

**CERRO GORDO CO. PCC PAV'T-GRADE & REPLACE**  
**NHSX-122-1(18)--3H-17**

LETTING DATE  
 12-19-17



**Highway Division**

PLANS OF PROPOSED IMPROVEMENT ON THE

PRIMARY ROAD SYSTEM  
**CERRO GORDO COUNTY**  
 PCC PAVEMENT - GRADE AND REPLACE

IN MASON CITY FROM MONROE AVE TO CAROLINA AVE

SCALES: As Noted

Refer to the Proposal Form for list of applicable specifications.

Value Engineering Saves. Refer to Article 1105.15 of the Specifications.

**NO MILEAGE SUMMARY**



REVISIONS

TOTAL

PROJECT IDENTIFICATION NUMBER

11-17-122-020

PROJECT NUMBER

NHSX-122-1(18)--3H-17

R.O.W. PROJECT NUMBER

**PROJECT LOCATION**



For Project Location Map  
 Refer to Sheet A.2

**DESIGN DATA**

20--	AADT	---	V.P.D.
20--	AADT	---	V.P.D.
20--	DHV	---	V.P.H.
	TRUCKS	---	%
	Total		
	Design ESALs	---	

**INDEX OF SEALS**

SHEET NO.	NAME	TYPE
A.1	X	Primary Signature Block
X	X	X

**PRELIMINARY PLANS**

Subject to change by final design.

Pre D2 PLAN - Date: 07/16/14

FILE NO.

ENGLISH

DESIGN TEAM **JIA\MILLER**

**CERRO GORDO** COUNTY

PROJECT NUMBER

**NHSX-122-1(18)--3H-17**

SHEET NUMBER

**A.1**

**STA 99+28.78 (WB)**  
**BEGIN CONSTRUCTION**  
**(5TH STREET)**

**STA 142+10.26 (WB)**  
**= STA 347+30.89 (EB & WB)**  
**(5TH STREET)**

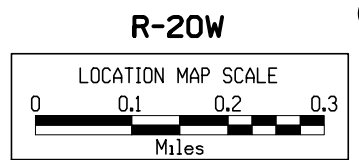
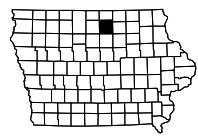
**STA 351+00.87 (EB & WB)**  
**END CONSTRUCTION**  
**(5TH STREET)**

**STA 202+31.36 (EB)**  
**BEGIN CONSTRUCTION**  
**(6TH STREET)**

**STA 240+29.14 (EB)**  
**= STA 2340+29.14 (EB)**  
**(6TH STREET)**

**STA 247+30.89 (EB)**  
**= STA 347+30.89 (EB & WB)**  
**(5TH STREET)**

**STA 2343+38.63 (EB)**  
**END CONSTRUCTION**  
**(6TH STREET)**



**LOCATION MAP**

<b>Roadway</b>	IA 122 in Mason City (5th St - WB and 6th St - EB)		
<b>PIN Number</b>	11-17-122-020	<b>Submittal Date</b>	
<b>Project Number</b>	NHSX-122-1(18)--3H-17	<b>Approval Date</b>	
<b>District</b>	2	<b>Assistant District Engineer</b>	
<b>County</b>	Cerro Gordo	<b>or</b>	
<b>Route</b>	IA 122	<b>Office Director</b>	
<b>Location</b>	In Mason City from Monroe Ave. to Carolina Ave.		
<b>Work Type</b>	Reconstruction		
<b>Segment Manager</b>	Yan Jia		
<b>Designer</b>	Ryan Miller		

Design Element		Preferred	Acceptable Criteria	Project Values	
Design speed (mph)		The anticipated posted speed limit	30	30	
Maximum superelevation rate (Refer to Section 2A-2)		4%	8%	Normal Crown	
Design lane width (ft)		12	11	12'	
Full depth paved width (ft)	Outside lane	Design lane width + curb and gutter unit or 14 feet for roadways with shoulders, 12' if using full depth shoulders	Match design lane width	18.5' incl bike In	
	Inside lane(s)	Design lane width + curb and gutter unit. 12' for roadways without a curb and gutter unit	Match design lane width	15.5'	
Right turn lane or an auxiliary lane (ft)		12	10	12'	
Left turn lane (ft)	With raised or painted median	12 ft + median	10 ft + median	---	
	With depressed median	12	10	12'	
Two-way left turn lane (ft)		14	11	---	
Parking lane width (ft)		10	7	---	
Pavement cross-slope (on tangent sections)	Through lanes	2%, However, when adjacent lanes slope in the same direction, increase slope by 0.5% per lane up to 3%	1.5% minimum, 3% maximum	2% - 2.5%	
	Auxiliary and turn lanes	3%	3% maximum	3%	
	Crown break at centerline	4%	4% maximum	4%	
Shoulder cross-slope (on tangent sections)	Shoulders	4%	Shoulder cross-slope cannot be less than the adjacent lane, 6% max for paved or granular shoulders, 8% max for earth shoulders	---	
	Curb and gutter units	Match pavement cross-slope		6% maximum	Match Pvt
	Parking lanes	1% greater than pavement cross-slope		6% maximum	
Curb type (Refer to Section 3C-2)		Design speed ≤ 45 mph	6-inch standard	any shape	6" Std.
Foreslope (For fill areas greater than 40 ft, contact the Soils Design Section for assistance)	Adjacent to shoulder	10:1 for 4' then 6:1	3:1		
	Beyond standard ditch depth and design clear zone	3.5:1	3:1		
	Curbed roadways	2%	not steeper than 3:1	2%	
Backslope (For cut areas greater than 25 feet, contact the Soils Design Section for assistance with backslope benches.)		3:1	2.5:1		
Transverse Slopes	w/ drainage structures	8:1	6:1		
	w/o drainage structures	10:1	6:1		
Ditches (Refer to Section 3G-1) Outside ditch (depth x width) (ft)		5 x 10	--		
Median width (ft) (Refer to Section 3E-1)		See Section 3E-1	0		
Bridge width—new	Bridge length ≤ 200 ft	design lane widths + effective shoulder widths or curb-to-curb width	design lane widths + effective shoulder widths or curb-to-curb width		
	Bridge length > 200 ft	design lane widths + effective shoulder widths or curb-to-curb width	Curb-to-curb width or design lane widths + 4 ft offset each side for roadways with shoulders		
Bridge width—existing		design lane widths + no less than 2 ft left and right	design lane widths + 2 ft left and right of the design widths		
Vertical clearance (ft) (above lanes, shoulders and 25 feet left and right of the center of railroad tracks)	Over primary	16.5	16		
	Over non-primary	16.5 at interchange locations, 15 at all other locations	14		
	Over railroad	23.3	23.3		
	Sign truss and pedestrian crossings	17.5	17		
Structural Capacity		Contact Office of Bridges and Structures	Contact Office of Bridges and Structures		
Level of Service		C	D		

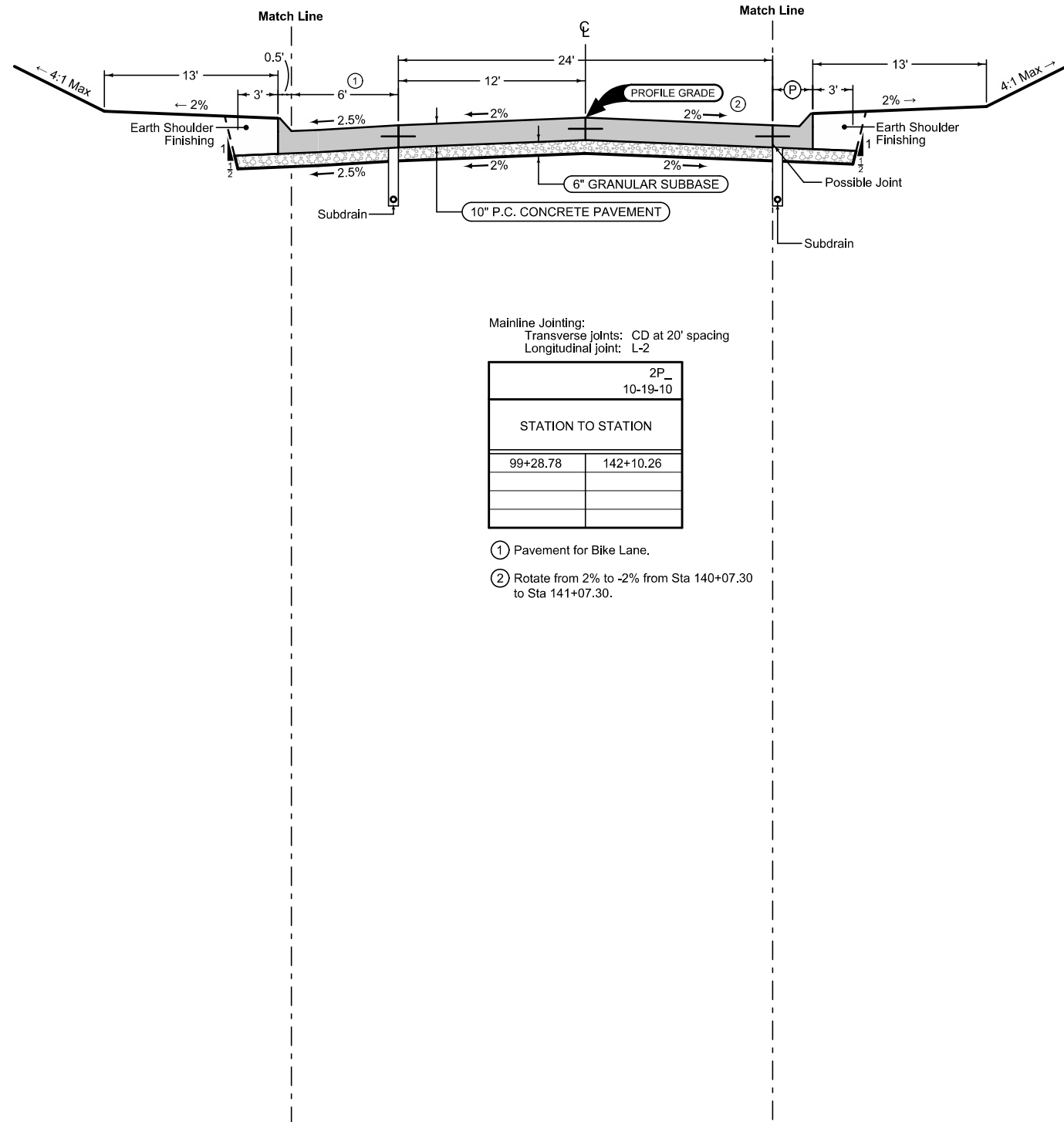
Roadway Design Speed (mph) = 35			Design Criteria for Low Speed Roadways										
Design Manual Section 1C-1 last update: 08-05-13			Design Criteria for Low Speed Roadways										
Design Element	Preferred Criteria					Acceptable Criteria					Project Values		
	Design Speed, mph					Design Speed, mph							
	25	30	35	40	45	25	30	35	40	45			
Stopping sight distance (ft) (Refer to Section 6D-1)			155	200	250	305	360	155	200	250	305	360	250
Minimum horizontal curve radius (ft) and superelevation rate (Refer to Sections 2A-2 and 2A-3)	Method 2 superelevation and side friction distribution	e = 4% max	See Table 10 in Section 2A-3					--					510
		e <sub>max</sub> = 6%	144	231	340	485	643	144	231	340	485	643	---
	Method 5 superelevation and side friction distribution	e <sub>max</sub> = 8%	--	--	--	--	--	134	214	314	444	587	---
Minimum vertical curve length (ft) (Refer to Section 2B-1)			75	90	105	120	135	75	90	105	120	135	***
Minimum rate of vertical curvature (K) (Refer to Section 2B-1)	crest vertical curves		12	19	29	44	61	12	19	29	44	61	29
	sag vertical curves	roadways without fixed-source lighting	26	37	49	64	79	26	37	49	64	79	
		roadways with fixed-source lighting	26	37	49	64	79	14	20	27	35	44	49
Minimum gradient (%) (Refer to Section 2B-1)			0.5					0.3% with a curb, 0.0% without a curb					0.5
Maximum gradient (%) (Refer to Section 2B-1)	Urban roadways		5					--	9	8	8	7	5
	Rural roadways							--	--	--	6	6	---
Clear zone			See "Preferred Clear Zone" table in Section 8A-2					See "Acceptable Clear Zone" table in Section 8A-2					16-14

### Curbed Shoulder

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of  
 curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

STATION TO STATION		Curb Type See PV-102
99+28.78	142+10.26	6" Std.



Mainline Jointing:  
 Transverse joints: CD at 20' spacing  
 Longitudinal joint: L-2

STATION TO STATION	
99+28.78	142+10.26

- ① Pavement for Bike Lane.
- ② Rotate from 2% to -2% from Sta 140+07.30 to Sta 141+07.30.

### Curbed Shoulder

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of  
 curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

STATION TO STATION		(P) Feet	Curb Type See PV-102
99+28.78	142+10.26	3.5	6" Std.

See Tab 100-24 or 100-25 for pavement quantities.  
 See Tab 112-9 for shoulder quantities.

**IA 122 WB (5th St. SE)**

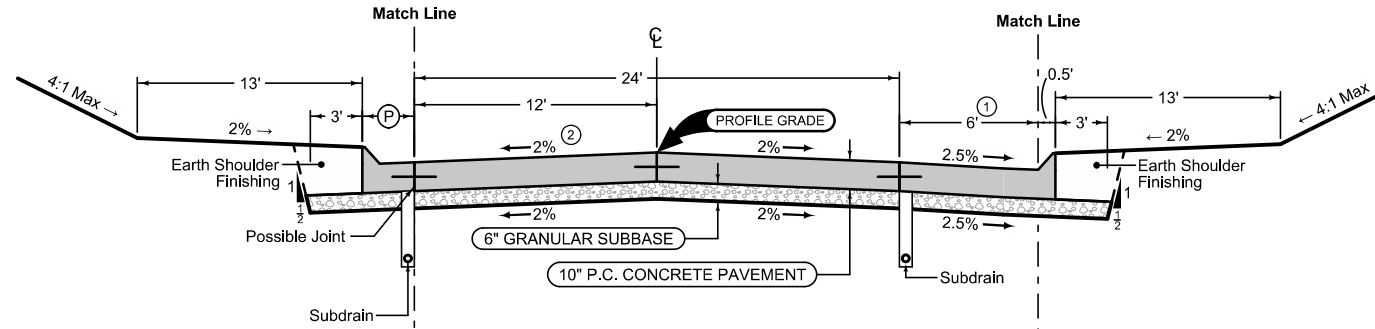
LOOKING IN DIRECTION OF STATIONING

### Curbed Shoulder

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

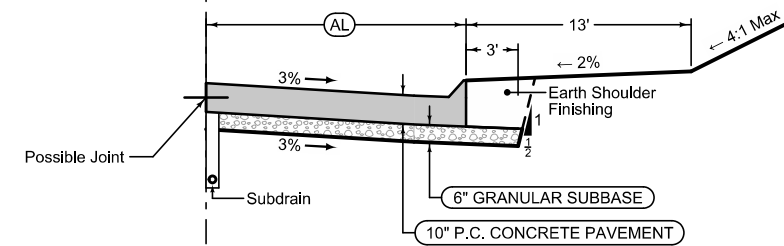
STATION TO STATION		(P) Feet	Curb Type See PV-102
202+31.36	247+30.89	3.5	6" Std.



Mainline Jointing:  
 Transverse joints: CD at 20' spacing  
 Longitudinal joint: L-2

STATION TO STATION	
202+31.36	247+30.89

- ① Pavement for Bike Lane.
- ② Rotate from 2% to -2% from Sta 245+31.55 to Sta 246+31.55.



### Auxiliary Lane

Longitudinal joint: L or KT  
 Transverse joint: Match Mainline

STATION TO STATION		(AL) Feet
207+00.00	208+20.60	0 - 12.5
208+20.60	209+84.37	12.5
220+50.07	221+70.07	0 - 12.5
221+70.07	222+69.55	12.5

### Curbed Shoulder

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

STATION TO STATION		Curb Type See PV-102
202+31.36	207+00.00	6" Std.
209+84.37	220+50.07	6" Std.
222+69.55	247+30.89	6" Std.

### Curbed Shoulder

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

STATION TO STATION		Curb Type See PV-102
207+00.00	208+20.60	6" Std.
208+20.60	209+84.37	6" Std.
220+50.07	221+70.07	6" Std.
221+70.07	222+69.55	6" Std.

See Tab 100-24 or 100-25 for pavement quantities.

See Tab 112-9 for shoulder quantities.

**IA 122 EB (6th St. SE)**

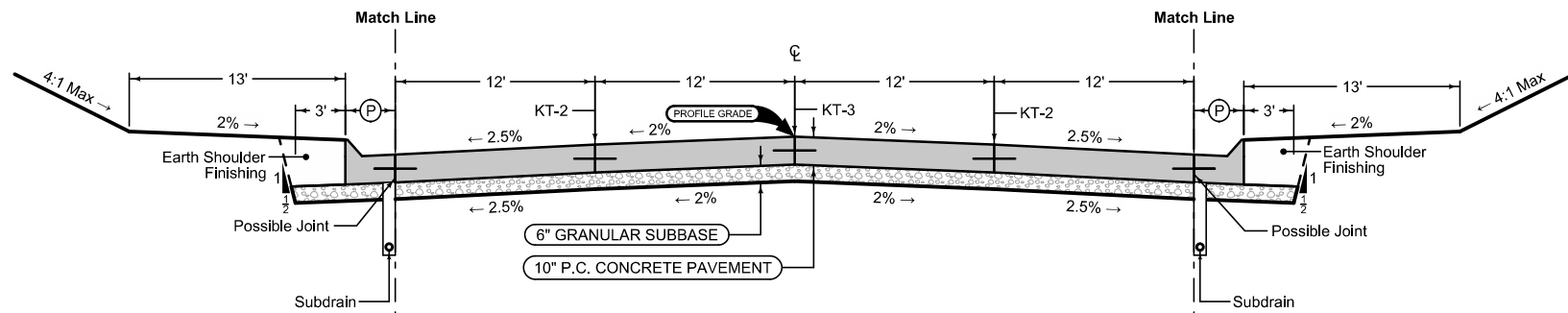
LOOKING IN DIRECTION OF STATIONING

### Curbed Shoulder

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of  
 curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

		2_Curb_ 04-19-11	
STATION TO STATION	(P) Feet	Curb Type See PV-102	
347+30.89	351+00.87	3.0	6" Std.



Mainline Jointing:  
 Transverse joints: CD at 20' spacing

4UP_ 10-16-12	
STATION TO STATION	
347+30.89	351+00.87

### Curbed Shoulder

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of  
 curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

		2_Curb_ 04-19-11	
STATION TO STATION	(P) Feet	Curb Type See PV-102	
347+30.89	351+00.87	3.0	6" Std.

See Tab 100-24 or 100-25 for pavement quantities.  
 See Tab 112-9 for shoulder quantities.

### IA 122 (5th St. SE)

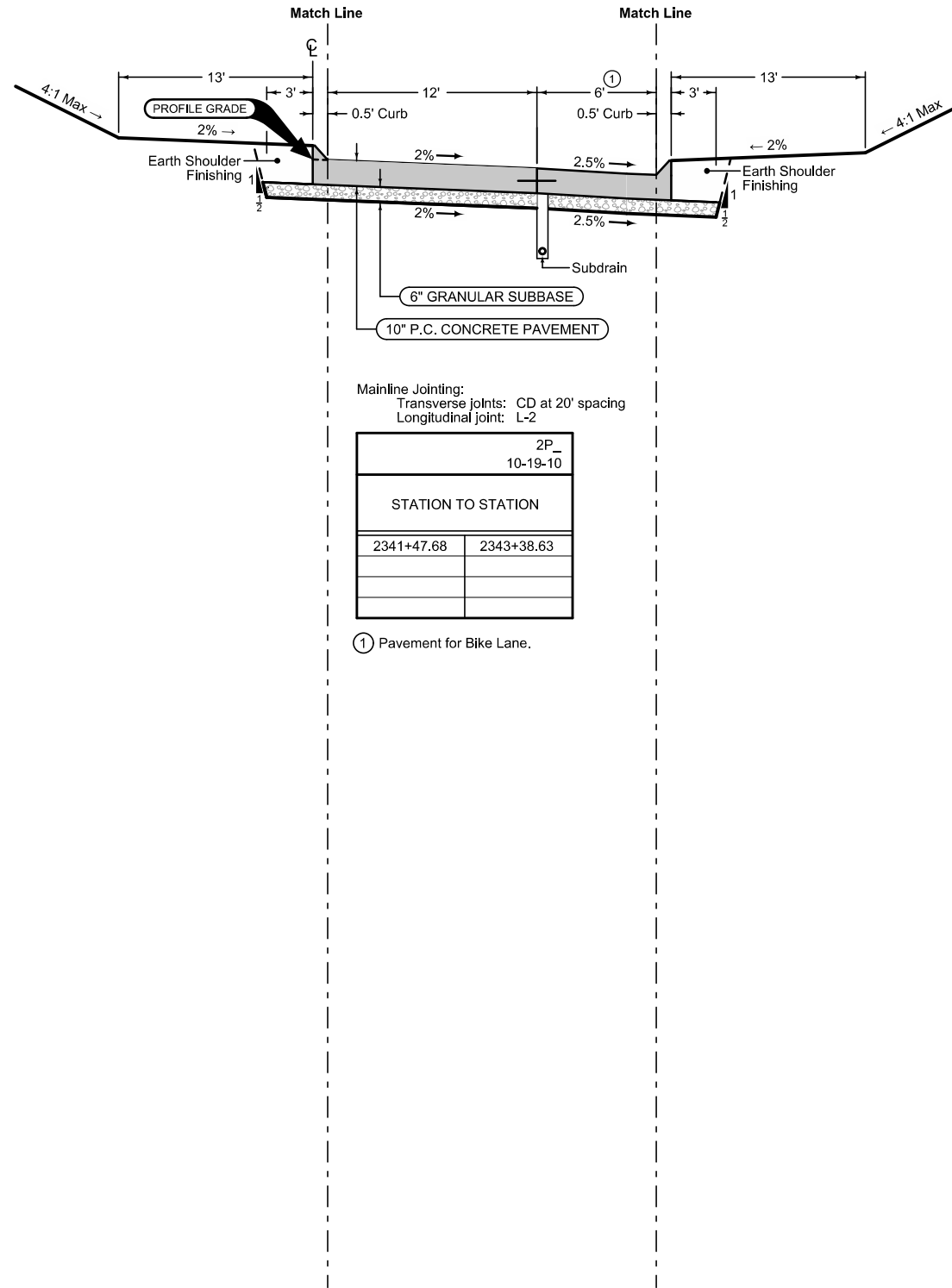
LOOKING IN DIRECTION OF STATIONING

### Curbed Shoulder

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of  
 curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

		2_Curb_ 04-19-11
STATION TO STATION		Curb Type See PV-102
2341+47.68	2343+38.63	6" Std.



Mainline Jointing:  
 Transverse joints: CD at 20' spacing  
 Longitudinal joint: L-2

		2P_ 10-19-10
STATION TO STATION		
2341+47.68	2343+38.63	

① Pavement for Bike Lane.

### Curbed Shoulder

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of  
 curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

		2_Curb_ 04-19-11
STATION TO STATION		Curb Type See PV-102
2341+47.68	2343+38.63	6" Std.

See Tab 100-24 or 100-25 for pavement quantities.  
 See Tab 112-9 for shoulder quantities.

