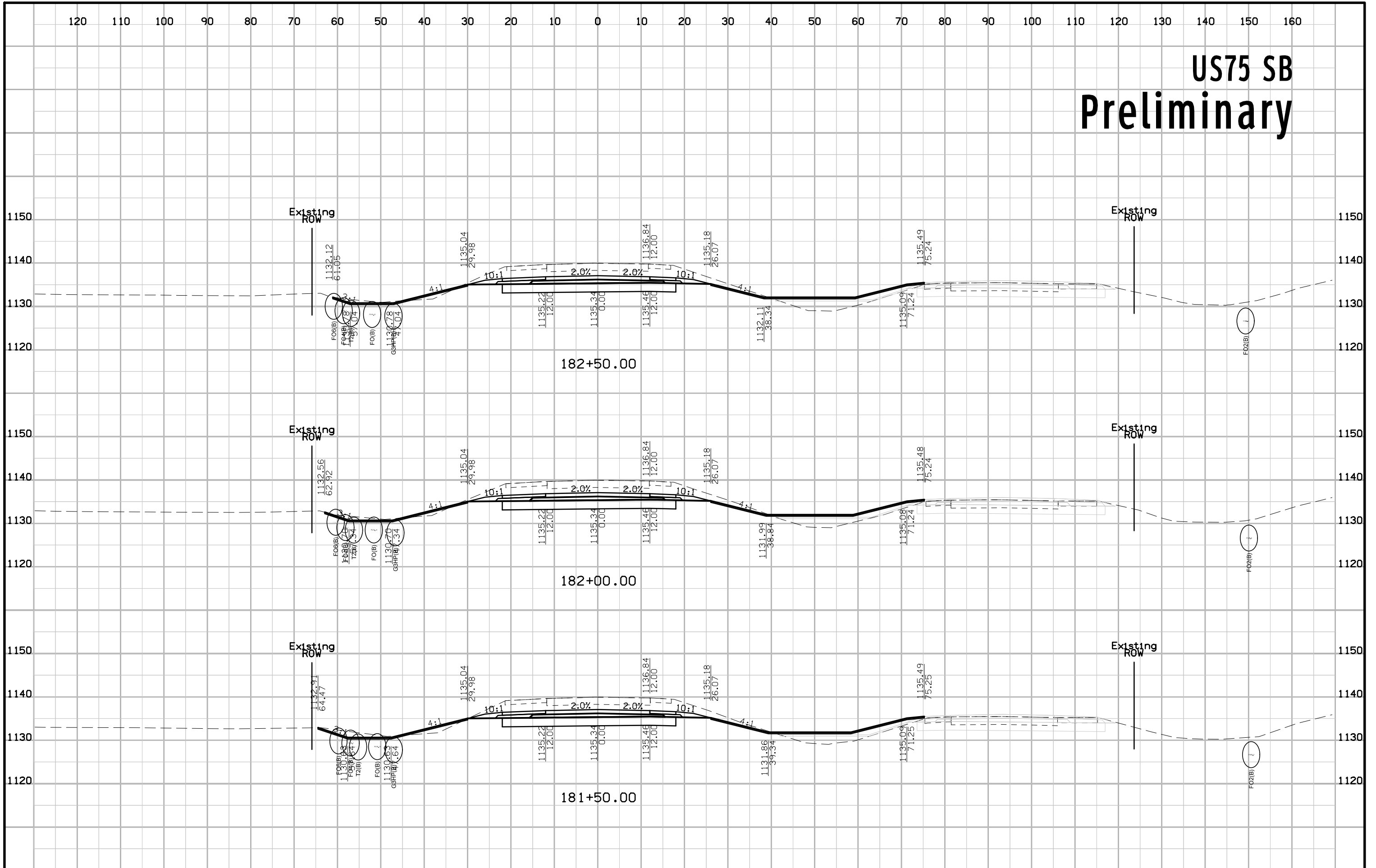
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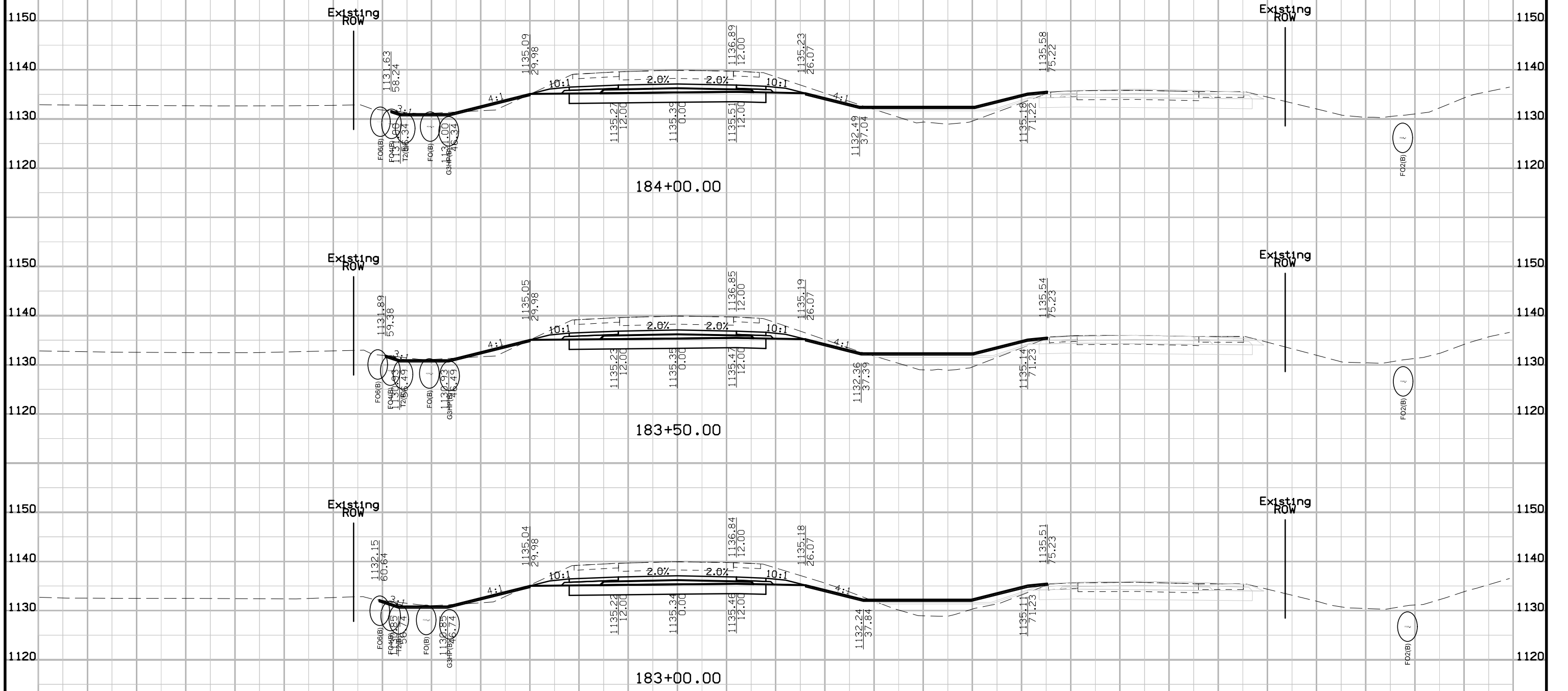
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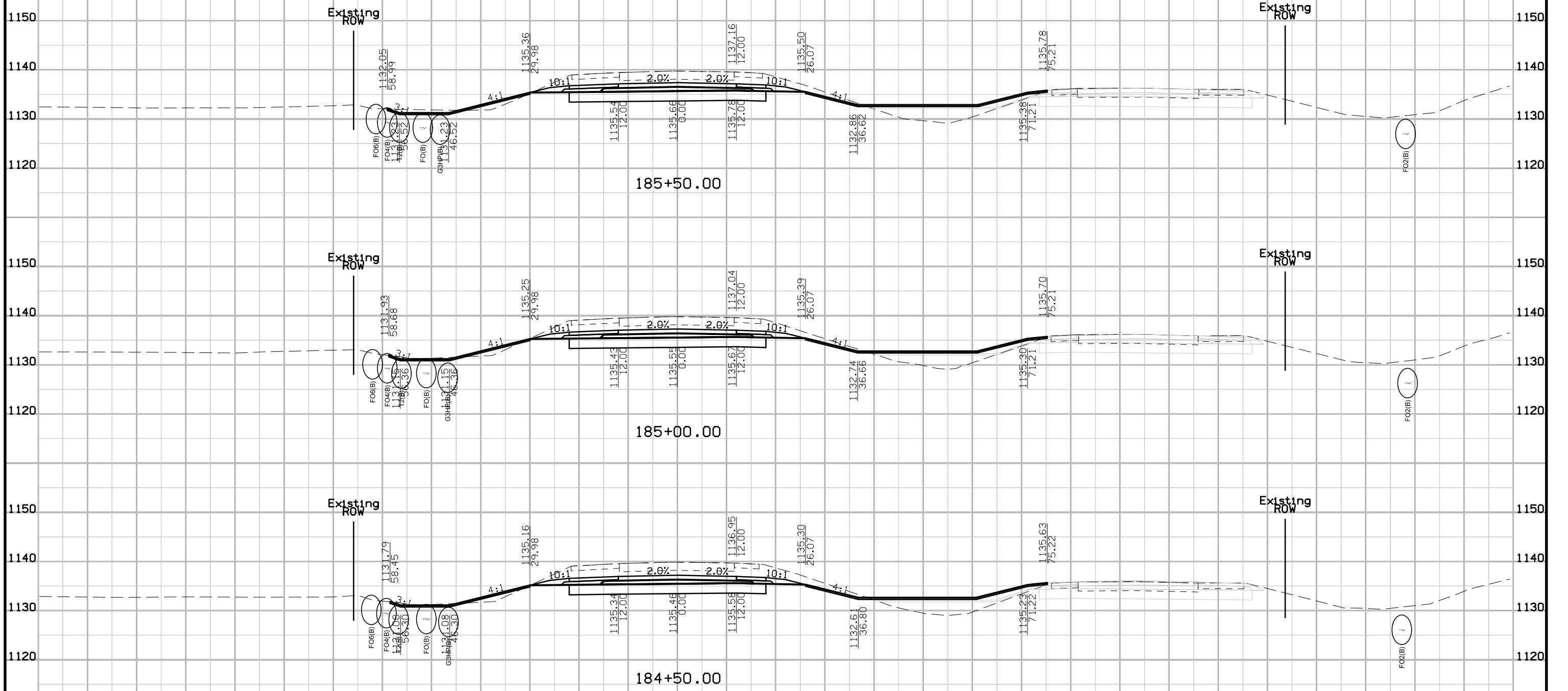
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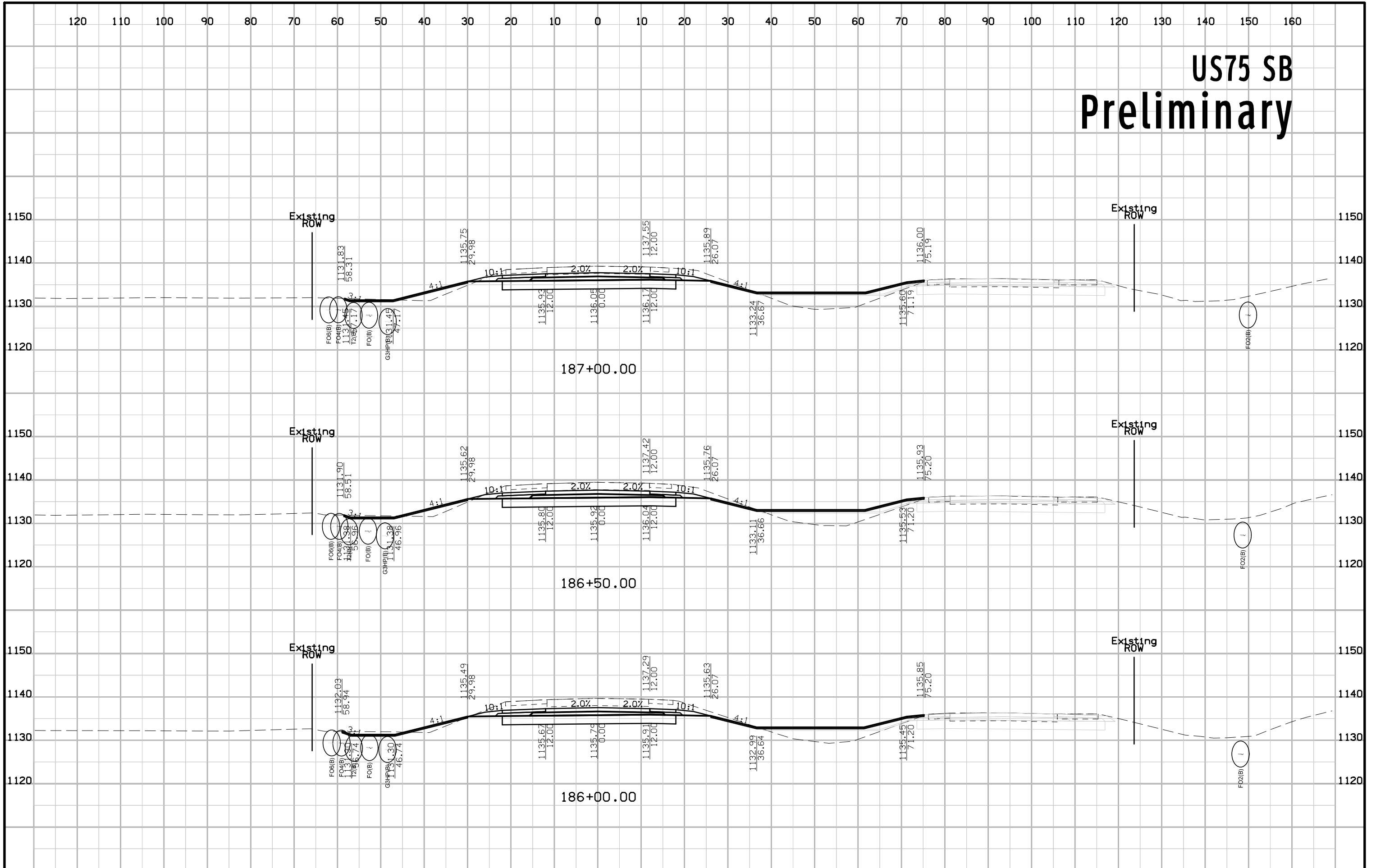
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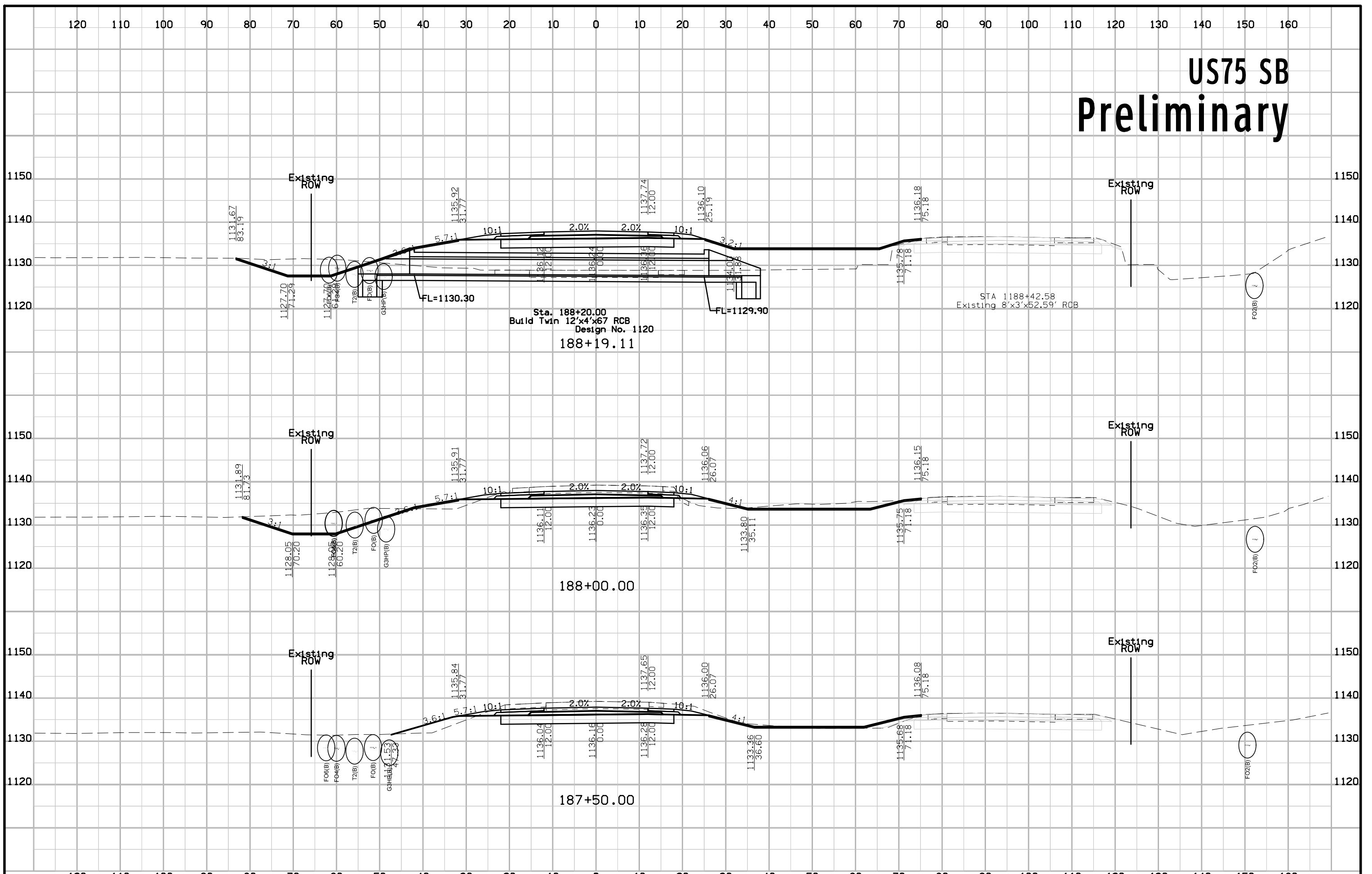


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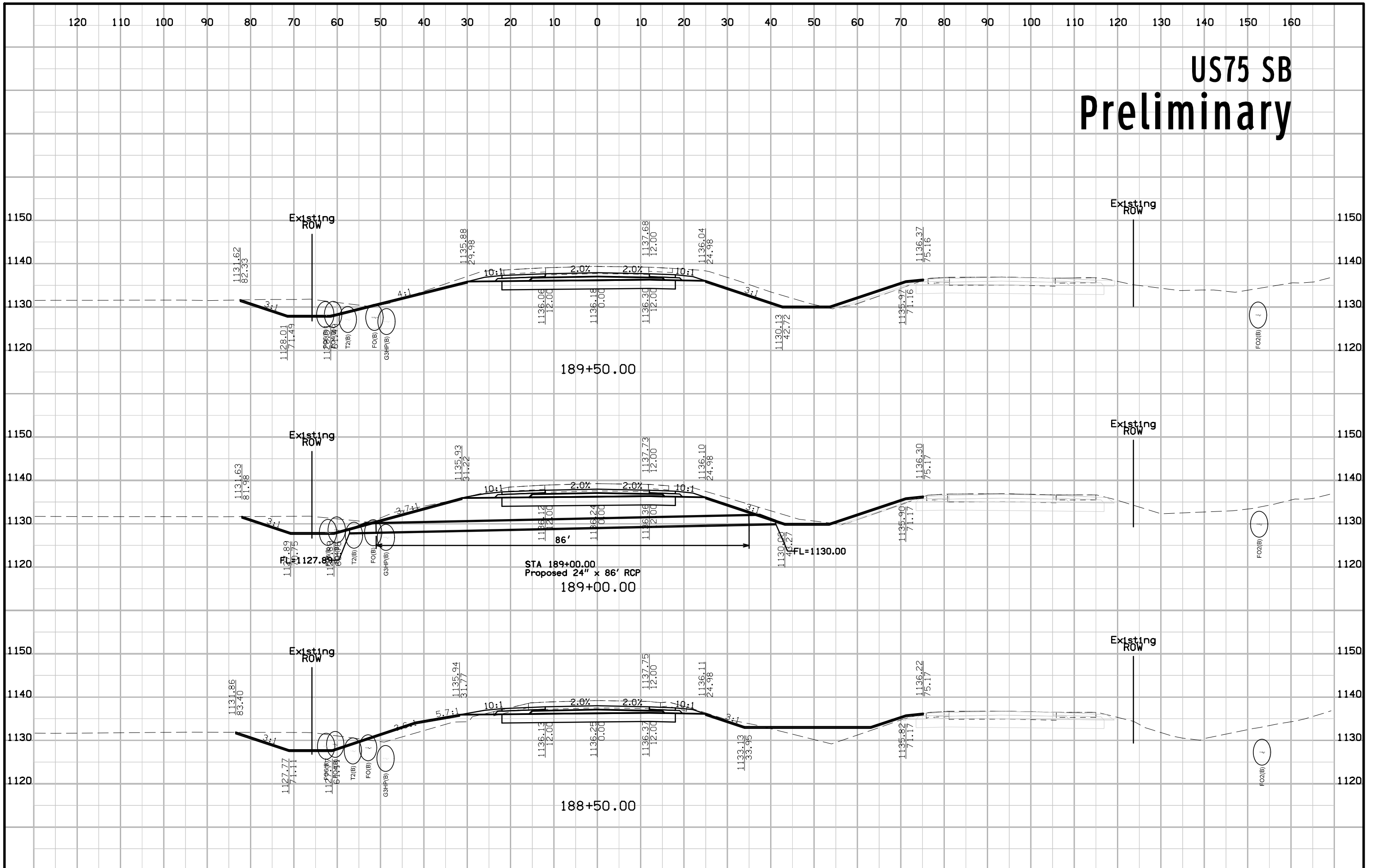




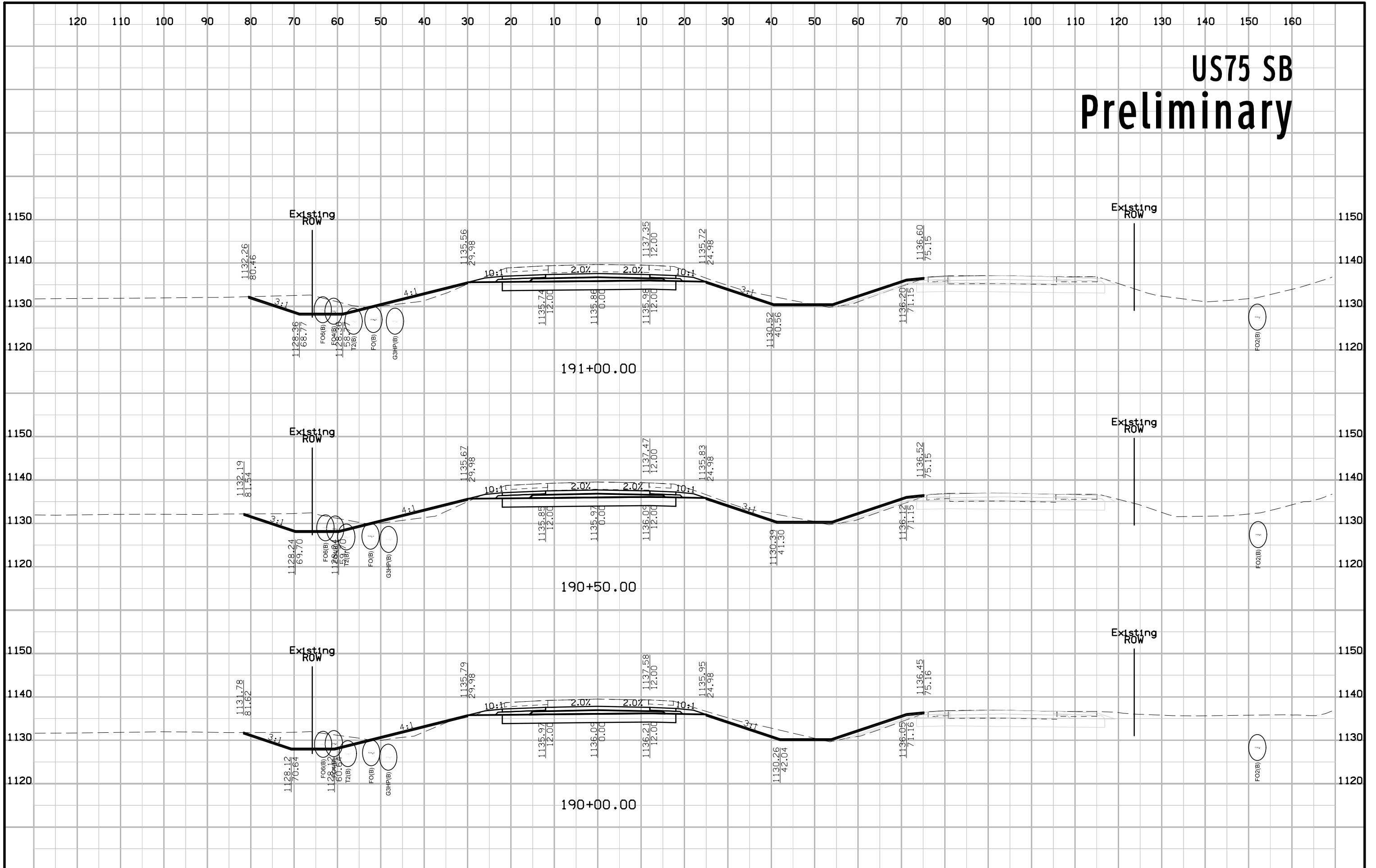
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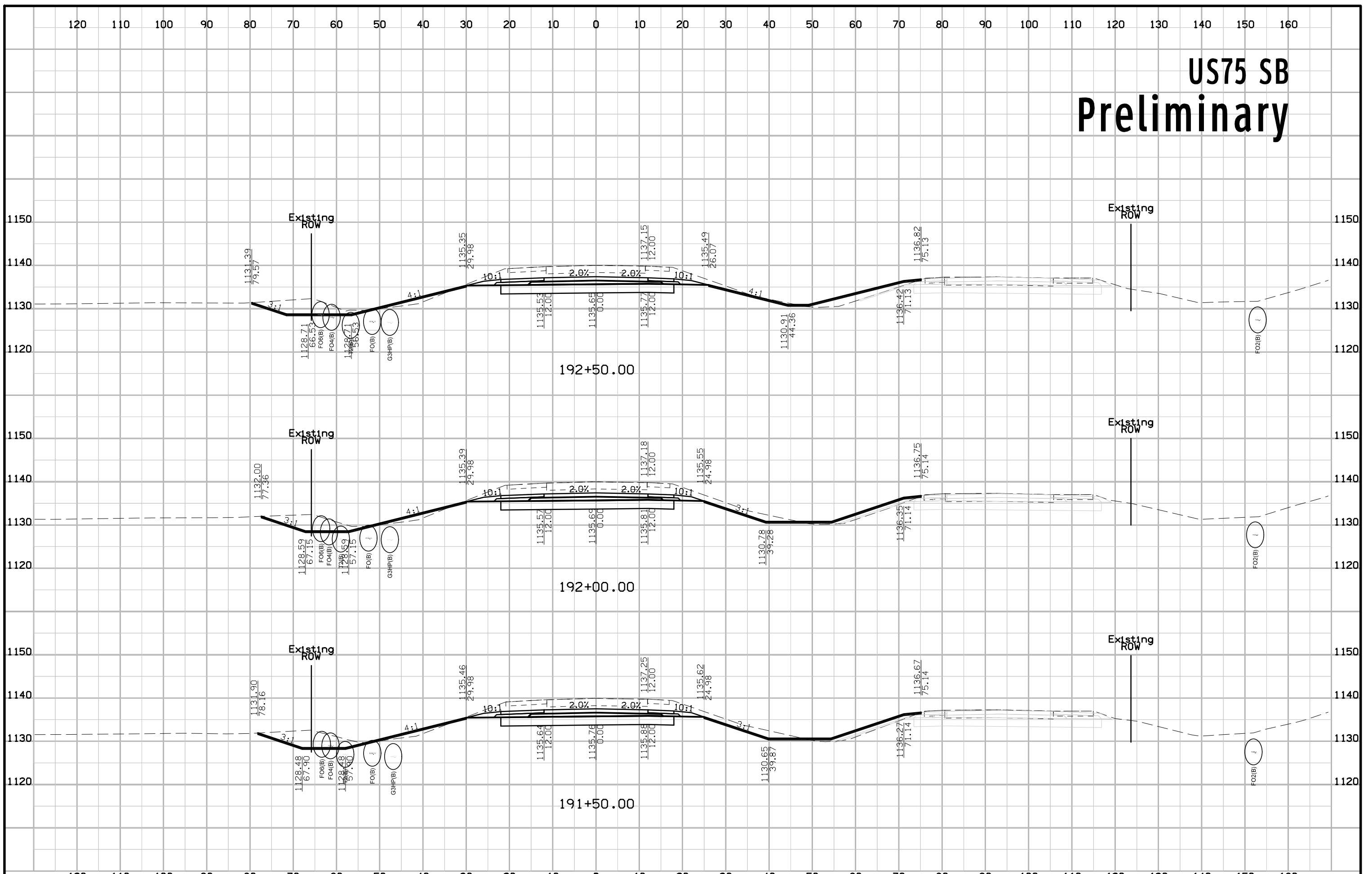
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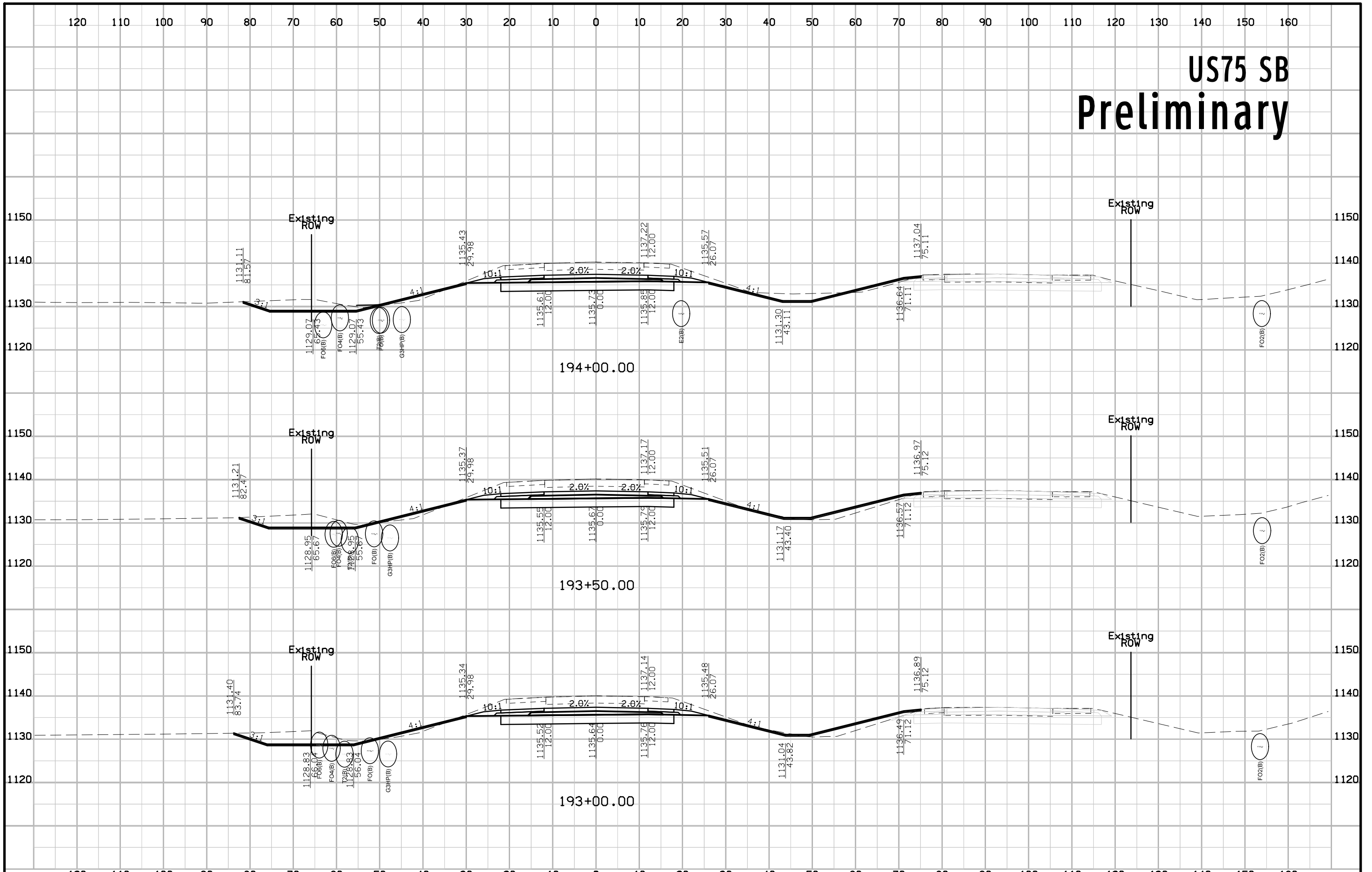
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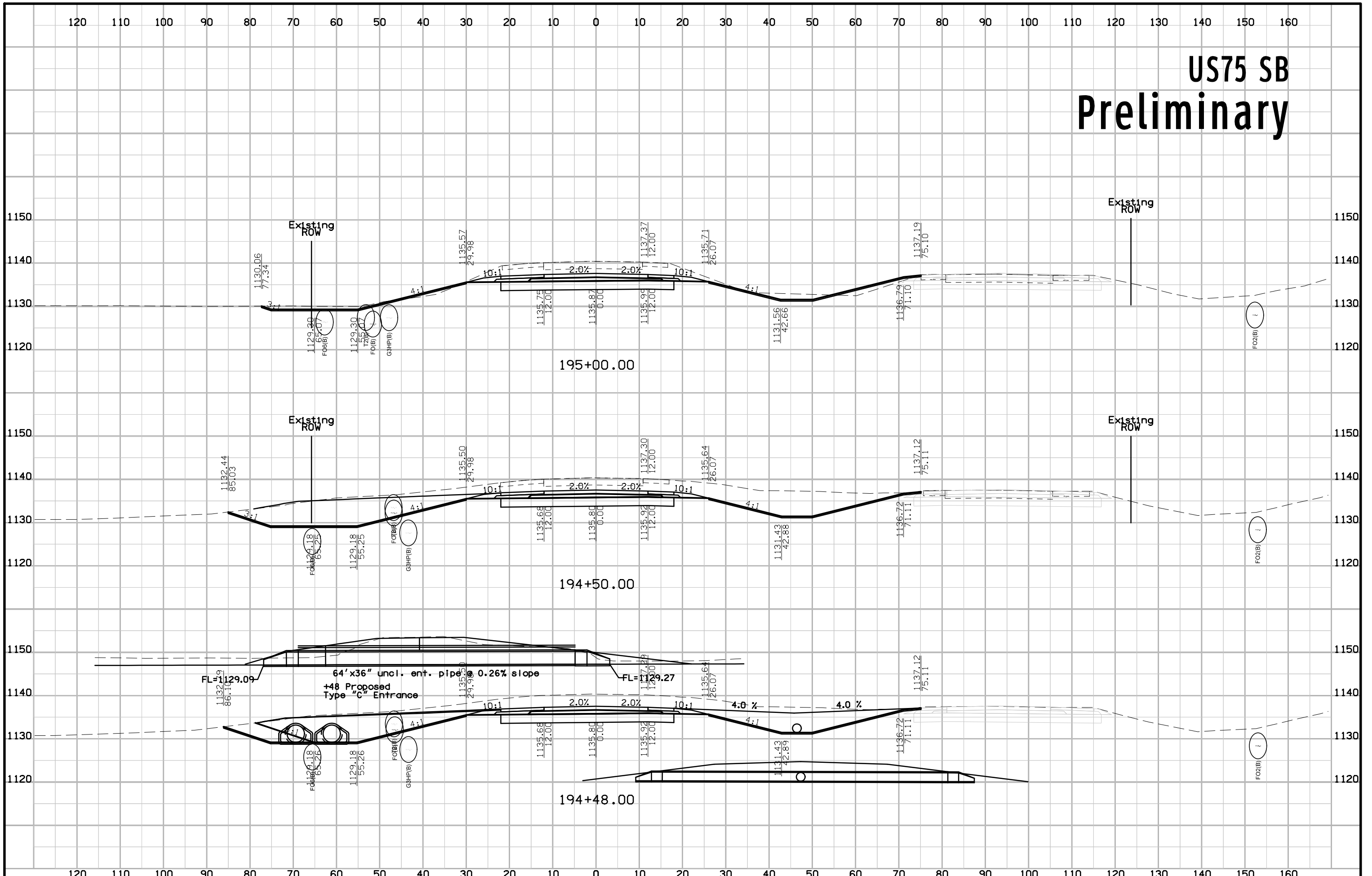
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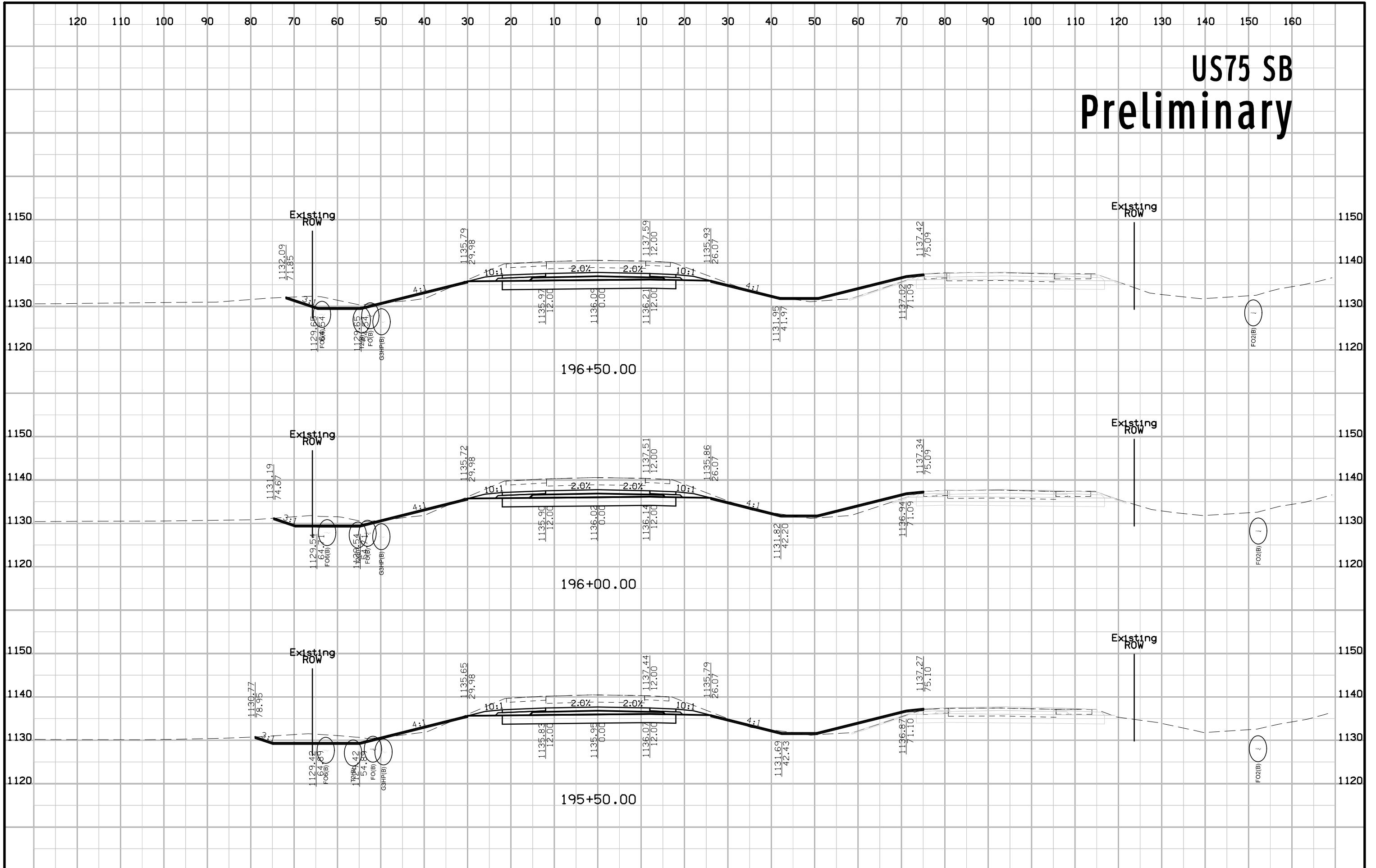
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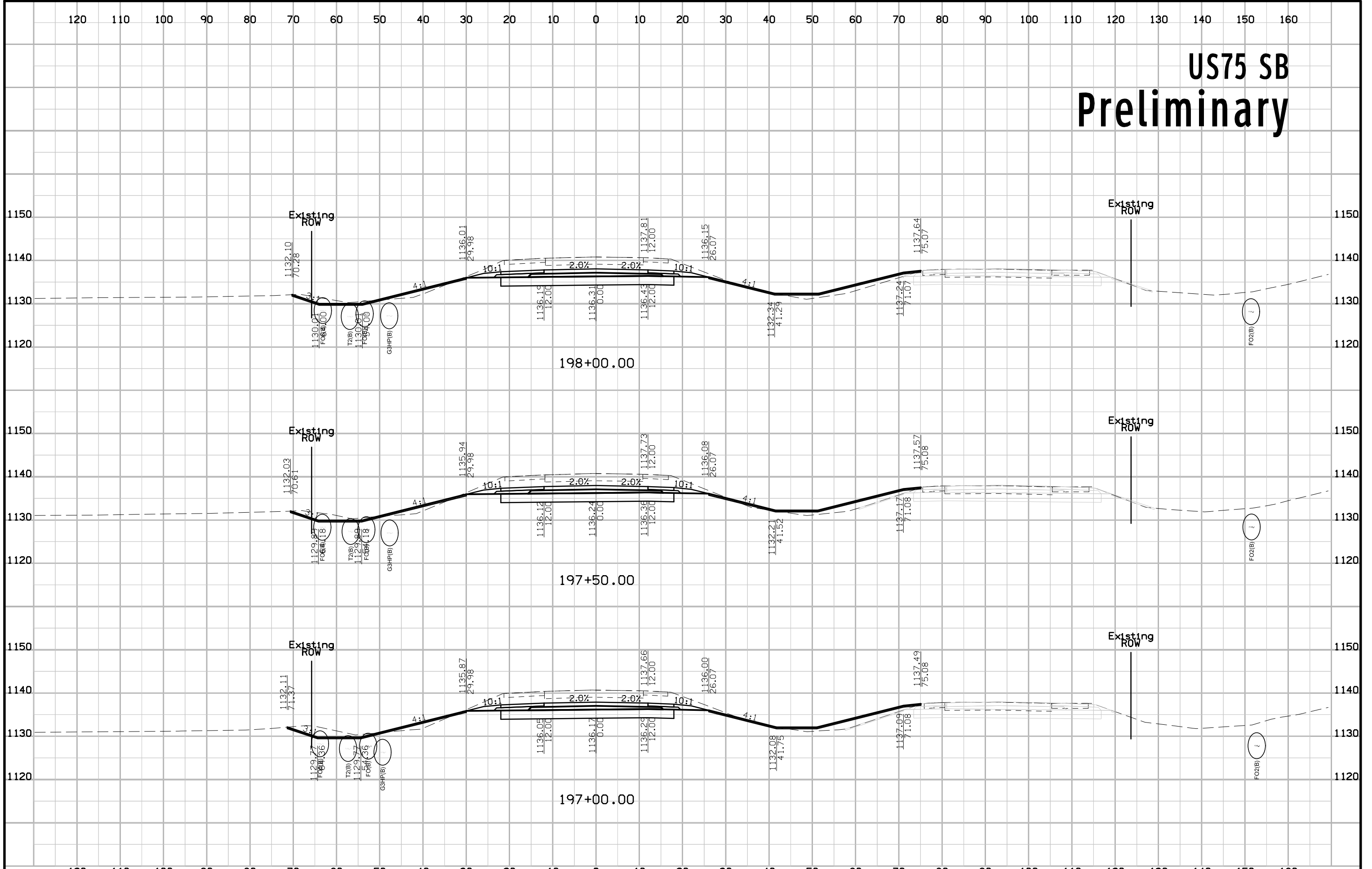
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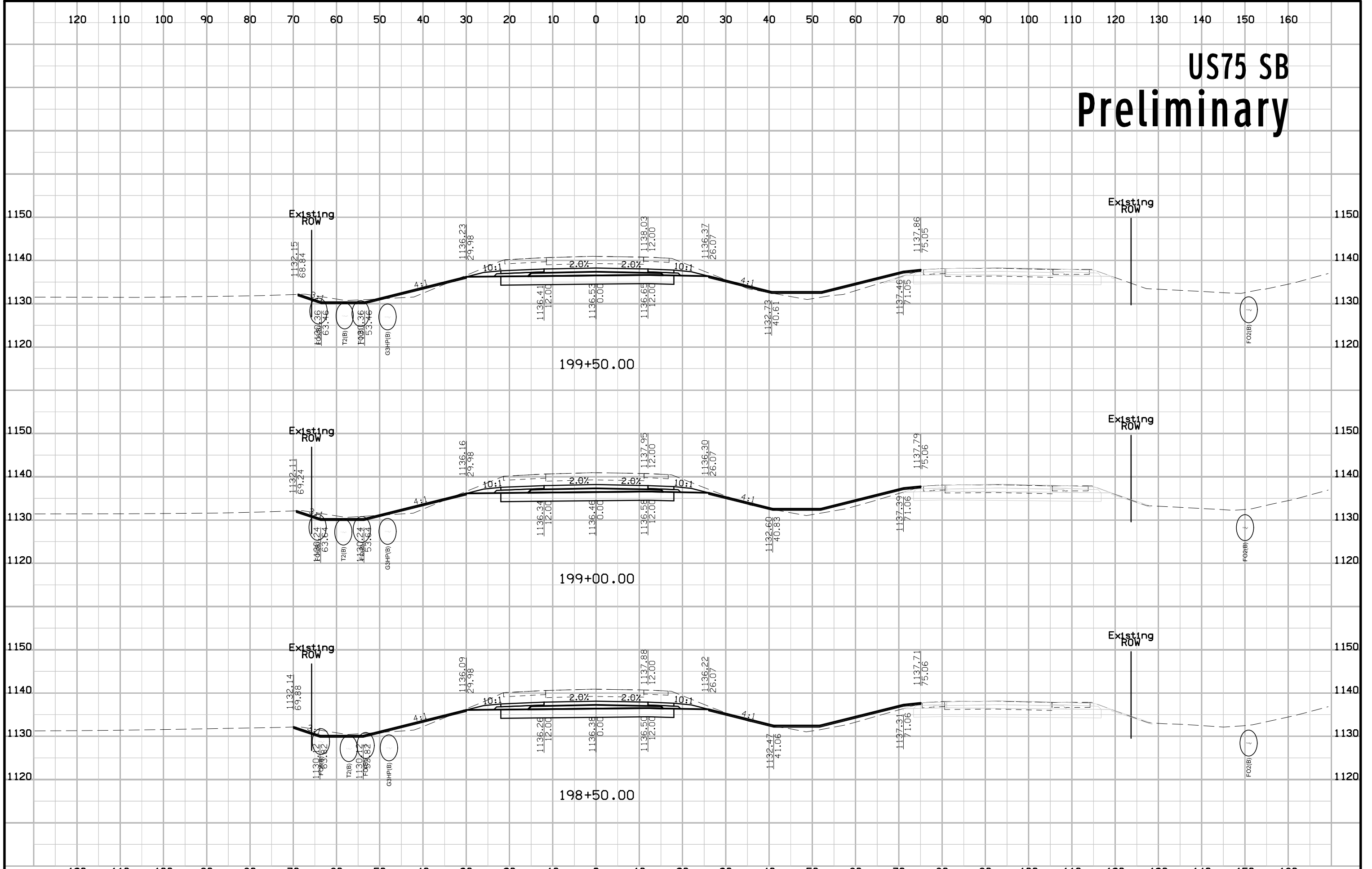