**6TH STREET SE**  
 LOOKING IN DIRECTION OF STATIONING

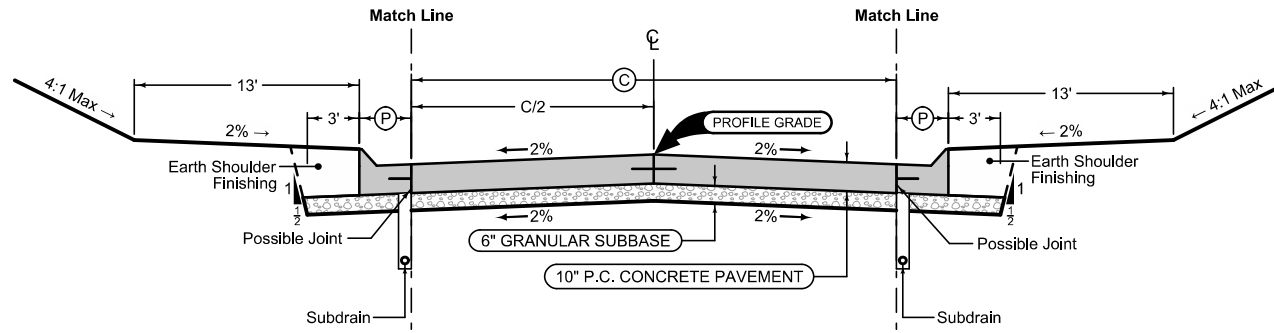


### Curbed Shoulder

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of  
 curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

		2_Curb_04-19-11	
STREET IDENTIFICATION	STATION TO STATION	(P) Feet	Curb Type See PV-102
Fire Station Entrance		2.5	6" Std.
S. Jefferson Av			
S. Adams Av			
S. President Av			
S. Washington Av			
S. Federal Av			
S. Delaware Av			
S. Pennsylvania Av			
S. Jersey Av			
S. Georgia Av			
S. Connecticut Av			
S. Massachusetts Av			



Mainline Jointing:  
 Transverse joints: CD at 20' spacing  
 Longitudinal joint: L-2

		2P_10-19-10	
STREET IDENTIFICATION	STATION TO STATION	(C) Feet	
Fire Station Entrance		15.0	
S. Jefferson Av			
S. Adams Av			
S. President Av			
S. Washington Av			
S. Federal Av			
S. Delaware Av			
S. Pennsylvania Av			
S. Jersey Av			
S. Georgia Av			
S. Connecticut Av			
S. Massachusetts Av			

### Curbed Shoulder

Shoulder Jointing:  
 Longitudinal joint not required when distance from back of  
 curb to nearest joint is less than 15':

Single pour: L-2  
 Staged : KT-2  
 Transverse:C at 20' spacing

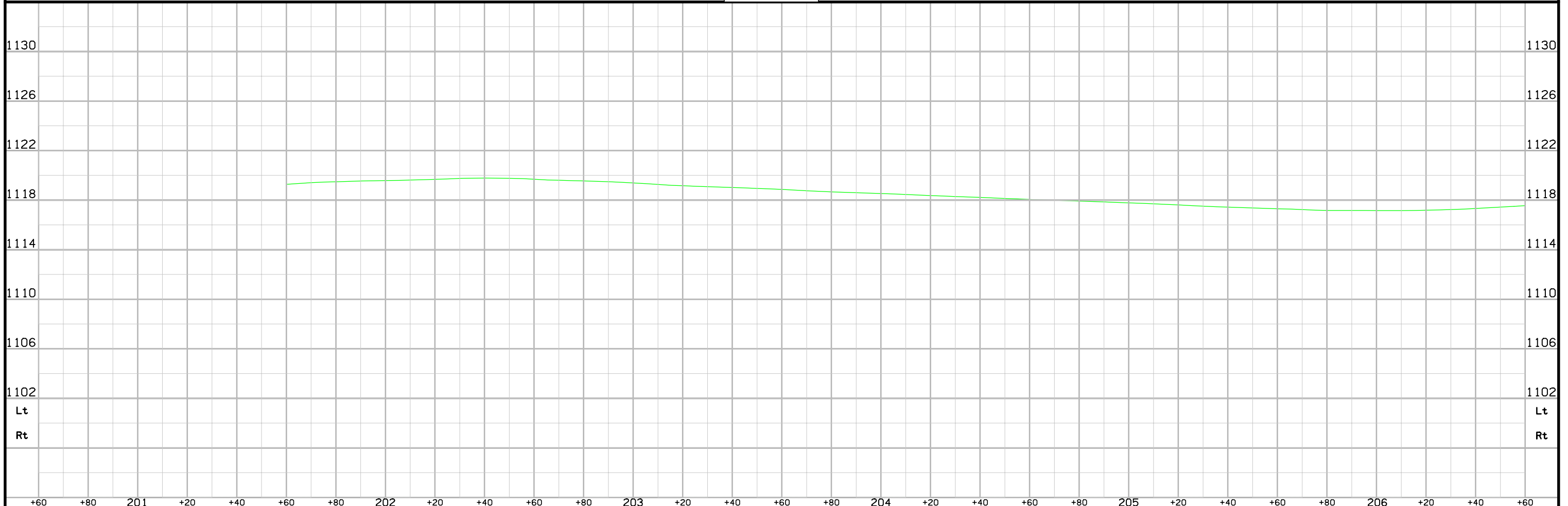
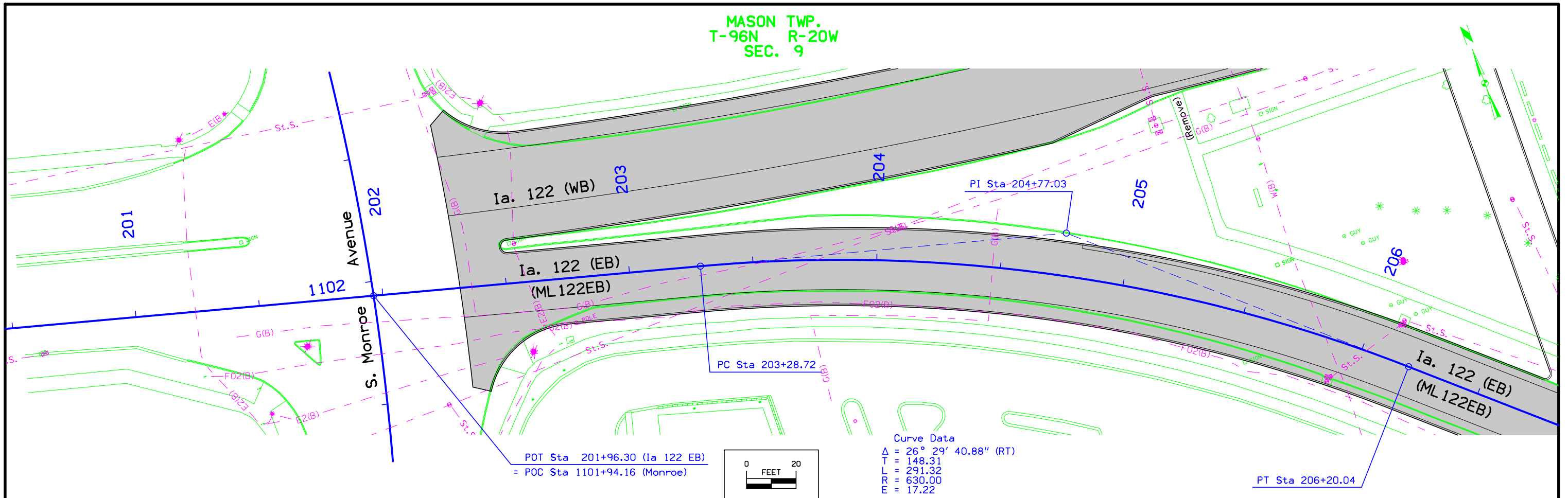
		2_Curb_04-19-11	
STREET IDENTIFICATION	STATION TO STATION	(P) Feet	Curb Type See PV-102
Fire Station Entrance		2.5	6" Std.
S. Jefferson Av			
S. Adams Av			
S. President Av			
S. Washington Av			
S. Federal Av			
S. Delaware Av			
S. Pennsylvania Av			
S. Jersey Av			
S. Georgia Av			
S. Connecticut Av			
S. Massachusetts Av			

See Tab 100-24 or 100-25 for pavement quantities.  
 See Tab 112-9 for shoulder quantities.

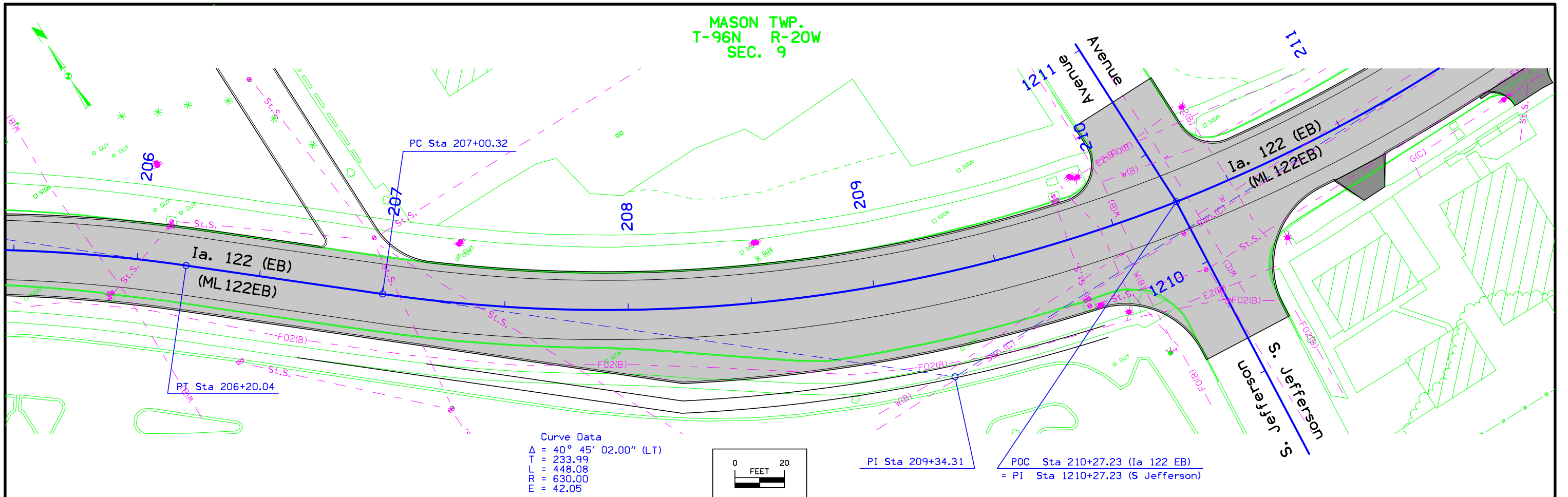
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LOOKING IN DIRECTION OF STATIONING

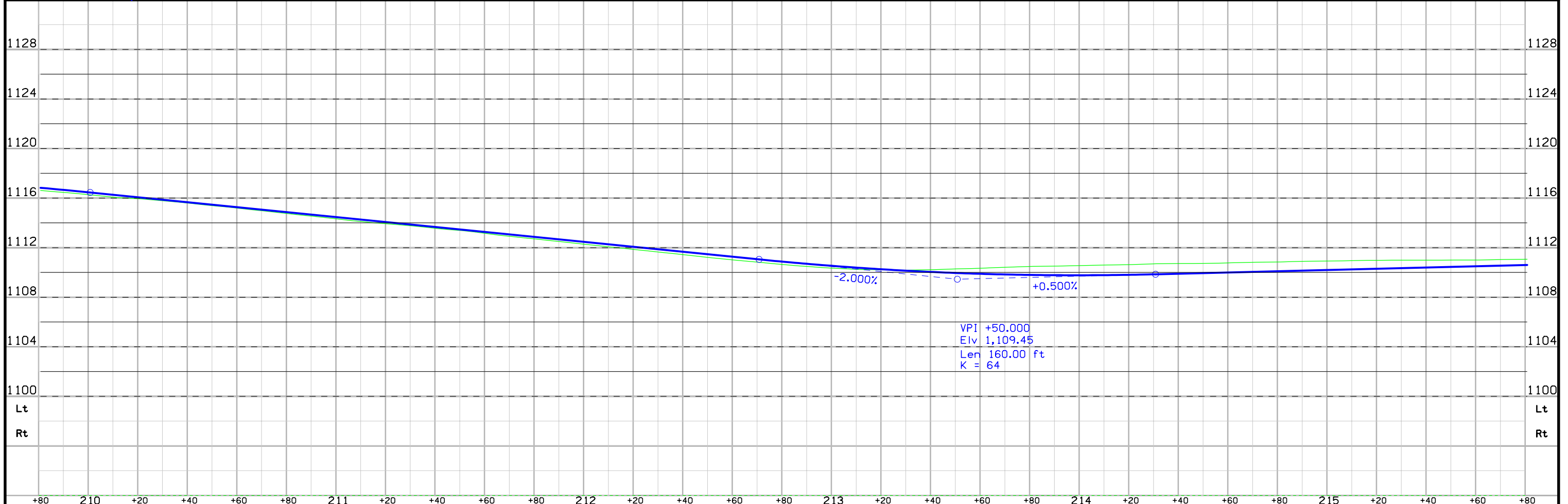
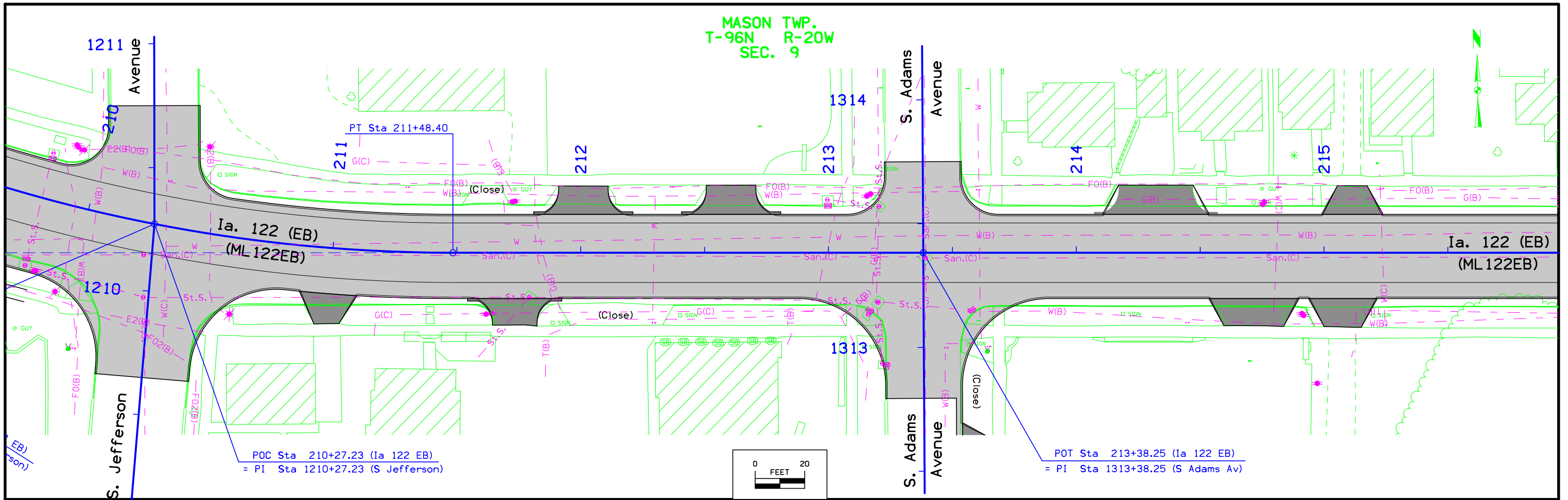
MASON TWP.  
T-96N R-20W  
SEC. 9



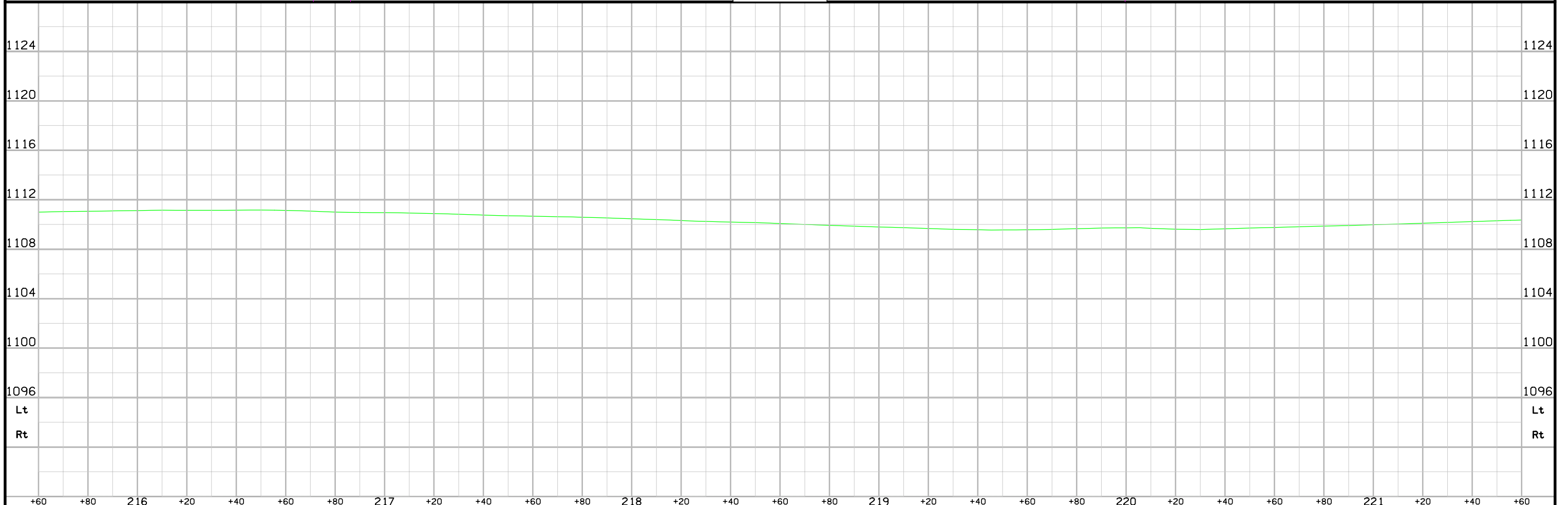
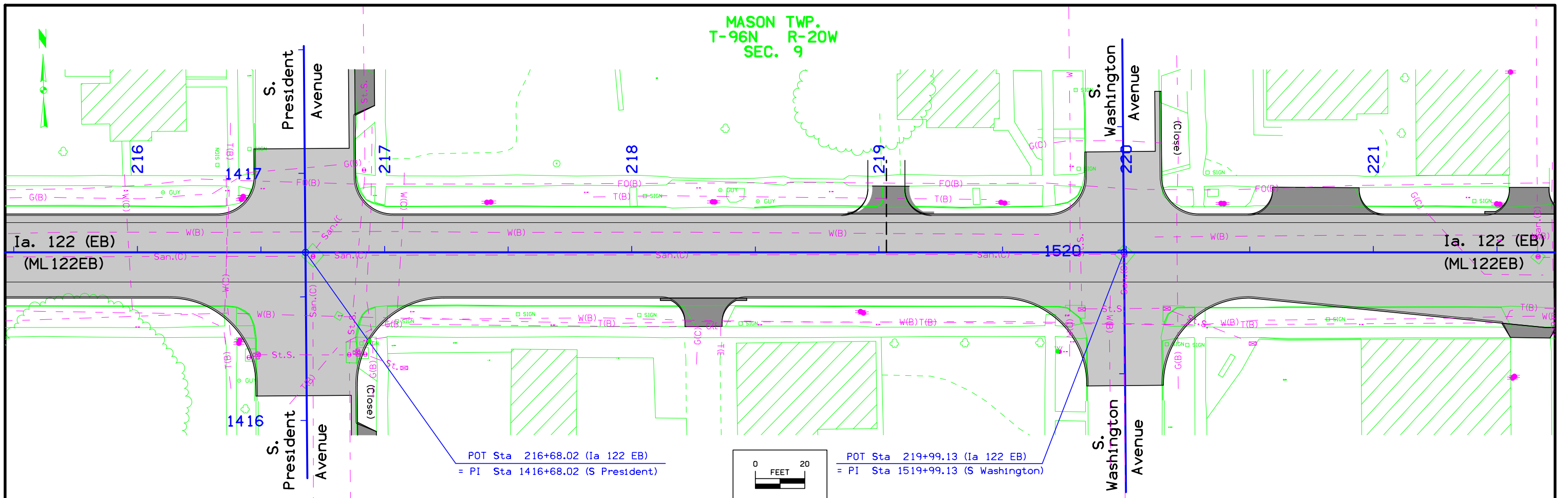
MASON TWP.  
T-96N R-20W  
SEC. 9



MASON TWP.  
T-96N R-20W  
SEC. 9



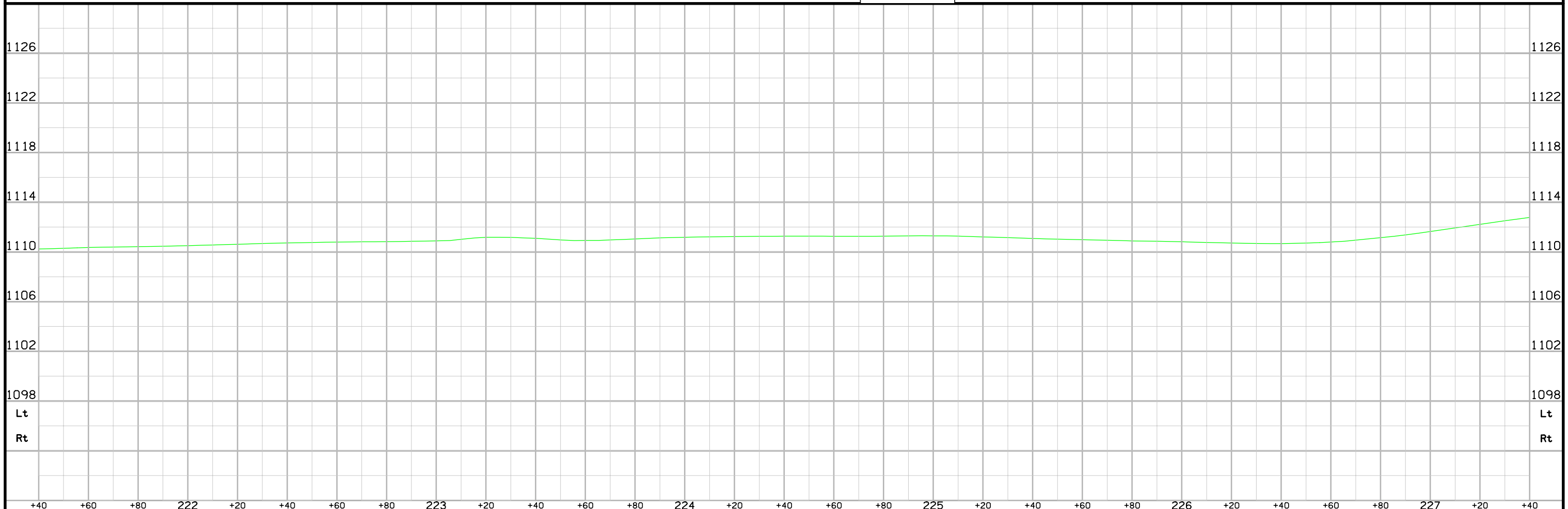
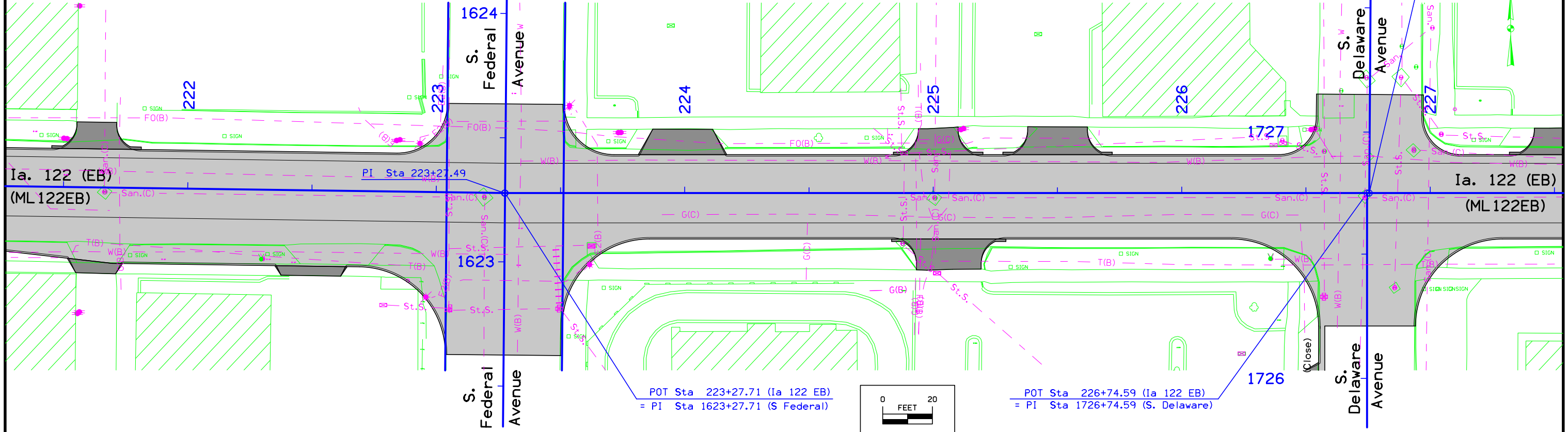
MASON TWP.  
T-96N R-20W  
SEC. 9



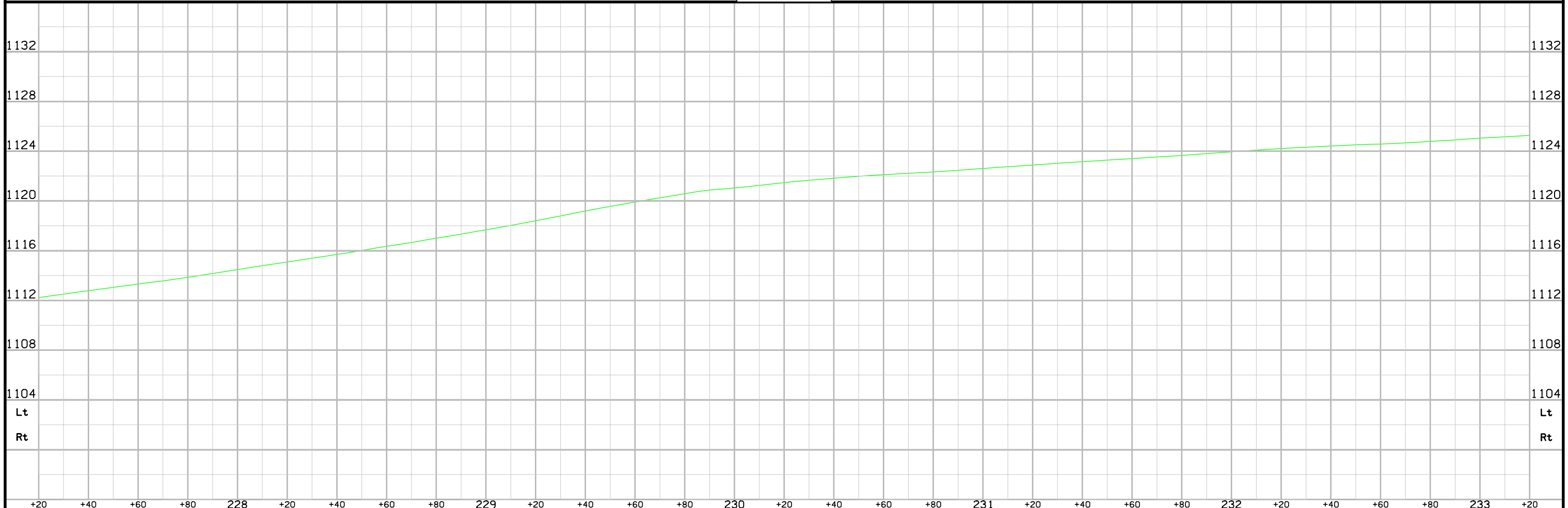
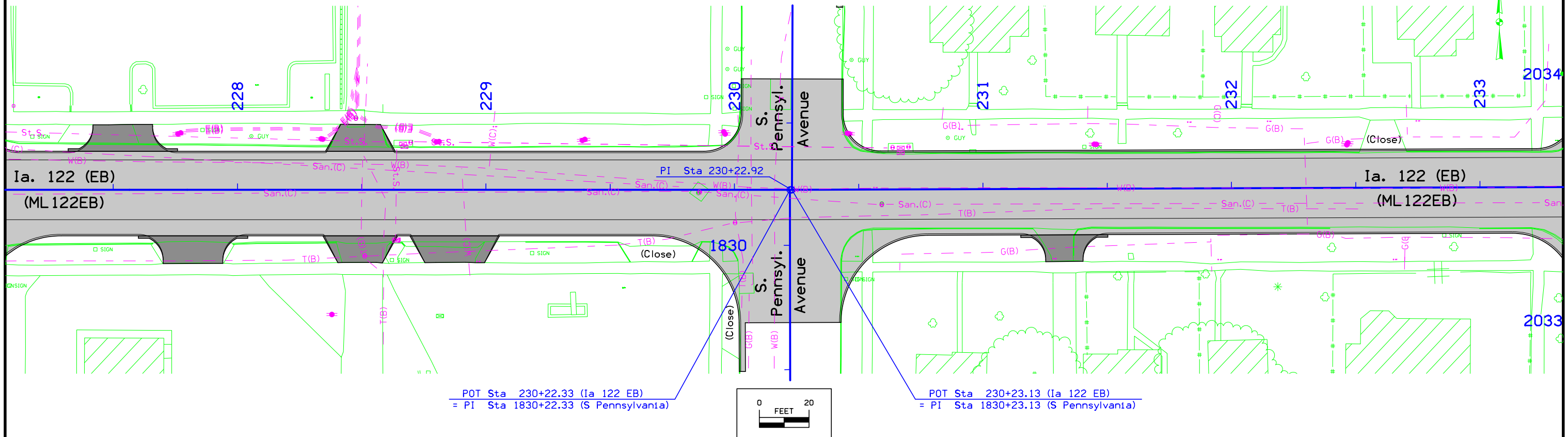
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T-96N R-20W  
SEC. 9

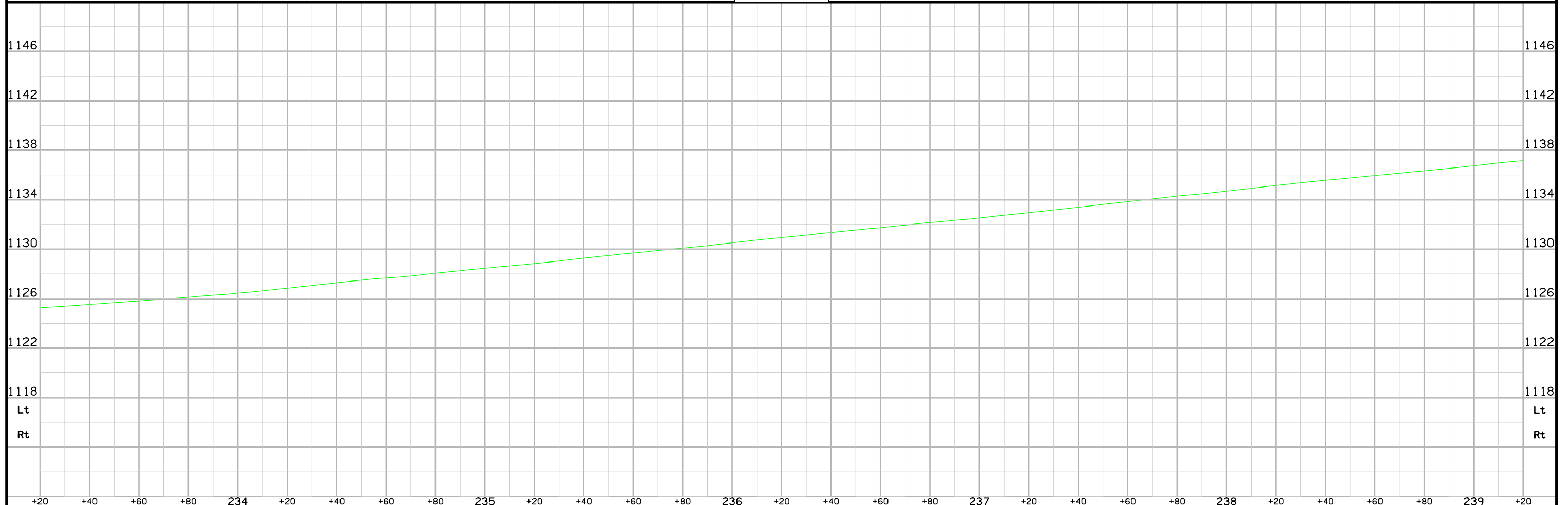
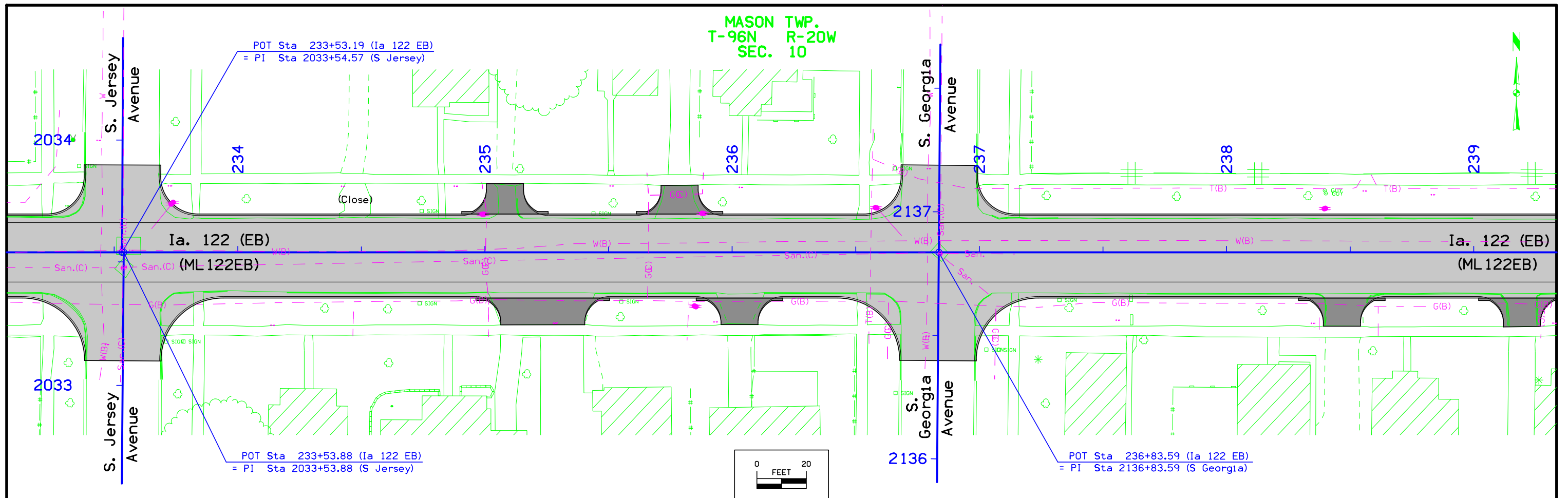
MASON TWP.  
T-96N R-20W  
SEC. 10

POT Sta 226+75.44 (Ia 122 EB)  
= PI Sta 1726+75.44 (S. Delaware)

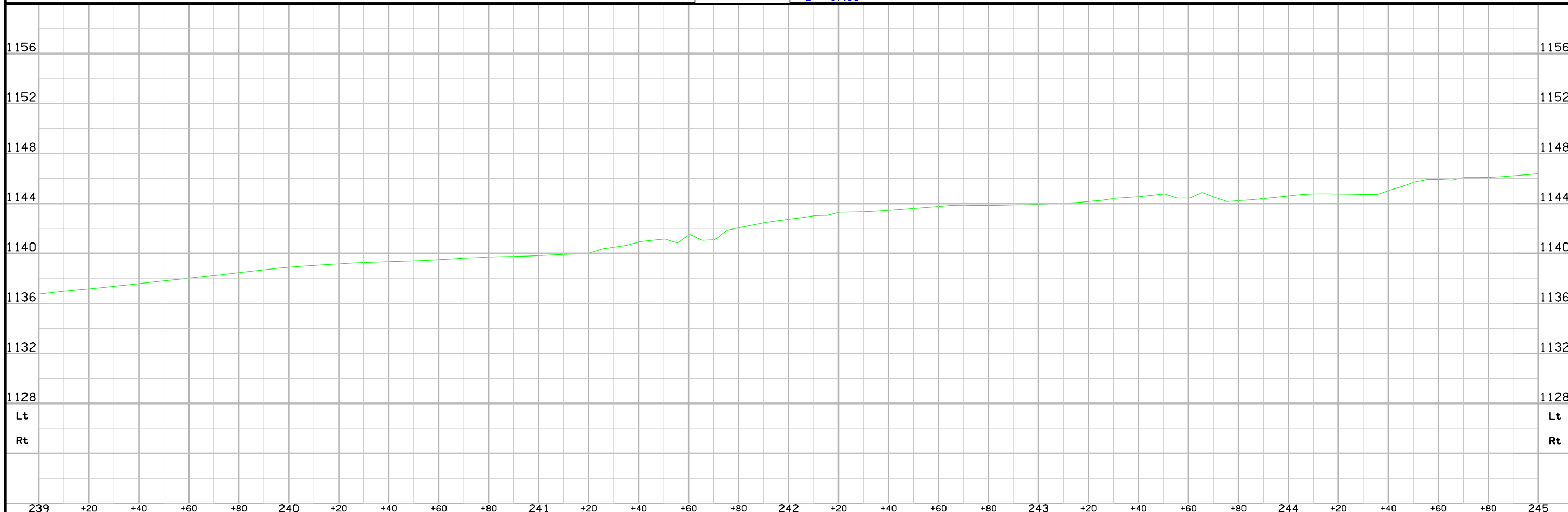
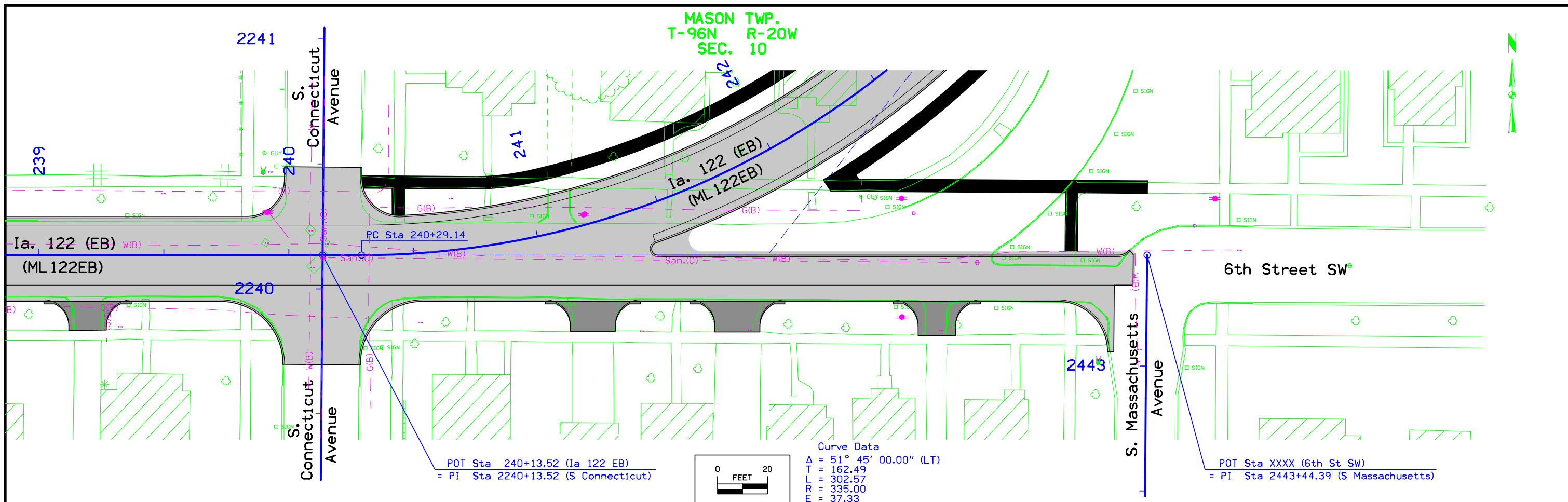


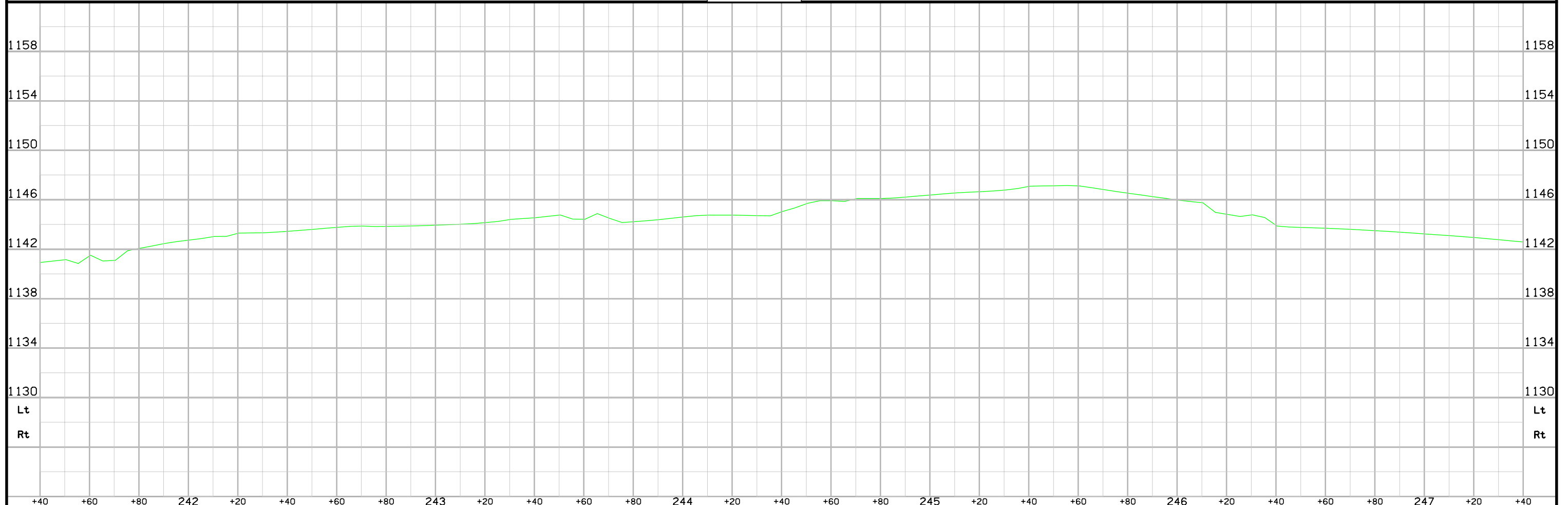
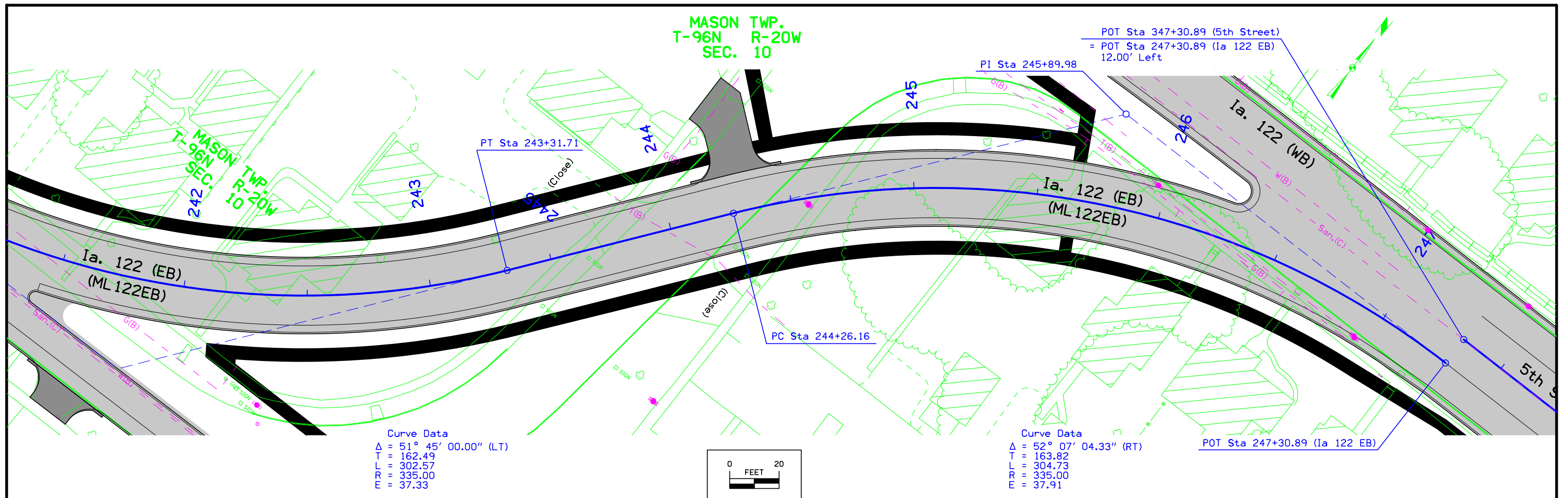
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T-96N R-20W  
SEC. 10



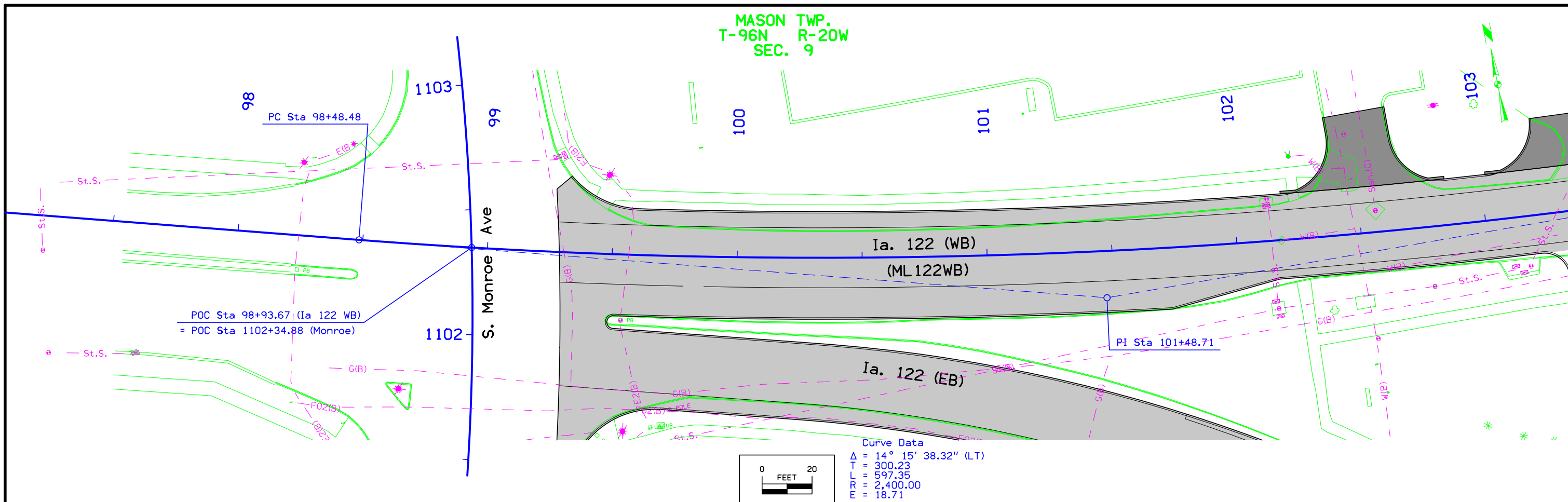




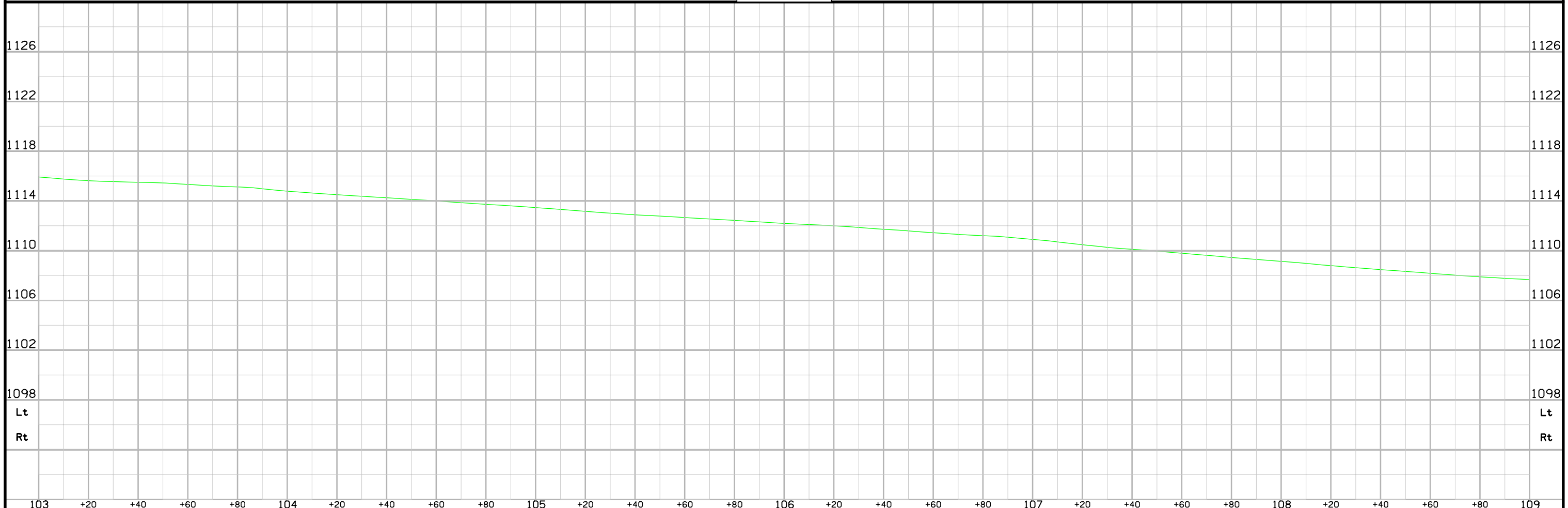
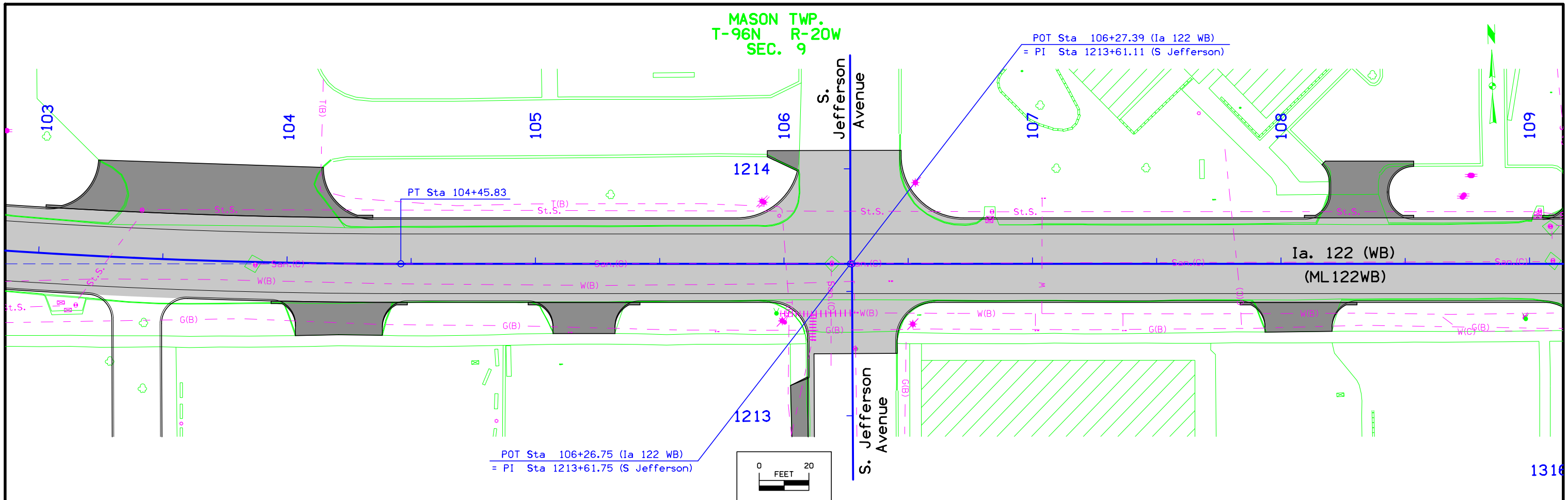