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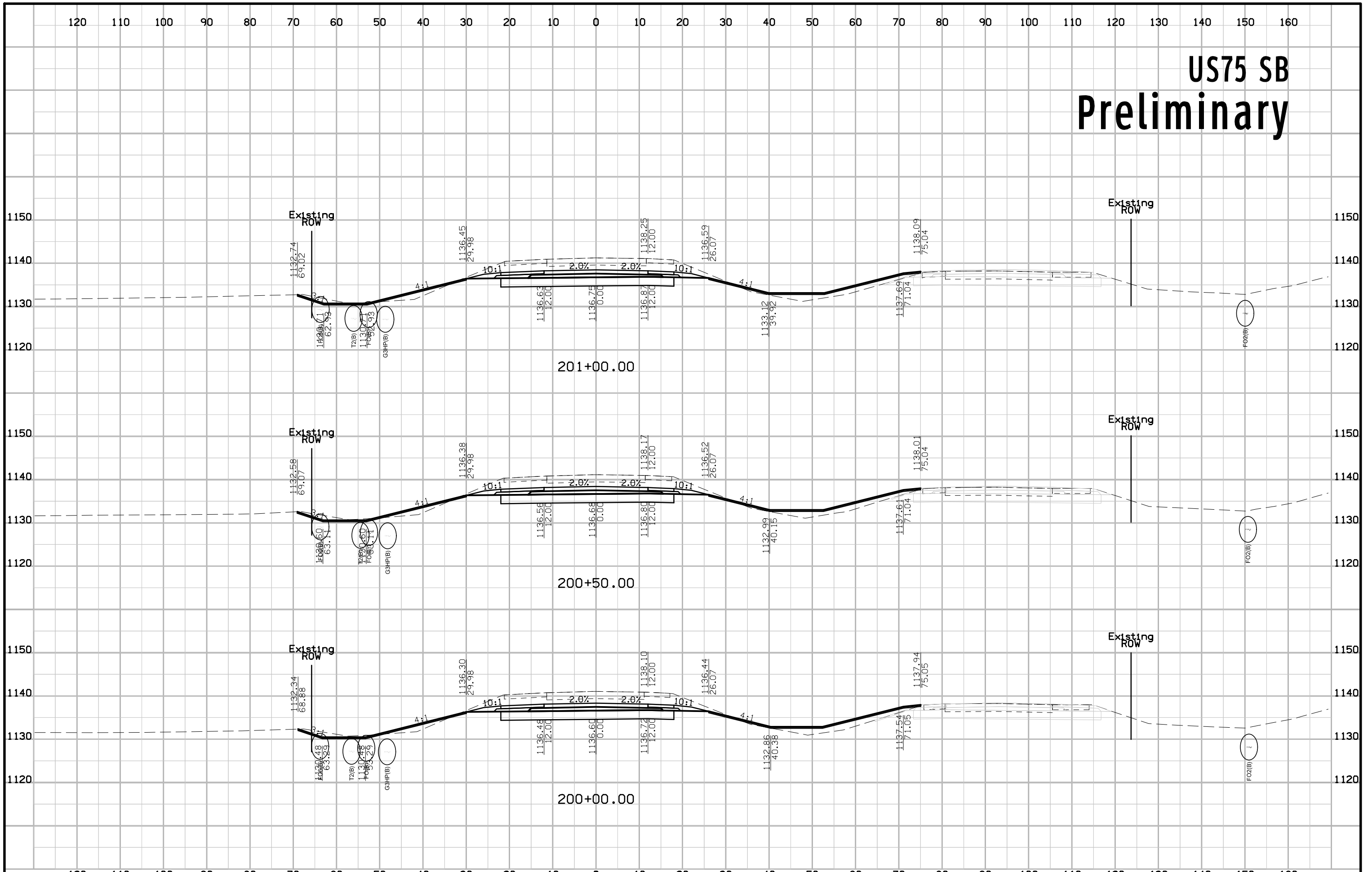




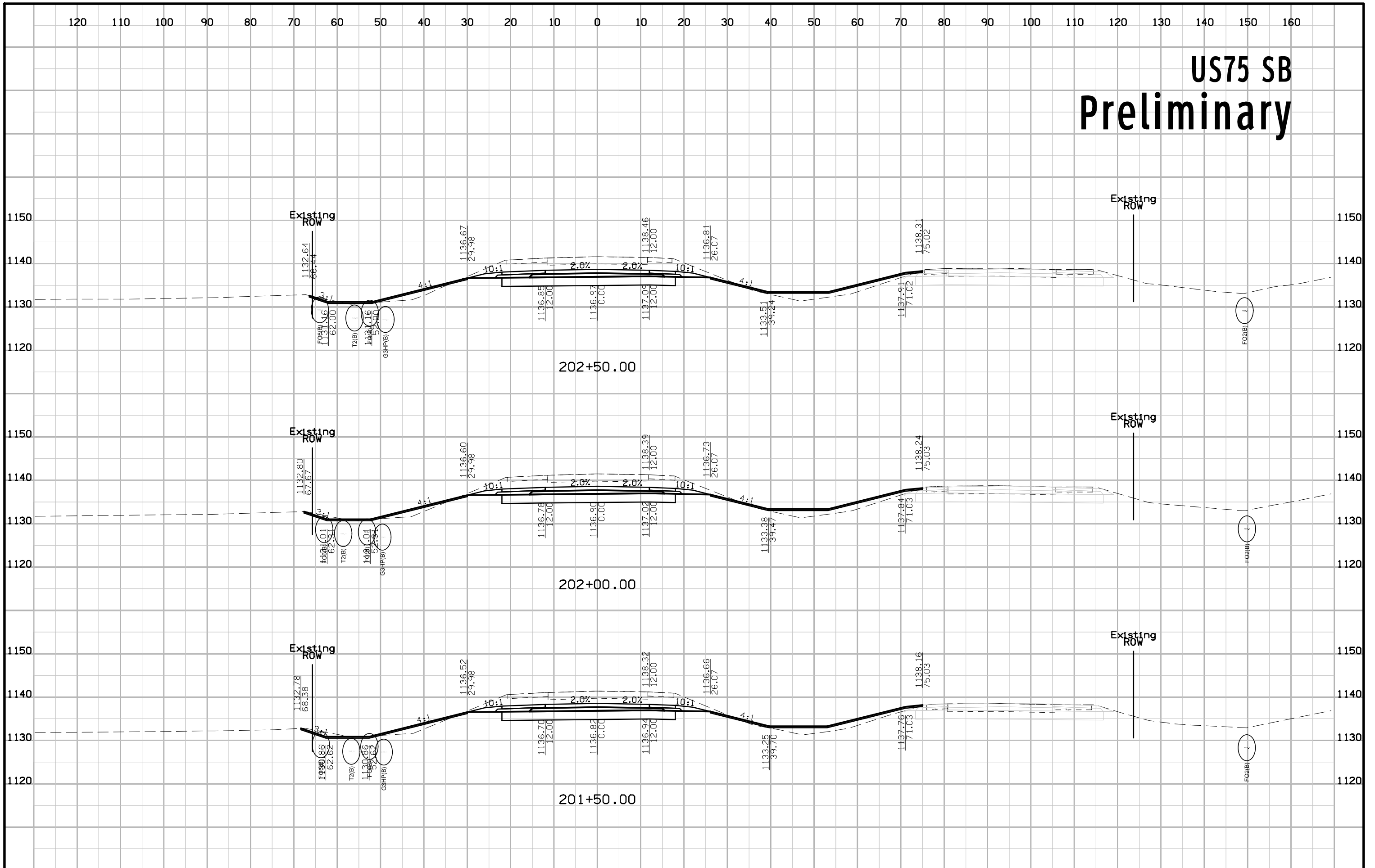
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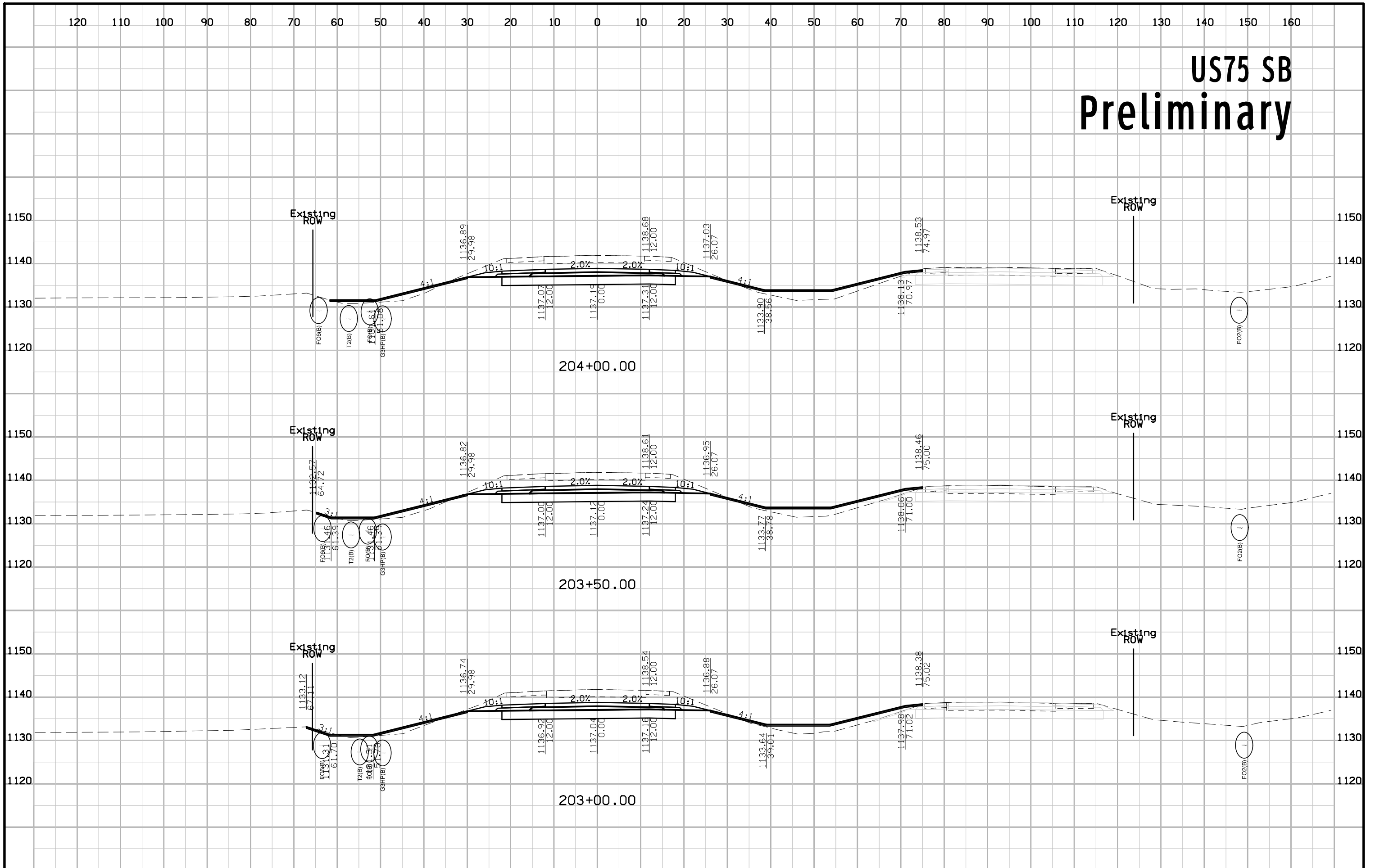
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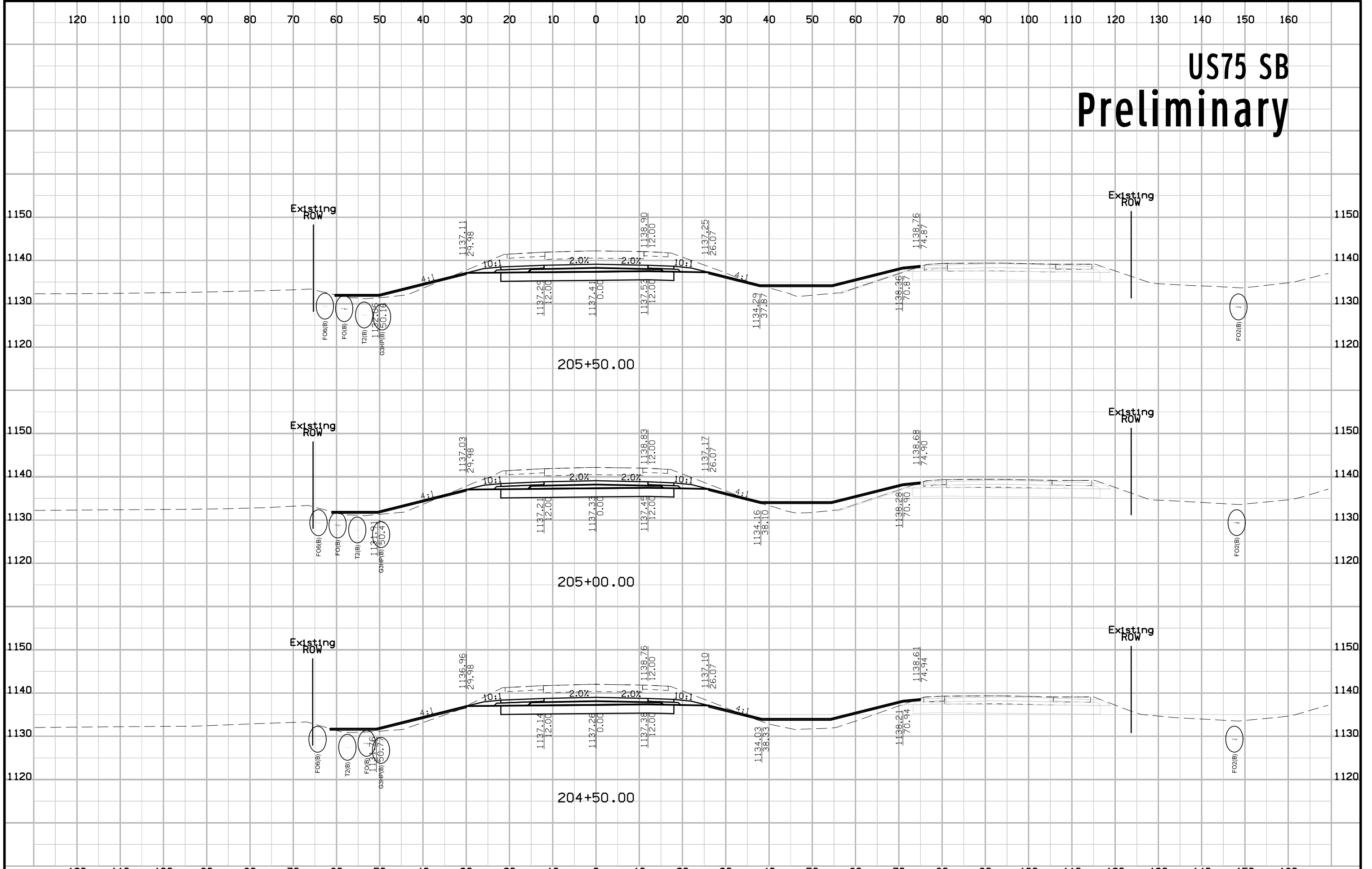
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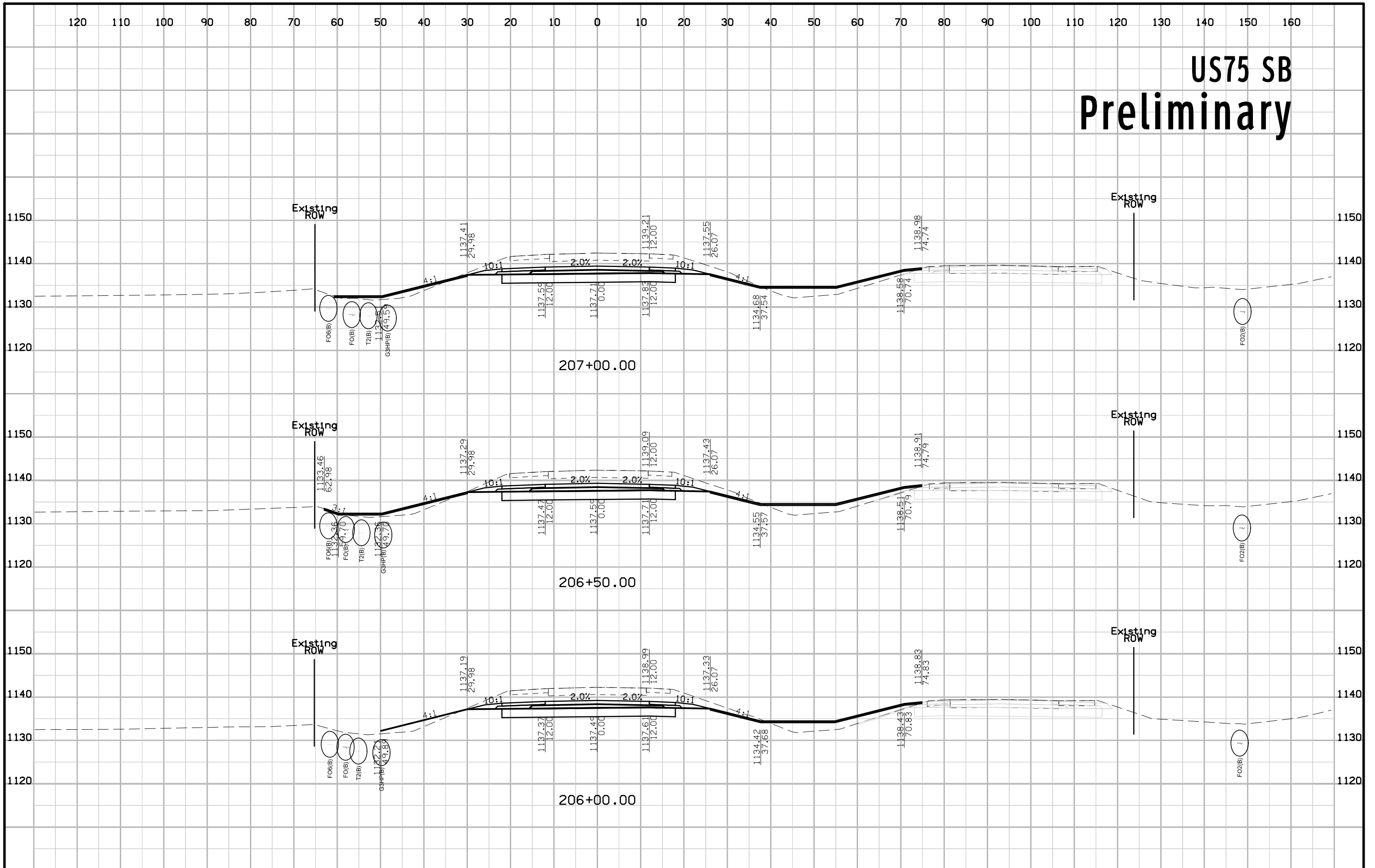
# US75 SB Preliminary



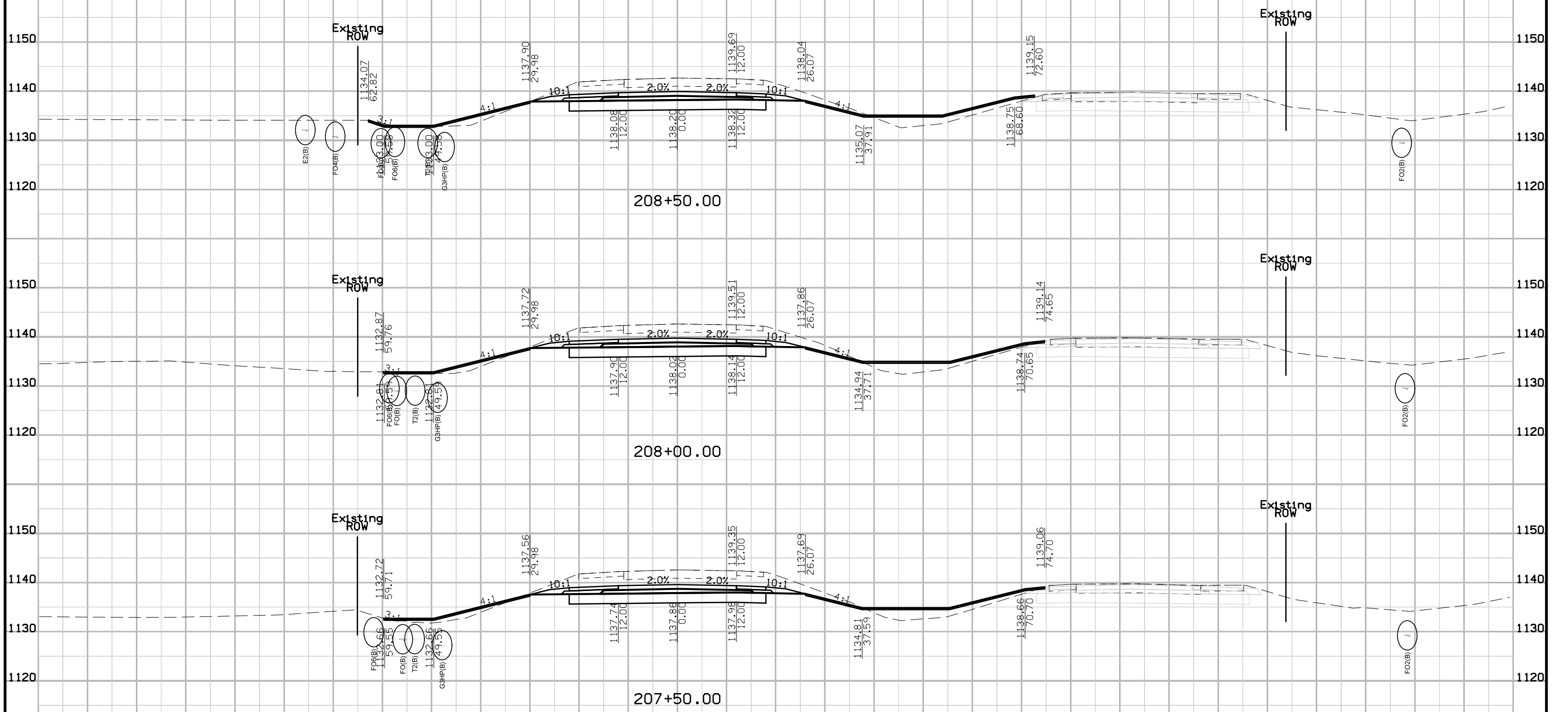
# US75 SB Preliminary



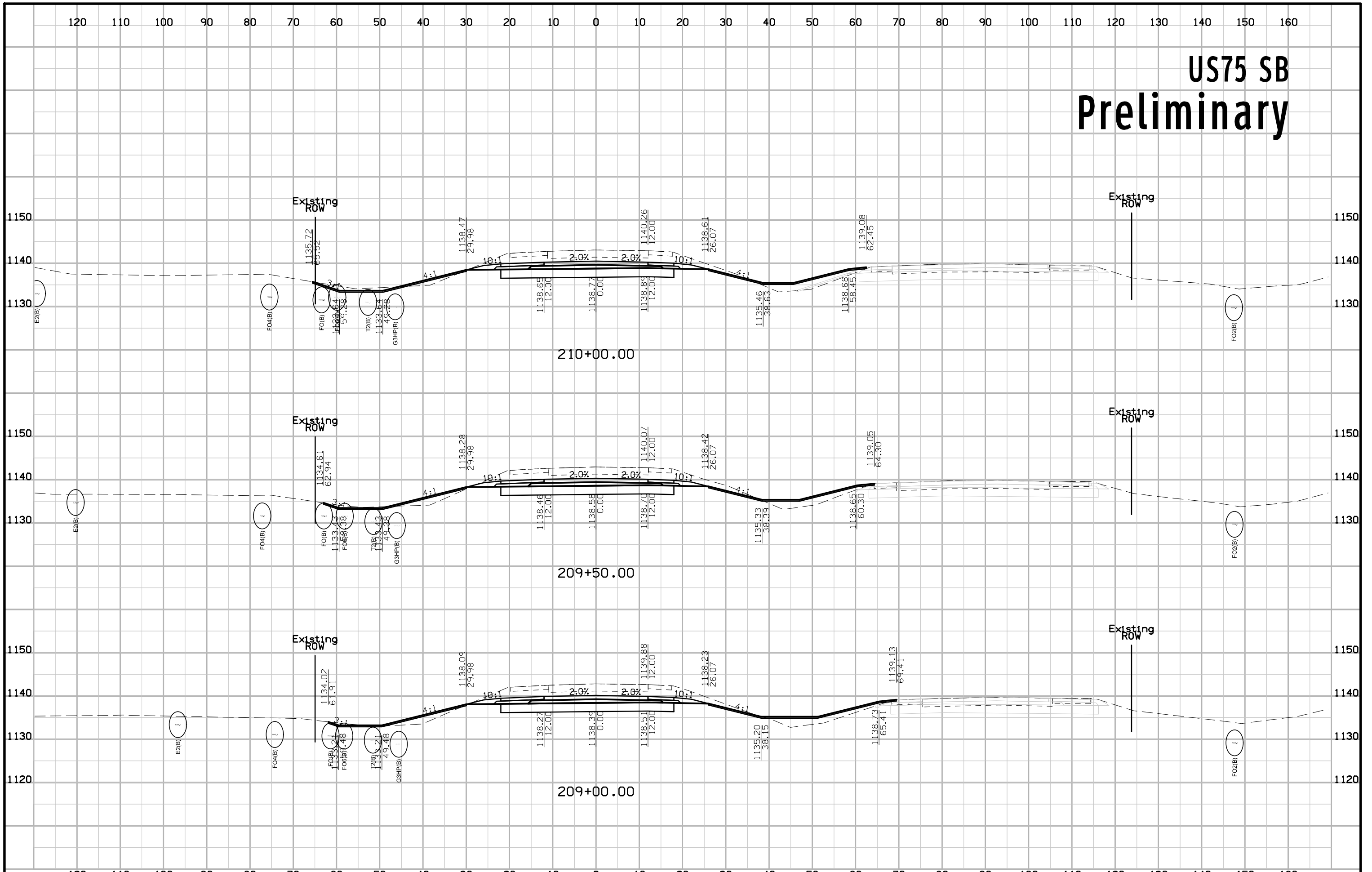
# US75 SB Preliminary



# US75 SB Preliminary

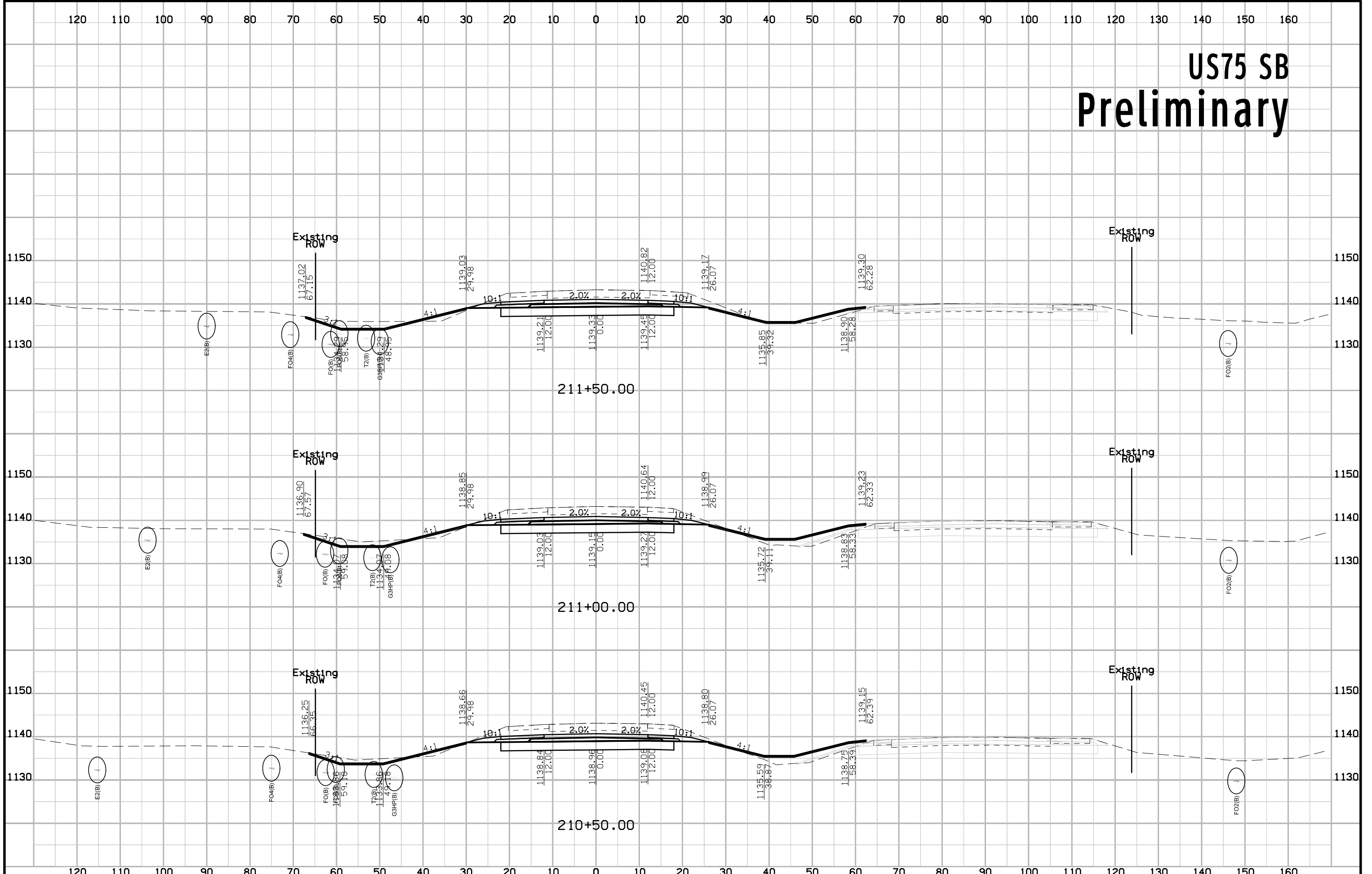


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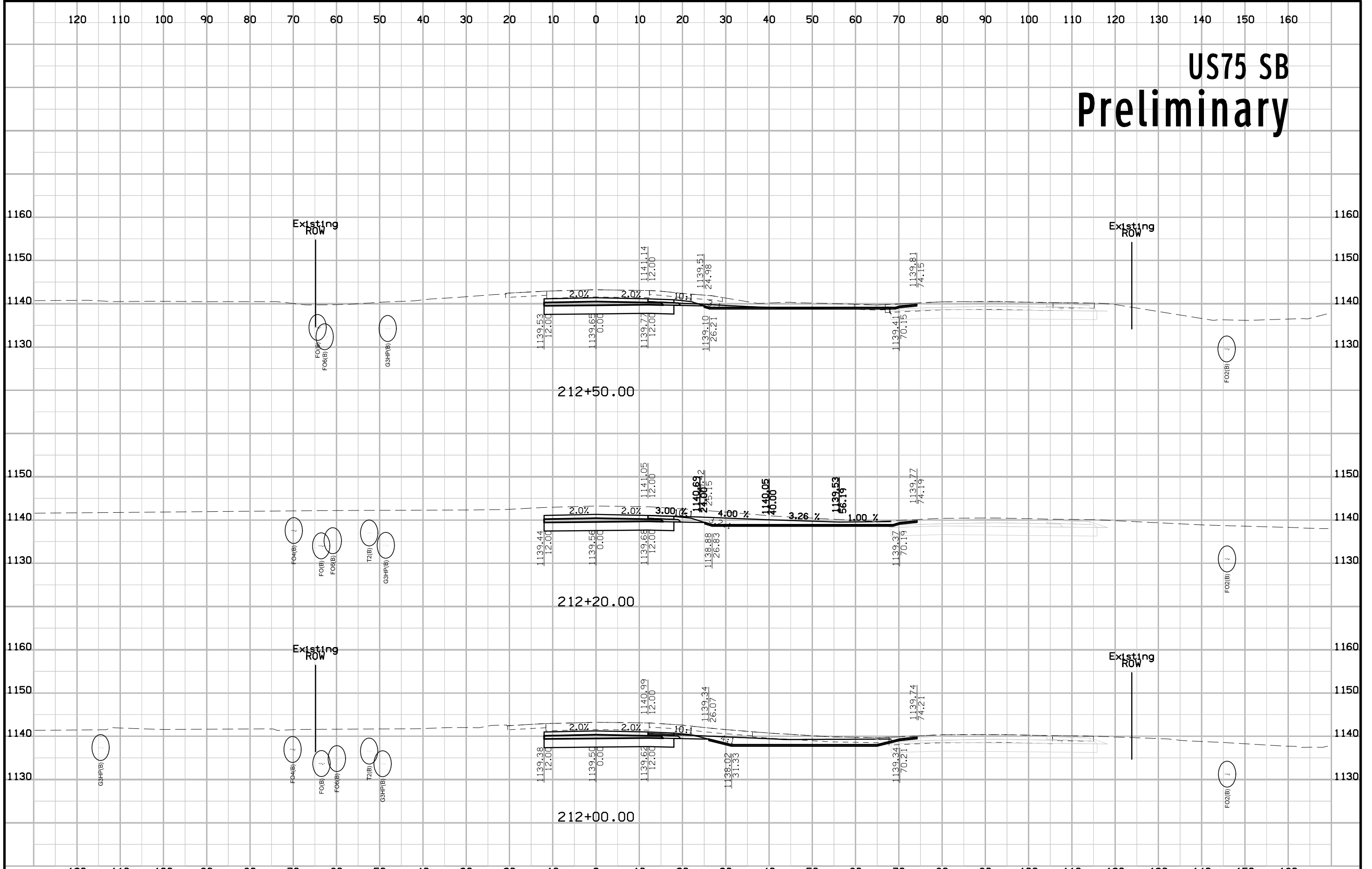




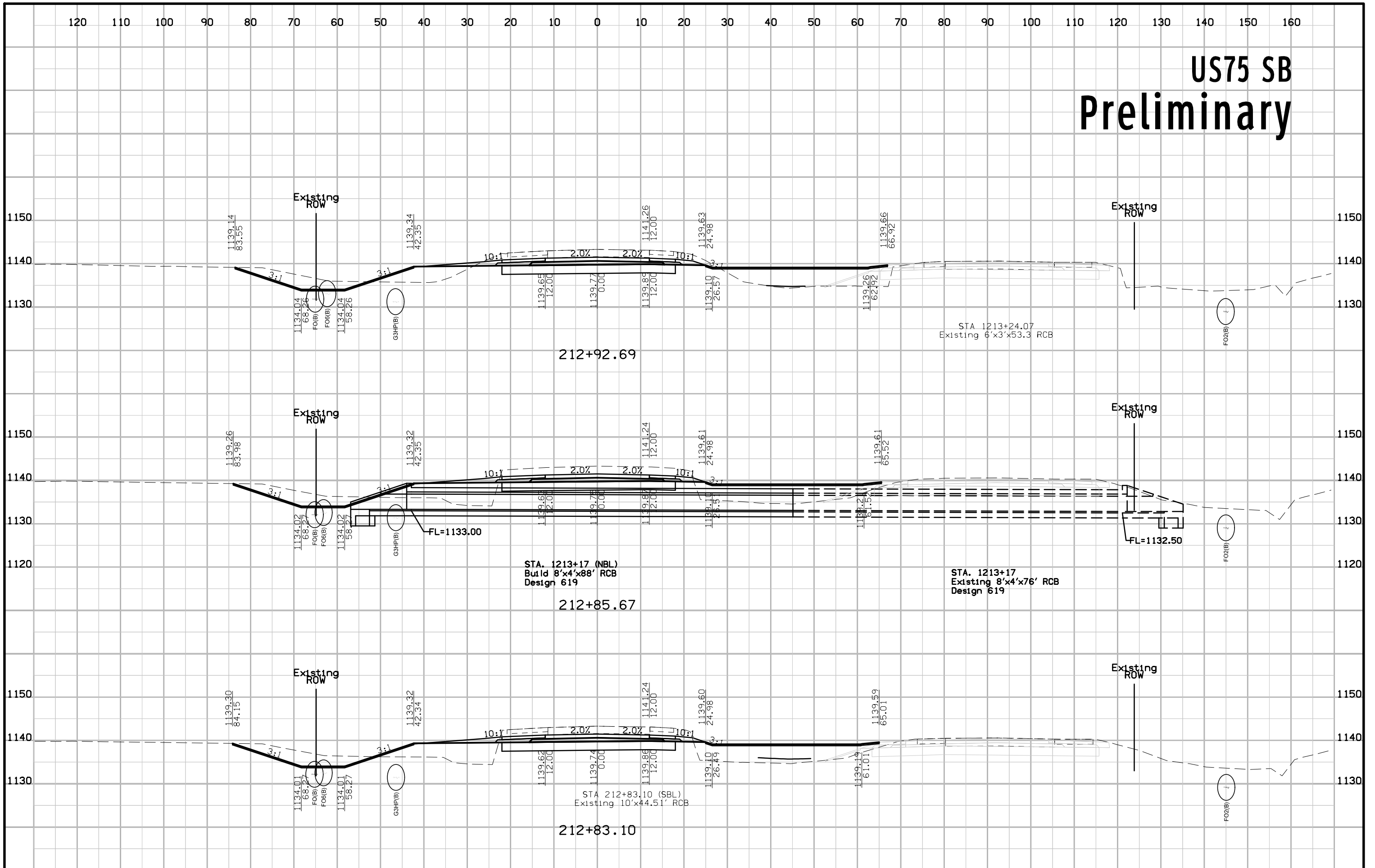
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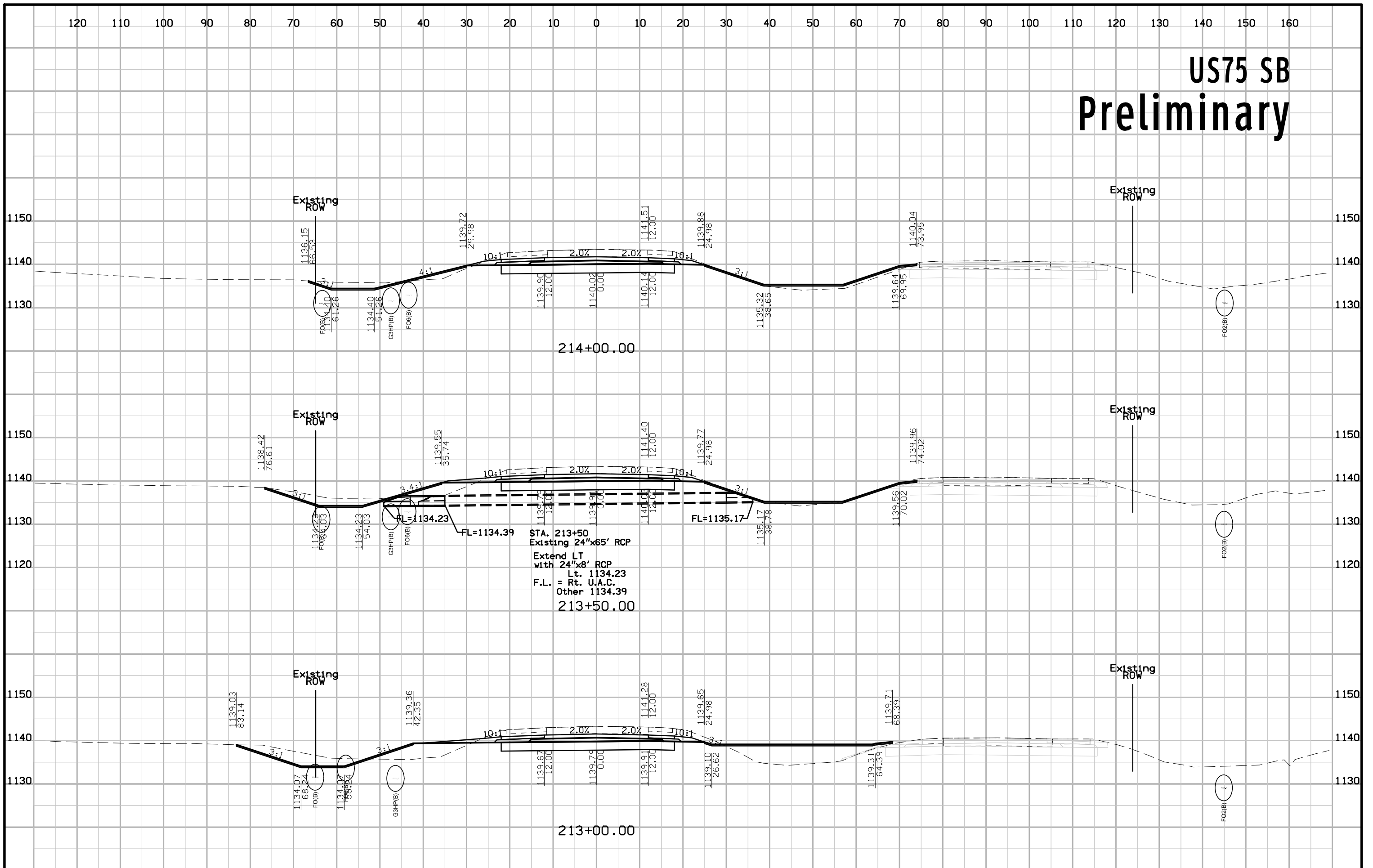
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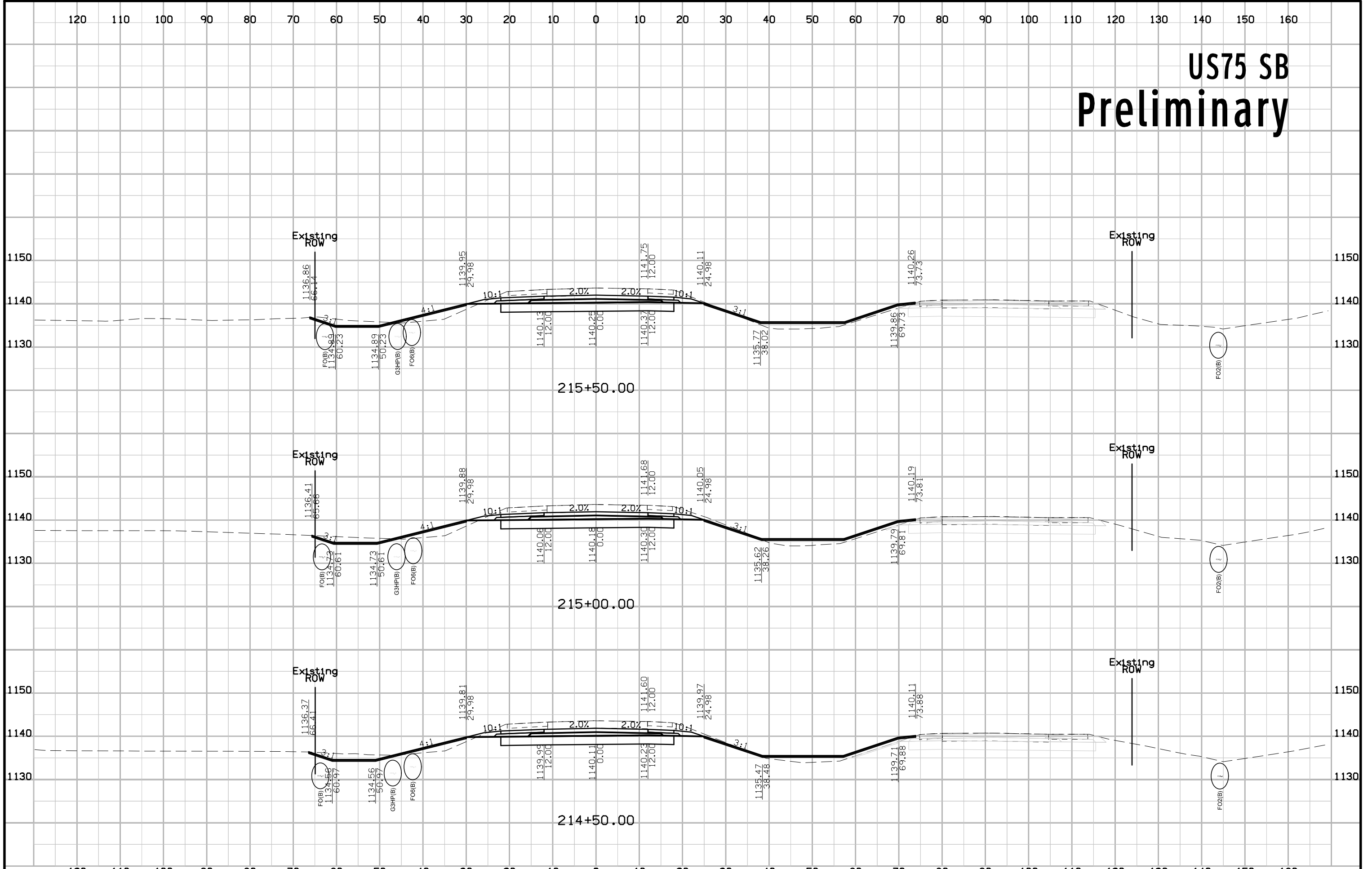
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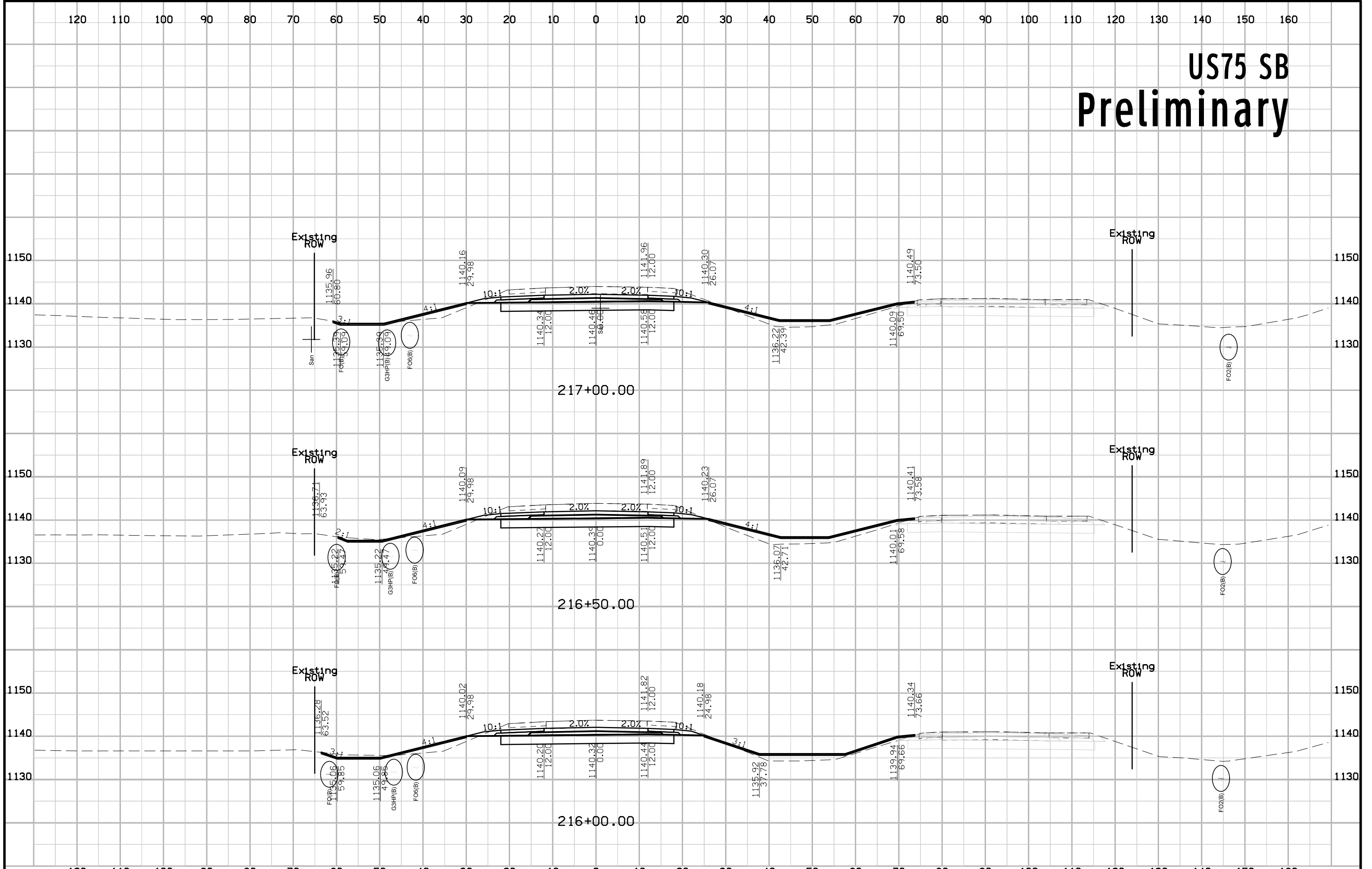
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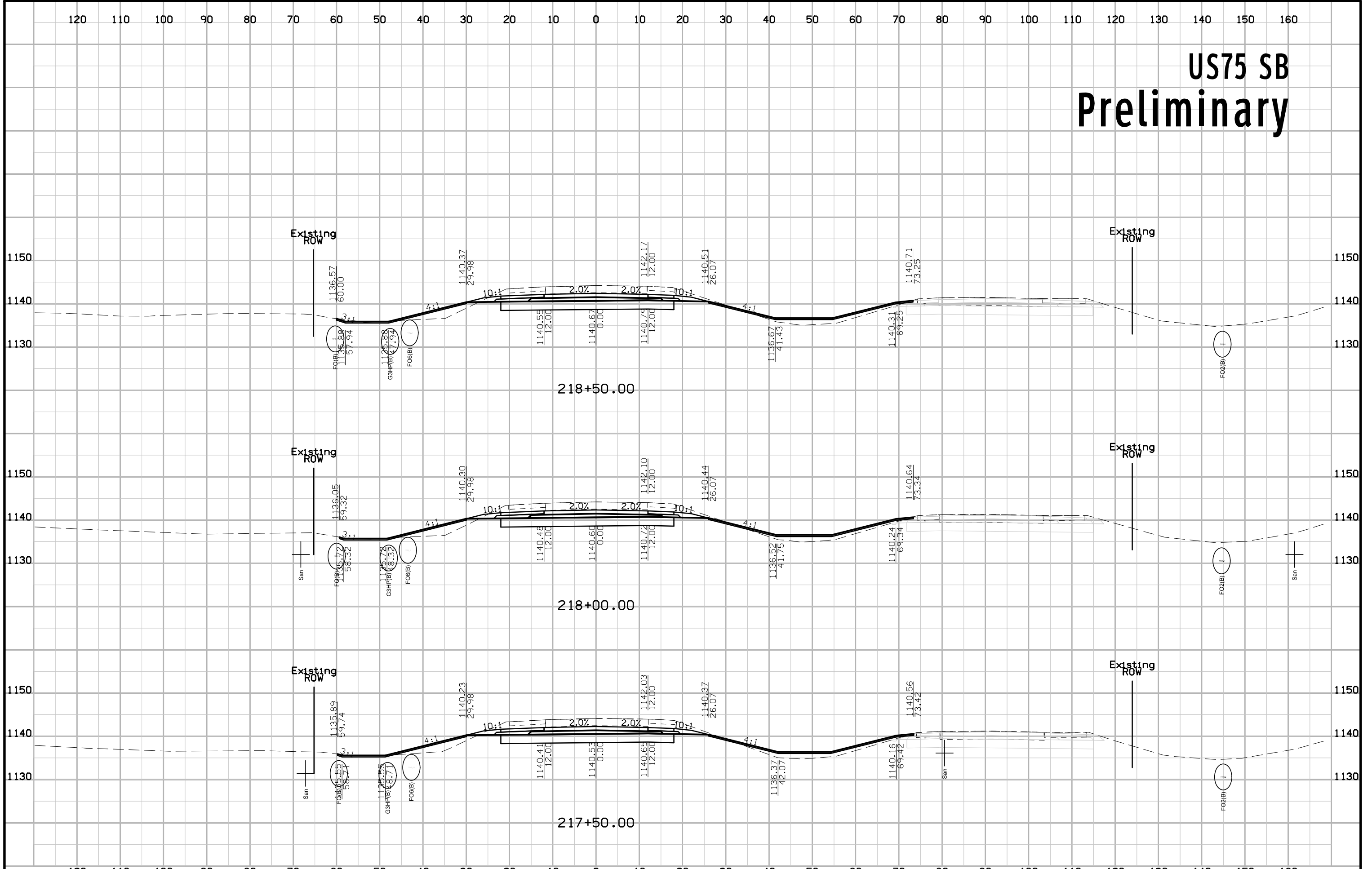
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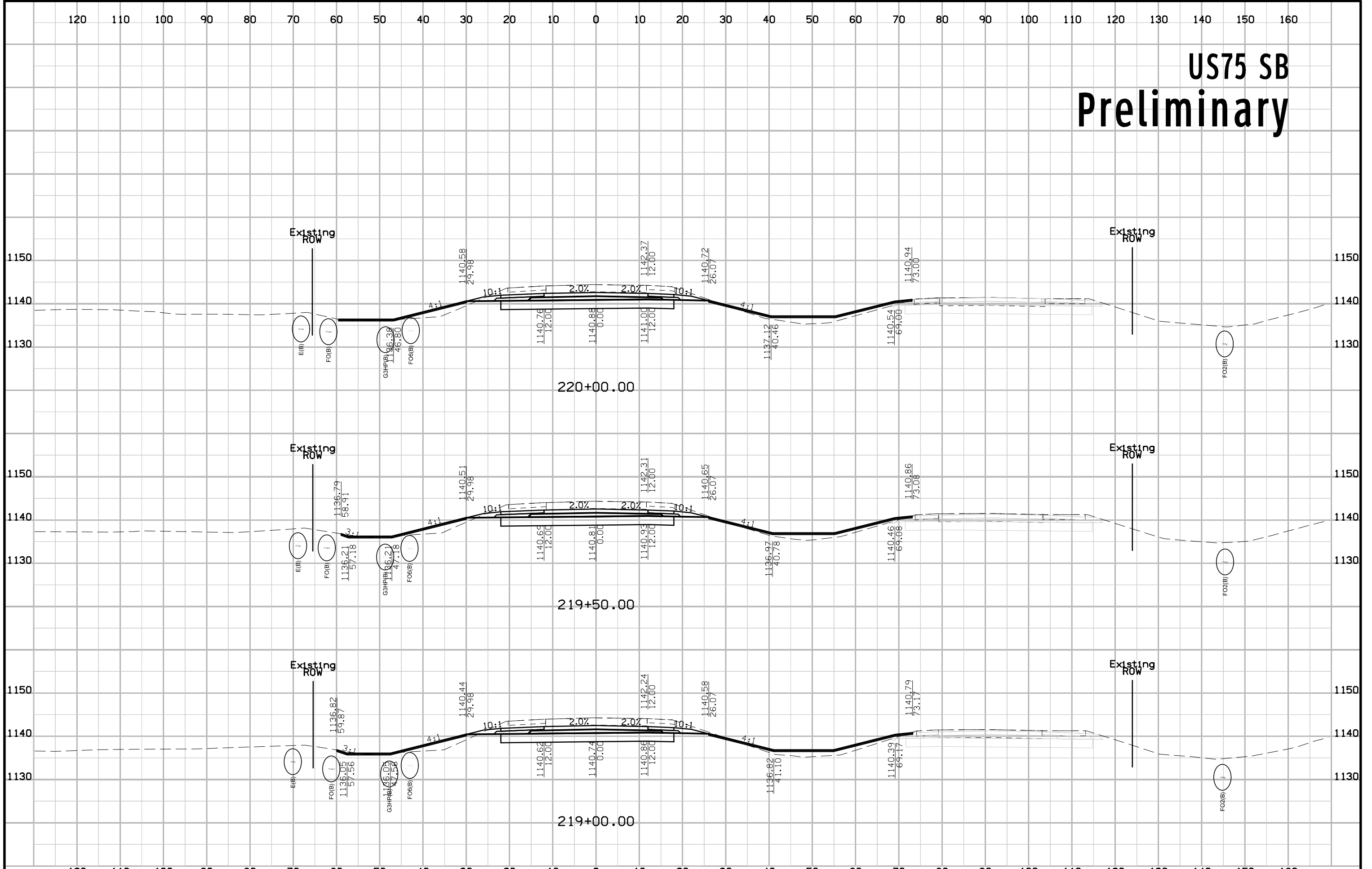
# US75 SB Preliminary



# US75 SB Preliminary

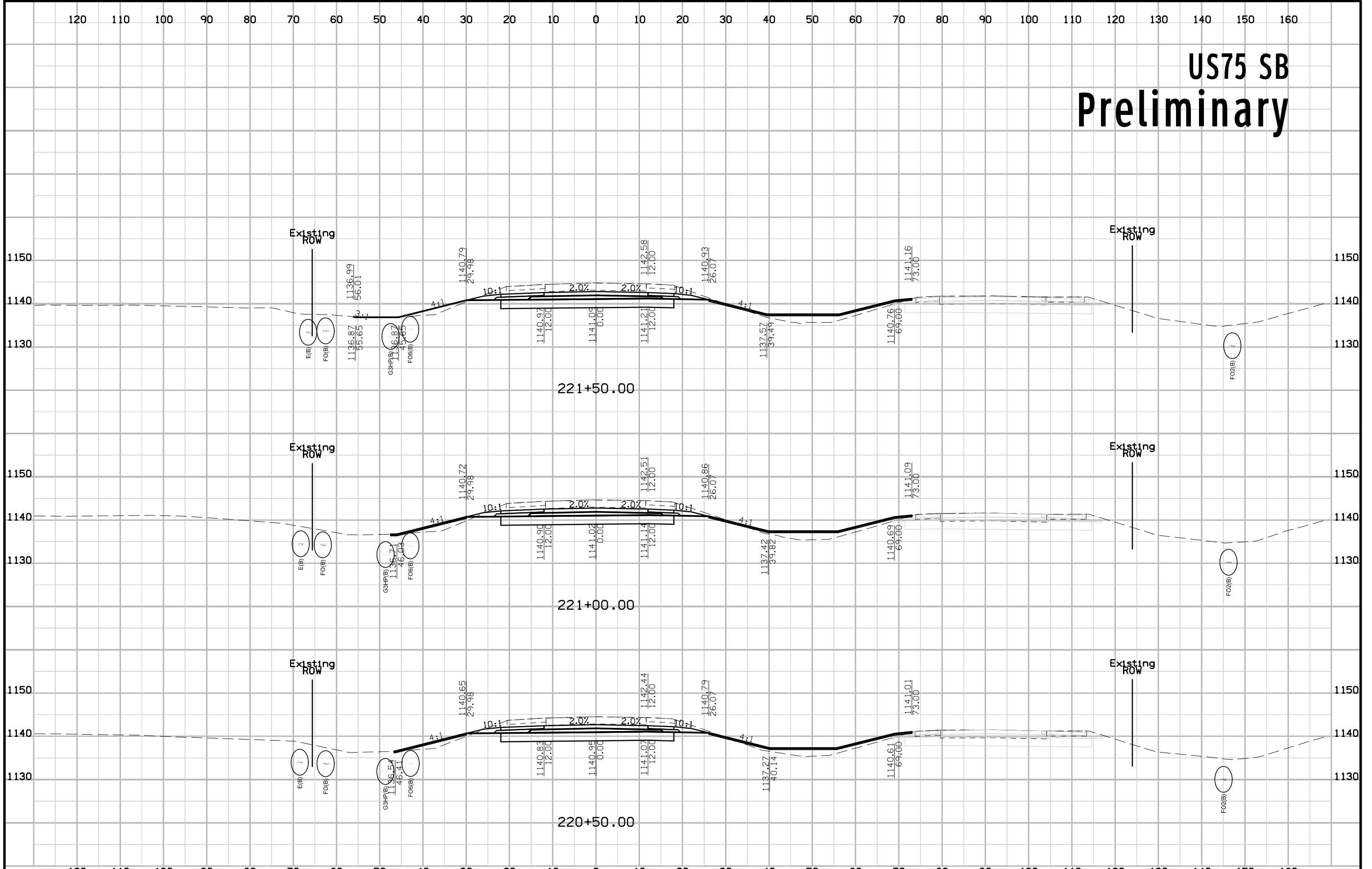


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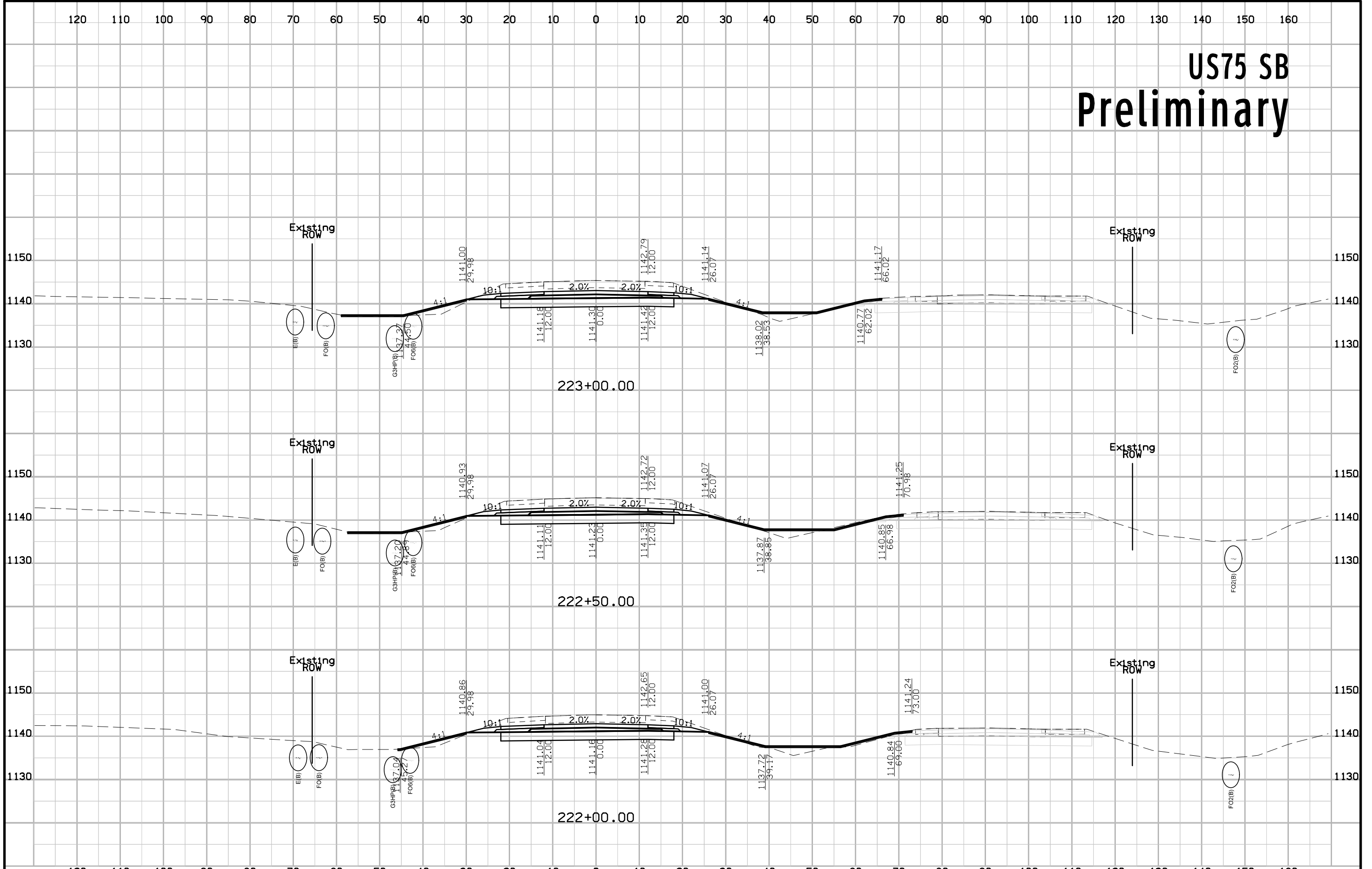