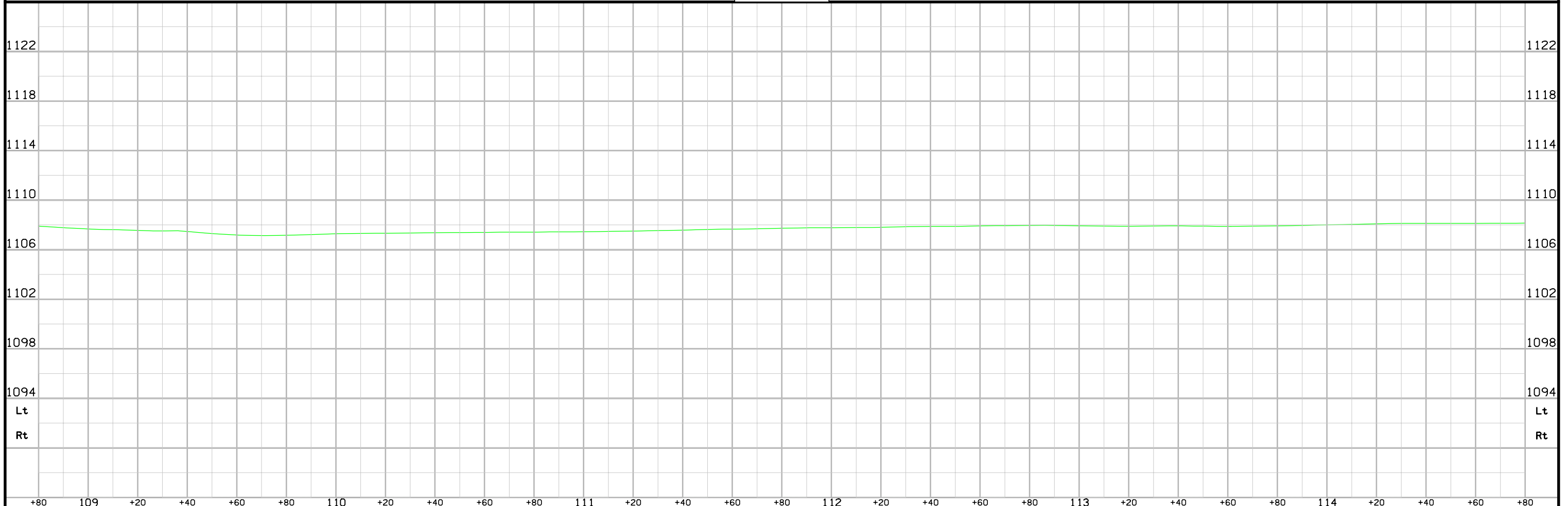
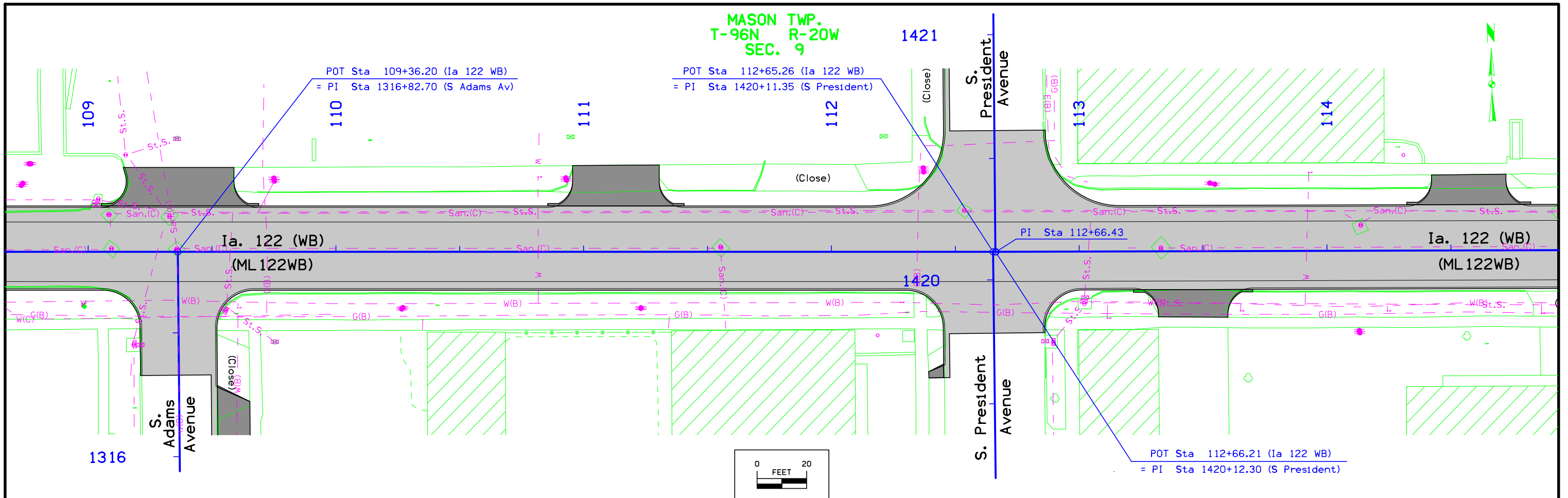
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T-96N R-20W  
SEC. 9

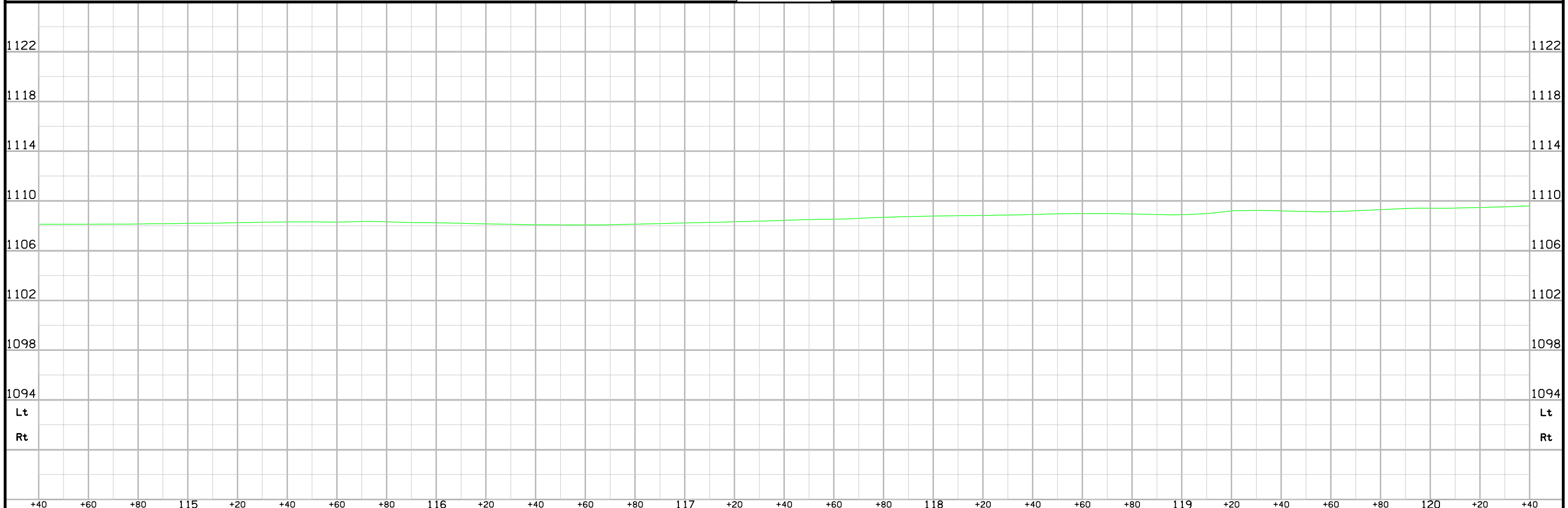
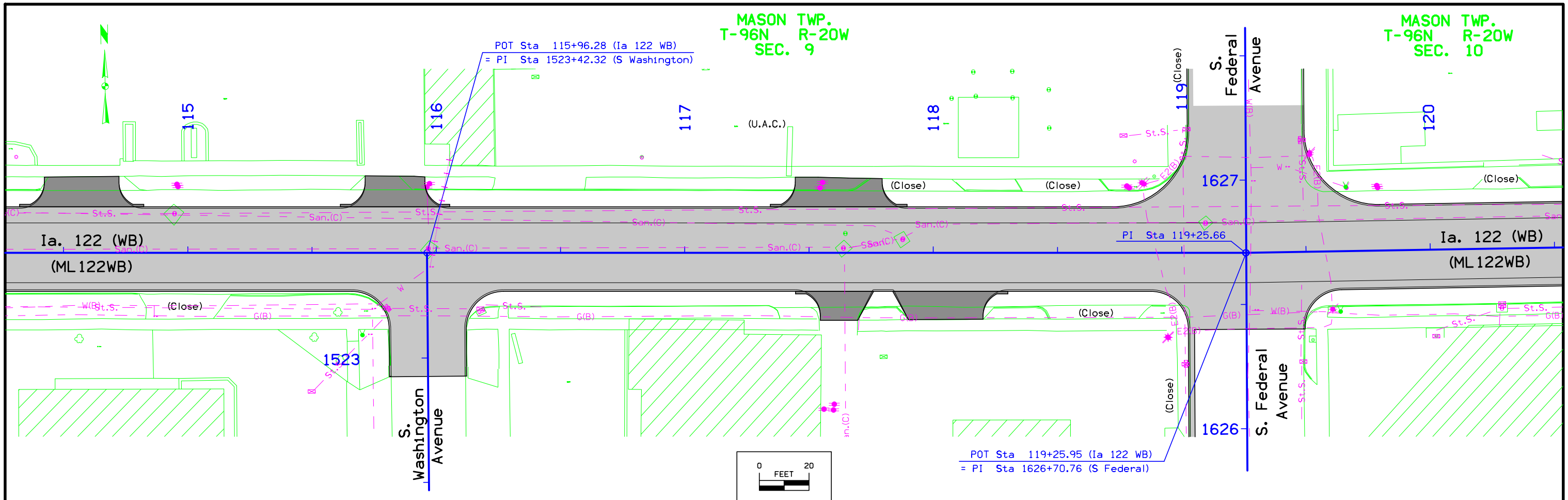


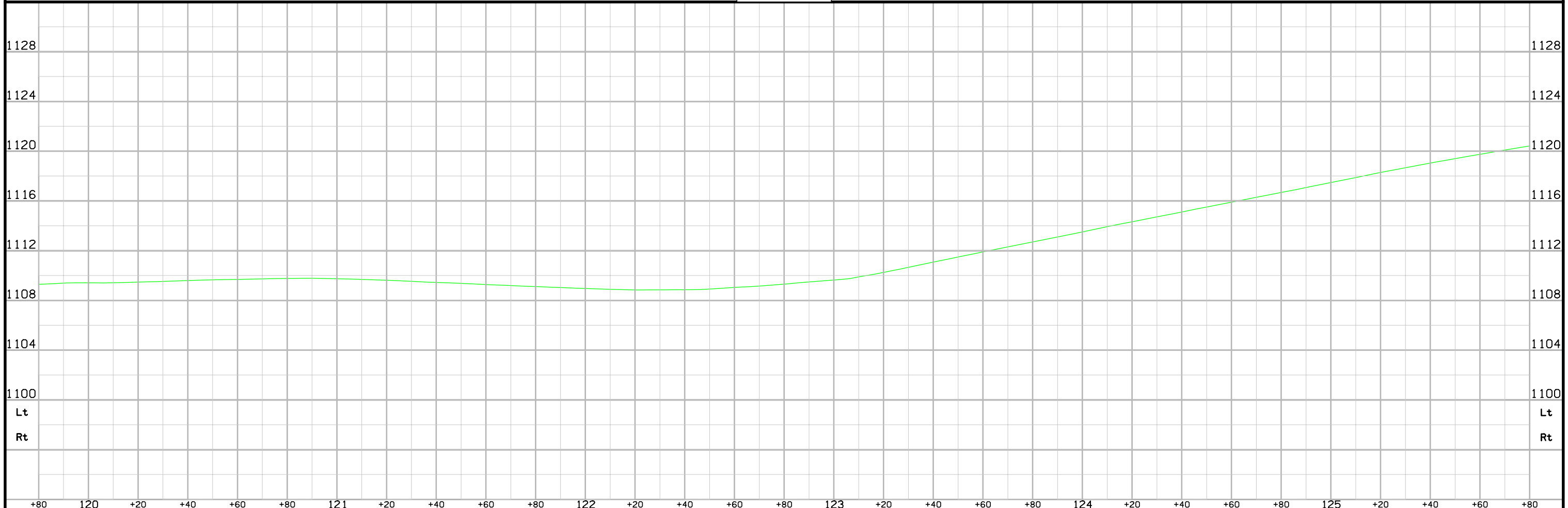
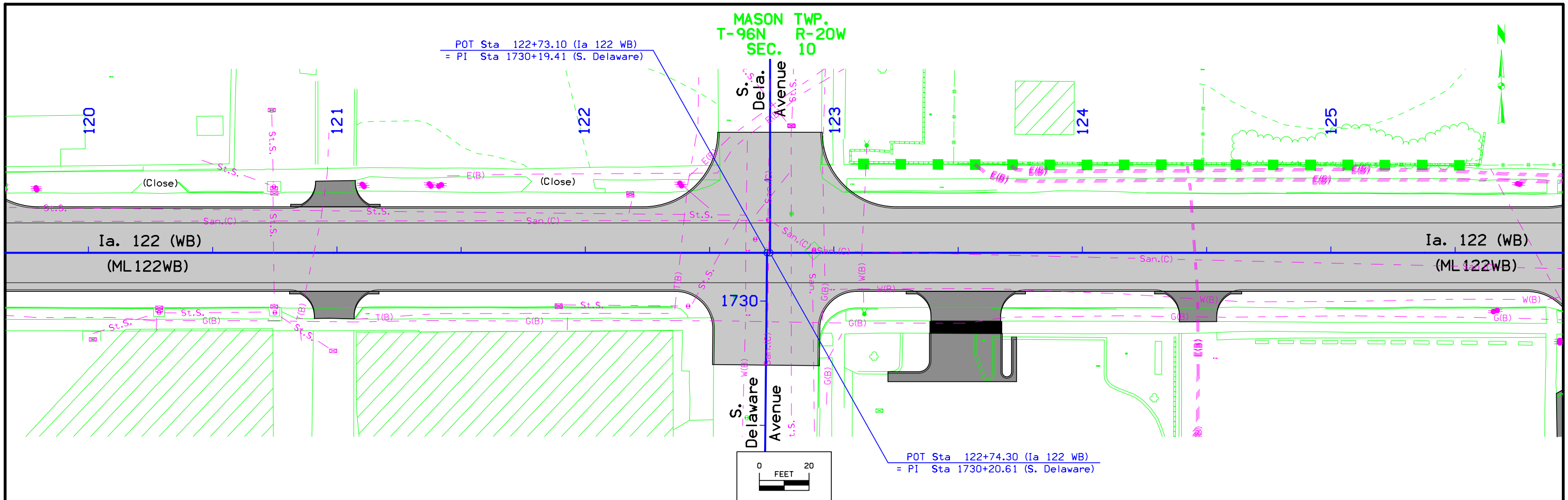
MASON TWP.  
T-96N R-20W  
SEC. 9



MASON TWP.  
T-96N R-20W  
SEC. 9





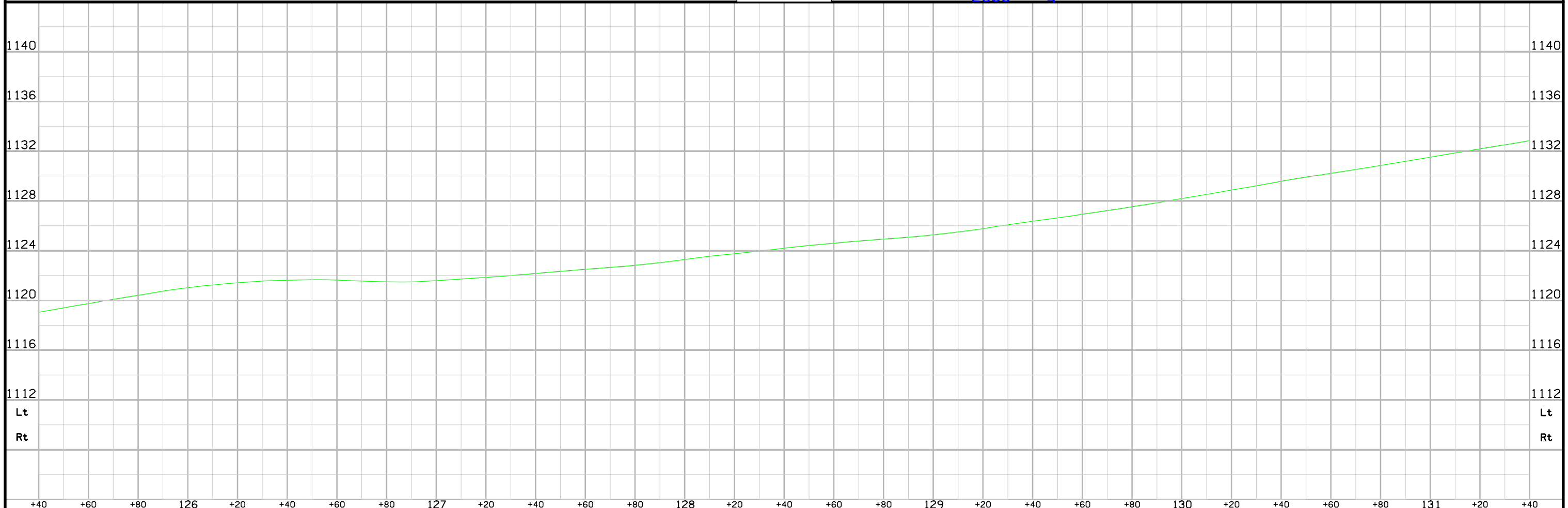
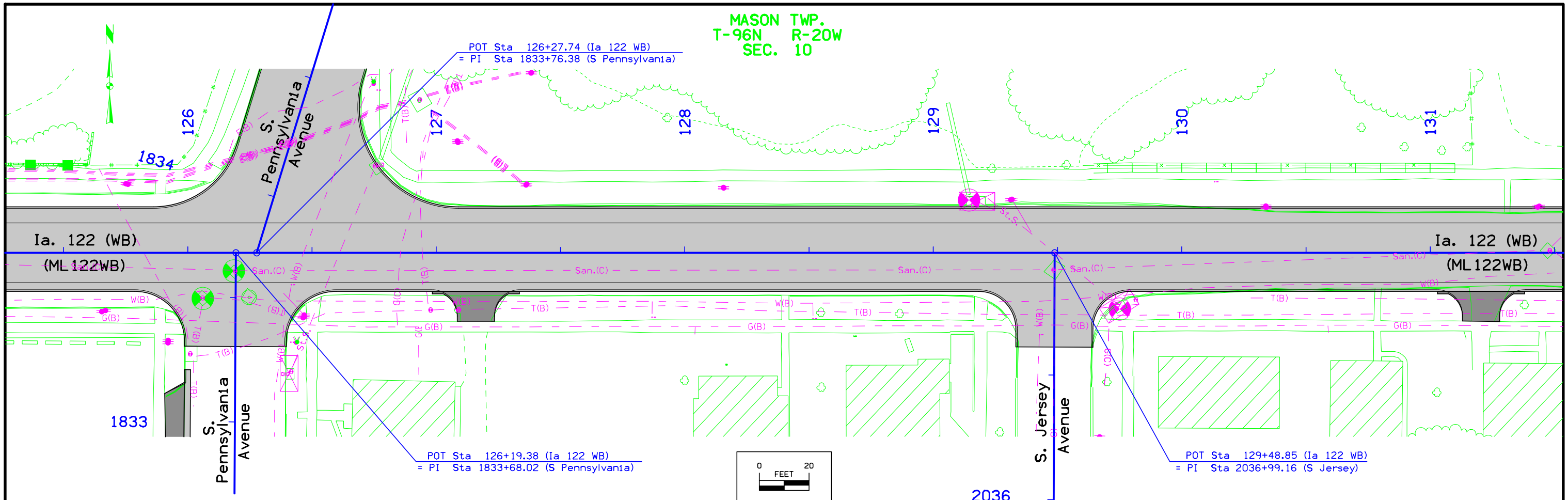
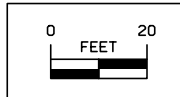


MASON TWP.  
T-96N R-20W  
SEC. 10

POT Sta 126+27.74 (Ia 122 WB)  
= PI Sta 1833+76.38 (S Pennsylvania)

POT Sta 126+19.38 (Ia 122 WB)  
= PI Sta 1833+68.02 (S Pennsylvania)

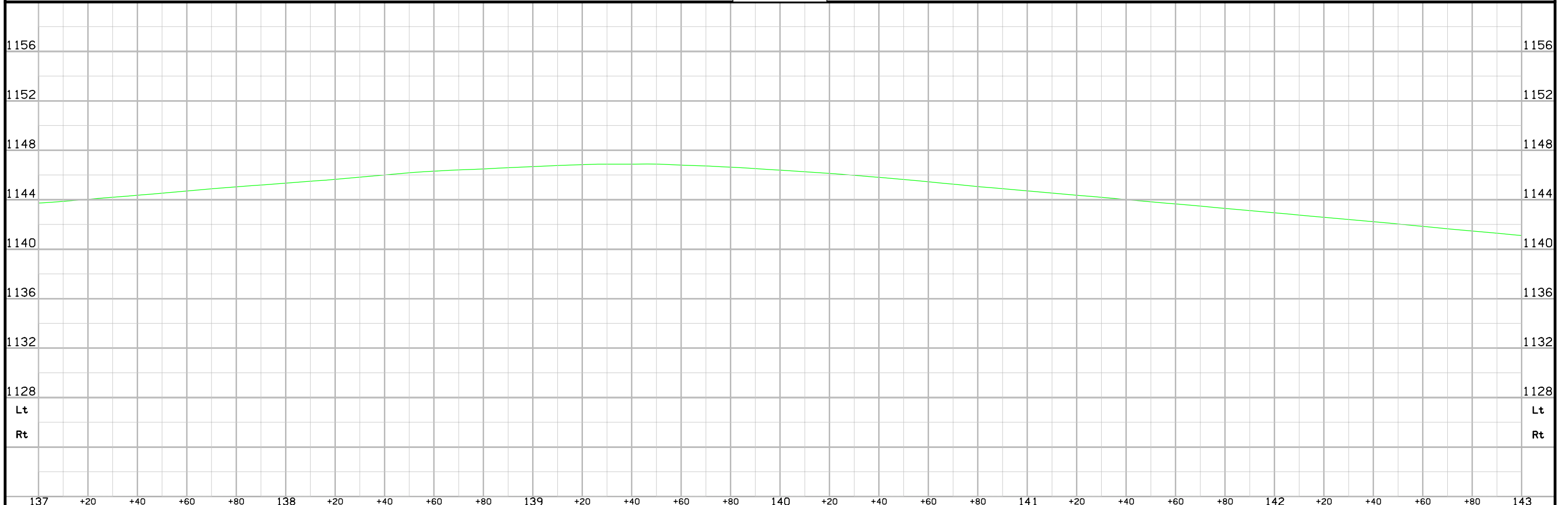
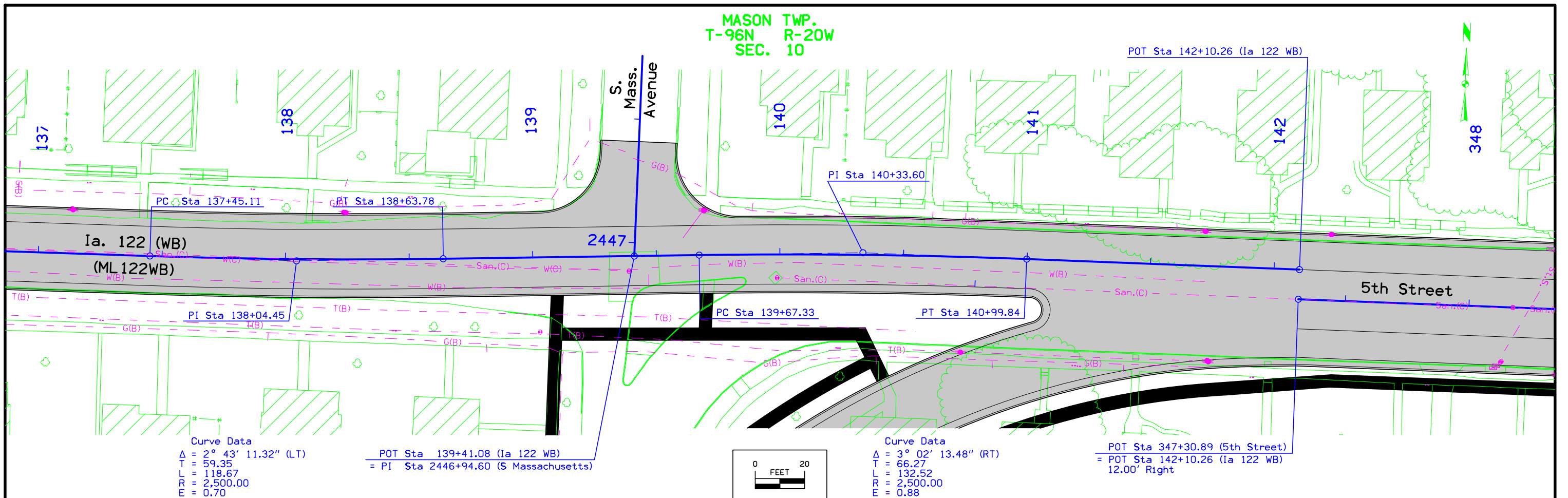
POT Sta 129+48.85 (Ia 122 WB)  
= PI Sta 2036+99.16 (S Jersey)







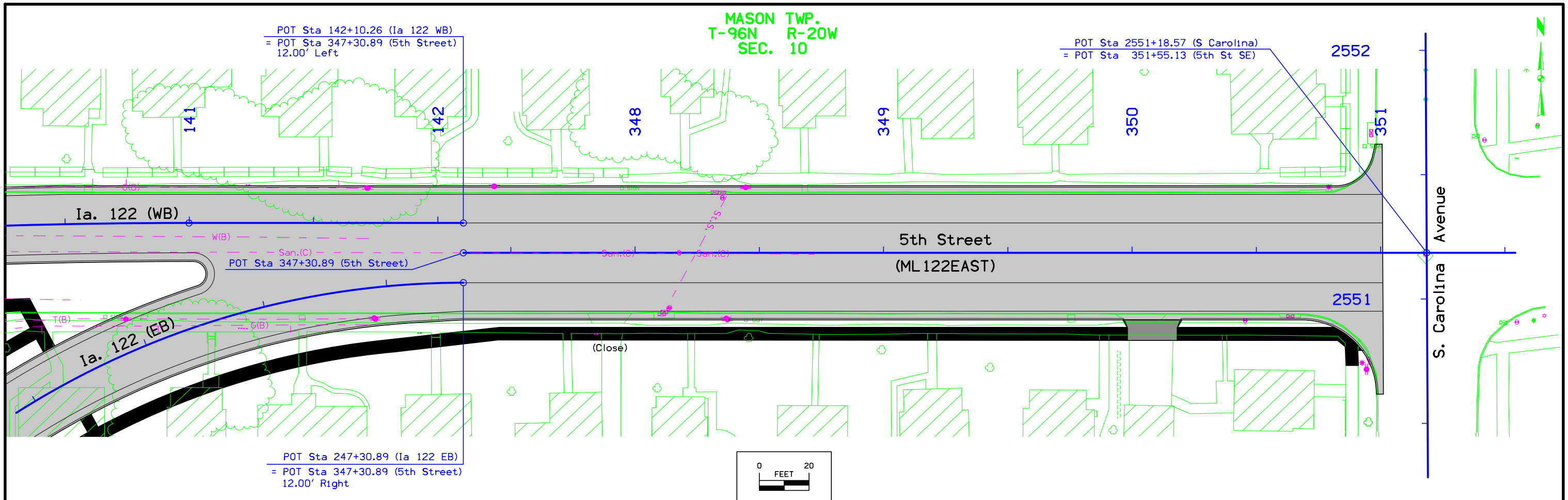
MASON TWP.  
T-96N R-20W  
SEC. 10



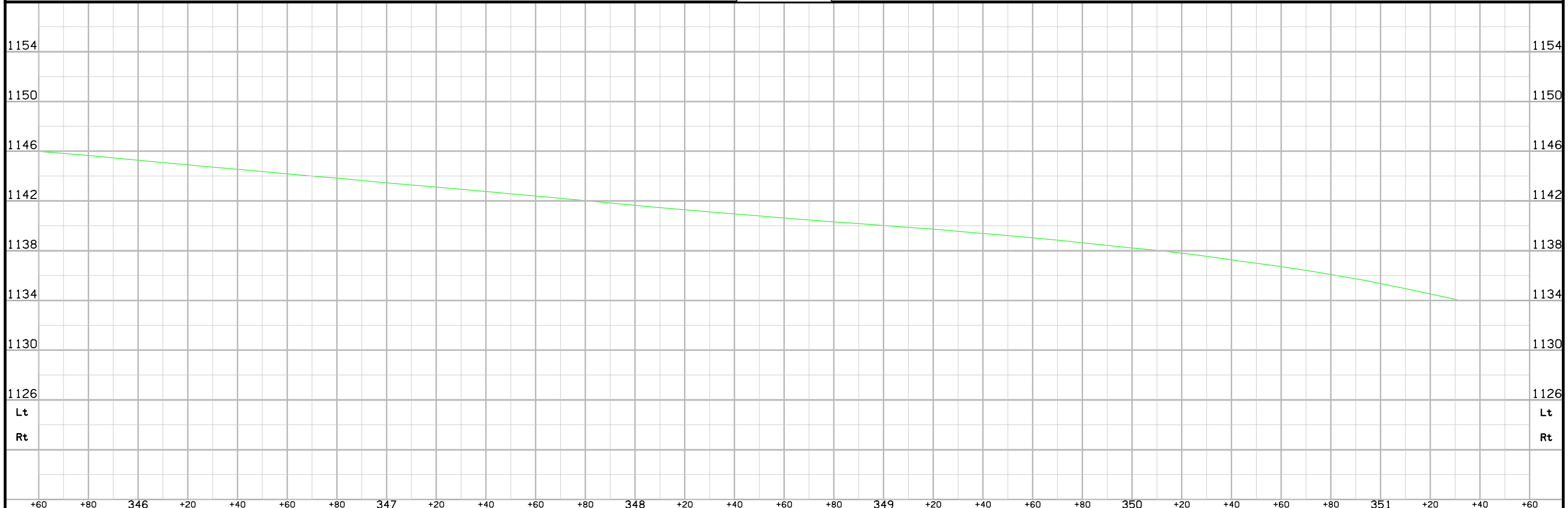
MASON TWP.  
T-96N R-20W  
SEC. 10

POT Sta 142+10.26 (Ia 122 WB)  
= POT Sta 347+30.89 (5th Street)  
12.00' Left

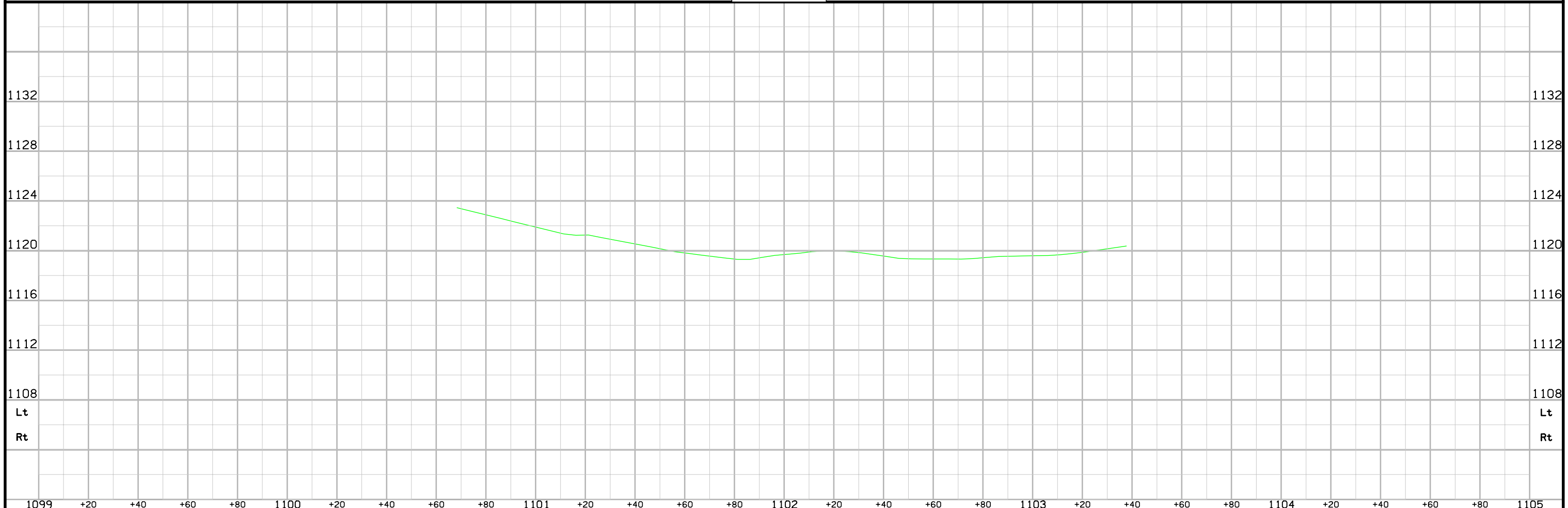
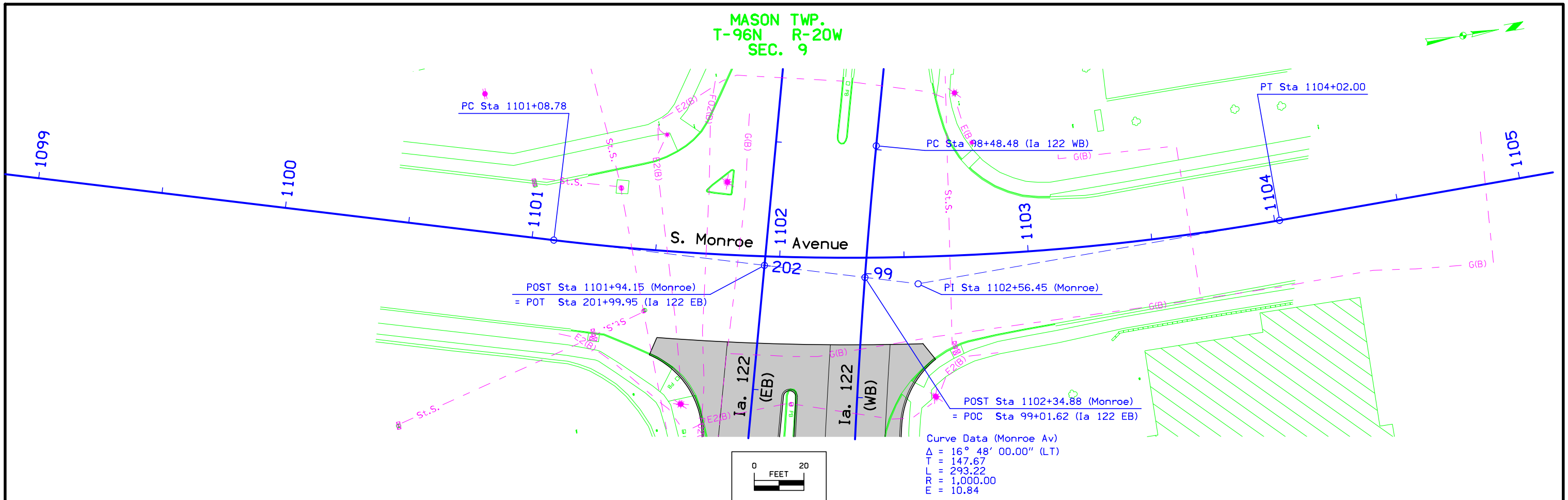
POT Sta 2551+18.57 (S Carolina)  
= POT Sta 351+55.13 (5th St SE)



POT Sta 247+30.89 (Ia 122 EB)  
= POT Sta 347+30.89 (5th Street)  
12.00' Right



MASON TWP.  
T-96N R-20W  
SEC. 9



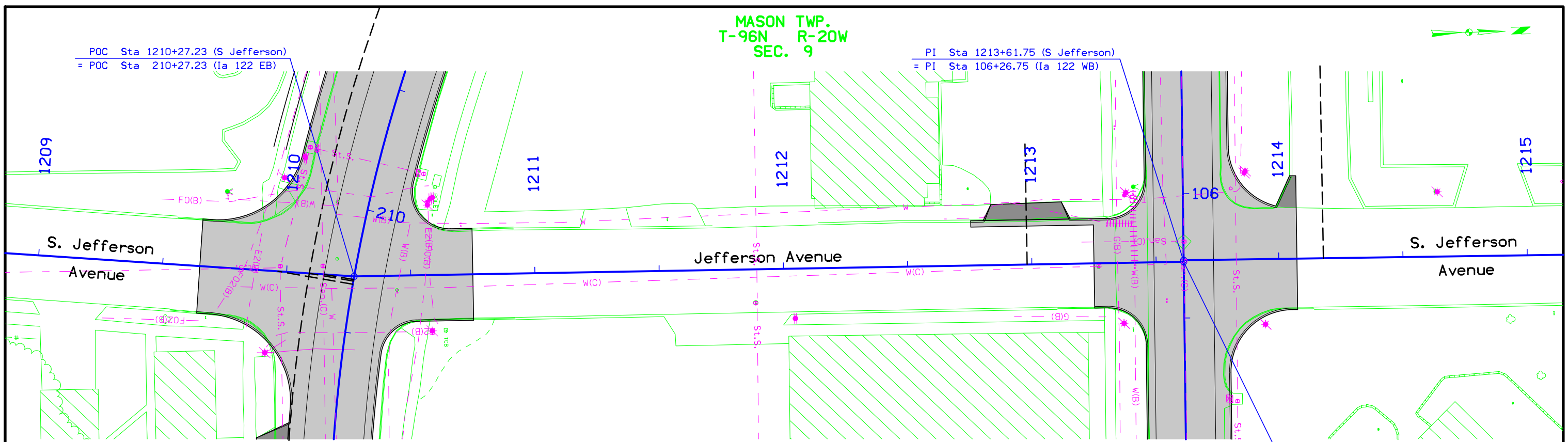
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FILE NO.	ENGLISH DESIGN TEAM JIA\MILLER										CERRO GORDO COUNTY PROJECT NUMBER NHSX-122-1(18)--3H-17										SHEET NUMBER E.1									

MASON TWP.  
T-96N R-20W  
SEC. 9

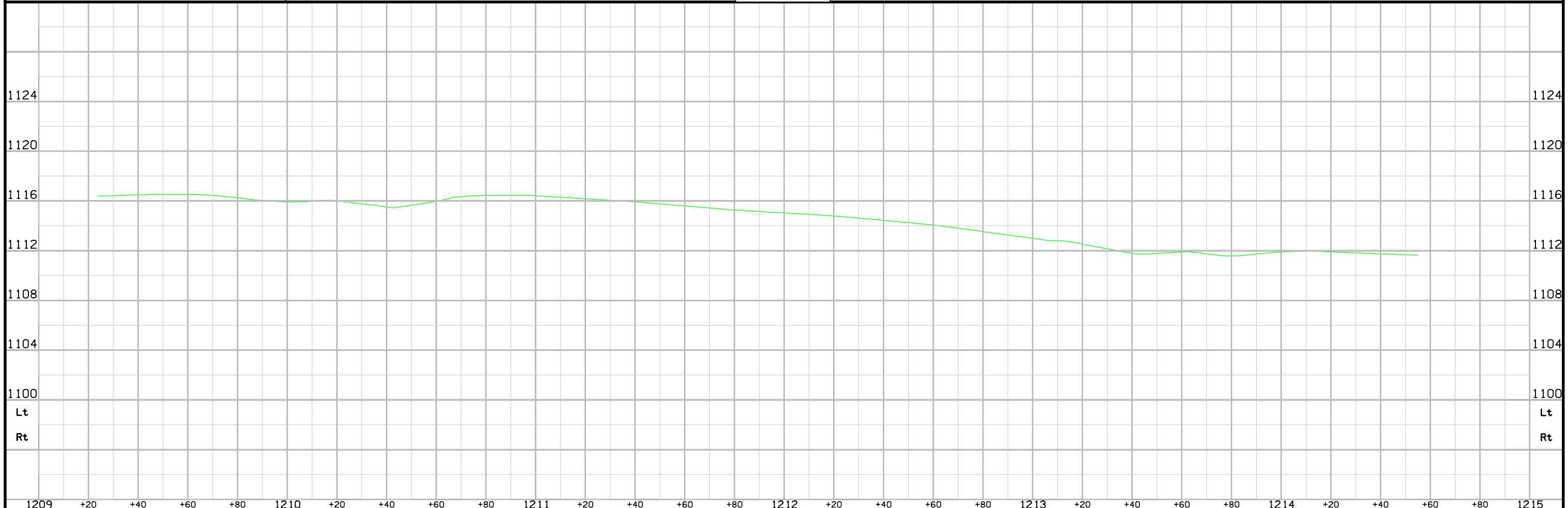
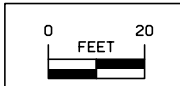


POC Sta 1210+27.23 (S Jefferson)  
= POC Sta 210+27.23 (Ia 122 EB)

PI Sta 1213+61.75 (S Jefferson)  
= PI Sta 106+26.75 (Ia 122 WB)



PI Sta 1213+61.11 (S Jefferson)  
= PI Sta 106+27.39 (Ia 122 WB)

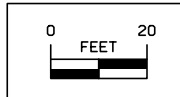
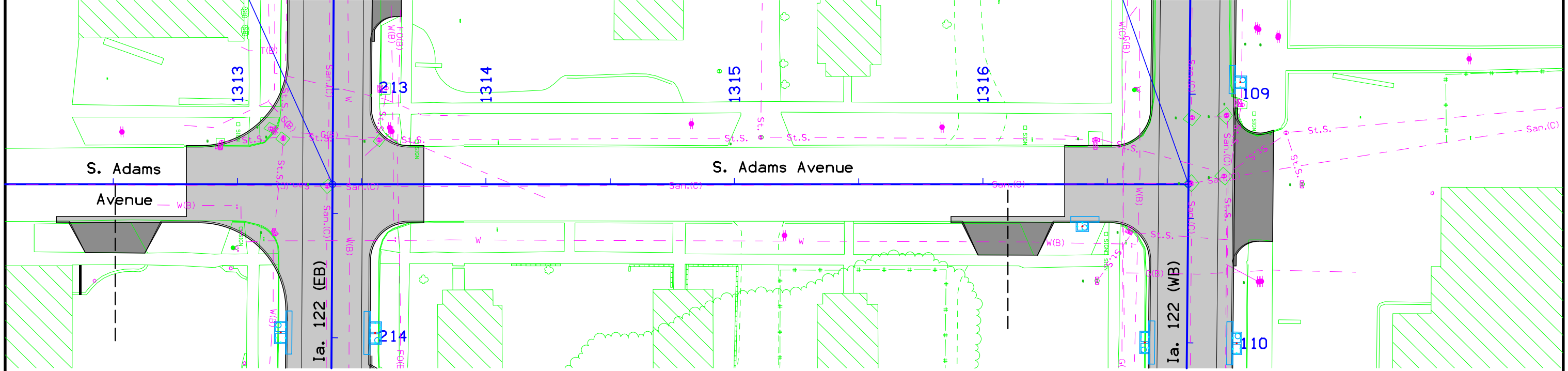


MASON TWP.  
T-96N R-20W  
SEC. 9



PI Sta 1313+38.25 (S Adams Av)  
= POT Sta 213+38.25 (Ia 122 EB)

PI Sta 1316+82.70 (S Adams Av)  
= POT Sta 109+36.20 (Ia 122 WB)

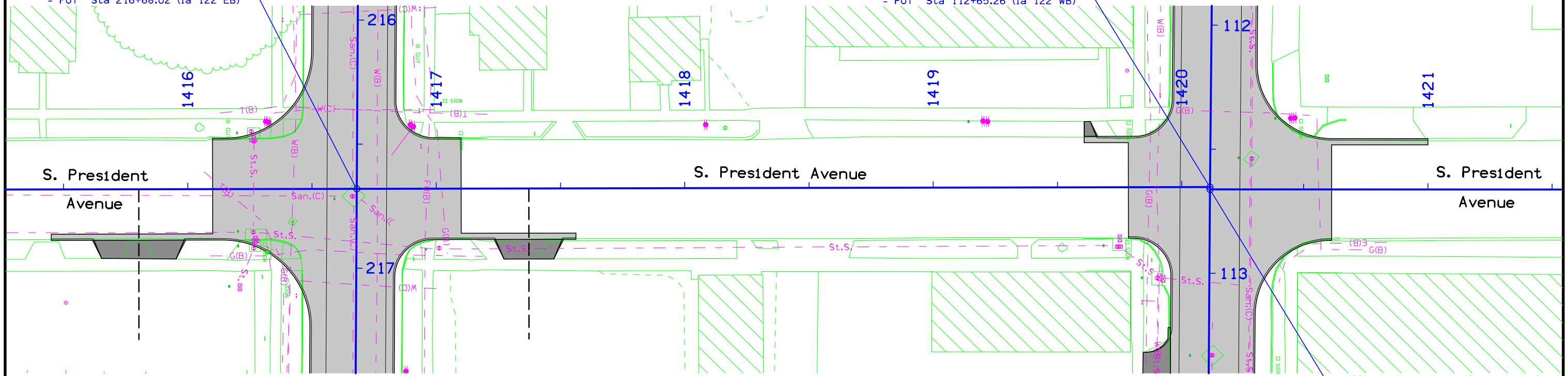


MASON TWP.  
T-96N R-20W  
SEC. 9

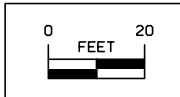


PI Sta 1416+68.02 (S President)  
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PI Sta 1420+11.35 (S President)  
= POT Sta 112+65.26 (Ia 122 WB)