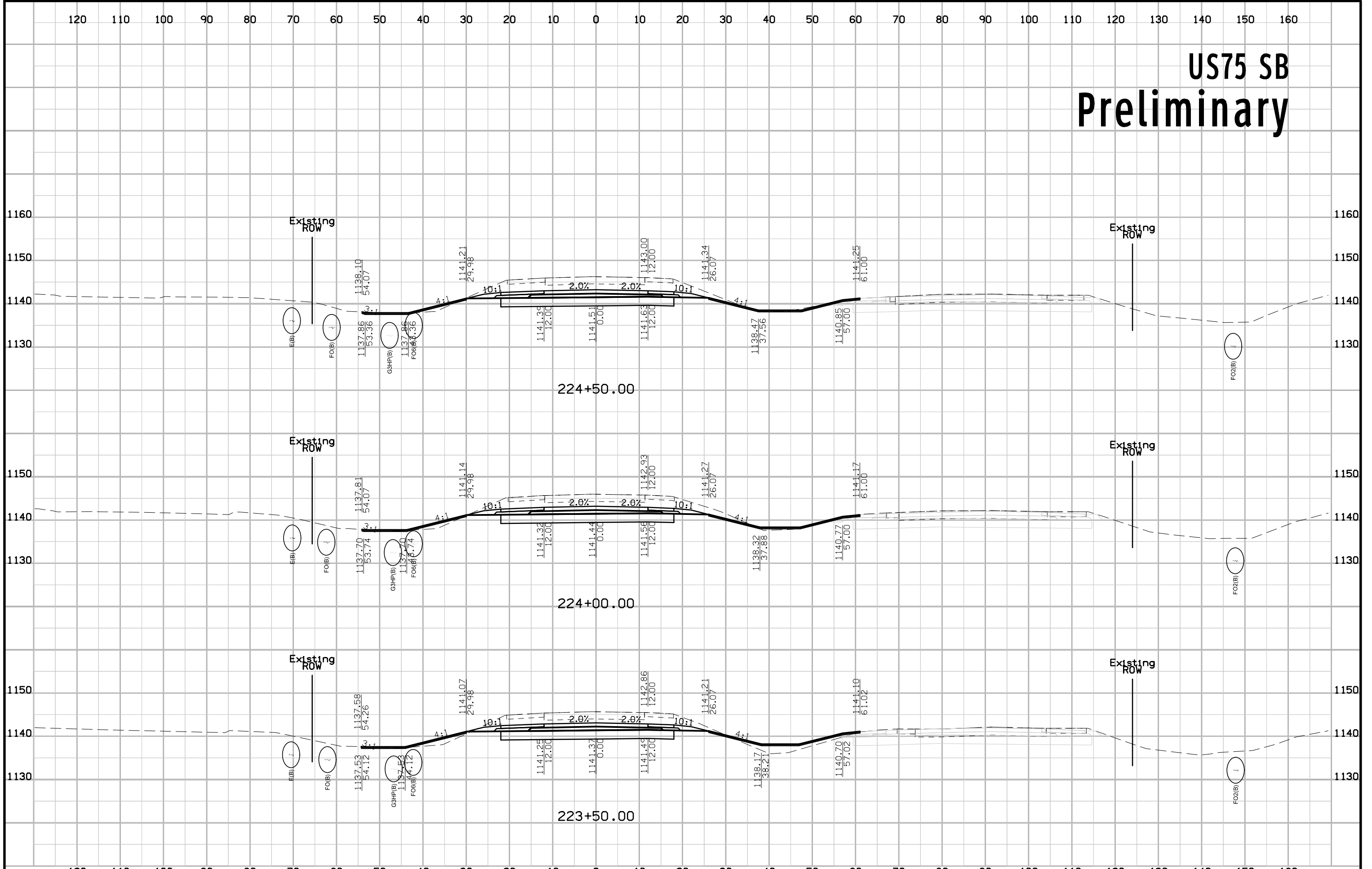
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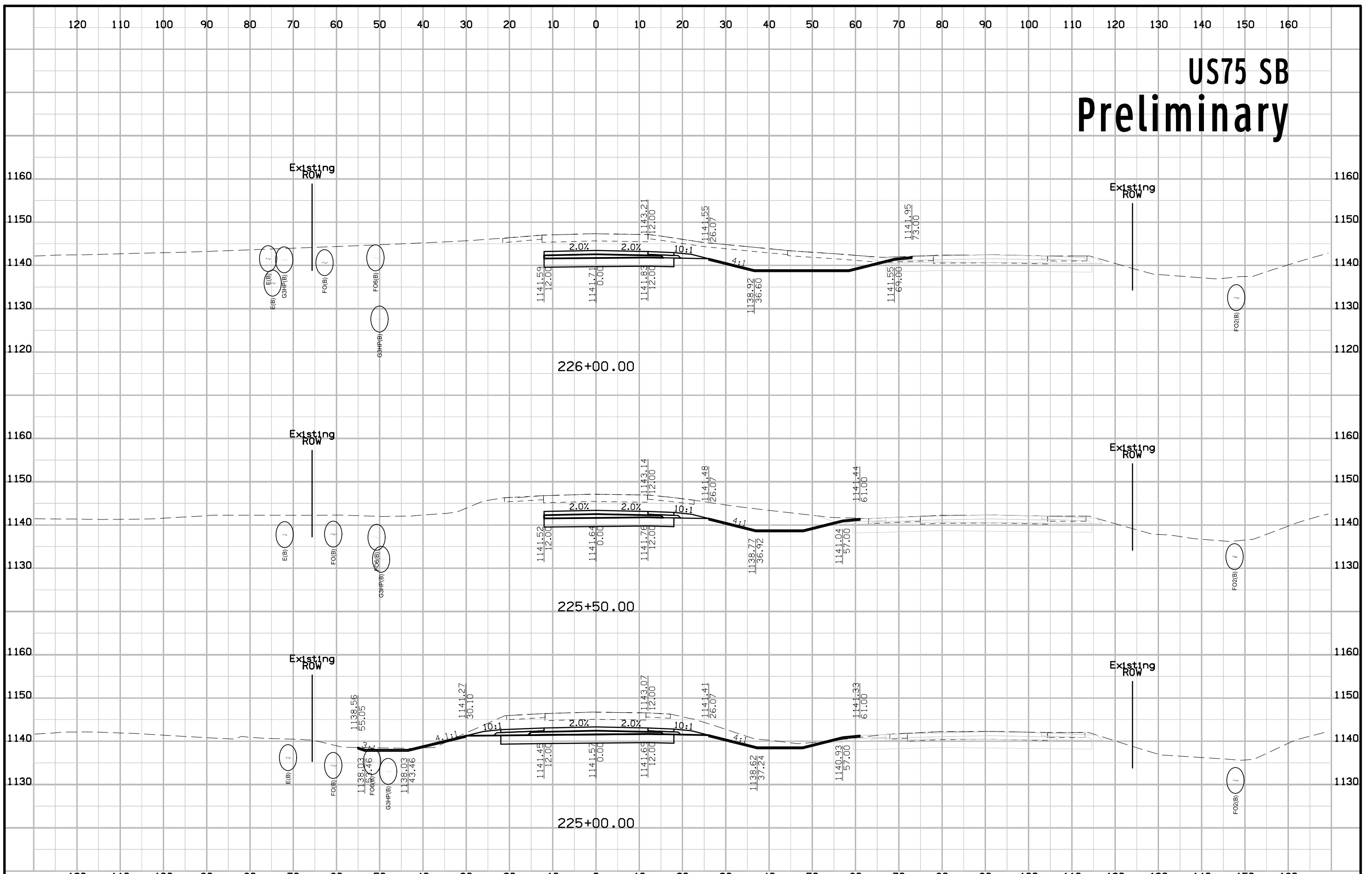
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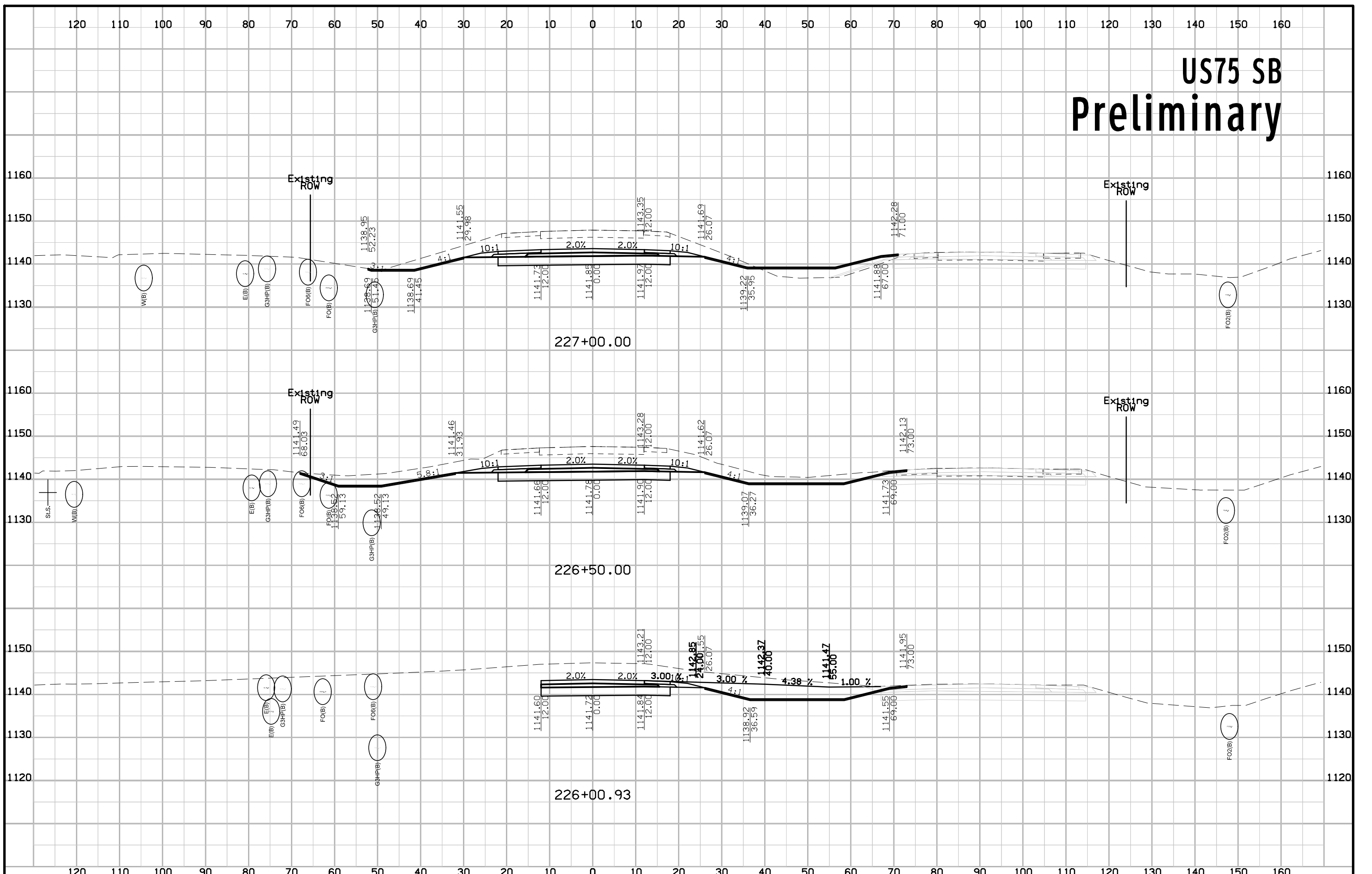
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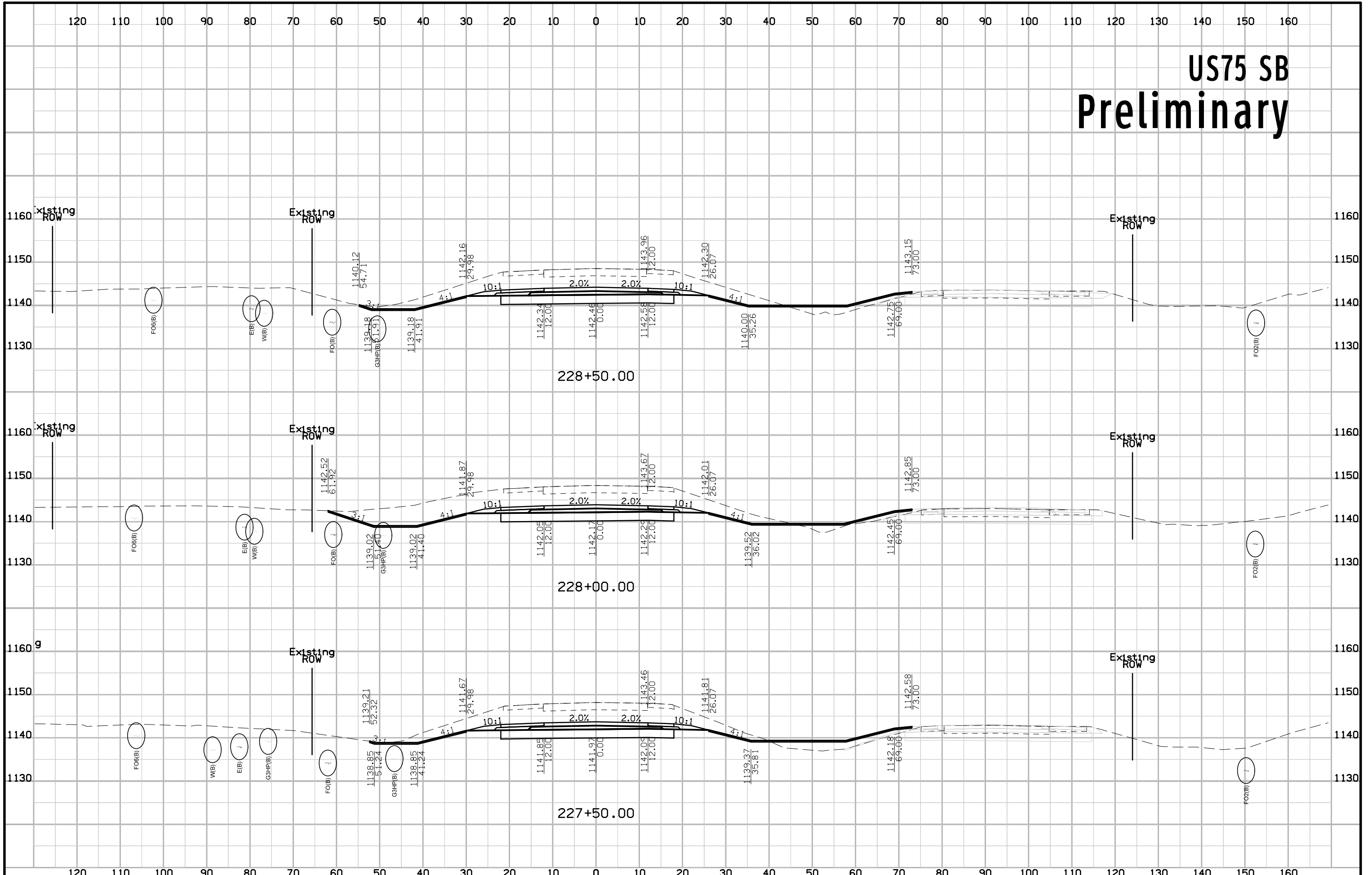
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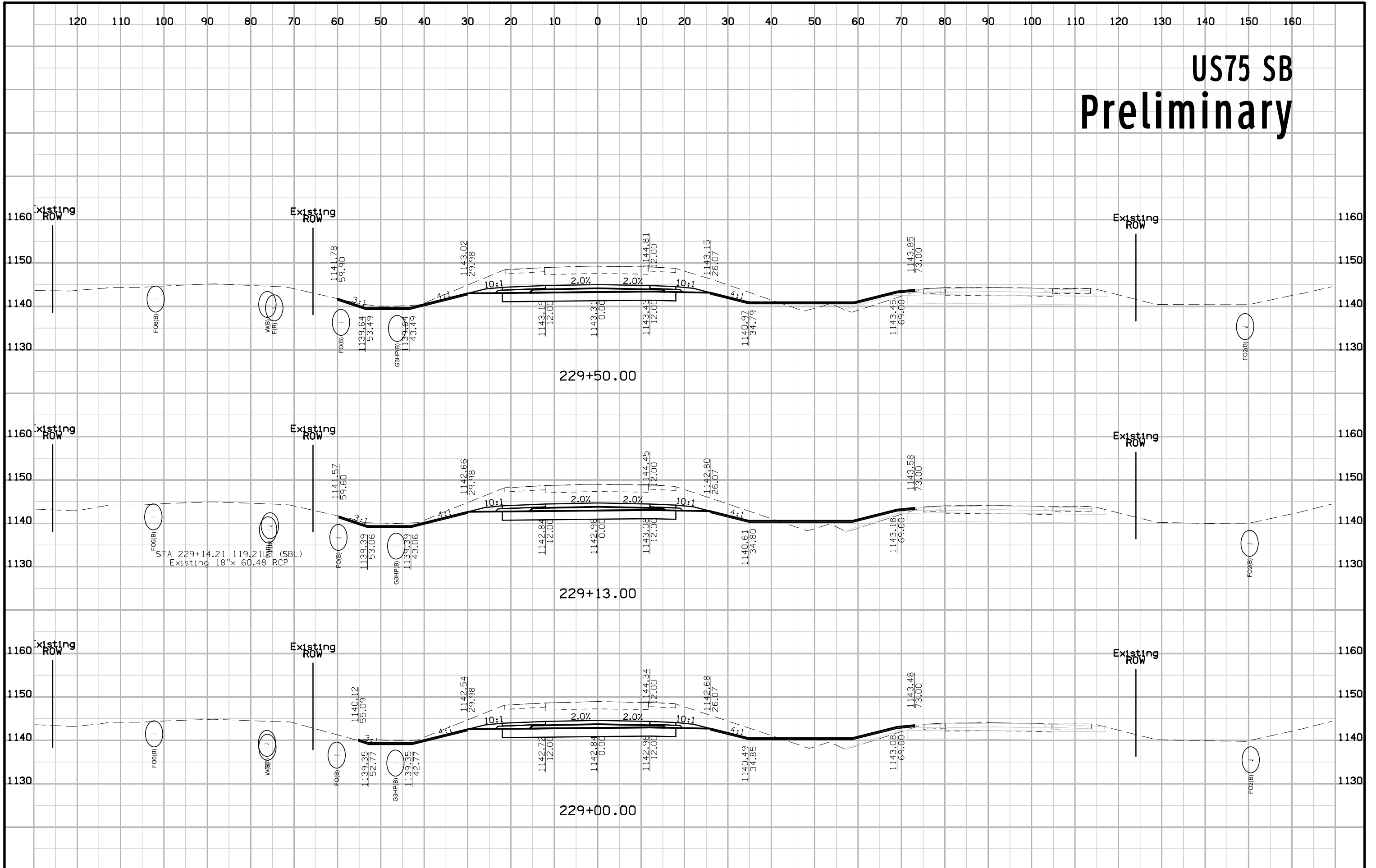
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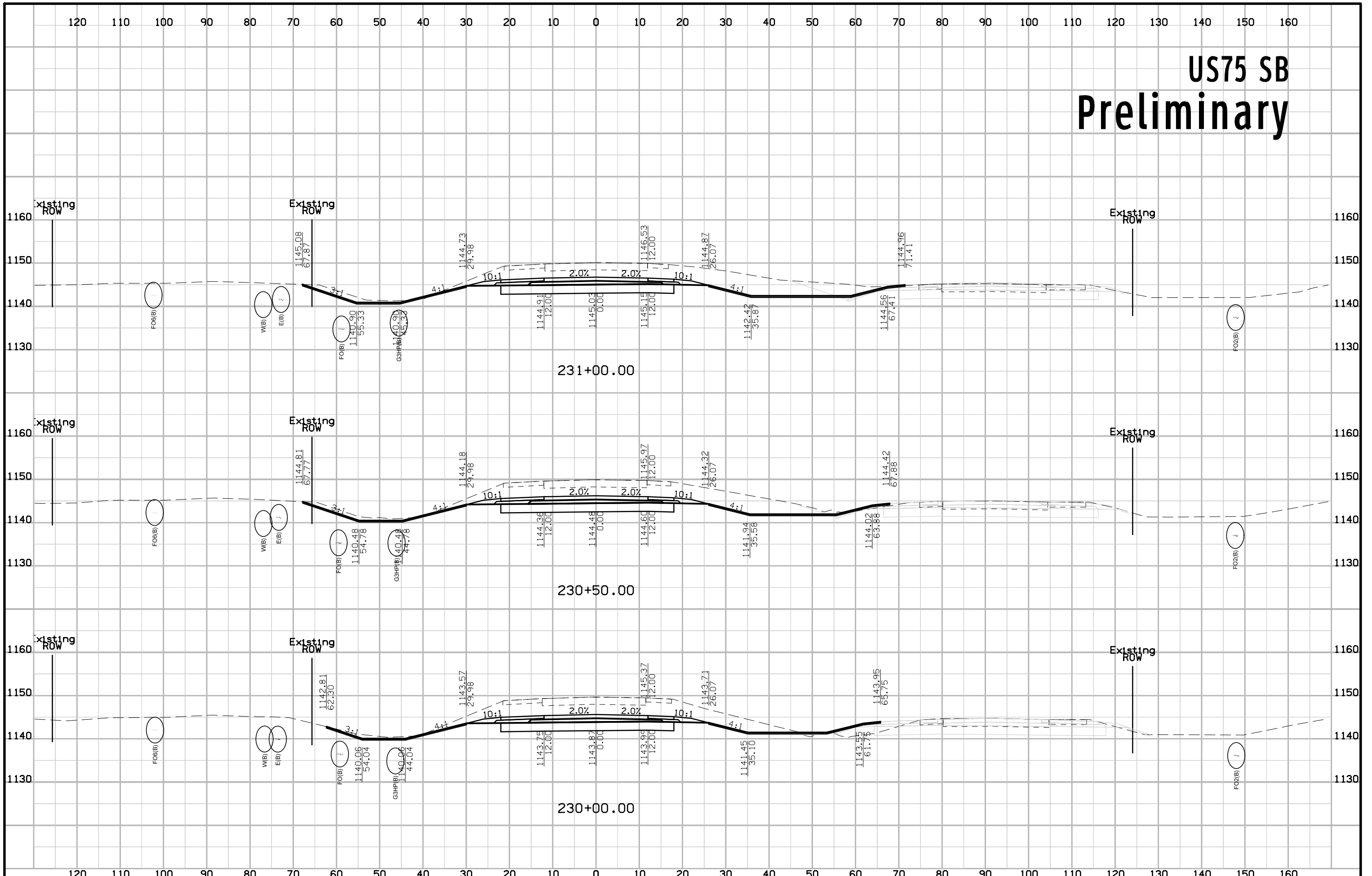
# US75 SB Preliminary



# US75 SB Preliminary

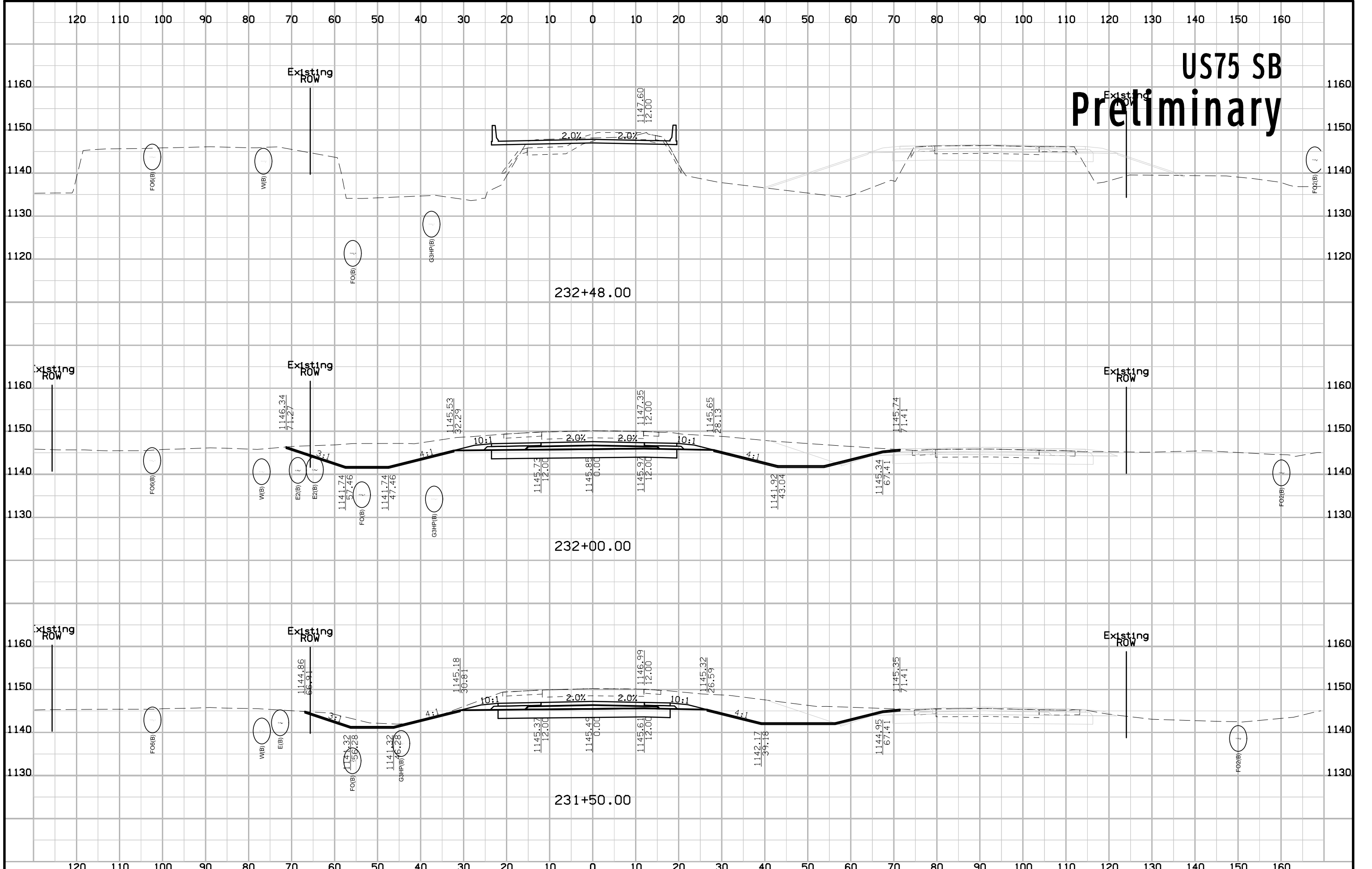


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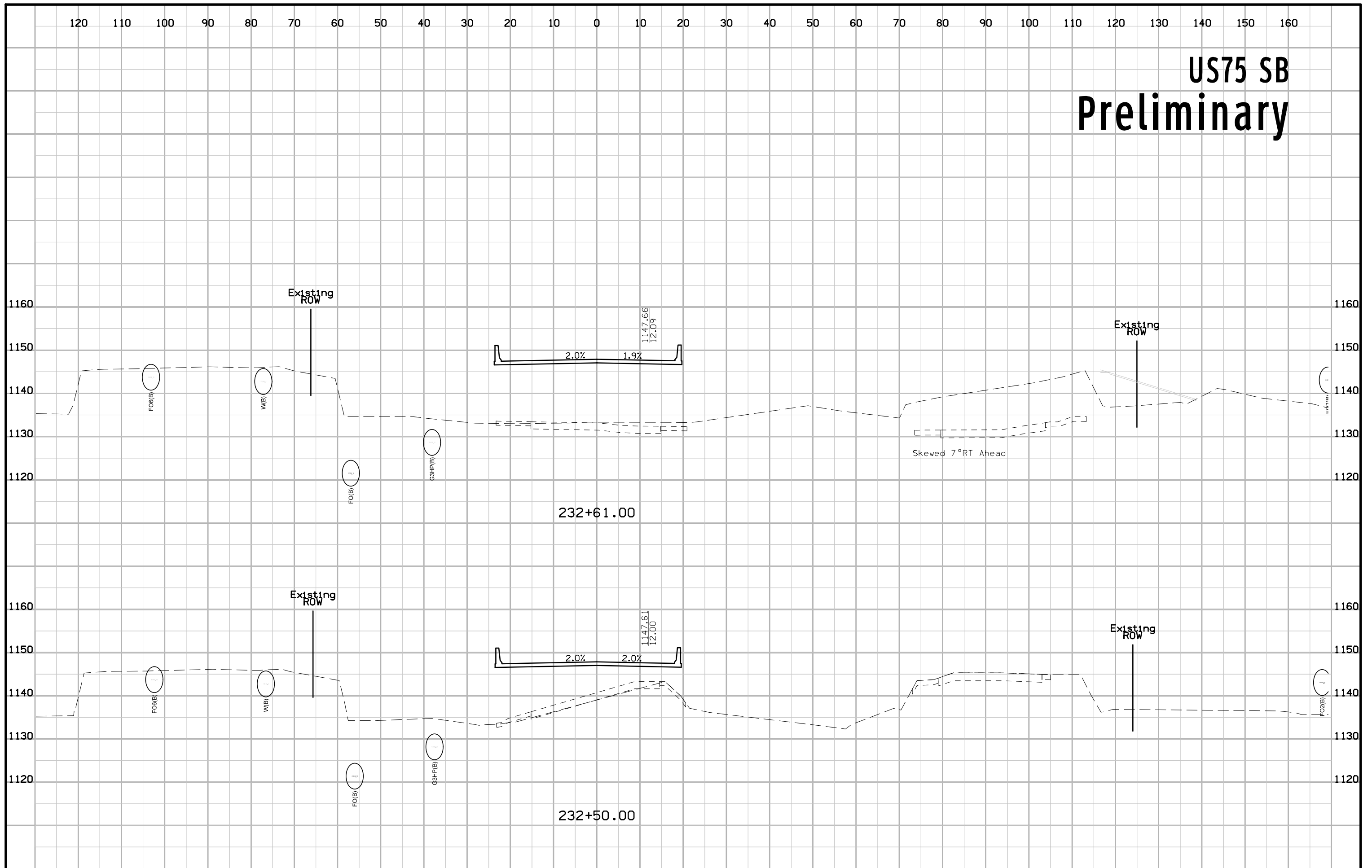




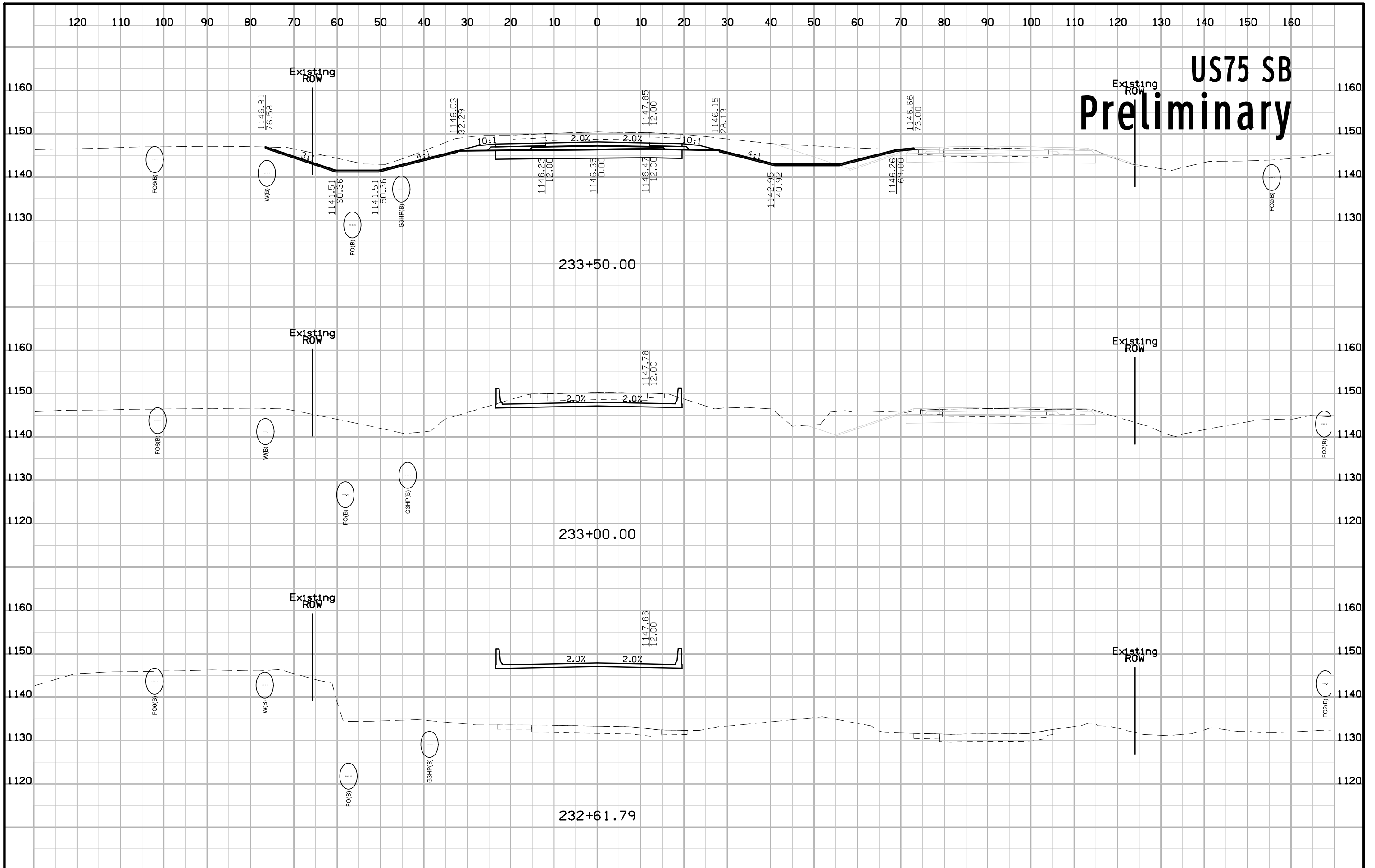
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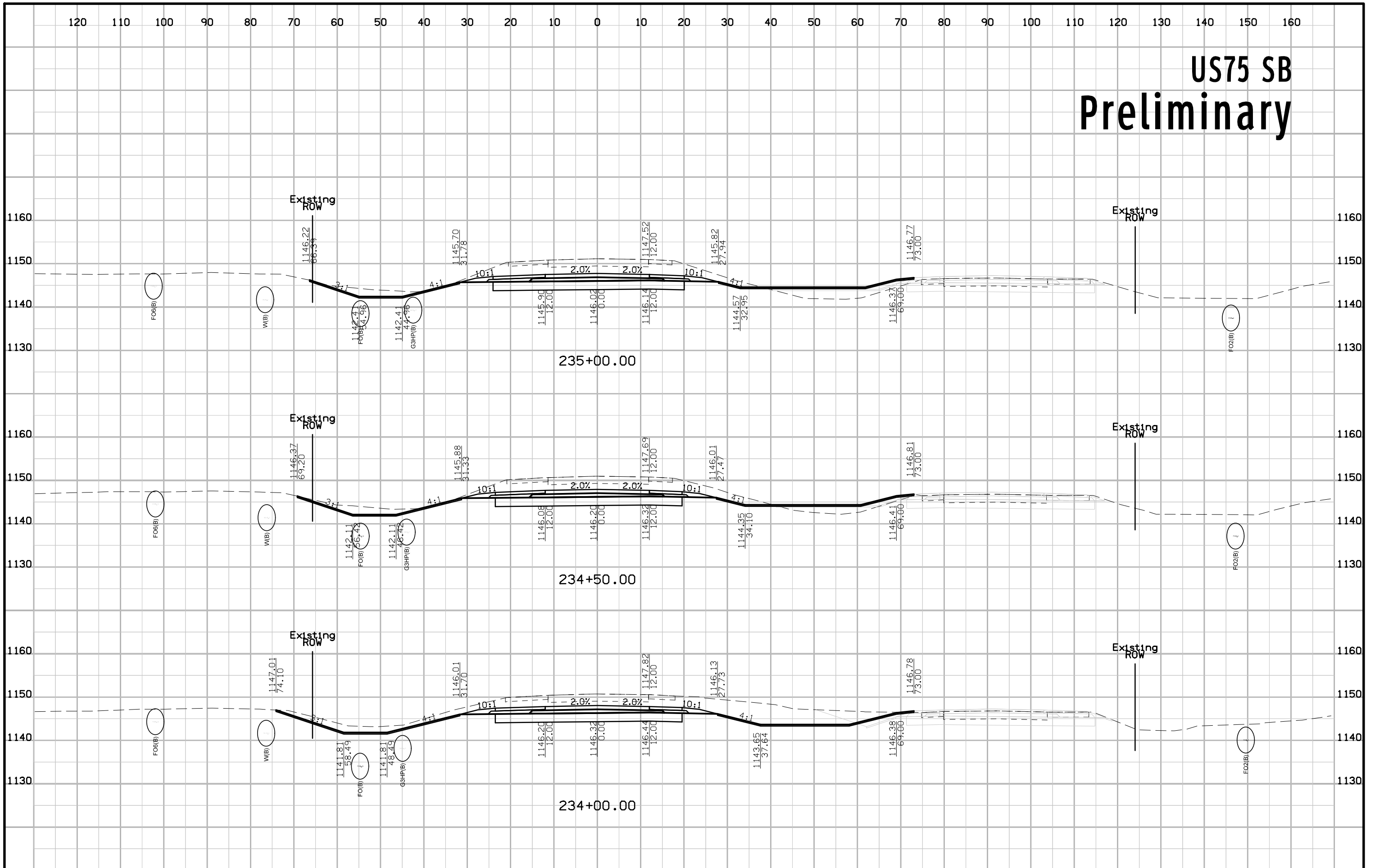
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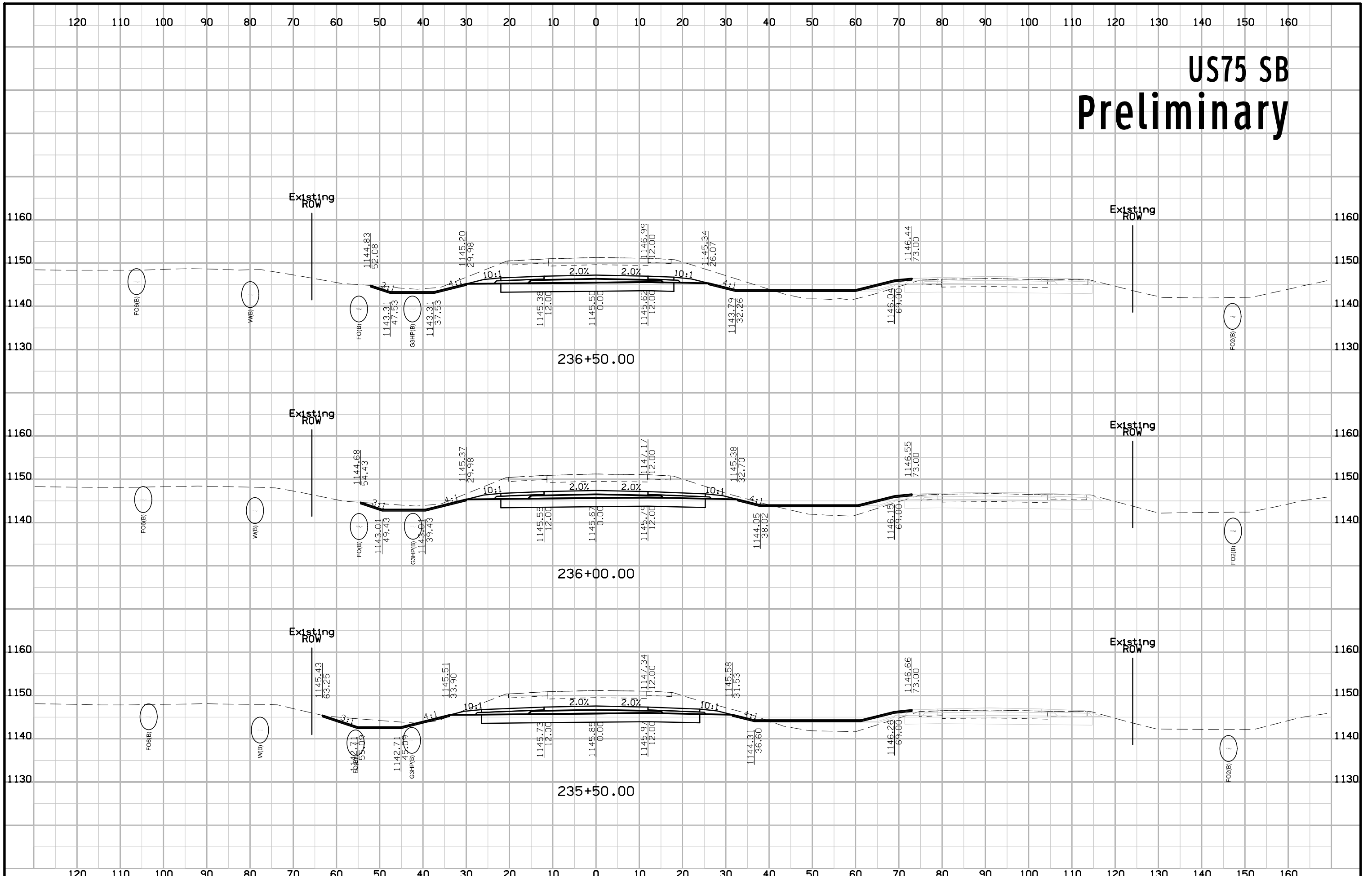
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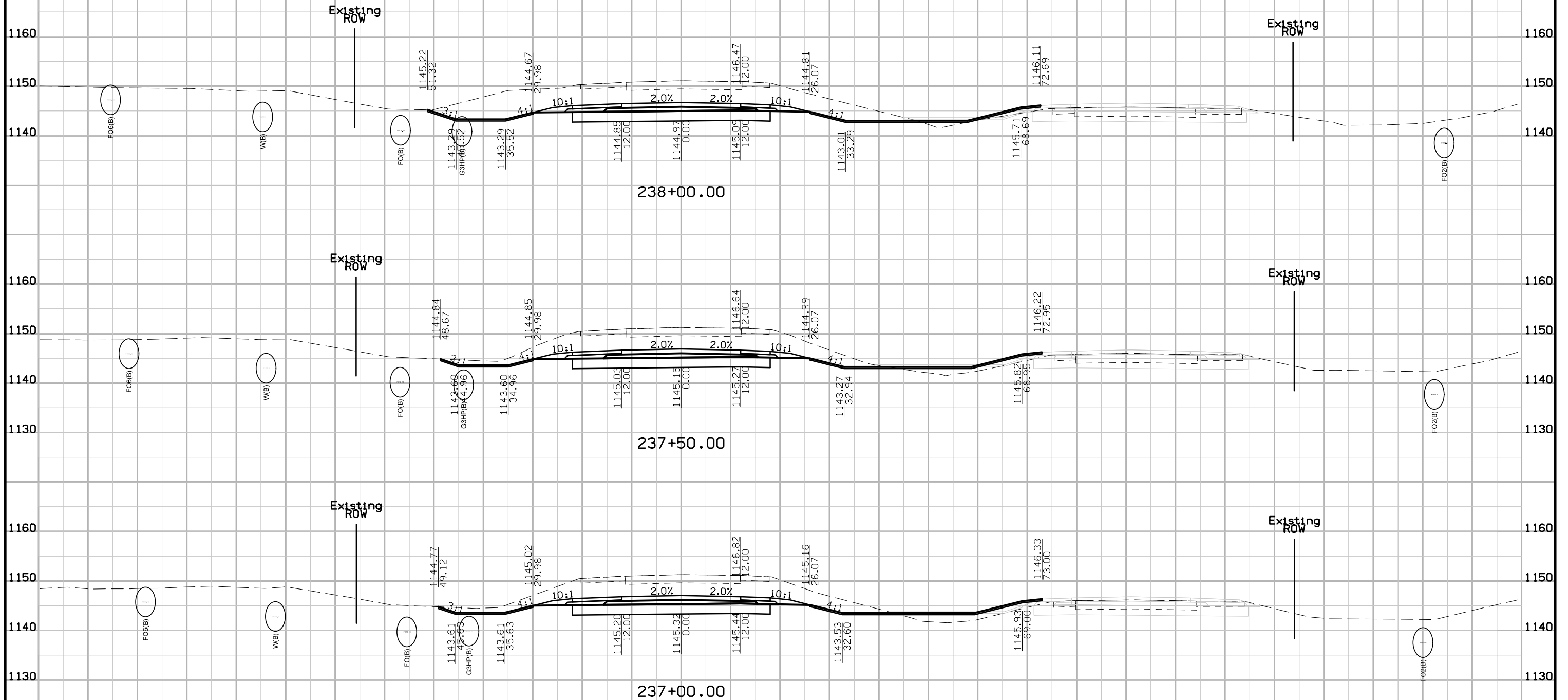
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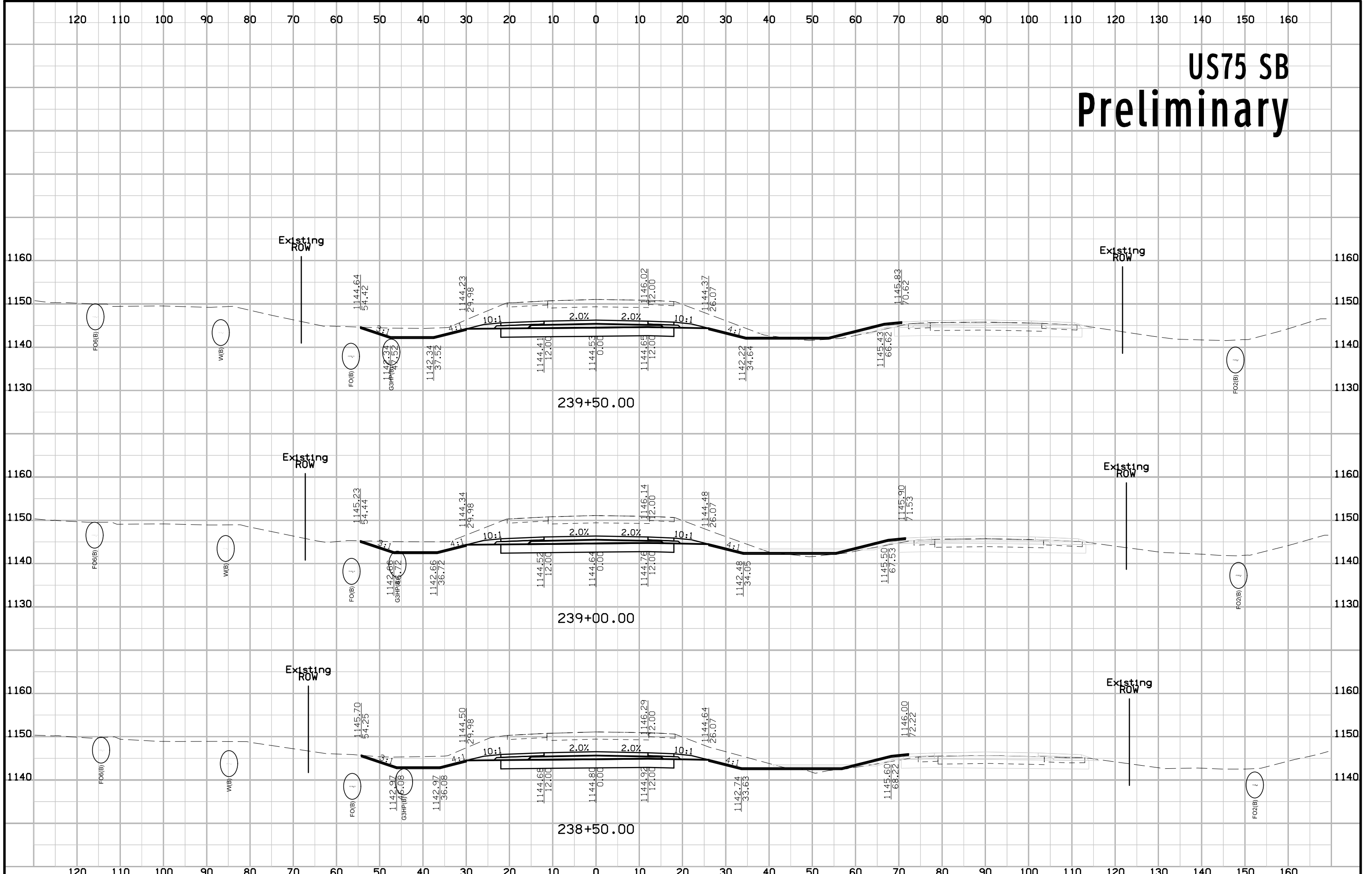
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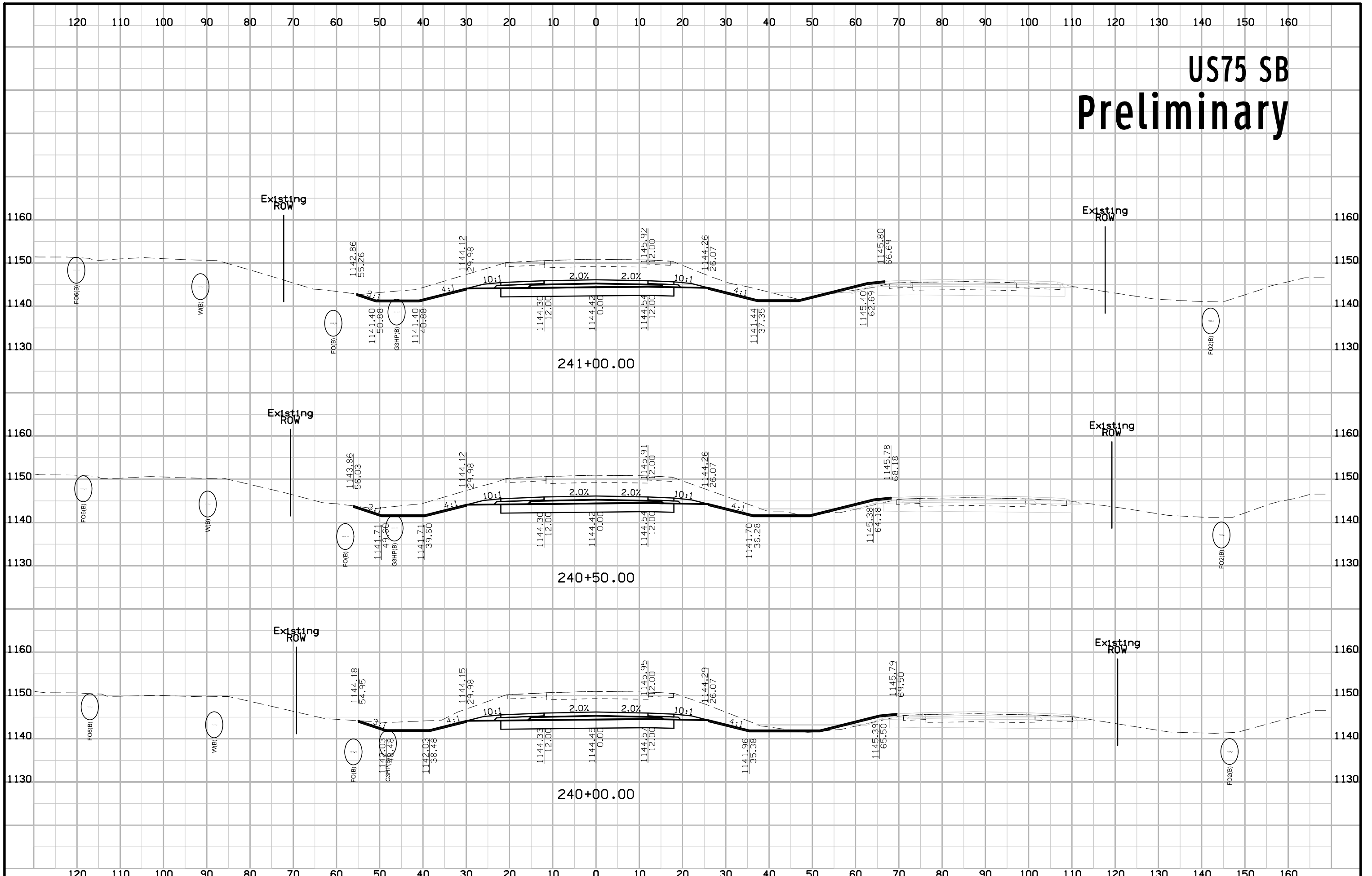
# US75 SB Preliminary



# US75 SB Preliminary

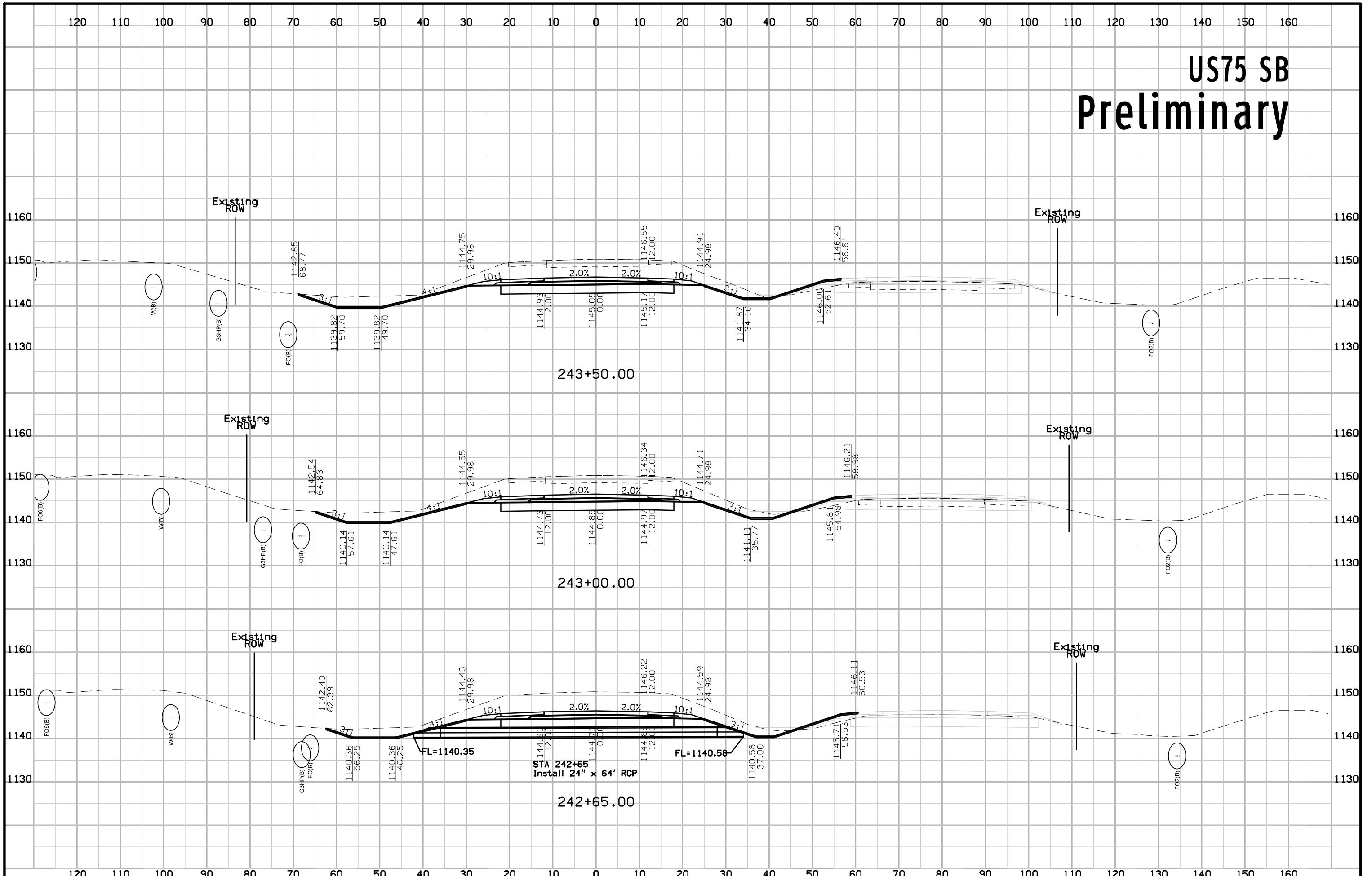


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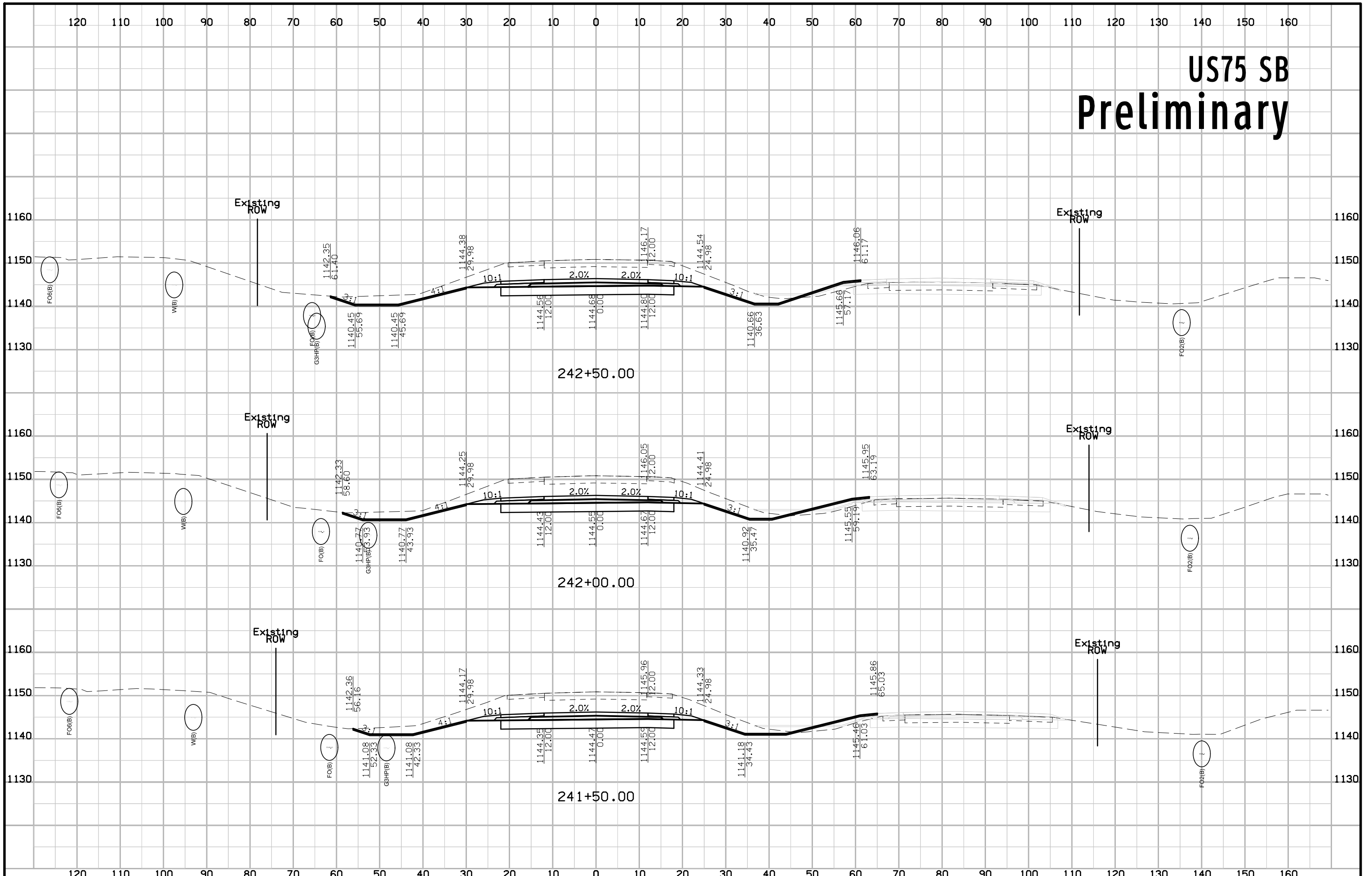




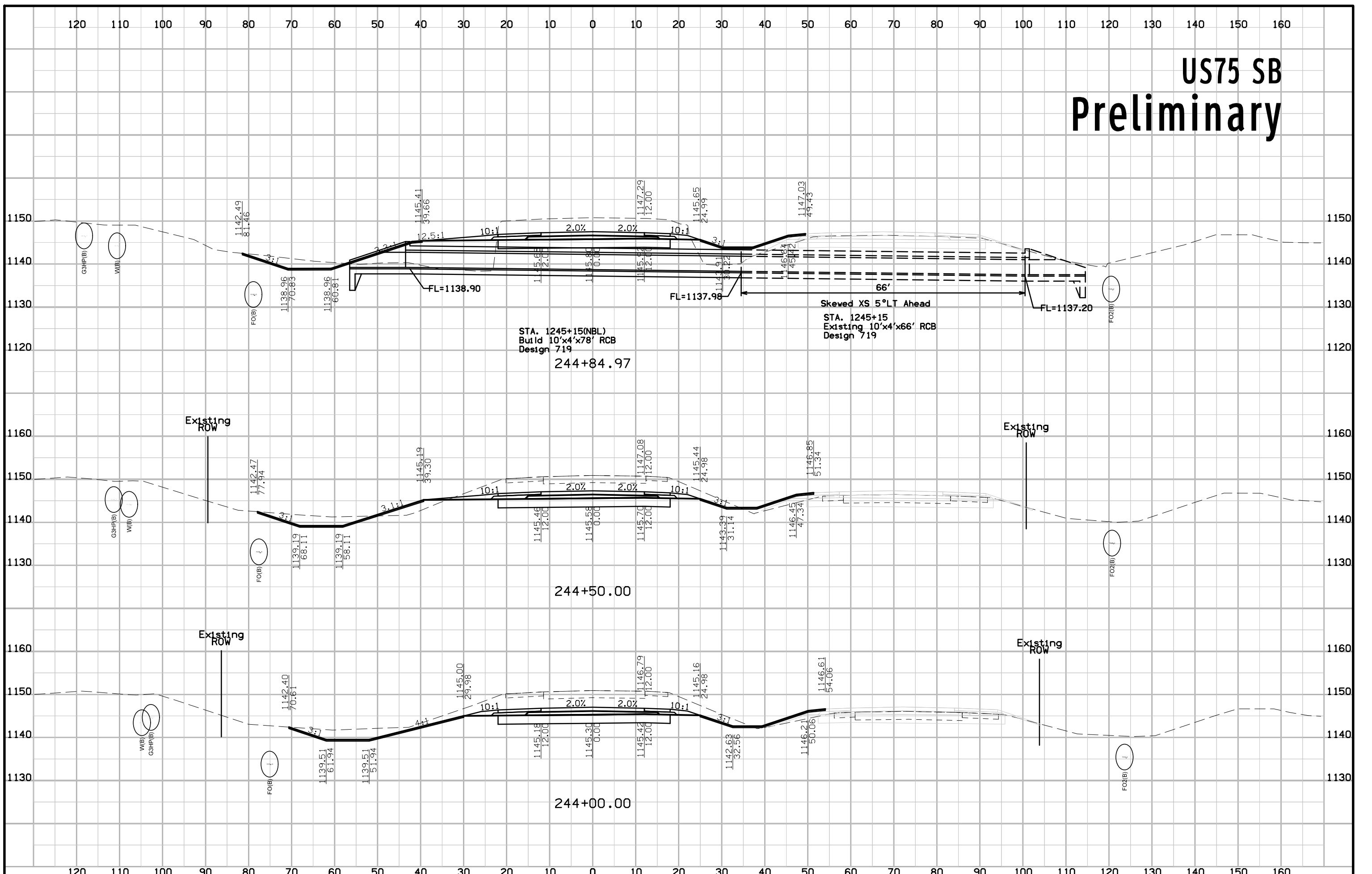
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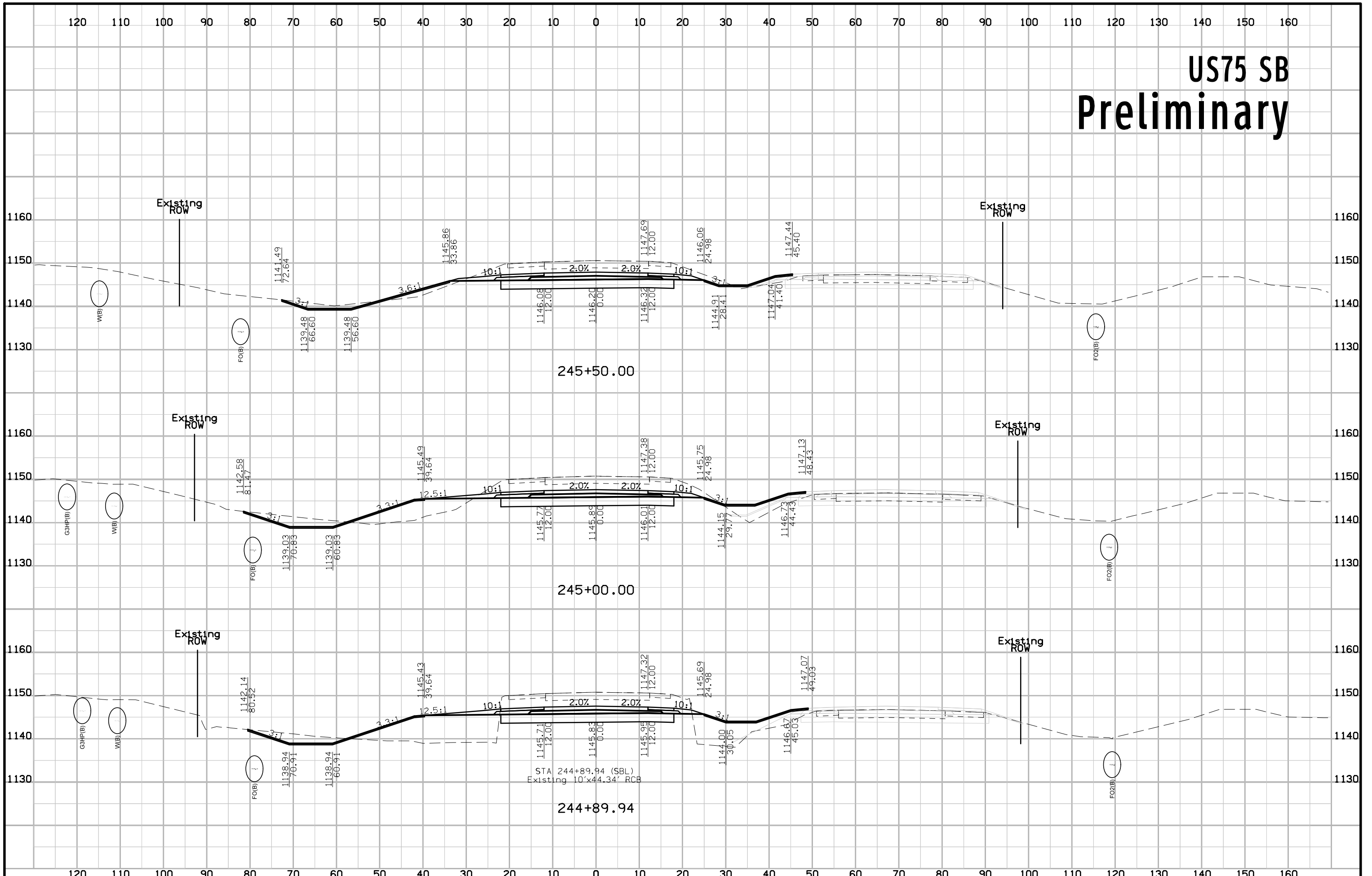
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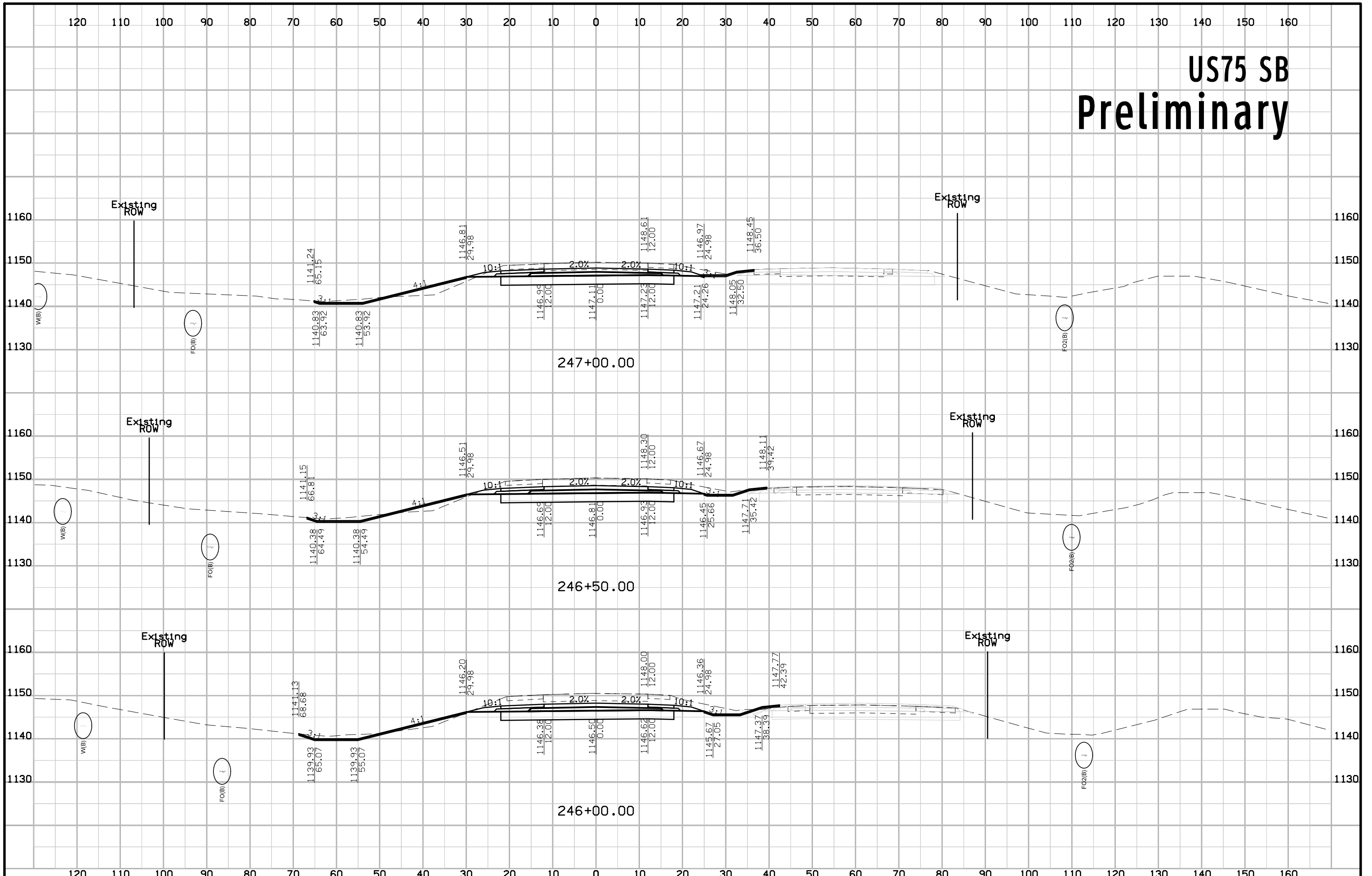
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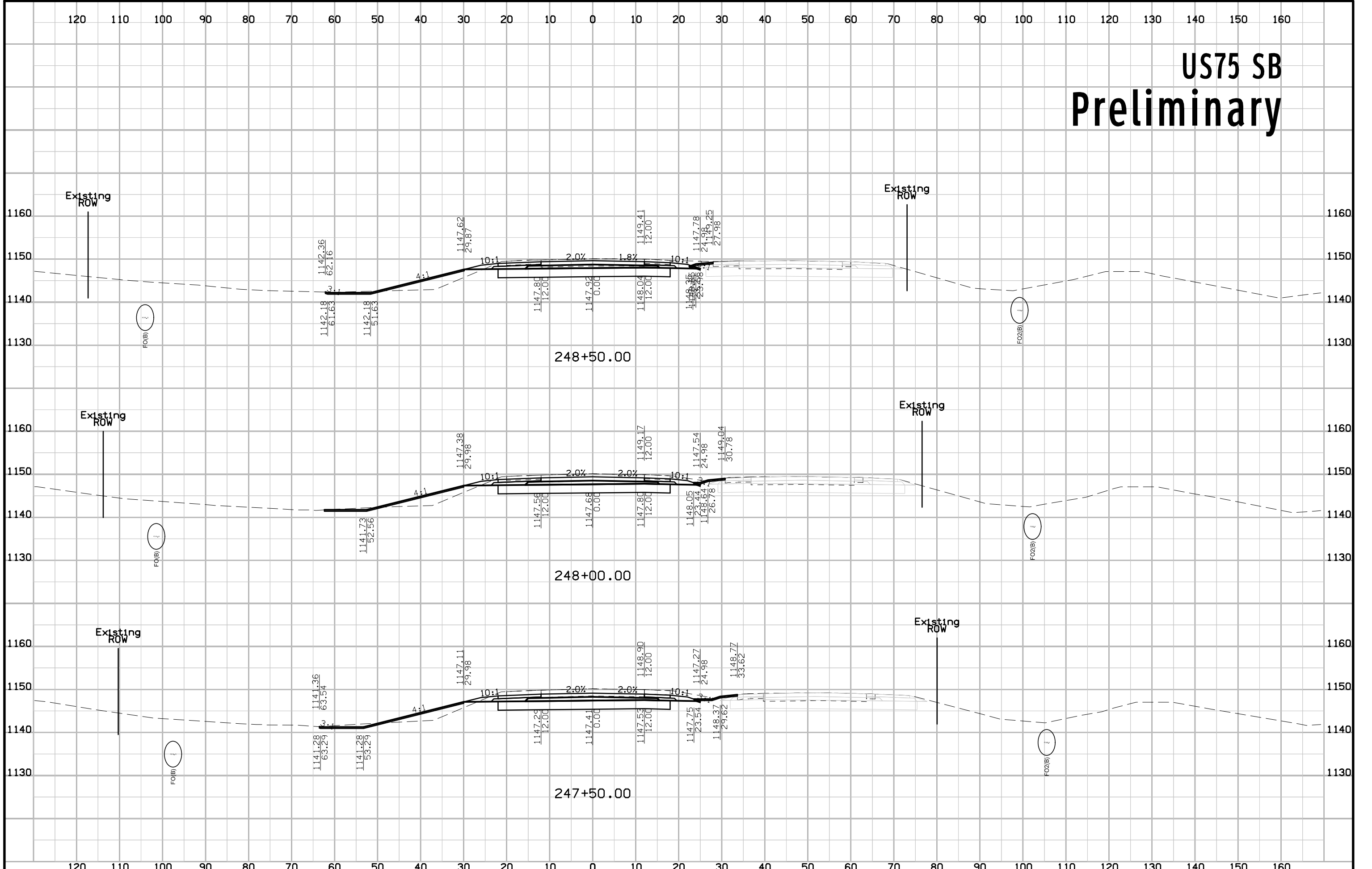
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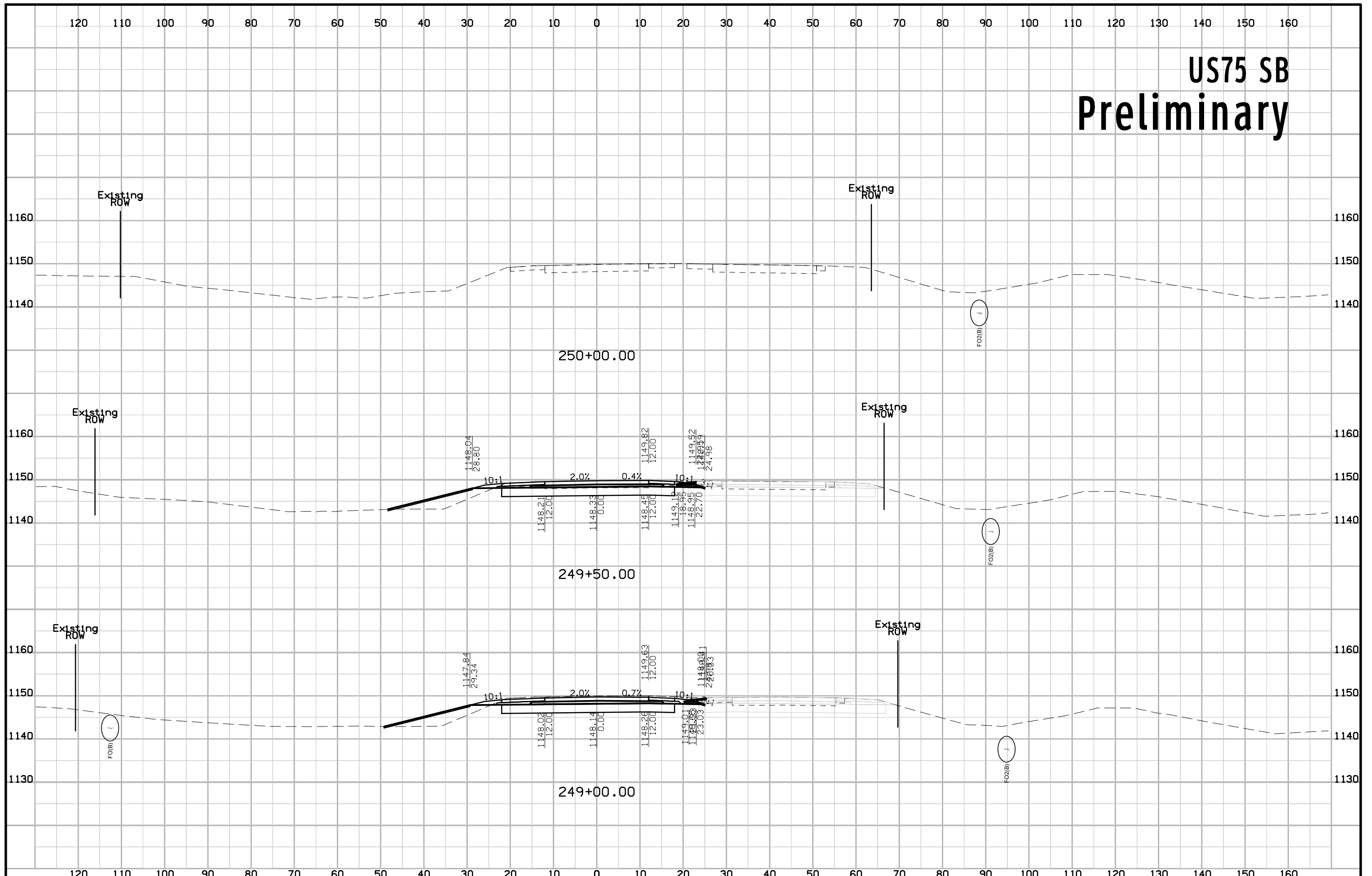
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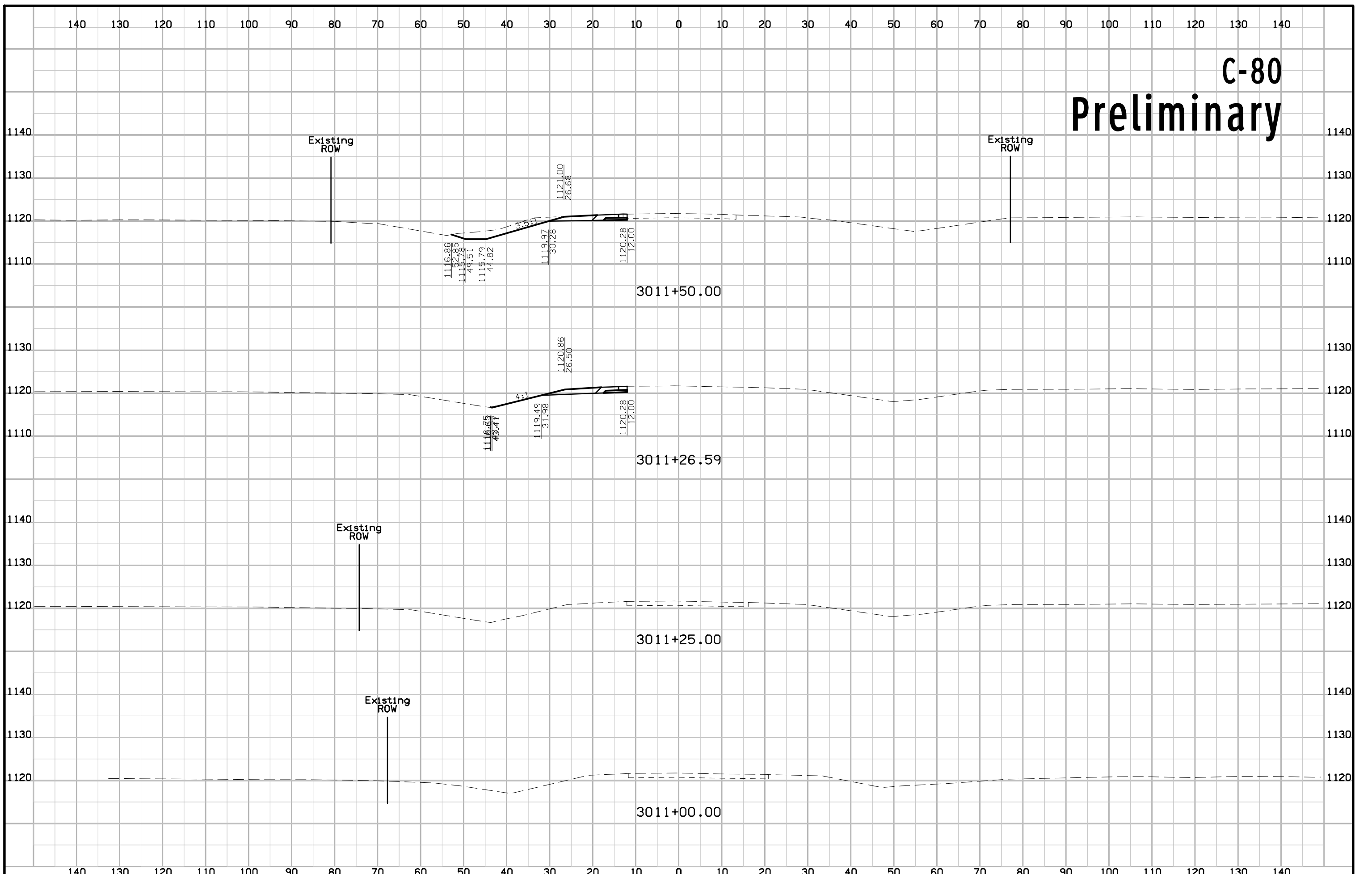
# US75 SB Preliminary