PI Sta 1420+12.30 (S President)  
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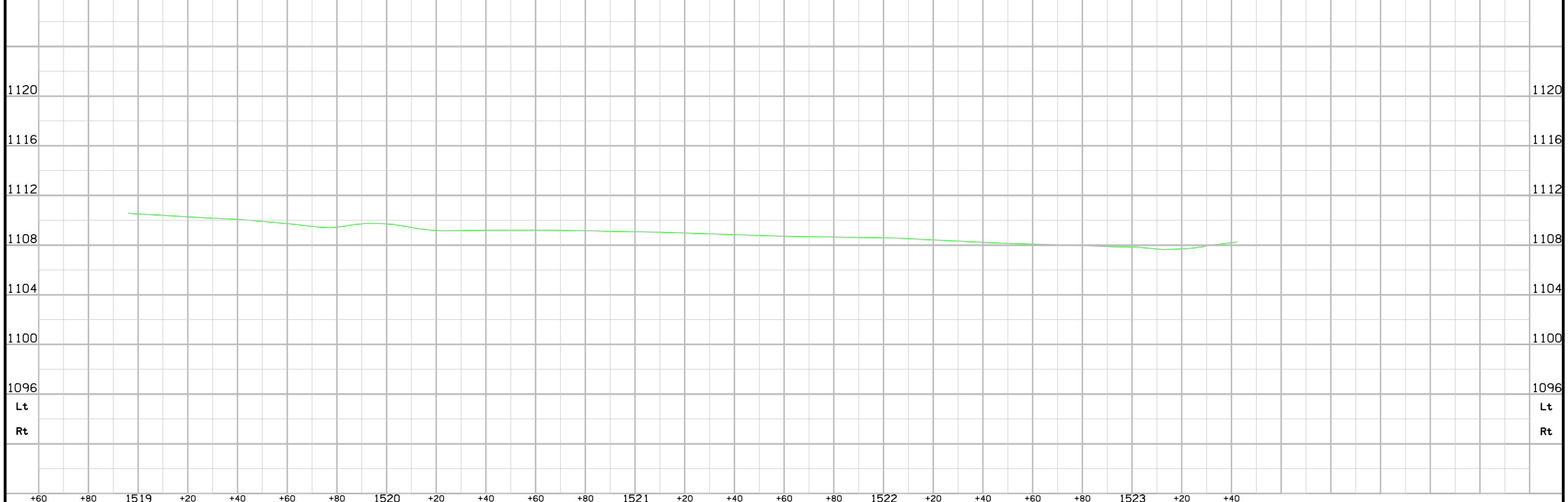
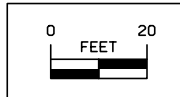
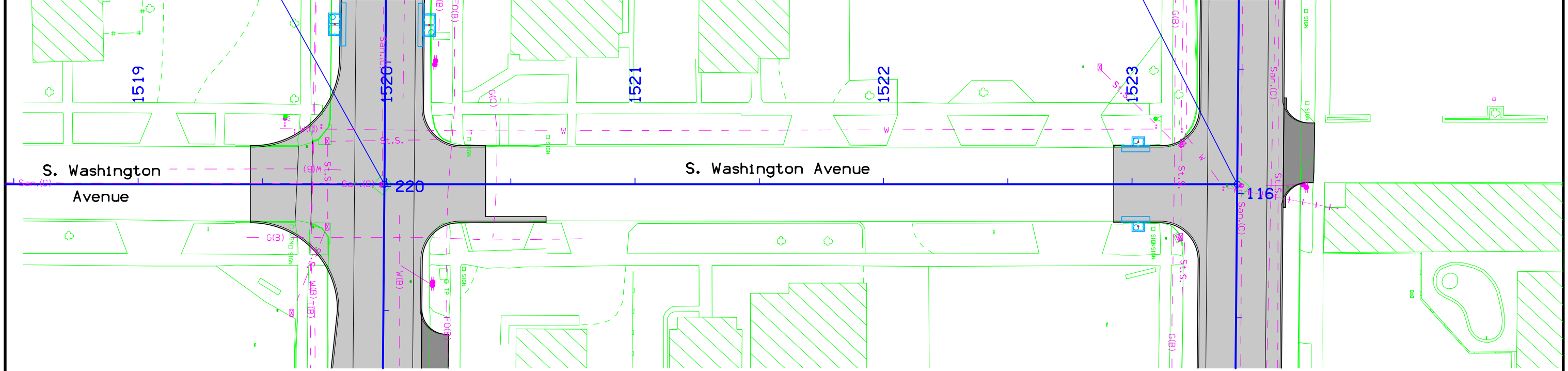


MASON TWP.  
T-96N R-20W  
SEC. 9



PI Sta 1519+99.13 (S Washington)  
= POT Sta 219+99.13 (Ia 122 EB)

POT Sta 1523+42.32 (S Washington)  
= POT Sta 115+96.28 (Ia 122 WB)



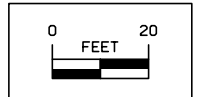
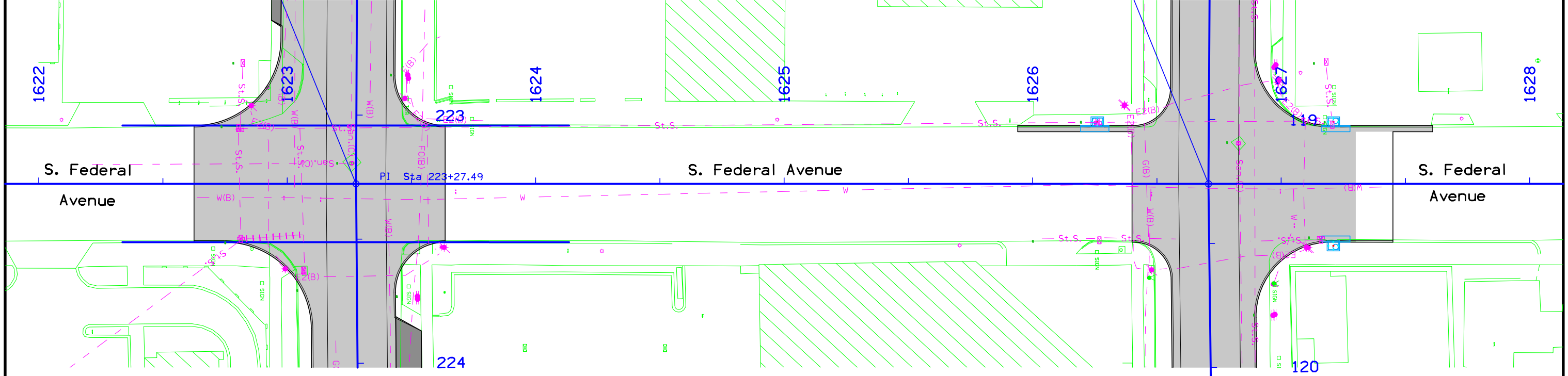


MASON TWP.  
T-96N R-20W  
SEC. 9

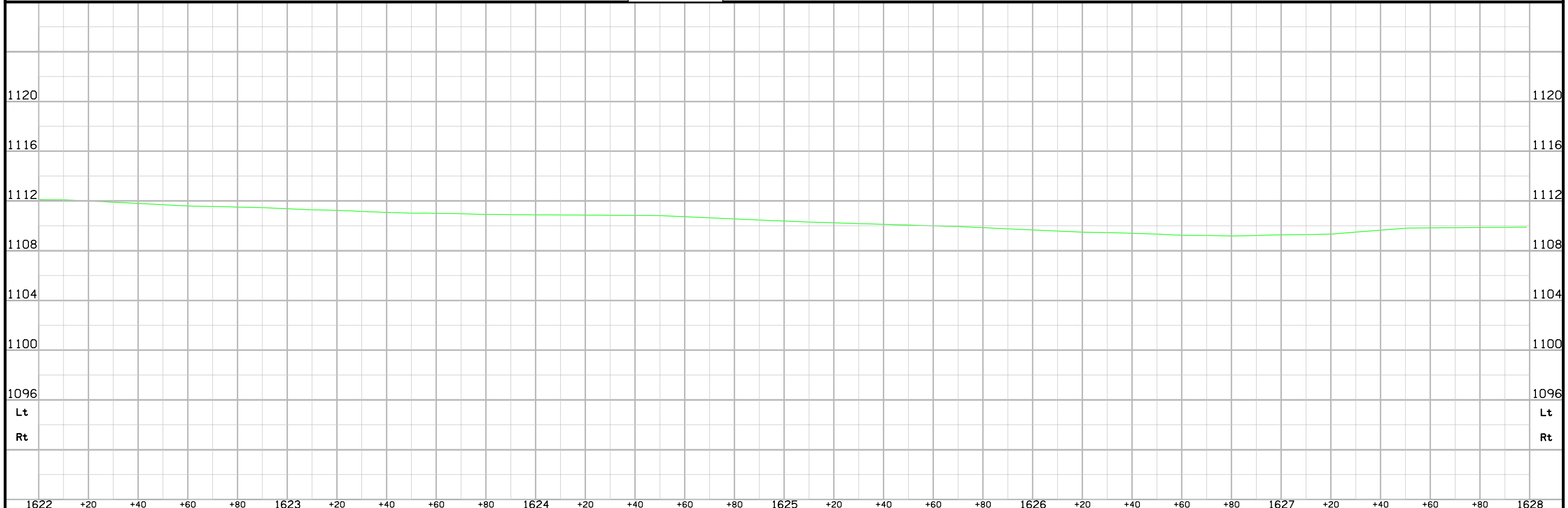


POT Sta 1623+27.71 (S Federal)  
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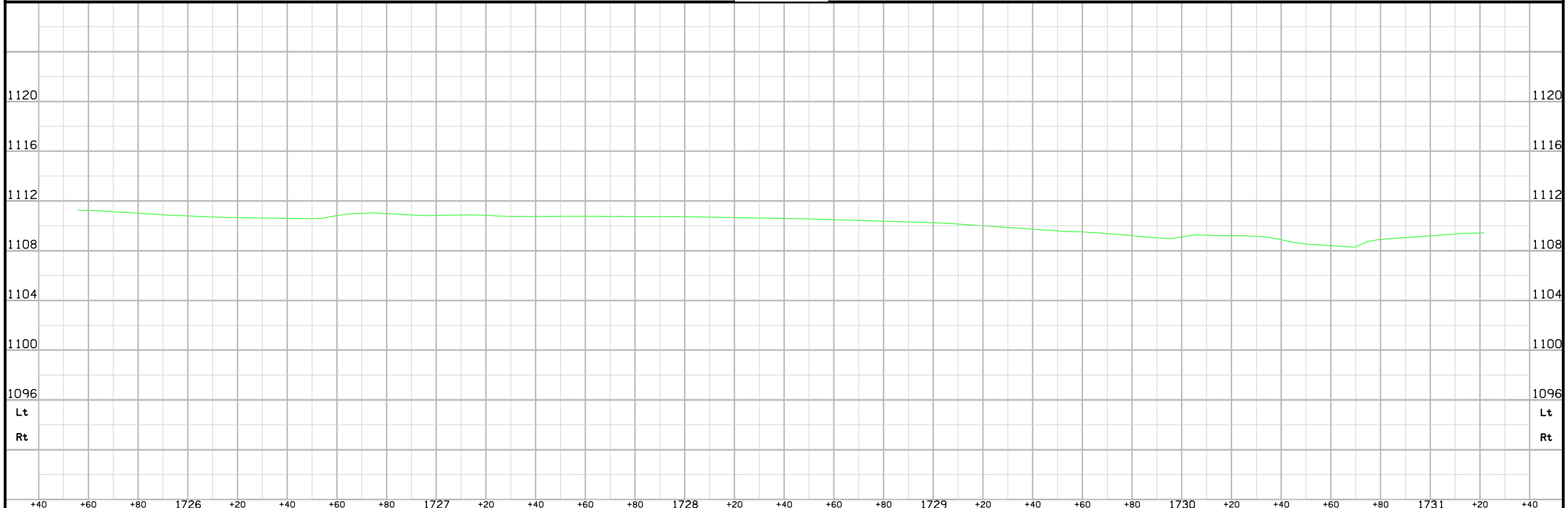
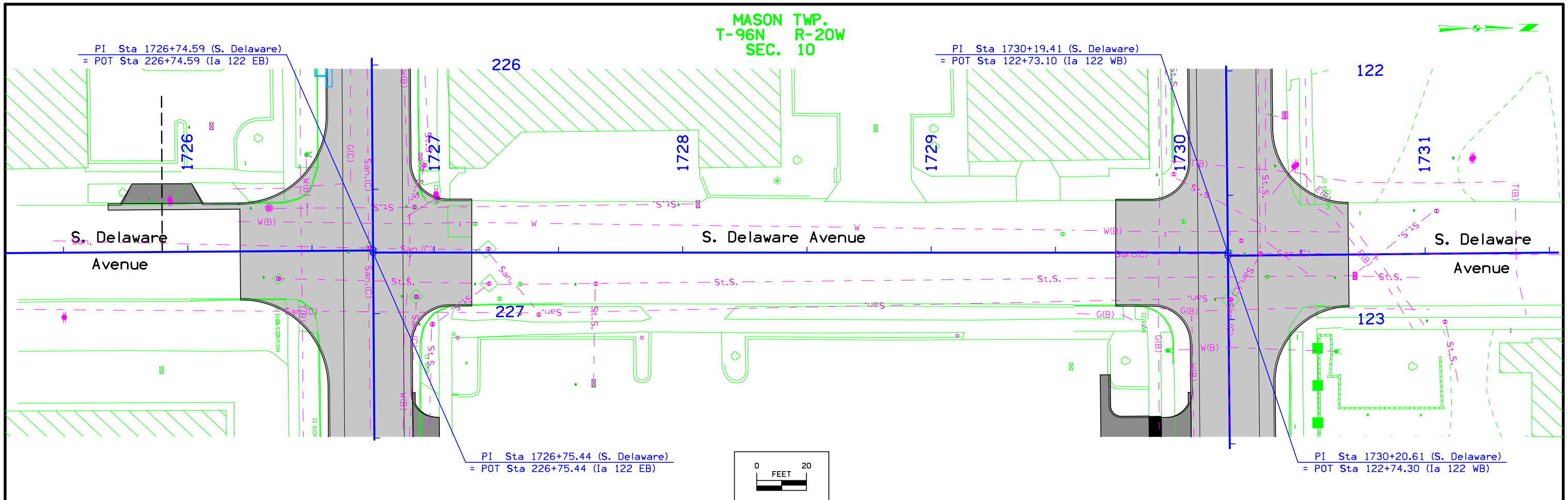
POT Sta 1626+70.76 (S Federal)  
= PI Sta 119+25.95 (Ia 122 WB)



SEC. 10

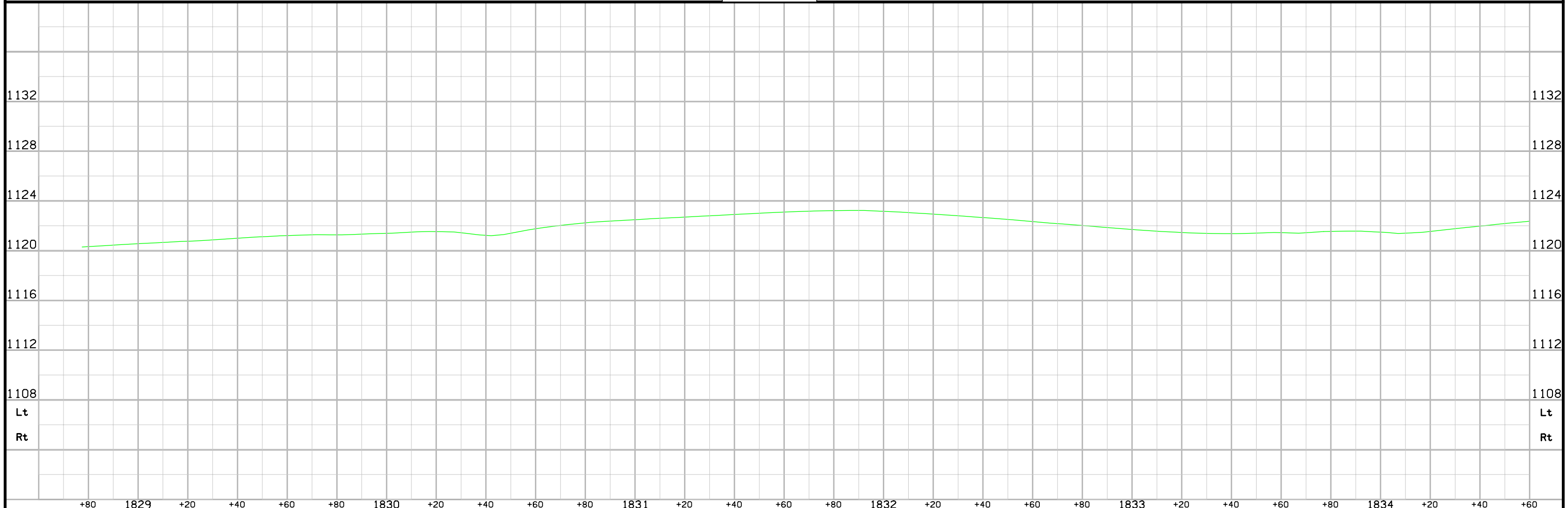
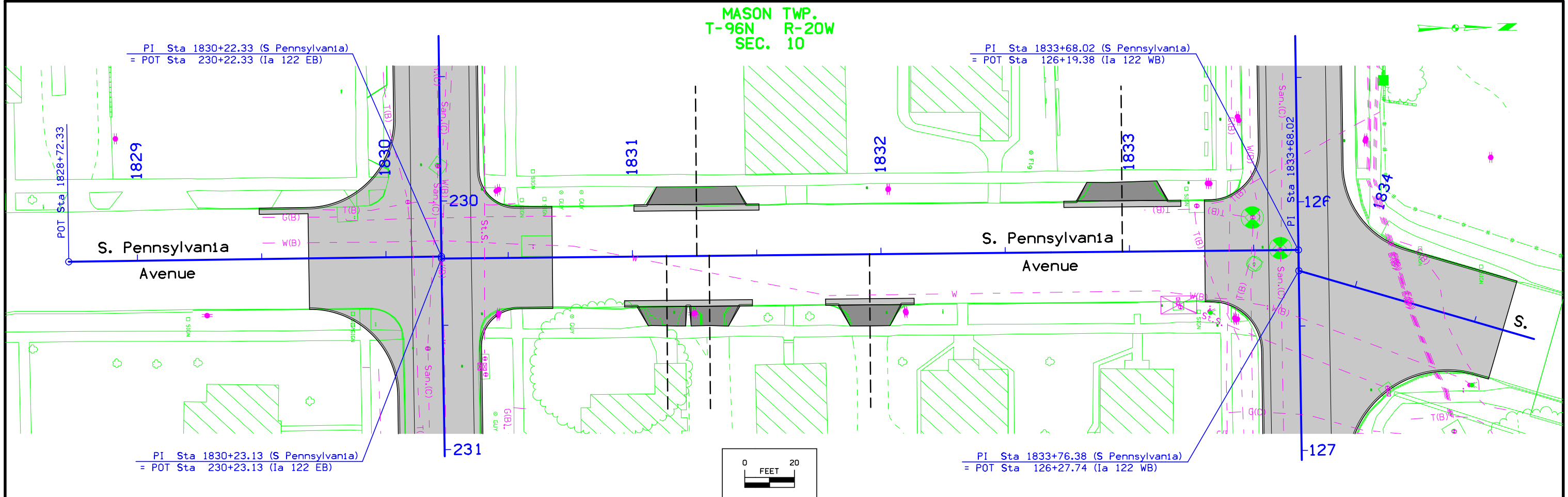


1622	+20	+40	+60	+80	1623	+20	+40	+60	+80	1624	+20	+40	+60	+80	1625	+20	+40	+60	+80	1626	+20	+40	+60	+80	1627	+20	+40	+60	+80	1628
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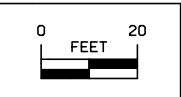
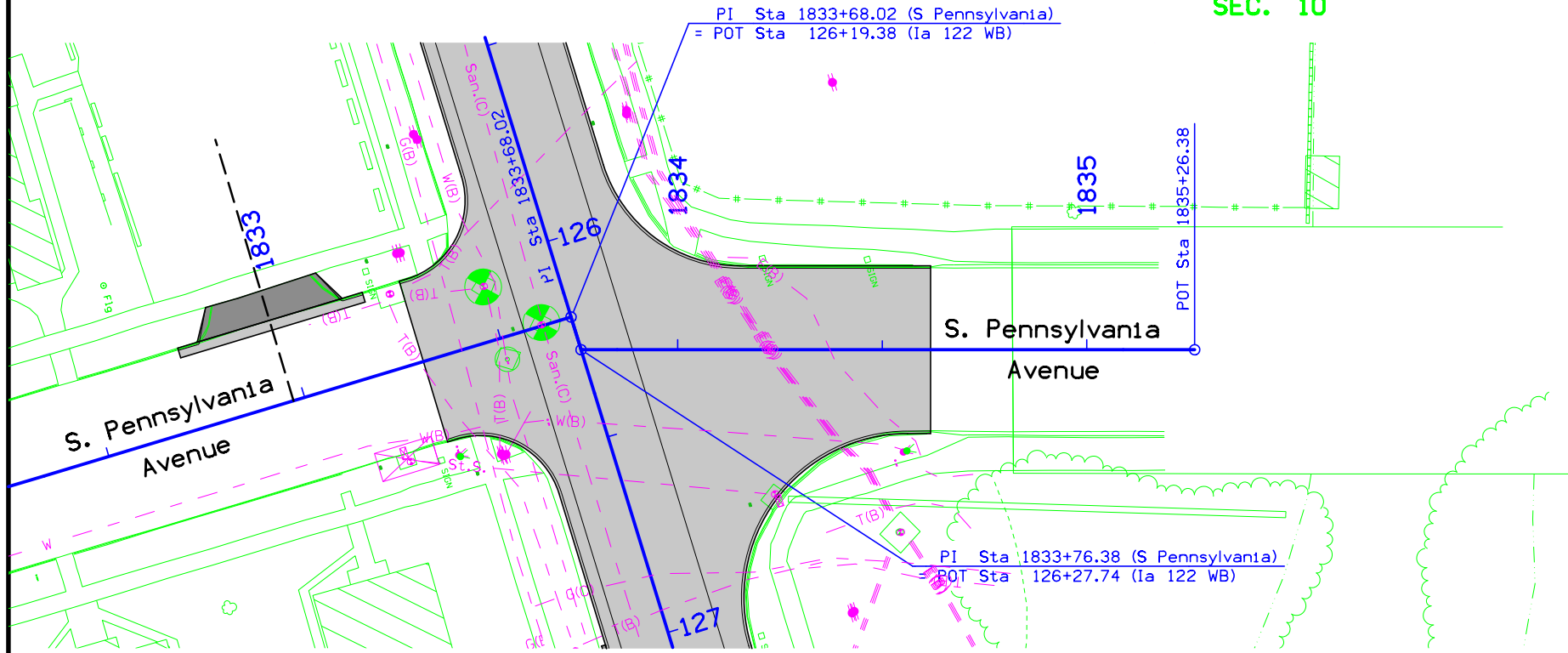
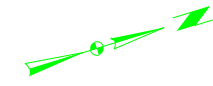


FILE NO.	ENGLISH	DESIGN TEAM	JIA\MILLER	CERRO GORDO COUNTY	PROJECT NUMBER	NHSX-122-1(18)--3H-17	SHEET NUMBER	E.7
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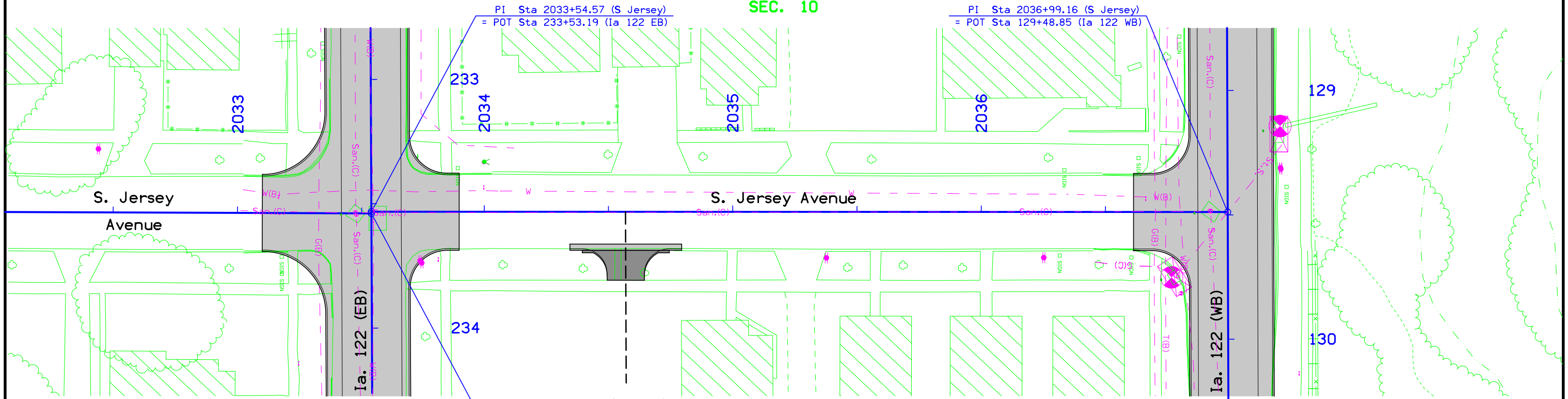
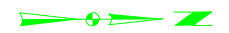
MASON TWP.  
T-96N R-20W  
SEC. 10