# US75 SB Preliminary

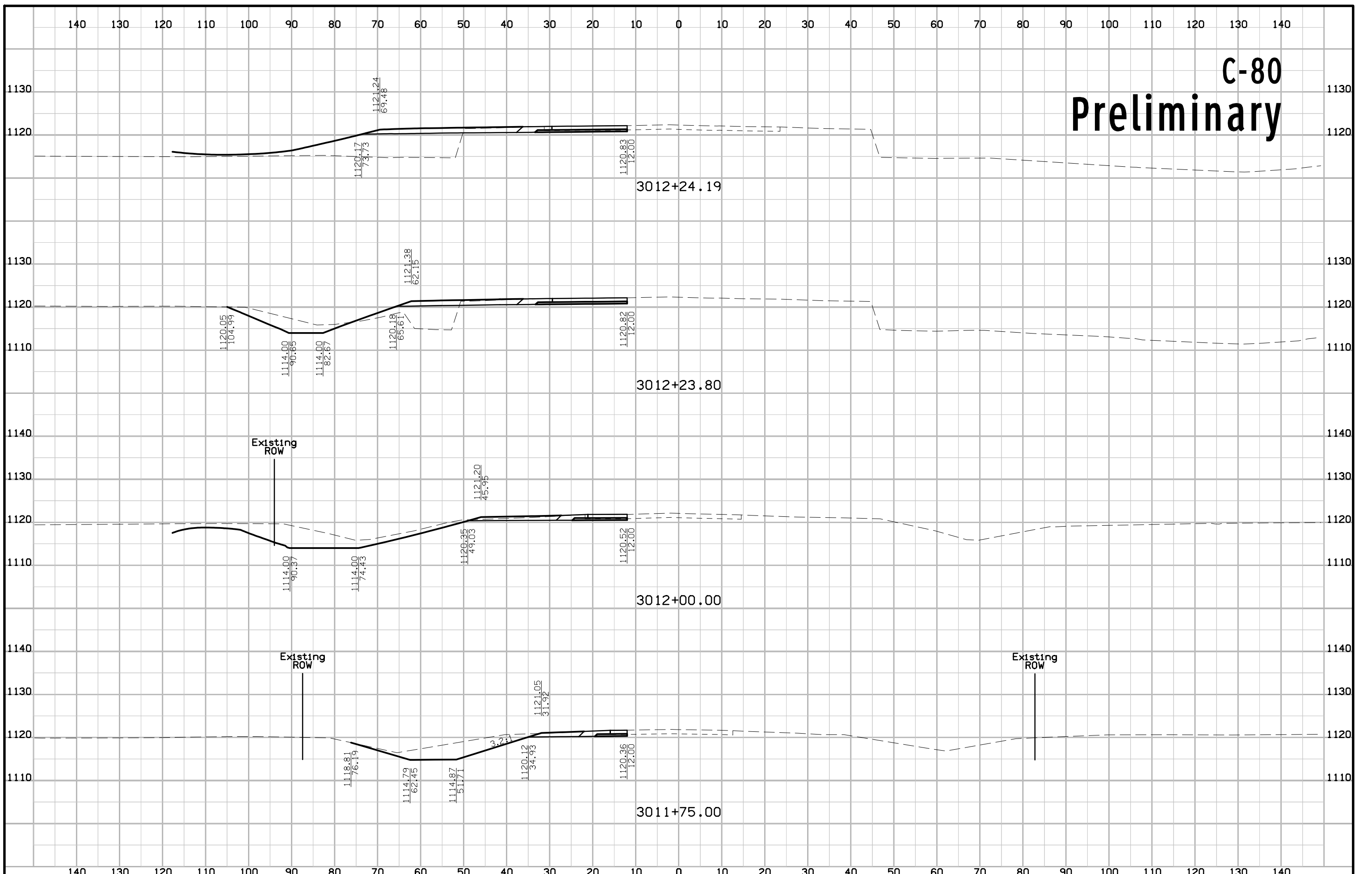


# C-80 Preliminary

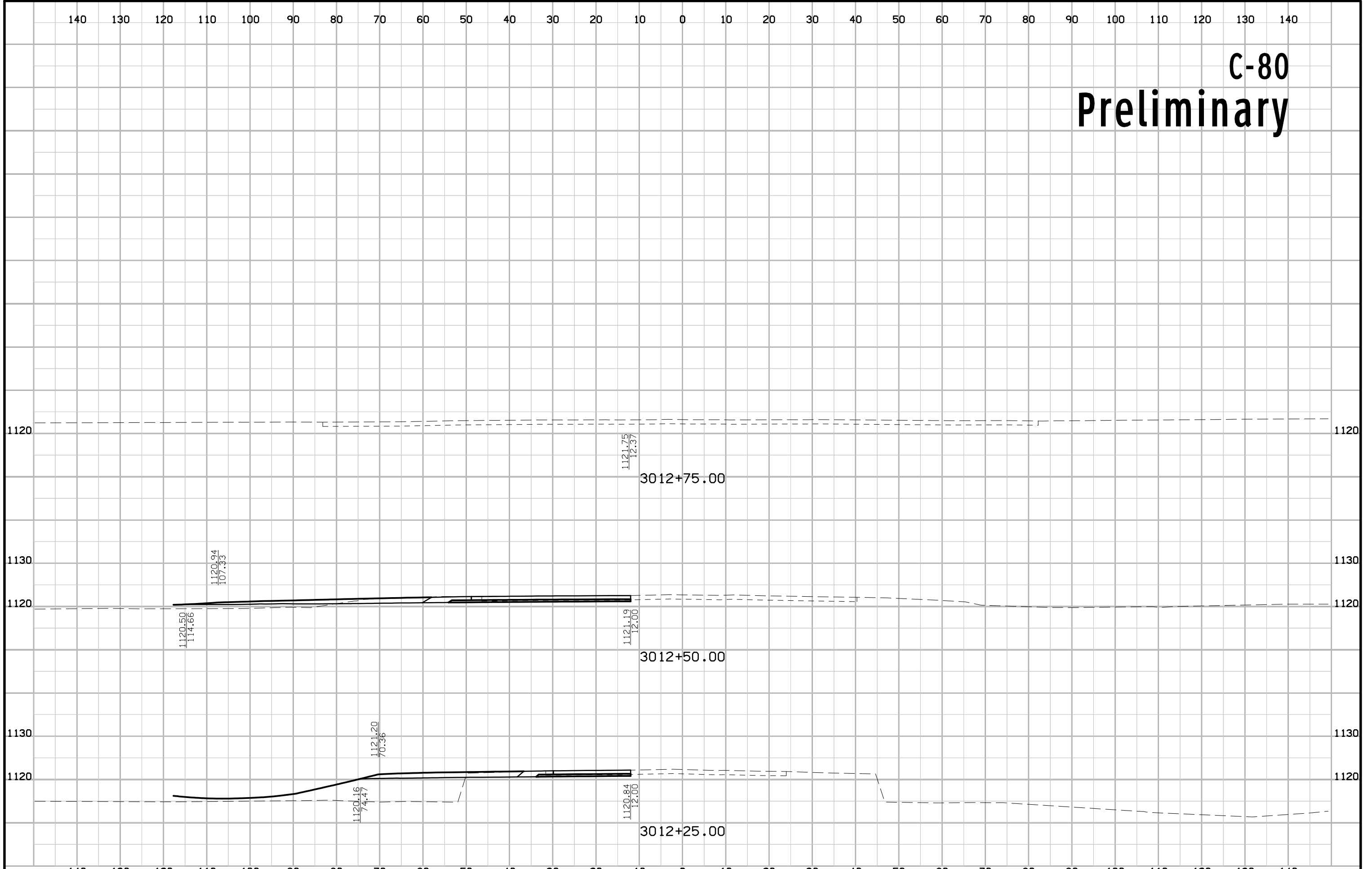




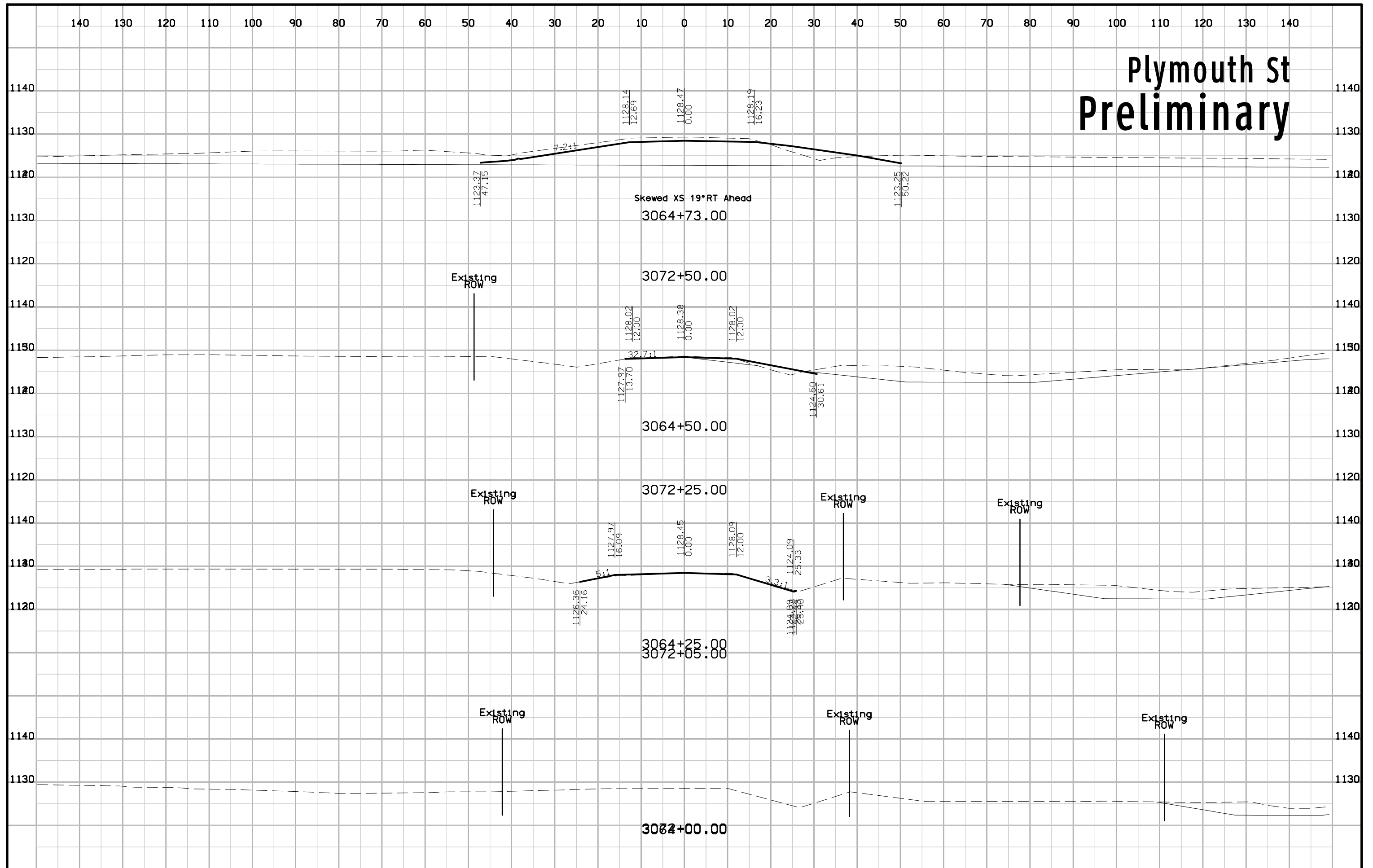
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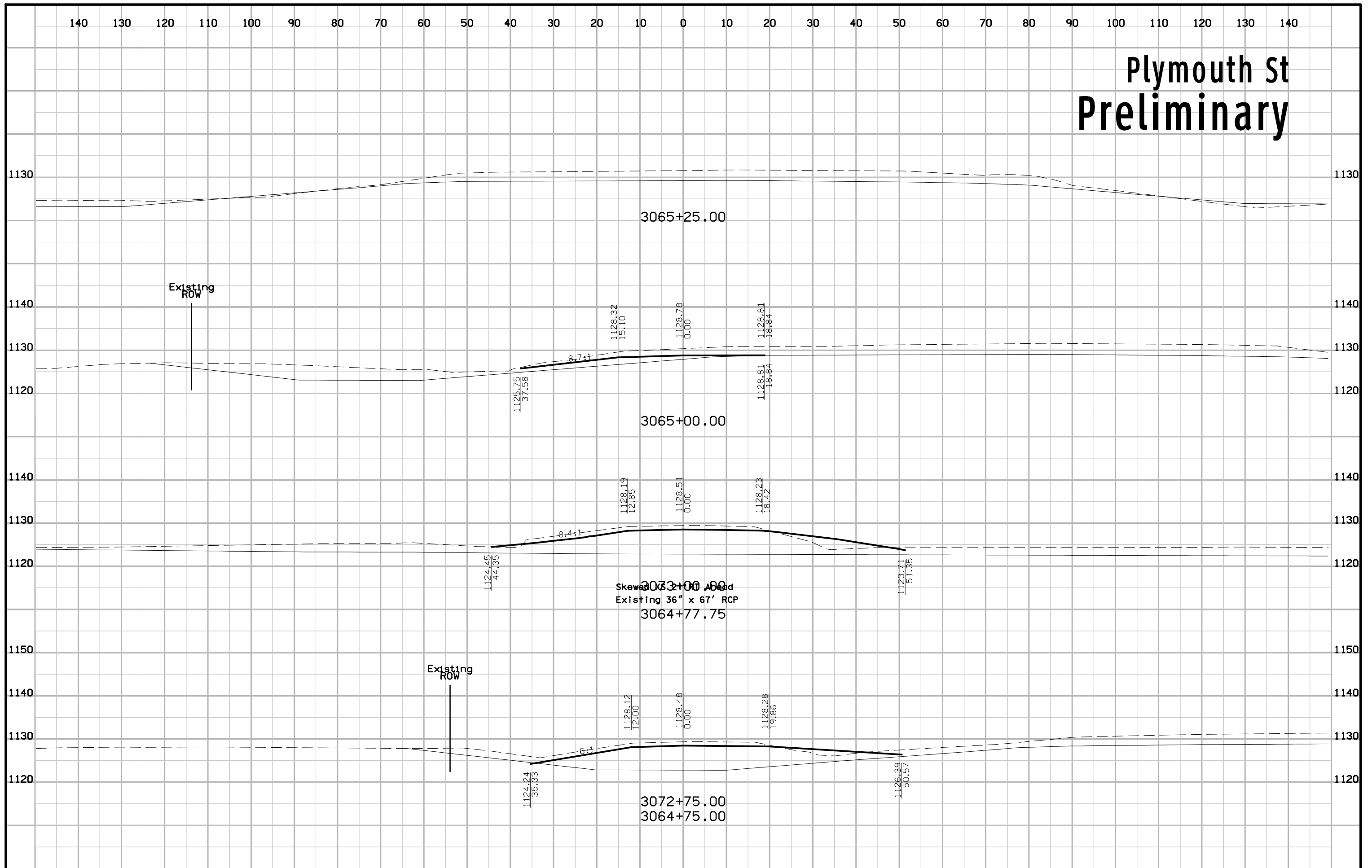
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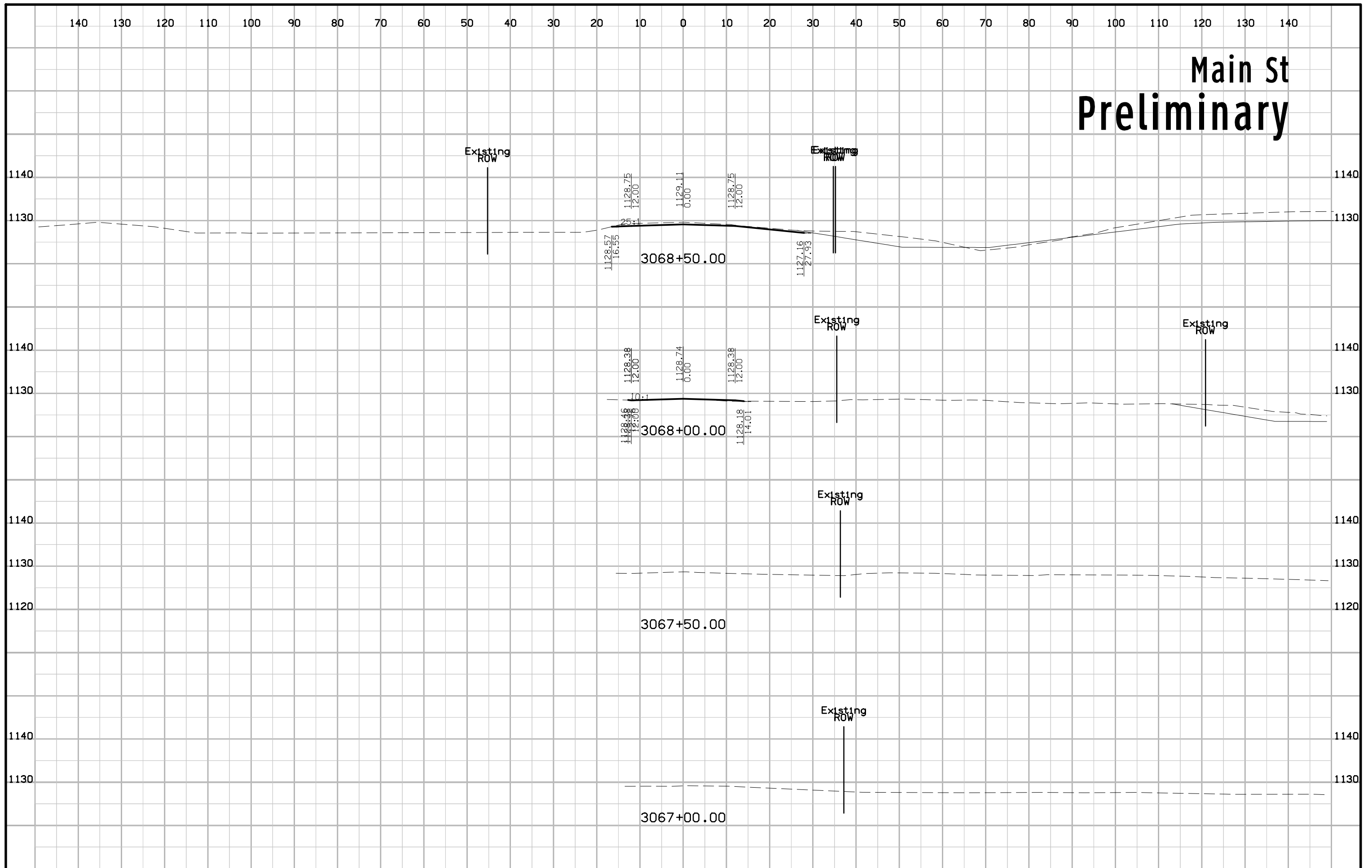
# Plymouth St Preliminary



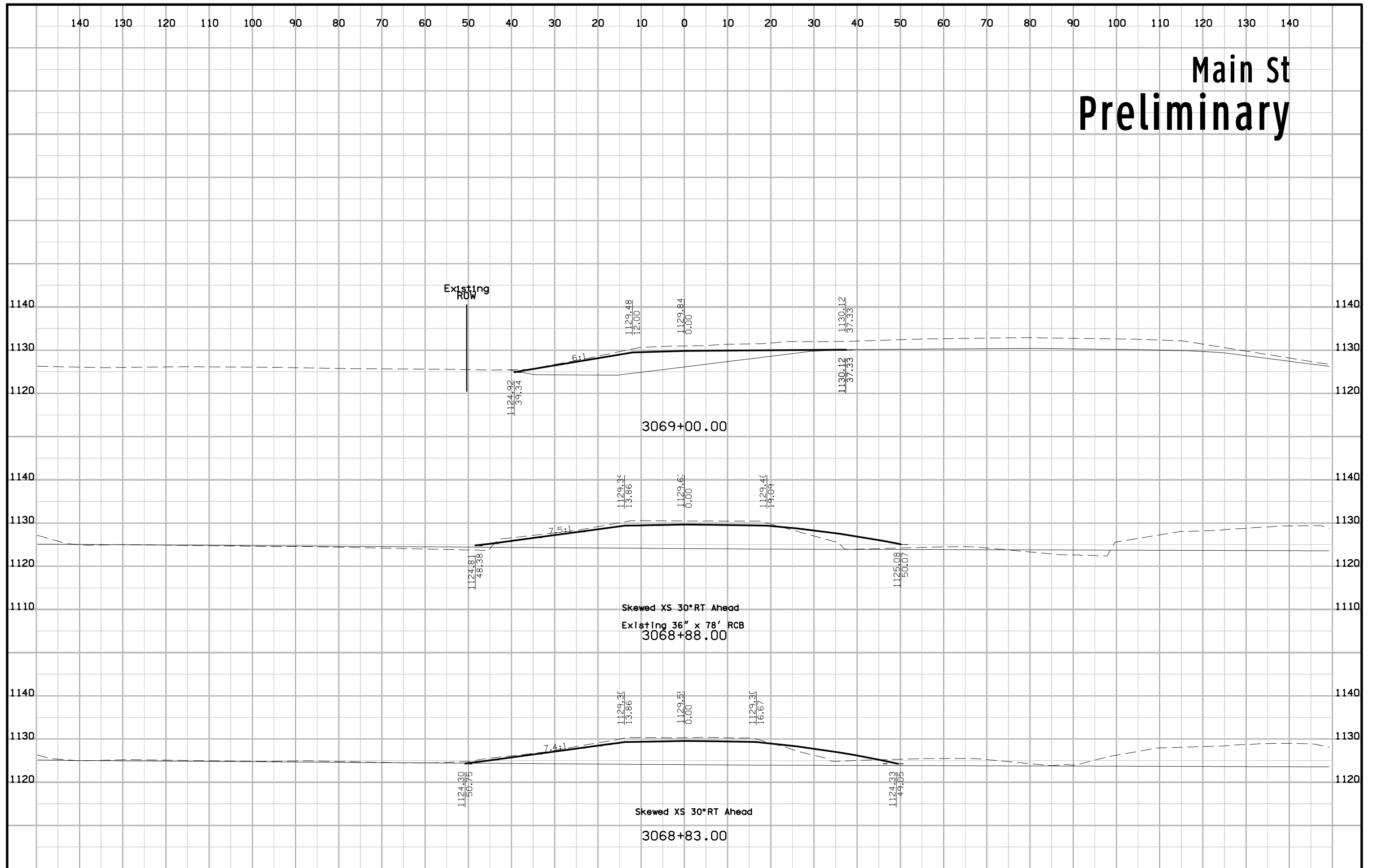
# Plymouth St Preliminary



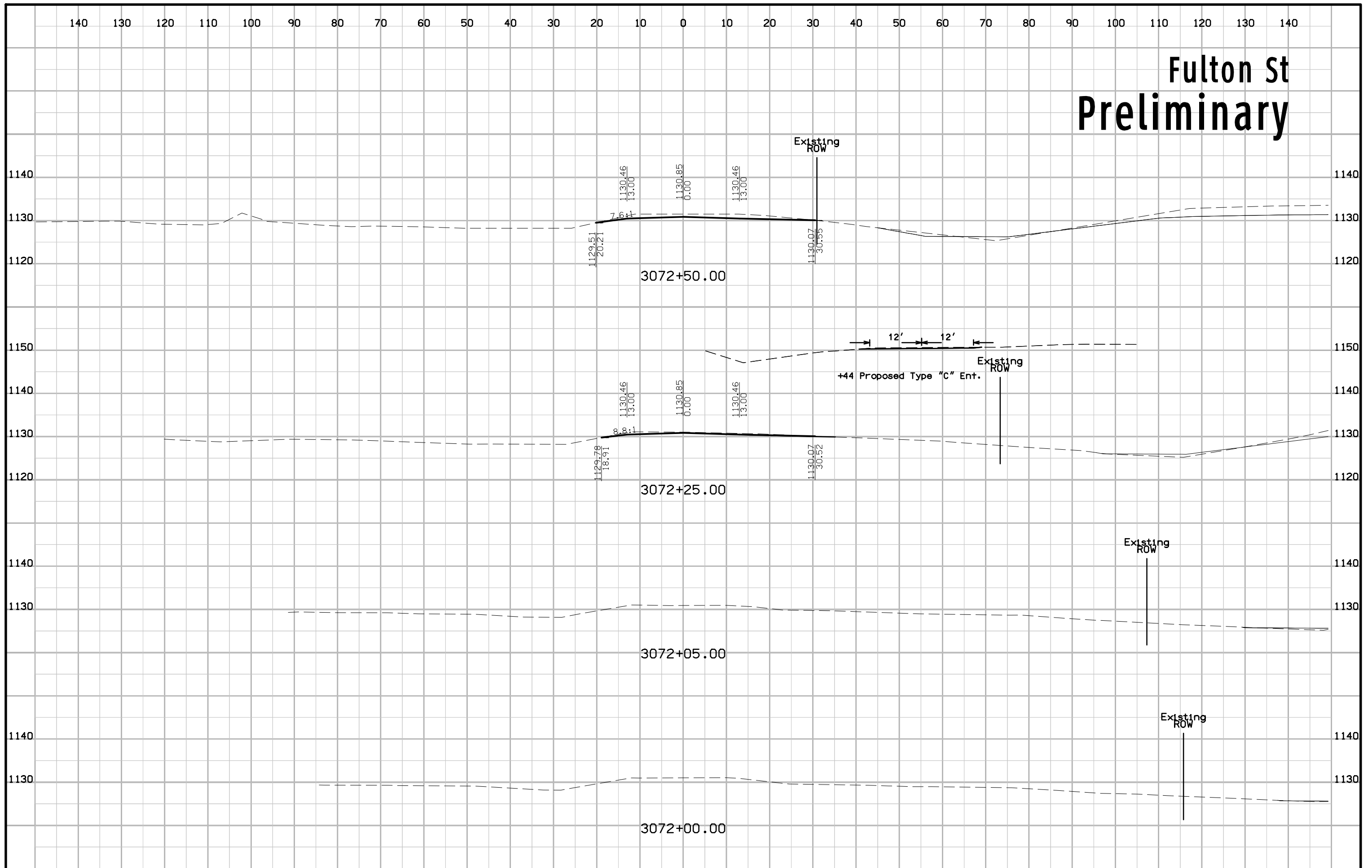
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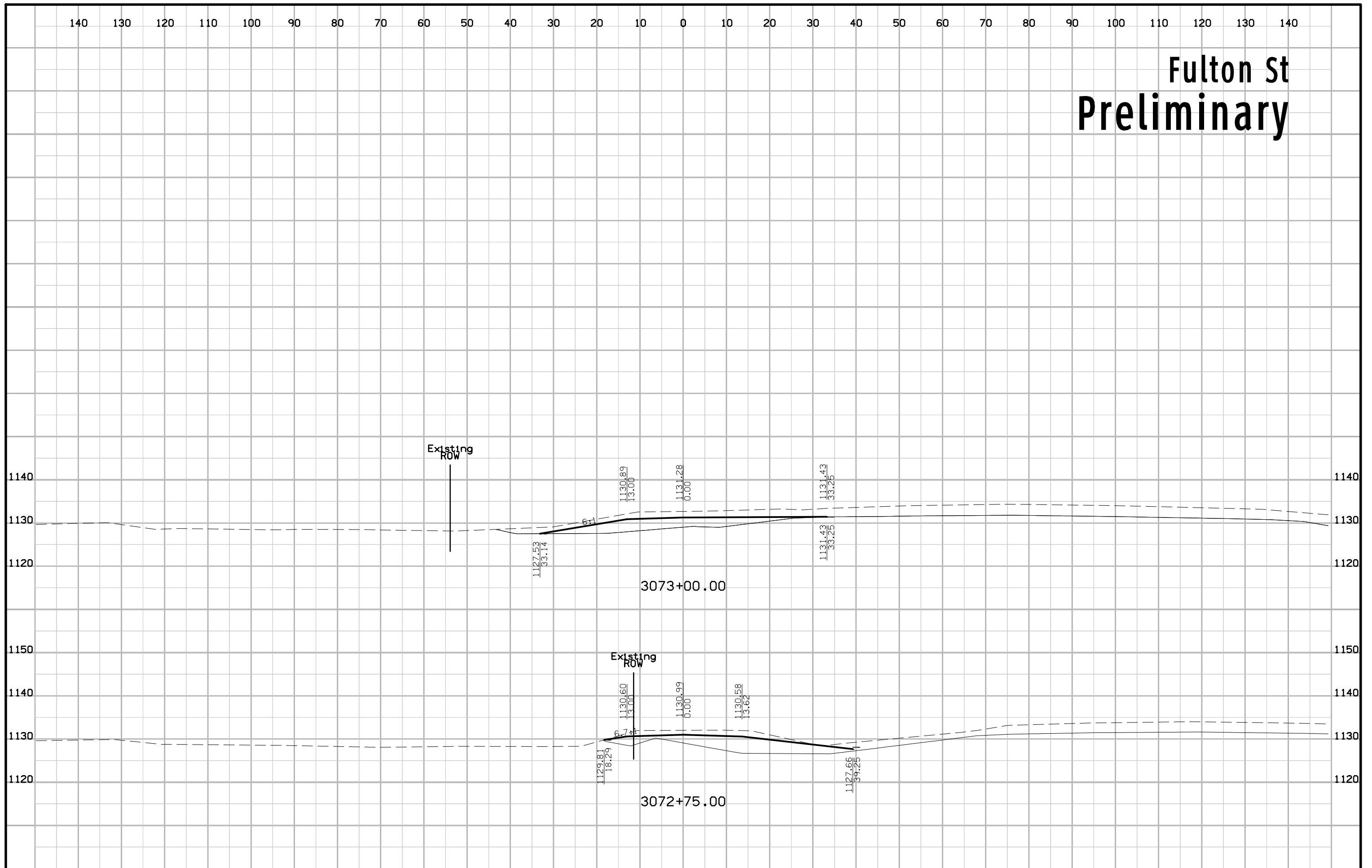
# Main St Preliminary



# Fulton St Preliminary

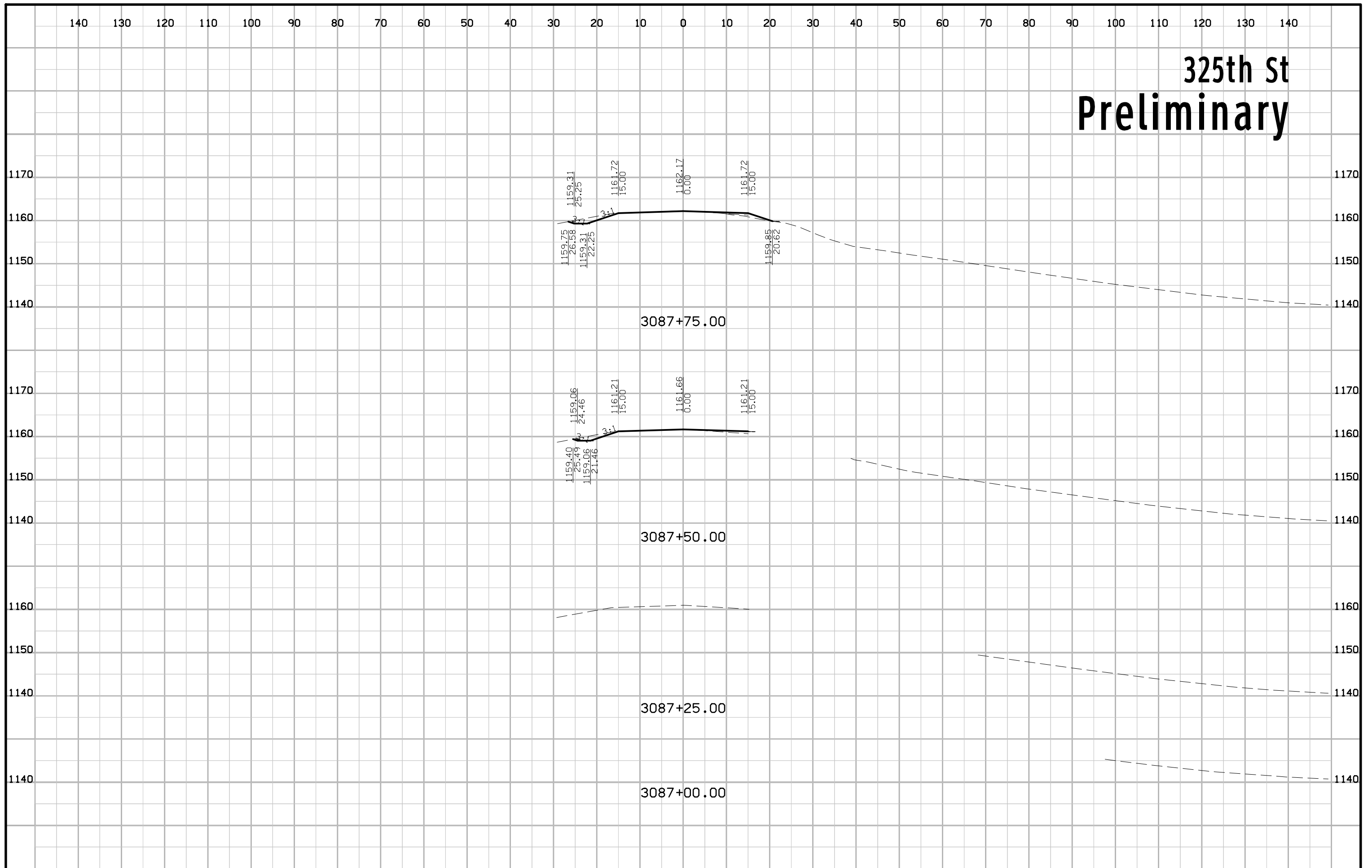


# Fulton St Preliminary

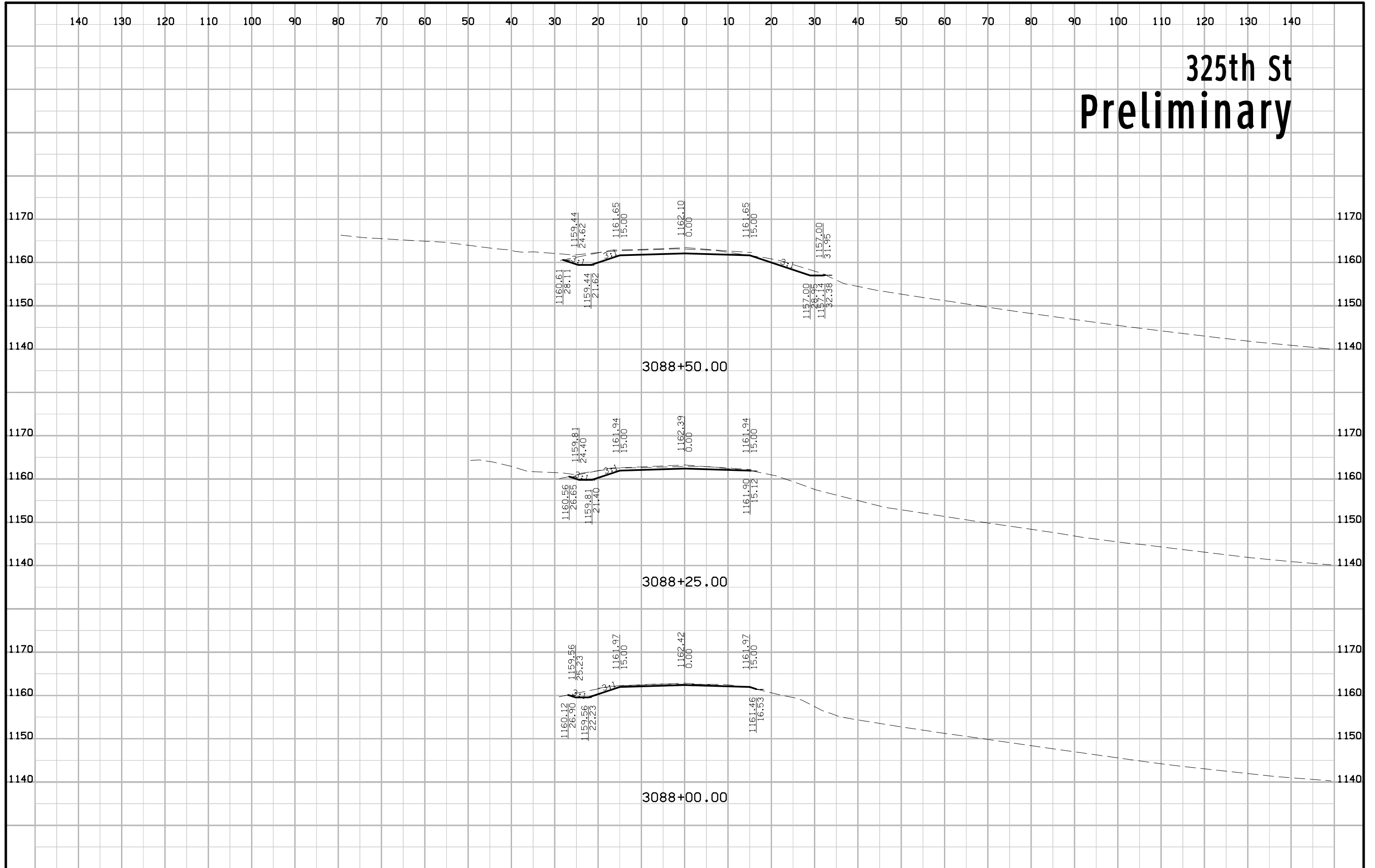




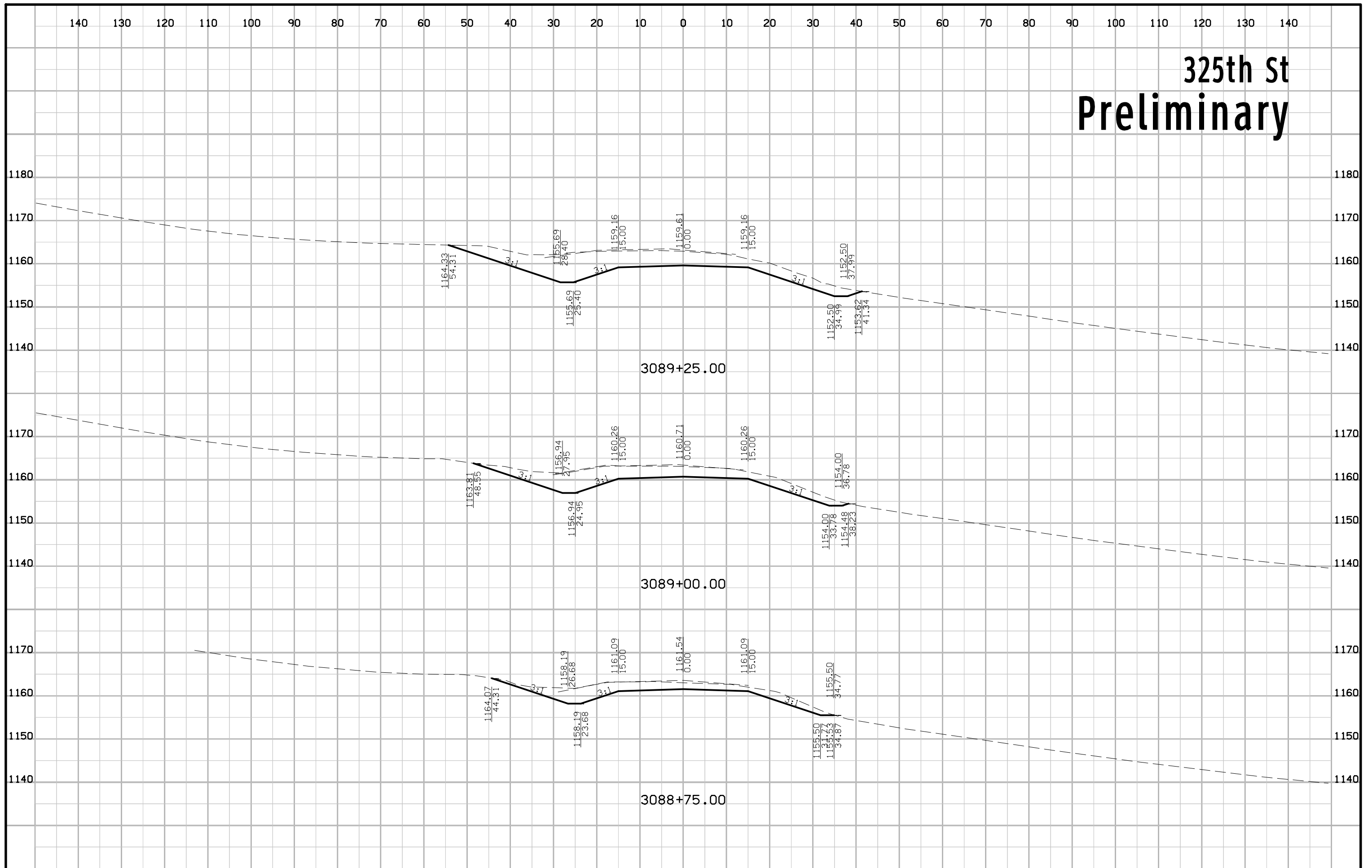
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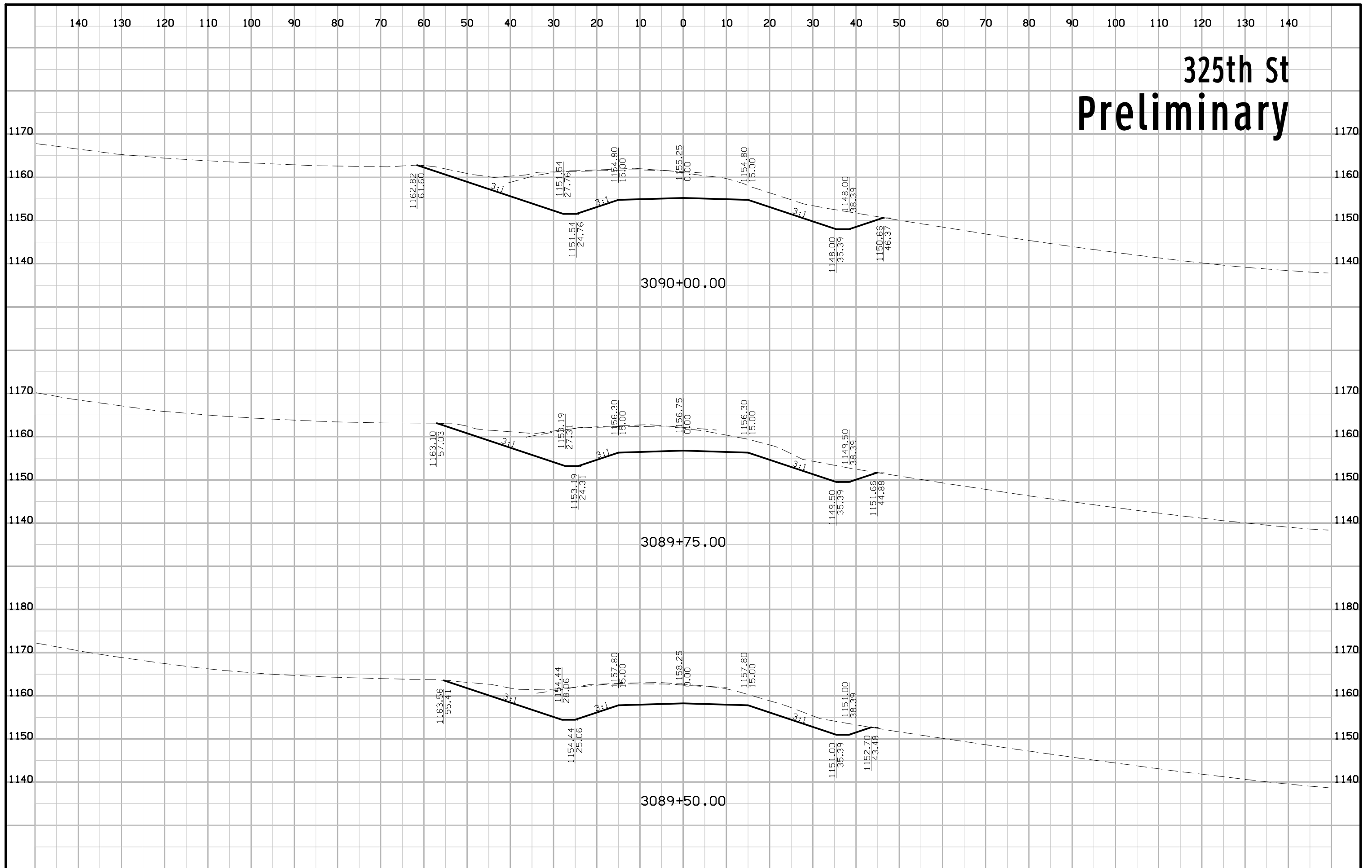
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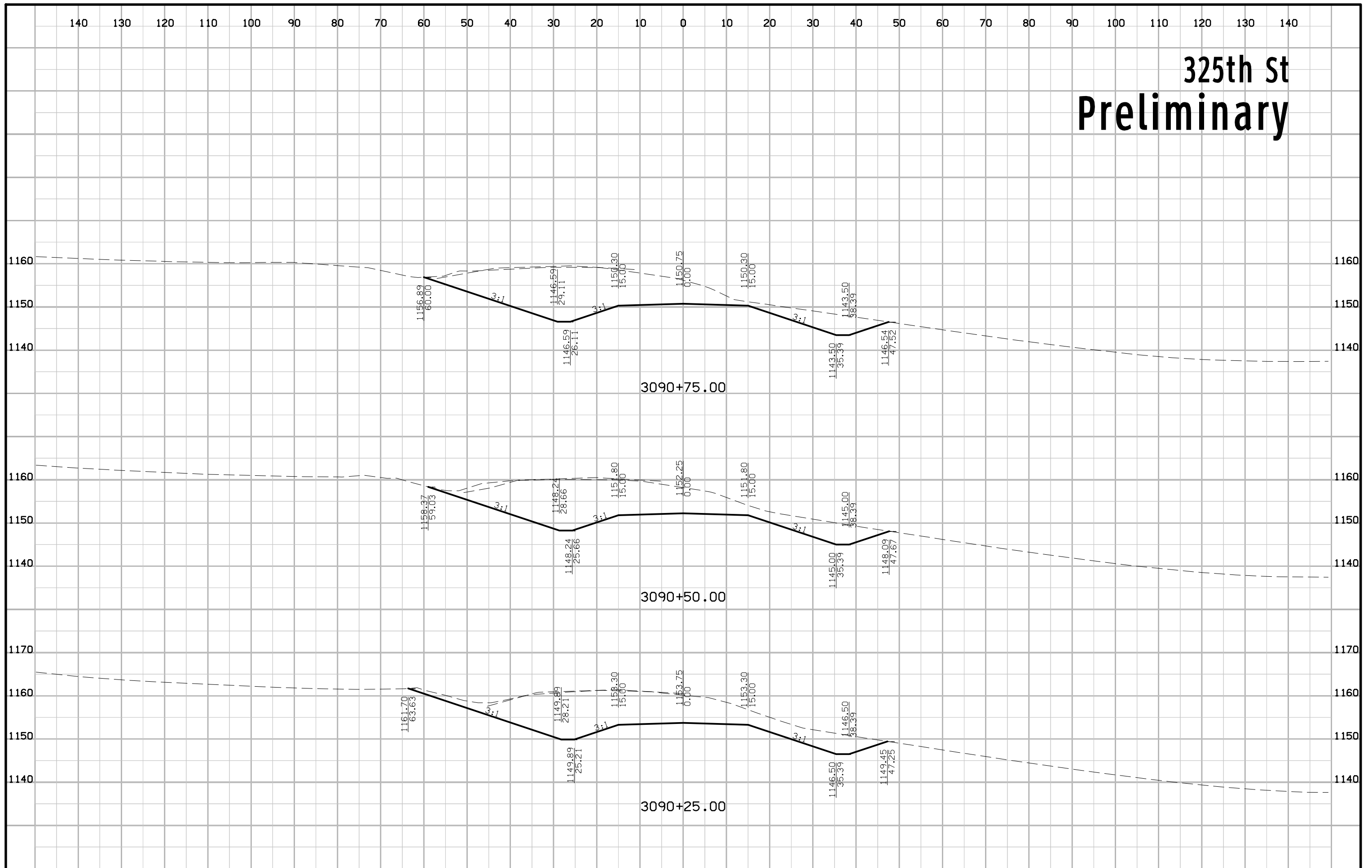
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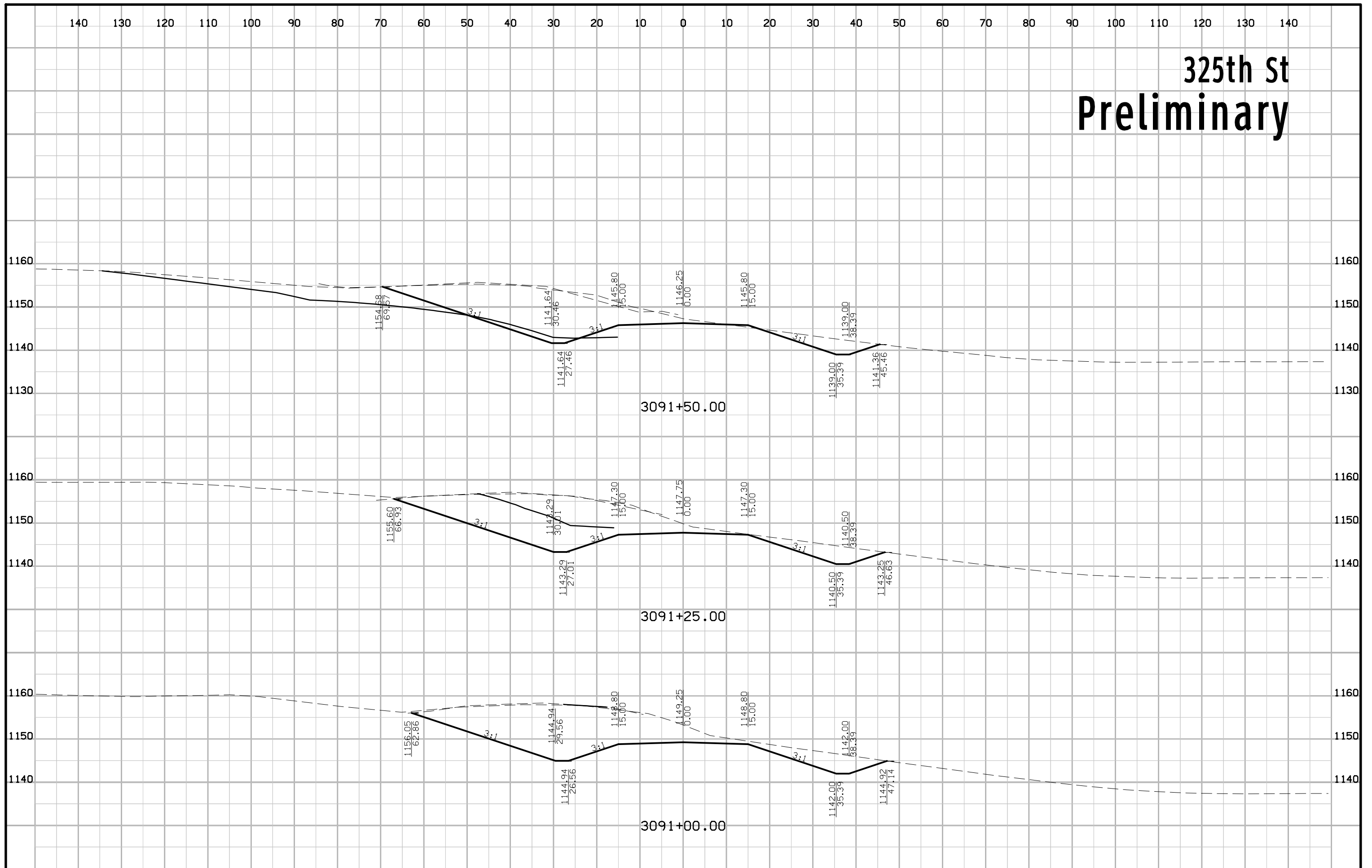
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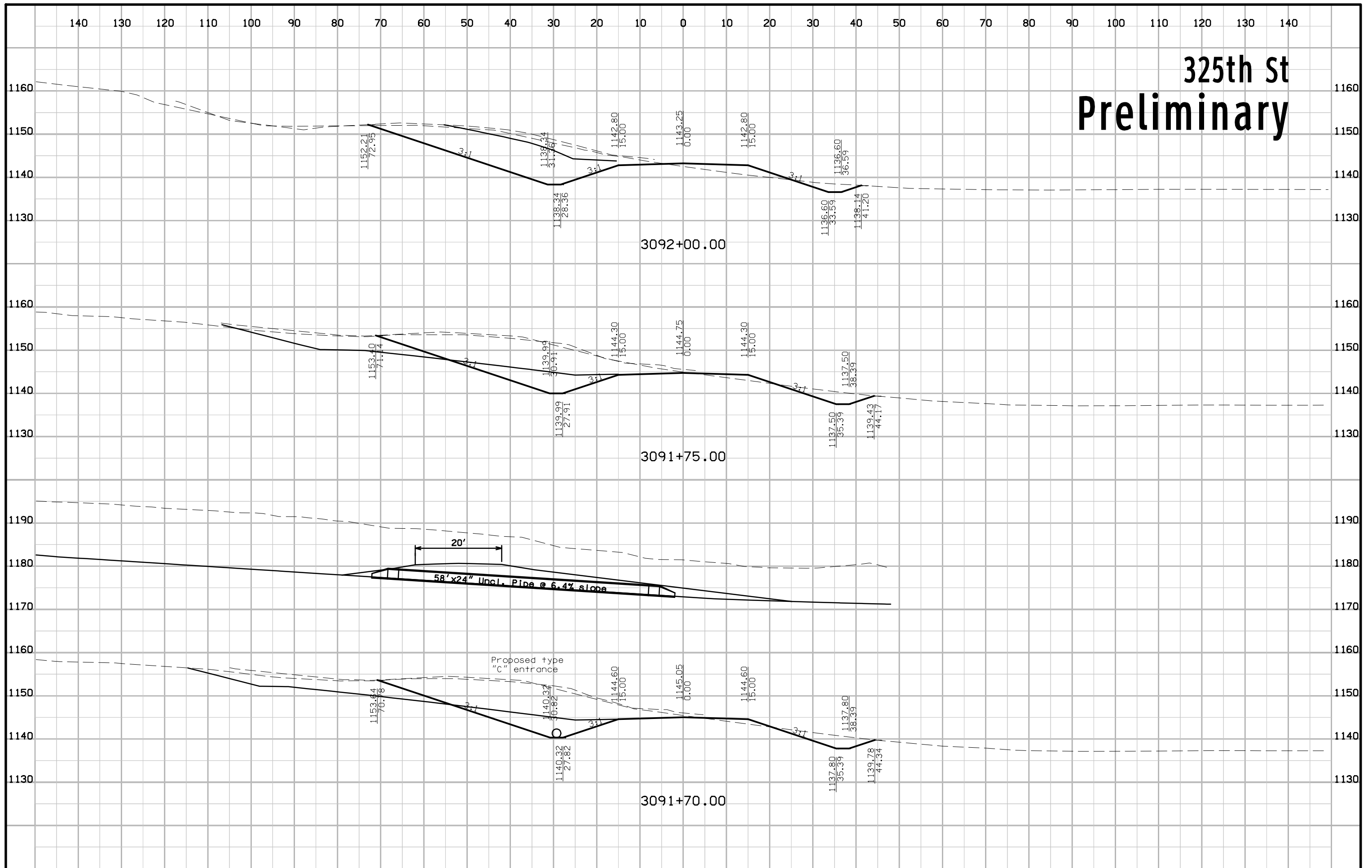
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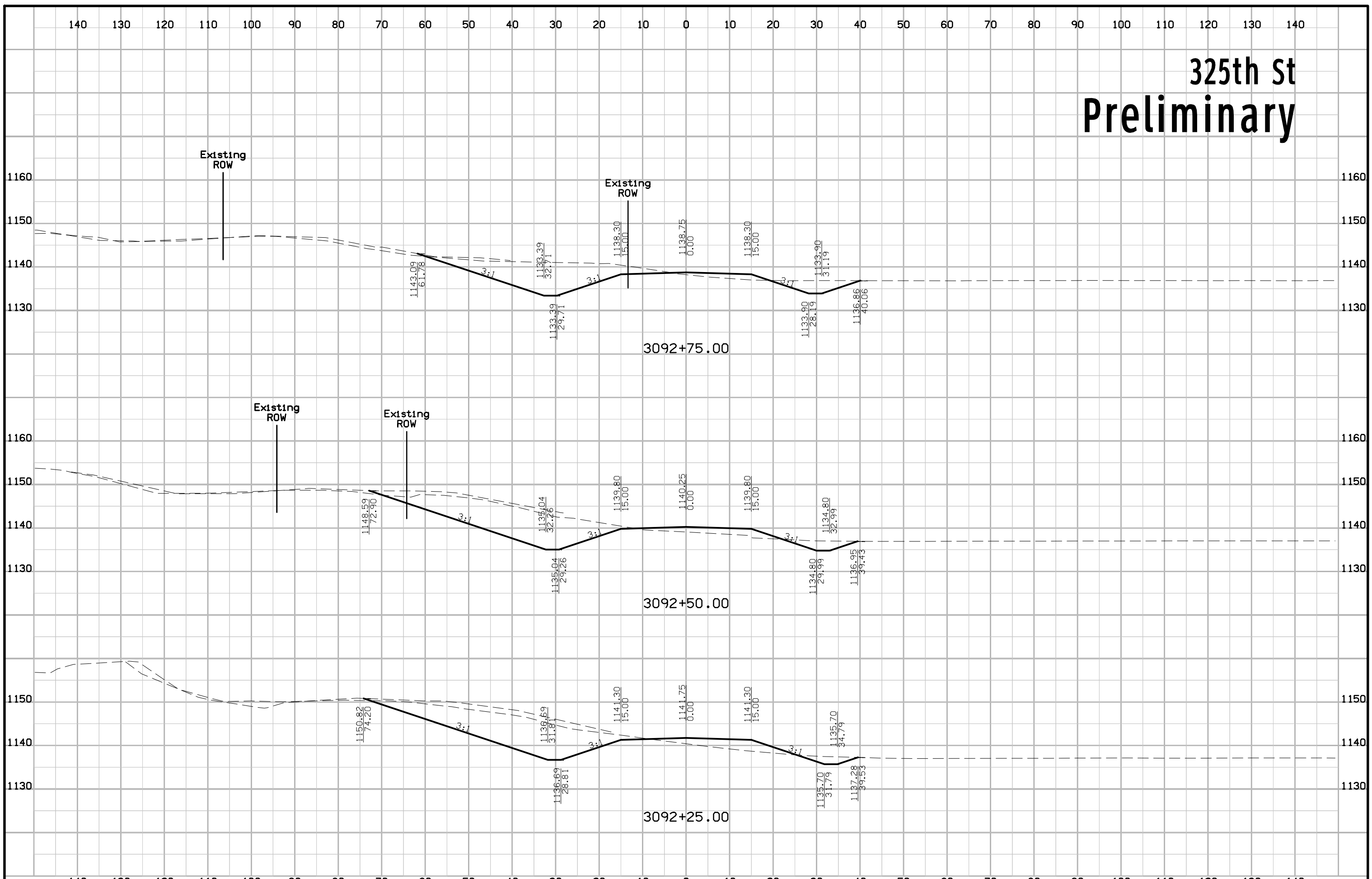
# 325th St Preliminary



# 325th St Preliminary

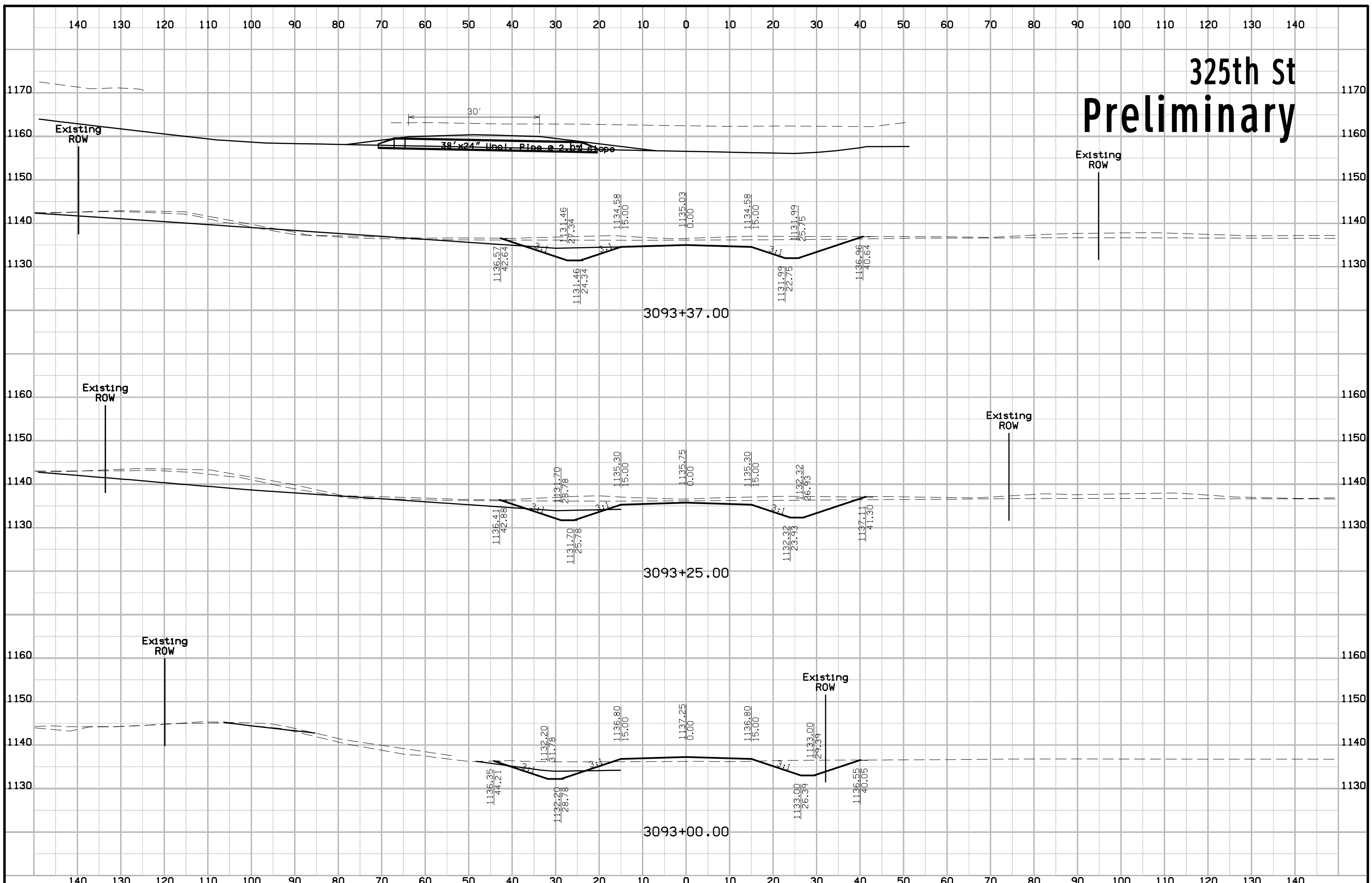


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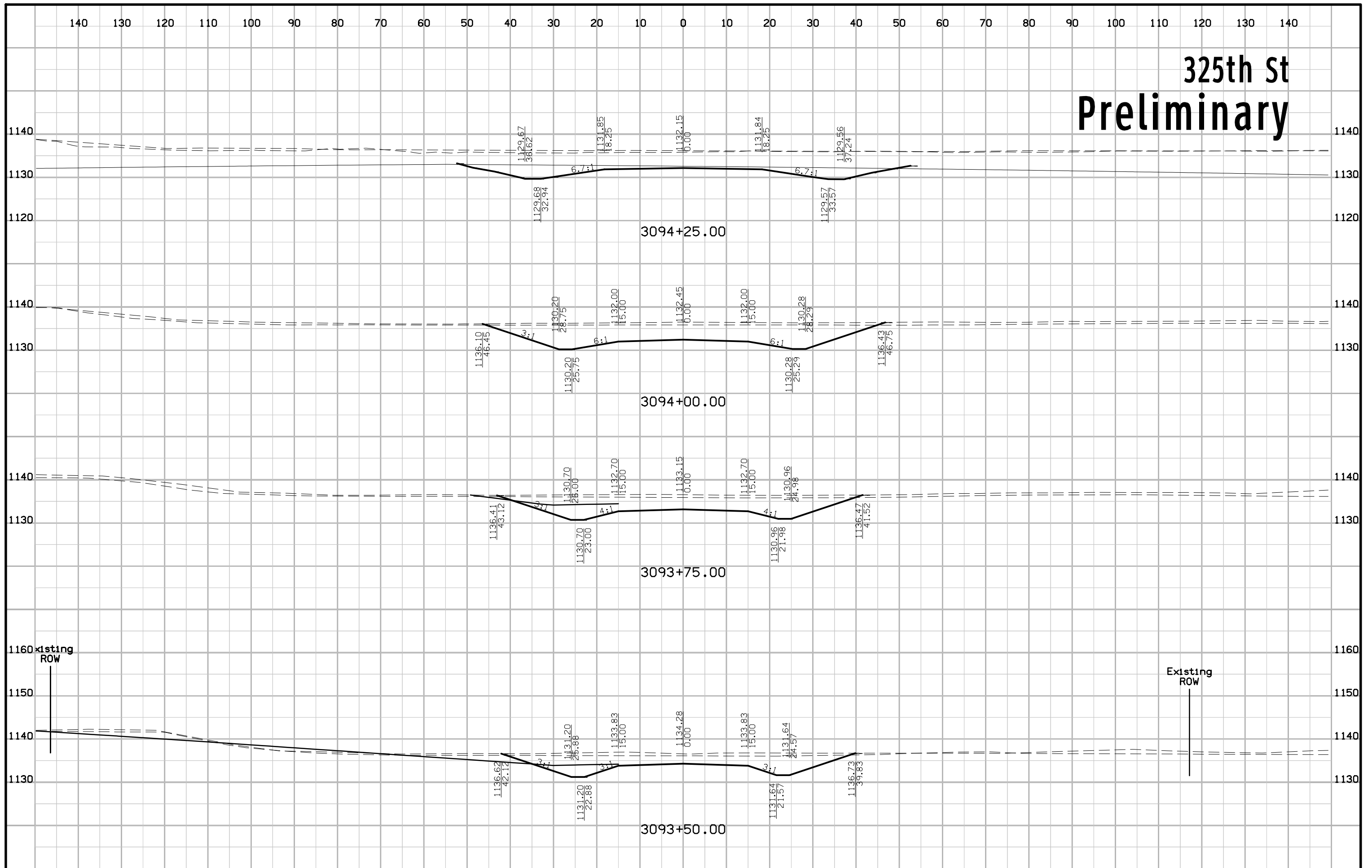