MASON TWP.  
T-96N R-20W  
SEC. 10



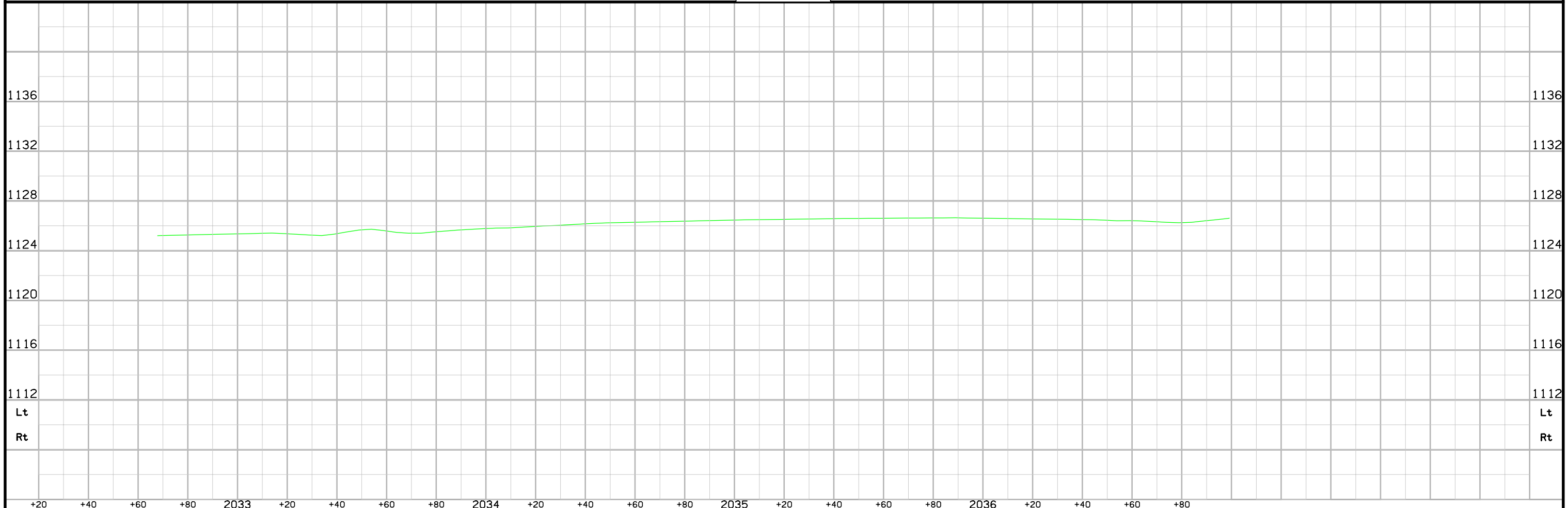
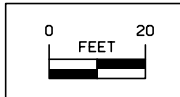
MASON TWP.  
T-96N R-20W  
SEC. 10



PI Sta 2033+54.57 (S Jersey)  
= POT Sta 233+53.19 (Ia 122 EB)

PI Sta 2036+99.16 (S Jersey)  
= POT Sta 129+48.85 (Ia 122 WB)

PI Sta 2033+53.88 (S Jersey)  
= POT Sta 233+53.88 (Ia 122 EB)

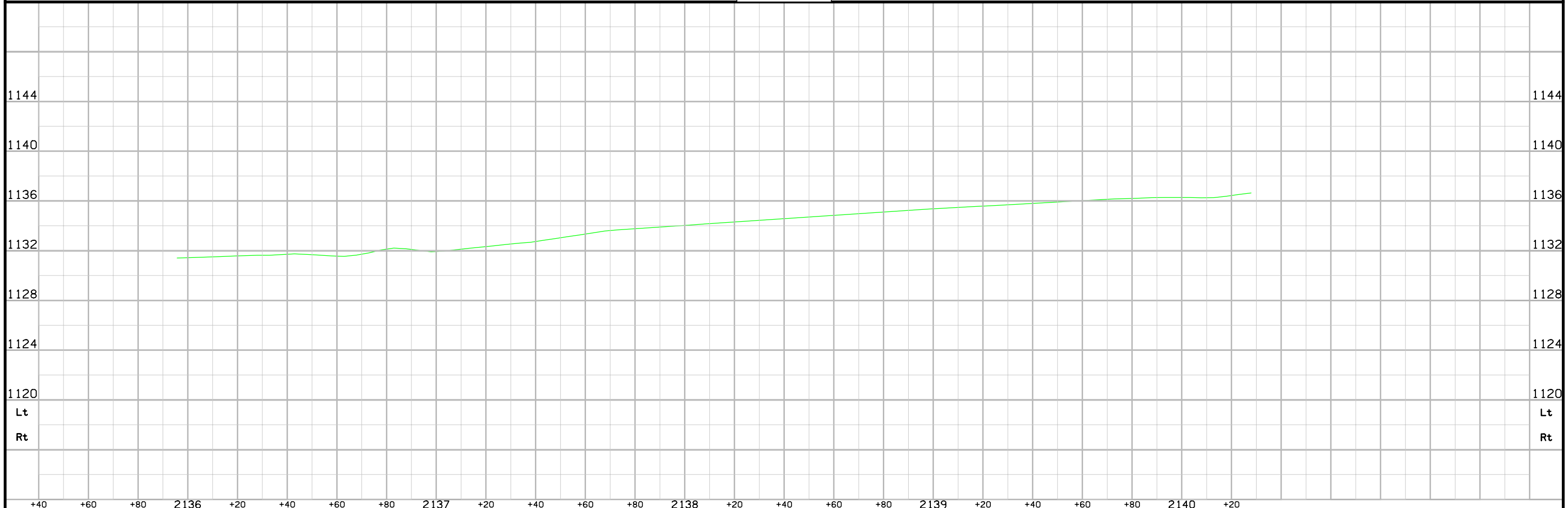
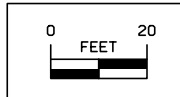
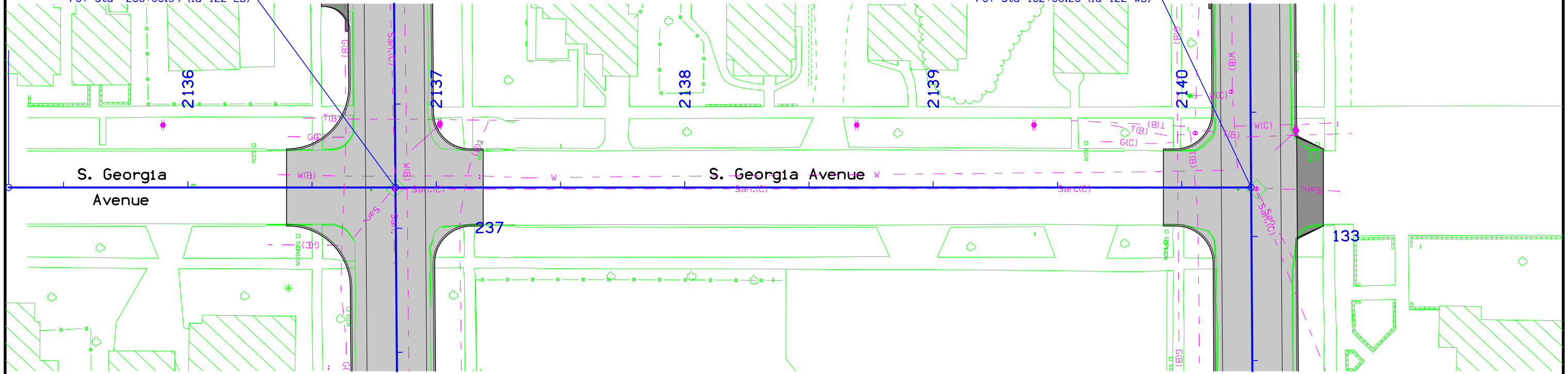


MASON TWP.  
T-96N R-20W  
SEC. 10



POT Sta 2136+83.59 (S Georgia)  
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POT Sta 2140+27.90 (S Georgia)  
= POT Sta 132+80.28 (Ia 122 WB)

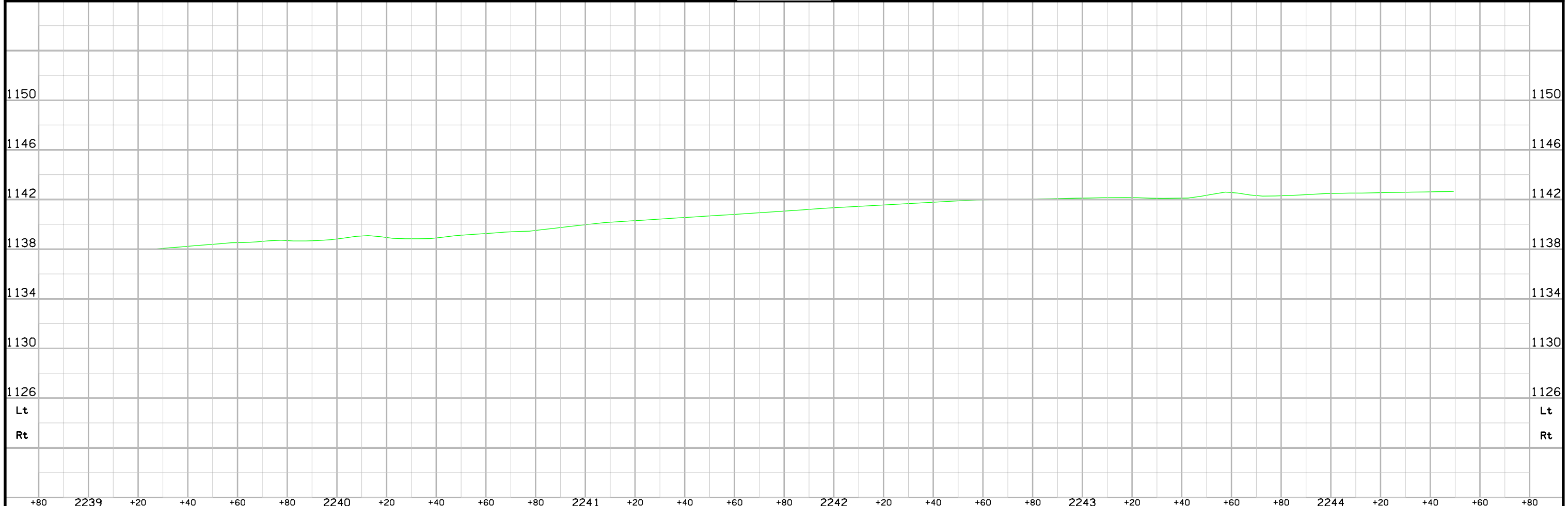
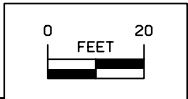
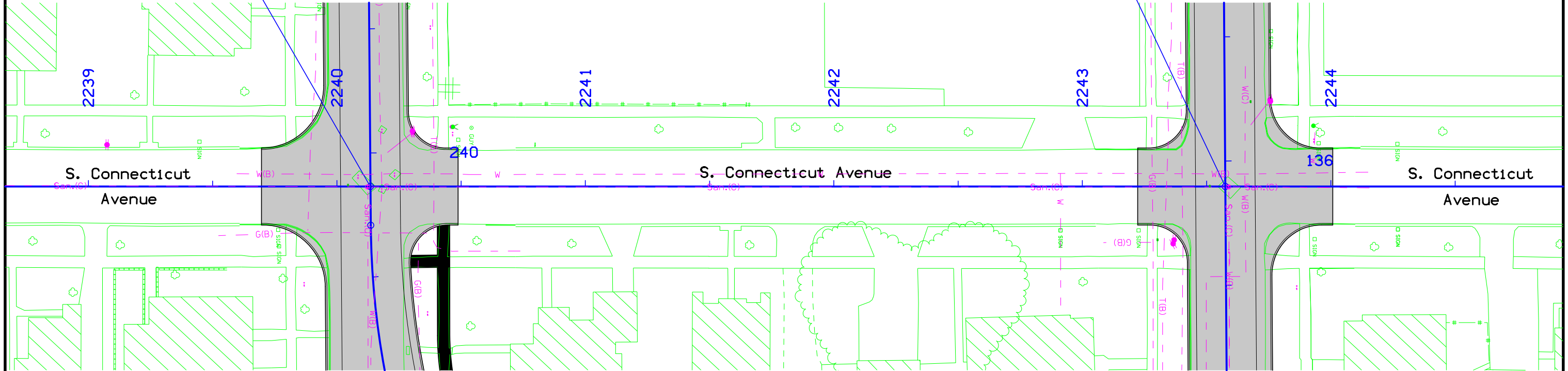


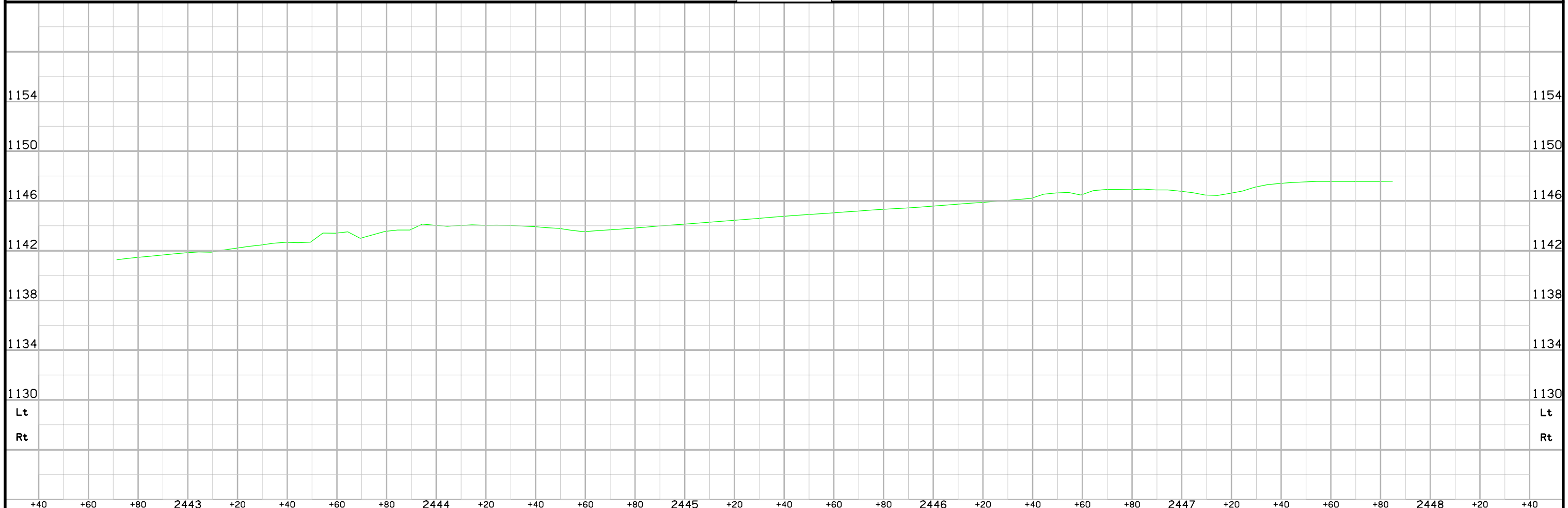
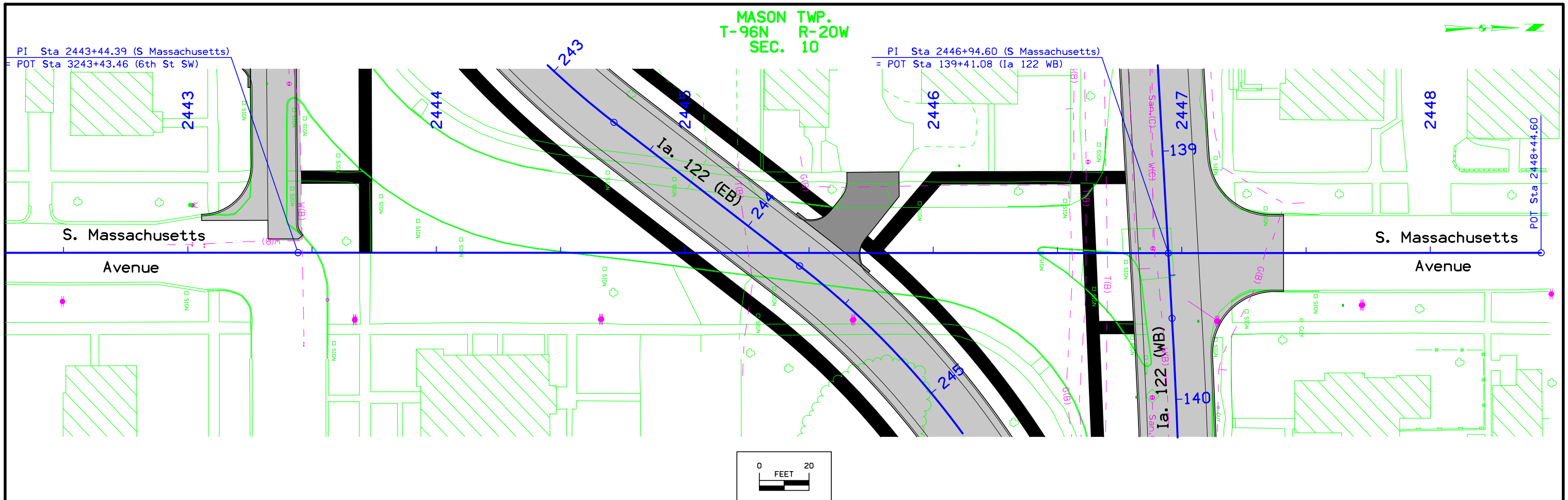
MASON TWP.  
T-96N R-20W  
SEC. 10



POT Sta 2240+13.52 (S Connecticut)  
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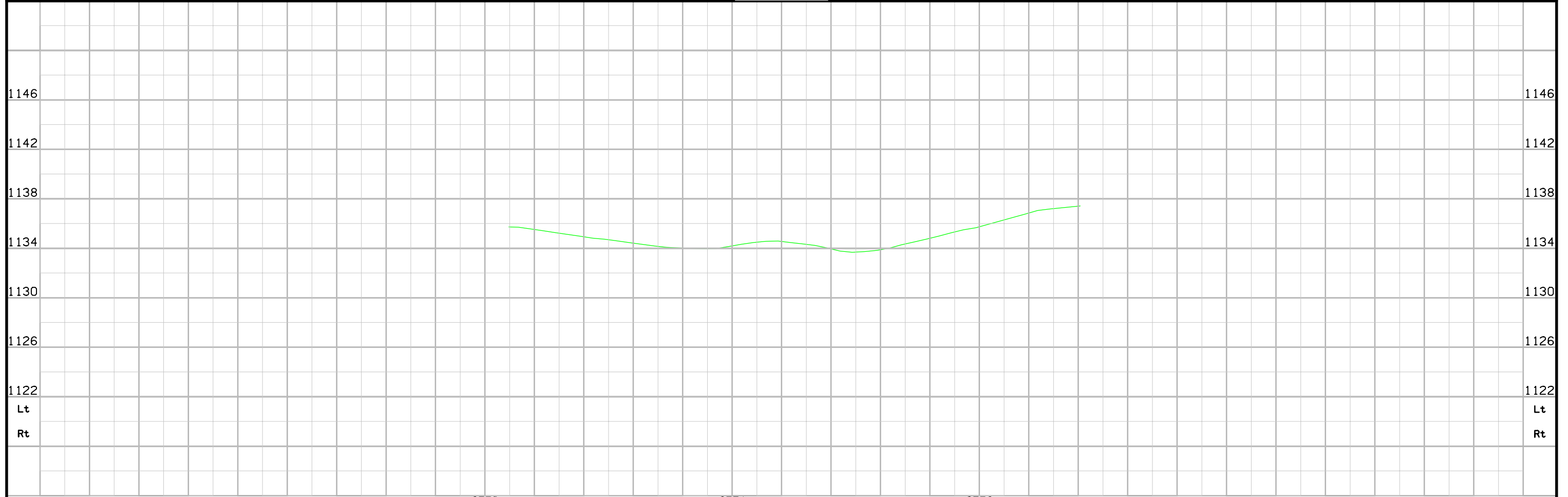
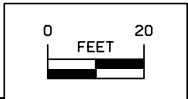
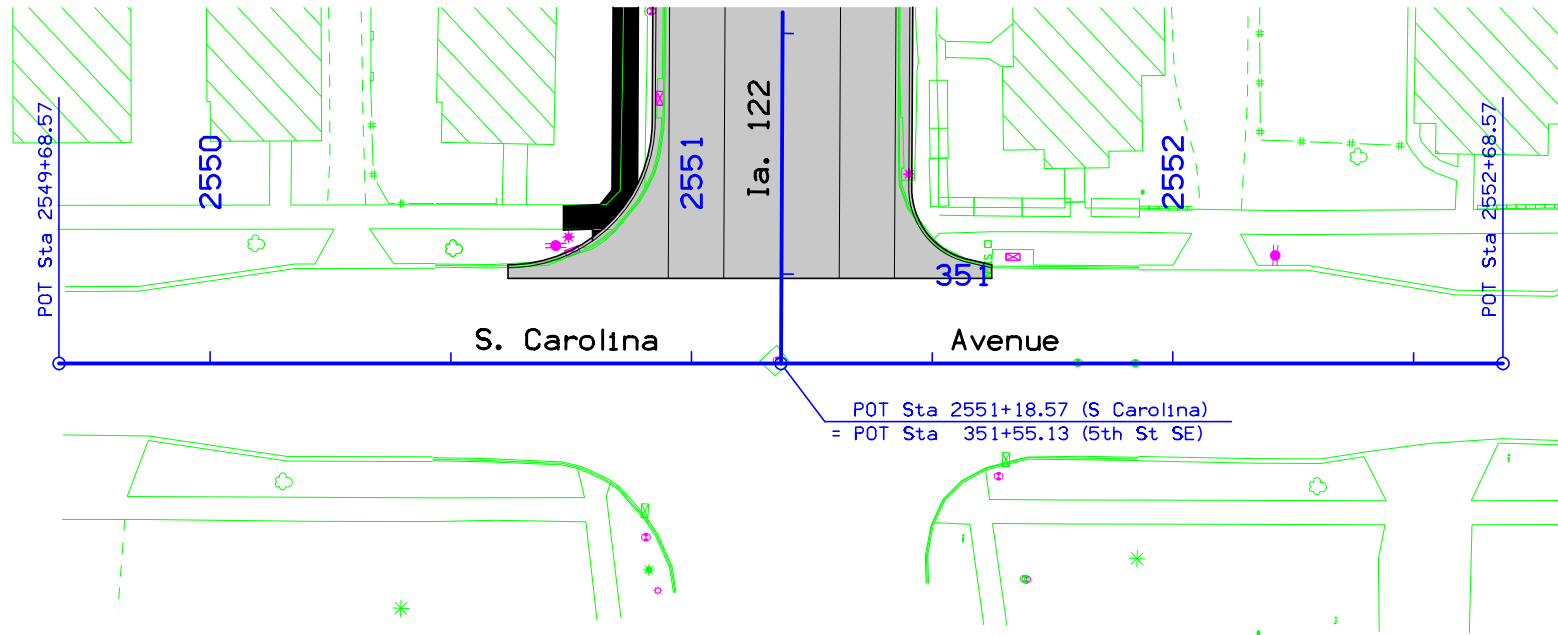
POT Sta 2243+57.53 (S Connecticut)  
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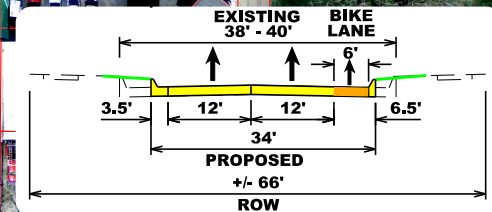
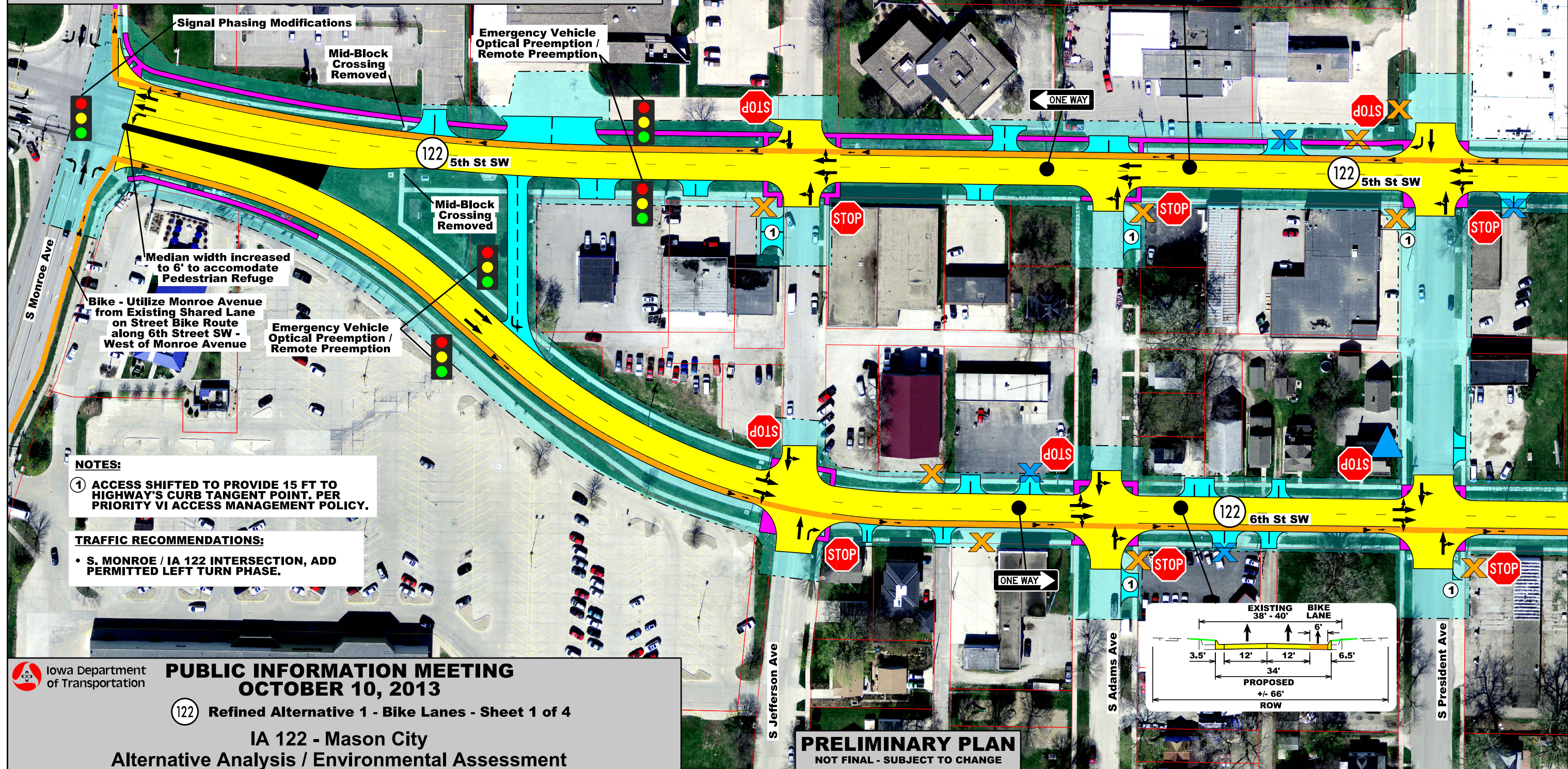
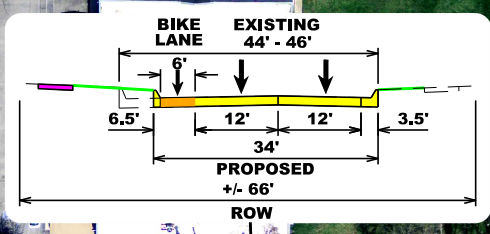
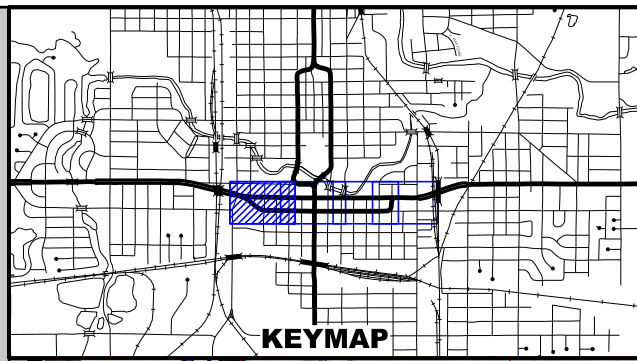