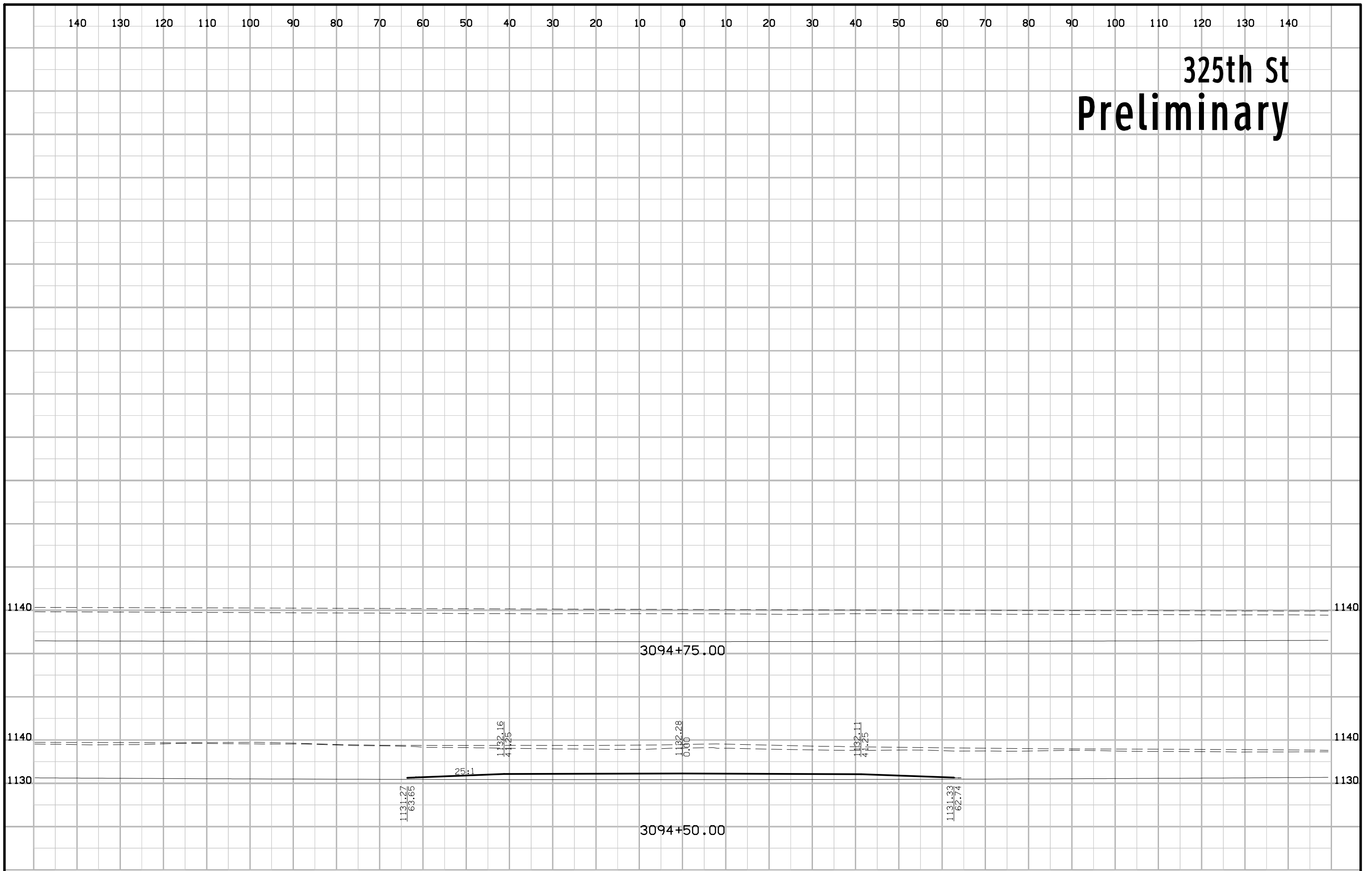
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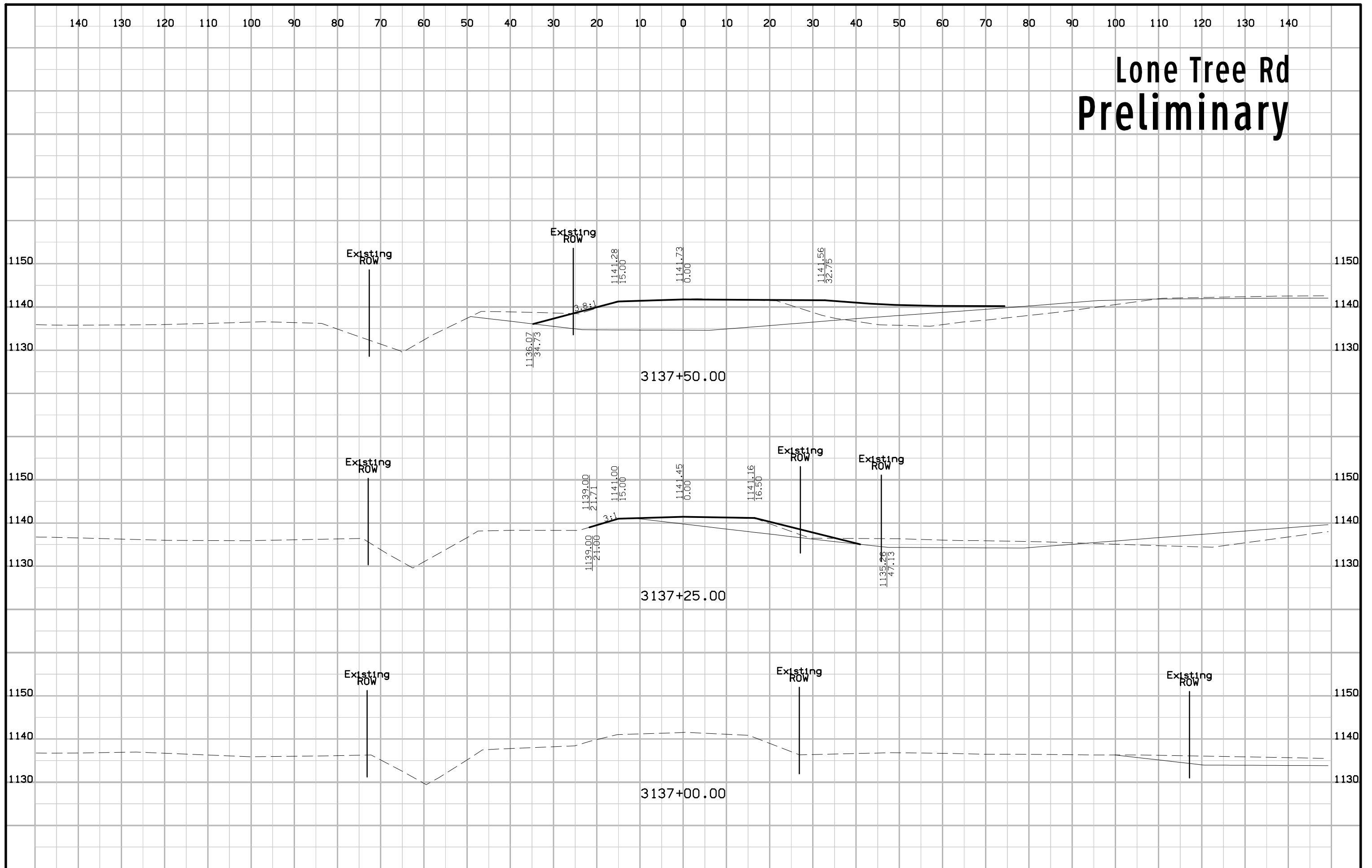
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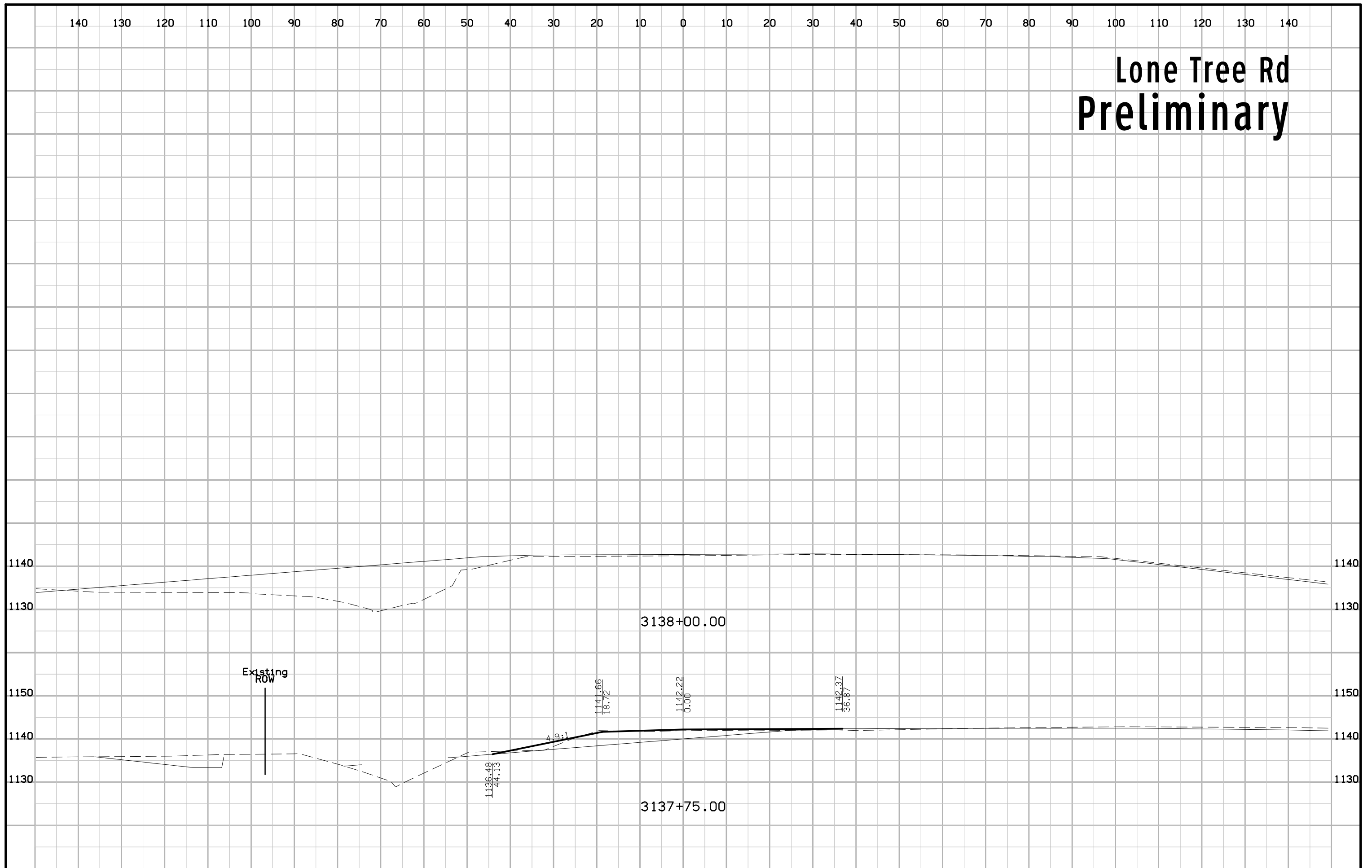
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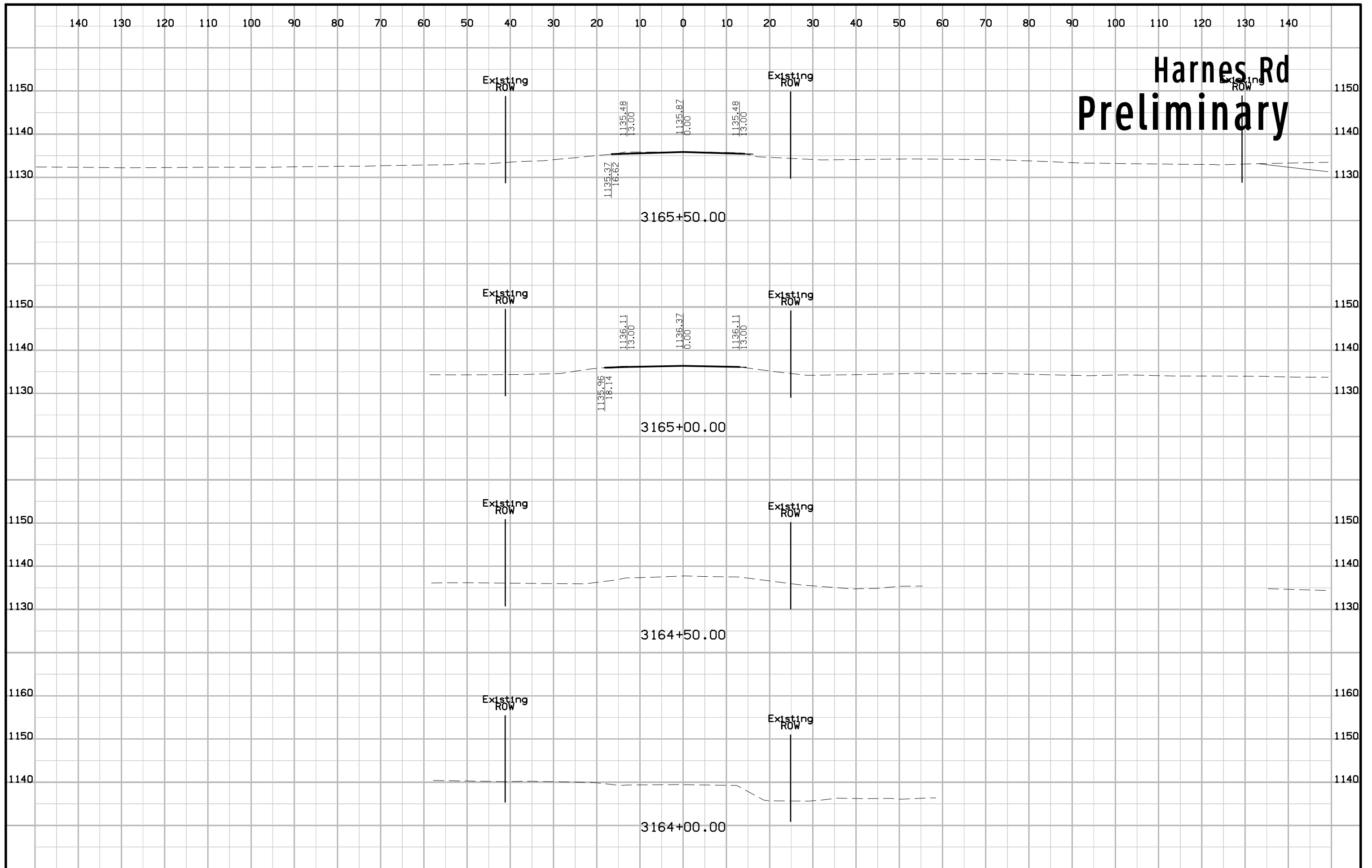
# Lone Tree Rd Preliminary



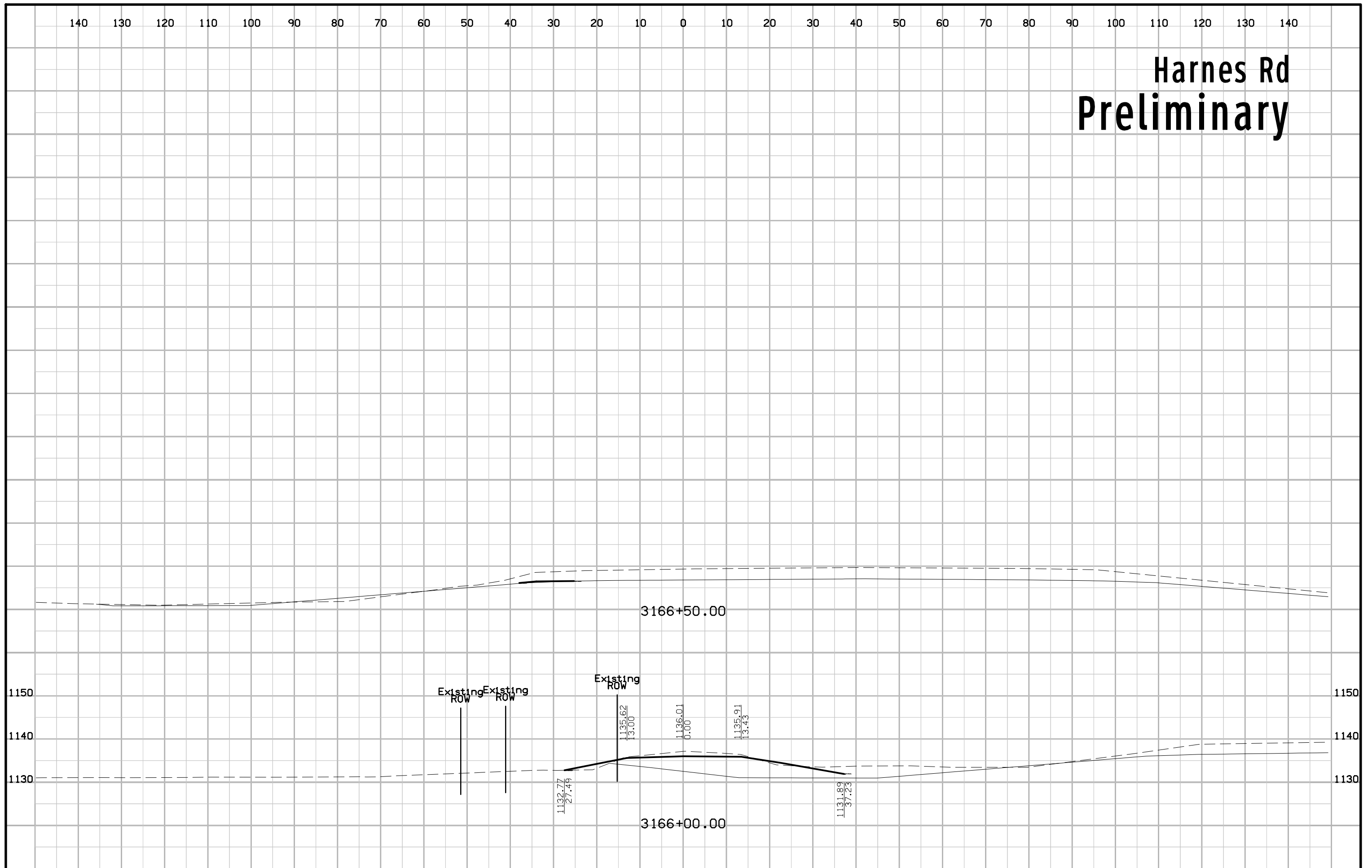
# Lone Tree Rd Preliminary



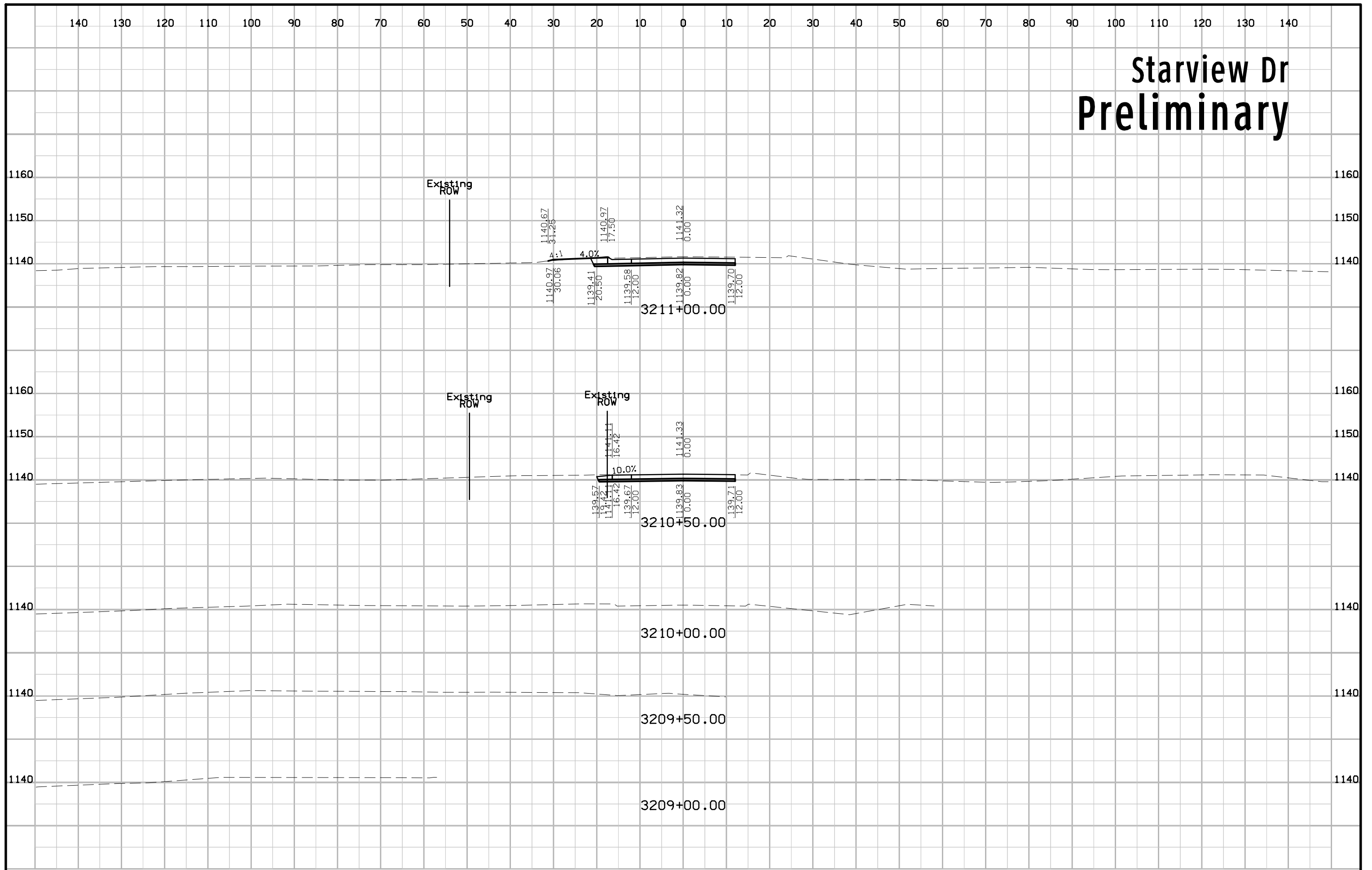
# Harnes Rd Preliminary



# Harnes Rd Preliminary

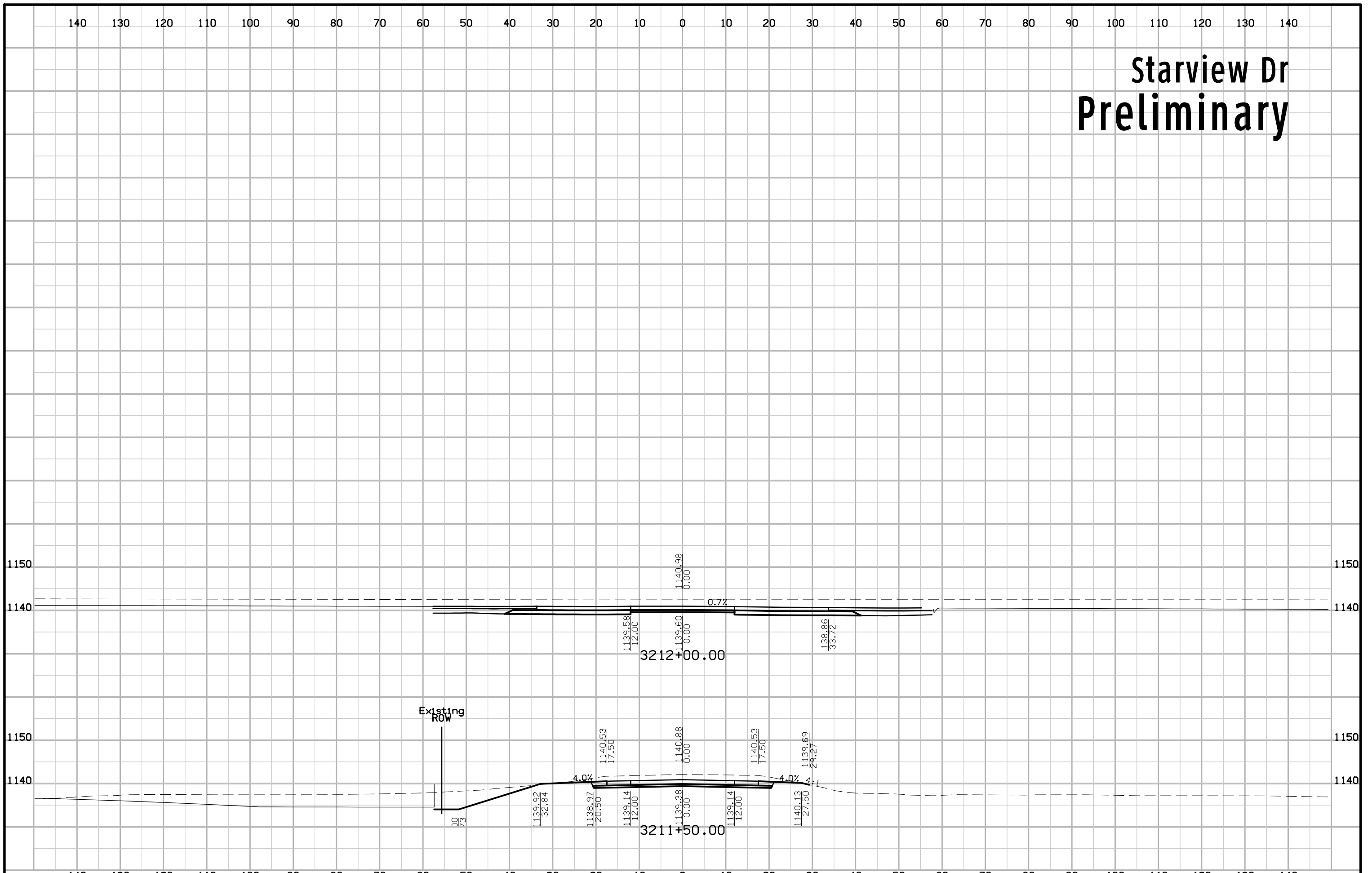


# Starview Dr Preliminary

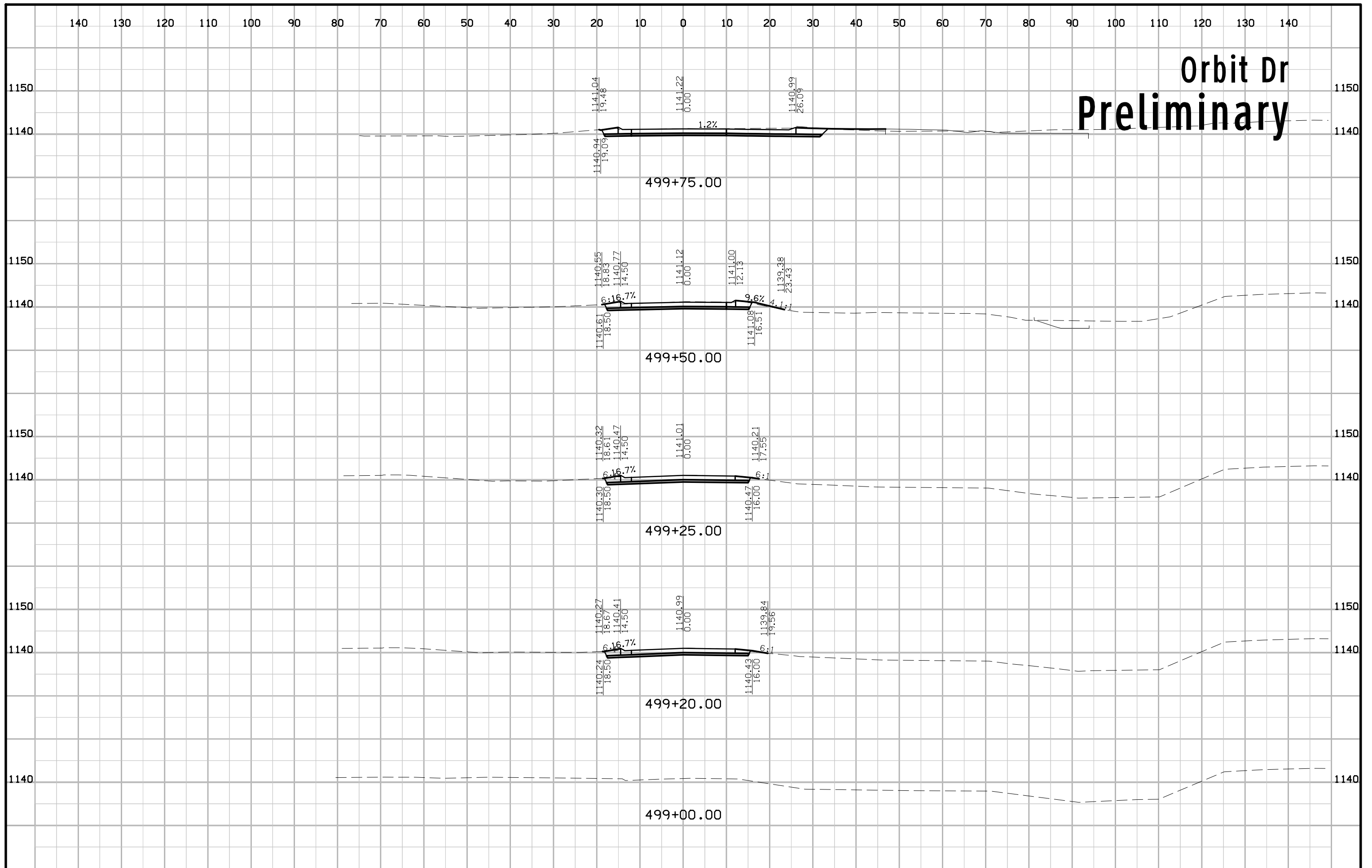




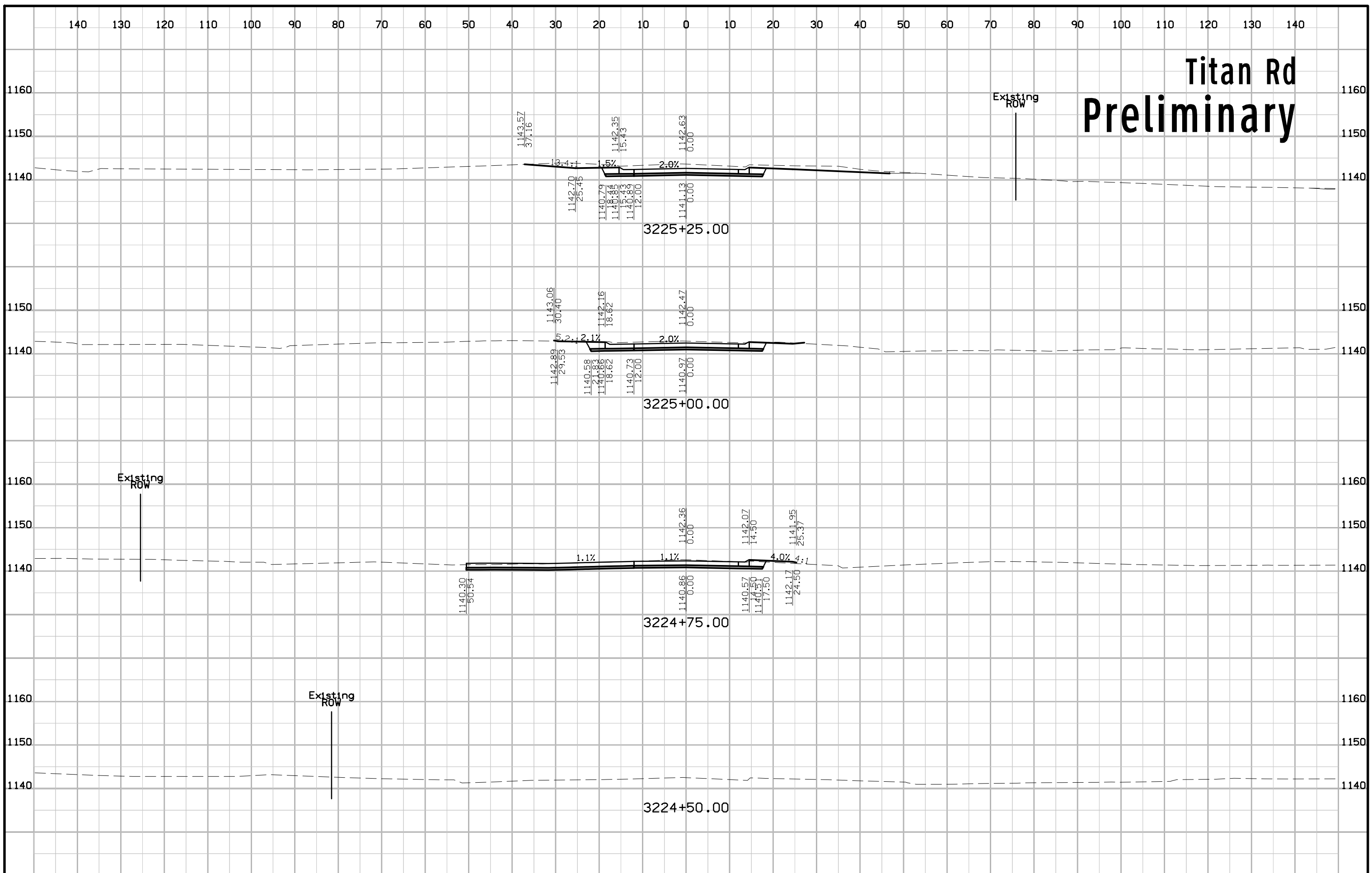
# Starview Dr Preliminary



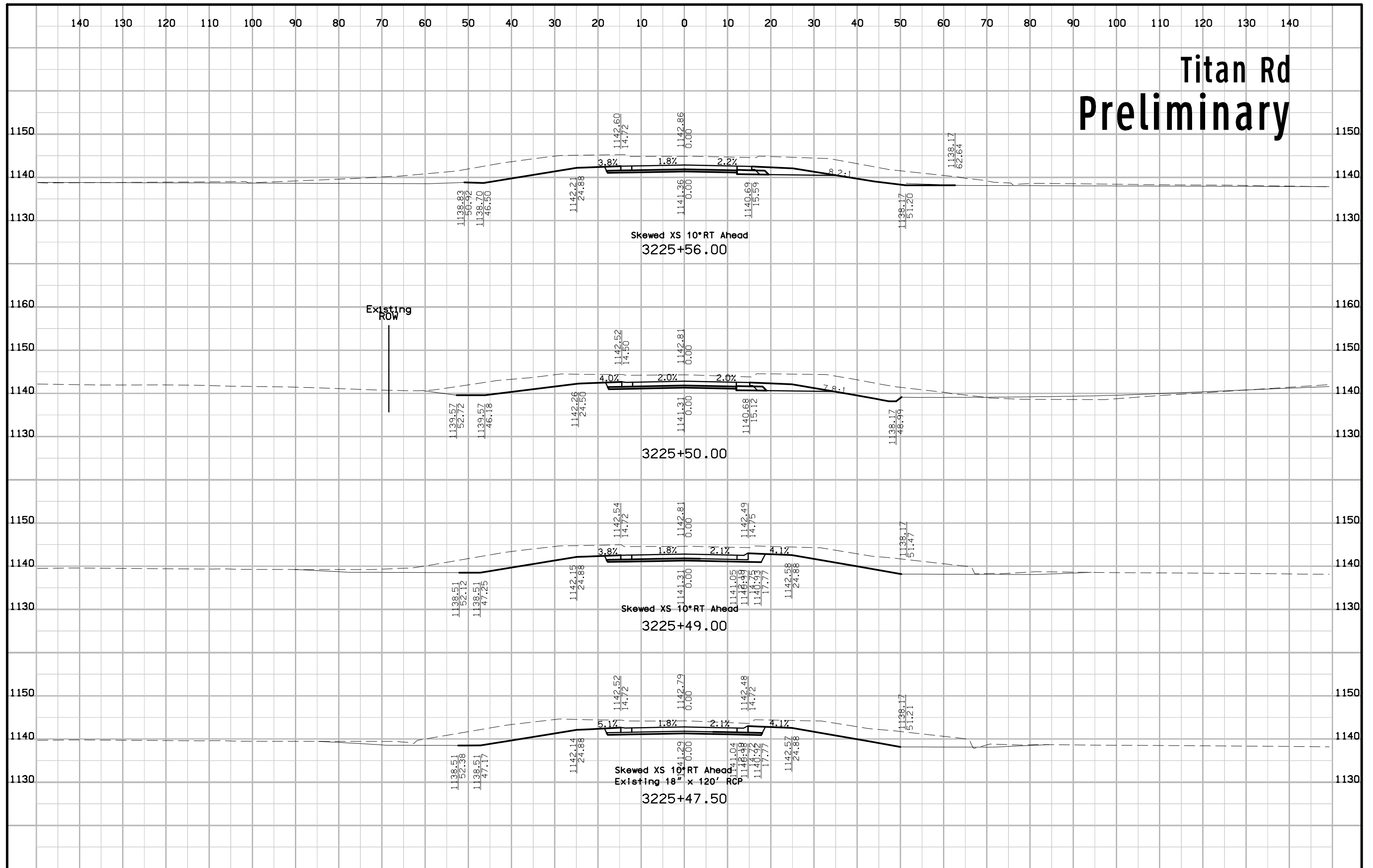
# Orbit Dr Preliminary



# Titan Rd Preliminary



# Titan Rd Preliminary



# Titan Rd Preliminary