MASON TWP.  
T-96N R-20W  
SEC. 10



# LEGEND

- PROPOSED 122 PAVEMENT
- PROPOSED CONCRETE MEDIAN
- PROPOSED DRIVEWAY
- PROPOSED SIDEWALK
- PROPOSED RETAINING WALL
- NEPA CLEARED AREA
- PROPERTY LINE
- X PROPOSED ACCESS CLOSURE
- X PROPOSED FUTURE ACCESS CLOSURE
- X PROPOSED ROAD CLOSURE
- BUS STOP
- STOP STOP SIGN LOCATION
- TRAFFIC SIGNAL LOCATION
- POTENTIAL IMPACT
- BIKE LANE / BIKE ROUTE



**NOTES:**

① ACCESS SHIFTED TO PROVIDE 15 FT TO HIGHWAY'S CURB TANGENT POINT, PER PRIORITY VI ACCESS MANAGEMENT POLICY.

**TRAFFIC RECOMMENDATIONS:**

- S. MONROE / IA 122 INTERSECTION, ADD PERMITTED LEFT TURN PHASE.

**PRELIMINARY PLAN**  
NOT FINAL - SUBJECT TO CHANGE

- Sources:**
1. Aerial Photography - Cerro Gordo County, Iowa, 2012
  2. Concept - HDR Engineering, December 2012
  3. Property Parcel Information- Cerro Gordo County Assessors Office, 2011

Iowa Department of Transportation

**PUBLIC INFORMATION MEETING**  
**OCTOBER 10, 2013**

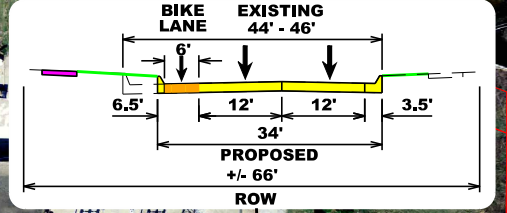
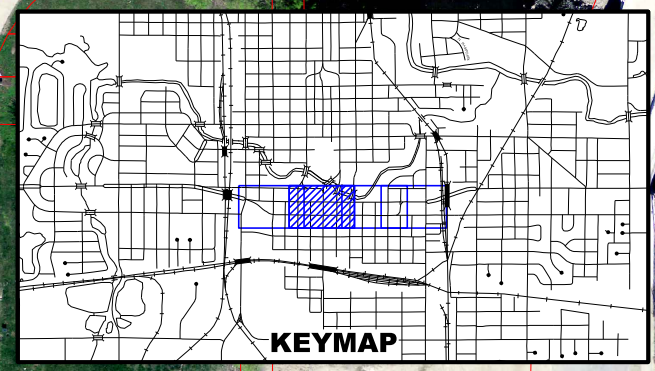
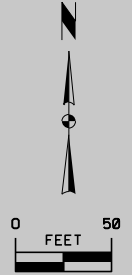
122 Refined Alternative 1 - Bike Lanes - Sheet 1 of 4

IA 122 - Mason City  
Alternative Analysis / Environmental Assessment

PROJECT NUMBER: STP-122-1(14)--2C-17  
CERRO GORDO, COUNTY

# LEGEND

- PROPOSED 122 PAVEMENT
- PROPOSED CONCRETE MEDIAN
- PROPOSED DRIVEWAY
- PROPOSED SIDEWALK
- PROPOSED RETAINING WALL
- NEPA CLEARED AREA
- PROPERTY LINE
- X PROPOSED ACCESS CLOSURE
- X PROPOSED FUTURE ACCESS CLOSURE
- X PROPOSED ROAD CLOSURE
- BUS STOP
- STOP STOP SIGN LOCATION
- TRAFFIC SIGNAL LOCATION
- POTENTIAL IMPACT
- BIKE LANE / BIKE ROUTE

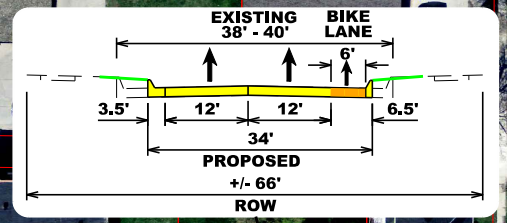
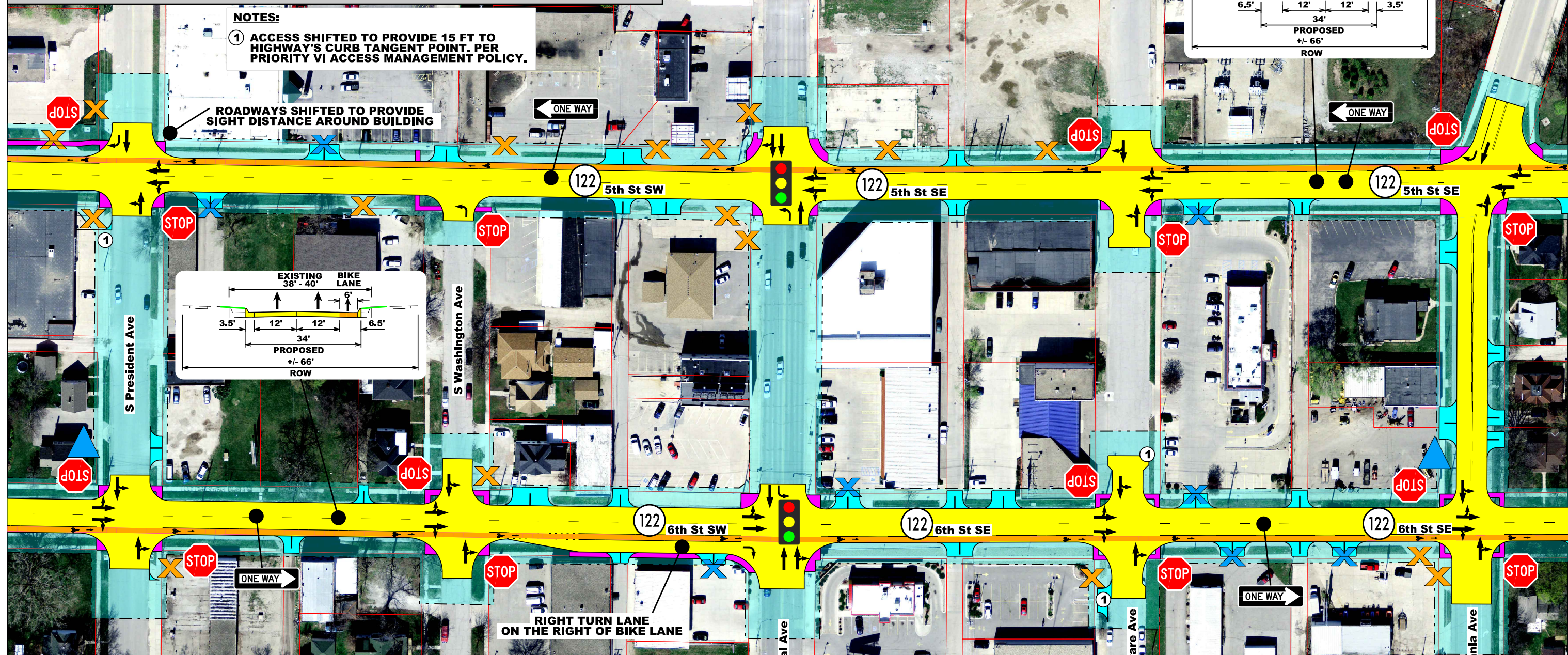


**NOTES:**  
 ① ACCESS SHIFTED TO PROVIDE 15 FT TO HIGHWAY'S CURB TANGENT POINT, PER PRIORITY VI ACCESS MANAGEMENT POLICY.

ROADWAYS SHIFTED TO PROVIDE SIGHT DISTANCE AROUND BUILDING

ONE WAY

ONE WAY



RIGHT TURN LANE ON THE RIGHT OF BIKE LANE


















**PUBLIC INFORMATION MEETING**  
**OCTOBER 10, 2013**  
 122 Refined Alternative 1 - Bike Lanes - Sheet 2 of 4  
 IA 122 - Mason City  
 Alternative Analysis / Environmental Assessment  
 PROJECT NUMBER: STP-122-1(14)--2C-17  
 CERRO GORDO, COUNTY

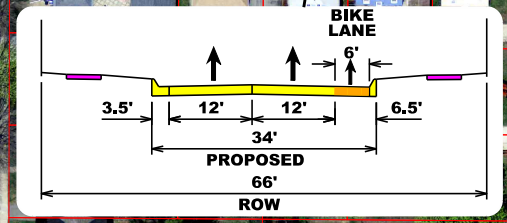
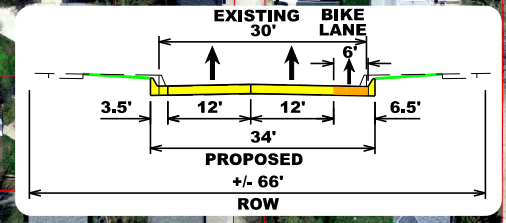
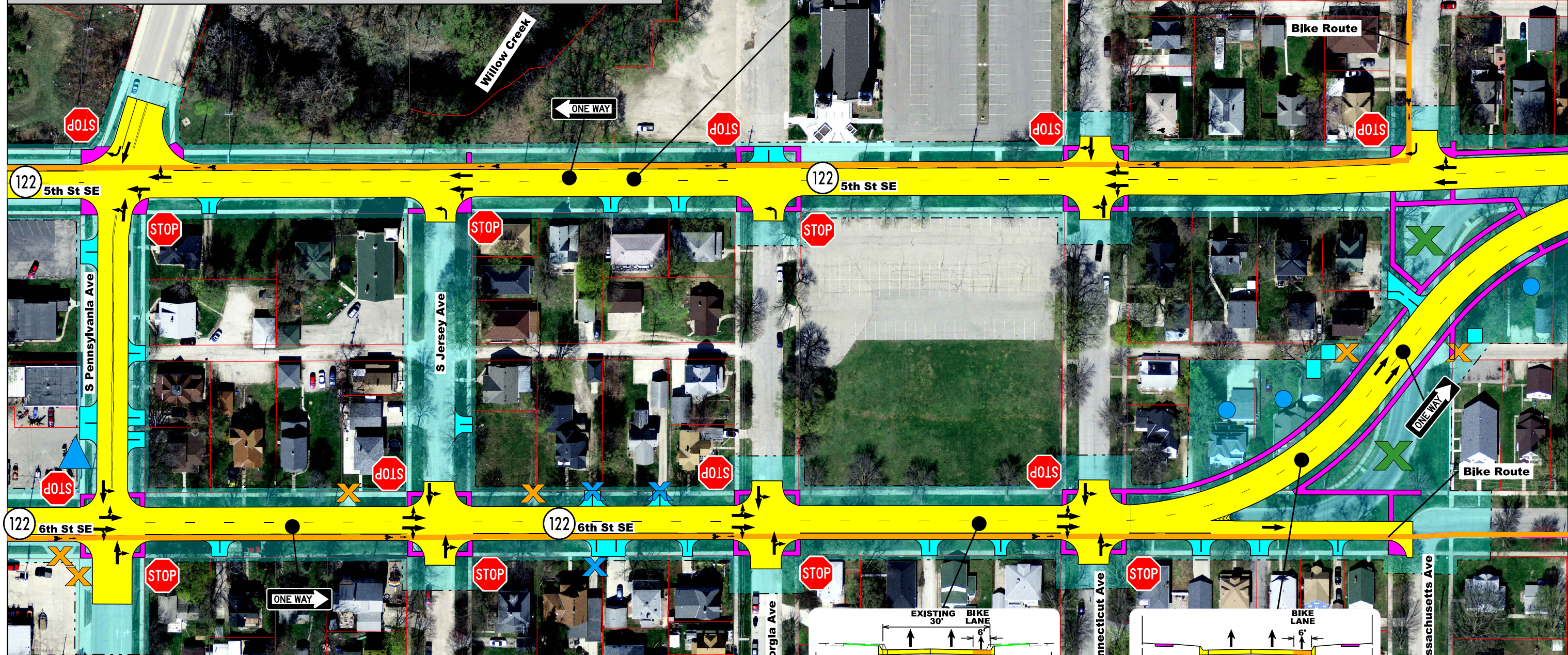
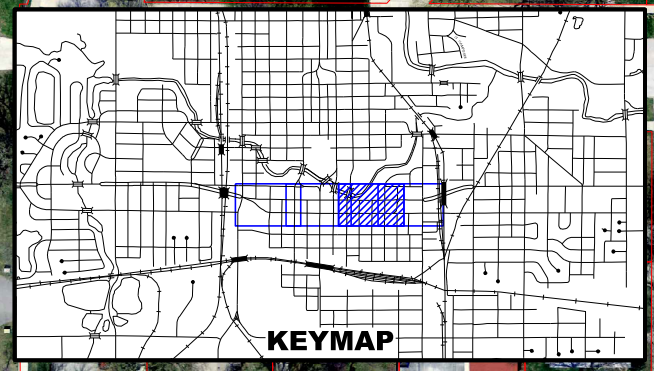
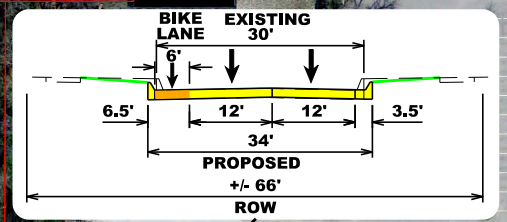
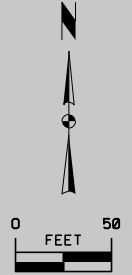


**PRELIMINARY PLAN**  
 NOT FINAL - SUBJECT TO CHANGE

- Sources:
1. Aerial Photography - Cerro Gordo County, Iowa, 2012
  2. Concept - HDR Engineering, December 2012
  3. Property Parcel Information- Cerro Gordo County Assessors Office, 2011

# LEGEND

-  PROPOSED 122 PAVEMENT
-  PROPOSED CONCRETE MEDIAN
-  PROPOSED DRIVEWAY
-  PROPOSED SIDEWALK
-  PROPOSED RETAINING WALL
-  NEPA CLEARED AREA
-  PROPERTY LINE
-  PROPOSED ACCESS CLOSURE
-  PROPOSED FUTURE ACCESS CLOSURE
-  PROPOSED ROAD CLOSURE
-  BUS STOP
-  STOP SIGN LOCATION
-  TRAFFIC SIGNAL LOCATION
-  POTENTIAL IMPACT
-  BIKE LANE / BIKE ROUTE



**PUBLIC INFORMATION MEETING**  
**OCTOBER 10, 2013**

122 Refined Alternative 1 - Bike Lanes - Sheet 3 of 4

IA 122 - Mason City  
 Alternative Analysis / Environmental Assessment
















PROJECT NUMBER: STP-122-1(14)--2C-17  
 CERRO GORDO, COUNTY

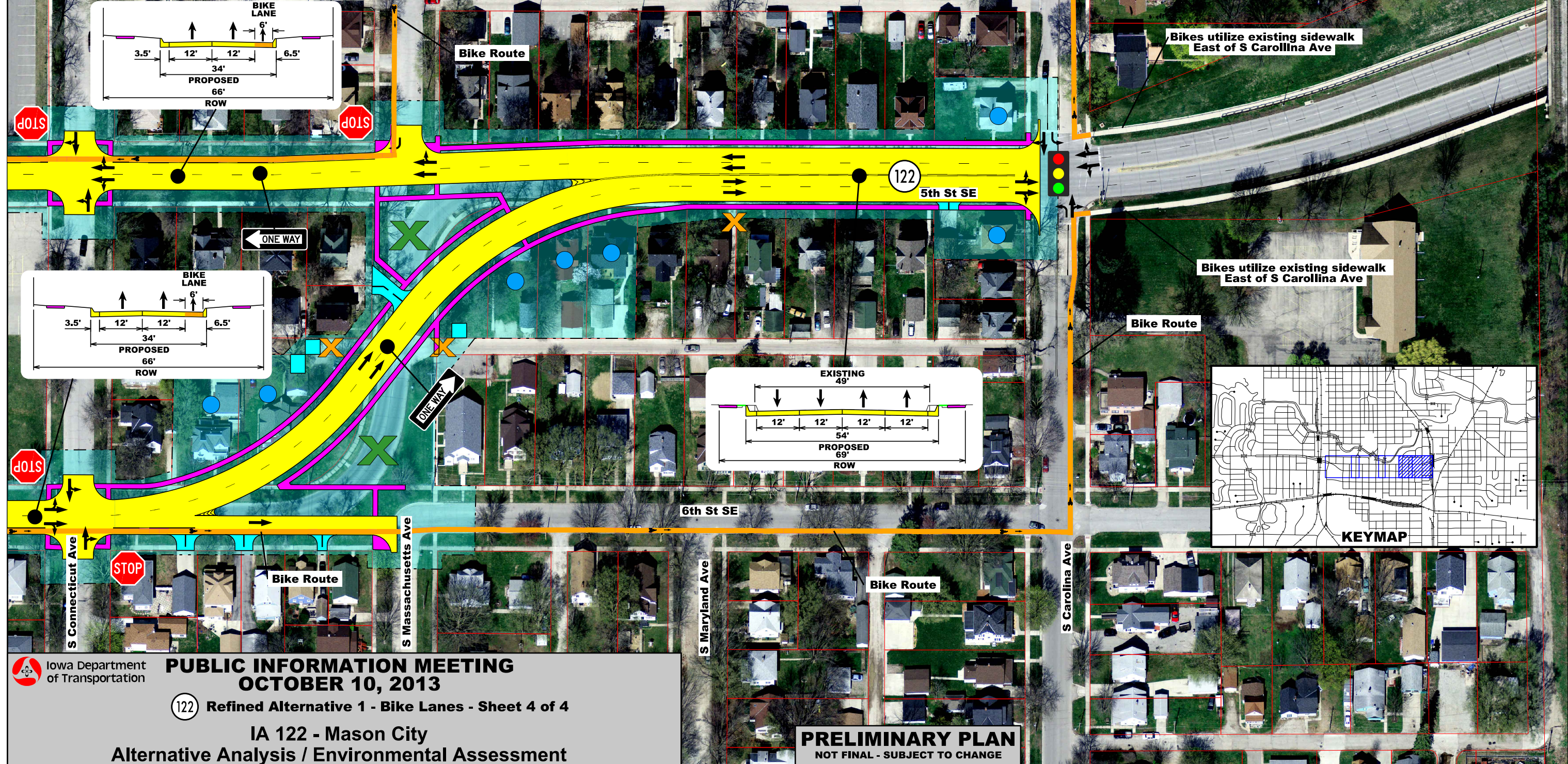
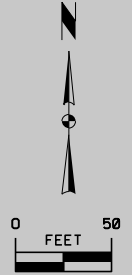


- Sources:**
1. Aerial Photography - Cerro Gordo County, Iowa, 2012
  2. Concept - HDR Engineering, December 2012
  3. Property Parcel Information- Cerro Gordo County Assessors Office, 2011

**PRELIMINARY PLAN**  
 NOT FINAL - SUBJECT TO CHANGE

# LEGEND

-  PROPOSED 122 PAVEMENT
-  PROPOSED CONCRETE MEDIAN
-  PROPOSED DRIVEWAY
-  PROPOSED SIDEWALK
-  PROPOSED RETAINING WALL
-  NEPA CLEARED AREA
-  PROPERTY LINE
-  PROPOSED ACCESS CLOSURE
-  PROPOSED FUTURE ACCESS CLOSURE
-  PROPOSED ROAD CLOSURE
-  BUS STOP
-  STOP SIGN LOCATION
-  TRAFFIC SIGNAL LOCATION
-  POTENTIAL IMPACT
-  BIKE LANE / BIKE ROUTE



**PUBLIC INFORMATION MEETING**  
**OCTOBER 10, 2013**

122 Refined Alternative 1 - Bike Lanes - Sheet 4 of 4

IA 122 - Mason City  
 Alternative Analysis / Environmental Assessment

PROJECT NUMBER: STP-122-1(14)--2C-17  
 CERRO GORDO, COUNTY



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